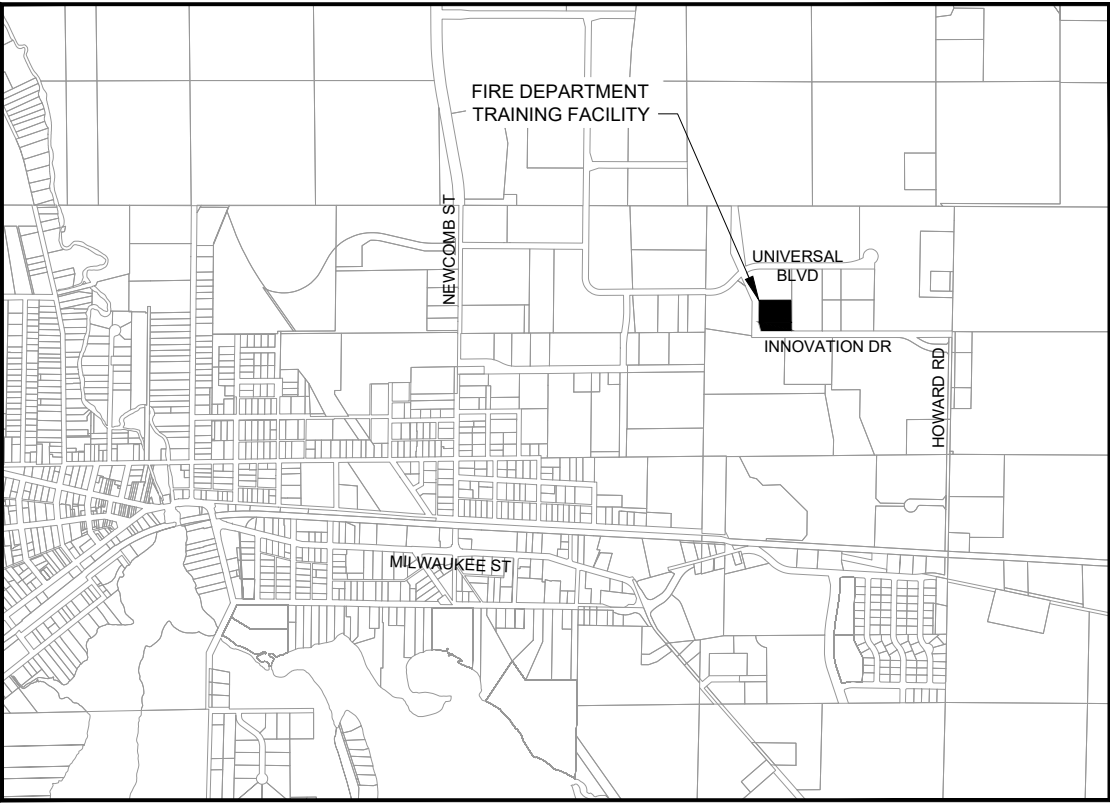
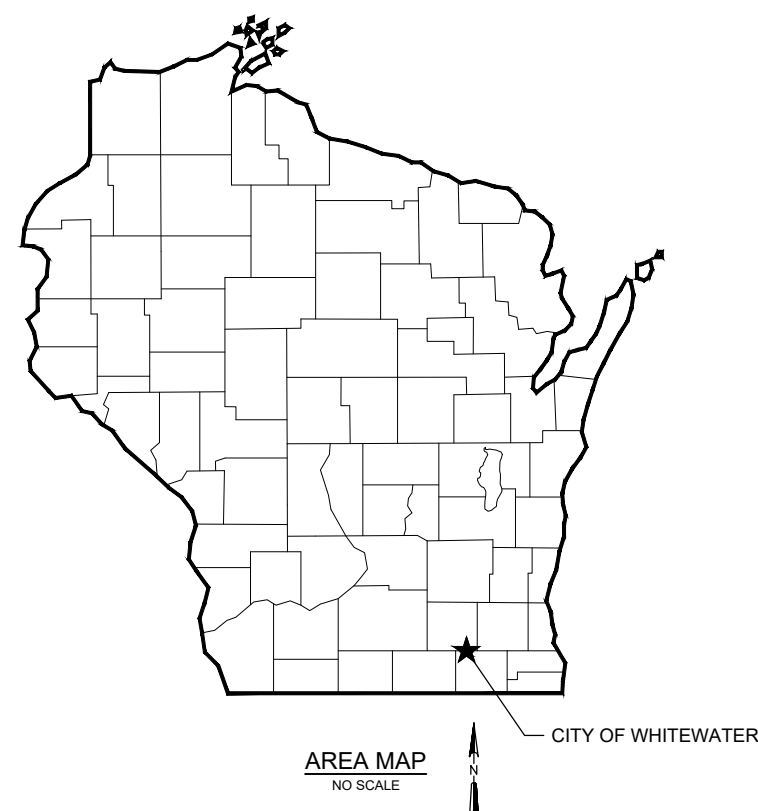


FIRE DEPARTMENT TRAINING FACILITY

FOR THE

CITY OF WHITEWATER

WALWORTH COUNTY, WISCONSIN



PROJECT LOCATION MAP
NO SCALE

LIST OF DRAWINGS		
SHEET NO.	DRAWING NO.	DRAWING TITLE
<u>GENERAL</u>		
1	G0.01	TITLE SHEET, LOCATION MAP, AND DRAWING LIST
2	G0.02	PROJECT CONTACTS AND LEGEND
3	G0.03	EROSION CONTROL NOTES AND DETAILS
<u>DEMOLITION</u>		
4	CD1.01	DEMOLITION PLAN
<u>CIVIL/STRUCTURAL</u>		
5	CS1.01	SITE AND UTILITY PLAN
6	CS1.02	EROSION CONTROL, RESTORATION, AND GRADING PLAN
7	CS4.01	ENLARGED FOUNDATION PLAN AND DETAILS
8	CS5.01	DETAILS AND SCHEDULES - 1
9	CS5.02	DETAILS AND SCHEDULES - 2

Strand Associates, Inc.
910 West Wingra Drive
Madison, WI 53715
608-251-4843
www.strand.com

CONTRACT 5-2025

ISSUED FOR OWNER REVIEW, OCTOBER 21, 2025



SHEET
1
G0.01

PROJECT CONTACTS

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CHARTER SPECTRUM - CABLE TV/FIBER



















CODY THOMPSON
cody.thompson@charter.com

LEGEND

EXISTING

	EXISTING RIGHT OF WAY
	EXISTING PROPERTY LINE
	EXISTING GAS LINE
	EXISTING SANITARY SEWER
	EXISTING WATER MAIN
	EXISTING STORM SEWER
	EXISTING TELEPHONE LINE
	EXISTING UNDERGROUND ELECTRIC LINE
	EXISTING OVERHEAD ELECTRIC LINE
	EXISTING FIBER OPTIC LINE
	EXISTING FENCE
	CONTROL POINT
	EXISTING BENCHMARK
	SOIL BORING
	EXISTING DECIDUOUS/CONIFEROUS TREE
	EXISTING TREE STUMP
	EXISTING MANHOLE
	EXISTING INLET
	EXISTING CATCH BASIN
	EXISTING POWER POLE
	EXISTING LIGHT POLE
	EXISTING SIGN
	EXISTING GAS VALVE
	EXISTING WATER VALVE
	EXISTING WATER MANHOLE
	EXISTING FIRE HYDRANT

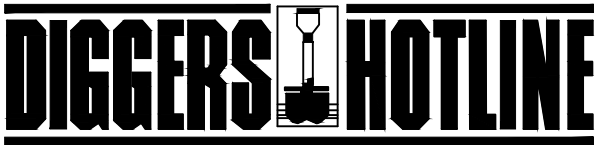
PROPOSED

	PROPOSED GAS LINE
	PROPOSED SANITARY SEWER
	PROPOSED WATER MAIN
	PROPOSED WATER MAIN W/ CASING
	PROPOSED STORM SEWER
	PROPOSED TELEPHONE LINE
	PROPOSED UNDERGROUND ELECTRIC LINE
	PROPOSED FIBER OPTIC LINE
	PROPOSED FENCE
	PROPOSED SILT FENCE
	PROPOSED SHEET PILING
	PROPOSED INLET PROTECTION
	PROPOSED END CAP
	PROPOSED WATER VALVE
	PROPOSED FIRE HYDRANT
	PROPOSED SANITARY SEWER MANHOLE
	PROPOSED STORM SEWER MANHOLE
	PROPOSED INLET

BENCHMARK TABLE				
BM NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
BM 100	TOP OF NUT OF FIRE HYDRANT	426306.82	719273.48	826.82
BM 101	TOP OF NUT OF FIRE HYDANT	426298.63	718990.43	823.81
BM 102	TOP OF NUT OF FIRE HYDRANT	426619.23	718936.06	829.25

CONTROL AND BENCHMARK TABLE				
BM NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
CP 5	MAG NAIL IN ASPHALT	426256.14	718909.07	819.25
CP 10	MAG NAIL IN ASPHALT	426681.58	718914.81	827.29

DIGGERS HOTLINE CONTACT



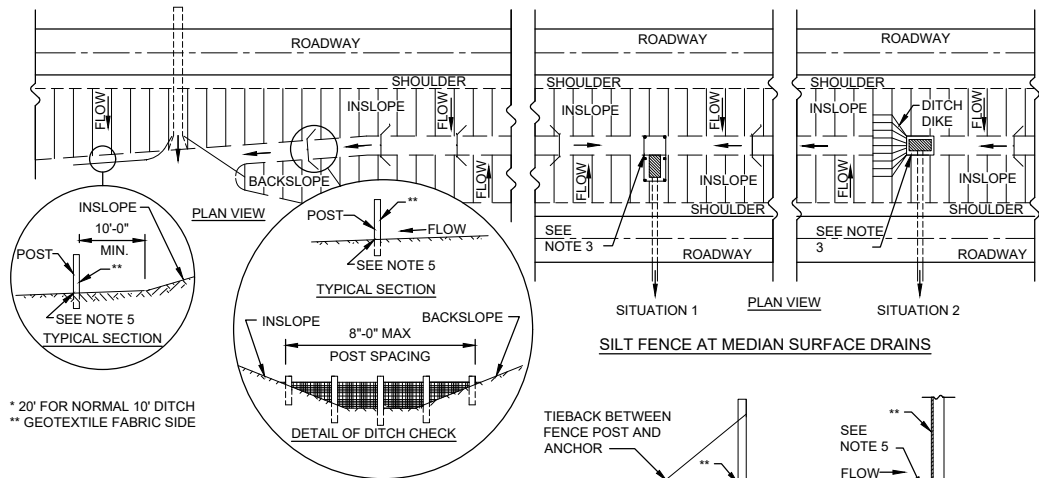
Toll Free (800) 242-8511
Milwaukee Area (414) 259-1181
Hearing Impaired TDD (800) 542-2289
www.DiggersHotline.com

GENERAL
PROJECT CONTACTS AND LEGEND
FIRE DEPARTMENT TRAINING FACILITY
FOR THE
CITY OF WHITEWATER
WHITEWATER, WI

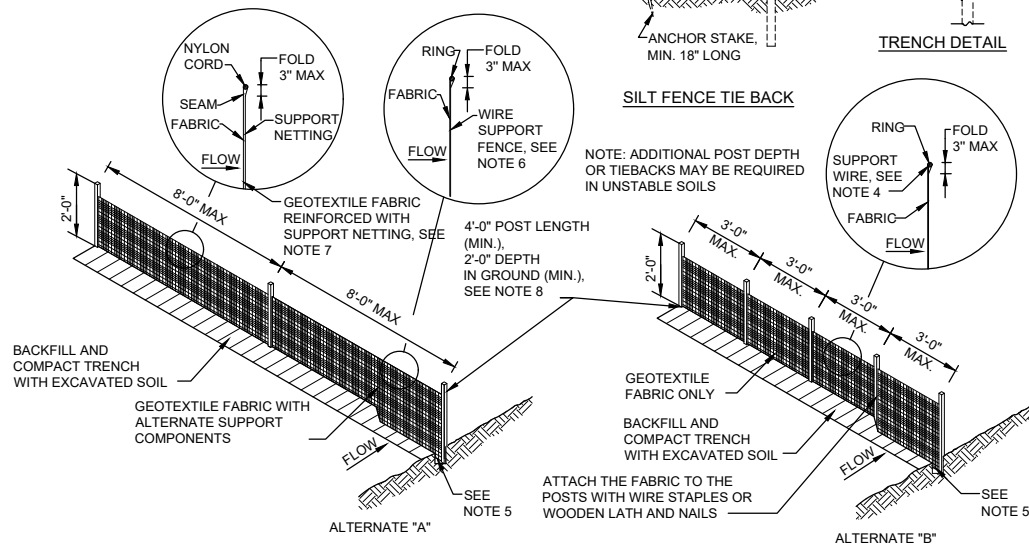
JOB NO.
1407.152
PROJECT MGR.
EJC



SHEET
2
G0.02

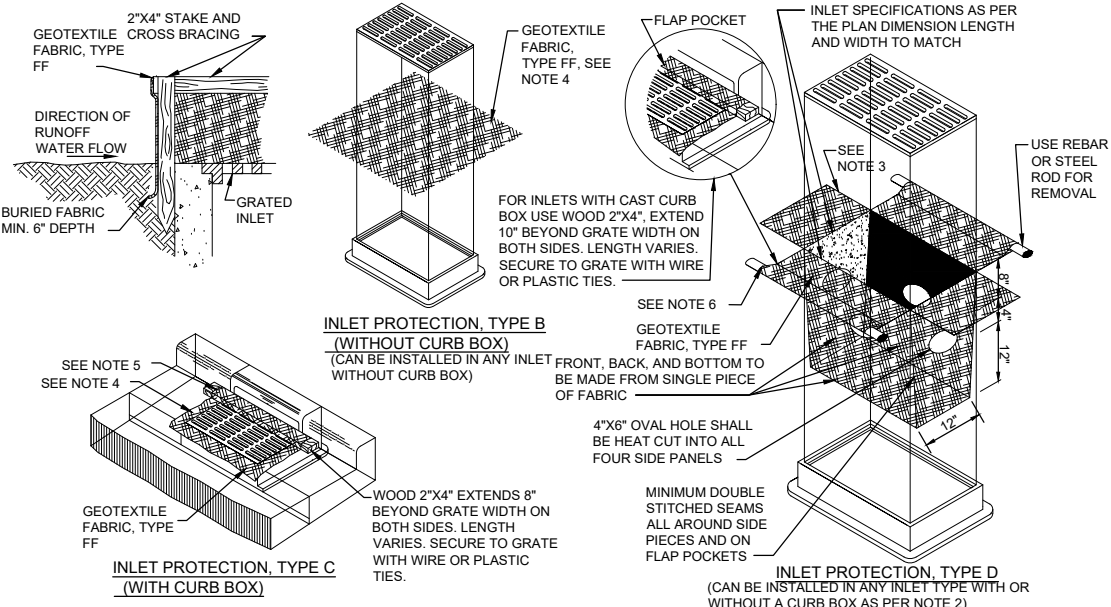


TYPICAL APPLICATIONS OF SILT FENCE



- NOTES:
1. DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.
 2. WHEN POSSIBLE THE SILT FENCE SHALL BE CONSTRUCTED IN AN ARC OR HORSESHOE SHAPE, WITH THE ENDS POINTING UPSLOPE TO MAXIMIZE BOTH STRENGTH AND EFFECTIVENESS.
 3. CROSS BRACE WITH 2"x4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
 4. MINIMUM 14 GAGE WIRE REQUIRED, FOLD FABRIC 3" OVER THE WIRE AND STAPLE OR PLACE WIRE RINGS ON 12" C-C.
 5. EXCAVATE TRENCH A MINIMUM OF 4" WIDE AND 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC; FOLD MATERIAL TO FIT TRENCH AND BACKFILL AND COMPACT TRENCH WITH EXCAVATED SOIL.
 6. WIRE SUPPORT FENCE SHALL BE 14 GAGE MINIMUM WOVEN WIRE WITH A MAXIMUM MESH SPACING OF 6". SECURE TOP OF GEOTEXTILE FABRIC TO TOP OF FENCE WITH STAPLES OR WIRE RINGS AT 12" C TO C.
 7. GEOTEXTILE FABRIC SHALL BE REINFORCED WITH AN INDUSTRIAL POLYPROPYLENE NETTING WITH A MAXIMUM MESH SPACING OF 1/4" OR EQUAL. A HEAVY DUTY NYLON TOP SUPPORT CORD OR EQUIVALENT IS REQUIRED.
 8. STEEL POSTS SHALL BE STUDDED "TEE" OR "U" TYPE WITH A MINIMUM WEIGHT OF 1.2 LBS/LINEAR FOOT WITHOUT ANCHORS, OR ANCHORS SUFFICIENT TO RESIST POST MOVEMENT ARE REQUIRED. WOOD POSTS SHALL BE A MINIMUM SIZE OF 4" DIAMETER, OR 2 1/2"x3 1/2", EXCEPT WOOD POSTS FOR GEOTEXTILE FABRIC REINFORCED WITH NETTING SHALL BE A MINIMUM SIZE OF 1 1/8"x1 1/8" OAK OR HICKORY.
 9. ALTERNATES "A" AND "B" ARE EQUAL AND EITHER MAY BE USED.

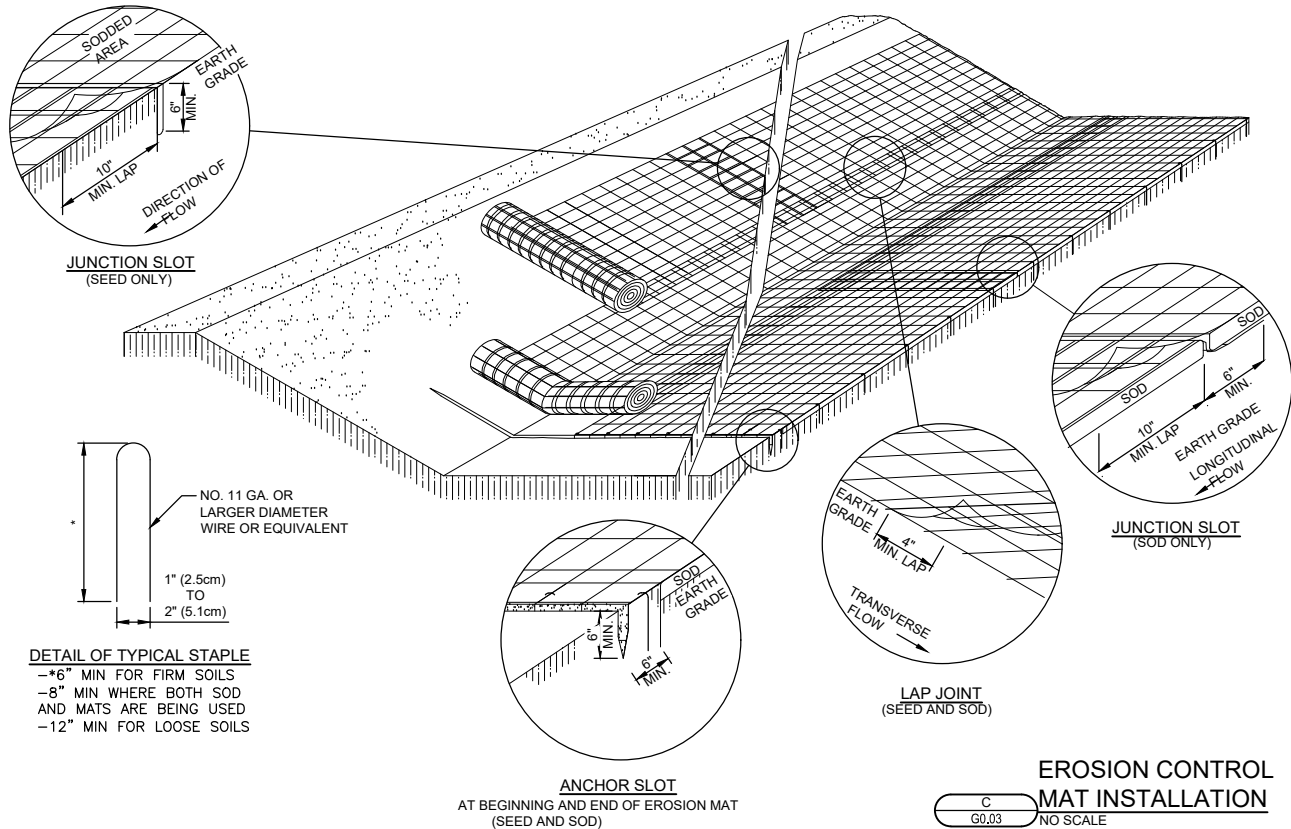
A SILT FENCE
G0.03 NO SCALE



NOTES:

1. MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.
2. WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET, ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.
3. FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
4. FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
5. FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.
6. FOR TYPES B AND C, TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE. THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.
7. FOR TYPE D, DO NOT INSTALL INLET PROTECTION TYPE D INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE. TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE. THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY, THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES, TO ACHIEVE THE 3" CLEARANCE.

B INLET PROTECTION
G0.03 TYPE A, B, C, AND D
NO SCALE



EROSION MAT OVER SOD

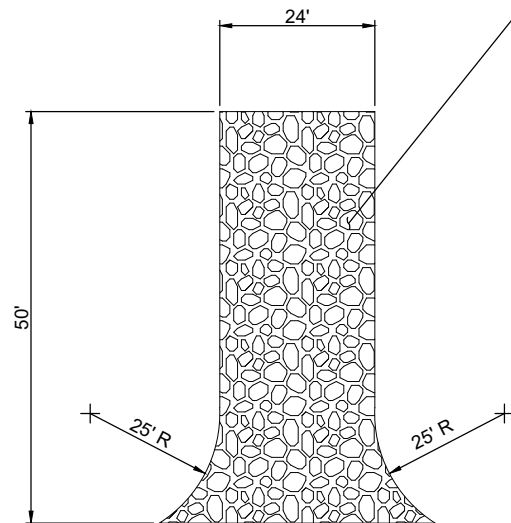
1. ONLY JUTE FABRIC WILL BE PERMITTED OVER SOD.
2. FLOOD STAKES FOR SOD MAY BE OMITTED IF THE EXISTING SLOPE AND SOIL CONDITIONS SO PERMIT.
3. THE WIDTH OF EROSION MAT SHALL ALWAYS EQUAL THE SOD WIDTH.
4. SOD STRIPS MAY BE PLACED EITHER LONGITUDINALLY OR TRANSVERSELY TO THE FLOW LINE OF THE DITCH.

EROSION MAT OVER SEEDING

- JUNCTION OR ANCHOR SLOTS SHALL BE AT MINIMUM INTERVALS OF 100 FEET ON GRADES UP TO AND INCLUDING 3 PERCENT, AND 50 FEET ON GRADES EXCEEDING 3 PERCENT.

- NOTES:
1. DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
 2. VARIATIONS IN THE DIMENSIONS OR MATERIALS SHOWN HEREON SHALL BE PERMITTED IF THEY PROVIDE EQUIVALENT PROTECTION AND MATERIAL STRENGTH.
 3. LAP JOINTS SHALL NOT BE PLACED IN THE BOTTOM OF V-SHAPED DITCHES.
 4. JUNCTION SLOTS ON ADJACENT STRIPS OF MATTING SHALL BE STAGGERED A MINIMUM OF 4 FEET APART.
 5. EDGES OF THE EROSION MAT SHALL BE IMPRESSED IN THE SOIL.
 6. EROSION MAT SHALL BE MEASURED AND PAID FOR IN ACCORDANCE WITH THE SPECIFICATIONS.

6" WISDOT GRADATION 2 COURSE AGGREGATE, UNDERLAIN WITH 6" WISDOT GRADATION 1 COURSE AGGREGATE, UNDERLAIN WITH GEOTEXTILE FABRIC.



D STONE TRACKING PAD FOR CONSTRUCTION ENTRANCE
G0.03 NO SCALE

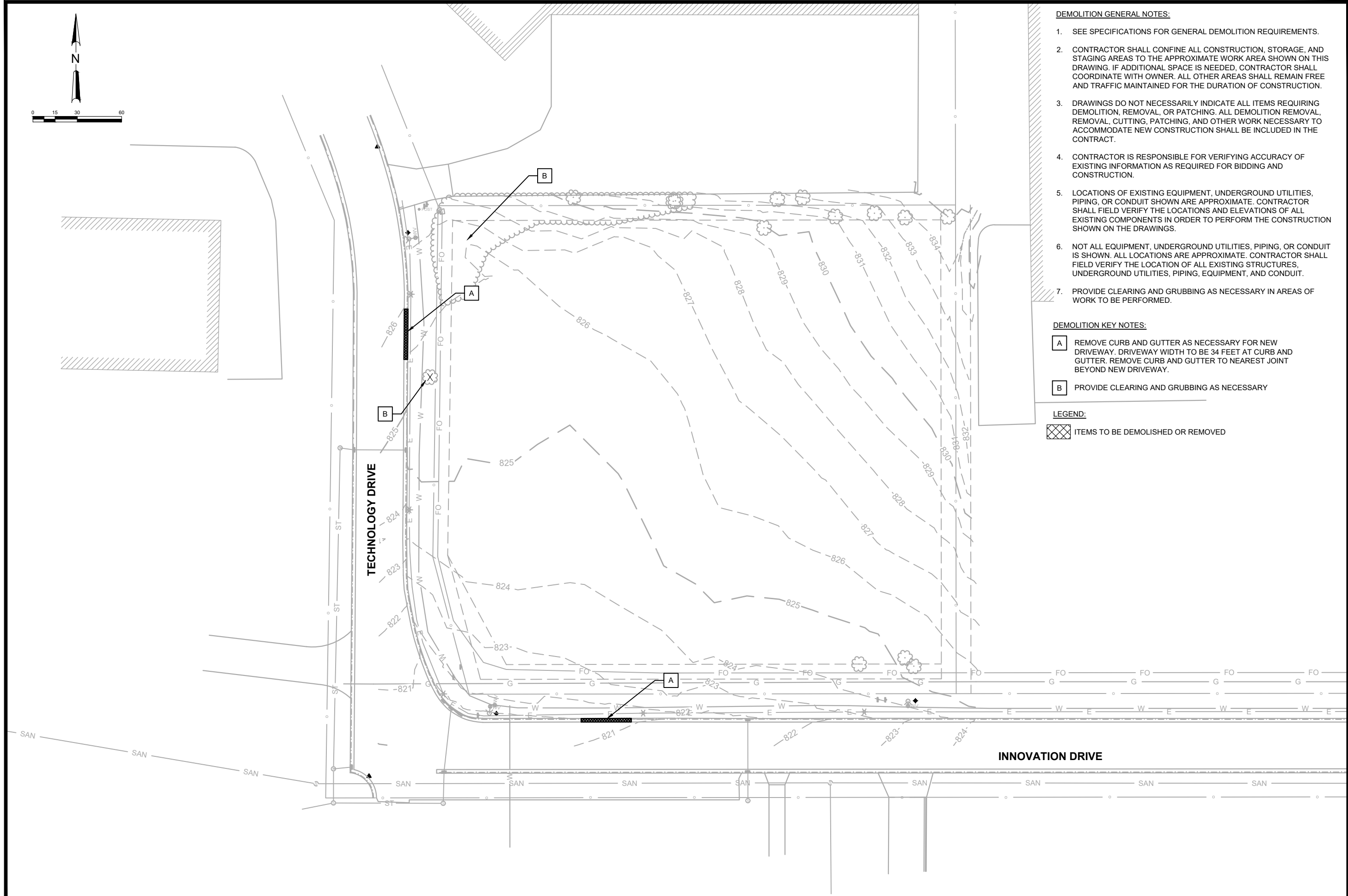
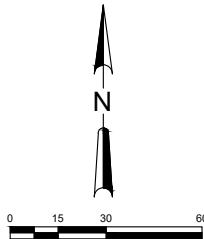
GENERAL
EROSION CONTROL NOTES AND DETAILS

FIRE DEPARTMENT TRAINING FACILITY
FOR THE
CITY OF WHITEWATER
WHITEWATER, WI

JOB NO.
1407.152
PROJECT MGR.
EJC



SHEET
3
G0.03



- DEMOLITION GENERAL NOTES:**
1. SEE SPECIFICATIONS FOR GENERAL DEMOLITION REQUIREMENTS.
 2. CONTRACTOR SHALL CONFINE ALL CONSTRUCTION, STORAGE, AND STAGING AREAS TO THE APPROXIMATE WORK AREA SHOWN ON THIS DRAWING. IF ADDITIONAL SPACE IS NEEDED, CONTRACTOR SHALL COORDINATE WITH OWNER. ALL OTHER AREAS SHALL REMAIN FREE AND TRAFFIC MAINTAINED FOR THE DURATION OF CONSTRUCTION.
 3. DRAWINGS DO NOT NECESSARILY INDICATE ALL ITEMS REQUIRING DEMOLITION, REMOVAL, OR PATCHING. ALL DEMOLITION REMOVAL, REMOVAL, CUTTING, PATCHING, AND OTHER WORK NECESSARY TO ACCOMMODATE NEW CONSTRUCTION SHALL BE INCLUDED IN THE CONTRACT.
 4. CONTRACTOR IS RESPONSIBLE FOR VERIFYING ACCURACY OF EXISTING INFORMATION AS REQUIRED FOR BIDDING AND CONSTRUCTION.
 5. LOCATIONS OF EXISTING EQUIPMENT, UNDERGROUND UTILITIES, PIPING, OR CONDUIT SHOWN ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF ALL EXISTING COMPONENTS IN ORDER TO PERFORM THE CONSTRUCTION SHOWN ON THE DRAWINGS.
 6. NOT ALL EQUIPMENT, UNDERGROUND UTILITIES, PIPING, OR CONDUIT IS SHOWN. ALL LOCATIONS ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY THE LOCATION OF ALL EXISTING STRUCTURES, UNDERGROUND UTILITIES, PIPING, EQUIPMENT, AND CONDUIT.
 7. PROVIDE CLEARING AND GRUBBING AS NECESSARY IN AREAS OF WORK TO BE PERFORMED.

- DEMOLITION KEY NOTES:**
- A** REMOVE CURB AND GUTTER AS NECESSARY FOR NEW DRIVEWAY. DRIVEWAY WIDTH TO BE 34 FEET AT CURB AND GUTTER. REMOVE CURB AND GUTTER TO NEAREST JOINT BEYOND NEW DRIVEWAY.
- B** PROVIDE CLEARING AND GRUBBING AS NECESSARY

LEGEND:

ITEMS TO BE DEMOLISHED OR REMOVED

**DEMOLITION
DEMOLITION PLAN**

**FIRE DEPARTMENT TRAINING FACILITY
FOR THE
CITY OF WHITEWATER
WHITEWATER, WI**

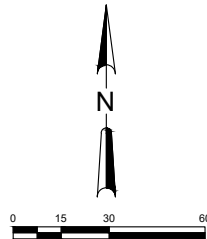
**JOB NO.
1407.152**

**PROJECT MGR.
EJC**



**SHEET
4
CD1.01**

NO.	REVISIONS	DATE:
A	OWNER REVIEW	10/21/2025

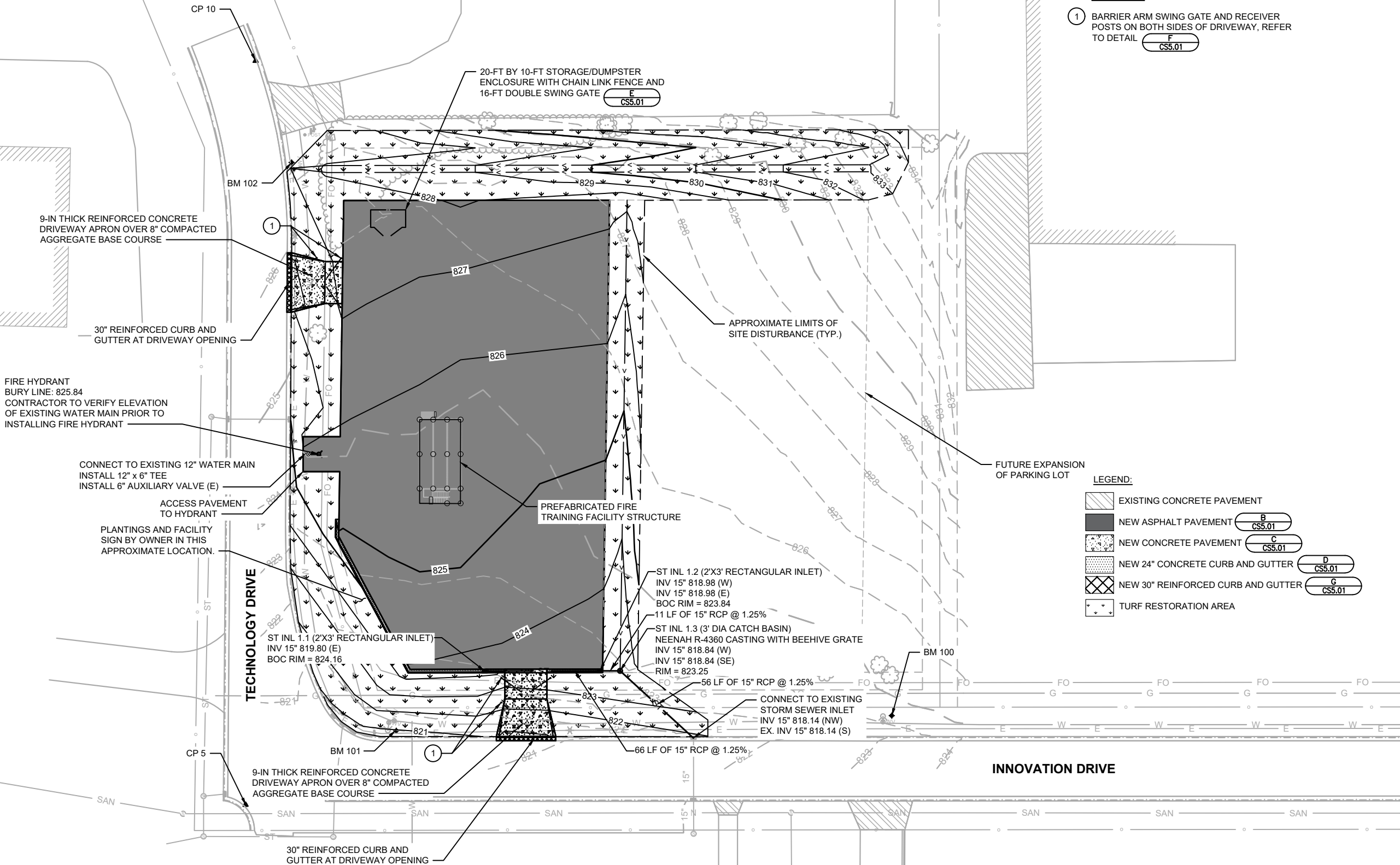


GENERAL NOTES:

1. SEE **A** **CS5.02** FOR INLET DETAILS.
2. REFER TO DRAWING CS4.01 FOR FOUNDATION DETAILS FOR PREFABRICATED FIRE TRAINING FACILITY STRUCTURE.

KEY NOTES:

1. BARRIER ARM SWING GATE AND RECEIVER POSTS ON BOTH SIDES OF DRIVEWAY, REFER TO DETAIL **F** **CS5.01**



LEGEND:

- EXISTING CONCRETE PAVEMENT
- NEW ASPHALT PAVEMENT **B** **CS5.01**
- NEW CONCRETE PAVEMENT **C** **CS5.01**
- NEW 24" CONCRETE CURB AND GUTTER **D** **CS5.01**
- NEW 30" REINFORCED CURB AND GUTTER **G** **CS5.01**
- TURF RESTORATION AREA

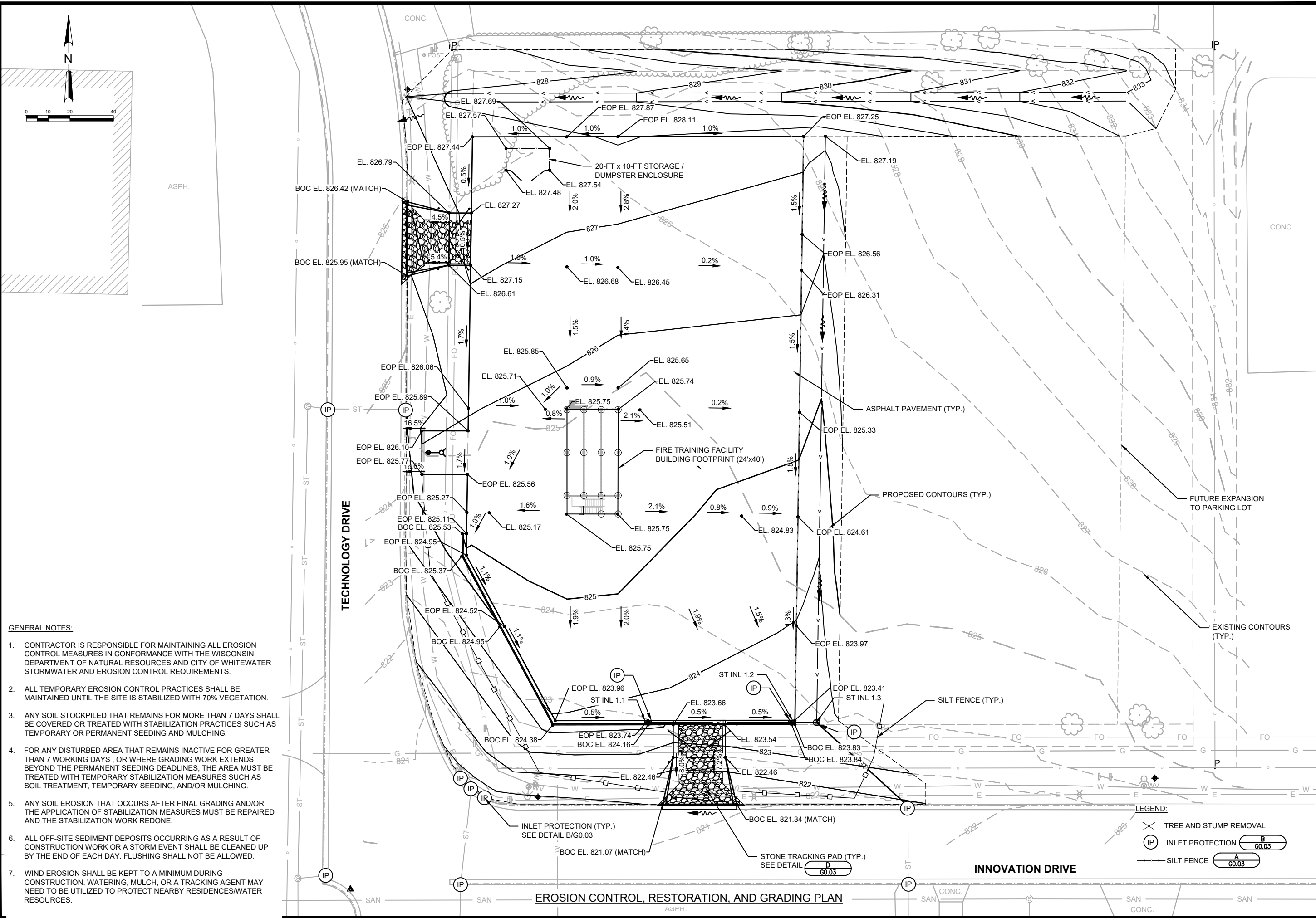
SITE AND UTILITY PLAN

CIVIL/STRUCTURAL
SITE AND UTILITY PLAN
FIRE DEPARTMENT TRAINING FACILITY
FOR THE
CITY OF WHITEWATER
WHITEWATER, WI

JOB NO.
1407.152
PROJECT MGR.
EJC



SHEET
5
CS1.01



GENERAL NOTES:

1. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ALL EROSION CONTROL MEASURES IN CONFORMANCE WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES AND CITY OF WHITEWATER STORMWATER AND EROSION CONTROL REQUIREMENTS.
2. ALL TEMPORARY EROSION CONTROL PRACTICES SHALL BE MAINTAINED UNTIL THE SITE IS STABILIZED WITH 70% VEGETATION.
3. ANY SOIL STOCKPILED THAT REMAINS FOR MORE THAN 7 DAYS SHALL BE COVERED OR TREATED WITH STABILIZATION PRACTICES SUCH AS TEMPORARY OR PERMANENT SEEDING AND MULCHING.
4. FOR ANY DISTURBED AREA THAT REMAINS INACTIVE FOR GREATER THAN 7 WORKING DAYS, OR WHERE GRADING WORK EXTENDS BEYOND THE PERMANENT SEEDING DEADLINES, THE AREA MUST BE TREATED WITH TEMPORARY STABILIZATION MEASURES SUCH AS SOIL TREATMENT, TEMPORARY SEEDING, AND/OR MULCHING.
5. ANY SOIL EROSION THAT OCCURS AFTER FINAL GRADING AND/OR THE APPLICATION OF STABILIZATION MEASURES MUST BE REPAIRED AND THE STABILIZATION WORK REDONE.
6. ALL OFF-SITE SEDIMENT DEPOSITS OCCURRING AS A RESULT OF CONSTRUCTION WORK OR A STORM EVENT SHALL BE CLEANED UP BY THE END OF EACH DAY. FLUSHING SHALL NOT BE ALLOWED.
7. WIND EROSION SHALL BE KEPT TO A MINIMUM DURING CONSTRUCTION. WATERING, MULCH, OR A TRACKING AGENT MAY NEED TO BE UTILIZED TO PROTECT NEARBY RESIDENCES/WATER RESOURCES.

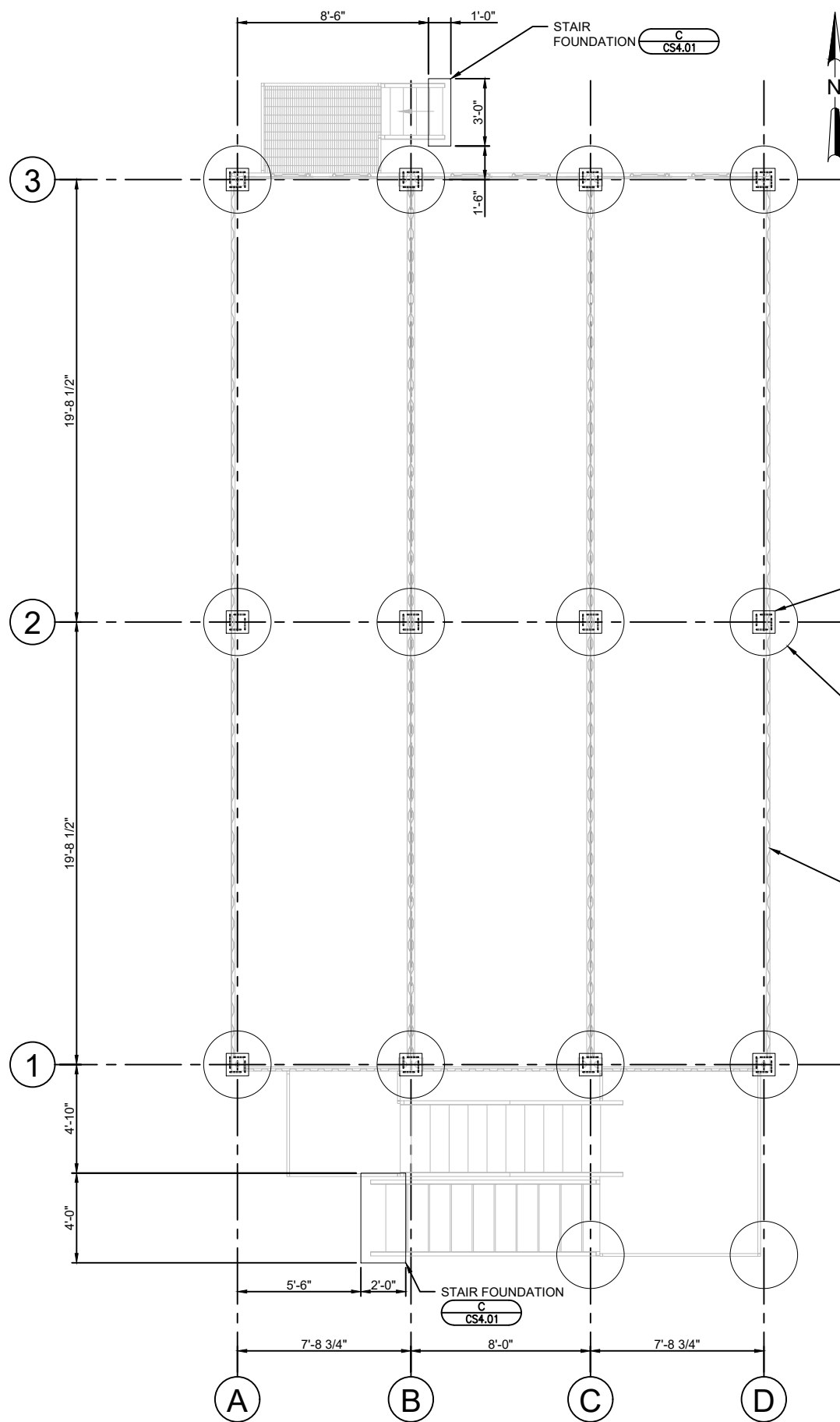
NO.	REVISIONS	DATE:
A	OWNER REVIEW	10/21/2025

CIVIL/STRUCTURAL
EROSION CONTROL, RESTORATION, AND GRADING PLAN
FIRE DEPARTMENT TRAINING FACILITY
FOR THE
CITY OF WHITEWATER
WHITEWATER, WI

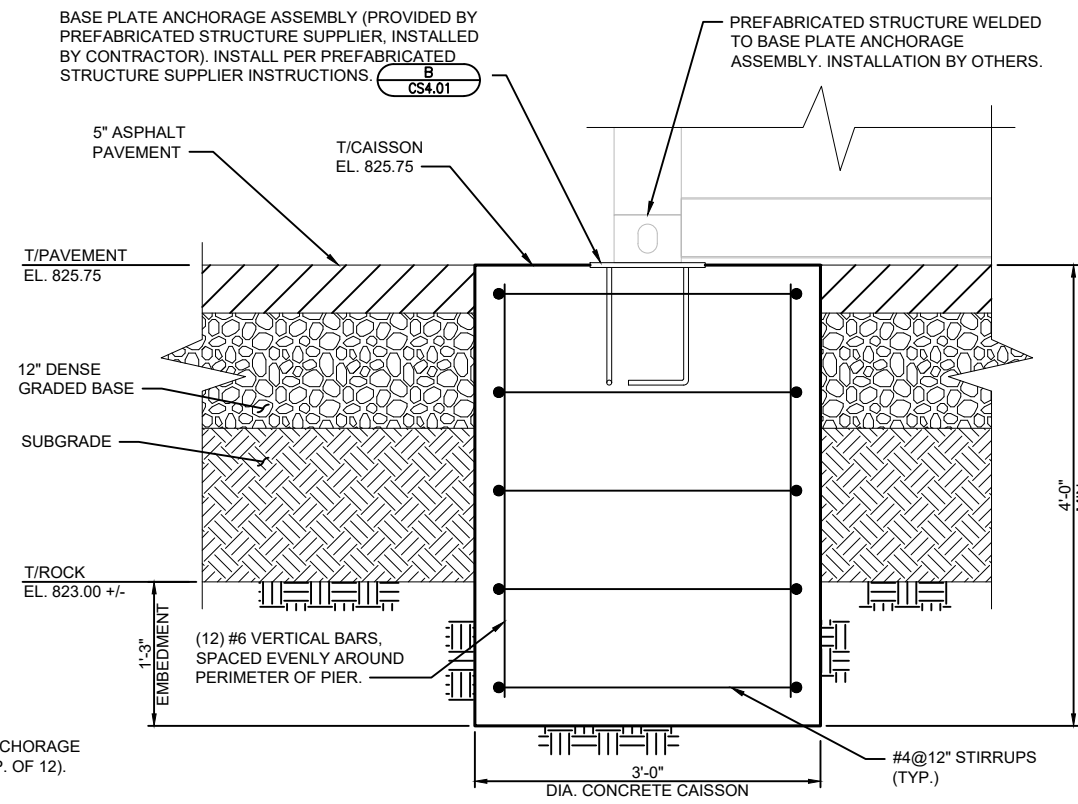
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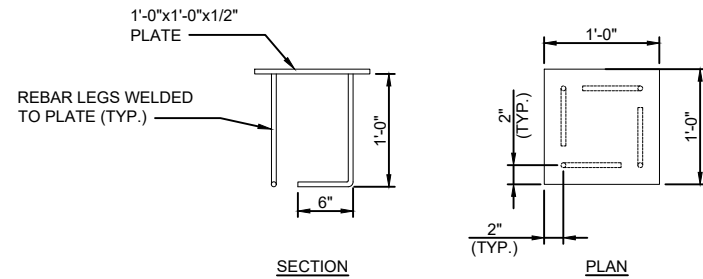
SHEET
6
CS1.02



FOUNDATION PLAN
NO SCALE

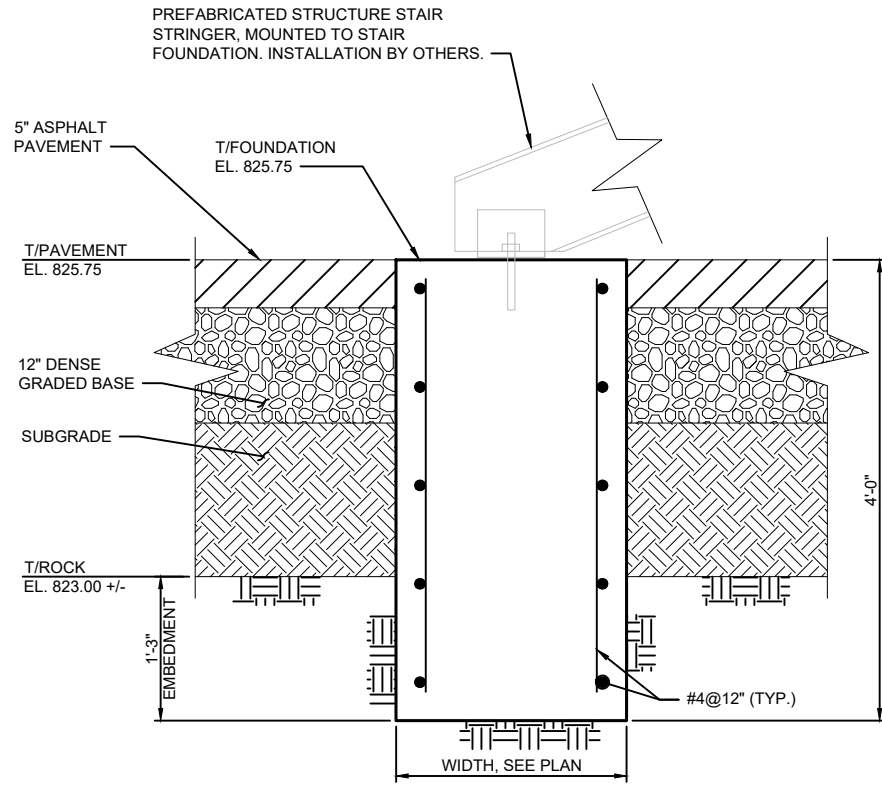


CONCRETE CAISSON DETAIL
NO SCALE



NOTES:
1. BASE PLATE ANCHORAGE ASSEMBLY TO BE PROVIDED BY PREFABRICATED BUILDING SUPPLIER. CONFIRM FINAL DIMENSIONS AND SIZES WITH SUPPLIER.
2. PLATES TO BE SET 1/8" ABOVE TOP OF CONCRETE. CONTRACTOR TO CONFIRM WITH PREFABRICATED BUILDING SUPPLIER.

BASE PLATE ANCHORAGE ASSEMBLY
NO SCALE



STAIR FOUNDATION
NO SCALE

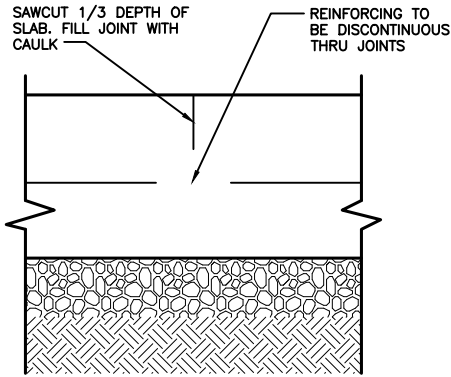
- GENERAL NOTES:
1. ROCK REMOVAL FOR STAIR FOUNDATIONS SHALL BE PAID BY THE CUBIC YARD UNDER BID ITEM ROCK EXCAVATION FOR STRUCTURES AND ROADS.
 2. ROCK REMOVAL FOR CONCRETE CAISSONS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR FIRE TRAINING FACILITY CONCRETE CAISSON FOUNDATIONS.
 3. FINAL LOCATION OF FOUNDATIONS FOR PREFABRICATED STRUCTURE AND STAIRS SHALL BE CONFIRMED BY SUPPLIER PRIOR TO CONSTRUCTION.

CIVIL/STRUCTURAL
ENLARGED FOUNDATION PLAN AND DETAILS
FIRE DEPARTMENT TRAINING FACILITY
FOR THE
CITY OF WHITEWATER
WHITEWATER, WI

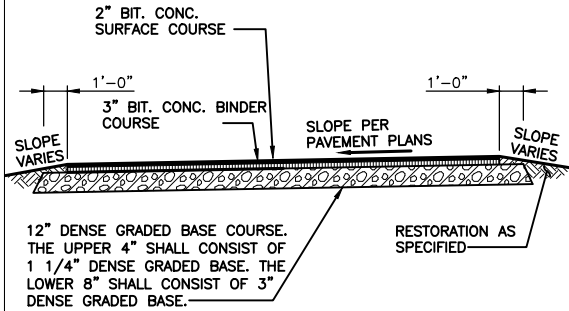
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EJC



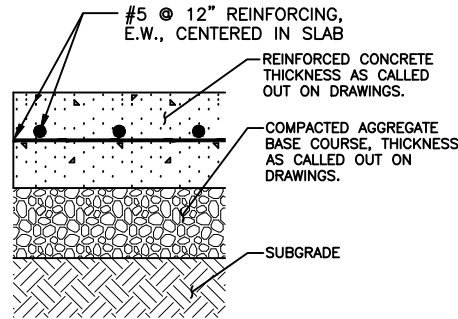
SHEET
7
CS4.01



A SLAB ON GRADE CONTROL JOINT
CS5.01 NO SCALE

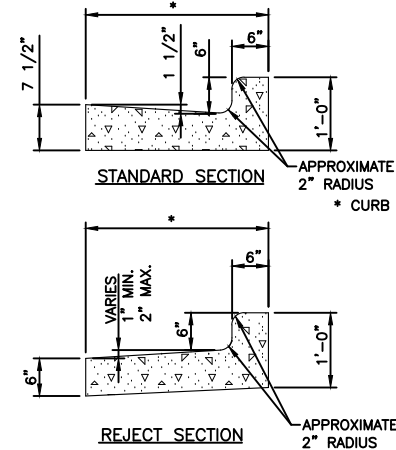


B ASPHALT PAVEMENT SECTION
CS5.01 NO SCALE



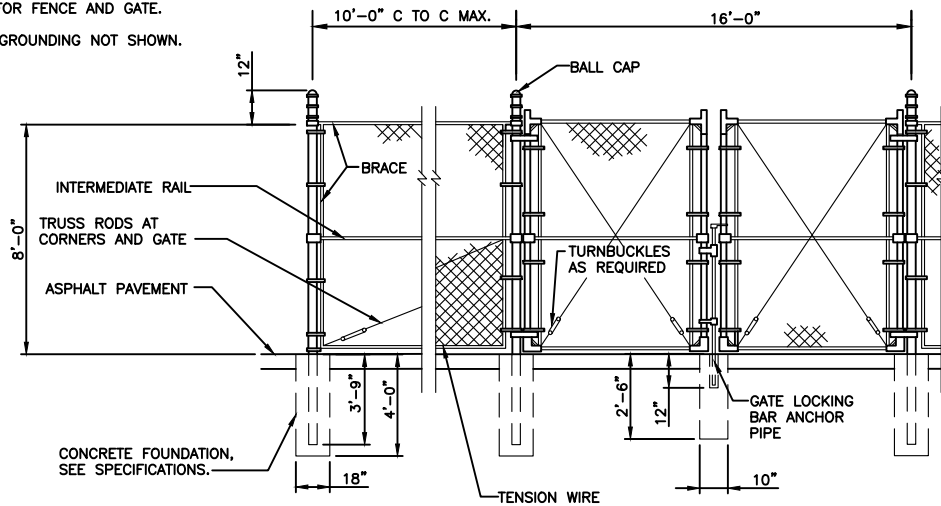
NOTES:
PROVIDE CONTROL JOINTS AT APPROXIMATELY 10 FT. ON CENTER PER

C CONCRETE APRON PAVEMENT
CS5.01 NO SCALE

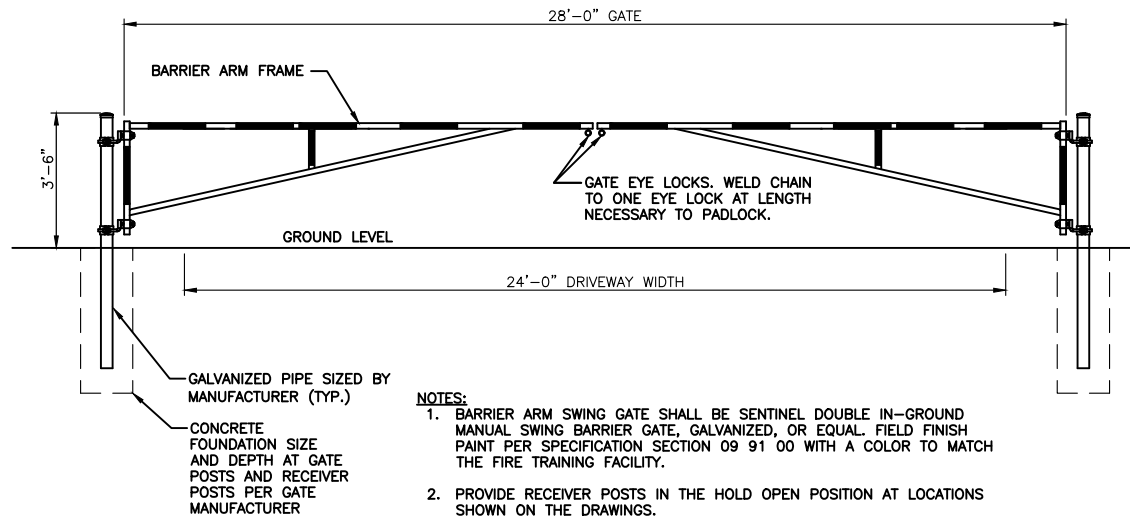


D CONCRETE CURB AND GUTTER
CS5.01 NO SCALE

NOTES:
1. PROVIDE PVC PRIVACY SLATS FOR FENCE AND GATE.
2. GROUNDING NOT SHOWN.

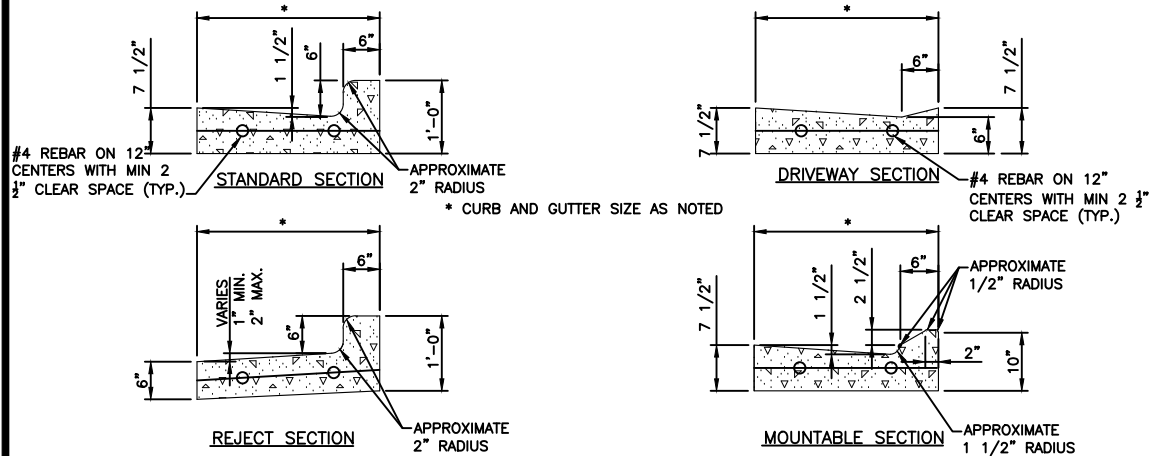


E CHAIN LINK FENCE AND SWING GATE
CS5.01 NO SCALE



NOTES:
1. BARRIER ARM SWING GATE SHALL BE SENTINEL DOUBLE IN-GROUND MANUAL SWING BARRIER GATE, GALVANIZED, OR EQUAL. FIELD FINISH PAINT PER SPECIFICATION SECTION 09 91 00 WITH A COLOR TO MATCH THE FIRE TRAINING FACILITY.
2. PROVIDE RECEIVER POSTS IN THE HOLD OPEN POSITION AT LOCATIONS SHOWN ON THE DRAWINGS.

F BARRIER ARM GATE
CS5.01 NO SCALE



G REINFORCED CONCRETE CURB AND GUTTER
CS5.01 NO SCALE

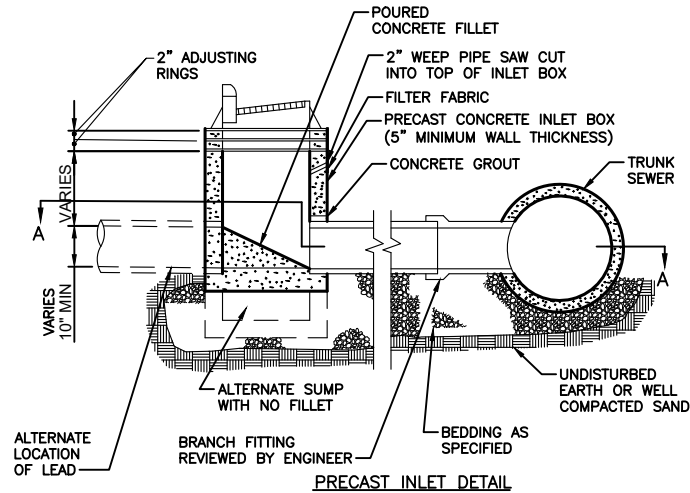
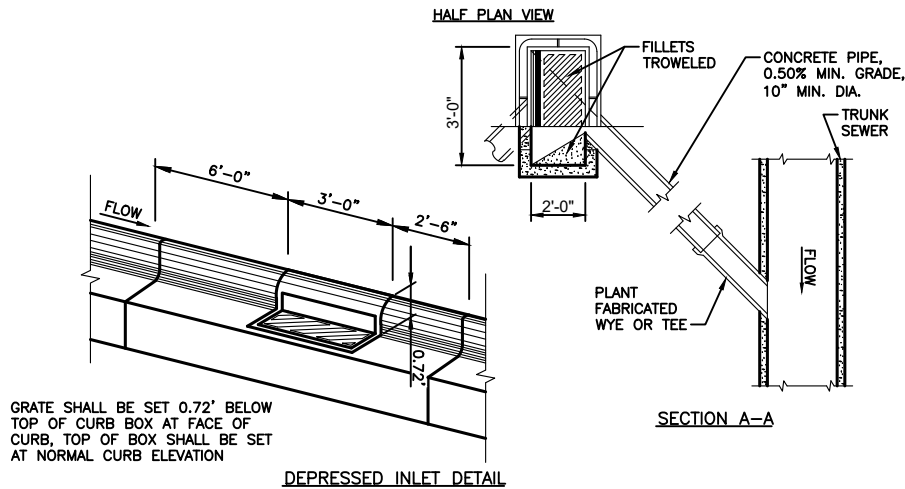
DATE:	10/21/2025
REVISIONS	
NO.	A
OWNER REVIEW	

**CIVIL/STRUCTURAL
DETAILS AND SCHEDULES - 2**
FIRE DEPARTMENT TRAINING FACILITY
FOR THE
CITY OF WHITEWATER
WHITEWATER, WI

JOB NO.
1407.152
PROJECT MGR.
EJC



SHEET
8
CS5.01



A
CS5.02

STORM SEWER INLETS
NO SCALE

NOTES:

1. DETAILS RELATIVE TO ITEMS SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE SPECIFICATIONS.
2. VARIATIONS IN DIMENSIONS AND DESIGN MAY BE PERMISSIBLE PROVIDING EQUIVALENT CAPACITY AND STRENGTH ARE ATTAINED.
3. WHEN ANY STRUCTURE IS CONSTRUCTED OF CONCRETE, CONCRETE BLOCK OR REINFORCED CONCRETE CULVERT PIPE, THE TOP OF THE MASONRY SHALL BE LEFT SUFFICIENTLY LOW TO PERMIT PROPER ADJUSTMENT OF COVER TO GRADE.
4. REINFORCED PRECAST FLAT SLAB SHALL BE USED IN LIEU OF PRECAST ECCENTRIC CONE WHEN SHOWN ON THE DRAWINGS AND SHALL BE DESIGNED FOR H-20 TRUCK LOADING.
5. PRECAST REINFORCED CONCRETE MANHOLE RISERS AND TOPS SHALL CONFORM TO ASTM C-478. JOINTS BETWEEN MANHOLE SECTIONS SHALL BE SEALED WITH MORTAR, AND HANDLING HOLES SHALL BE FILLED WITH MORTAR, AND BOTH MADE WATER TIGHT.
6. STEPS SHALL BE INSTALLED IN ALL MANHOLES.
7. AT ALL BENDS IN SEWER, A SMOOTH RADIUS FLOW LINE SHALL BE PROVIDED IN MANHOLES. ALL CONCRETE FILLETS SHALL BE HAND troweled.
8. INSIDE DIMENSIONS FOR MANHOLES: USE MINIMUM 4' DIAMETER FOR SEWER LESS THAN 18" IN DIAMETER; USE MINIMUM 5' DIAMETER FOR SEWER 18" THRU 24" IN DIAMETER; USE MINIMUM 6' DIAMETER OR MINIMUM 6' SQUARE FOR SEWER OVER 24" IN DIAMETER, UNLESS OTHERWISE SPECIFIED.
9. ARRANGEMENT AND NUMBER OF INLETS AND DISCHARGE PIPES SHALL CONFORM TO THE NEEDS OF THE PERTINENT LOCATION.
10. PROVIDE MINIMUM 2'-0" COVER FOR ALL INLET LEAD PIPE, UNLESS OTHERWISE SPECIFIED.
11. ALL INLETS SHALL BE DEPRESSED AS SHOWN.
12. INLET DEPTH AND GRADE OF INLET LEAD PIPE VARY ACCORDING TO DEPTH OF TRUNK SEWER. MINIMUM INLET DEPTH BELOW TOP OF CURB SHALL BE 4'-0".

CIVIL/STRUCTURAL DETAILS AND SCHEDULES - 2

FIRE DEPARTMENT TRAINING FACILITY
FOR THE
CITY OF WHITEWATER
WHITEWATER, WI

JOB NO.
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