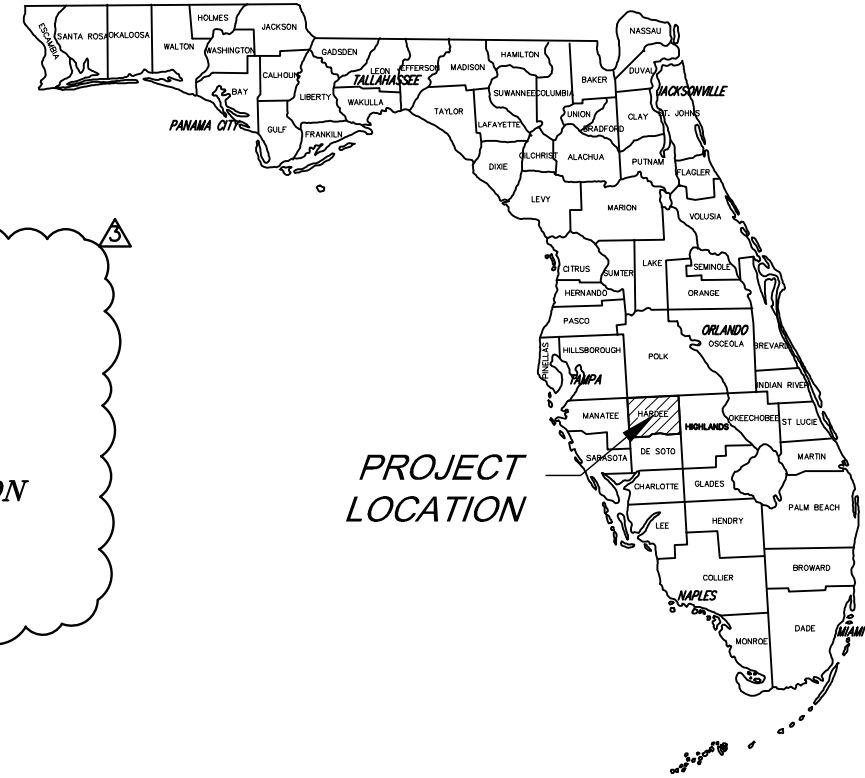


# FDOT CONSTRUCTION PLANS FOR HOGAN STREET EXTENSION IMPROVEMENTS

**CONSTRUCTION AGREEMENT#: 2025-C-191-00001**  
**U.S. ROUTE #: US-17**  
**STATE ROAD #: S.R. 35**  
**LOCAL ROAD: 6TH AVE. AND 5TH AVE.**  
**ROADWAY IDENTIFICATION #: 06010101 AND 06010102**  
**POSTED SPEED LIMIT: 45 MPH**  
**LOCATION: 0.60 MILES SOUTH OF THE MAIN STREET AND US17 SB INTERSECTION**  
**ROADWAY MILE POST: SOUTHBOUND: 13.037**  
**NORTHBOUND 13.032**  
**ENGINEER OF RECORD: MARK S DAVIES P.E.**



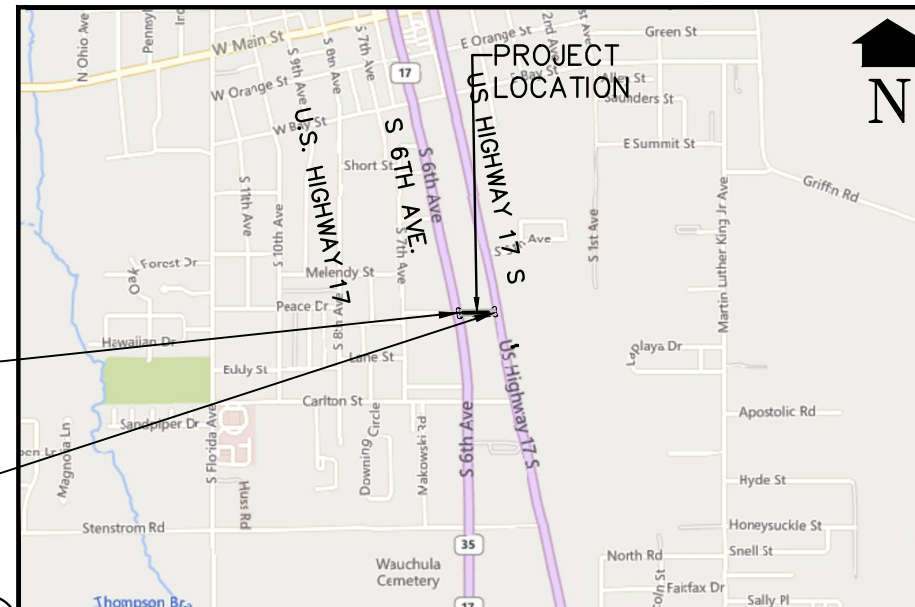
**CITY OF WAUCHULA**  
**HARDEE COUNTY, FLORIDA**

## PROJECT TEAM:

**DEVELOPER:** CITY OF WAUCHULA  
 107 E. MAIN STREET  
 WAUCHULA, FLORIDA 33873  
**SURVEYOR:** SURVTECH SOLUTIONS, INC.  
 10220 US 92 EAST  
 TAMPA, FLORIDA 33610  
 PHONE: (813) 621-4929  
 FAX: (813) 621-7194

**CIVIL ENGINEER:**  
**KIMLEY-HORN AND ASSOCIATES, INC**  
 109 SOUTH KENTUCKY AVENUE  
 LAKELAND, FL 33801  
 TEL: (863) 701-8702  
 mark.davies@kimley-horn.com

**GEOTECHNICAL ENGINEER:**  
**IMPERIAL TESTING AND ENGINEERING, INC.**  
 3905 KIDRON RD, LAKELAND, FL 33811  
 PHONE: (863) 647-2877  
 FAX: (863) 647-1770

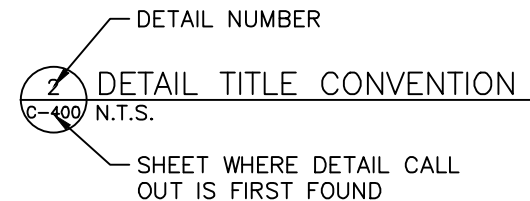


BEGIN CONSTRUCTION  
 @ HOGAN STREET  
 STA: 1+48.32  
 END CONSTRUCTION  
 @ HOGAN STREET  
 STA: 5+25.58

## VICINITY MAP

N.T.S.

**POSTED SPEED : 45 MPH**



**GOVERNING STANDARD PLANS:**  
 Florida Department of Transportation, FY 2025-26 Standard Plans for Road and Bridge Construction and applicable Interim Revisions (IRs).

Standard Plans for Road Construction and associated IRs are available at the following website: <https://www.fdot.gov/design/standardplans>

**GOVERNING STANDARD SPECIFICATIONS:**  
 Florida Department of Transportation, FY 2025-26 Standard Specifications for Road and Bridge Construction at the following website: <https://www.fdot.gov/programmanagement/Implemented/SpecsBooks>

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Plotted By: Donkes, Mark. Sheet No: 2025-C-191-00001-01. Date: 09/18/2025. Scale: 1"=40'. Project: HOGAN STREET EXTENSION IMPROVEMENTS. City of Wauchula, Hardee County, Florida. Engineer: Mark S. Davies, P.E.

No.	REVISIONS	DATE	BY
1	PER FDOT COMMENTS	01/29/26	MSD
2	PER FDOT COMMENTS	11/25/25	MSD

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 109 S. KENTUCKY AVENUE, LAKELAND, FL 33801  
 PHONE: 863-701-8702  
 WWW.KIMLEY-HORN.COM  
 REGISTRY NO. 35106

**KHA PROJECT**  
 046292037  
**DATE**  
 SEPT 2025  
**SCALE** AS SHOWN  
**DESIGNED BY** MSD  
**DRAWN BY** KM  
**CHECKED BY**

**HOGAN STREET EXTENSION IMPROVEMENTS**  
 PREPARED FOR  
**CITY OF WAUCHULA**  
 CITY OF WAUCHULA FLORIDA

01/18/2023

**COVER SHEET**

SHEET NUMBER  
**C-100**

Plotted By: D:\work\2026\09\13\57\m... April 01, 2026 09:13:57am K:\ok\_civil\046292037 - Hogan Street Extension\CAD\plansheets\C-200 GENERAL NOTES.dwg  
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1. THE LOCATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS ARE APPROXIMATE AND BASED ON INFORMATION FURNISHED TO THE ENGINEER BY THE UTILITY OWNER(S) AND ARE SHOWN AS NOTICE TO THE CONTRACTOR THAT UNDERGROUND UTILITIES EXIST. THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OWNER(S) FOR LOCATION AND STAKING OF UNDERGROUND FACILITIES BEFORE EXCAVATING.
2. ALL STATIONS AND OFFSETS ARE FROM B/L CONSTRUCTION, UNLESS OTHERWISE NOTED.
3. ALL PRIVATE AND PUBLIC PROPERTY AFFECTED BY THIS WORK SHALL BE RESTORED TO A CONDITION EQUAL TO OR BETTER THAN THE EXISTING CONDITION BEFORE COMMENCING CONSTRUCTION WORK, UNLESS SPECIFICALLY EXEMPTED BY THE PLANS. COST TO BE INCIDENTAL TO OTHER CONSTRUCTION AND NO EXTRA COMPENSATION TO BE ALLOWED.
4. ALL SURVEY CORNERS INDICATED ON THE PLANS SHALL BE REFERENCED AND CERTIFIED BY A REGISTERED PROFESSIONAL LAND SURVEYOR PRIOR TO COMMENCEMENT OF CONSTRUCTION. ALL CORNERS DESTROYED OR OBLITERATED BY CONSTRUCTION SHALL BE RESET AND SO CERTIFIED BY A LAND SURVEYOR PRIOR TO COMPLETION OF THE PROJECT. CERTIFIED SKETCHES OF RESET CORNERS SHALL BE SUBMITTED TO: KIMLEY-HORN AND ASSOCIATES.
5. ANY PUBLIC LAND CORNER WITHIN THE LIMITS OF CONSTRUCTION IS TO BE PROTECTED. IF A CORNER MONUMENT IS IN DANGER OF BEING DESTROYED, THE CONTRACTOR SHOULD NOTIFY THE COUNTY LOCATION SURVEYOR WITHOUT DELAY BY TELEPHONE.
6. GRADES SHOWN ARE FINISHED GRADES.
7. ALL CURB RADII ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
8. ALL CONCRETE PIPE TO BE CLASS III, WALL B, UNLESS OTHERWISE NOTED.
9. FLORIDA STATUTE 553.851 (1978) REQUIRES THAT BEFORE EXCAVATION, NOTICE BE GIVEN TO THE UTILITY OWNER A MINIMUM OF TWO (2) DAYS AND A MAXIMUM OF FIVE (5) DAYS, EXCLUDING SATURDAY, SUNDAY AND LEGAL HOLIDAYS. NOT ALL UTILITY COMPANIES ARE MEMBERS OF THE SUNSHINE STATE ONE-CALL 1-800-432-4770 OR 811.
10. AT LOCATIONS WHERE PIPES AND DRAINAGE STRUCTURES ARE TO BE CONSTRUCTED OR REMOVED UNDER THE EXISTING PAVEMENT, THE REPLACEMENT OF EMBANKMENT, BASE, ASPHALT, ETC. NECESSARY FOR EXCAVATION IS TO BE INCLUDED IN THE COST OF PIPE.
11. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH AND CONFORM TO THE MOST STRINGENT REQUIREMENTS OF THE PROJECT SPECIFICATIONS: THE LATEST EDITION OF THE FLORIDA DEPARTMENT OF TRANSPORTATION (FDOT) STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND SUPPLEMENTS THERETO, AND HILLSBOROUGH COUNTY ROAD CONSTRUCTION SPECIFICATIONS.
12. NO EXISTING BASE MATERIAL REMOVED IN EXCAVATION SHALL BE REUSED AS PROPOSED BASE MATERIAL.
13. EROSION CONTROL MEASURES SHALL BE UTILIZED BY CONTRACTOR DURING CONSTRUCTION AS PER FDOT EROSION AND SEDIMENT CONTROL MANUAL
14. THE DISPOSAL OF EXCESS EARTHWORK MATERIALS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
15. ALL EXISTING TREES WITHIN THE RIGHT-OF-WAY ARE TO BE REMOVED AS CLEARING AND GRUBBING UNLESS OTHERWISE DIRECTED.
16. THE INFORMATION SHOWN ON THESE DRAWINGS CONCERNING TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES IS BASED ON AVAILABLE RECORDS AND SURVEYS BUT IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR SHALL MAKE HIS OWN DETERMINATION AS TO THE TYPE AND LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO AND IS RESPONSIBLE FOR COORDINATING UTILITY RELOCATION WITH PROJECT CONSTRUCTION. PRIOR TO ORDERING DRAINAGE STRUCTURES, THE CONTRACTOR SHALL DETERMINE IF DRAINAGE/UTILITY CONFLICTS EXIST. INFORMATION ON CONFLICTS IS TO BE SUBMITTED TO THE ENGINEER AS SOON AS POSSIBLE AFTER DISCOVERY FOR RESOLUTION.
17. ALL INLET/MANHOLE - PIPE JOINTS SHALL BE FILLED WITH NON-SHRINK GROUT, COVERED WITH AN ASPHALTIC MASTIC COATING, AND WRAPPED WITH A FILTER FABRIC MATERIAL.
18. ALL DITCH BOTTOM INLETS AND MANHOLES SHALL HAVE TRAFFIC BEARING FRAMES AND COVERS OR GRATES MEETING HS-20 LOADING REQUIREMENTS.
19. PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A SET OF FIELD NOTES VERIFYING THE BENCHMARK ELEVATIONS AND THE REFERENCE POINT TIES SHOWN ON THE TITLE SHEET AND PLAN AND PROFILE SHEETS, AND/OR A SET OF FIELD NOTES FOR ALL ADDITIONAL BENCHMARK AND REFERENCE POINT TIES PROPOSED TO BE USED IN CONSTRUCTING THE PROJECT WITH THEIR LOCATION, DESCRIPTION AND SIGNED AND SEALED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF FLORIDA.
20. IF LANE CLOSURES ARE REQUIRED, CONSTRUCTION OPERATIONS SHALL BE CONFINED TO ONE TRAFFIC LANE, LEAVING THE OPPOSITE LANE OPEN TO TRAFFIC AT ALL TIMES. BOTH LANES SHALL BE OPEN TO TRAFFIC DURING NON-WORKING HOURS. FOR GENERAL TRAFFIC CONTROL, ZONE REQUIREMENTS AND ADDITIONAL INFORMATION, PLEASE REFER TO FDOT STANDARD PLANS INDEX NO. 102-600.
21. ALL SIGNING AND STRIPING WITHIN THE FDOT RIGHT-OF-WAY IS TO BE IN ACCORDANCE WITH FDOT STANDARD PLANS INDEX 711-001.
22. ALL PAVEMENT MARKINGS WITHIN FDOT RIGHT-OF-WAY SHALL BE THERMOPLASTIC.
23. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH PROPOSED MARKINGS SHALL BE REMOVED BY AN FDOT ACCEPTABLE METHOD.
24. ALL RPM PLACEMENT WITHIN THE FDOT RIGHT-OF-WAY IS TO BE IN ACCORDANCE WITH FDOT STANDARD PLANS INDEX 706-001.
25. THE FINISHED ROADWAY SECTION SHALL BE SMOOTH AND SET TO DRAIN.
26. THE EXACT LOCATIONS OF PROPOSED SWALES, BACK SLOPE LINES AND LIMITS OF CONSTRUCTION WILL BE IN ACCORDANCE WITH THE PLANS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
27. ANY DEWATERING SHALL BE INCLUDED IN THE CONTRACTORS BID.

28. CONTRACTOR SHALL ADJUST ALL UTILITY LIDS AND COVERS TO FINISHED GRADE AS REQUIRED. ADJUSTMENTS TO BE INCLUDED IN CONTRACTORS BID.
29. THE CONTRACTOR WILL RESTORE ALL AREAS DISTURBED BY THIS CONSTRUCTION TO A CONDITION EQUAL TO, OR BETTER THAN EXISTED PRIOR TO CONSTRUCTION. ALL DISTURBED AREAS IN THE FDOT RIGHT-OF-WAY WILL BE SODDED.
30. ITEMS IN CONFLICT WITH DESIGN SUCH AS EXISTING CURBS AND GUTTERS, SIDEWALKS, DRAINAGE STRUCTURES, PAVEMENT AND BASE AND EXCESS EXCAVATIONS ARE TO BE REMOVED BY CONTRACTOR AND DISPOSED OF IN A LEGAL AND PROPER MANNER AWAY FROM THE JOB SITE AT THE CONTRACTOR'S EXPENSE UNLESS THE ITEMS ARE TO BE REUSED ON THE PROJECT.
31. THE MAINTENANCE OF TRAFFIC FOR THIS PROJECT SHALL BE, AT A MINIMUM, IN ACCORDANCE WITH FDOT STANDARD PLANS INDEX NUMBERS 102-600, 102-611, 102-612, 102-613, 102-616, 102-660, 102-670 AND THESE DOCUMENTS; THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (U.S. DEPARTMENT OF TRANSPORTATION, FHWA), SHALL BE FOLLOWED IN THE DESIGN, APPLICATION, INSTALLATION, MAINTENANCE AND REMOVAL OF ALL TRAFFIC CONTROL DEVICES, WARNING DEVICES AND BARRIERS NECESSARY TO PROTECT THE PUBLIC AND WORKMEN FROM HAZARDS WITHIN THE PROJECT LIMITS. PEDESTRIAN AND VEHICULAR TRAFFIC SHALL BE MAINTAINED AND PROTECTED AT ALL TIMES.
32. THE LOCATION OF EXISTING UTILITIES SHOWN ARE APPROXIMATE ONLY; THE EXACT LOCATION SHALL BE DETERMINED BY THE CONTRACTOR. RELOCATION OF UTILITIES SHALL BE COORDINATED WITH UTILITY COMPANIES AFTER IDENTIFICATION OF CONFLICT BY CONTRACTOR. CONTRACTOR SHALL NOTIFY ENGINEER IN ADVANCE BEFORE ANY RELOCATION.
33. THE UTILITY COMPANIES SHALL BE NOTIFIED BY THE CONTRACTOR 48 HOURS IN ADVANCE OF ANY EXCAVATION INVOLVING THEIR UTILITIES SO THAT COMPANY REPRESENTATIVE CAN BE PRESENT.
34. THE CONTRACTOR IS TO USE CAUTION WHEN WORKING IN OR AROUND AREAS OF OVERHEAD TRANSMISSION LINES OR UNDERGROUND UTILITIES.
35. THE CONTRACTOR IS RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES TO REMAIN IN PLACE.
36. DRIVEWAYS THAT ARE DISTURBED ARE TO BE RECONSTRUCTED TO THEIR ORIGINAL OR BETTER CONDITIONS, UNLESS OTHERWISE STATED ON PLANS.
37. STAGING AND MATERIAL STORAGE SHALL NOT BE CONDUCTED ON ABUTTING PRIVATE PROPERTY WITHOUT APPROVAL FROM THE OWNER.
38. ALL CONTRACTORS AND/OR PERMITTEES WHO CONTRACT THE APPLICATION OF FERTILIZER SHALL ENSURE THEY ARE LICENSED COMMERCIAL APPLICATORS WHO HAVE BEEN TRAINED THROUGH THE GREEN INDUSTRY BMP PROGRAM, AND HAVE OBTAINED A LIMITED CERTIFICATION FOR URBAN LANDSCAPE COMMERCIAL FERTILIZER APPLICATION UNDER SECTION 482-1562, FS. WITH A CURRENT COPY OF THE LICENSE ON FILE.

UTILITY COMPANY CONTACTS

COMPANY	CONTACT PERSON	PHONE NUMBER
TECO PEOPLES GAS		877-832-6747
CITY OF WAUCHULA (WATER, SEWER, ELECTRIC)		863-773-3131
FRONTIER	USIC DISPATCH	800-778-9140
CHARTER COMMUNICATIONS	USIC DISPATCH	800-778-9140

UTILITY COMPANIES LISTED ARE KNOWN UTILITIES ONLY. CONTRACTOR SHALL VERIFY ALL UTILITIES LOCATED WITHIN PROJECT AREA.

ANY UTILITY RELOCATES SHALL BE COORDINATED WITH THE UTILITY OWNER. CONTRACTOR TO ENSURE COORDINATION AS NOTED.

CALL 2 BUSINESS DAYS BEFORE YOU DIG

IT'S THE LAW! Know what's below. Call before you dig.

DIAL 811

SUNSHINE STATE ONE CALL OF FLORIDA, INC.

No.	REVISIONS	DATE	BY

© 2025 KIMLEY-HORN AND ASSOCIATES, INC.  
 109 S. KENTUCKY AVENUE, LAKELAND, FL 33801  
 PHONE: 863-701-8702  
 WWW.KIMLEY-HORN.COM  
 REGISTRY NO. 35106

KHA PROJECT 046292037
DATE SEPT 2025
SCALE AS SHOWN
DESIGNED BY MSD
DRAWN BY KM
CHECKED BY

**HOGAN STREET EXTENSION  
 IMPROVEMENTS**  
 PREPARED FOR  
**CITY OF WAUCHULA**  
 CITY OF WAUCHULA FLORIDA

GENERAL NOTES

SHEET NUMBER  
**C-200**

Plotted By: D:\Projects\Mark, April 01, 2026 09:14:00am K:\lok\_civil\046292037 - Hogan Street Extension\CAD\plansets\C-200 GENERAL NOTES.dwg  
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**FDOT GENERAL NOTES**

1. ELEVATIONS ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88). REFERENCE BENCHMARK IS HILLSBOROUGH COUNTY BENCHMARK – VB-528, HILLS. CO. SURVEY DEPT. FDOT DISK IN A HEADWALL, PUBLISHED ELEVATION = 105.22'.
2. THE BEARING STRUCTURE FOR THIS SURVEY IS BASED ON A NAD 1983 FLORIDA STATE PLANE WEST ZONE, BEARING OF S 12°09'41" E FOR THE EASTERLY RIGHT-OF-WAY BOUNDARY OF 6TH AVENUE SOUTH.
3. THE HORIZONTAL DATUM UTILIZED FOR THIS PROJECT IS NAD 1983 FLORIDA WEST ZONE, 2011 ADJUSTMENT, U.S. SURVEY FEET. SAID DATUM WAS ESTABLISHED BY UTILIZING THE FLORIDA PERMANENT REFERENCE NETWORK (FPRN).
4. THE VERTICAL DATUM UTILIZED FOR THIS PROJECT IS NAVD 1988, U.S. SURVEY FEET. THE BENCHMARK UTILIZED WAS NATIONAL GEODETIC SURVEY (NGS) CONTROL STATION "L 703" WITH AN ELEVATION OF 74.79 FEET.
5. APPROXIMATE LOCATION OF KNOWN UTILITIES TAKEN FROM SUNSHINE 811 RESPONSES, AND PROVIDER INFORMATION.
6. ALL WORK INSIDE FDOT RIGHT-OF-WAY SHALL BE REFERENCED UNDER THESE NOTES.
7. THESE PLANS HAVE BEEN PREPARED IN ACCORDANCE WITH THE MOST CURRENT VERSION OF THE FDOT DESIGN MANUAL (FDM) AND ARE GOVERNED BY THE MOST CURRENT VERSION OF THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE MOST CURRENT VERSION OF THE FDOT STANDARD PLANS.
8. AT THE END OF EACH DAY, THE RIGHT OF WAY (INCLUDING ALL TRAVEL LANES) SHALL BE RESTORED FOR SAFE VEHICULAR TRAFFIC. NO DROP-OFF CONDITIONS ARE ALLOWED TO REMAIN. NO MATERIALS, EQUIPMENT, ETC. SHALL BE STORED WITHIN THE RIGHT OF WAY WHILE THE WORK ZONE IS INACTIVE.
9. IT IS THE RESPONSIBILITY OF THE PERMITTEE TO DETERMINE AND COMPLY WITH ALL APPLICABLE COUNTY AND MUNICIPAL ORDINANCES THAT ARE MORE STRINGENT THAN THE DEPARTMENT'S REQUIREMENTS.
10. THE PERMITTEE SHALL NOT DURING OR AFTER CONSTRUCTION INTRODUCE ANY FORM OF UNPERMITTED STORM WATER DISCHARGE INTO THE DEPARTMENT'S RIGHT OF WAY OR EASEMENTS.
11. ALL WORK INSIDE FDOT RIGHT OF WAY SHALL BE IN ACCORDANCE WITH THE MOST CURRENT VERSION OF FDOT DESIGN MANUAL AND STANDARD PLANS FOR ROAD AND BRIDGE CONSTRUCTION.
12. ALL AREAS IN THE F.D.O.T. RIGHT-OF-WAY DISTURBED DURING CONSTRUCTION SHALL BE RESTORED, COMPACTED, SODDED AND WATERED IN CONFORMANCE WITH THE F.D.O.T. STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND STANDARD PLANS 570-000 SERIES. RESTORATION MUST BE COMPLETE WITHIN 72 HOURS OF COMPLETION OF THE PROJECT.
13. LANE CLOSURES ARE AT THE DISCRETION OF THE DEPARTMENT AND MUST BE PRE-APPROVED. LANE CLOSURES MUST BE IN ACCORDANCE WITH THE LOCATION SPECIFIC RESTRICTIONS ON THE MASTER LANE CLOSURE LIST. CONTACT THE FDOT PERMIT OFFICE OR THE FDOT INSPECTOR FOR DETAILS.
14. ALL STRIPING WITHIN THE FDOT RIGHT-OF-WAY MUST BE THERMOPLASTIC STRIPING AND MUST BE PLACED 30 DAYS AFTER FRICTION COURSE ASPHALT PAVEMENT.
15. ALL PROPOSED TRAFFIC SIGNS AND PAVEMENT MARKINGS SHALL BE IN CONFORMANCE WITH THE F.D.O.T. STANDARD PLANS SERIES 700-101, 706-001, 711-001, 711-002. THE PROPERTY OWNER MUST MAINTAIN THE DRIVEWAY TRAFFIC SIGNS AND MARKINGS.
16. THE F.D.O.T. REQUIRES DOCUMENTATION FOR SUCCESSFUL COMPLETION OF AN APPROVED WORK ZONE TRAFFIC CONTROL TRAINING COURSE FOR THE AGENCY, UTILITY OR CONTRACTOR EMPLOYEE(S) INSTALLING AND/OR MAINTAINING THE APPROVED MAINTENANCE OF TRAFFIC PLAN. DOCUMENTATION IS TO BE FURNISHED TO THE DEPARTMENT AT THE PRE-CONSTRUCTION MEETING OR BEFORE OCCUPYING STATE RIGHT-OF-WAY.
17. ALL MAINTENANCE OF TRAFFIC WITHIN FDOT RIGHT OF WAY MUST CONFORM TO THE MOST CURRENT VERSION OF FDOT STANDARD PLANS 102-000 SERIES.
18. IN ACCORDANCE TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2020 EDITION SECTION 102-7, A TRAFFIC CONTROL OFFICER IS REQUIRED WHEN PERFORMING LANE CLOSURES DURING NIGHTTIME OPERATIONS ON ROADWAYS WITH POSTED SPEEDS OF 55MPH OR GREATER.
19. PAVEMENT DROP OFFS SHALL NOT LAST OVER 24 HOURS, UNLESS MITIGATED WITH FILL MATERIAL USED TO CONSTRUCT A TEMPORARY 1:4 COMPACTED SLOPE ADJACENT TO THE OPERATIONAL TRAVEL LANE AS SHOWN ON STANDARD 102-600 SHEET 9 OF 12, AT THE END OF EACH DAY.
20. REFLECTIVE PAVEMENT MARKERS PLACEMENT WILL BE IN COMPLIANCE WITH STANDARD PLANS INDEX 706-001
21. THE PERMITTEE SHALL BE RESPONSIBLE FOR PROVIDING FINAL CERTIFICATION FROM A PROFESSIONAL ENGINEER THAT ALL CONSTRUCTION WITHIN FDOT RIGHT-OF-WAY HAS BEEN CONSTRUCTED ACCORDING TO THE MOST CURRENT VERSION OF FDOT DESIGN MANUAL AND STANDARD PLANS. TWO SETS OF 11"X 17" AS-BUILT PLANS, RECORD DRAWING REPORTS, ENGINEER'S CERTIFICATION, DELIVERY TICKETS OF ALL MATERIALS USED INSIDE FDOT RIGHT OF WAY AND ANY TESTING RESULTS WILL BE REQUIRED FOR THE FINAL ACCEPTANCE OF THIS PERMIT.
22. THE CONTRACTOR MUST PROVIDE PROOF OF COVERAGE UNDER A LIABILITY INSURANCE POLICY IN ACCORDANCE WITH FLORIDA ADMINISTRATIVE CODE 14-96.007(5) PRIOR TO THE COMMENCEMENT OF CONSTRUCTION WITHIN THE DEPARTMENT'S RIGHT OF WAY. THE PERMIT NUMBER AND PERMIT NAME SHALL BE SHOWN ON THE CERTIFICATE OF LIABILITY INSURANCE POLICY.
23. THE PERMITTEE SHALL BE RESPONSIBLE FOR RESOLVING ANY UTILITY CONFLICTS BEFORE CONSTRUCTION BEGINS. ALL CONFLICTING UTILITIES MUST BE ADJUSTED AT THE PERMITTEE'S EXPENSE. A SEPARATE UTILITY PERMIT MUST BE OBTAINED FOR UTILITY WORK WITHIN THE DEPARTMENT'S RIGHT-OF-WAY.
24. AN ON-SITE PRE-CONSTRUCTION MEETING SHALL BE HELD PRIOR TO ANY CONSTRUCTION BEING PERFORMED WITHIN THE FDOT RIGHT-OF-WAY. PLEASE CONTACT THE LOCAL FDOT PERMIT OFFICE TO COORDINATE THE PRE-CONSTRUCTION MEETING AND ALL INSPECTIONS.
25. ALL CONTRACTORS AND/OR PERMITTEE'S WHO CONTRACT THE APPLICATION OF FERTILIZER SHALL ENSURE THEY ARE LICENSED COMMERCIAL APPLICATORS WHO HAVE BEEN TRAINED THROUGH THE GREEN INDUSTRY BMP PROGRAM, AND HAVE OBTAINED A LIMITED CERTIFICATION FOR URBAN LANDSCAPE COMMERCIAL FERTILIZER APPLICATION UNDER SECTION 482-1562, FS. WITH A CURRENT COPY OF THE LICENSE ON FILE.
26. THE PERMITTEE OR CONTRACTOR IS REQUIRED TO CONTACT THE LOCAL FDOT PERMIT OFFICE AT LEAST 48 HOURS PRIOR TO WORKING WITHIN THE FDOT'S RIGHT OF WAY. A VERIFICATION NUMBER WILL BE ISSUED TO AUTHORIZE COMMENCEMENT OF CONSTRUCTION WITHIN THE FDOT'S RIGHT OF WAY.

27. THE DEPARTMENT WILL NOT RELEASE OUR HOLD THE CERTIFICATE OF OCCUPANCY UNTIL AFTER ALL WORK HAS BEEN COMPLETED AND APPROVED.
28. THE FDOT IS NOT RESPONSIBLE FOR DESIGN AND/OR PLAN ERRORS. ALL CONSTRUCTION IN FDOT RIGHT OF WAY SHALL BE IN ACCORDANCE WITH FDOT STANDARDS AND SPECIFICATIONS.
29. THE PERMITTEE SHALL SECURE THE SERVICES OF A QUALIFIED CONSULTANT TO PERFORM CONSTRUCTION ENGINEERING & INSPECTION SERVICES FOR ALL WORK THAT HAS BEEN DONE ON THE FDOT RIGHT OF WAY IN ACCORDANCE WITH THE DEPARTMENT'S STANDARDS, SPECIFICATIONS, PERMIT REQUIREMENTS AND ALL APPLICABLE FEDERAL, STATE, AND LOCAL STATUTES, RULES, AND REGULATIONS
30. SOD ALL DISTURBED AREAS WITHIN THE FDOT RIGHT-OF-WAY.
31. IT IS THE CONTRACTOR OR PERMITTEE'S RESPONSIBILITY TO MAINTAIN THE RIGHT-OF-WAY FOR THE DURATION OF THE PROJECT. THIS INCLUDES PICKING UP LITTER, MOWING, TREE TRIMMING, AND KEEPING THE ROADWAY CLEAN AND FREE OF DEBRIS (FDOT WILL NOT ENTER AN ACTIVE PROJECT TO HELP MINIMIZE THE CHANCE OF CONFLICTS).
32. FOR WORK WITHIN FDOT RIGHT-OF-WAY, THE CONTRACTOR SHALL PROVIDE A TEMPORARY TRAFFIC CONTROL PLAN PREPARED UNDER THE DIRECTION OF, AND SIGNED AND SEALED BY, A LICENSED FLORIDA PROFESSIONAL ENGINEER WHO IS EXPERIENCED IN PREPARING TRAFFIC CONTROL PLANS AND WHO IS CERTIFIED PER FDOT PROCEDURE, TOPIC NO. 625-010-010
33. FOR WORK WITHIN FDOT RIGHT-OF-WAY, THE FDOT REQUIRES DOCUMENTATION FOR SUCCESSFUL COMPLETION OF AN APPROVED WORK ZONE TRAFFIC CONTROL TRAINING COURSE FOR THE AGENCY, UTILITY, OR CONTRACTOR EMPLOYEE(S) DESIGNING, INSTALLING, AND/OR MAINTAINING THE APPROVED MAINTENANCE OF TRAFFIC PLAN IN ACCORDANCE WITH DEPARTMENT PROCEDURE, TOPIC NO. 625-010-010
34. ALL TEMPORARY TRAFFIC CONTROL DEVICES FOR THE FOLLOWING FACILITIES SHALL BE DESIGNED AND INSTALLED TO MEET THE EXISTING POSTED SPEEDS AS STATED FOR ALL TRAFFIC CONTROL PHASES: (SR 52-55 MPH).
35. THE CONTRACTORS SHALL NOTIFY THE DEPARTMENT A MINIMUM 14 DAYS IN ADVANCE OF ANY LANE CLOSURES WITH LANE CLOSURE ANALYSIS IN ACCORDANCE WITH THE FLORIDA DESIGN MANUAL (FDM) SECTION 241.

**Benchmark Information: NAVD 1988**

<p><b>TBM #1</b>            Elevation = 107.54            Set nail &amp; disk labeled "SurvTech Trav. Pt" in asphalt pavement; ±26 feet Southeast of a sign; ±19 feet Northeast of a water valve.</p> <p><b>TBM #2</b>            Elevation = 104.60            Set nail &amp; disk labeled "SurvTech Trav. Pt" in concrete sidewalk; ±13 feet Northwest of a water valve; ±21 feet South of a second water valve.</p>
--



No.	REVISIONS	DATE	BY

**Kimley»Horn**

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 WWW.KIMLEY-HORN.COM  
 REGISTRY NO. 35106

KHA PROJECT	046292037
DATE	SEPT 2025
SCALE	AS SHOWN
DESIGNED BY	MSD
DRAWN BY	KM
CHECKED BY	

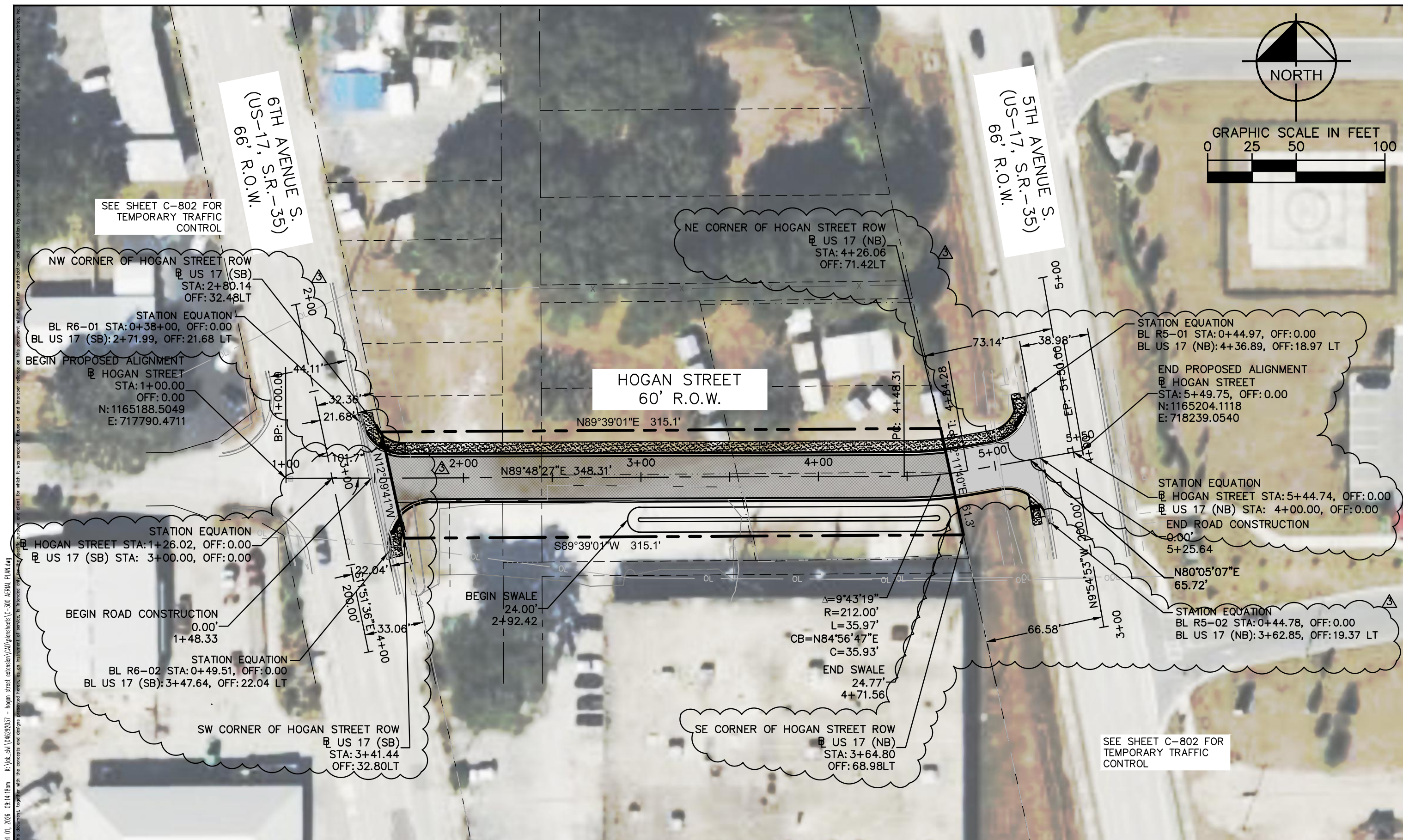
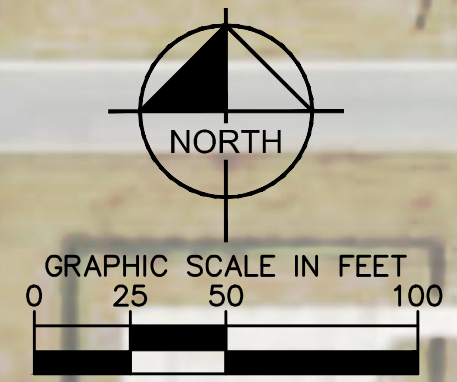
**HOGAN STREET EXTENSION IMPROVEMENTS**

PREPARED FOR  
**CITY OF WAUCHULA**

CITY OF WAUCHULA FLORIDA

**GENERAL NOTES**

SHEET NUMBER  
**C-201**



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 April 01, 2026 09:14:18am  
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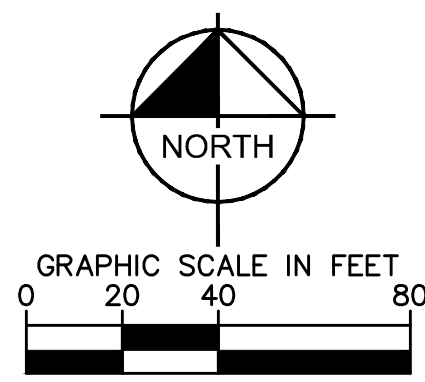
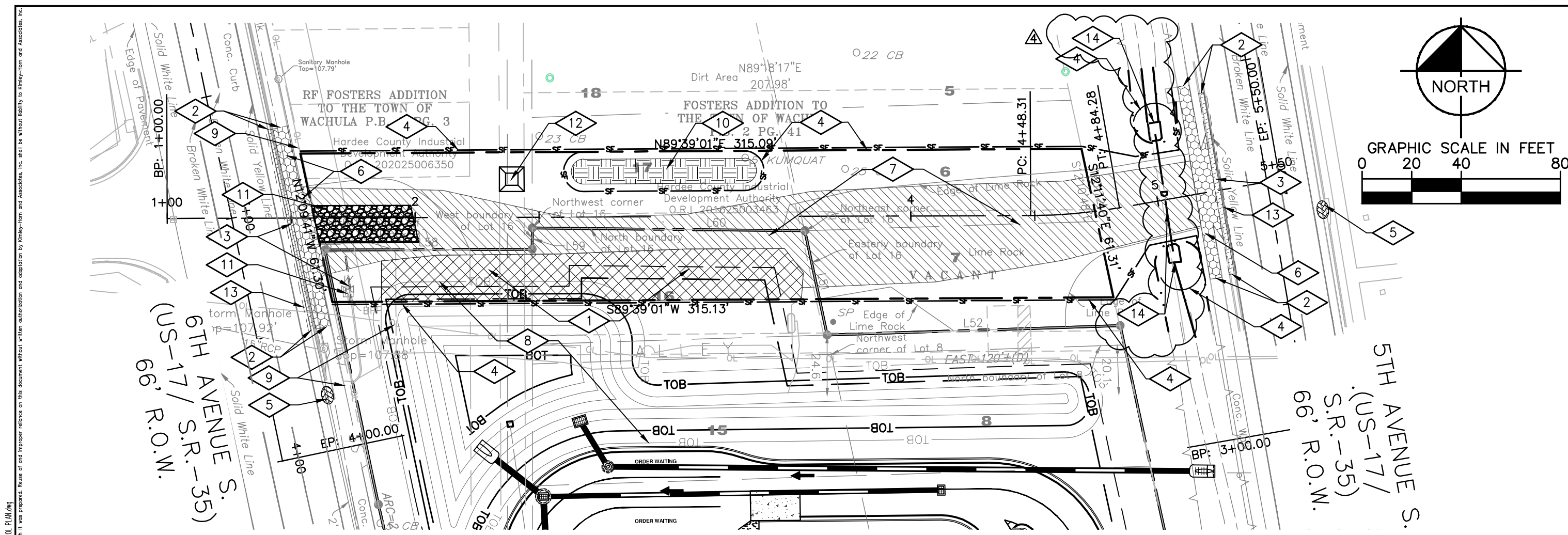
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HOGAN STREET EXTENSION  
 IMPROVEMENTS  
 PREPARED FOR  
 CITY OF WAUCHULA  
 CITY OF WAUCHULA  
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SEE SHEET C-802 FOR  
 TEMPORARY TRAFFIC  
 CONTROL

SHEET NUMBER  
**C-300**



KEYNOTES: THE KEYNOTES ON THIS SHEET ARE SPECIFIC TO THE SHEET C-40X SERIES ONLY

- 1 CONTRACTOR SHALL STRIP EXISTING ASPHALT, PAVING, BASE COURSE AND SUBGRADE TO PERMIT FULL DEPTH ROAD RECONSTRUCTION. EXISTING ROAD BASE MAY BE USED AS STABILIZING AGENT IN PROPOSED STABILIZED SUBGRADE CONSTRUCTION IF IT MEETS STABILIZING REQUIREMENTS. LEGALLY DISPOSE OF MATERIALS IN EXCESS OF THE NEEDS OF THE PROJECT OFF SITE.
- 2 EXISTING SIDEWALK AND CURB SHALL BE SAWCUT IF NECESSARY AND REMOVED TO THE NEAREST JOINT ADJACENT THE PROPOSED WORK
- 3 CONTRACTOR SHALL REMOVE EXISTING CURB AND UNDERLYING BASE MATERIALS. EXISTING ROAD BASE MAY BE USED AS STABILIZING AGENT IN PROPOSED SUBGRADE CONSTRUCTION. LEGALLY DISPOSE OF MATERIALS IN EXCESS OF THE NEEDS OF THE PROJECT OFF SITE.
- 4 CONTRACTOR SHALL INSTALL TYPE III SILT FENCE PER DETAIL 1/C-401 TO DENOTE LIMITS OF DISTURBANCE PER FDOT EROSION AND SEDIMENT CONTROL DESIGNER AND REVIEWER MANUAL (TYP.) SILT FENCE LOCATION MAY BE ADJUSTED TO SUIT SITE CONDITIONS.
- 5 CONTRACTOR SHALL INSTALL INLET PROTECTION AS OUTLINED IN SECTION V OF THE FDOT EROSION AND SEDIMENT CONTROL DESIGNER AND REVIEWER MANUAL. EMPLOY TECHNIQUES SUCH AS WATER FILLED BARRIERS; ROCK BARRIERS FOR AREA DRAINS; FRAME AND FILTER BARRIERS FOR AREA DRAINS; CURB INLET "SUMP" BARRIERS; CURB INLET DIVERSION BERMS AND FILTRATION LOGS PER DETAIL 4/C-401; CURB AND GUTTER SEDIMENT CONTAINMENT SYSTEMS; AND CURB INLET INSERTS. INLET PROTECTIONS SHALL BE INSTALLED AT DOWNSTREAM LOCATIONS ON THE EAST SIDE OF NORTHBOUND US17 AND BOTH SIDES OF SOUTHBOUND US17.
- 6 CONTRACTOR SHALL DEMOLISH AND LEGALLY DISPOSE OF EXISTING SIDEWALK.
- 7 CONTRACTOR SHALL STRIP GRAVEL, STONE, AND LIMEROCK AND REMOVE ALL BASE MATERIALS AND LEGALLY DISPOSE OF OFFSITE. GRASS STRIPPINGS MAY BE STOCKPILED FOR USE WITHIN THE PROJECT IN LANDSCAPING AREAS. DISPOSE OF SURPLUS MATERIALS OFFSITE.
- 8 DETENTION BASIN ON ADJACENT PROPERTY SHALL NOT BE DISTURBED FOR PORTIONS THAT ENCROACH INTO THE R.O.W. PROTECT AND SAFEGUARD GRASSING AND GRADING.
- 9 EXISTING UTILITIES, WHETHER OR NOT SHOWN OR IMPLIED ON THE DRAWINGS, SHALL BE PROTECTED UNLESS OTHERWISE NOTED. CONTRACTOR IS RESPONSIBLE FOR PROTECTING SUCH UTILITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING/REPLACING DAMAGED UTILITIES TO THE UTILITIES OWNERS' SATISFACTION. CONTRACTORS ARE RESPONSIBLE FOR UTILITY SHORING, HOLDING, SWINGING OR OTHER TEMPORARY/PERMANENT MEASURE TO PERMIT THE INSTALLATION OF THE WORK MEASURES.
- 10 TOPSOIL STOCKPILE LOCATION ON SITE, STOCKPILE GEOMETRY AND LOCATION MAY BE ADJUSTED TO SUIT SITE CONDITIONS, STOCKPILE SHALL BE PROTECTED FROM WIND AND WATER BORNE EROSION USING SILTFENCE AND/OR TEMPORARY SEED AND MULCH. MATERIAL SURPLUS TO THE NEEDS OF THE PROJECT SHALL BE LEGALLY DISPOSED OF OFF SITE
- 11 ADD CONSTRUCTION EXIT PER DETAIL 2/C-401. LOCATION MAY VARY TO SUIT SITE CONDITIONS. ONE EXIT IS SHOWN, IF MORE IS NEEDED, THEY SHALL BE INCLUDED AT NO COST TO THE OWNER.
- 12 CONCRETE WASHOUT PER DETAIL 3/C-401. LOCATION MAY VARY TO SUIT SITE CONDITIONS.
- 13 12" WIDE STRIP OF LIMIT OF PAVEMENT MILLING FOR PROPOSED TO EXISTING ASPHALT CONNECTION, SEE DETAIL 3/C-800
- 14 DEMOLISH EXISTING 18" RCP AND MITERED END SECTIONS, SEE SHEETS C-500 AND C-600 FOR MORE INFORMATION ON REPLACEMENT.

4	PER FDOT COMMENTS	03/04/26	MRF		
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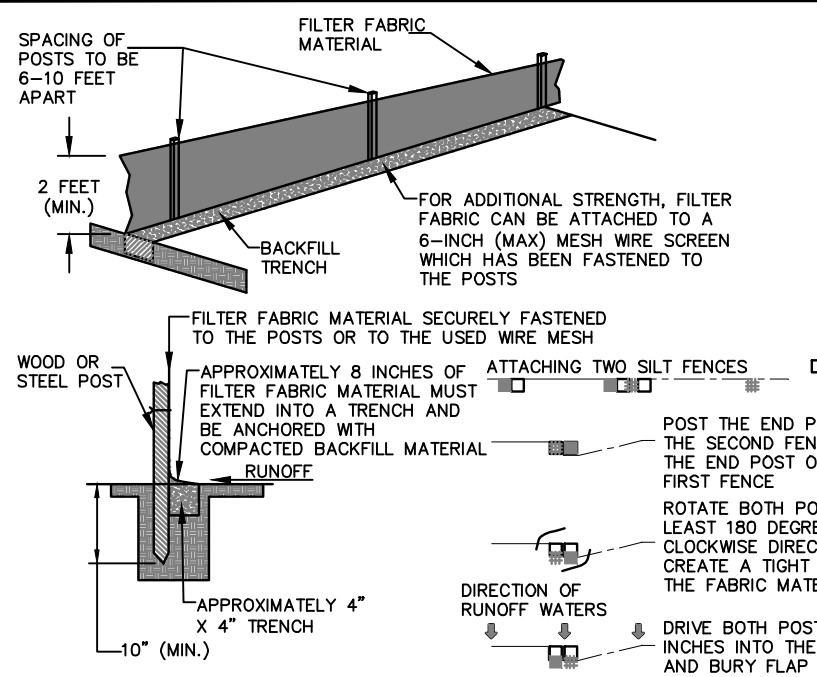
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DATE	SEPT 2025
SCALE	AS SHOWN
DESIGNED BY	MSD
DRAWN BY	KM
CHECKED BY	

**HOGAN STREET EXTENSION  
IMPROVEMENTS**  
PREPARED FOR  
**CITY OF WAUCHULA**  
CITY OF WAUCHULA FLORIDA

<p style="font-size: 24pt; margin: 0;"><b>DEMOLITION - EROSION AND SEDIMENTATION CONTROL PLAN</b></p>	<p style="font-size: 24pt; margin: 0;"><b>C-400</b></p>
SHEET NUMBER	

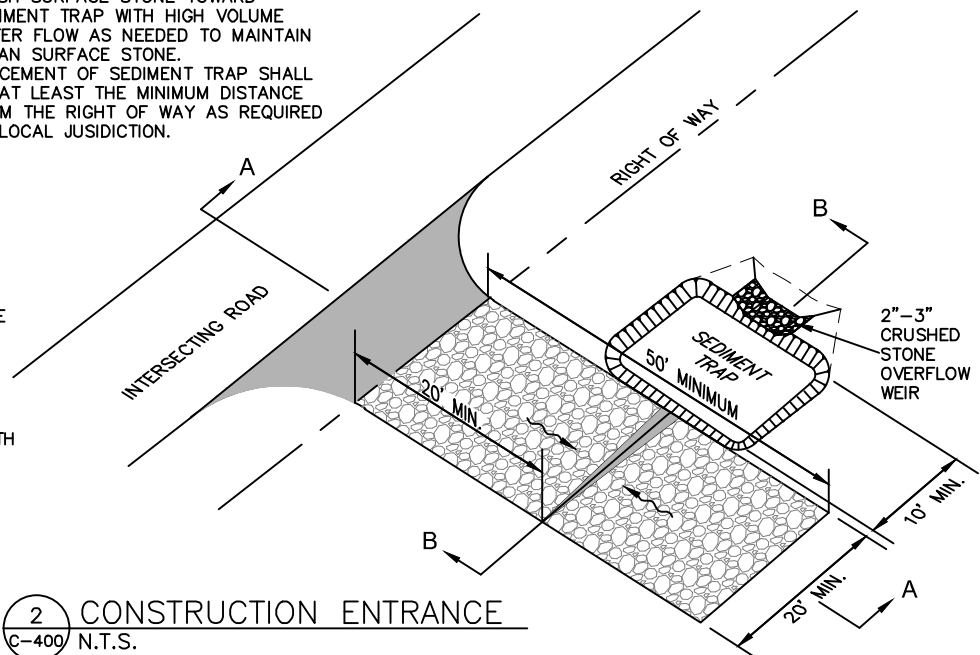
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**1 SINGLE ROW OF SILT FENCE**  
C-400 N.T.S.

- NOTES:**
1. PROVIDE SEDIMENT TRAP ON DOWN GRADIENT SIDE (OR BOTH SIDES) AS REQUIRED.
  2. FLUSH SURFACE STONE TOWARD SEDIMENT TRAP WITH HIGH VOLUME WATER FLOW AS NEEDED TO MAINTAIN CLEAN SURFACE STONE.
  3. PLACEMENT OF SEDIMENT TRAP SHALL BE AT LEAST THE MINIMUM DISTANCE FROM THE RIGHT OF WAY AS REQUIRED BY LOCAL JURISDICTION.



**2 CONSTRUCTION ENTRANCE**  
C-400 N.T.S.

**NOTES:**

**SELF-INSTALLED CONCRETE WASHOUT FACILITY**

WASHOUT SHALL BE BUILT BELOW-GRADE TO PREVENT BREACHES AND REDUCE THE LIKELIHOOD OF RUNOFF. WASHOUTS SHALL BE SIZED TO HANDLE SOLIDS, WASH WATER, AND RAINFALL TO PREVENT OVERFLOW. CONCRETE WASHOUT SYSTEMS, INC., (2006) ESTIMATES THAT 7 GALLONS OF WASH WATER ARE USED TO WASH ONE TRUCK CHUTE AND 50 GALLONS ARE USED TO WASH OUT THE HOPPER OF A CONCRETE PUMP TRUCK. THE BELOW-GRADE WASHOUT SHOULD BE AT LEAST 15 FEET WIDE AND 15 FEET LONG AND DEEP ENOUGH SIZED TO CONTAIN ALL LIQUID AND SOLID WASTE YOU EXPECT TO GENERATE IN BETWEEN CLEANOUT PERIODS (CASQA, 2003). INCLUDE A MINIMUM 12-INCH FREEBOARD IN THE SIZING CALCULATIONS. LINE THE PIT WITH PLASTIC SHEETING OF AT LEAST 10-MIL THICKNESS THAT HAS NO HOLES OR TEARS TO PREVENT LEACHING OF LIQUIDS INTO THE GROUND (CASQA, 2003). CONCRETE WASH WATER SHOULD NEVER BE PLACED IN A PIT THAT IS CONNECTED TO THE STORM DRAIN SYSTEM OR THAT DRAINS TO NEARBY WATERWAYS.

ACCORDING TO CASQA (2003), YOU SHOULD NOT PLACE CONCRETE WASHOUT FACILITIES WITHIN 50 FEET OF STORM DRAINS, OPEN DITCHES, OR WATERBODIES. YOU SHOULD PLACE THEM IN A LOCATION THAT ALLOWS CONVENIENT ACCESS FOR CONCRETE TRUCKS, PREFERABLY NEAR THE AREA WHERE THE CONCRETE IS BEING POURED. APPROPRIATE GRAVEL OR ROCK SHOULD COVER PATHS TO CONCRETE WASHOUT FACILITIES.

**INSPECTION**

CHECK ALL CONCRETE WASHOUT FACILITIES DAILY TO DETERMINE IF THEY HAVE BEEN FILLED TO 75 PERCENT CAPACITY, WHICH IS WHEN MATERIALS NEED TO BE REMOVED. BOTH ABOVE- AND BELOW- GROUND SELF-INSTALLED WASHOUT SHOULD BE INSPECTED DAILY TO ENSURE THAT PLASTIC LININGS ARE INTACT AND SIDEWALLS HAVE NO BEEN DAMAGED BY CONSTRUCTIONS ACTIVITIES. INSPECTORS SHOULD ALSO NOTE WHETHER THE FACILITIES ARE BEING USED REGULARLY; IF DRIVERS HAVE WASHED OUT THEIR CHUTES OR HOPPERS IN OTHER LOCATIONS, YOU MAY NEED TO PROVIDE MORE EDUCATION, INSTALL ADDITIONAL SIGNAGE, OR PLACE ADDITIONAL WASHOUTS IN MORE CONVENIENT LOCATIONS.

**MATERIAL REMOVAL**

CONCRETE WASHOUTS AREA DESIGNED TO PROMOTE EVAPORATION WHERE FEASIBLE. HOWEVER, IF STORED LIQUIDS HAVE NOT EVAPORATED AND THE WASHOUT IS NEARING CAPACITY, VACUUM AND DISPOSE OF THEM IN AN APPROVED MANNER - CHECK WITH THE LOCAL SANITARY SEWER AUTHORITY TO DETERMINE IF THERE ARE SPECIAL DISPOSAL REQUIREMENTS FOR CONCRETE WASH WATER. REMOVE LIQUIDS OR COVER THE STRUCTURES BEFORE PREDICTED RAINSTORMS TO PREVENT OVERFLOWS. COMPANIES THAT OFFER PREFABRICATED AND WATERTIGHT WASHOUT CONTAINERS GENERALLY OFFER A VACUUM SERVICE TO REMOVE THE LIQUID MATERIAL.

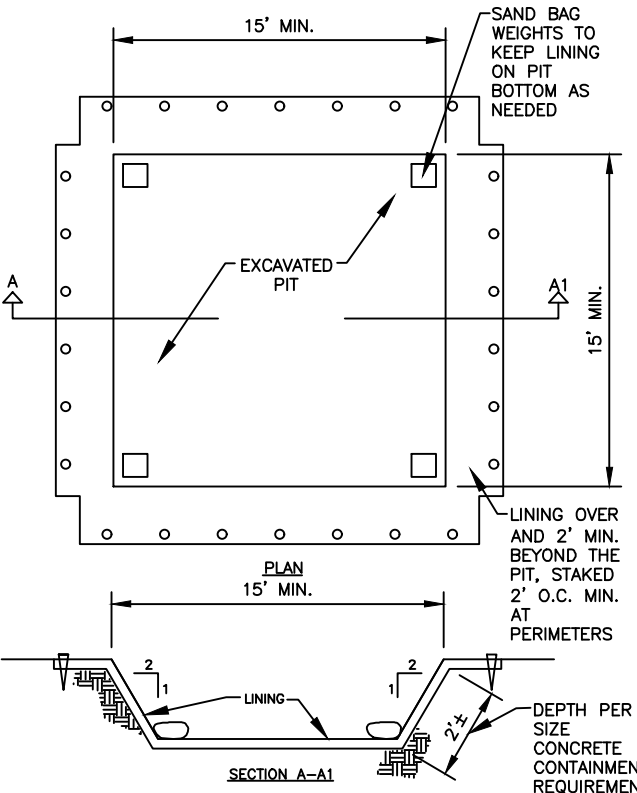
REMOVE HARDENED SOLIDS WHOLE OR BREAK THEM FIRST DEPENDING ON THE TYPE OF EQUIPMENT AVAILABLE AT YOUR SITE. REUSE THE SOLIDS ONSITE OR HAUL THEM AWAY FOR RECYCLING - CRUSHED CONCRETE MAKES EXCELLENT AGGREGATE FOR ROADBEDS AND OTHER BUILDING APPLICATIONS. CHECK YOU YOUR LOCAL RECYCLING AGENCY TO IDENTIFY OPPORTUNITIES FOR CONCRETE RECYCLING.

WHEN YOU REMOVE MATERIALS FROM THE CONCRETE WASHOUT, BUILD A NEW STRUCTURE OR, IF THE PREVIOUS STRUCTURE IS STILL INTACT, INSPECT THE STRUCTURE FOR SIGNS OF WEAKENING OR DAMAGE AND MAKE ANY NECESSARY REPAIRS. LINE THE STRUCTURE WITH NEW PLASTIC THAT IS FREE OF HOLES OR TEARS AND REPLACE SIGNAGE IF NECESSARY. IT IS VERY IMPORTANT THAT NEW PLASTIC IS USED AFTER EVERY CLEANING BECAUSE PUMPS AND CONCRETE REMOVAL EQUIPMENT CAN DAMAGE THE EXISTING LINER.

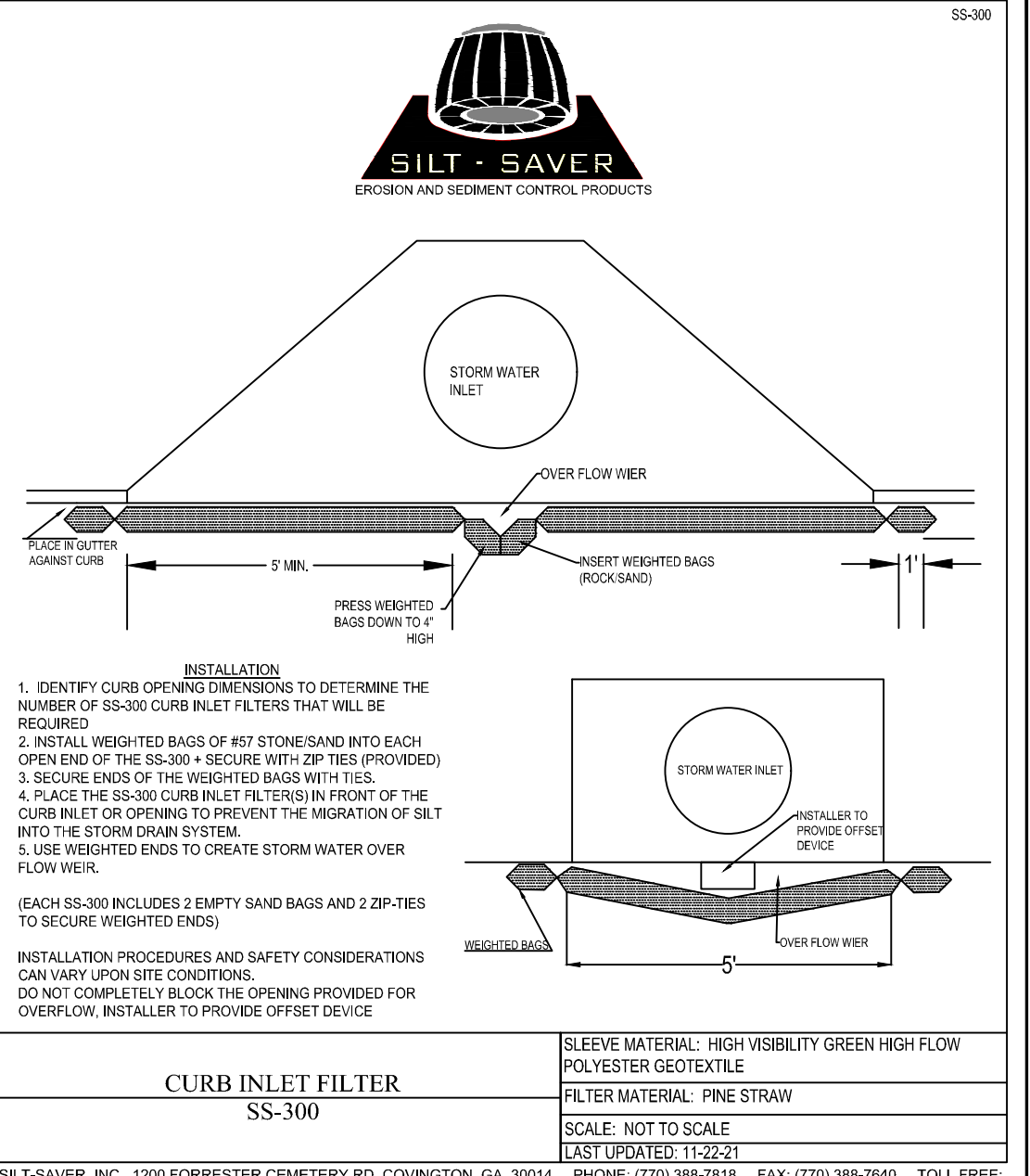
**EDUCATION FOR CONCRETE SUBCONTRACTORS**

MAKE SITE SUPERINTENDENT AWARE OF THE PRESENCE OF THESE FACILITIES. SITE SUPERINTENDANT SHALL EDUCATE CONCRETE SUBCONTRACTORS, POST SIGNAGE INDICATING THE LOCATION AND DESIGNATED USE OF THESE AREAS, AND PROVIDE CAREFUL OVERSIGHT TO INSPECT FOR EVIDENCE OF IMPROPER DUMPING OF CONCRETE WASTE AND WASH WATER. INCLUDE REQUIREMENTS IN CONTRACTS WITH CONCRETE DELIVERY COMPANIES THAT DRIVERS MUST USE DESIGNATED CONCRETE WASHOUT FACILITIES.

SEE DRAWINGS CD1.1 FOR CONCRETE (CONC.) WASHOUT LOCATION.



**3 CONCRETE WASHOUT FACILITY**  
C-400 N.T.S.



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**4 CURB INLET FILTER**  
C-400 N.T.S.

INSTALLATION PROCEDURES AND SAFETY CONSIDERATIONS CAN VARY UPON SITE CONDITIONS. DO NOT COMPLETELY BLOCK THE OPENING PROVIDED FOR OVERFLOW, INSTALLER TO PROVIDE OFFSET DEVICE

INSTALLATION  
 1. IDENTIFY CURB OPENING DIMENSIONS TO DETERMINE THE NUMBER OF SS-300 CURB INLET FILTERS THAT WILL BE REQUIRED  
 2. INSTALL WEIGHTED BAGS OF #57 STONE/SAND INTO EACH OPEN END OF THE SS-300 + SECURE WITH ZIP TIES (PROVIDED)  
 3. SECURE ENDS OF THE WEIGHTED BAGS WITH TIES.  
 4. PLACE THE SS-300 CURB INLET FILTER(S) IN FRONT OF THE CURB INLET OR OPENING TO PREVENT THE MIGRATION OF SILT INTO THE STORM DRAIN SYSTEM.  
 5. USE WEIGHTED ENDS TO CREATE STORM WATER OVER FLOW WEIR.

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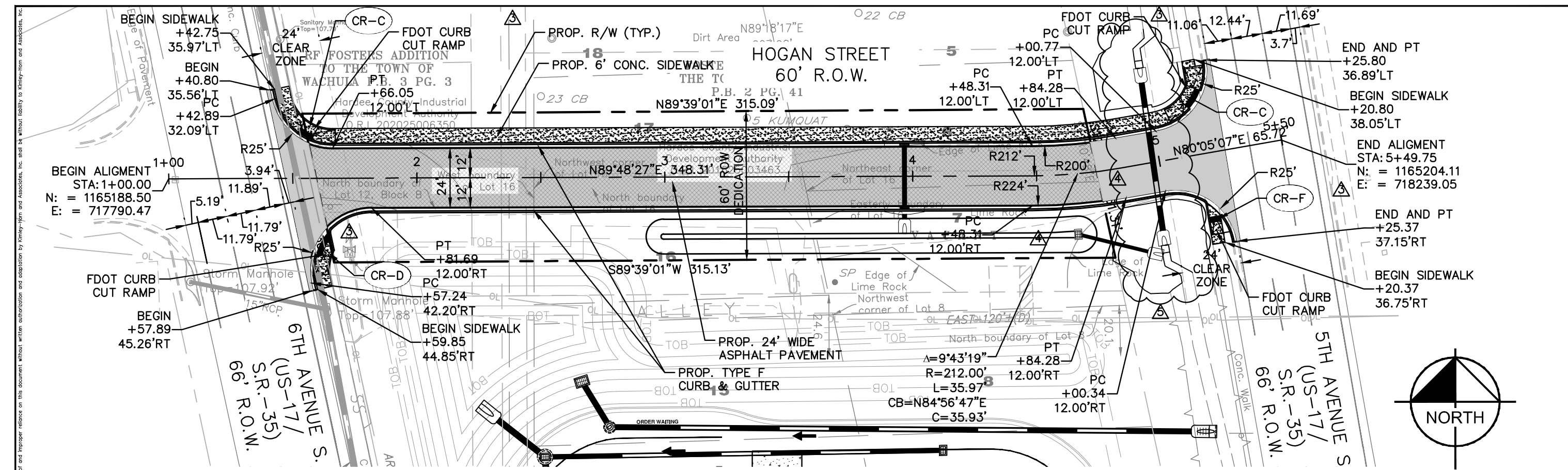
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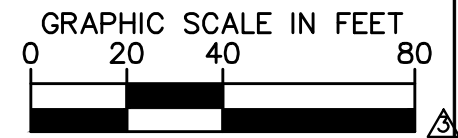
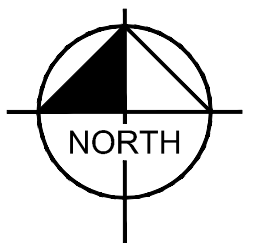
**HOGAN STREET EXTENSION IMPROVEMENTS**  
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**EROSION AND SEDIMENTATION CONTROL DETAILS**

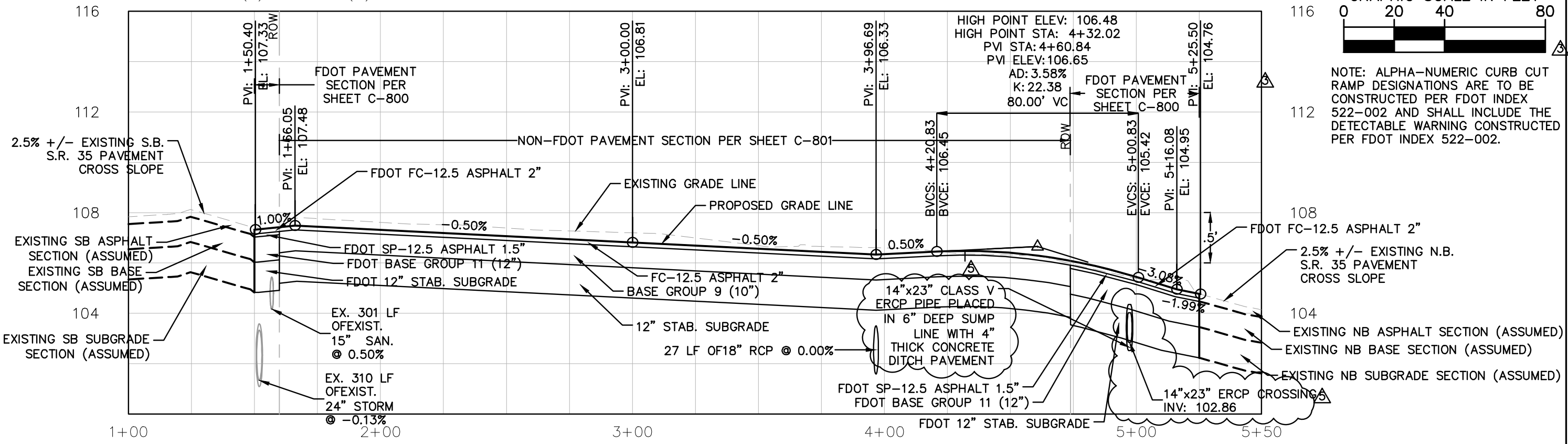
SHEET NUMBER  
**C-401**



HOGAN STREET  
SCALE: 1" = 4'(V), 1" = 40'(H)



NOTE: ALPHA-NUMERIC CURB CUT RAMP DESIGNATIONS ARE TO BE CONSTRUCTED PER FDOT INDEX 522-002 AND SHALL INCLUDE THE DETECTABLE WARNING CONSTRUCTED PER FDOT INDEX 522-002.



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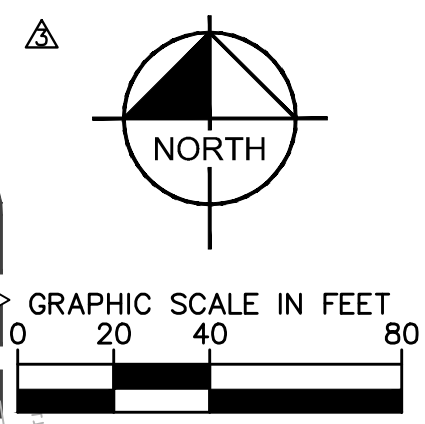
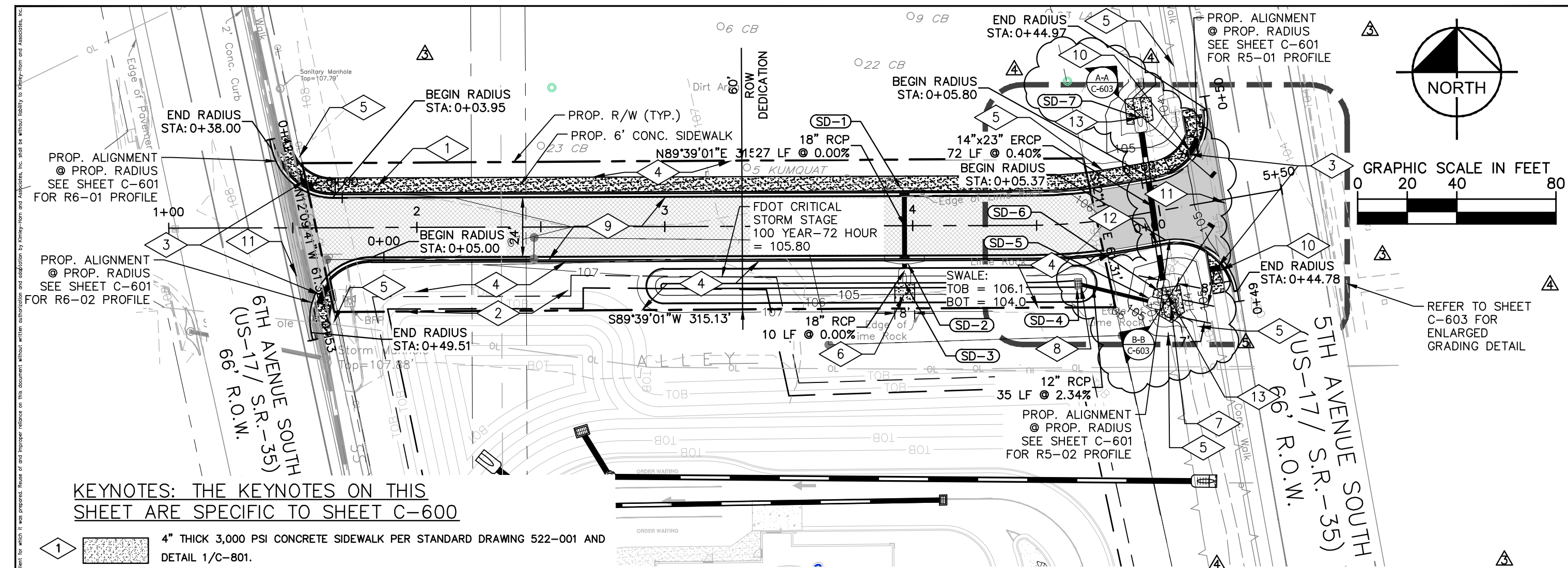
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DATE	SEPT 2025
SCALE	AS SHOWN
DESIGNED BY	MSD
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PREPARED FOR  
**CITY OF WAUCHULA**

CITY OF WAUCHULA FLORIDA

<b>PLAN AND PROFILE</b>		SHEET NUMBER <b>C-500</b>
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KEYNOTES: THE KEYNOTES ON THIS SHEET ARE SPECIFIC TO SHEET C-600

- 1 4" THICK 3,000 PSI CONCRETE SIDEWALK PER STANDARD DRAWING 522-001 AND DETAIL 1/C-801.
- 2 DO NOT DISTURB EXISTING GRADING AND GRASSING IN DRY DETENTION BASIN THAT ENCREACHES IN R.O.W.
- 3 ALPHA-NUMERIC CURB CUT RAMP PER STANDARD DRAWING 522-002. SEE SHEET C-500 FOR IDENTIFICATION.
- 4 GRASSED AREA OF HOGAN STREET R.O.W. SHALL BE SODDED WITH BAHIA PERFORMANCE TURF PER FDOT SPECIFICATION SECTION 570. MAINTAIN BY WATERING AND MOWING UNTIL SOD IS ESTABLISHED.
- 5 ALL DISTURBED FDOT R.O.W. AREA SHALL BE SODDED WITH BAHIA SOD TO MATCH EXISTING TURF AND SHALL BE WATERED AND MAINTAINED UNTIL ESTABLISHED.
- 6 8'X8' 3,000 PSI CONCRETE SPLASH APRON IN BOTTOM OF SWALE SHALL TIE INTO MITERED END SECTION CONCRETE COLLAR AND COVER FLOOR AND OPPOSITE SLOPE. APRON TOP SHALL BE FLUSH WITH SWALE FLOOR AND SIDE SLOPE FINISH GRADES. APRON SHALL BE LAID ON PROOF COMPACTED SUBGRADE.
- 7 7'X8' 3,000 PSI CONCRETE SPLASH APRON IN BOTTOM OF SWALE SHALL TIE INTO MITERED END SECTION CONCRETE COLLAR AND COVER FLOOR AND OPPOSITE SLOPE. APRON TOP SHALL BE FLUSH WITH SWALE FLOOR AND SIDE SLOPE FINISH GRADES. DO NOT INTERFERE WITH EXISTING SIDE DRAIN FLOWLINE. APRON SHALL BE LAID ON PROOF COMPACTED SUBGRADE.
- 8 CONTROL STRUCTURE SD-4 SEE DETAIL 1/C-800
- 9 TYPE F CURB AND GUTTER PER DETAIL 2/C-800
- 10 DETECTABLE WARNING IN SIDEWALK CURB RAMP PER STANDARD PLANS INDEX 522-002.

STRUCTURE TABLE	
STRUCTURE:	DETAILS:
SD-1 N: 1165202.84 E: 718087.11	CURB INLET TYPE '6' FDOT 425-021 EOP: 106.17 INV: 101.80 (S)
SD-2 N: 1165176.17 E: 718087.20	CURB INLET TYPE '6' FDOT 425-021 EOP: 106.17 INV: 101.80 (N) INV: 101.80 (S)
SD-3 N: 1165166.50 E: 718087.24	18" MES 103.59 INV: 101.80 (N)
SD-4 N: 1165166.08 E: 718157.31	TYPE 'C' INLET FDOT 425-052 GRATE: 105.21 INV: 103.55 (E)

STRUCTURE TABLE	
STRUCTURE:	DETAILS:
SD-5 N: 1165157.65 E: 718191.38	12" MES 103.97 INV: 102.72 (W)
SD-6 N: 1165161.71 E: 718192.22	14"X23" EMES 104 INV: 102.72 (N)
SD-7 N: 1165233.09 E: 718181.95	14"X23" EMES 104 INV: 103.01 (S)

- LEGEND**
- TYPICAL HOGAN STREET PAVEMENT SECTION (OUTSIDE FDOT ROW LIMITS) ON SHEET C-801
  - TYPICAL HOGAN STREET PAVEMENT SECTION (WITHIN FDOT ROW LIMITS) ON SHEET C-800
  - PROPOSED CONCRETE SIDEWALK
  - PROPOSED CONCRETE SPLASHPAD
  - 11 PROPOSED TO EXISTING ASPHALT PAVEMENT CONNECTION PER DETAIL 3/C-800
  - 12 CONSTRUCT 72 LF OF 14"X23" CLASS V ERCP PER FDOT INDEX 430-001 AND 2 X 14"X23" EMES PER FDOT INDEX 430-022
  - 13 MITERED END SECTIONS SHALL BE SET IN SUMP NOMINALLY 6" DEEPER THAN EXISTING SWALE FLOOR LINED WITH 4" THICK CONCRETE DITCH PAVEMENT

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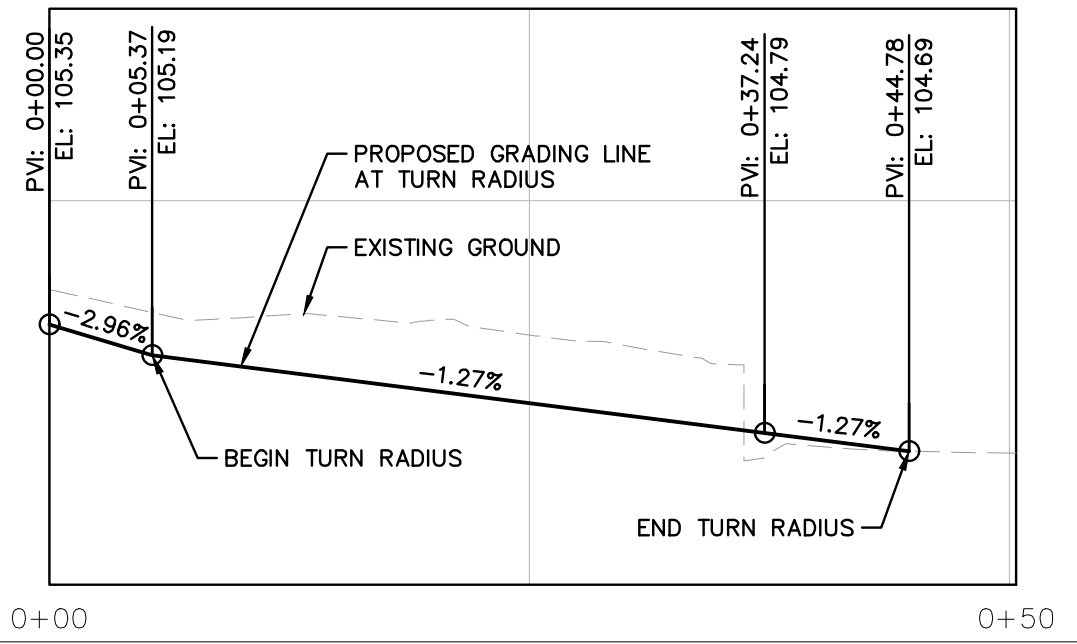
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GRADING AND DRAINAGE

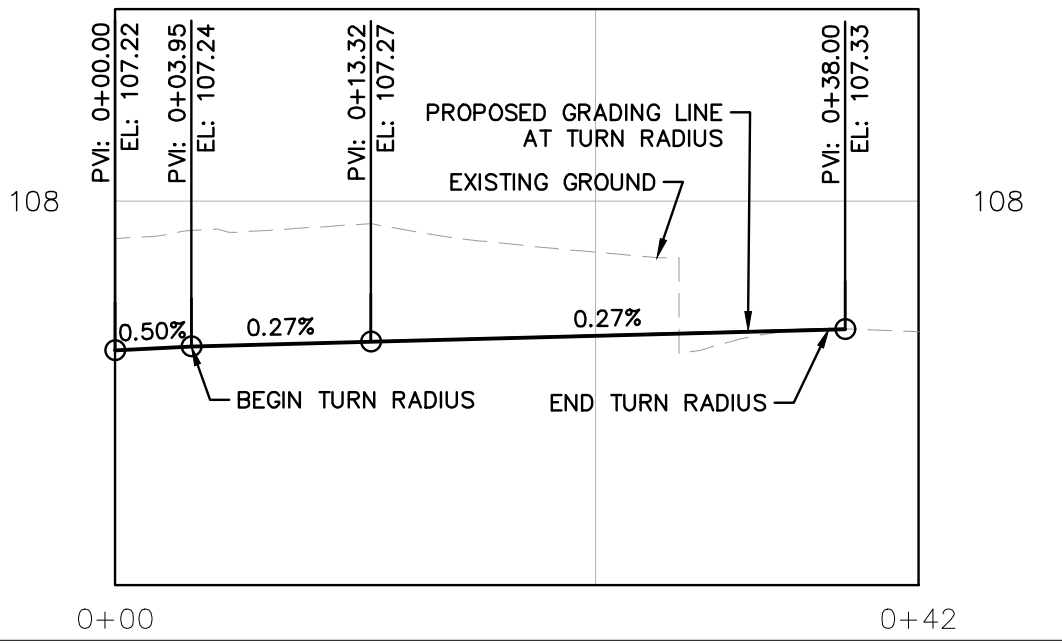
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**C-600**

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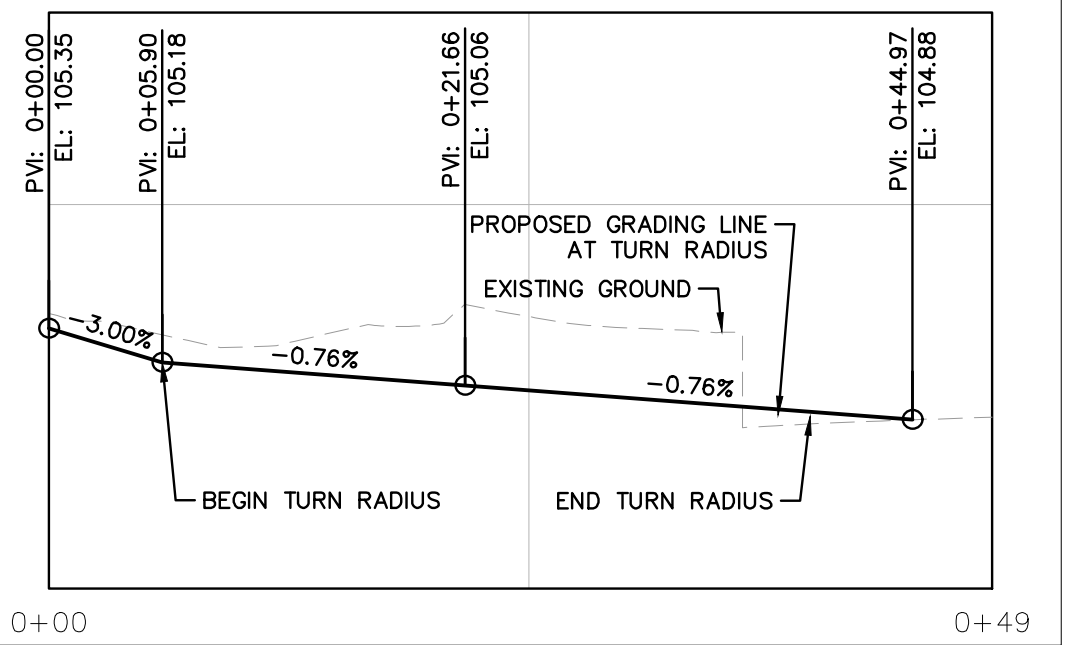
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 SCALE (VERT.): 1" = 10'



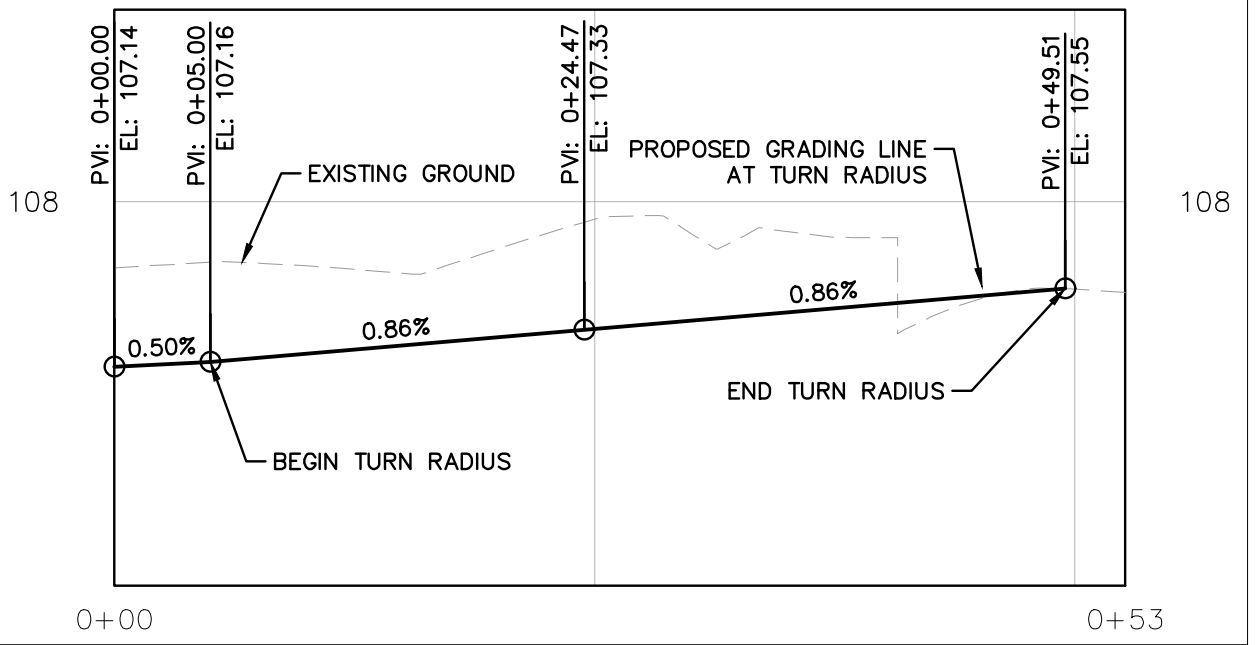
PROFILE R6-01  
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 SCALE (VERT.): 1" = 10'



PROFILE R5-02  
 SCALE (HORZ.): 1" = 2'  
 SCALE (VERT.): 1" = 10'



PROFILE R6-02  
 SCALE (HORZ.): 1" = 2'  
 SCALE (VERT.): 1" = 10'



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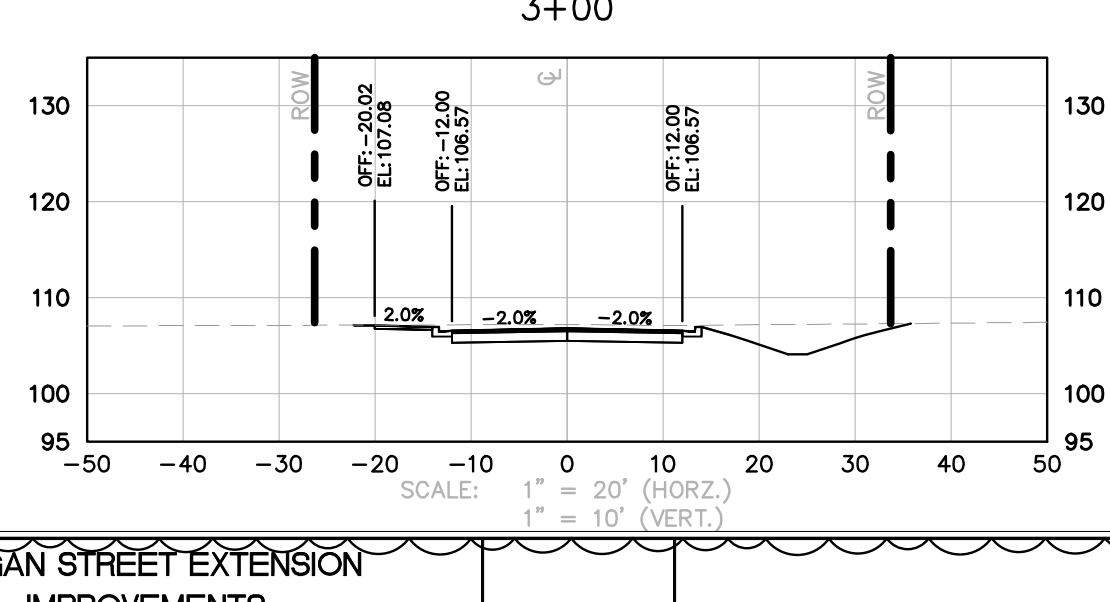
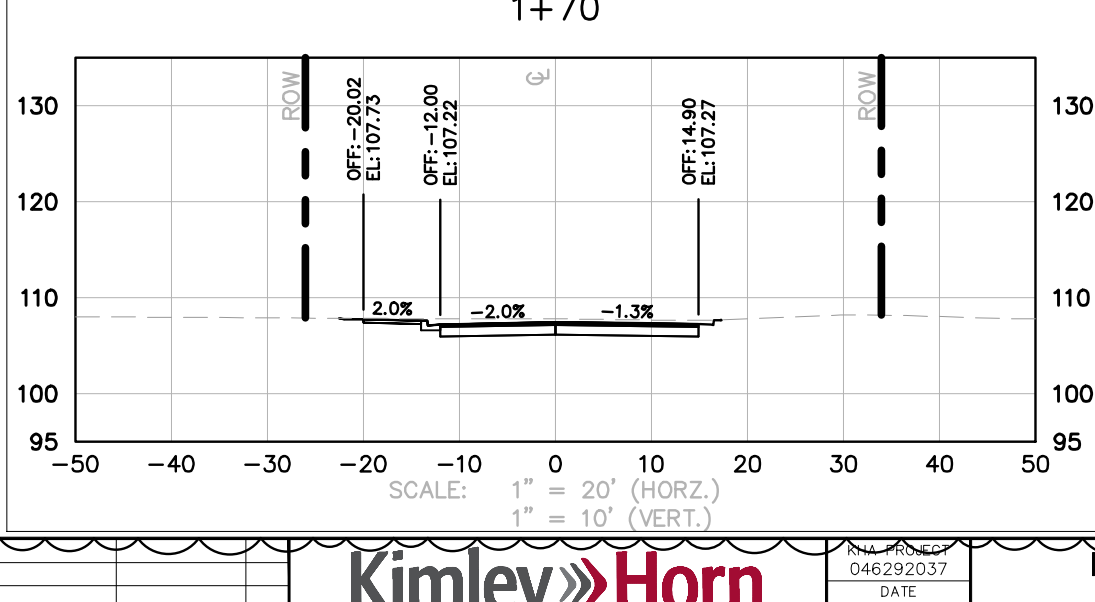
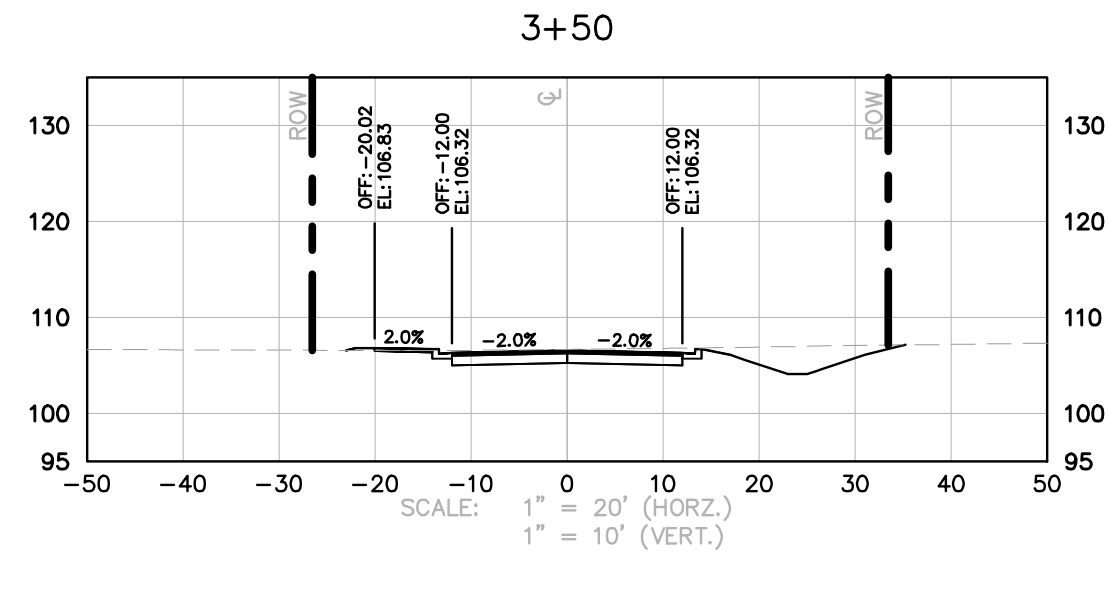
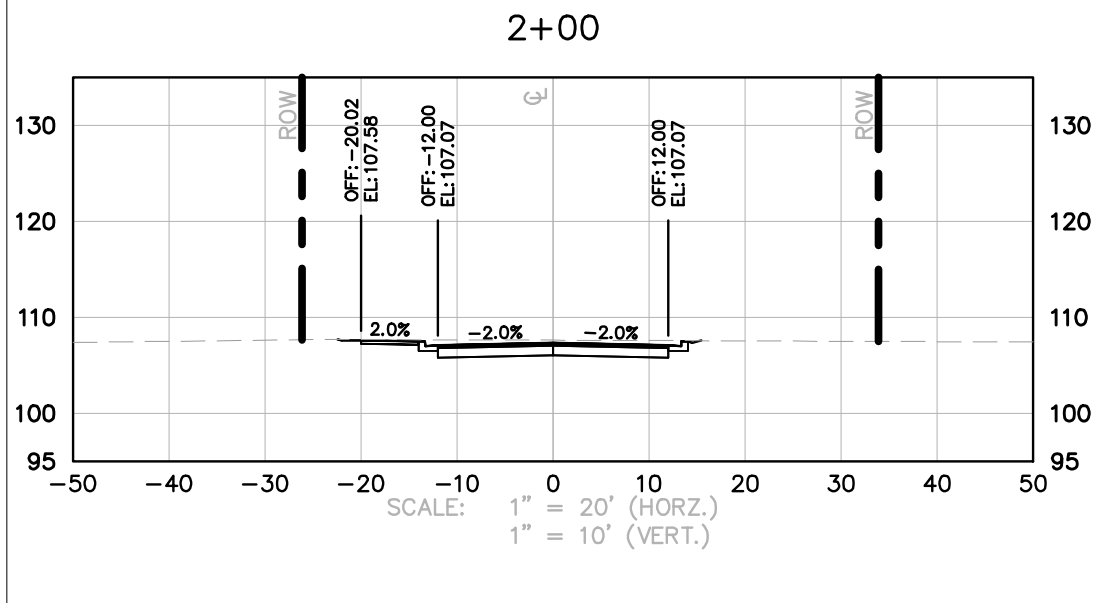
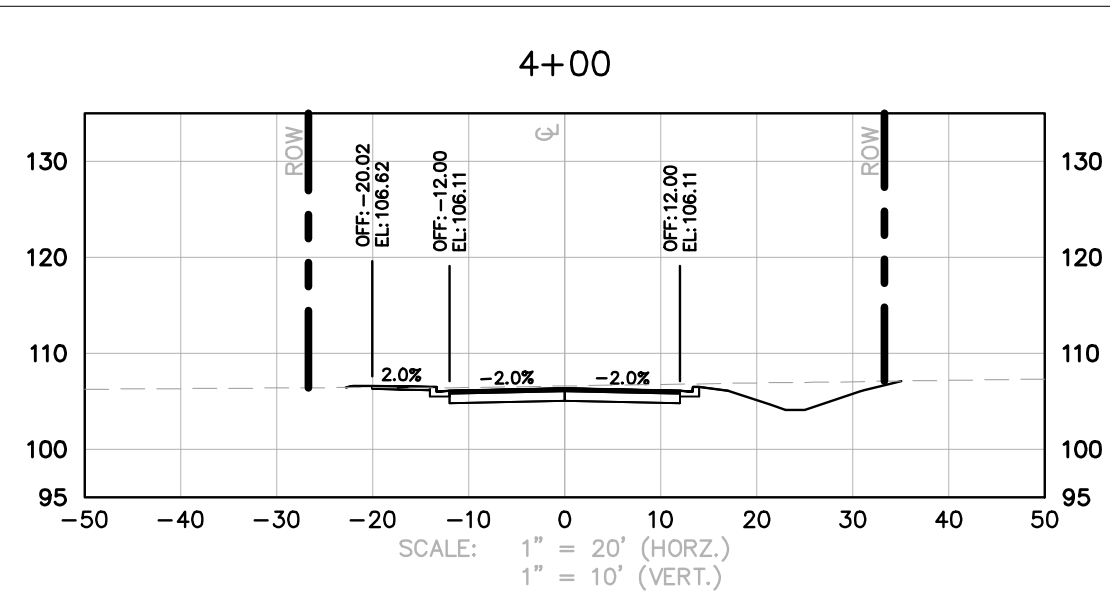
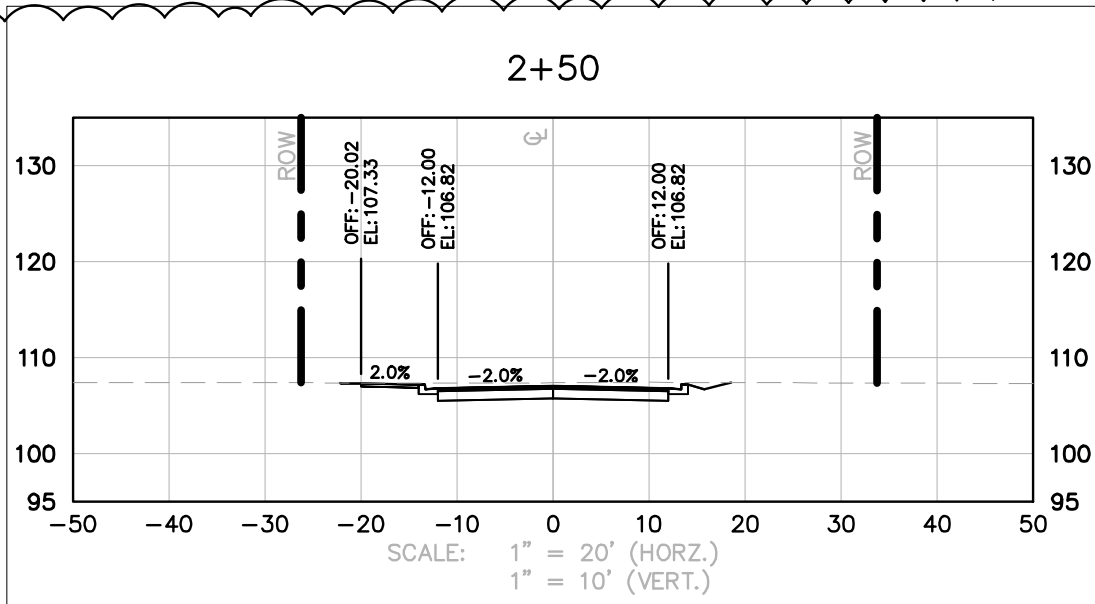
KHA PROJECT	046292037
DATE	SEPT 2025
SCALE	AS SHOWN
DESIGNED BY	MSD
DRAWN BY	KM
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HOGAN STREET EXTENSION  
 IMPROVEMENTS  
 PREPARED FOR  
 CITY OF WAUCHULA  
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PROPOSED GRADING LINE PROFILES  
 FOR TURNING RADIUS

SHEET NUMBER  
**C-601**

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 K:\ok\_civil\146292037 - Hogan Street Extension\CAD\plansheets\C-602 CROSS SECTIONS (1).dwg  
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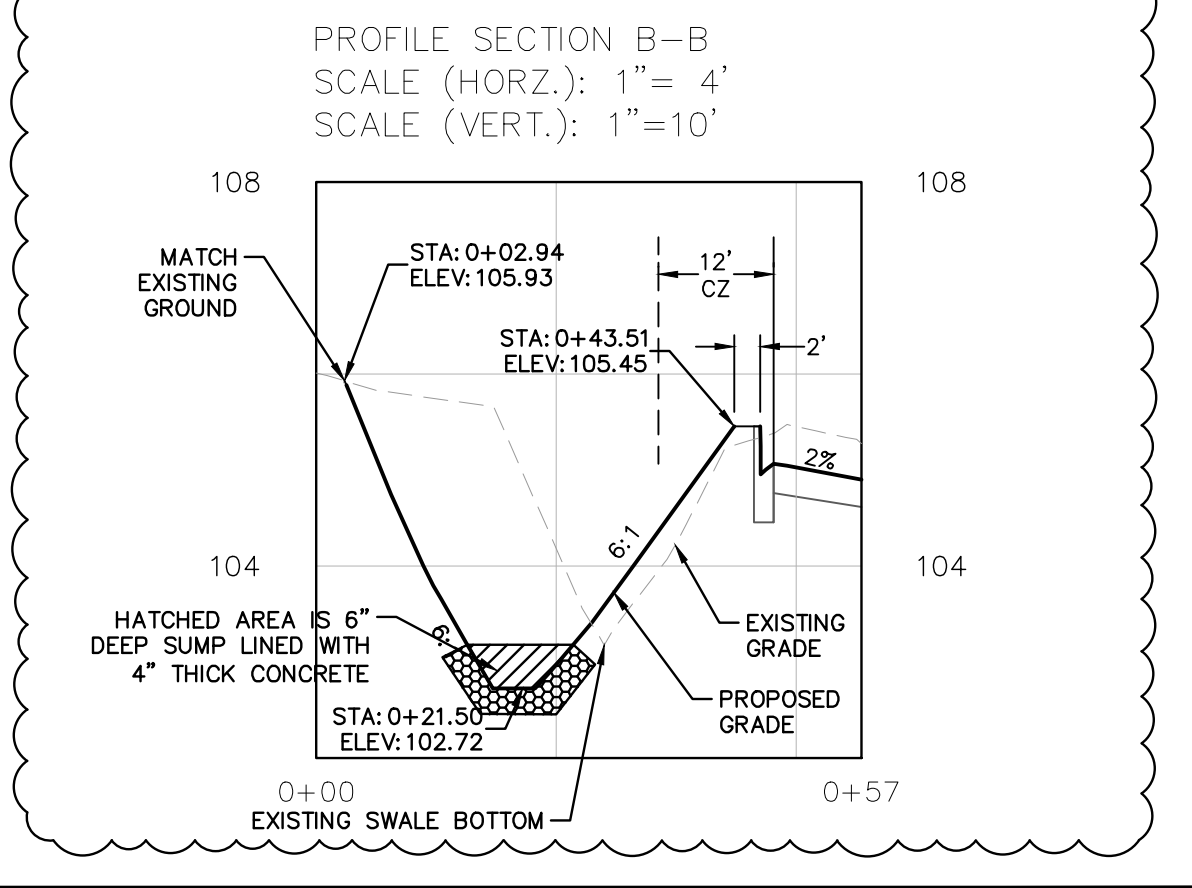
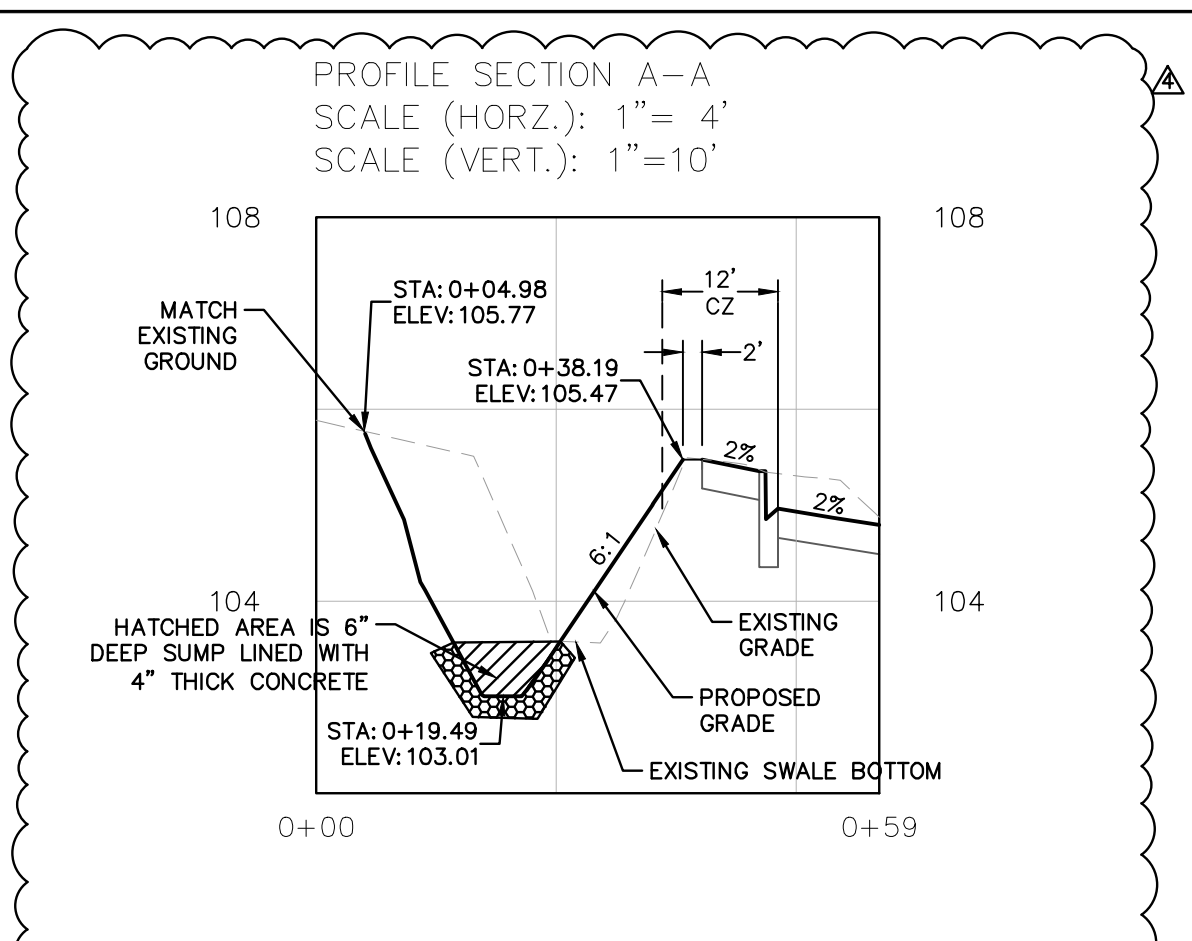
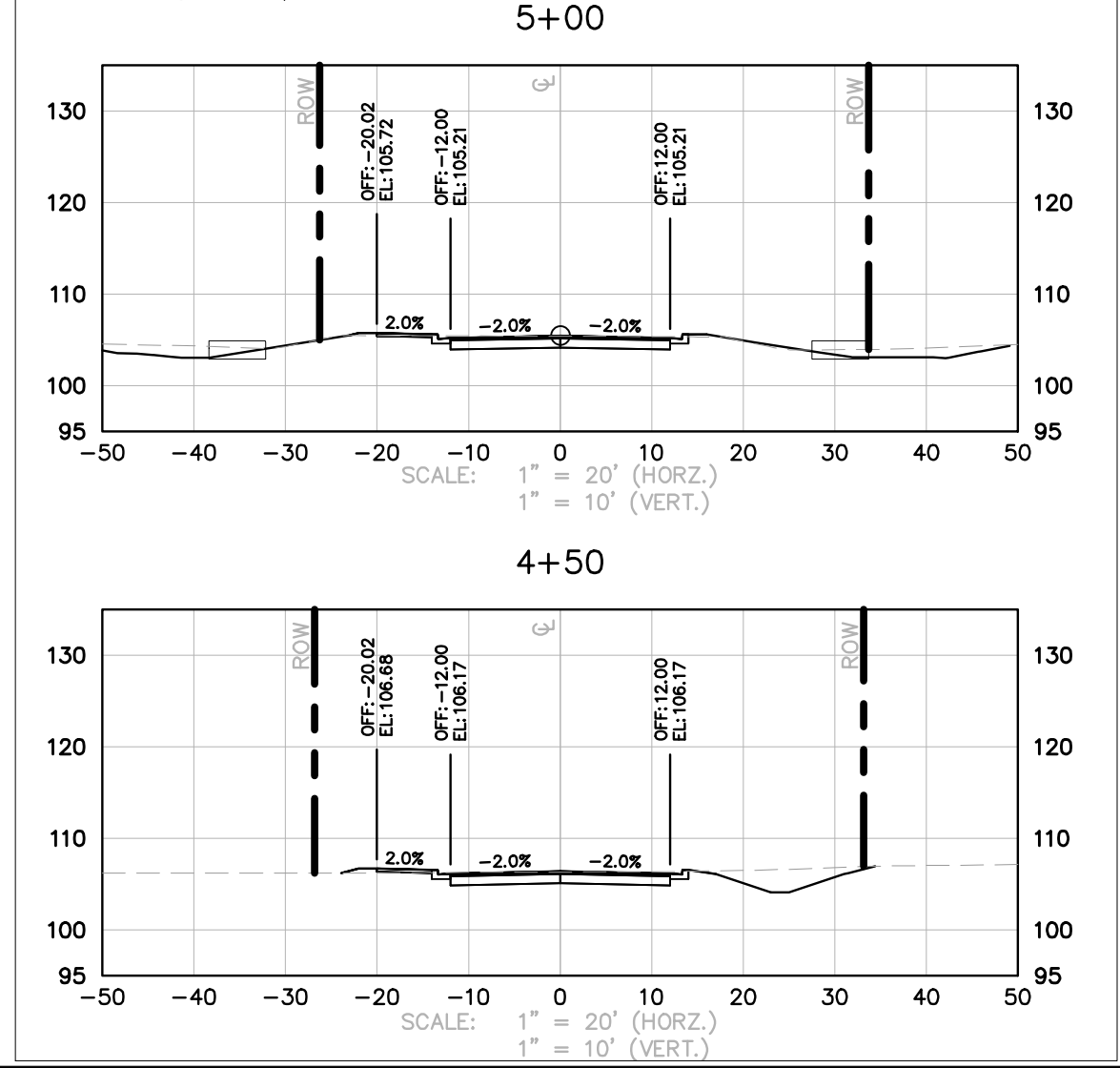
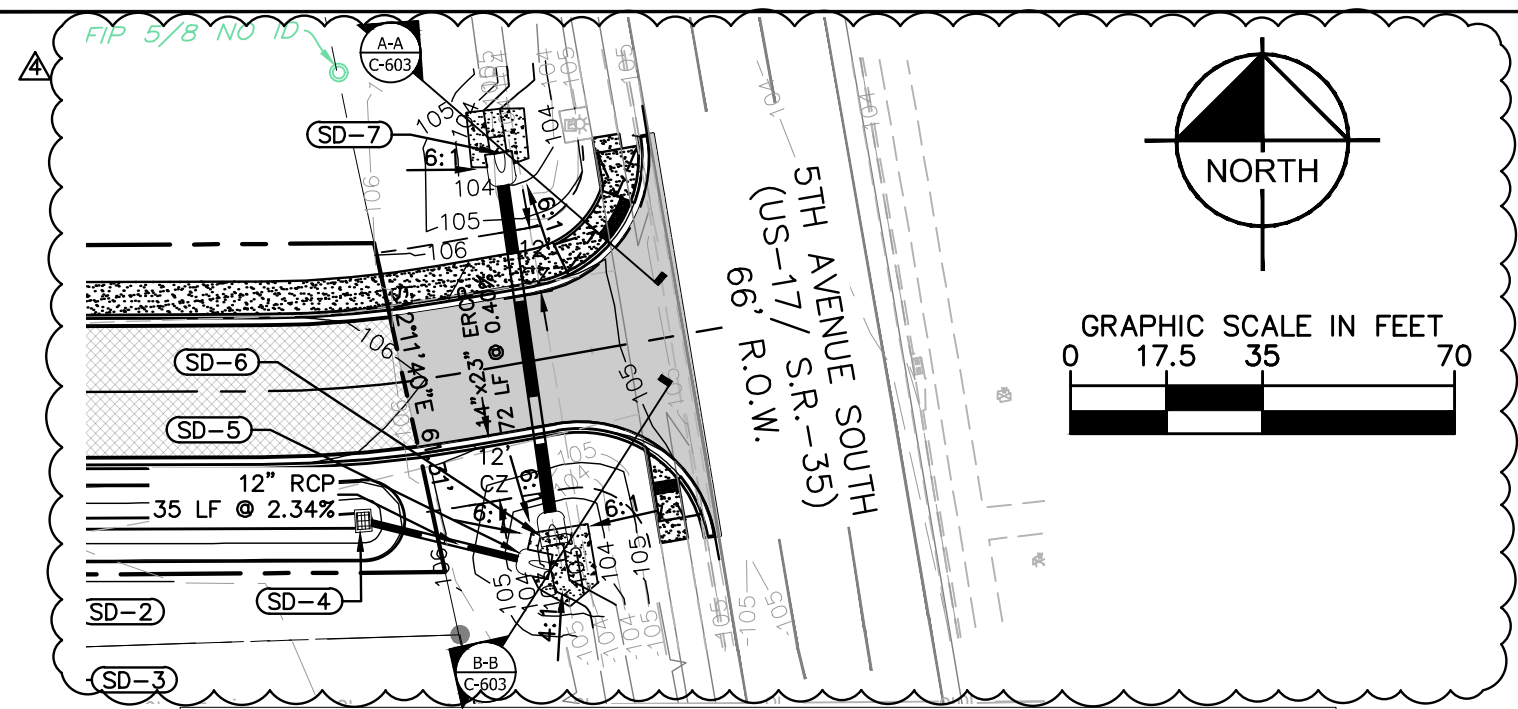
KNA PROJECT	046292037
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**CROSS SECTIONS (1)**

SHEET NUMBER  
**C-602**

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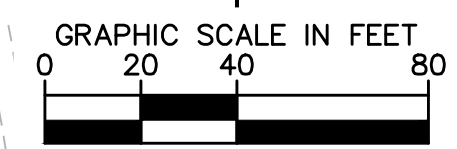
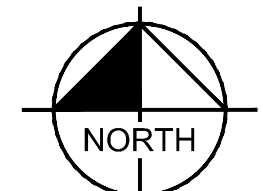
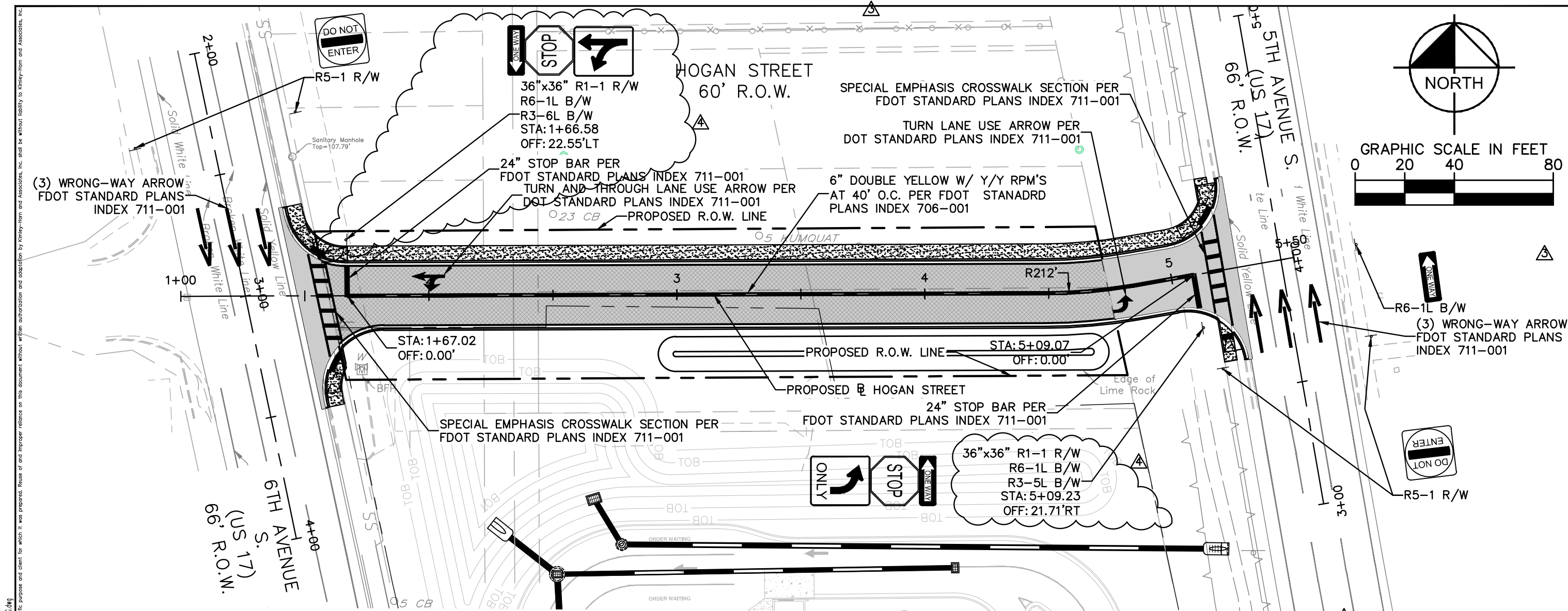
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3	PER FDOT COMMENTS	11/25/25	MSD

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HOGAN STREET EXTENSION  
 IMPROVEMENTS  
 PREPARED FOR  
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 CITY OF WAUCHULA  
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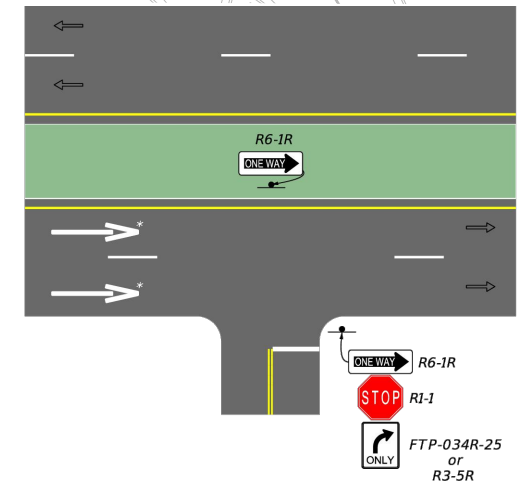
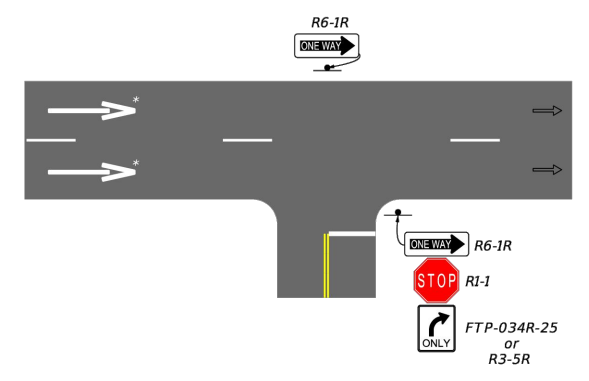
CROSS SECTIONS (2)  
 SHEET NUMBER  
**C-603**



**Installation Details**

- 1) R6-1 shall be placed on far side median or shoulder depending on facility type.
- 2) RIGHT TURN ARROW (FTP-034R-25) or LEFT TURN ARROW (FTP-034L-25) typically added by district driveway permit.
- 3) Mandatory Movement Lane Control (R3-5) sign must be added to side street.

\* See FDM 230.4.4 for how often to place Wrong-Way arrows based on context classification.



**Legend**

→ Direction of travel  
 → Wrong Way Arrow

NOT TO SCALE  
 EXHIBIT 230-5

- NOTES:**
1. REFERENCE FDOT STANDARD PLANS INDEX NO. 706-001 FOR TYPICAL PLACEMENT OF REFLECTIVE PAVEMENT MARKERS.
  2. REFERENCE FDOT STANDARD PLANS INDEX NO. 711-001 FOR TYPICAL PLACEMENT OF MARKING AND SIGNAGE.

CALL 2 WORKING DAYS BEFORE YOU DIG

IT'S THE LAW! DIAL 811

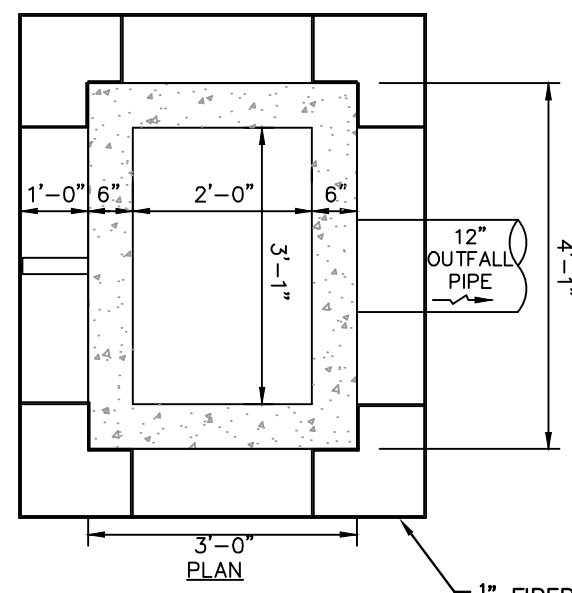
Know what's below. Call before you dig.

SUNSHINE STATE ONE CALL OF FLORIDA, INC.

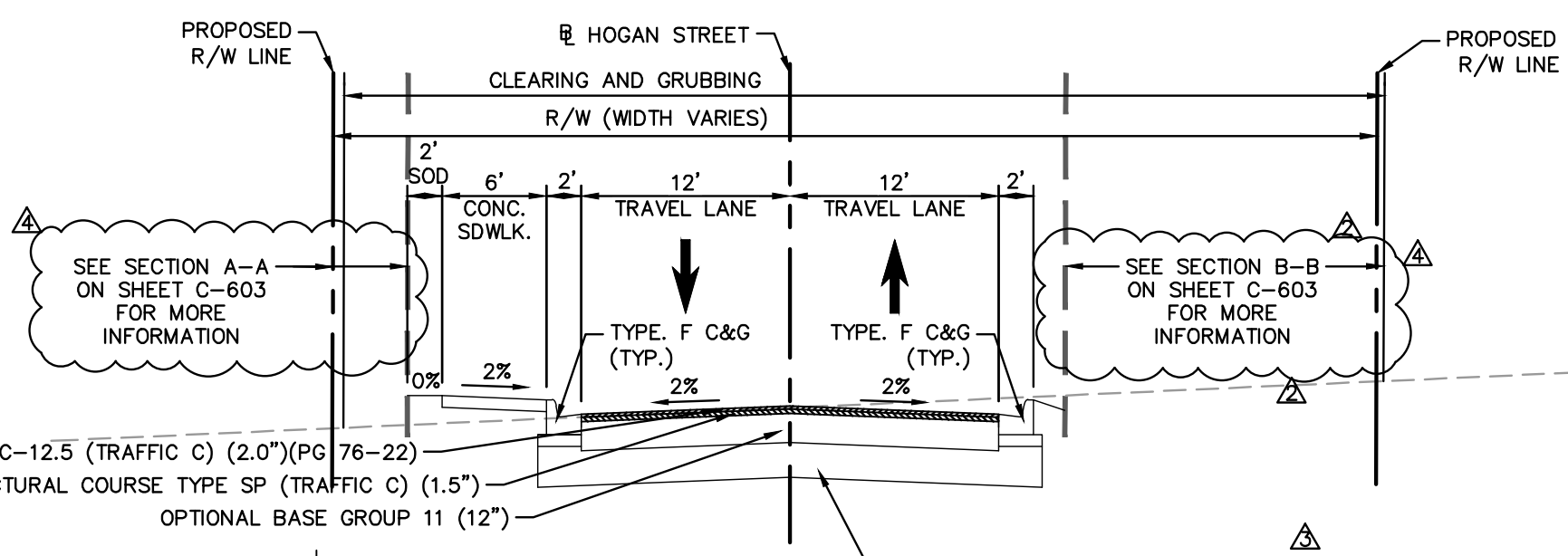
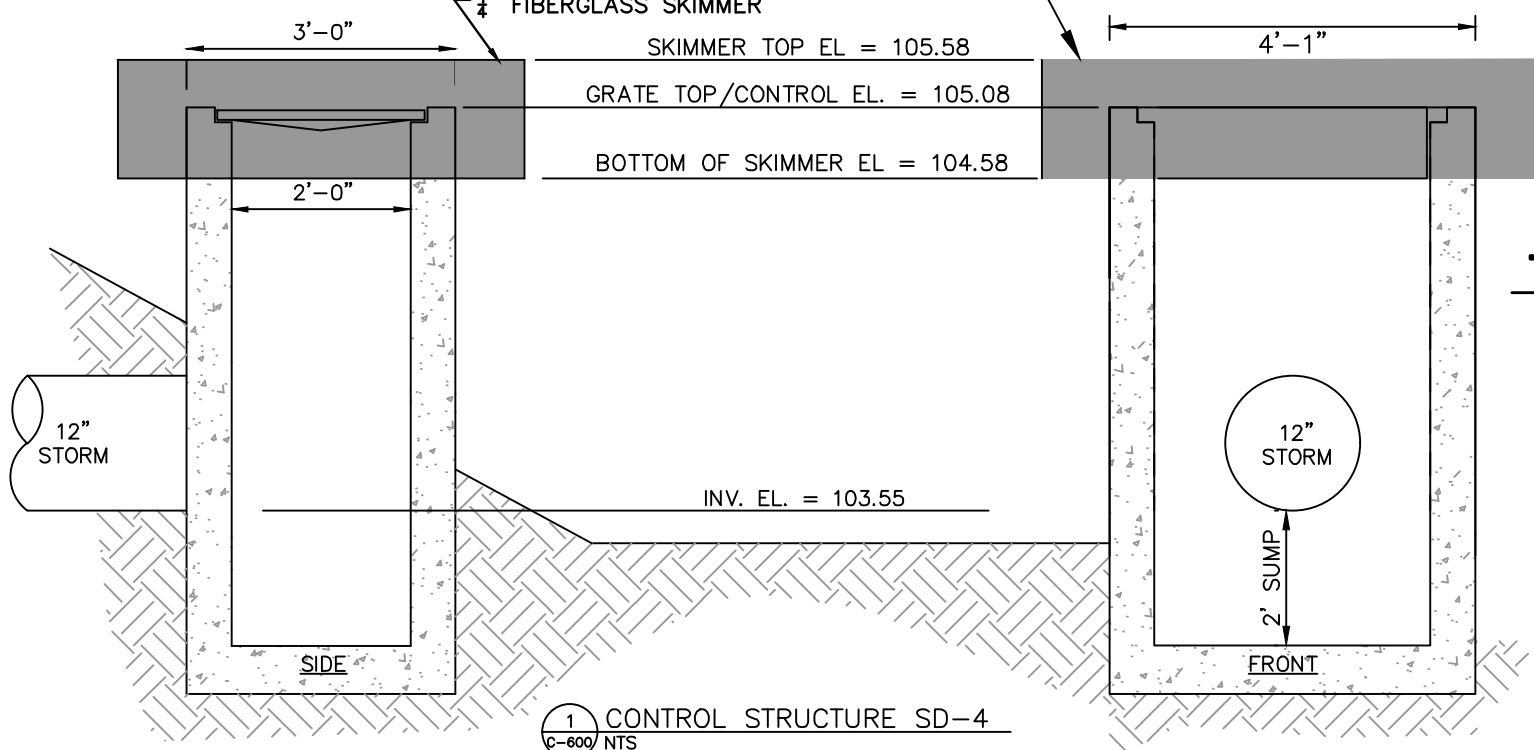
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NOTE:  
 1. CONTRACTOR IS TO GRADE AROUND CONTROL STRUCTURE TO ALLOW FOR 6" MINIMUM BETWEEN BOTTOM OF SKIMMER AND SLOPE OR BOTTOM OF POND ON THE NORTH, SOUTH AND EAST SIDE.  
 2. A CONCRETE PAD SHALL BE INSTALLED UNDER THE SKIMMER WHERE SEPARATION BETWEEN THE SKIMMER AND POND BOTTOM/BERM IS LESS THAN 12". THE CONCRETE PAD SHALL EXTEND FROM THE CONTROL STRUCTURE TO 12" BEYOND THE SKIMMER.

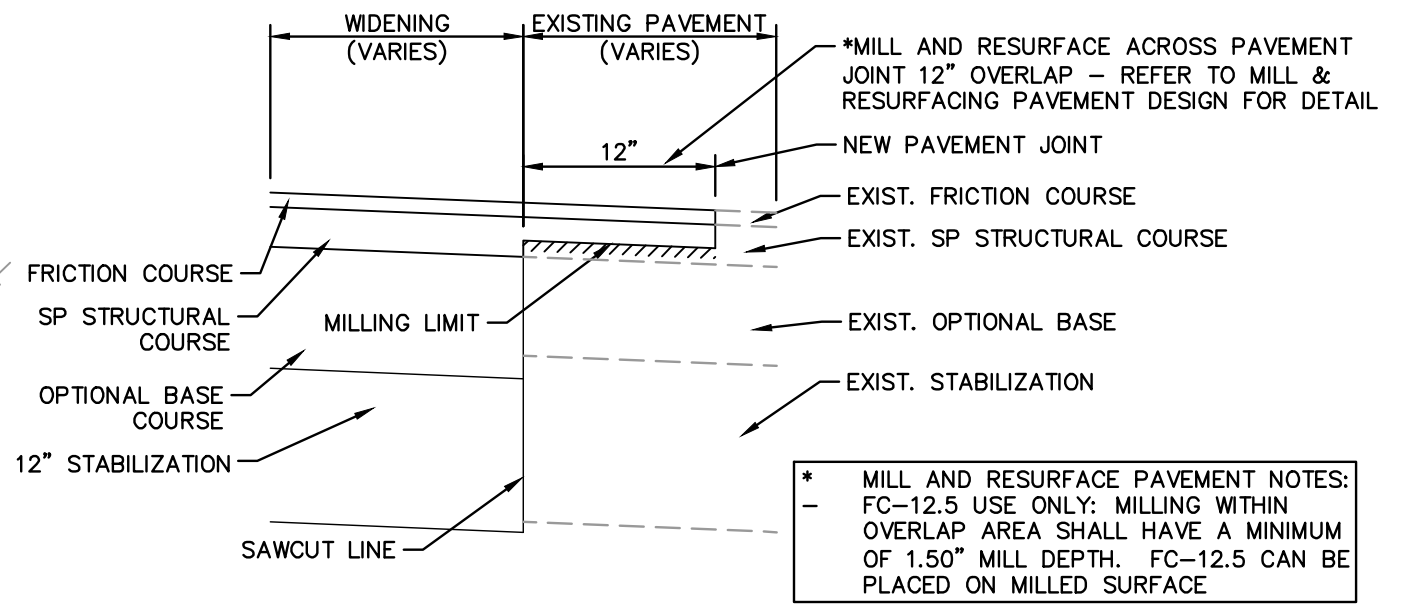


**WIDENING**

STABILIZED SUBGRADE: TYPE-B 12" STABILIZATION (LBR 40)  
 BASE: OPTIONAL BASE GROUP 11  
 STRUCTURAL COURSE: TYPE SP STRUCTURAL COURSE (TRAFFIC C)(1.5")  
 FRICTION COURSE: FC-12.5 (TRAFFIC C)(2.0")(PG 76-22)

**TYPICAL SECTION HOGAN STREET (WITHIN FDOT ROW LIMITS)**

STA. 1+48.32 TO STA. 5+25.58  
 (DESIGN SPEED 35 MPH)



**3 SAWCUT DETAIL**  
C-401 NTS

No.	REVISIONS	DATE	BY
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3	PER FDOT COMMENTS	11/25/25	MSD
4	PER WMD COMMENTS	11/19/25	MSD

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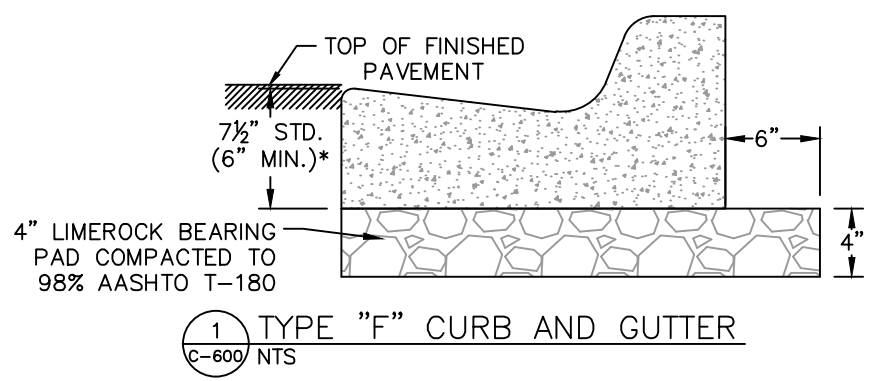
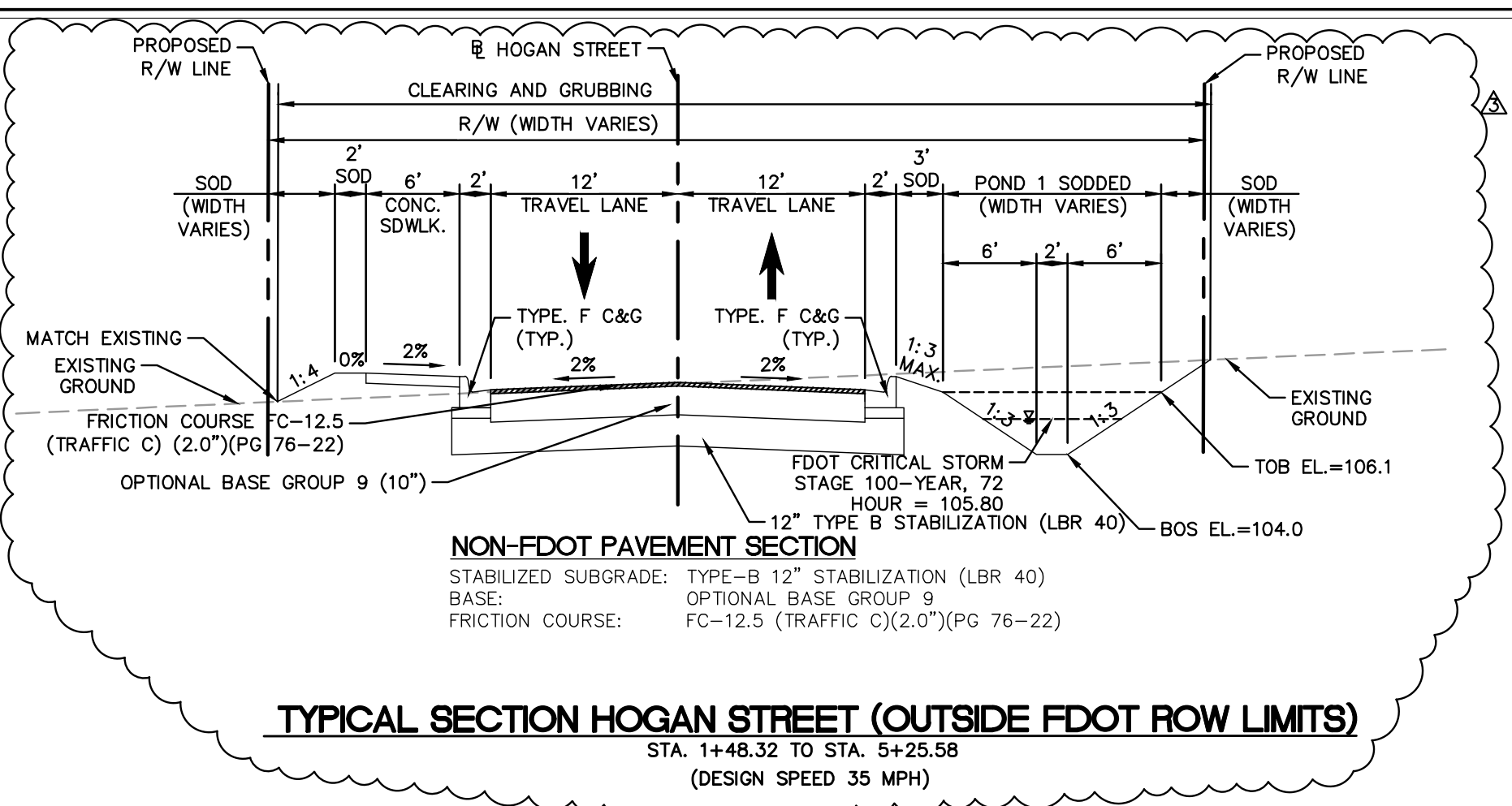
KHA PROJECT	046292037
DATE	SEPT 2025
SCALE	AS SHOWN
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**HOGAN STREET EXTENSION IMPROVEMENTS**  
 PREPARED FOR  
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 CITY OF WAUCHULA FLORIDA

**TYPICAL SECTION AND DETAILS**

SHEET NUMBER  
**C-800**

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**NON-FDOT PAVEMENT SECTION**  
 STABILIZED SUBGRADE: TYPE-B 12" STABILIZATION (LBR 40)  
 BASE: OPTIONAL BASE GROUP 9  
 FRICTION COURSE: FC-12.5 (TRAFFIC C)(2.0")(PG 76-22)

**TYPICAL SECTION HOGAN STREET (OUTSIDE FDOT ROW LIMITS)**  
 STA. 1+48.32 TO STA. 5+25.58  
 (DESIGN SPEED 35 MPH)

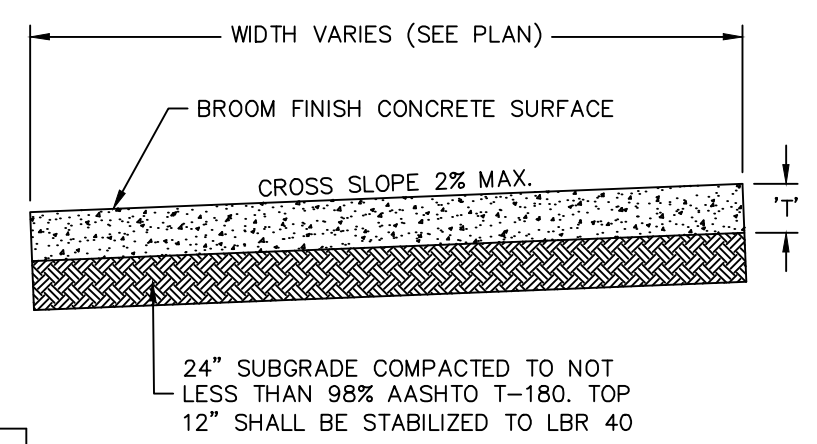
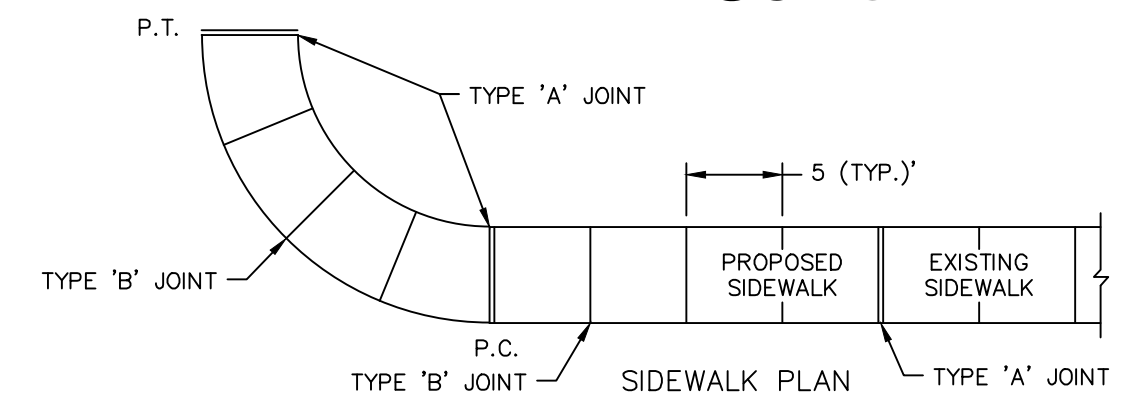


TABLE OF SIDEWALK THICKNESS	
LOCATION	'T'
PEDESTRIAN ONLY AREAS	5"
VEHICULAR ACCESSIBLE AREAS	6"

NOTE:  
 ALL SIDEWALK STREET CROSSINGS MUST MEET THE REQUIREMENTS OF THE AMERICAN DISABILITIES ACT (ADA) COMPLIANCE GUIDE. TRANSITIONS FROM RAMP TO WALKS, GUTTERS, OR STREETS SHALL BE FLUSH AND FREE OF ABRUPT CHANGES.

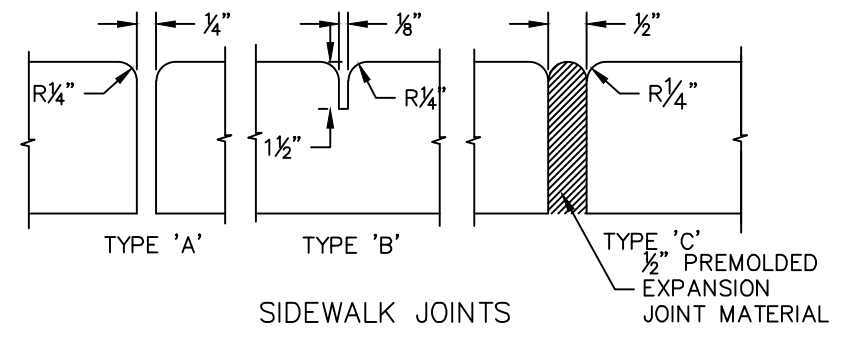


TABLE OF SIDEWALK JOINTS	
TYPE	LOCATION
'A'	100' MAX. SPACING, P.C./P.T. OF CURVES, JUNCTION OF EXISTING AND NEW SIDEWALKS.
'B'	5'-0" CENTER TO CENTER
'C'	WHERE SIDEWALK ABUTS CONCRETE CURBS, DRIVEWAYS AND SIMILAR STRUCTURES.

No.	REVISIONS	DATE	BY
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3	PER WMD COMMENTS	11/19/25	MSD

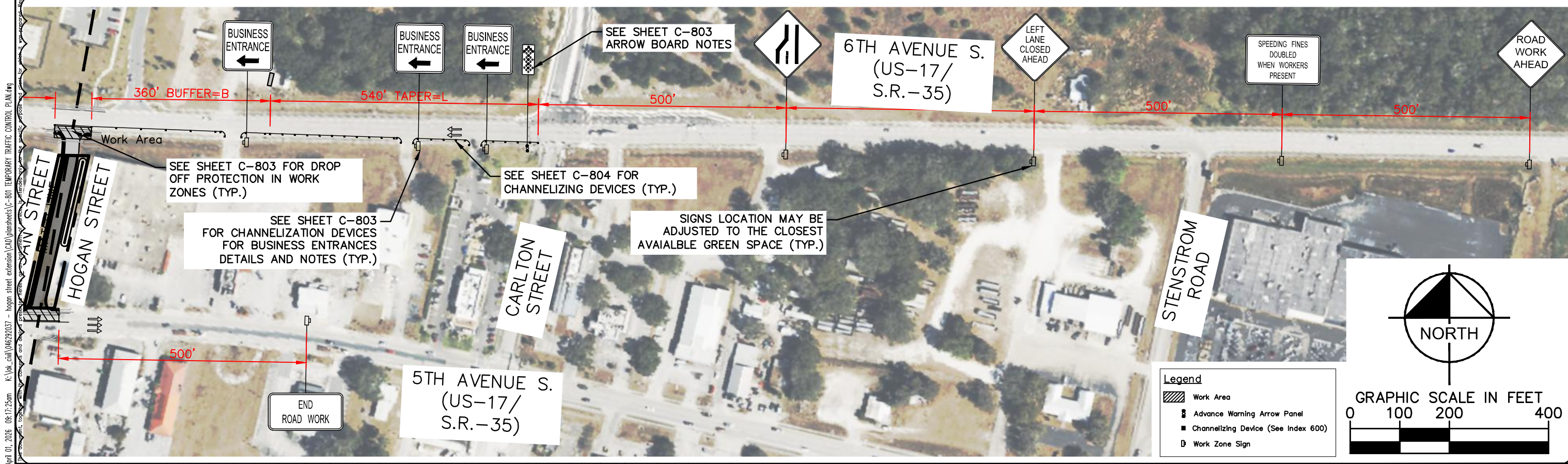
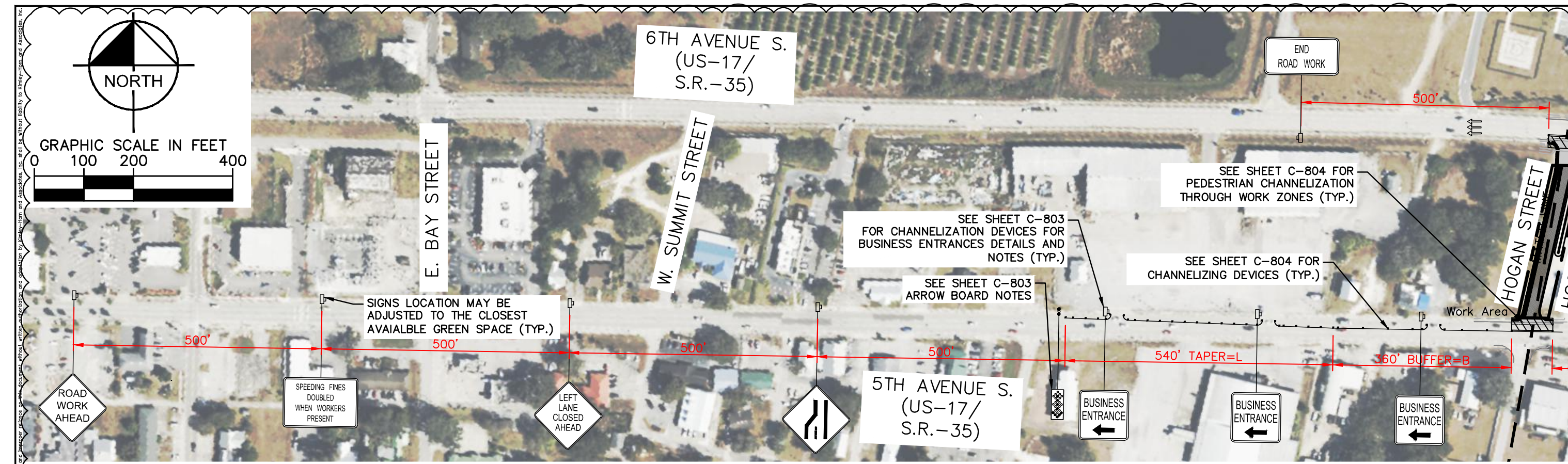
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**TYPICAL SECTION AND DETAILS**

SHEET NUMBER  
**C-801**



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 10/10/2026 10:00:00 AM

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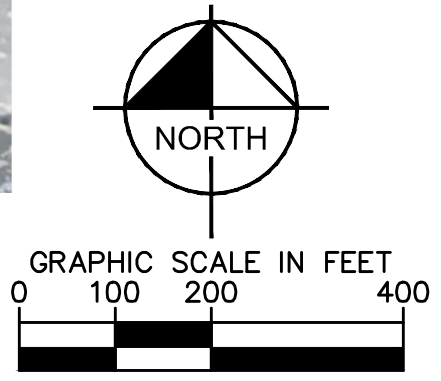
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**TEMPORARY TRAFFIC CONTROL PLAN**  
 SHEET NUMBER  
**C-802**

**Legend**

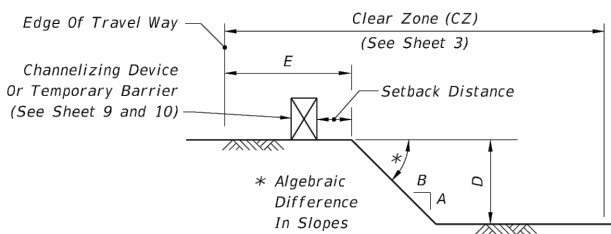
- Work Area
- Advance Warning Arrow Panel
- Channelizing Device (See Index 600)
- Work Zone Sign



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**DROP-OFF CONDITION NOTES**

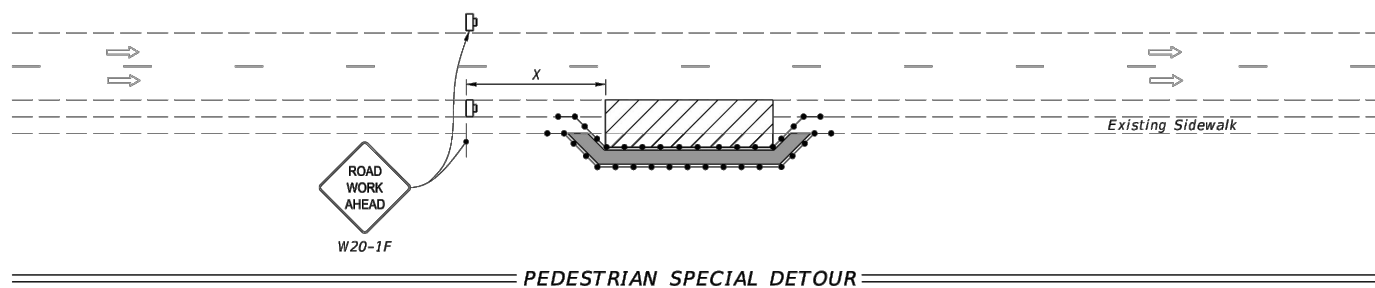
1. These conditions and treatments can be applied only in work areas that fall within a properly signed work zone.
2. When drop-offs occur within the clear zone due to construction or maintenance activities, protection devices are required (See Table 8). A drop-off is defined as a drop in elevation, parallel to the adjacent travel lanes, greater than 3" with slope (A:B) steeper than 1:4. In superelevated sections, the algebraic difference in slopes should not exceed 0.25 (See Drop-off Condition Detail).
3. Drop-offs may be mitigated by placement of slopes with optional base material per Specifications Section 285. Slopes shallower than 1:4 may be required to avoid algebraic difference in slopes greater than 0.25. Include the cost for the placement and removal of the material in Maintenance of Traffic, LS. Use of this treatment in lieu of a temporary barrier is not eligible for CSIP consideration. Conduct daily inspections for deficiencies related to erosion, excessive slopes, rutting or other adverse conditions. Repair any deficiencies immediately.
4. For Setback Distance, refer to the Index or Approved Products List (APL) drawing of the selected barrier.
5. For Conditions 1 and 3 provided in Table 8, any drop-off condition that is created and restored within the same work period will not be subject to use of temporary barriers; however, channelizing devices will be required.
6. When permanent curb heights are  $\geq 6"$ , no channelizing device will be required. For curb heights  $< 6"$ , see Table 8.



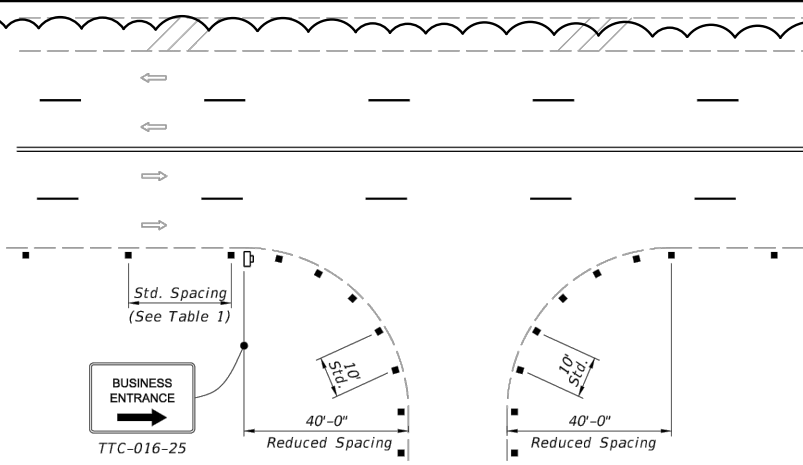
**DROP-OFF CONDITION DETAIL**

Condition	E (ft)	D (in.)	Device Required
1	0-12	> 3	Temporary Barrier
2	> 12-CZ	> 3 to $\leq 5$	Channelizing Device
3	0-CZ	> 5	Temporary Barrier
4	Removal of Bridge or Retaining Wall Barrier		Temporary Barrier
5	Removal of portions of Bridge Deck		Temporary Barrier

1. L = Taper Length  
B = Buffer Length  
X = Work Zone Sign Distance  
See Index 102-600 for "L", "B", "X", channelizing device spacing values.
2. Provide a 5' wide temporary pedestrian way with a maximum cross-slope of 0.02, except where space restrictions warrant a minimum width of 4'. Provide a 5' x 5' passing space for temporary pedestrian ways less than 5' in width at intervals not to exceed 200'.
3. When temporary pedestrian ways require curb ramps, meet the requirements of Index 522-002. Detectable warnings are not required for curb ramps diverting pedestrian traffic into a closed lane.
4. The "Speeding Fines Doubled When Workers Present" signs (TTC-011A-25) and "End Road Work" signs (G20-2), along with associated work zone sign distances, may be omitted when the work operation will be in place for 24 hours or less.



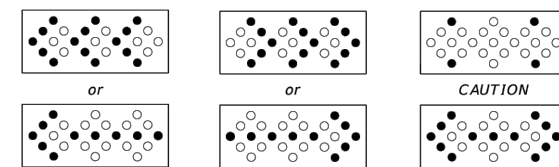
- SYMBOLS:**
- Work Area
  - Temporary Pedestrian Way
  - Channelizing Device (See Index 102-600)
  - Pedestrian Longitudinal Channelizing Device (LCD)
  - Work Zone Sign
  - Arrow Board
  - Crash Cushion
  - Lane Identification and Direction of Traffic



**NOTES:**

1. For single business entrances, place one 24" x 36" BUSINESS ENTRANCE sign (TTC-016-25) showing the specific business name for each affected driveway entrance. Logos may be provided by business owners. Standard BUSINESS ENTRANCE sign (TTC-016-25) may be used when approved by the Engineer.
2. When several businesses share a common driveway entrance, place one 24" x 36" standard BUSINESS ENTRANCE sign (TTC-016-25) in accordance with Index 700-102 at the common driveway entrance.
3. Channelizing devices shall be placed at a reduced spacing on each side of the driveway entrance, but shall not restrict sight distance for the driveway users.
4. Business entrance signs are intended to guide motorist to business entrances moved/modified or disturbed during construction projects. Business entrance signs are not required where there is minimal disruption to business driveways which is often the case with resurfacing type projects.

**BUSINESS ENTRANCE SIGNS AND CHANNELIZING DEVICES PLACEMENT AT BUSINESS ENTRANCE**



- MODES**
- Minimum Required Lamps
  - Additional Lamps Allowed

**NOTES:**

- An arrow board in the arrow or chevron mode shall be used only for stationary or moving lane closures on multilane roadways.
- For shoulder work, blocking the shoulder, for roadside work near the shoulder, or for temporarily closing one lane on a two-lane, two-way roadway, an arrow board shall be used only in the caution mode.
- A single arrow board shall not be used to merge traffic laterally more than one lane. When arrow boards are used to close multiple lanes, a single board shall be used at the merging taper for each closed lane.
- When Advance Warning Arrow Boards are used at night, the intensity of the flashers shall be reduced during darkness when lower intensities are desirable.

**ADVANCE WARNING ARROW BOARDS**

**PEDESTRIAN WAY DROP-OFF CONDITION NOTES**

1. A pedestrian way drop-off is defined as:
  - a. a drop in elevation greater than 10" that is closer than 2' from the edge of the pedestrian way
  - b. a slope steeper than 1:2 that begins closer than 2' from the edge of the pedestrian way when the total drop-off is greater than 60"
2. Protect any drop-off adjacent to a pedestrian way with pedestrian longitudinal channelizing devices, temporary barrier wall, or approved handrail.

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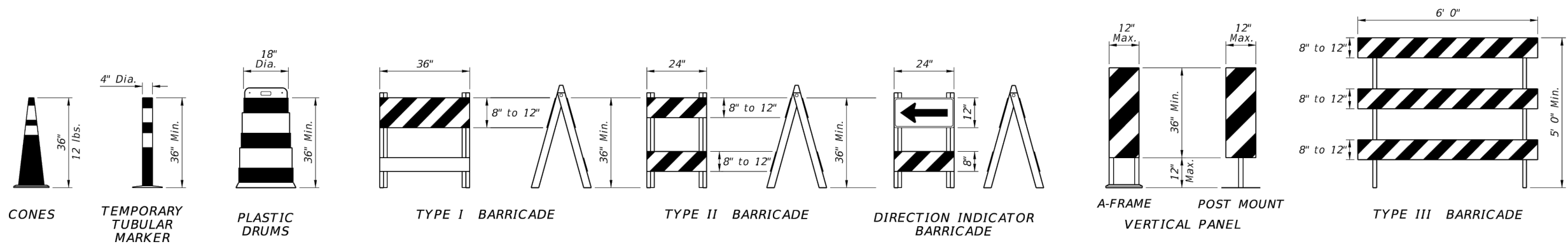
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IMPROVEMENTS**  
 PREPARED FOR  
**CITY OF WAUCHULA**

CITY OF WAUCHULA FLORIDA

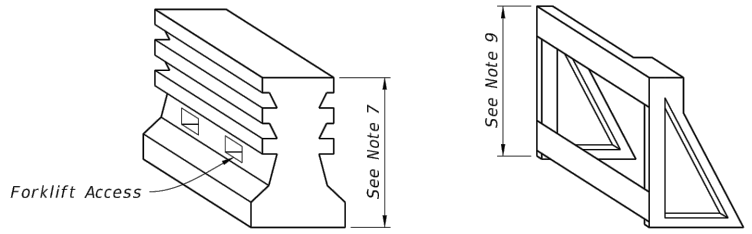
SELECT FDOT INDICES

SHEET NUMBER  
**C-803**

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**CHANNELIZING DEVICES**



**PEDESTRIAN LONGITUDINAL CHANNELIZING DEVICES**

**CHANNELIZING DEVICE NOTES:**

1. The details shown on this sheet are for the following purposes:
  - a. For ease of identification and
  - b. To provide information that supplements or supersedes that provided by the MUTCD.
2. The Type III Barricade shall have a unit length of 6'-0" only. When barricades of greater lengths are required those lengths shall be in multiples of the 6'-0" unit.
3. No sign panel should be mounted on any channelizing device unless the channelizing device/sign combination was found to be crashworthy and the sign panel is mounted in accordance with the vendor drawing for the channelizing device shown on the Approved Products List (APL).
4. Ballast shall not be placed on top rails or any striped rails or higher than 13" above the driving surface.
5. The direction indicator barricade may be used in tapers and transitions where specific directional guidance to drivers is necessary. If used, direction indicator barricades shall be used in series to direct the driver through the transition and into the intended travel lane.
6. The splicing of sheeting is not permitted on channelizing devices or MOT signs.
7. For rails less than 3'-0" long, 4" stripes shall be used.
8. Cones shall:
  - a. Be used only in active work zones where workers are present.
  - b. Be reflectorized as per the MUTCD with Department-approved reflective collars when used at night.
9. For pedestrian longitudinal channelizing devices, the device shall have a minimum of 8" continuous detectable edging above the walkway. A gap not exceeding a height of 2" is allowed to facilitate drainage. The top surface of the device shall be a minimum height of 32" and have a 1/8" or less difference in any plane at all connection points between the devices to facilitate hand trailing. The bottom and the top surface of the device shall be in the same vertical plane. If pedestrian drop-off protection is required, the device shall have a footprint or offset of at least 2', otherwise the device must be at least 42" in height above the walkway and be anchored or ballasted to withstand a 200 lb lateral point load at the top of the device.

No.	REVISIONS	DATE	BY
3	PER FDOT COMMENTS	01/29/26	MSD

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KHA PROJECT	046292037
DATE	SEPT 2025
SCALE	AS SHOWN
DESIGNED BY	MSD
DRAWN BY	KM
CHECKED BY	

**HOGAN STREET EXTENSION IMPROVEMENTS**  
 PREPARED FOR  
**CITY OF WAUCHULA**  
 CITY OF WAUCHULA FLORIDA

SHEET NUMBER  
**SELECT FDOT INDICES**  
**C-804**