



PROJECT LOCATION

CONSTRUCTION PLANS FOR COLEY ROAD RIDGEWAY DRIVE TO McCULLOUGH BOULEVARD CITY OF TUPELO TUPELO, MISSISSIPPI

MUNICIPAL OFFICIALS

MAYOR
TODD JORDAN

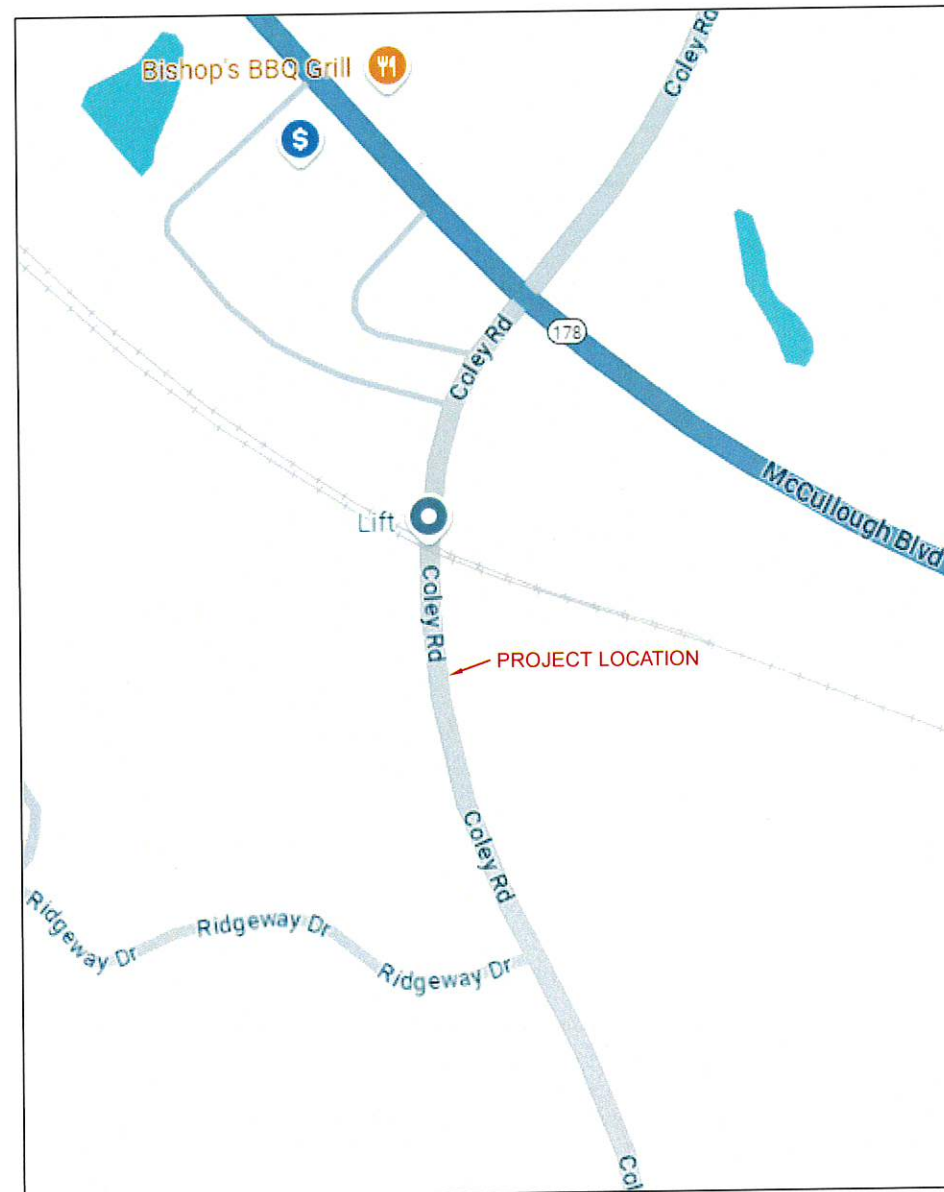
CHIEF OPERATIONS OFFICER
DON LEWIS

PUBLIC WORKS DIRECTOR
KELLY KNIGHT

CITY ENGINEER
DENNIS BONDS

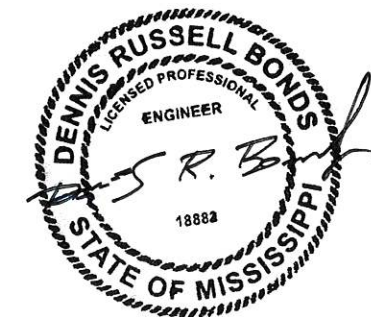
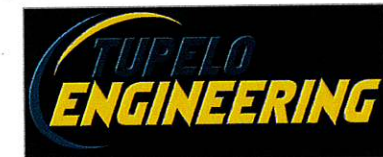
CITY COUNCIL
CHAD MIMS
LYNN BRYAN
TRAVIS BEARD
NETTIE DAVIS
BUDDY PALMER
JANET GASTON
ROSIE JONES

CITY ATTORNEY
BEN LOGAN



VICINITY MAP

PLANS PREPARED BY:



FEBRUARY 2025

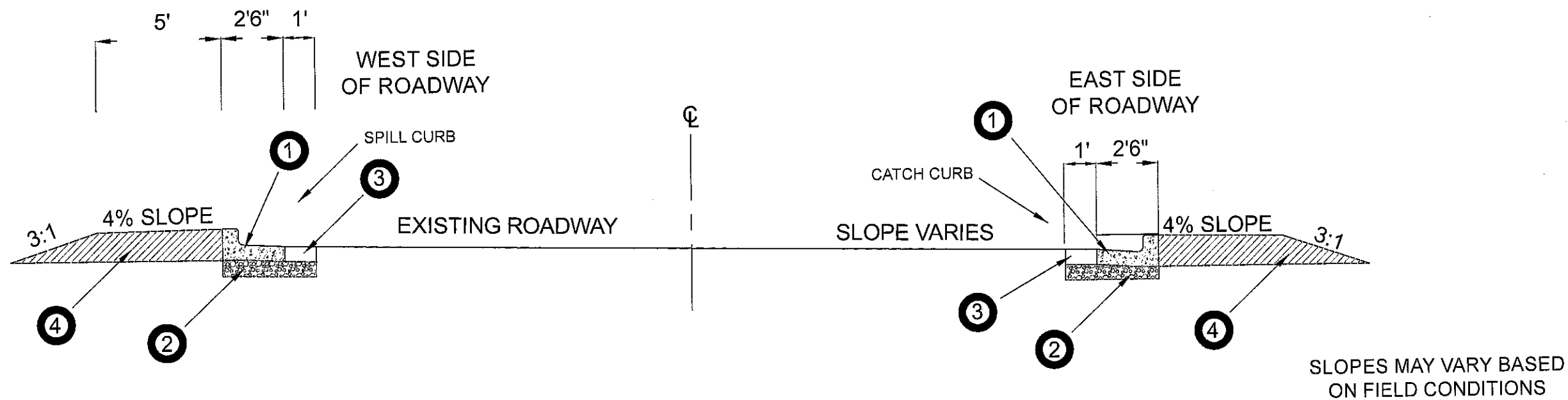
PLAN ASSEMBLY		
SHEET NO.	WORKING NO.	DESCRIPTION
		PLAN SHEETS
1		TITLE SHEET
2		DETAILED INDEX AND QUANTITY SHEET
3		GENERAL NOTES SHEET
4		TYPICAL SECTION SHEET
5		MISCELLANEOUS DETAIL SHEET
6-11		PLAN PROFILE SHEETS
12		TRAFFIC CONTROL PLAN FOR COLEY ROAD
13		TRAFFIC CONTROL PLAN FOR MCCULLOUGH BLVD
14		TRUCK DETOUR FOR MCCULLOUGH BLVD
		STANDARD SHEETS
6101	ECD-1	TYPICAL TEMPORARY EROSION / SEDIMENT CONTROL APPLICATIONS
6102	ECD-2	DETAILS OF SEDIMENT BARRIER APPLICATIONS
6103	ECD-3	DETAILS OF SILT FENCE INSTALLATION
6104	ECD-4	DITCH CHECK STRUCTURES, TYPICAL APPLICATIONS AND DETAILS
6105	ECD-5	TEMPORARY EROSION, SEDIMENT, AND WATER POLLUTION CONTROL MEASURES
6106	ECD-6	DETAILS OF EROSION CONTROL WATTLE DITCH CHECK
6352	TCP-2	TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT LESS THAN 65 MPH
6358	TCP-8	HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS
6366	TCP-16	TRAFFIC CONTROL DETAILS DRUM PLACEMENT AND SHOULDER CLOSURE
6419	SD-1	DRIVEWAYS & CURB & GUTTER & SIDEWALK
6420	SD-2	DRIVEWAYS, INTEGRAL CURB & SIDEWALK
6426	PF-1	DETAILS OF PAVED FLUMES

RECAPITULATION OF QUANTITIES				
ITEM NO.	PAY ITEM	UNIT	PRELIMINARY	FINAL
1	CLEARING AND GRUBBING	LUMP SUM	1	
2	MAINTENANCE OF TRAFFIC	LUMP SUM	1	
3	MOBILIZATION	LUMP SUM	1	
4	TYPE I CURB & GUTTER (SPILL)	LIN. FT.	650	
5	TYPE I CURB & GUTTER (CATCH)	LIN. FT.	1300	
6	CONCRETE DRIVEWAY	SQ. YD.	11.54	
7	UNCLASSIFIED EXCAVATION, FM, AH	CU. YD.	505	
8	SELECT BORROW, FM, AH	CU. YD.	520	
9	GRANULAR MATERIAL (CLAY GRAVEL)	CU. YD.	50	
10	GRANULAR MATERIAL (CRUSHED STONE)	CU. YD.	475	
11	HOT MIX ASPHALT, 9.5-MM MIXTURE	TON	100	
12	CONCRETE FLUME - TYPE A	LIN. FT.	300	
13	CONCRETE FLUME - TYPE C	LIN. FT.	30	
14	100 # RIP RAP	TON	100	
15	GRASSING	ACRE	1	
16	SOLID SODDING	SQ. YD.	75	
17	SILT FENCE	LIN. FT.	2000	
18	WATTLES, 20"	LIN. FT.	200	
19	ADDITIONAL CONSTRUCTION SIGNS	SQ. FT.	1	

DETAILED INDEX AND QUANTITIES	COLEY ROAD CURB & GUTTER IMPROVEMENTS McCullough Blvd to Ridgeway Drive
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GENERAL NOTES		EROSION CONTROL NOTES		PUBLIC UTILITIES		
1	EXISTING UTILITY LINES ARE SHOWN ON THE DRAWINGS BASED UPON THE BEST INFORMATION AVAILABLE TO THE ENGINEER. THE ENGINEER CANNOT AND DOES NOT WARRANT THAT THIS INFORMATION IS COMPLETE OR ACCURATE. THE CONTRACTOR MUST COORDINATE DIRECTLY WITH THE INVOLVED UTILITY OWNERS TO HAVE UTILITY LINES FIELD LOCATED IN ADVANCE OF CONSTRUCTION.	12	IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT EXISTING STRUCTURES SUCH AS PIPES, INLETS, APRONS, BRIDGES, ETC. FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. EXTREME CARE SHALL BE EXERCISED IN UNDERCUT AREAS AND THE UNDERCUT DEPTH MAY BE ADJUSTED AT CROSS DRAINS, AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.	21	SILT BARRIERS TO BE ERECTED AS SHOWN ON APPROVED EROSION CONTROL PLAN AND MAINTAINED BY THE CONTRACTOR IN ACCORDANCE WITH THE TECHNICAL SPECIFICATIONS OR AS DIRECTED.	ELECTRICAL POWER TUPELO WATER & LIGHT DEPARTMENT (662) 620-6598
2	ACCESS TO ALL BUSINESSES AND/OR RESIDENCES MUST BE MAINTAINED AT ALL TIMES.			22	ANY DISTURBED AREAS ON WHICH WORK IS COMPLETED OR SUSPENDED FOR MORE THAN 14 DAYS SHALL IMMEDIATELY RECEIVE VEGETATIVE STABILIZATION COVER SUCH AS SEEDING AND MULCHING, SOLID SOD, OR OTHER EROSION CONTROL MEASURES WHICH SHALL BE MAINTAINED UNTIL WORK CAN RESUME OR PROJECT IS COMPLETE.	TELEPHONE AT&T (800) 737-2478
3	ALL EXISTING ITEMS DESIGNATED FOR REMOVAL SHALL BE DISPOSED OF BY THE CONTRACTOR OFF THE PROJECT LIMITS AT A SITE TO BE PROVIDED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ACCOMPLISHING THIS WORK IN FULL CONFORMANCE WITH THE REQUIREMENTS OF LOCAL OFFICIALS AND THE MISSISSIPPI DEPARTMENT OF ENVIRONMENTAL QUALITY (MDEQ). IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY THAT ALL APPLICABLE REGULATIONS ARE MET. THE COST FOR THIS WORK IS TO BE INCLUDED IN THE CLEARING AND GRUBBING PAY ITEM.	13	WORK ON STRUCTURES FOR THIS PROJECT REQUIRES EXCAVATION IN THE IMMEDIATE VICINITY OF TRAFFIC AND ADJACENT PROPERTIES. THEREFORE, THE RISK OF A FAILURE OCCURING DURING THE EXCAVATION REQUIRES THAT EXTREME CAUTION BE EXERCISED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE WHAT BRACING, SHORING OR GROUND SUPPORT SYSTEM THAT IS DEEMED NECESSARY TO PREVENT A FAILURE AND PROTECT THE PERSONS WORKING NEAR THE EXCAVATION OR ANY STRUCTURE ADJACENT TO THE EXCAVATION. ALL COSTS FOR ANY PROTECTIVE MEASURES, INCLUDING THE MATERIALS AND LABOR FOR DESIGNING, DRAWING AND CONSTRUCTING THE FACILITY, TO BE ABSORBED IN OTHER ITEMS.	23	EROSION CONTROL MEASURES AS SHOWN ON PLANS ARE TO BE CONSIDERED MINIMUM REQUIREMENTS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN ADEQUATE EROSION CONTROL AND TO ENSURE COMPLIANCE WITH STATE AND FEDERAL REGULATIONS FOR THE DURATION OF THE PROJECT BY WHATEVER MEANS NECESSARY.	NATURAL GAS ATMOS ENERGY (662) 842-2441
4	ANY UTILITY MAINS OR SERVICE LINES DAMAGED BY THE CONTRACTORS OPERATIONS SHALL BE REPAIRED AND/OR REPLACED BY THE CONTRACTOR IMMEDIATELY. THERE SHALL BE NO SEPARATE PAYMENT FOR THIS WORK. PRIOR TO COMMENCEMENT OF WORK IN THE VICINITY OF UTILITY MAINS OR SERVICE LINES, THE CONTRACTOR SHALL HAVE APPROPRIATE RESOURCES AVAILABLE TO MAKE NEEDED REPAIRS WITHOUT UNDUE LENGTH OF SERVICE OUTAGE TO LOCAL PROPERTY OWNERS AND BUSINESSES. THE LOCATION OF ALL VALVES NECESSARY FOR ISOLATION OF THE AREA UNDER CONSTRUCTION SHALL BE DETERMINED PRIOR TO COMMENCEMENT OF WORK.	14	FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED IN THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.	24	TEMPORARY EROSION CHECKS TO BE PLACED AND MAINTAINED AT ALL WATER FLOW AREAS.	WATER & SEWER TUPELO WATER & LIGHT DEPARTMENT (662) 620-6598
5	ALL PAVEMENT TO BE REMOVED SHALL BE SAW CUT. (COST OF SAW CUTTING TO BE ABSORBED)	15	ROADWAY SIGNS THAT ARE IN CONFLICT WITH CONSTRUCTION OF THIS PROJECT SHALL BE COVERED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS.	25	ALL EROSION CONTROL MEASURES SHALL CONFORM TO THE MISSISSIPPI STORM WATER POLLUTION PREVENTION PLAN (SWPPP) GUIDANCE MANUAL FOR CONSTRUCTION ACTIVITIES PUBLISHED BY MDEQ.	CABLE: COMCAST CABLE (662) 253-7190
6	PAVEMENT REMOVAL SHALL BE PAID FOR AT THE UNIT PRICE PER SQUARE YARD REGARDLESS OF THE DEPTH OR TYPE OF MATERIAL ENCOUNTERED.	16	THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY UNAUTHORIZED REMOVAL OF EXISTING TREES OR LANDSCAPE TO REMAIN. CAUTION SHALL BE TAKEN SO AS NOT TO DAMAGE ANY EXISITING TREES DESIGNATED TO REMAIN.	26	THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL PROPERTY, STRUCTURES, AND IMPROVEMENTS (BOTH ON-SITE AND OFF-SITE) FROM SILTATION, AND SHALL CORRECT ANY DAMAGE RESULTING FROM FAILURE TO DO SO.	ELECTRICAL POWER TRANSMISSION LINE TVA (662) 690-3400
7	EXCAVATION AND BACKFILL FOR PIPES, STRUCTURES, ETC. WILL NOT BE PAID SEPARATELY, BUT WILL BE INCLUDED IN THE PRICE BID FOR THE ITEM REQUIRING SAID EXCAVATION.	17	REINFORCING STEEL IN DRAINAGE STRUCTURE IS TO BE ABSORBED IN THE COST OF THE STRUCTURES.	27	THE CONTRACTOR SHALL COMPLY WITH ALL RULES AND REGULATIONS GOVERNING THE ELIMINATION AND CONTROL OF POLLUTANTS IN STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES AS REGULATED BY THE ENVIRONMENTAL PROTECTION AGENCY AND SET FORTH IN THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS.	
8	FLOW LINES FOR REQUIRED STORM DRAIN AND TOP ELEVATIONS FOR REQUIRED INLETS HAVE BEEN APPROXIMATED AS CLOSELY AS POSSIBLE. SOME ADJUSTMENTS MAY BE REQUIRED TO MEET FIELD CONDITIONS.	18	UNCLASSIFIED EXCAVATION WILL INITIALLY BE TO A DEPTH OF 6" BELOW EXISITING ROAD GRADE. THE EXISITING SUBGRADE WILL THEN BE EXAMINED BY THE ENGINEER TO SEE IF IT IS SUITABLE TO SUPPORT THE NEW CURB AND GUTTER. IF IT IS FOUND TO BE DEFICIENT AFTER PROOF ROLLING, ANOTHER 10" OF UNCLASSIFIED EXCAVATION WILL BE AUTHORIZED BY THE ENGINEER AND CRUSHED LIMESTONE BASE WILL BE PLACED AND COMPACTED.			
9	ALL DISTURBED AREAS SHALL BE GRASSED IN ACCORDANCE WITH THE SPECIFICATIONS. AREAS TO RECEIVE SOLID SOD SHALL MATCH EXISTING SOD TYPE UNLESS OTHERWISE NOTED.	19	HORIZONTAL ALIGNMENT HAS BEEN DETERMINED BASED ON FIELD SURVEY DATA. SOME FIELD ADJUSTMENTS MAY BE NECESSARY.			
10	ALL DRIVEWAYS SHALL BE GRADED AS REQUIRED TO CONNECT NEW PAVEMENT TO EXISITING PARKING LOTS OR DRIVES. ANY DEVIATION FROM TYPICAL SECTIONS MUST BE APPROVED BY THE ENGINEER PRIOR TO CONSTRUCTION.	20	VERTICAL ALIGNMENT HAS BEEN DETERMINED BASED ON FIELD SURVEY DATA. SOME FIELD ADJUSTMENTS MAY BE NECESSARY.			
11	THE LOCATION AND SPACING OF SIGNS, SHOWN ON THE TRAFFIC CONTROL PLANS, ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.					

COLEY ROAD
CURB & GUTTER ADDITION

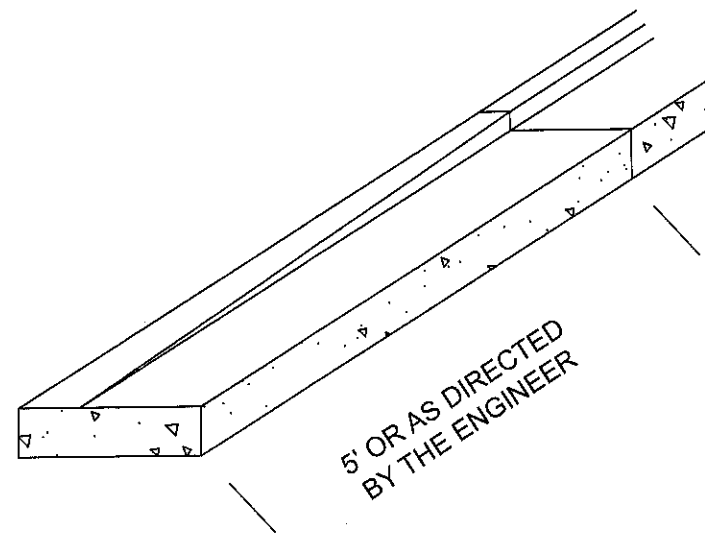


TYPICAL SECTION

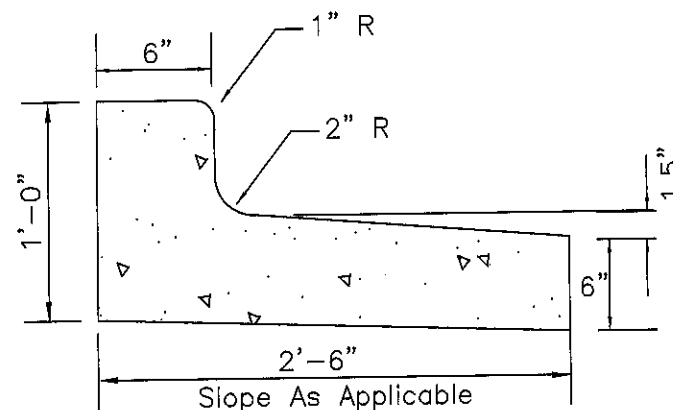
- ① TYPE 1 CURB & GUTTER
- ② EXISTING SUBGRADE BASE OR 10" CRUSHED STONE BASE (SEE GENERAL NOTE 18)
- ③ HOT MIX ASPHALT
- ④ SELECT BORROW MATERIAL

RATE OF APPLICATION USED FOR ESTIMATED QUANTITIES

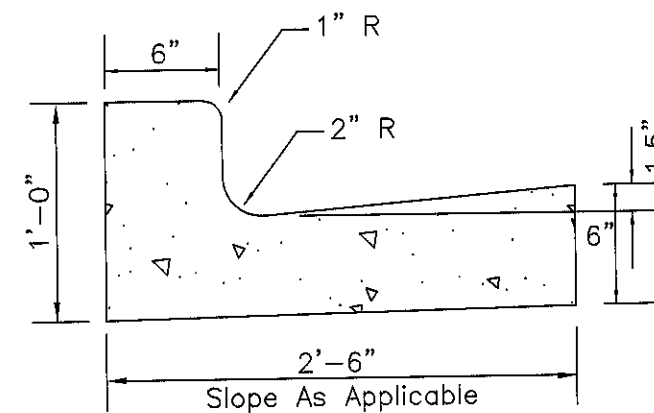
ITEM	RATE	
BORROW MATERIAL	181.56	CU.YD./ STA
COMMERCIAL FERTILIZER (13-13-13)	1.0	TON/ACRE
AMMONIA NITRATE	500	LBS./ACRE
AGRICULTURAL LIMESTONE	2.0	TON/ACRE
VEGETATIVE MATERIALS FOR MULCH	2.0	TON/ACRE



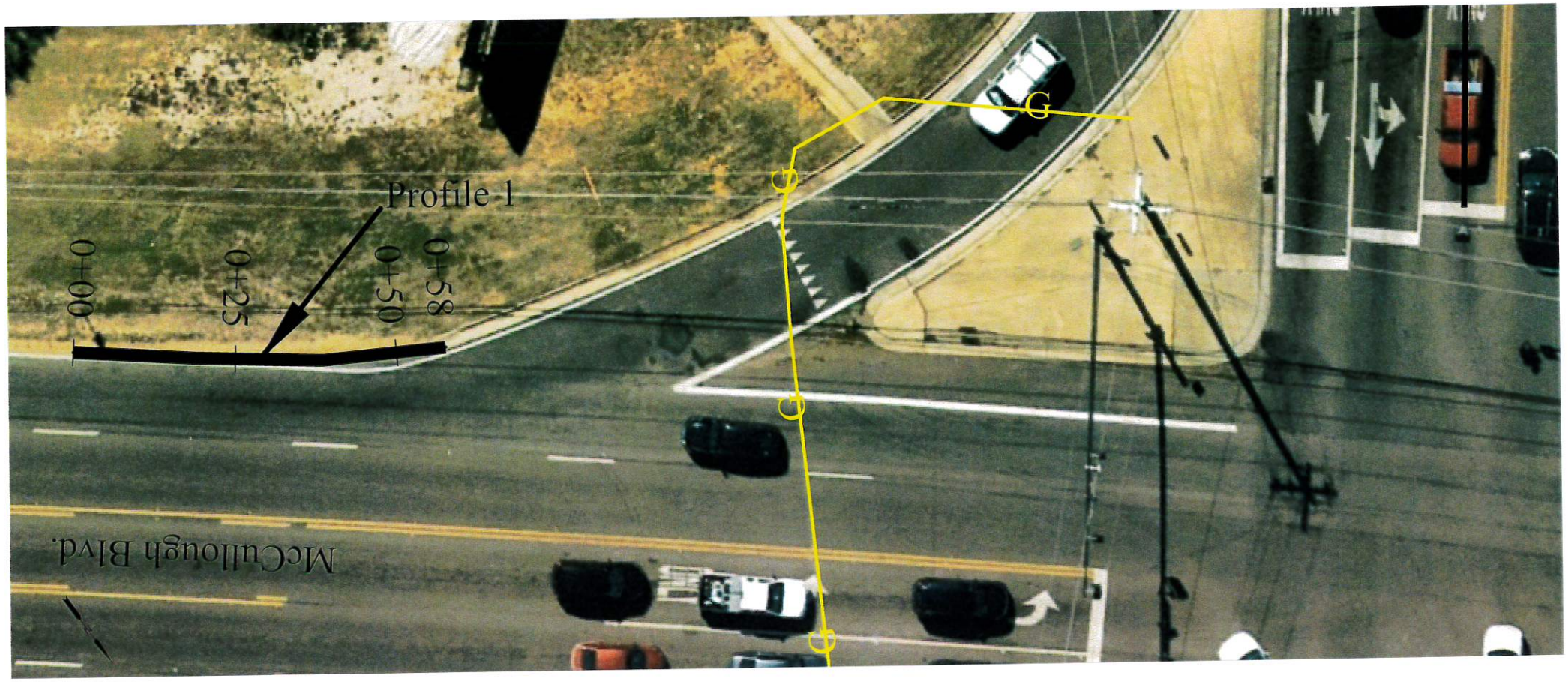
CURB TRANSITION DETAIL
SCALE: NOT TO SCALE



TYPE I CURB & GUTTER
3,000 psi Concrete
Spill Curb

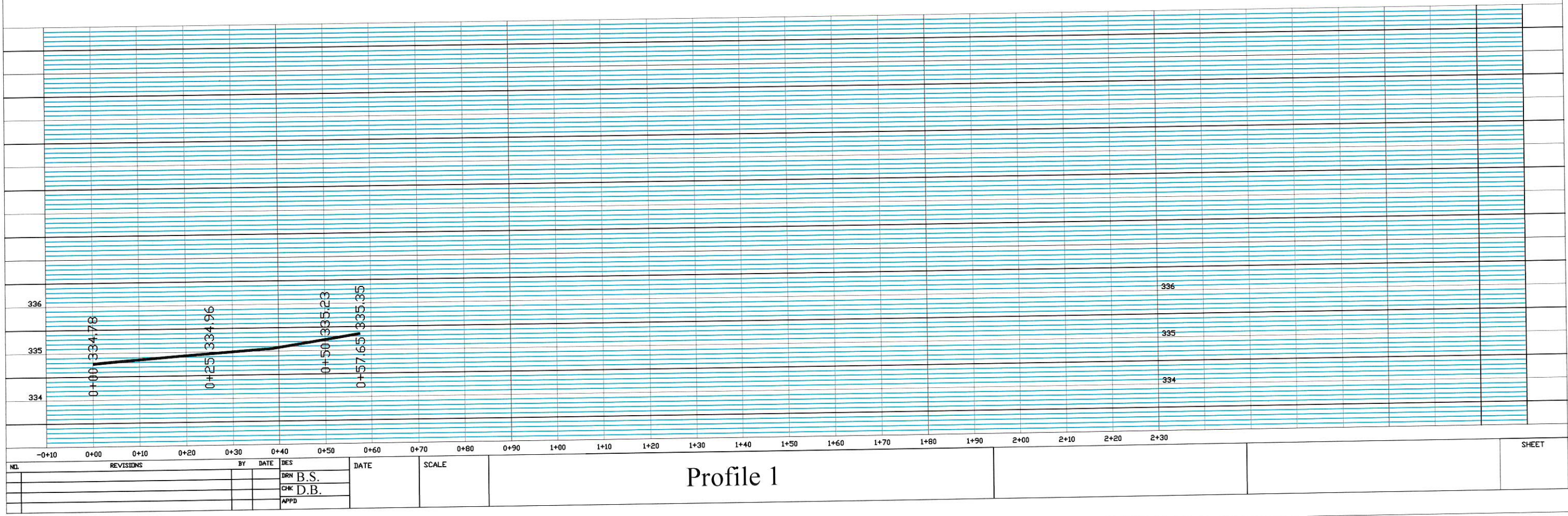


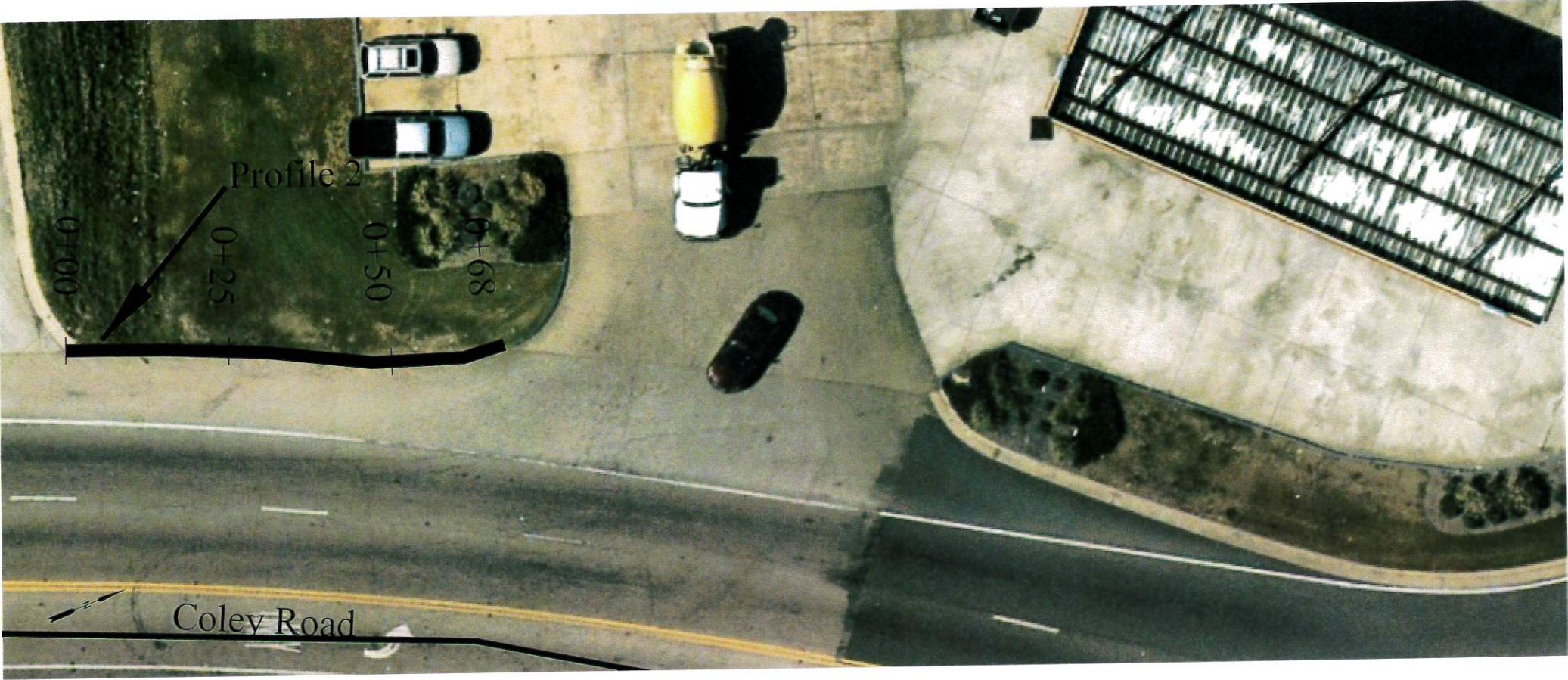
TYPE I CURB & GUTTER
3,000 psi Concrete
Catch Curb



SAWCUT AND REMOVE EXISTING CURB & GUTTER AS NEEDED. NO SEPERATE PAYMENT, TO BE ABSORBED.

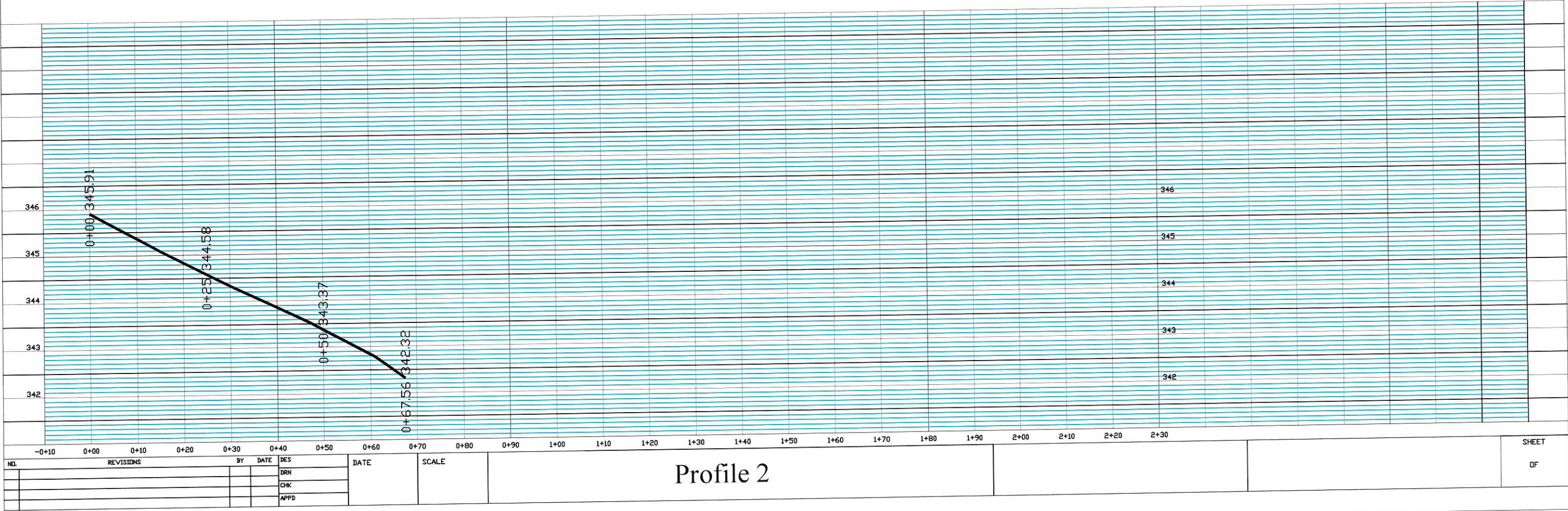
TIE NEW CURB SECTION TO EXISTING CURB.

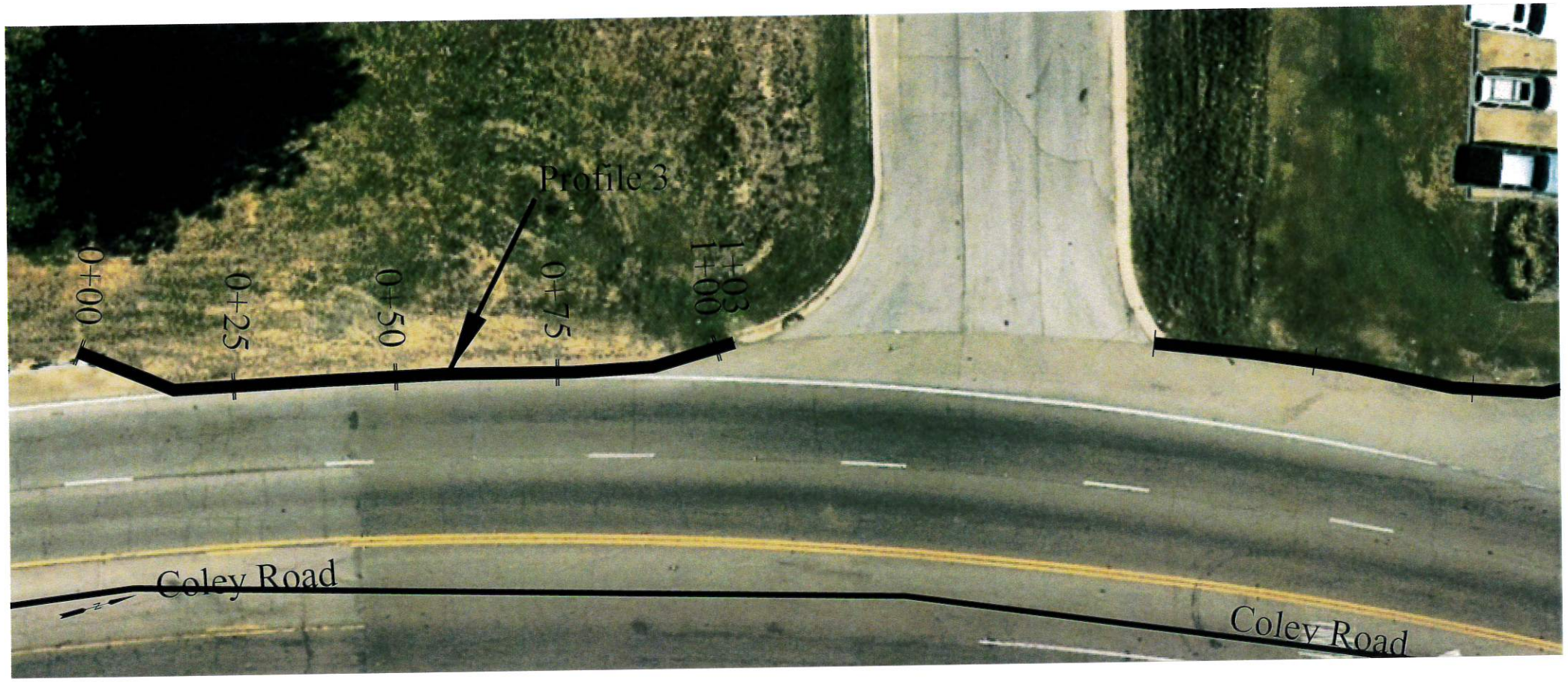




SAWCUT AND REMOVE EXISTING CURB & GUTTER AS NEEDED. NO SEPERATE PAYMENT, TO BE ABSORBED.

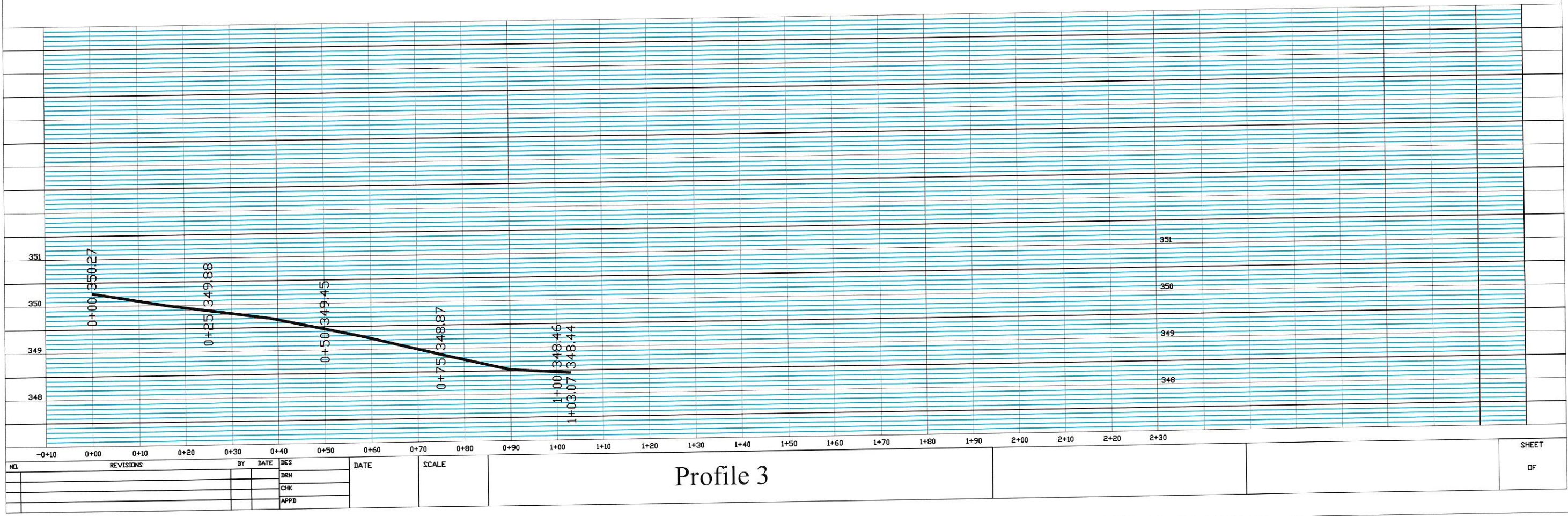
TIE NEW CURB SECTION TO EXISTING CURB.

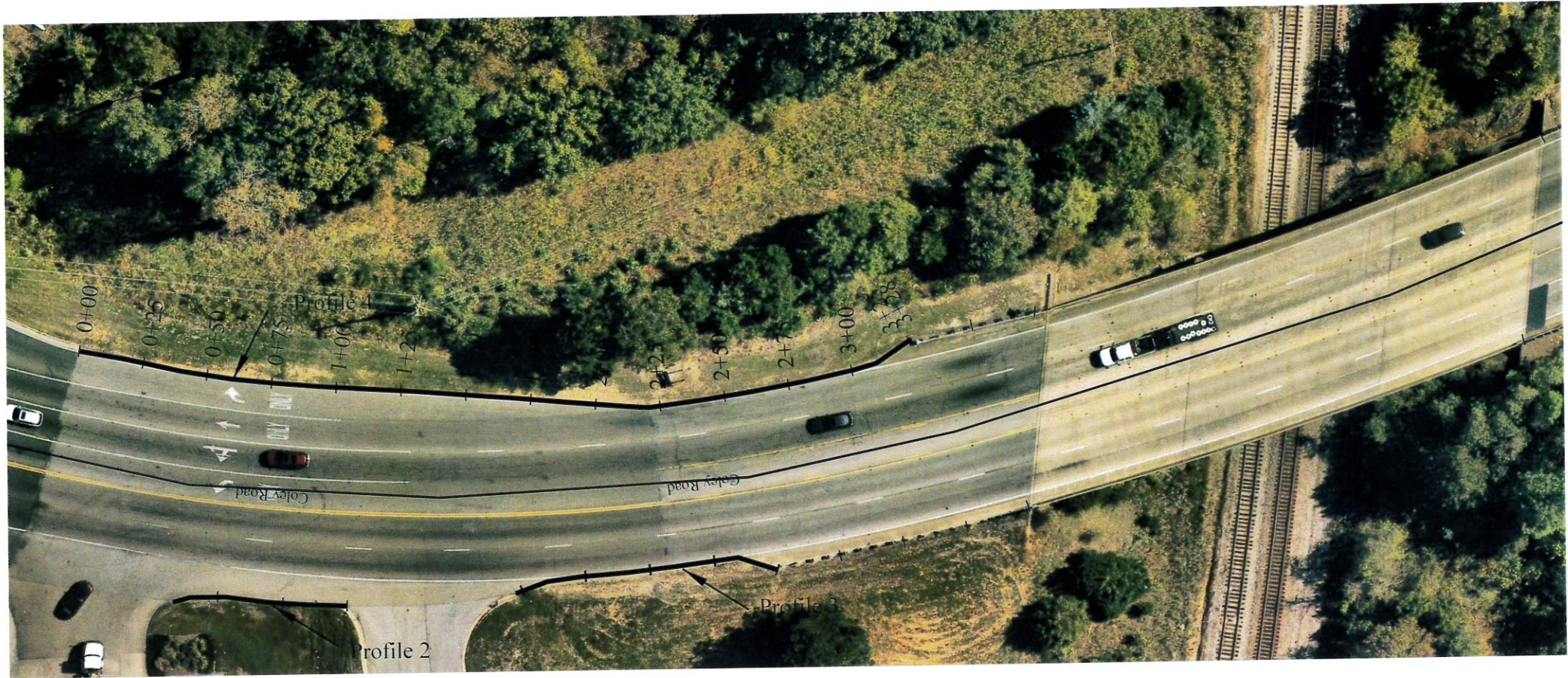




SAWCUT AND REMOVE EXISTING CURB & GUTTER AS NEEDED. NO SEPERATE PAYMENT, TO BE ABSORBED.

TIE NEW CURB SECTION TO EXISTING CURB.

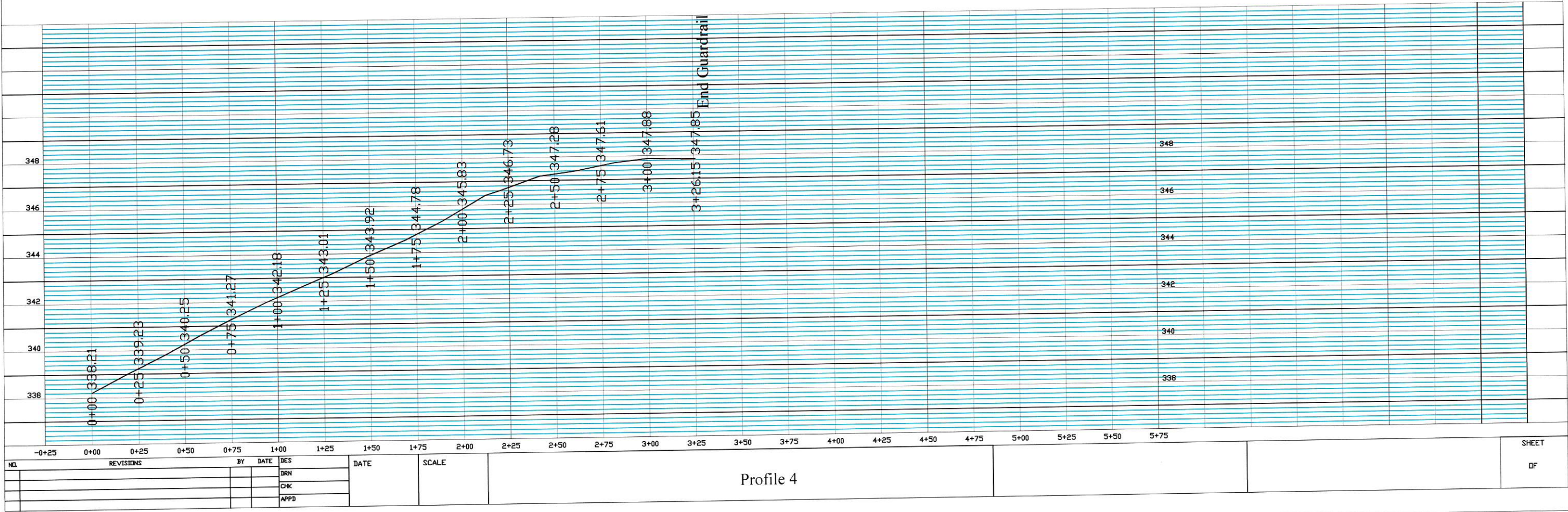




25' OF TYPE A FLUME REQ'D W/ RIP RAP SPLASH PAD
STA. 0+00 AND STA. 1+50

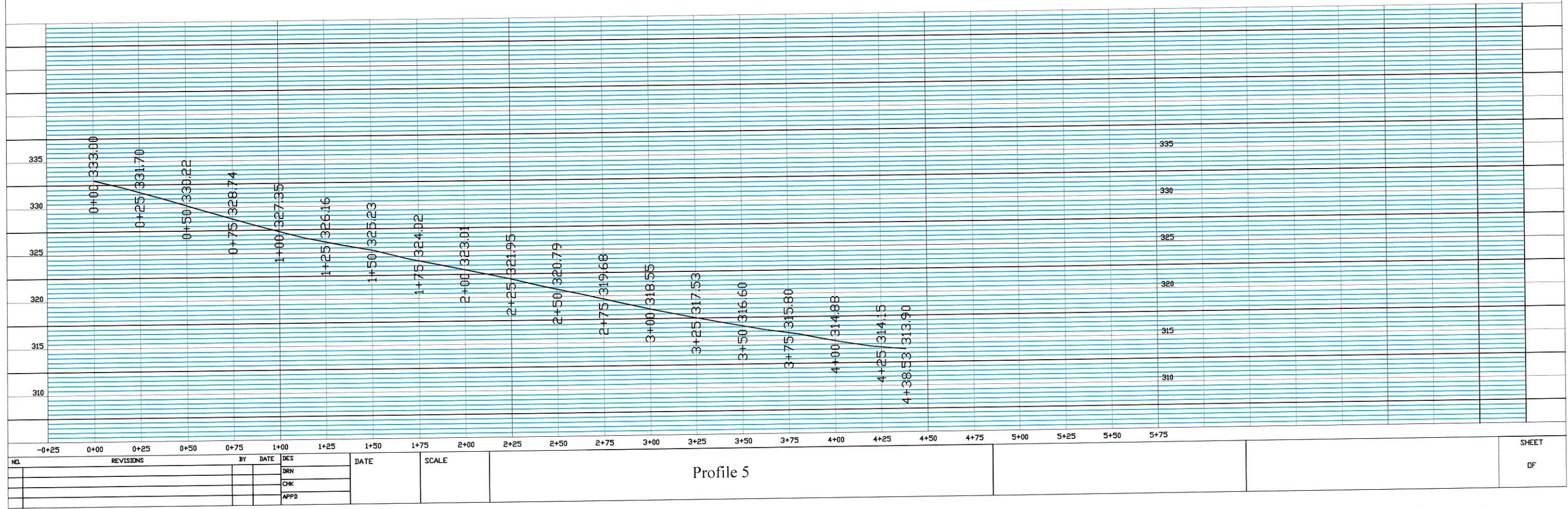
SAWCUT AND REMOVE EXISTING CURB &
GUTTER AS NEEDED. NO SEPERATE
PAYMENT, TO BE ABSORBED.

TIE NEW CURB SECTION TO EXISTING CURB.





STA. 4+39 - STA. 3+75
SAW CUT AND REMOVE ASPHALT. ALIGN NEW CURB
SECTION WITH EXISTING RIDGEWAY DRIVE CURB
NO SEPERATE PAYMENT, TO BE ABSORBED.
25' OF TYPE A FLUME REQ'D
STA. 3+75



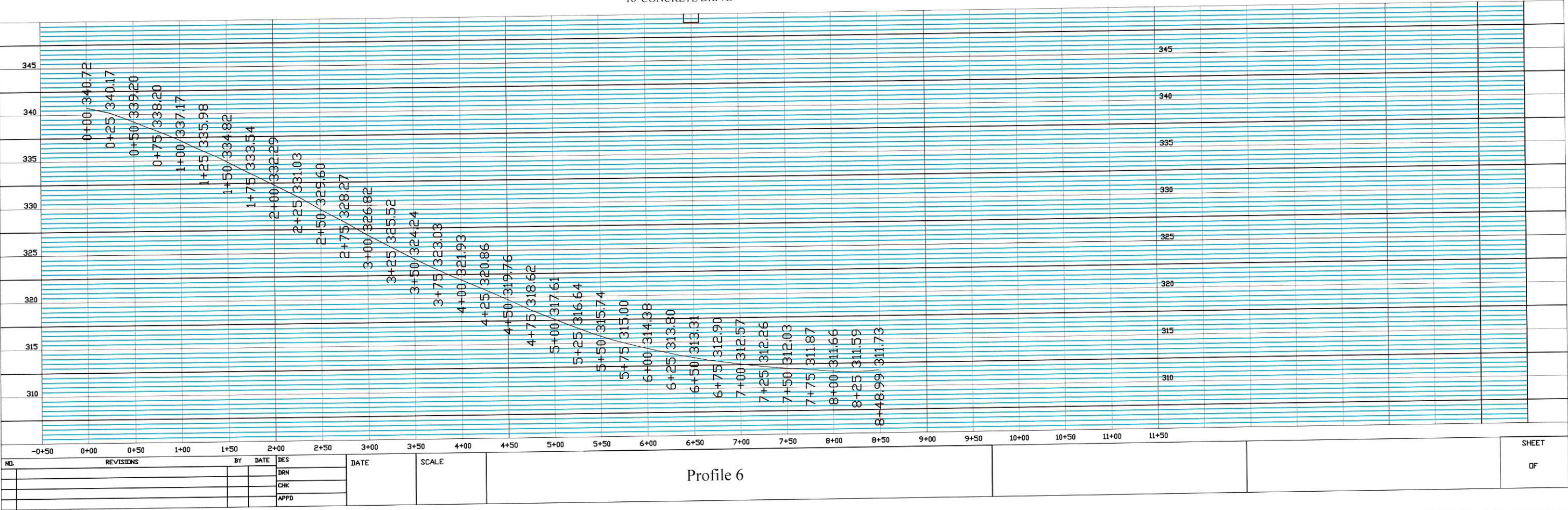


25' OF TYPE A FLUME REQ'D W/ RIP RAP SPLASH PAD
STA. 2+00, STA. 4+50, AND STA. 7+00

25' OF TYPE C FLUME REQ'D W/ RIP RAP SPLASH PAD
STA. 8+49

STA. 6+50 - 16' CONCRETE DRIVE REQ'D

STA. 6+50
16' CONCRETE DRIVE



GENERAL NOTES:

ALL CONSTRUCTION SIGNS SHALL BE COVERED WHEN NOT APPROPRIATE FOR CURRENT CONSTRUCTION ACTIVITIES.

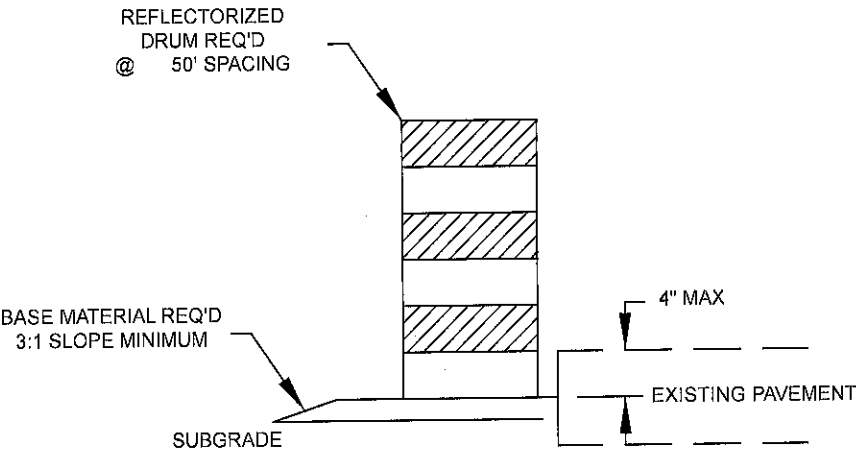
ALL TRAFFIC CONTROL DEVICES NECESSARY FOR THE SAFE MAINTENANCE OF TRAFFIC SHALL BE INSTALLED AND SHALL REMAIN AS LONG AS NEEDED.

ANY TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE LATEST EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".

THE REQUIREMENTS SHOWN ARE CONSIDERED AS MINIMUM, AND SHALL BE ADDED TO AS NECESSARY FOR THE SAFE MAINTENANCE OF TRAFFIC.

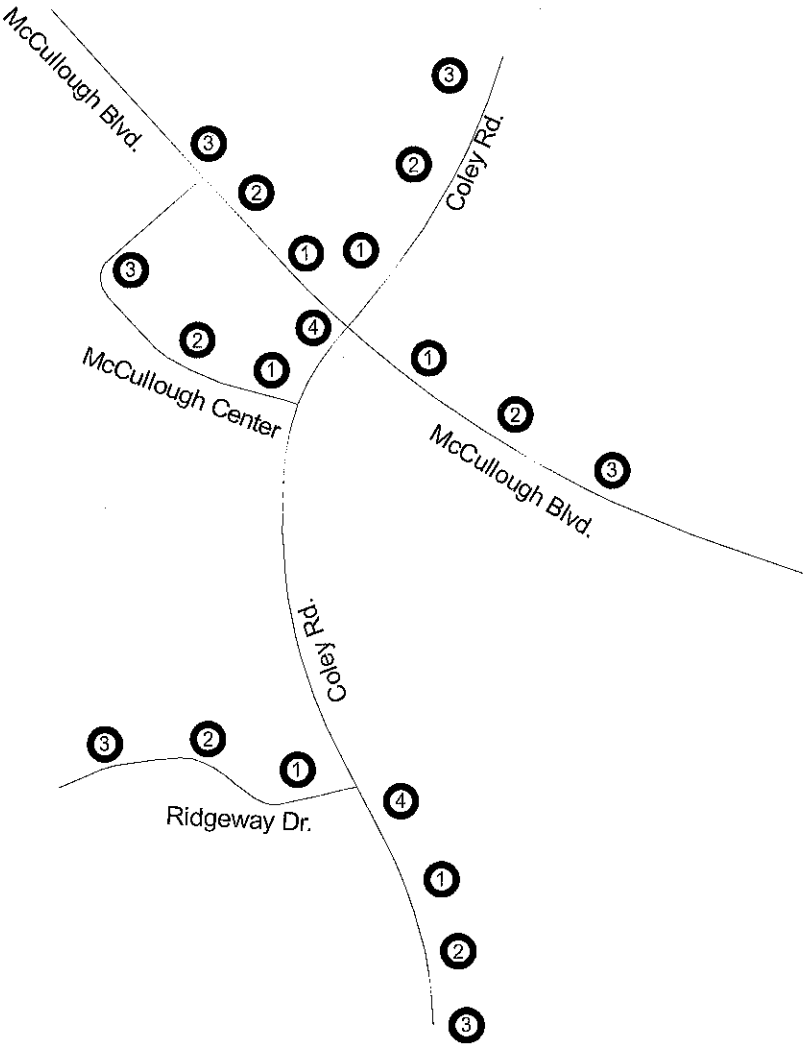
THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE SAFE MAINTENANCE OF TRAFFIC.

ALL ITEMS SHOWN ON THIS SHEEET SHALL BE PAID FOR UNDER THE MAINTENACE OF TRAFFIC PAY ITEM.



DETAIL OF DRUM PLACEMENT ON LOW SHOULDERS

- ① ROAD WORK AHEAD (W20-1)
- ② ROAD WORK 1000' (W20-1)
- ③ ROAD WORK 1500' (W20-1)
- ④ ROAD WORK SIGN REQ'D MOUNTED ON TYPE 3 BARRICADE (6' WIDE) DOUBLE FACED (G20-1)



TRAFFIC CONTROL PLAN FOR CURB AND GUTTER ADDITION ON COLEY ROAD.

SEE FOLLOWING SHEETS FOR TRAFFIC CONTROL PLAN FOR McCULLOUGH BLVD. CURB & GUTTER ADDITION. A

ADVANCE WARNING SIGNS COLEY:

W20-1

@ 1/2 MILE:

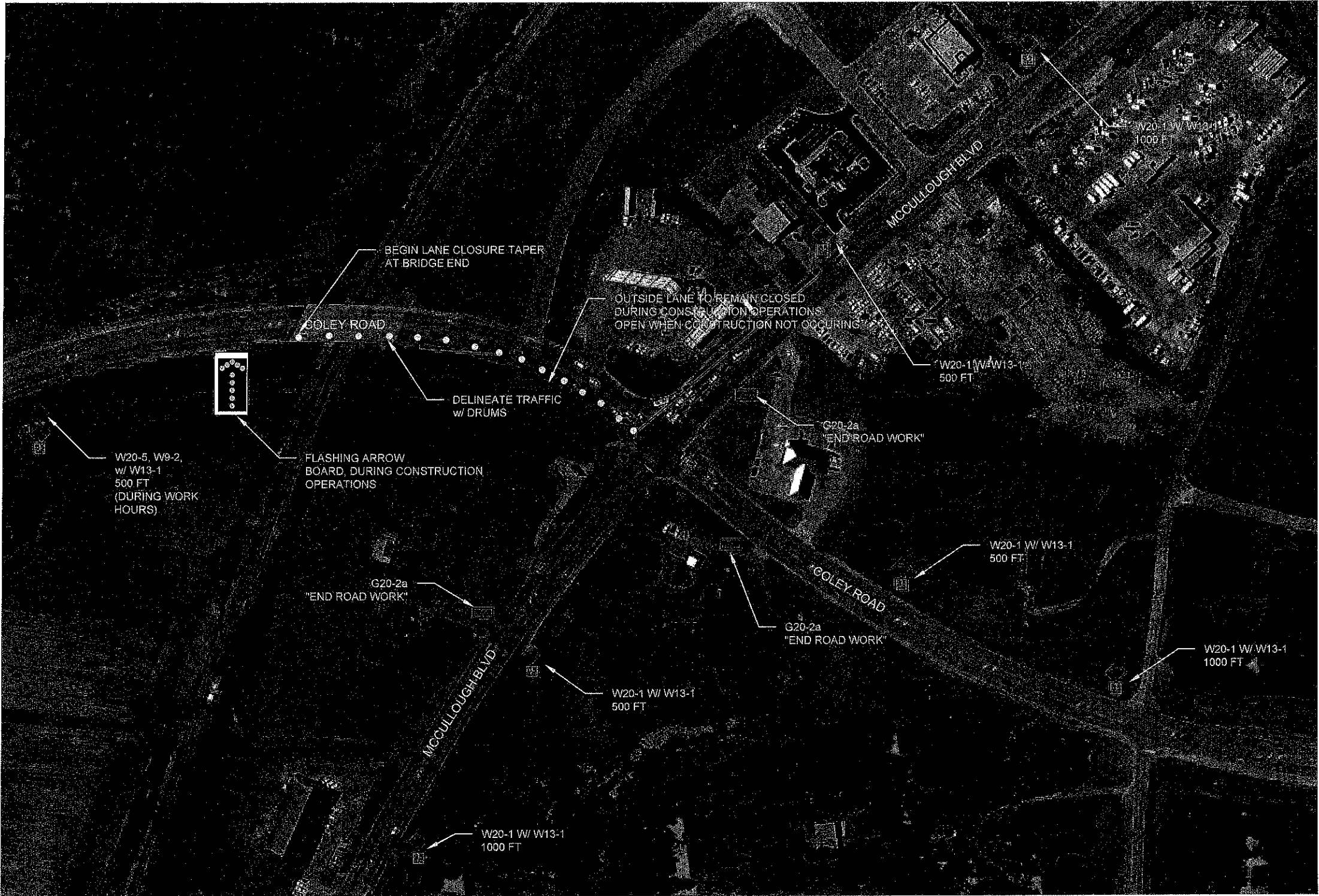
W20-5 (DURING HOURS)

@ 1000 FT:
(DURING WORK HOURS)

W20-5

W8-2

W13-1



NOT TO SCALE

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THE SAFE MAINTENANCE OF TRAFFIC IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

COSTS FOR ALL SIGNS, DEVICES AND PLACEMENT THEREOF ASSOCIATED WITH THIS TRAFFIC CONTROL PLAN ARE TO BE ABSORBED INTO PAY ITEM NO. 3, MAINTENANCE OF TRAFFIC.

THE CONTRACTORS ATTENTION IS CALLED TO SHEET 6364 OF THE PLANS FOR TEMPORARY STRIPING REQUIREMENTS

THE CONTRACTORS ATTENTION IS CALLED TO SHEET 6366 OF THE PLANS FOR DRUM PLACEMENT AND SHOULDER CLOSURE

THE CITY OF TUPELO, MS			
COLEY ROAD / MCCULLOUGH BLVD			
TRAFFIC CONTROL			
SITE "B"			
DATE	REVISION	FILE: \\MTP\Projects\Coley	
		WORKING NUMBER: TC-2	
DR. BY:	OK'D. BY:	JOB:	DATE:
JDC	JDC	Coley	11.08.17
			SHEET NUMBER:
			11



NOT TO SCALE

GENERAL NOTES:

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THE CONTRACTORS ATTENTION IS CALLED TO SHEET 6364 OF THE PLANS FOR TEMPORARY STRIPING REQUIREMENTS

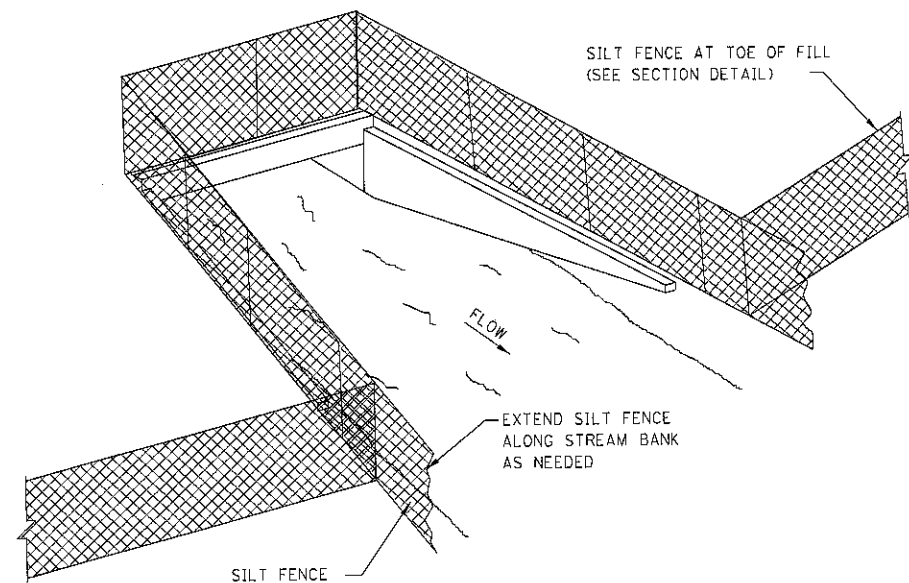
THE CONTRACTORS ATTENTION IS CALLED TO SHEET 6366 OF THE PLANS FOR DRUM PLACEMENT AND SHOULDER CLOSURE

THE CITY OF TUPELO, MS
COLEY ROAD / MCCULLOUGH BLVD

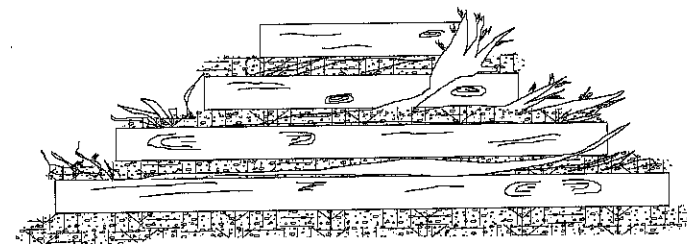
TRUCK DETOUR
SITE "B"

REVISION	FILE: \\MTP\Projects\Coley				WORKING NUMBER: TC-3
	DATE	DR. BY: JDC	CK'D. BY: JDC	JOB: Coley	DATE: 11.08.17

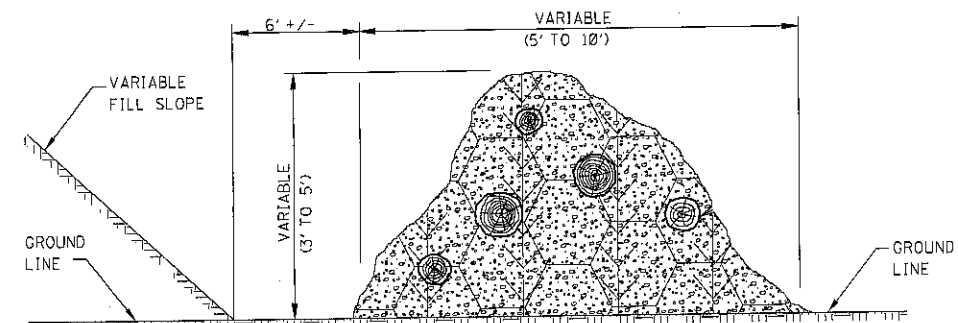
12



SEDIMENT BARRIER AT CROSS DRAIN



FRONT ELEVATION



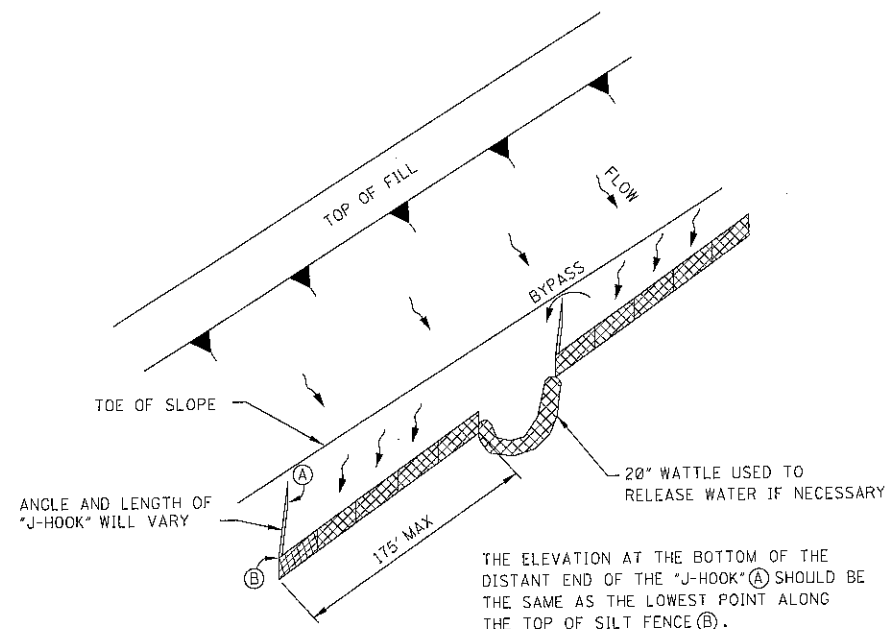
SIDE ELEVATION

TEMPORARY BRUSH BARRIER

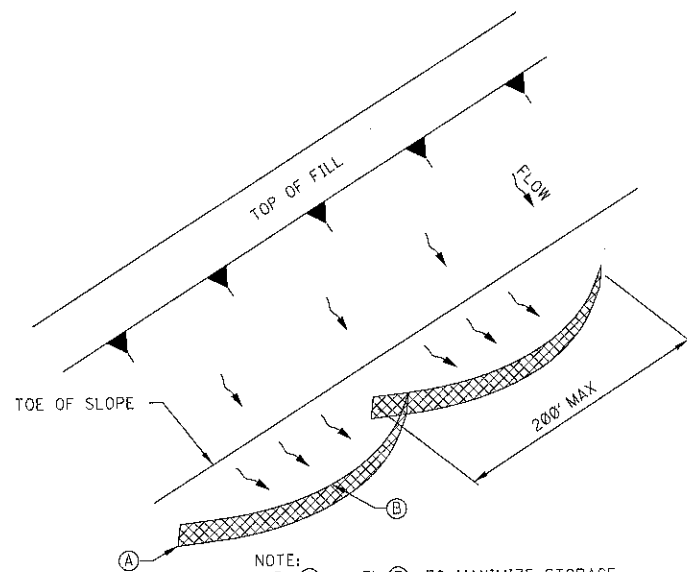
NOTES:

1. BRUSH BARRIER MAY BE USED WHERE NATURAL GROUND IS LEVEL OR SLOPING AWAY FROM PROJECT.
2. PLACE BRUSH, LOG AND TREE LAPS APPROXIMATELY PARALLEL TO TOE OF FILL SLOPE WITH SOME OF THE HEAVIER MATERIALS BEING PLACED ON TO TO PROPERLY SECURE THE BARRIER AS DETAILED AT LOCATIONS SHOWN ON PLANS OR AS DIRECTED OR PERMITTED BY THE ENGINEER.
3. TO ALLOW WATER TO SEEP THROUGH BRUSH BARRIER, INTERMINGLE THE BRUSH, LOG AND TREE LAPS SO AS NOT TO FORM A SOLID DAM.
4. THE BRUSH BARRIER MAY BE CHOKED WITH FILTER FABRIC. THE COST OF FABRIC TO BE INCLUDED IN OTHER ITEMS BID.
5. TEMPORARY BRUSH BARRIER WILL NOT BE MEASURED FOR SEPARATE PAYMENT.

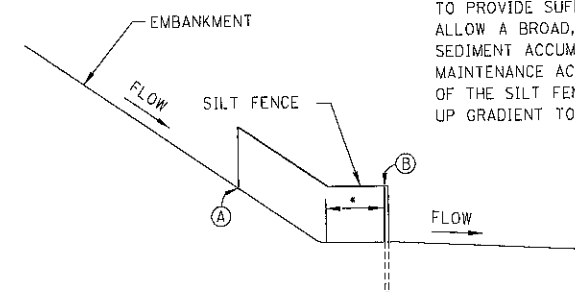
NOTE: ANCHOR AND INSTALL SILT FENCE PER DETAILS SHOWN ON WK. NO. ECD-3



"J-HOOK" SILT FENCE APPLICATION




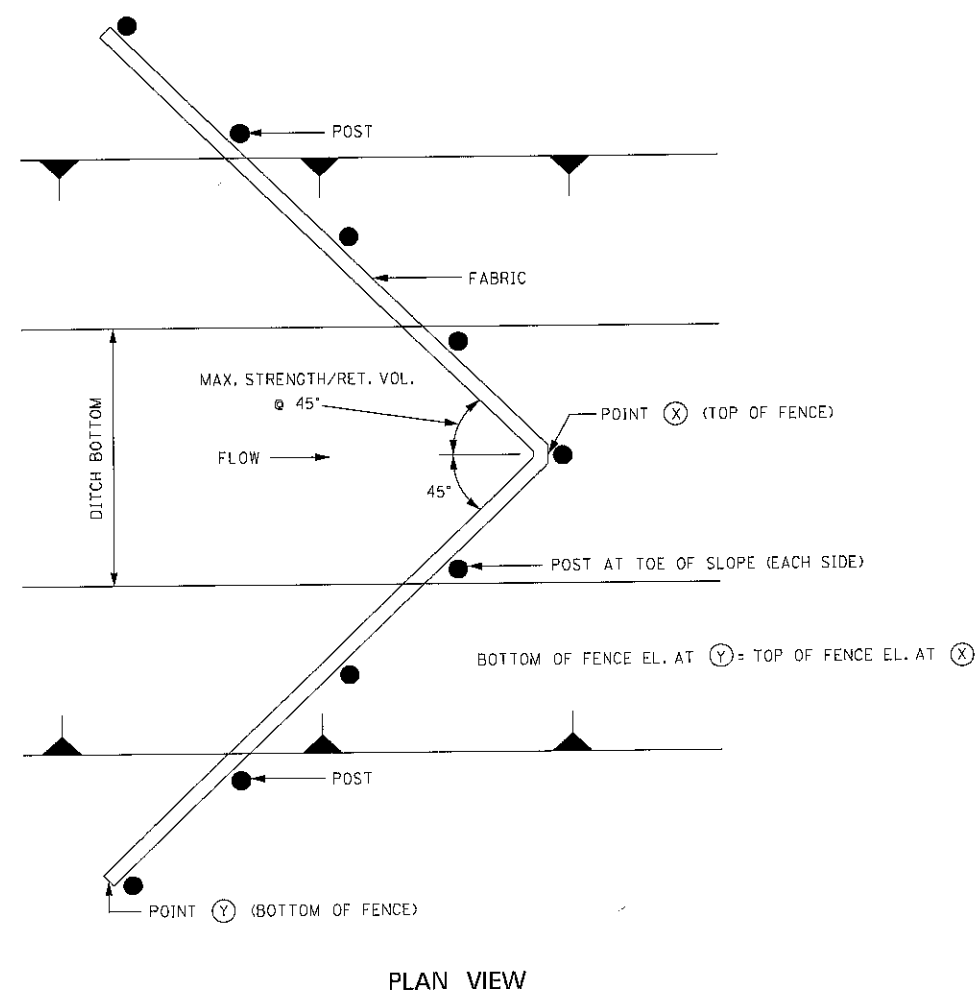
"SMILE-CONFIGURATION" SILT FENCE APPLICATION



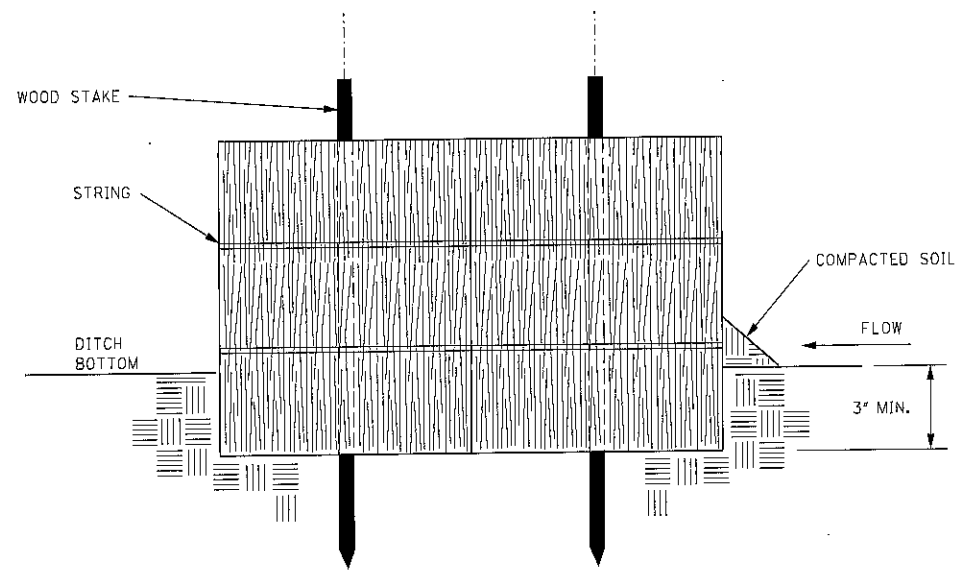
SILT FENCE SECTION AT TOE OF FILL

- SILT FENCE SHOULD BE LOCATED AWAY FROM THE TOE OF THE SLOPE TO PROVIDE SUFFICIENT SPACE TO ALLOW A BROAD, FLAT AREA FOR SEDIMENT ACCUMULATION AND MAINTENANCE ACTIVITIES. THE ENDS OF THE SILT FENCE SHOULD BE TURNED UP GRADIENT TO MAXIMIZE STORAGE.

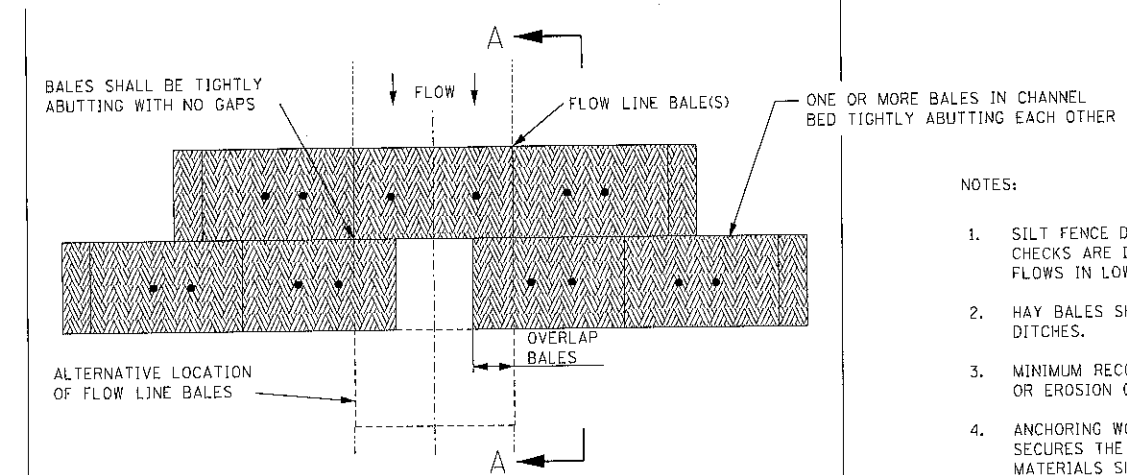
				MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
				ROADWAY DESIGN DIVISION	
				STANDARD PLAN	
				DETAILS OF SEDIMENT BARRIER APPLICATIONS	
					
				WORKING NUMBER	
				ECD-2	
				SHEET NUMBER	
				6102	
				ISSUE DATE: AUGUST 01, 2017	



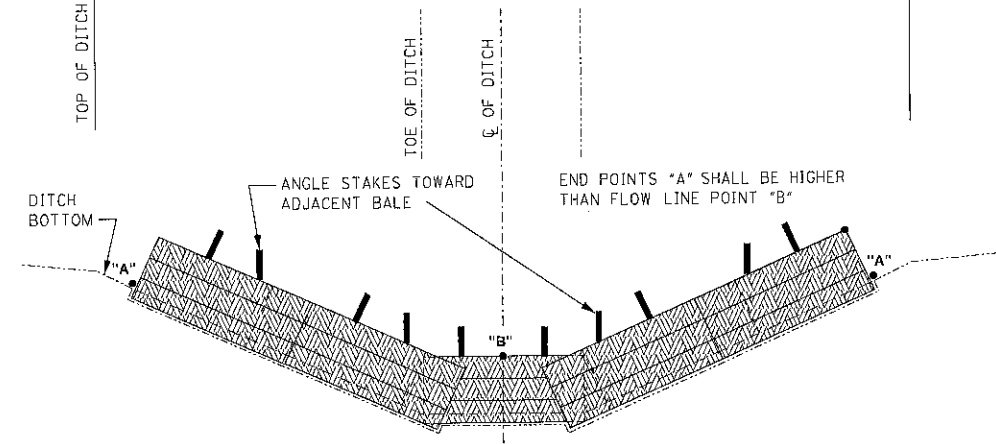
- NOTES:
1. ANCHOR AND INSTALL PER DETAILS FOR SILT FENCE SPACING GUIDELINES ON WK. NO. ECD-4.
 2. A "W" SHAPE MAY BE USED FOR WIDER DITCHES.



SECTION A-A



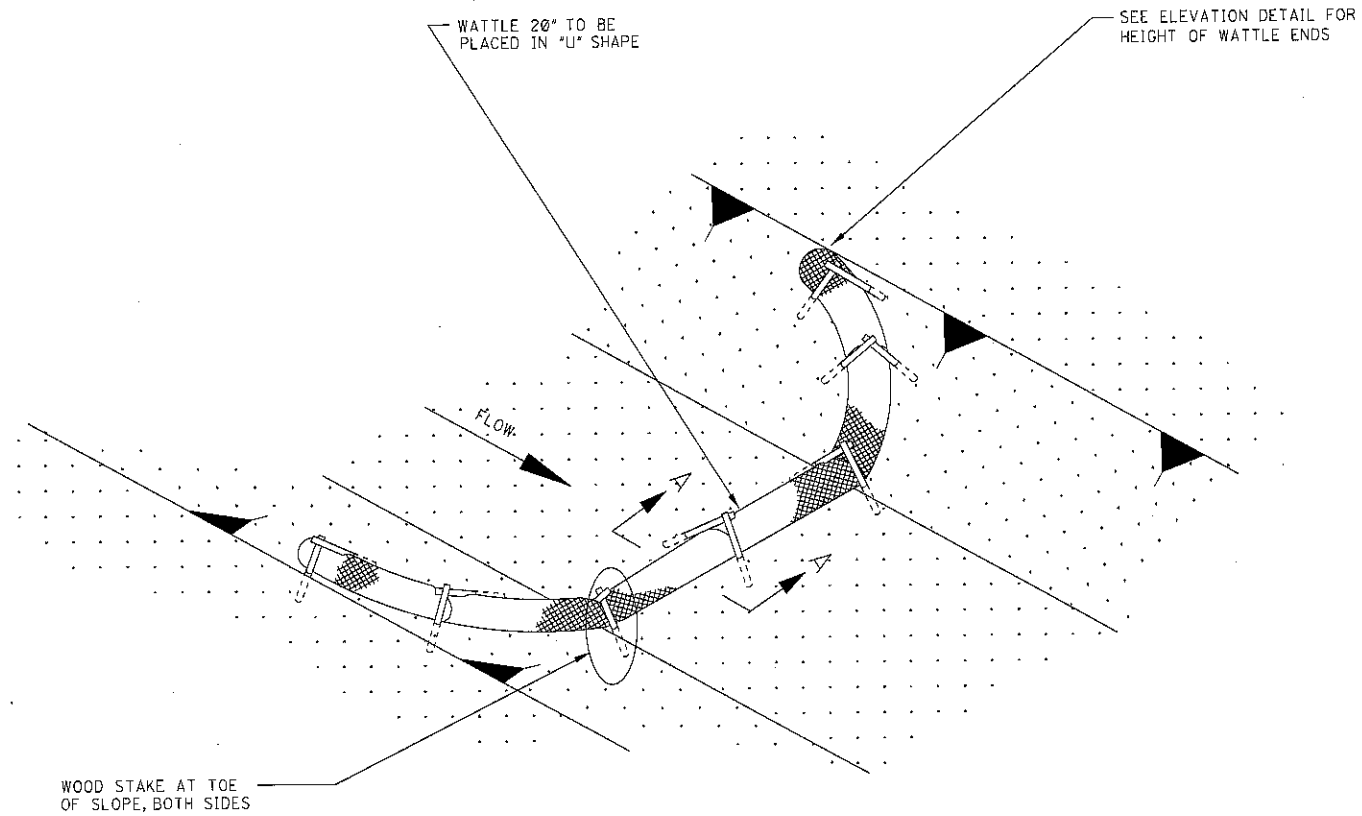
PLAN VIEW
TRAPEZOIDAL DITCH



PROFILE VIEW
TRAPEZOIDAL DITCH

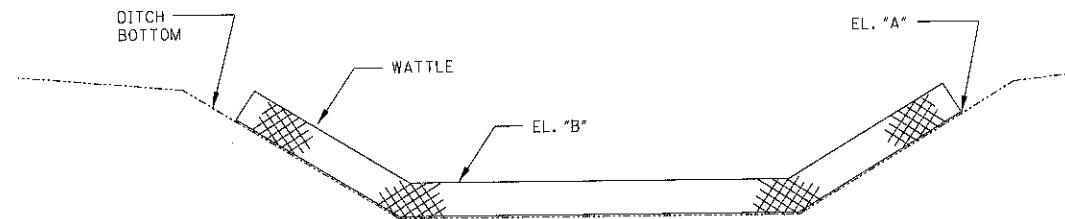
- NOTES:
1. SILT FENCE DITCH CHECKS SHOULD BE USED WHERE IT HAS BEEN DETERMINED THAT HAY BALE CHECKS ARE INADEQUATE. SILT FENCE DITCH CHECKS SHOULD BE USED TO INTERCEPT LOW VOLUME FLOWS IN LOW TO MODERATE GRADIENT DITCHES.
 2. HAY BALES SHOULD BE USED TO INTERCEPT LOW VOLUME FLOWS IN LOW TO MODERATE GRADIENT DITCHES.
 3. MINIMUM RECOMMENDED CHECK SPACING IS 100 FEET UNLESS SHOWN OTHERWISE ON THE PLANS OR EROSION CONTROL PLAN APPROVED BY THE ENGINEER. SEE SPACING GUIDANCE ON WK. NO. ECD-4.
 4. ANCHORING WOOD STAKES SHALL BE SIZED, SPACED, AND BE OF A MATERIAL THAT EFFECTIVELY SECURES THE CHECK. A MINIMUM OF TWO STAKES PER BALE IS REQUIRED. ALL NON-DEGRADABLE MATERIALS SHALL BE REMOVED WHEN NO LONGER NEEDED.
 5. BALES SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 3 INCHES.
 6. BALES SHALL BE PLACED IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES. THE BALES SHALL BE PLACED WITH BINDINGS PARALLEL TO THE GROUND.
 7. SOIL IS COMPACTED ALONG THE BASE OF THE UPSTREAM FACE TO PREVENT PIPING.
 8. MULTIPLE ADJACENT ROWS OF BALES ARE REQUIRED AS SHOWN.

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		TEMPORARY EROSION, SEDIMENT, AND WATER POLLUTION CONTROL MEASURES (SILT FENCE AND HAY BALE DITCH CHECKS)	
DATE		ISSUE DATE: AUGUST 01, 2017	
		WORKING NUMBER ECD-5 SHEET NUMBER 6105	

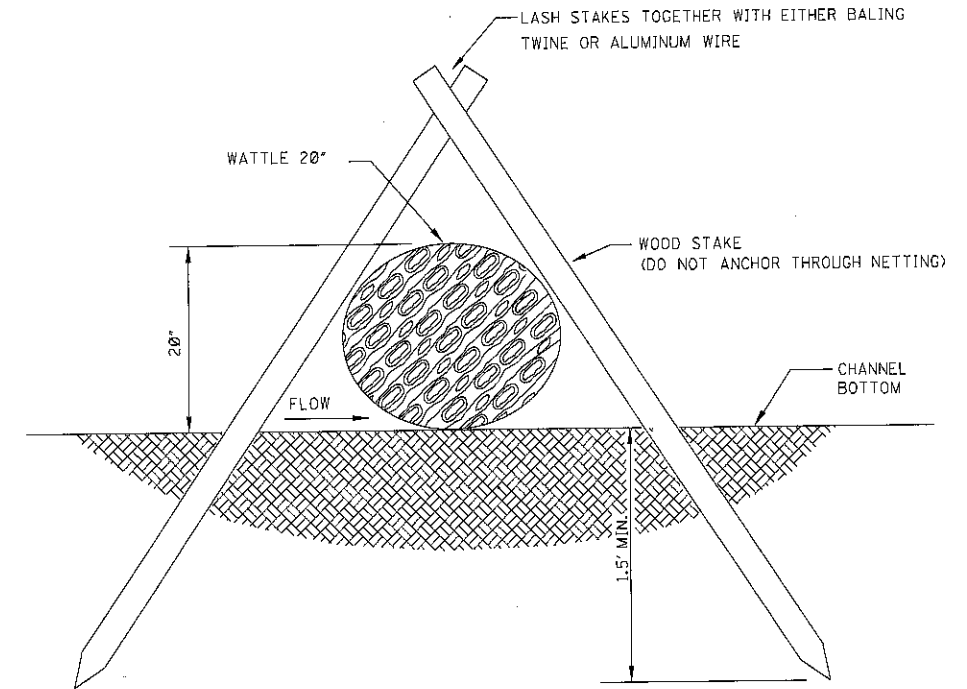


DETAIL (DITCH CHECK)

NOTE: END POINTS "A" SHALL BE HIGHER THAN FLOWLINE POINT "B".



ELEVATION DETAIL

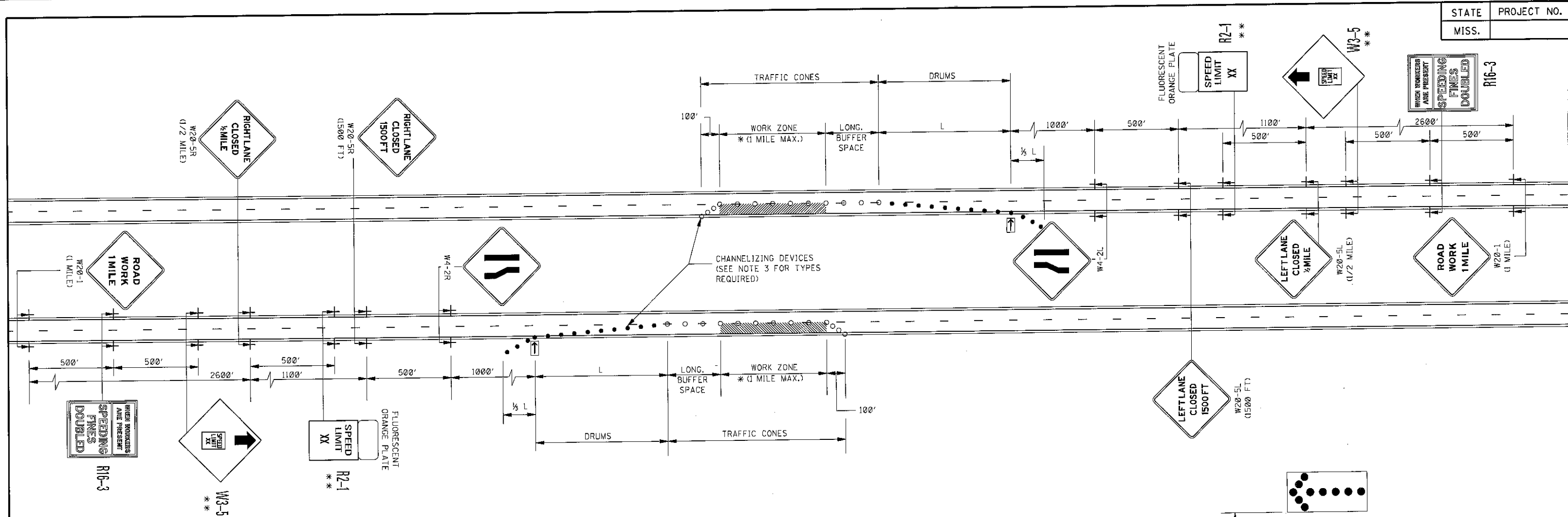


SECTION A-A

NOTES:

1. WATTLE DITCH CHECKS CAN BE USED FOR VELOCITY REDUCTION AND CONTROL OF SEDIMENT TRANSPORT UNDER LOW TO MEDIUM FLOW CONDITIONS.
2. THE PLACEMENT INTERVAL BETWEEN WATTLE DITCH CHECK SHALL BE 100' UNLESS SHOWN OTHERWISE ON THE PLANS OR EROSION CONTROL PLAN APPROVED BY THE ENGINEER. SEE SPACING GUIDANCE ON WK. NO. ECD-4.
3. ANCHORING WOOD STAKES SHALL BE SIZED, SPACED, DRIVEN, AND BE OF A MATERIAL THAT EFFECTIVELY SECURES THE CHECK. STAKE SPACING SHALL BE A MAXIMUM OF THREE FEET. ALL NON-DEGRADABLE MATERIALS SHALL BE REMOVED WHEN NO LONGER NEEDED.
4. TRENCHING OF WATTLES MAY BE NECESSARY IF PIPING BECOMES EVIDENT.
5. WATTLES SHOULD NOT BE USED IN HARD BOTTOM CHANNELS.
6. IN THE EVENT WATTLES CANNOT BE SECURED IN PLACE USING WOOD STAKES, SAND BAGS MAY BE USED IN LIEU OF WOOD STAKES IN ORDER TO SECURE THE WATTLES IN PLACE. IF SANDS BAGS ARE USED IN THIS APPLICATION THEY WILL NOT BE A SEPARATE PAY ITEM.

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		DETAILS OF EROSION CONTROL WATTLE DITCH CHECK	
DATE		ISSUE DATE: AUGUST 01, 2017	
		WORKING NUMBER ECD-6	
		SHEET NUMBER 6106	



GENERAL NOTES:

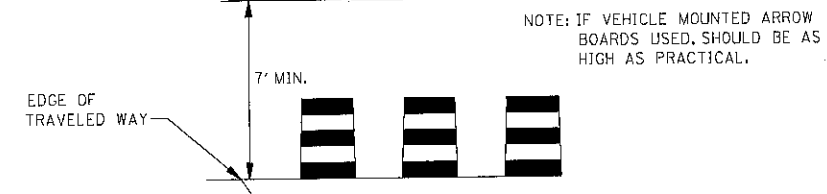
1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER RATES
	TAPER	ALONG LANE LINE & WORK ZONE		
mph				
≤40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
L = WS FOR SPEEDS OF 45 mph OR GREATER
L = WS²/60 FOR SPEEDS OF 40 mph OR LESS
WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

†† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

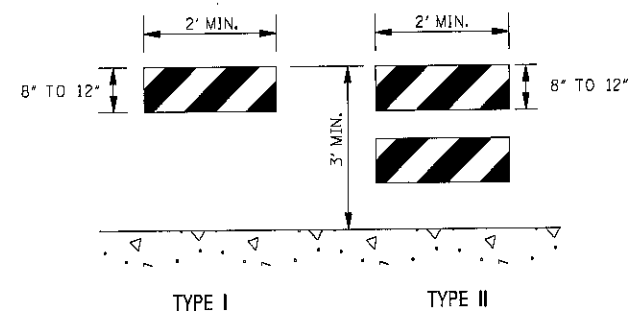
2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
A. APPROACH TAPER- RETROREFLECTIVE PLASTIC DRUMS
B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT MINIMUM)
C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT MINIMUM)
4. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
6. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48", AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
7. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.



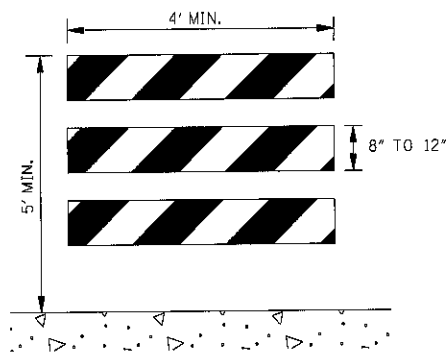
LEGEND

- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- ** THE SPEED ON R2-1 AND W3-5 SIGNS SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER.
- ➡ FLASHING ARROW PANEL (TYPE "C")
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT MINIMUM)

BY	DATE	REVISION	DATE
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT LESS THAN 65 MPH (4-LANE: MEDIAN LANE OR OUTSIDE LANE CLOSURE) (WORK DAY ONLY)			
ISSUE DATE: AUGUST 01, 2017			MDOT WORKING NUMBER TCP-2 SHEET NUMBER 6352



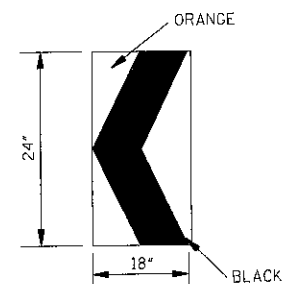
TYPE I TYPE II



TYPE III

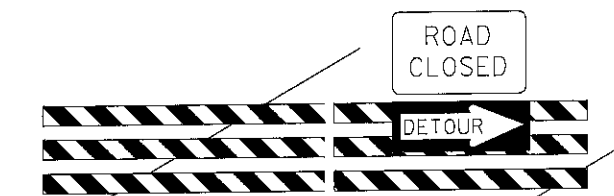
STANDARD BARRICADES

1. THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
2. RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
3. DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.
5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES WHICH REQUIRE CRASHWORTHINESS ACCEPTANCE LETTERS. TO DATE, 2-IN. THICK TIMBER RAILS HAVE NOT BEEN SUCCESSFULLY CRASH TESTED. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE:
http://safety.fhwa.dot.gov/roadway_dept/policy_guide/road_hardware/cat2.cfm



CHEVRON SIGN
DETAIL

1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.



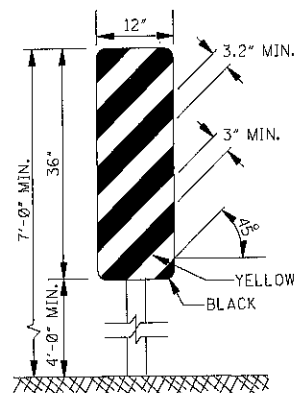
BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

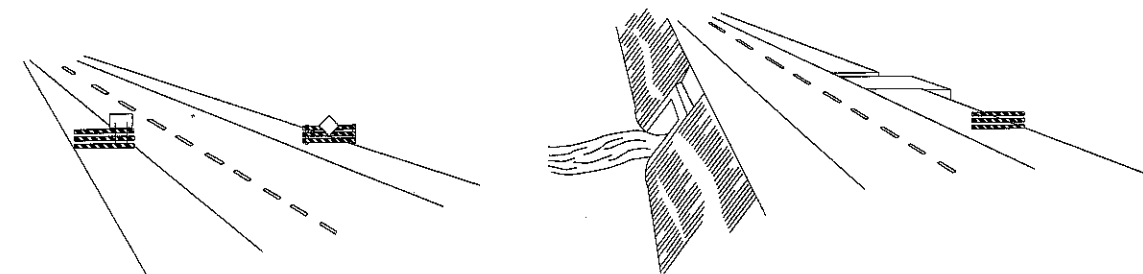
* 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.

** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 in² OF REFLECTIVE AREA FACING TRAFFIC.



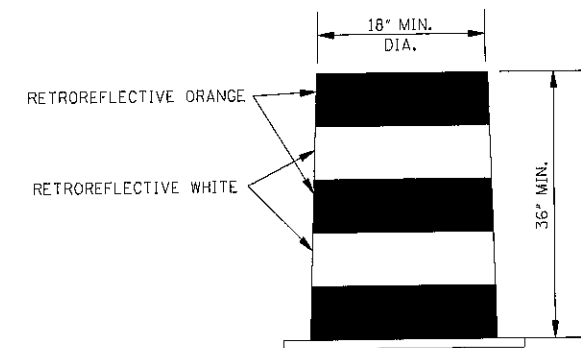
TYPE 3 OBJECT MARKER
(OM-3R)

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
2. THE OM-3R IS SHOWN. THE OM-3L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
2. WING BARRICADES SHOULD BE USED:
A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.



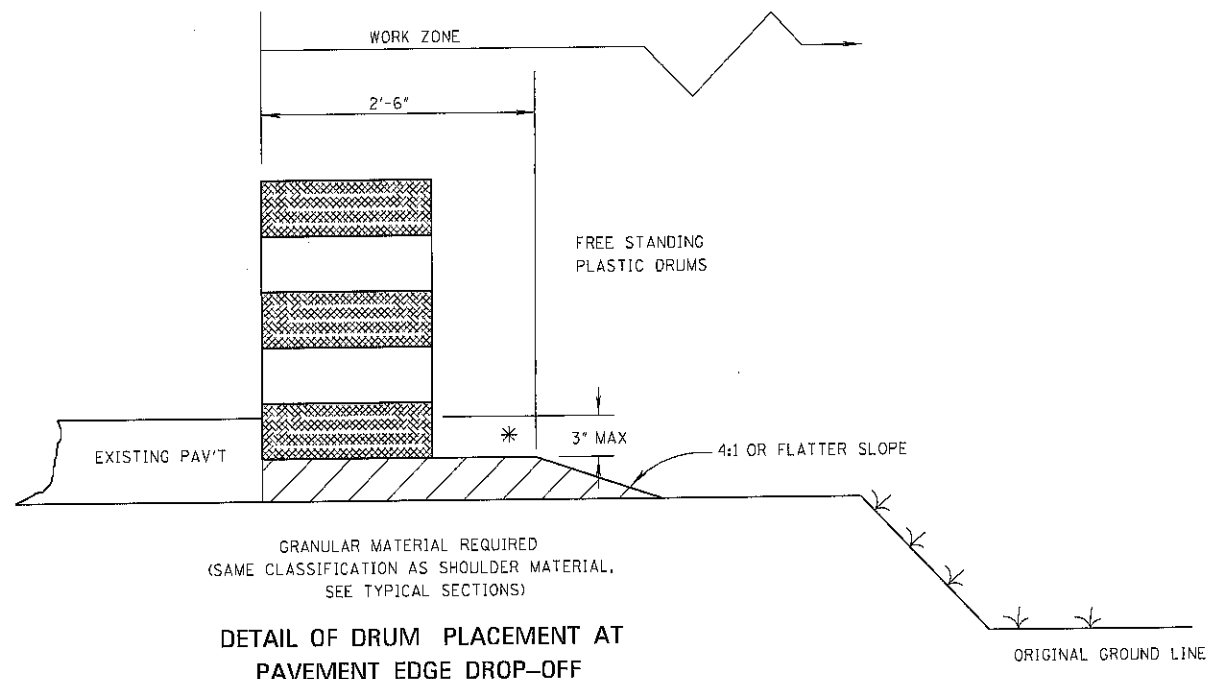
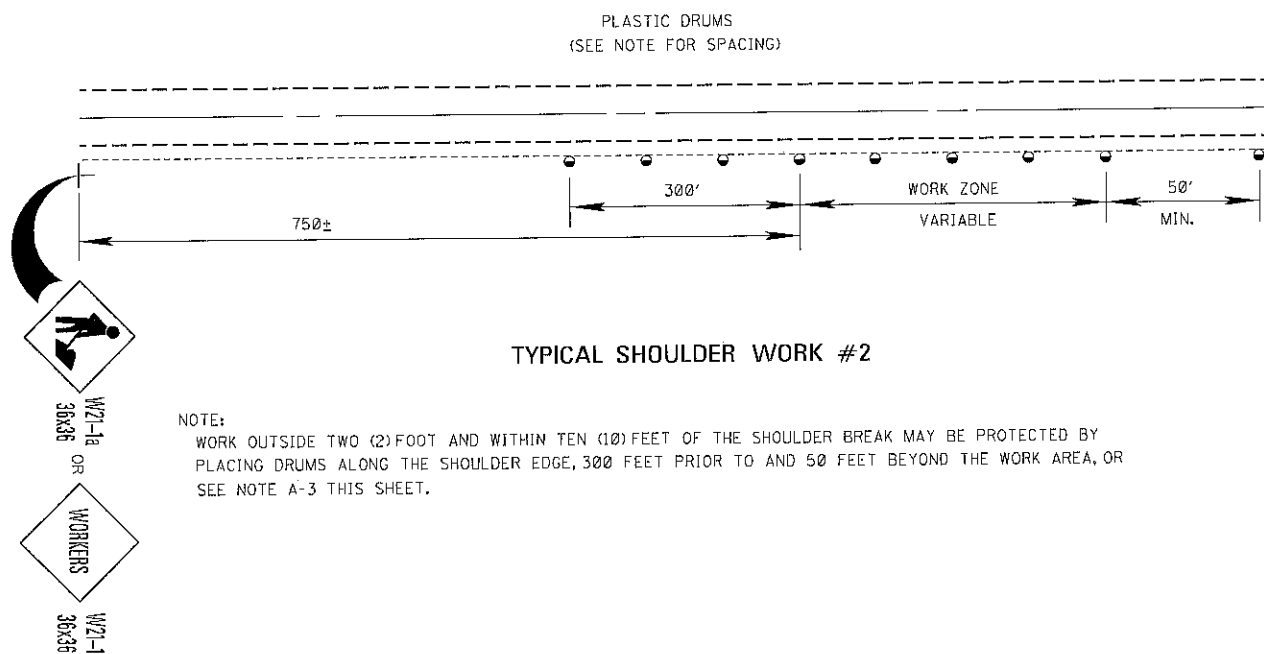
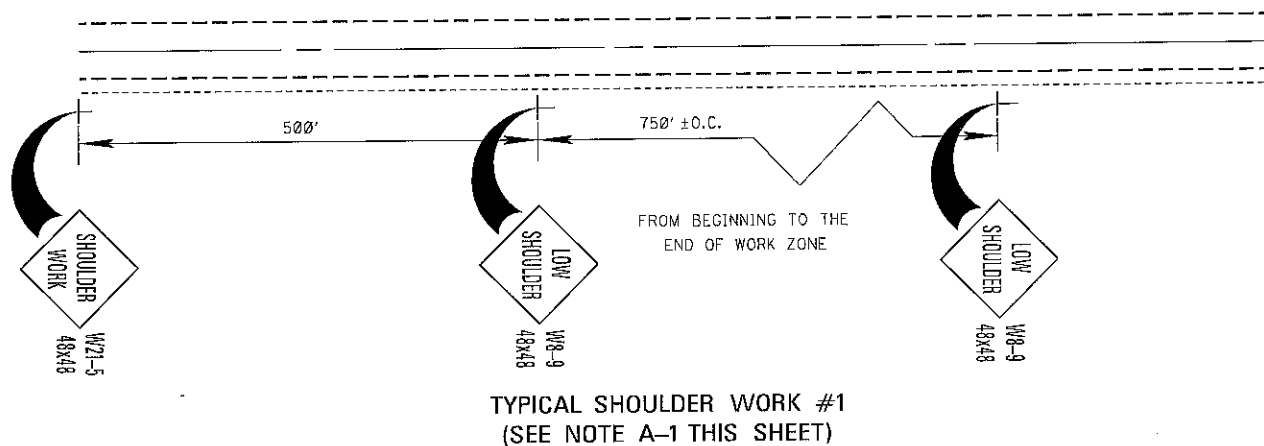
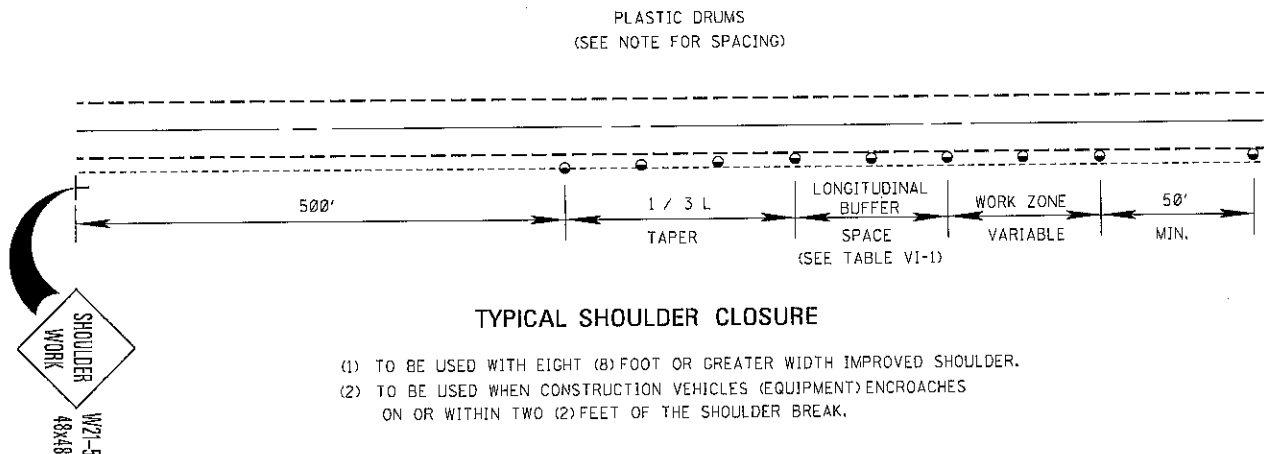
PLASTIC DRUM STRIPING DETAIL

1. PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE WITH FOUR (4) RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
2. DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
3. WHERE PRACTICAL PLASTIC DRUMS SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	
DATE	ISSUE DATE: AUGUST 01, 2017

**HIGHWAY SIGN AND
BARRICADE DETAILS
FOR CONSTRUCTION
PROJECTS**

MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
WORKING NUMBER
TCP-8
SHEET NUMBER
6358



NOTES:

* A. PAVEMENT EDGE DROP-OFF

1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750' ± O.C.).
2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE M.U.T.C.D. (1 / 3 L, WHERE L IS THE TAPER LENGTH IN FEET.)
3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

B. DRUM SPACING

1. TANGENTS = 2 X S
2. TAPERS = L / 3
WHERE L = S X W
L = TAPER LENGTH IN FEET
S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
W = WIDTH OF OFFSET IN FEET

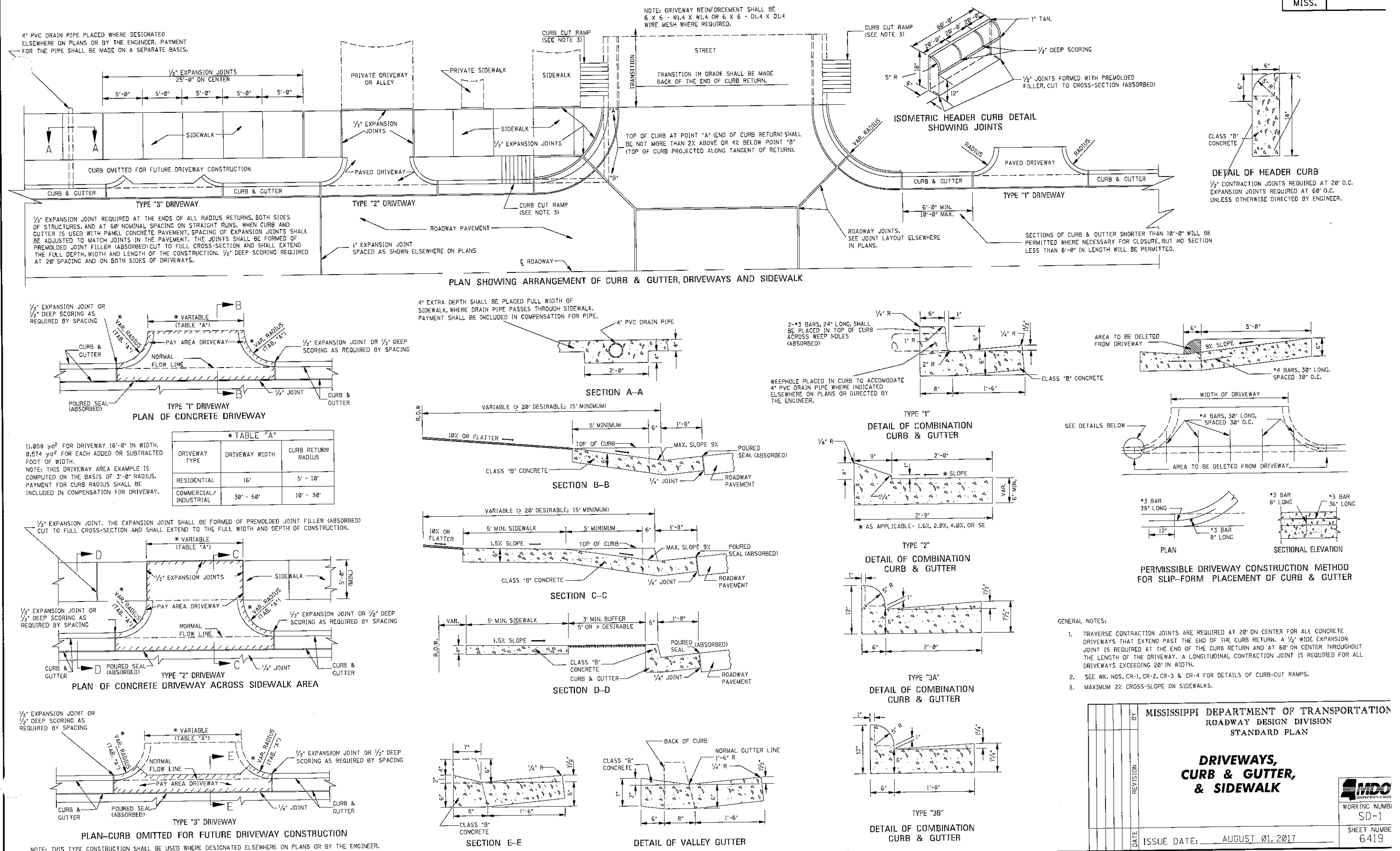
C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

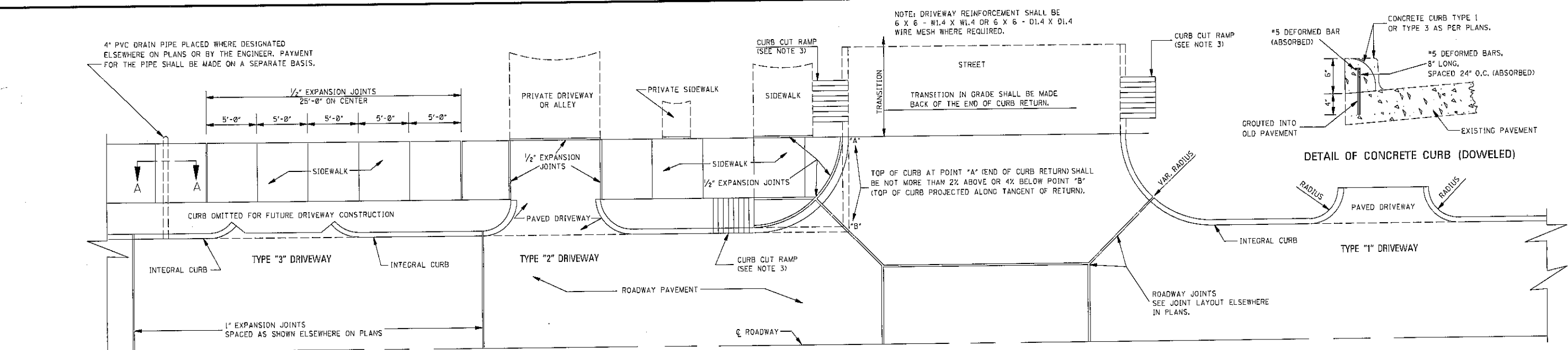
TABLE VI-1. GUIDELINES FOR LENGTH OF
LONGITUDINAL BUFFER SPACE

* * SPEED (MPH)	LENGTH (FEET)
20	35
25	55
30	85
35	120
40	170
45	220
50	280
55	335
60	415
65	485

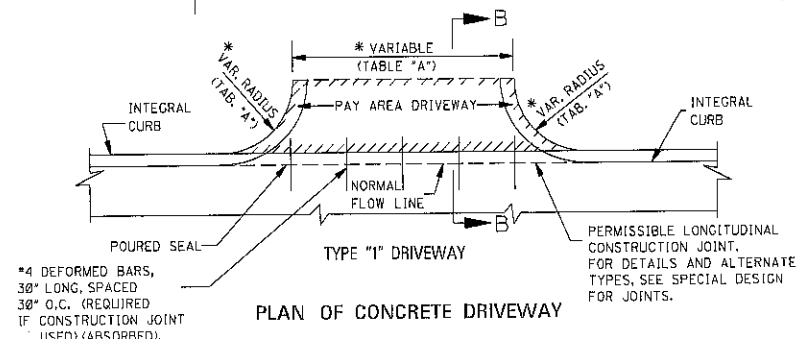
* * POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED
PRIOR TO WORK STARTING, OR THE ANTICIPATED
OPERATING SPEED IN MPH.

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	TRAFFIC CONTROL DETAILS DRUM PLACEMENT AND SHOULDER CLOSURE
DATE	ISSUE DATE: AUGUST 01, 2017
WORKING NUMBER	TCP-16
SHEET NUMBER	6366



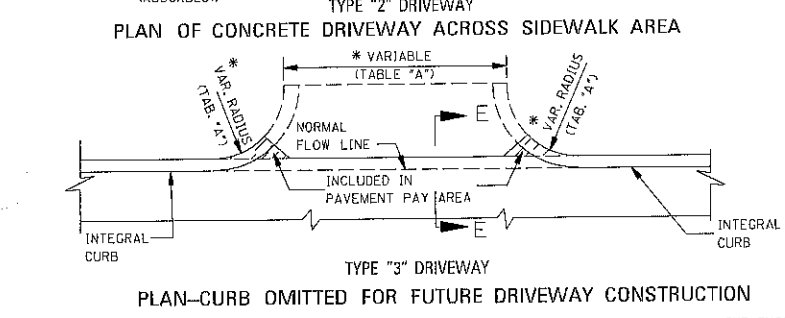
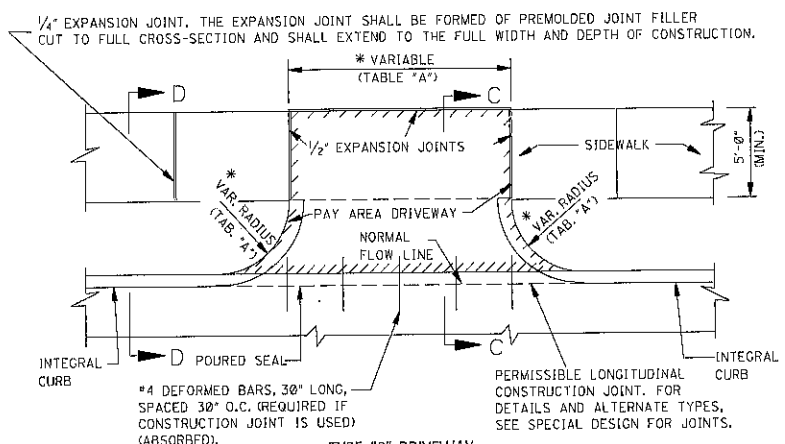
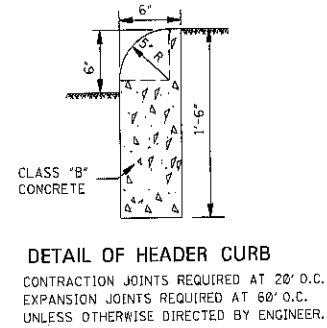
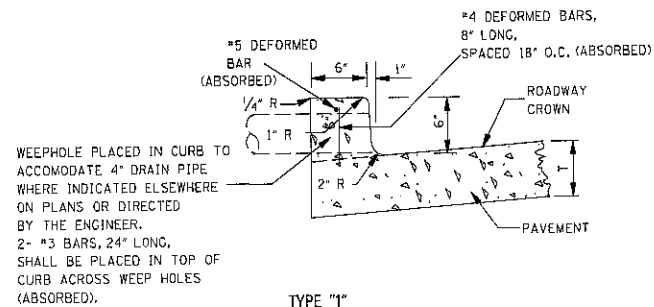
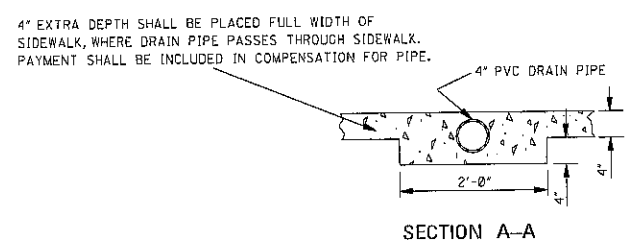


PLAN SHOWING ARRANGEMENT OF INTEGRAL CURB, DRIVEWAYS AND SIDEWALK

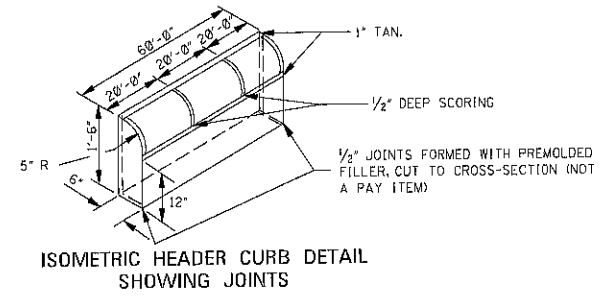
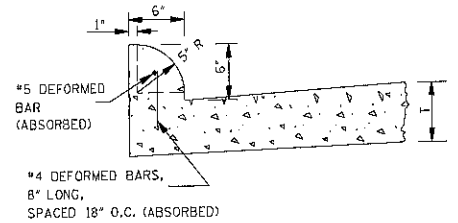
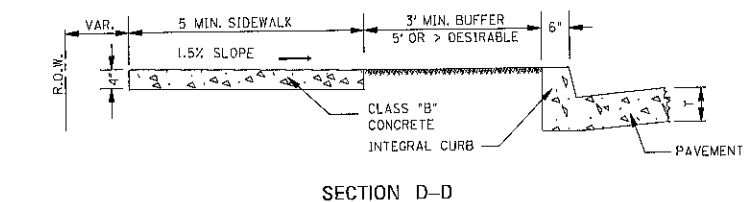
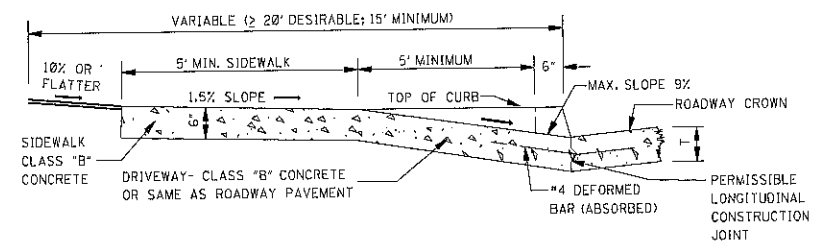
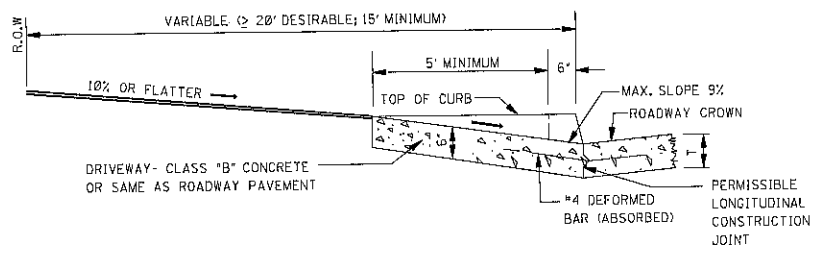


5,761 yd² FOR DRIVEWAY 16'-0" IN WIDTH.
0.333 yd² FOR EACH ADDED OR SUBTRACTED FOOT OF WIDTH.
NOTE: THIS DRIVEWAY AREA EXAMPLE IS COMPUTED ON THE BASIS OF 3'-0" RADIUS. PAYMENT FOR CURB RADIUS SHALL BE INCLUDED IN COMPENSATION FOR DRIVEWAY.

*TABLE "A"		
DRIVEWAY TYPE	DRIVEWAY WIDTH (ft)	CURB RETURN RADIUS (ft)
RESIDENTIAL	16'	5' - 10'
COMMERCIAL/ INDUSTRIAL	30' - 50'	10' - 30'



NOTE: THIS TYPE CONSTRUCTION SHALL BE USED WHERE DESIGNATED ELSEWHERE ON PLANS OR BY THE ENGINEER.



- GENERAL NOTES:
1. TRAVERSE CONTRACTION JOINTS ARE REQUIRED AT 20' ON CENTER FOR ALL CONCRETE DRIVEWAYS THAT EXTEND PAST THE END OF THE CURB RETURN. A 1/2" WIDE EXPANSION JOINT IS REQUIRED AT THE END OF THE CURB RETURN AND AT 60' ON CENTER THROUGHOUT THE LENGTH OF THE DRIVEWAY. A LONGITUDINAL CONTRACTION JOINT IS REQUIRED FOR ALL DRIVEWAYS EXCEEDING 20' IN WIDTH.
 2. SEE WK. NOS. CR-1, CR-2, CR-3 & CR-4 FOR DETAILS OF CURB-CUT RAMPS.
 3. MAXIMUM 2% CROSS-SLOPE ON SIDEWALKS.

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
REVISION		ROADWAY DESIGN DIVISION	
DATE		STANDARD PLAN	
		DRIVEWAYS, INTEGRAL CURB & SIDEWALK	
		ISSUE DATE: AUGUST 01, 2017	
		SHEET NUMBER 6420	

WORKING NUMBER SD-2

MDC

