ROSCOE 2025 RESIDENTIAL STREET PROGRAM

FOR

VILLAGE OF ROSCOE

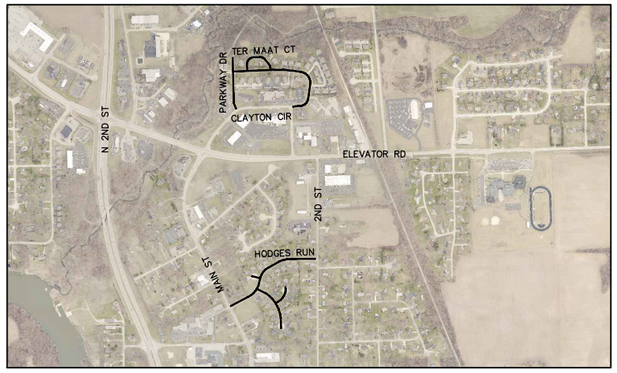
ROSCOE, ILLINOIS

PROPOSED STREET PLANS

WINNEBAGO COUNTY

MAY 2025

INDEX OF SHEETS



LOCATION MAP

N.T.S.

UTILITIES

UTILITY TYPE	COMMON NAME
WATER	NORTH PARK PUBLIC WATER
SEWER	FOUR RIVERS SANITATION AUTHORITY
ELECTRIC	COMED
TELEPHONE	CHARTER COMMUNICATIONS
GAS	NICOR
CABLE	CHARTER COMMUNICATIONS

(CONTRACTOR TO BE RESPONSIBLE FOR COORDINATING ANY ADJUSTMENTS TO BE MADE.)

SPEED LIMIT = ≤ 25 MPH

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FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL

ILLINOIS IOWA WISCONSIN

ILLINOIS PROFESSIONAL DESIGN FIRM NUMBER: 184003525

LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
TYLER V. NELSON
062-061714
EXPIRES 11/30/25

SIGNATURE DATE

PRELIMINARY

ORIGINAL SET FOR PROJECT: 25-694	DATE CREATED: 05/15/2025

REVISIONS

REV. NO.	DESCRIPTION	DATE

G:\C30\25\25-694B\Plans\25-694 Plans.dwg, Title
PLOT DATE: 5/15/25

GENERAL NOTES

1. THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE MUNICIPAL CODE, VILLAGE OF ROSCOE, ILLINOIS, CURRENT EDITION, THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", CURRENT EDITION, "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS," CURRENT EDITION, SPECIAL PROVISIONS AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", CURRENT EDITION. SIGN CONSTRUCTION AND PAVEMENT MARKINGS SHALL CONFORM TO THE REQUIREMENTS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", CURRENT EDITION.

2. IN THESE CONTRACT DOCUMENTS MENTION IS MADE OF THE "ENGINEER", WHICH SHALL MEAN FEHR GRAHAM OR THEIR DULY AUTHORIZED AGENT. IN THESE CONTRACT DOCUMENTS MENTION IS MADE OF THE "OWNER", WHICH SHALL MEAN VILLAGE OF ROSCOE, OR THEIR DULY AWARDED AGENT.

3. AS PART OF THE BIDDING PROCEDURE, THE CONTRACTOR SHALL VERIFY THAT THE QUANTITIES FOR PAY ITEMS, AS PRESENTED IN THESE PLAN DOCUMENTS, ARE SUBSTANTIALLY CORRECT. IF DISCREPANCIES ARE DETECTED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF THE DISCREPANCY PRIOR TO THE BID DATE.

4. QUANTITIES SHOWN ARE ESTIMATES FOR INFORMATION ONLY. PAYMENT WILL BE BASED ON ACTUAL QUANTITIES MEASURED IN THE FIELD OR ON PAYMENT LIMIT DETAILS.

5. THE CONTRACTOR SHALL BE PAID FOR MATERIALS AND EQUIPMENT SUCCESSFULLY INSTALLED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS AS MEASURED OR VERIFIED IN PLACE BY THE ENGINEER OR HIS AGENT.

6. IN CASE OF CONFLICT BETWEEN THE ABOVE MENTIONED SPECIFICATIONS, THE ENGINEER SHALL DETERMINE WHICH OF THE SPECIFICATIONS SHALL GOVERN. THE ENGINEER'S DECISION SHALL BE FINAL AND NO ADDITIONAL COMPENSATION SHALL BE AWARDED UNLESS APPROVED BY THE ENGINEER.

7. THE PROPOSED IMPROVEMENTS MUST BE CONSTRUCTED IN ACCORDANCE WITH THE ENGINEERING PLANS AS APPROVED BY THE OWNER. IMPROVEMENT REPRESENTATIONS AS SHOWN ON THESE PLANS, ARE AS ACCURATE AS POSSIBLE FROM THE INFORMATION AVAILABLE. HOWEVER SOME FIELD REVISIONS MAY BE REQUIRED TO ACCOMMODATE UNFORESEEN CIRCUMSTANCES – THE ENGINEER SHALL BE ADVISED OF ANY NECESSARY REVISIONS WITH SUFFICIENT LEAD TIME ALLOWED TO PROPERLY CONSIDER AND ACT UPON SAID REQUESTS. PROPER CONSTRUCTION TECHNIQUES MUST BE FOLLOWED IN CONSTRUCTING THOSE IMPROVEMENTS AS DETAILED IN THIS ENGINEERING PLAN.

8. THE ENGINEER SHALL HAVE THE AUTHORITY TO INSPECT, APPROVE OR REJECT THE WORKMANSHIP AND/OR MATERIALS WHICH GO TO MAKE UP IMPROVEMENTS AS DETAILED IN THESE PLANS AND SPECIFICATIONS.

9. GENERAL SAFETY PROVISION: TO PROVIDE DRIVERS WITH SAFE TRAVEL CONDITIONS DURING THE CONSTRUCTION PROJECT, AND TO PROVIDE SAFE WORKING CONDITIONS FOR ALL EMPLOYEES, THE RULES, REGULATIONS, AND CONDITIONS STATED BELOW WILL PREVAIL FOR THE DURATION OF THIS CONTRACT. ANY EMPLOYEE OF THE CONTRACTOR OR HIS SUBCONTRACTORS WHO REFUSES TO COMPLY WITH THESE GENERAL SAFETY PROVISIONS SHALL BE REMOVED FROM THE JOB SITE IN ACCORDANCE WITH STATE AND LOCAL REQUIREMENTS. THE CONTRACTOR AND ANY SUBCONTRACTORS RETAINED BY HIM SHALL COMPLY WITH THE STATE AND FEDERAL REQUIREMENTS OF THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970 (OSHA), JULY 1, 1987 AS IT RELATES TO CONTRACTOR'S OPERATIONS.

10. THE CONTRACTOR SHALL COMPLY WITH ALL STATE REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION. THE CONTRACTOR WILL NOT BE ALLOWED TO BUILD FIRES ON THE SITE.

11. THE SCALE SHOWN ON THE DRAWINGS APPLIES ONLY TO THE FULL SIZE PLANS NOT THE REDUCED SIZE PLANS.

12. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN DRAINAGE FLOWS AT ALL TIMES DURING THE PERFORMANCE OF THE WORK. METHODS USED BY THE CONTRACTOR SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. COST OF MAINTAINING DRAINAGE FLOWS SHALL BE INCIDENTAL TO THE CONTRACT.

13. WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED OR DISTURBED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS, MONUMENTS AND RIGHT-OF-WAY PINS UNTIL THE OWNER, AND AUTHORIZED SURVEYOR, OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DESTROYED BY HIS OPERATIONS. REPLACEMENT OF MONUMENTS WILL BE DETERMINED BY THE ENGINEER.

14. THE CONTRACTOR SHALL REMOVE, STORE, AND RELOCATE TO THE SATISFACTION OF THE ENGINEER ALL EXISTING SIGNAGE IN ACCORDANCE WITH STATE AND LOCAL REQUIREMENTS, AND CONSIDER THIS AS INCIDENTAL TO THE CONTRACT.

15. OUTSIDE THE EXISTING RIGHT-OF-WAY, THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATION NEAR ANY AND ALL EXISTING SIGNS OUTSIDE THE RIGHT-OF-WAY. ANY SIGNS REMOVED FOR CONSTRUCTION PURPOSES SHALL BE CAREFULLY REMOVED AND RE-ERECTED BY THE CONTRACTOR AT A LOCATION NEAREST TO THE ORIGINAL LOCATION, OR AT A LOCATION DETERMINED BY THE ENGINEER IN THE FIELD. REMOVAL AND RE-ERECTED SIGNS AND ANY DAMAGE DONE TO EXISTING SIGNS BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL EXPENSE TO THE OWNER.

16. ALL ITEMS SHALL INCLUDE ALL THE NECESSARY MATERIALS AND LABOR TO COMPLETE THE ITEM IN PLACE. MATERIALS AND LABOR NOT SPECIFICALLY IDENTIFIED SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

17. AT THE END OF EACH DAY, THE CONTRACTOR SHALL SECURE THE CONSTRUCTION WORK ZONE FROM POTENTIAL INTRUDERS.

18. THE CONTRACTOR SHALL FIELD VERIFY THE ELEVATIONS OF THE BENCHMARKS PRIOR TO COMMENCING WORK. THE CONTRACTOR SHALL ALSO FIELD VERIFY LOCATION, ELEVATION AND SIZE OF EXISTING UTILITIES, AND VERIFY PAVEMENT ELEVATIONS WHERE MATCHING INTO EXISTING WORK. THE CONTRACTOR SHALL FIELD VERIFY HORIZONTAL CONTROL BY REFERENCING SHOWN COORDINATES TO KNOWN PROPERTY LINES. NOTIFY ENGINEER OF DISCREPANCIES IN EITHER VERTICAL OR HORIZONTAL CONTROL PRIOR TO PROCEEDING WITH WORK.

19. THE CONTRACTOR SHALL CONTACT THE ENGINEER OF ANY ERRORS OR DISCREPANCIES WHICH MAY BE SUSPECTED IN LINES AND GRADES, AND SHALL NOT PROCEED WITH THE WORK UNTIL ALL LINES AND GRADES WHICH ARE BELIEVED TO BE IN ERROR HAVE BEEN VERIFIED OR CORRECTED BY THE ENGINEER OR HIS REPRESENTATIVE.

20. THE ENGINEER AND OWNER ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCE OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF THEIR WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.

21. ALL ITEMS TO BE REMOVED AND NOT DEFINED AS A PAY ITEM SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

22. ALL EXCESS EARTH EXCAVATION, EXCESS MATERIALS, OR OTHER REMOVED ITEMS SHALL BE HAULED OFF-SITE AT THE CONTRACTOR'S EXPENSE, UNLESS OTHERWISE APPROVED BY THE OWNER.

23. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL OBSTRUCTIONS, TREES, DEBRIS AND BRUSH AS DESIGNATED BY THE OWNER AND AS INDICATED ON THE PLANS. THIS WORK SHALL BE IN ACCORDANCE WITH SECTION 201 OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", CURRENT EDITION. ALL MATERIALS SHALL BE DISPOSED OF AT THE CONTRACTOR'S EXPENSE. DURING CONSTRUCTION, CARE SHALL BE TAKEN TO MINIMIZE DAMAGE TO THE EXISTING TREES AND LANDSCAPING. ONLY THOSE ITEMS DESIGNATED BY THE OWNER SHALL BE REMOVED.

GENERAL NOTES CONTINUED

24. ALL ROADWAY REMOVAL ITEMS SHALL CONFORM TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", CURRENT EDITION. ALL JOINTS BETWEEN THE PORTION REMOVED AND THAT LEFT IN PLACE SHALL BE SAWED TO SUCH A DEPTH THAT A CLEAN, NEAT EDGE WILL RESULT WITH NO SPALLING TO THE REMAINING PORTION. THE COST OF SAWING SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. ADDITIONAL SAWING OR RE-SAWING MAY BE REQUIRED AS DIRECTED BY THE ENGINEER WITH NO ADDITIONAL COMPENSATION BEING ALLOWED. THE COST OF SAWCUTTING THE EXISTING PAVEMENT SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

25. WHEN ARTIFICIAL LIGHTING IS UTILIZED DURING NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC, AS WELL AS ADJOINING RESIDENTIAL AREAS.

26. THE CONTRACTOR IS REQUIRED TO STAY WITHIN THE NOTED PROPERTY BOUNDARIES RIGHT-OF-WAY AND EASEMENTS AS SHOWN IN THE PLANS. ANY ADDITIONAL EASEMENTS SHALL BE SECURED BY THE CONTRACTOR AT NO EXTRA COST.

27. ANY AREAS DAMAGED OR DISTURBED DURING THE PROJECT AS A DIRECT OR INDIRECT RESULT OF CONTRACTOR OPERATIONS, SHALL BE RESTORED TO A CONDITION EQUAL TO OR BETTER THAN THE ORIGINAL CONDITION. THE COST OF SAID RESTORATION OR REPAIR SHALL BE BORNE TOTALLY BY THE CONTRACTOR, WITH NO EXTRA COMPENSATION BEING AWARDED UNDER THIS CONTRACT. THE RESPONSIBILITY FOR THE REPAIR OR REPLACEMENT OF ANY UTILITY, STRUCTURE, LANDSCAPING, ETC., DAMAGED OR DESTROYED BY THE CONTRACTOR DURING MOBILIZATION OR CONSTRUCTION SHALL BE BORNE SOLELY BY THE CONTRACTOR, WITH NO EXPENSE BEING CHARGED TO THE ENGINEER OR OWNER. PRIOR TO ACCEPTANCE OF THIS REPAIR OR REPLACEMENT, THE CONTRACTOR SHALL PRESENT THE OWNER WITH A "SIGNOFF LETTER", SIGNED BY A RESPONSIBLE OFFICIAL OF THE OWNER OF THE DAMAGED UTILITY STATING THAT THE REPAIR OR REPLACEMENT IS ACCEPTABLE.

CONSTRUCTION STAKING

1. CONSTRUCTION STAKING SERVICES WILL BE PROVIDED BY THE OWNER. STAKE POINTS WILL BE STAKED ONE TIME WHEN REQUESTED BY THE CONTRACTOR. THE SAME STAKE POINTS REQUESTED BY THE CONTRACTOR A SECOND TIME WILL BE PAID FOR BY THE CONTRACTOR.

EROSION CONTROL NOTES

1. UNLESS OTHERWISE SPECIFIED, ALL EROSION AND SEDIMENT CONTROL MEASURES AND THEIR MAINTENANCE, CLEARING AND REMOVAL SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION.

2. THIS WORK SHALL CONFORM TO THE APPLICABLE STANDARDS FROM THE ILLINOIS URBAN MANUAL, THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATION, CURRENT EDITION, THE PROJECT SPECIFICATIONS, AND THE APPROPRIATE DETAILS.

3. THE CONTRACTOR SHALL IMPLEMENT THE EROSION AND SEDIMENT CONTROL MEASURES AS INDICATED ON THESE EROSION CONTROL PLANS.

4. THE CONTROLS SHALL BE INSTALLED AS DETAILED AND WHERE INDICATED ON THE EROSION CONTROL PLAN SHEETS AND AS DIRECTED BY THE INSPECTOR.

5. SITE ACTIVITIES SHOULD ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE PRACTICABLE.

6. DISTURBED PORTIONS OF THE SITE SHALL BE STABILIZED (TEMPORARILY OR PERMANENTLY SEEDED, MULCHED, SODDED OR PAVED) AS SOON AS PRACTICABLE, BUT IN NO CASE MORE THAN 7 CALENDAR DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

7. UNTIL SUCH TIME AS THE PROJECT SITE REACHES FINAL STABILIZATION THE CONTRACTOR SHALL BE RESPONSIBLE TO ADJUST, REPAIR, OR REPLACE, ALL VEGETATION, EROSION CONTROLS, SEDIMENT CONTROLS, AND ANY OTHER PROTECTIVE MEASURES AS REQUIRED IN ORDER TO MAINTAIN THEIR INTENDED FUNCTION IN A GOOD AND EFFECTIVE OPERATING CONDITION.

8. EXCEPT FOR FLOWS FROM FIRE FIGHTING ACTIVITIES, SOURCES OF NON-STORM WATER EXPECTED DURING THE CONSTRUCTION PROCESS THAT MAY BE COMBINED WITH STORM WATER DISCHARGES ARE:

   A. FIRE HYDRANT FLUSHING
   B. WATERS USED TO WASH VEHICLES (DETERGENTS ARE NOT TO BE USED)
   C. WATERS USED TO CONTROL DUST
   D. POTABLE WATER FROM WATER MAIN FLUSHING
   E. LANDSCAPE IRRIGATION DRAINAGE
   F. UNCONTAMINATED GROUND WATER FROM DEWATERING EXCAVATED TRENCHES
   G. PAVEMENT WASH WATERS WHERE SPILLS OR LEAKS OF TOXIC OR HAZARDOUS MATERIALS HAVE NOT OCCURRED (UNLESS ALL SPILLED MATERIAL HAS BEEN REMOVED) ALSO, DETERGENTS ARE NOT TO BE USED
   H. UNCONTAMINATED AIR CONDITIONING CONDENSATE
   I. THE ABOVE NON-STORM DISCHARGES SHALL BE DIRECTED AWAY FROM UNPROTECTED, BARE, OR OTHERWISE UNSTABILIZED SOIL. THE CONTRACTOR SHALL FURTHER IMPLEMENT APPROPRIATE POLLUTION PREVENTION MEASURES TO ENSURE THAT ANY OF THE ABOVE DISCHARGES DO NOT CAUSE EROSION OR DEGRADE THE QUALITY OF RUNOFF FROM THE CONSTRUCTION SITE.

9. THE OWNER SHALL HAVE AUTHORIZATION TO DETERMINE THE ADEQUACY OF THE CONTRACTOR'S EROSION CONTROL EFFORTS. THE OWNER SHALL HAVE FULL AUTHORITY OVER THE GENERAL CONTRACTOR AND ANY SUBCONTRACTOR TO CAUSE POLLUTANT CONTROL MEASURES TO BE REPAIRED, MODIFIED, MAINTAINED, SUPPLEMENTED, OR WHATEVER ELSE IS NECESSARY IN ORDER TO ACHIEVE EFFECTIVE POLLUTANT CONTROL OR TO SUSPEND OR LIMIT THE CONTRACTORS OPERATIONS PENDING ADEQUATE PERFORMANCE.

10. PERIMETER EROSION BARRIER TO BE CONSTRUCTED OF SILT FENCE UNLESS NOTED OTHERWISE.

11. INLET PROTECTION SHALL BE A DANDY BAG, DANDY SACK, ROCSOC, OR APPROVED EQUAL.

12. EROSION CONTROL BLANKET SHALL BE OF NORTH AMERICAN GREEN DS75 OR APPROVED EQUAL.

13. A TEMPORARY CONCRETE WASHOUT FACILITY SHALL BE CONSTRUCTED AT A LOCATION APPROVED BY THE ENGINEER. WASHOUT FACILITY SHALL BE UTILIZED FOR ALL APPLICABLE OPERATIONS.

14. STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED, TO THE DIMENSIONS AS SHOWN, AT APPROVED LOCATIONS FOR THIS PROJECT. ALL CONSTRUCTION TRAFFIC MUST UTILIZE THE STABILIZED CONSTRUCTION ENTRANCES WHEN EXITING THE SITE. ALL COST FOR EROSION CONTROL AND RESTORATION WORK ASSOCIATED WITH THE APPROVED STABILIZED CONSTRUCTION ENTRANCES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

15. TEMPORARY EROSION CONTROL MEASURES INCLUDE TEMPORARY DITCH CHECKS, PERIMETER EROSION BARRIER, INLET AND PIPE PROTECTION, TEMPORARY SEEDING, AND ANY OTHER TEMPORARY EROSION CONTROL MEASURE NEEDED TO LIMIT THE AMOUNT OF SOIL EROSION AND SEDIMENTATION DURING CONSTRUCTION.

16. AT THE COMPLETION OF THE PROJECT, ALL TEMPORARY EROSION CONTROL ITEMS SHALL BE REMOVED FROM THE SITE, AND BECOME THE PROPERTY OF THE CONTRACTOR. CONTRACTOR MUST STABILIZE ANY AREA DISTURBED BY THE REMOVAL OF EROSION CONTROL ITEMS.

17. CONTRACTOR SHALL CLEAN ANY DEBRIS TRACKED OFFSITE DAILY.

SEEDING OF DISTURBED AREAS

1. THE FINAL TOP 6" INCHES OF SOIL IN ANY DISTURBANCE AREA MUST BE A COHESIVE SOIL CAPABLE OF SUPPORTING VEGETATION.

2. FERTILIZER HAVING AN ANALYSIS OF 10-10-10 SHALL BE APPLIED AT A RATE OF 90 LBS/ACRE TO ALL DISTURBED AREAS AND INCORPORATED INTO THE SEEDBED PRIOR TO SOWING THE SEED.

3. THE CONTRACTOR SHALL SEED AND STABILIZE ALL DISTURBED AREAS ADJACENT TO IMPROVEMENTS WITH SEEDING, IDOT CLASS 1A AND NAG DS75 EROSION CONTROL BLANKET OR APPROVED EQUAL IN ACCORDANCE WITH IDOT STANDARD SPECIFICATION OR AS APPROVED BY THE ENGINEER.

4. GUARANTEE: ALL SEEDED AREAS SHALL BE MAINTAINED AND MOWED FOR AT LEAST 30 DAYS AFTER GERMINATION. SCATTERED BARE SPOTS NO LARGER THAN TWO SQUARE FOOT WILL BE ALLOWED UP TO A MAXIMUM OF 5% OF ANY SEEDED AREA INCLUDING 30-DAY MAINTENANCE, MOWING AND WATERING AS NECESSARY.

5. THIS WORK SHALL CONFORM TO THE APPLICABLE STANDARDS FROM THE ILLINOIS URBAN MANUAL, THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATION SECTIONS, CURRENT EDITION, THE PROJECT SPECIFICATIONS, AND THE APPROPRIATE DETAILS.

6. RESTORATION – THE CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED DURING CONSTRUCTION OF THE IMPROVEMENTS AND RELATED APPURTENANCES OR AS PART OF ANY OF THEIR ACTIVITIES TO A CONDITION EQUAL TO OR BETTER THAN THE ORIGINAL CONDITION.

STORM SEWER

1. CONTRACTOR SHALL FURNISH ALL PIPE BEDDING. PIPE BEDDING MATERIAL SHALL BE AS SHOWN IN THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", CURRENT EDITION. (COST SHALL BE INCIDENTAL TO THE PROJECT).

2. CONTRACTOR SHALL FIELD VERIFY ALL EXISTING STORM SEWER ELEVATIONS THAT PROJECT CONNECTS TO.

SANITARY SEWER

1. THE CONTRACTOR SHALL NOTIFY THE FOUR RIVERS SANITATION AUTHORITY (FRSA) SURVEYOR AND FIELD OPERATIONS MANAGER, BEN CHRISTIANSEN C (815) 209-7952 A MINIMUM OF 48 HOURS PRIOR TO PERFORMING ANY SANITARY WORK. FRSA WILL PROVIDE AN INSPECTOR AT NO COST TO THE VILLAGE.

2. NEW FRAMES AND LIDS WILL BE PROVIDED BY FRSA. IF THE SANITARY MANHOLES TO BE ADJUSTED DO NO HAVE STANDARD FRAMES AND LIDS OR IF THE FRAMES AND LIDS ARE IN POOR CONDITION, THE FOUR RIVERS SANITATION AUTHORITY COLLECTION SYSTEMS DEPARTMENT SHALL BE CONTACTED PRIOR TO LIDS. CONTACT BRIAN MARKGRAF C (815) 543-3470 OR NOAH SMITH C (815) 721-1508 TO COORDINATE THE EXCHANGE OF FRAMES AND LIDS. THE CONTRACTOR IS RESPONSIBLE FOR EXCHANGING THE OLD FRAMES AND LIDS AND INSTALLING THE NEW ONES.

UTILITIES

1. UTILITIES SHOWN ON THE PLANS ARE FOR ILLUSTRATIVE PURPOSES ONLY AND NO GUARANTEE OF THEIR ACCURACY IS MADE OR INFERRED. THE LOCATION OF EXISTING UTILITIES AS SHOWN ON THE DRAWINGS REPRESENT DATA RECEIVED FROM VARIOUS SOURCES. IT IS NOT GUARANTEED TO BE CORRECT OR ALL-INCLUSIVE. THE CONTRACTOR SHALL CONDUCT HIS OWN INVESTIGATION INTO THE LOCATION, SIZE, DEPTH AND NATURE OF ANY AND ALL EXISTING UTILITIES THAT MAY INTERFERE WITH THE WORK UNDER THIS CONTRACT. ANY EXISTING UTILITIES THAT ARE TO REMAIN IN SERVICE SHALL BE FULLY PROTECTED BY THE CONTRACTOR AND ANY DAMAGE CAUSED BY THE CONSTRUCTION OPERATIONS SHALL BE IMMEDIATELY REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE OWNER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ANY AND ALL UTILITY COMPANIES REGARDING ADJUSTMENTS NECESSARY. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE AND CONSIDERED INCIDENTAL TO THE PROJECT COST. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND, OVERHEAD, OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER OR THE OWNER OR REPLACED. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

2. THE CONTRACTOR MUST VERIFY AND LOCATE ALL EXISTING UTILITIES ON OR ADJACENT TO THE SITE. PRIOR TO BEGINNING CONSTRUCTION ACTIVITIES, CONTACT J.U.L.I.E. AT 1-800-892-0123 (OR 811) FOR EXACT FIELD LOCATION OF UTILITIES. DAMAGE, AND THE COST THEREOF, TO ANY AND ALL UTILITIES SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. ANY AND ALL EXISTING UTILITIES SHOWN HEREON ARE APPROXIMATE. THE ENGINEER AND SURVEYOR ASSUMES NO RESPONSIBILITY FOR THE LOCATION OF THE EXISTING UTILITIES SHOWN HEREON.

3. IF THERE ARE ANY UTILITIES WHICH ARE NOT MEMBERS OF THE J.U.L.I.E. SYSTEM, THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR DETERMINING THIS AND REQUESTING SAID UTILITIES TO FIELD VERIFY AND MARK PERTINENT UTILITY LOCATIONS.

4. THE UTILITY LOCATIONS, DEPTHS, ETC. SHOWN ON THESE PLANS ARE APPROXIMATE ONLY, AND SHALL BE VERIFIED BY THE CONTRACTOR WITH ALL AFFECTED UTILITY COMPANIES PRIOR TO INITIATING CONSTRUCTION OPERATIONS; THE ENGINEER AND OWNER ASSUME NO RESPONSIBILITY FOR THE ADEQUACY, SUFFICIENCY OR EXACTNESS OF THESE UTILITY REPRESENTATIONS.

5. THE CONTRACTOR SHALL CONTACT THE NECESSARY UTILITY COMPANIES FOR ANY UTILITY RELOCATIONS. THE CONTRACTOR SHALL PAY FOR ALL COSTS ASSOCIATED WITH RELOCATION OF UTILITIES ON OR ADJACENT TO THE SUBJECT PROPERTY OR WITHIN THE ROAD RIGHT-OF-WAY.

6. TRENCH BACKFILL SHALL BE USED IN LOCATIONS WHERE THERE IS AN EXISTING OR PROPOSED PERMANENT SURFACE.

7. ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION OR HAVE THE POTENTIAL FOR CREATING FUTURE PROBLEMS SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE PROJECT AT AN APPROVED LOCATION OBTAINED BY THE CONTRACTOR, ACCORDING TO THE "STANDARD SPECIFICATIONS FOR WATER & SEWER CONSTRUCTION IN ILLINOIS", CURRENT EDITION, AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO EARTH EXCAVATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

8. ANY AND ALL FIELD TILES AND OR STORM SEWERS DAMAGED OR ENCOUNTERED DURING THE CONSTRUCTION ACTIVITIES SHALL BE REPAIRED, REPLACED AND/OR CONNECTED IMMEDIATELY BY THE CONTRACTOR. COST FOR SAID REPAIRS, REPLACEMENT, AND/OR CONNECTION SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

REV. NO.	DESCRIPTION	DATE

DRAWING: GENERAL NOTES

SET TYPE: PRELIMINARY

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JOB NUMBER: 25-694

SHEET NUMBER: 2 of 24

PLOT DATE: 5/15/25

TRAFFIC CONTROL

1. THE CONTRACTOR SHALL PROVIDE, INSTALL AND MAINTAIN ALL TRAFFIC CONTROL ITEMS NECESSARY FOR THE CONSTRUCTION OF ITEMS WITH IN THE ROAD RIGHT-OF-WAY. ALL WORK PERFORMED SHALL HAVE TRAFFIC CONTROL IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" AND OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", CURRENT EDITION.

2. ALL TRAFFIC CONTROL DEVICES USED FOR THE MAINTENANCE OF TRAFFIC SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS NECESSARY THROUGHOUT THE DURATION OF THE CONTRACT. ALL SIGNS SHALL BE FURNISHED, INSTALLED AND MAINTAINED BY THE CONTRACTOR. PAYMENT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

3. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN CONDITIONS MAY REQUIRE THE ENGINEER TO MODIFY THE LOCATION OF THE TRAFFIC CONTROL DEVICES. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND WITHIN 30 MINUTES FROM THE TIME OF NOTIFICATION BY THE ENGINEER TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION, IMPROVEMENT OR MODIFICATION OF THE MAINTENANCE OF TRAFFIC CONTROL DEVICES. DURING CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS TO PROTECT ADJACENT TRAFFIC LANES OPEN TO TRAFFIC FROM DEBRIS BEING BLOWN OR OTHERWISE REMOVED FROM THE CONSTRUCTION AREAS. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR KEEPING DEBRIS OFF THE ADJACENT TRAVELED LANE SURFACE. COST INCIDENTAL TO THE PROJECT.

4. THE CONTRACTOR SHALL SUBMIT MAINTENANCE OF TRAFFIC AND STAGING OF CONSTRUCTION PLANS FOR APPROVAL BY THE ENGINEER PRIOR TO COMMENCING WORK.

5. THE CONTRACTOR SHALL PERFORM THE WORK UNDER STAGE CONSTRUCTION IN THE EVENT THAT THE CONTRACTOR WILL NEED TO CLOSE PUBLIC ROADS, CONTRACTOR SHALL SUBMIT PROPOSED DETOUR ROUTE AND ASSOCIATED SIGNAGE TO THE ENGINEER PRIOR TO COMMENCING WORK.

6. TRAFFIC CONTROL DEVICES, STREET NAME SIGNS, AND PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH VILLEGA OF ROSCOE ORDINANCES AND THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES". LOCATIONS OF SIGNS AND MARKINGS SHALL BE SPECIFIED BY THE PLANS, AND/OR AS DIRECTED BY THE ENGINEER.

7. PROVIDE TO THE ENGINEER AND THE OWNER THE NAME AND PHONE NUMBER OF INDIVIDUALS RESPONSIBLE FOR MAINTAINING TRAFFIC CONTROL MEASURES DURING CONSTRUCTION. THIS INDIVIDUAL SHALL BE AVAILABLE TO CORRECT TRAFFIC CONTROL PROBLEMS 24 HOURS PER DAY.

8. THE CONTRACTOR SHALL NOTIFY THE POST OFFICE, POLICE DEPARTMENT, FIRE DEPARTMENT, 911 DISPATCH CENTER, ILLINOIS DEPARTMENT OF TRANSPORTATION, STATE POLICE, APPROPRIATE SCHOOL DISTRICT AND THE LOCAL AGENCY A MINIMUM OF 5 DAYS PRIOR TO CLOSING ANY PORTION OF THE STREET OR ALLEY.

SUBGRADES, SUBBASES, AND BASE COURSES

1. THE CONTRACTOR WILL BE REQUIRED TO SUBSTANTIATE BASE COURSE THICKNESSES AND FINISH PAVEMENT THICKNESSES. THE ENGINEER SHALL INSPECT BASE COURSE COREOUT PRIOR TO PLACING BASE COURSE TO ENSURE REQUIRED BASE COURSE DEPTH IS PRESENT. IN ADDITION, THE ENGINEER AND/OR THE CITY ENGINEER SHALL WITNESS THE PLACEMENT OF BITUMINOUS BINDER AND SURFACE COURSE. CORE DRILLING MAY BE REQUIRED TO DEMONSTRATE THAT BASE COURSE AND PAVEMENT THICKNESSES CONFORM TO THE SPECIFICATIONS. PRIOR TO PLACING BASE COURSE MATERIAL, THE CONTRACTOR SHALL TEST ROLL THE SUBGRADE, IN THE PRESENCE OF THE ENGINEER OR HIS AGENT TO DEMONSTRATE THAT SAID SUBGRADE IS READY FOR BASE. PRIOR TO PLACEMENT OF THE BITUMINOUS SURFACE, THE SAME VERIFICATION PROCEDURE SHALL BE PERFORMED ON THE BASE COURSE MATERIAL. THE CONTRACTOR SHALL NOTIFY THE ENGINEER AT LEAST 48 HOURS PRIOR TO PERFORMING ANY OF THE REQUIRED TESTS SO THAT A REPRESENTATIVE MAY BE PRESENT.

2. PRIOR TO ANY EMBANKMENT OR ROAD BASE BEING PLACED, SHOULD IT BE DETERMINED BY THE ENGINEER THAT THE SUBGRADE MATERIAL IS UNSUITABLE ON WHICH TO CONSTRUCT THE ROADWAY STRUCTURE, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING THE UNSUITABLE MATERIAL TO THE SATISFACTION OF THE ENGINEER AND REPLACING SAME WITH STABILIZING SUBBASE CONSISTING OF SUBBASE GRANULAR MATERIAL, TYPE B IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", CURRENT EDITION. TO HELP MINIMIZE THE AMOUNT OF SUBBASE MATERIAL INSTALLED FOR GROUND STABILIZATION, GEOTECHNICAL FABRIC MAY BE INSTALLED AS APPROVED BY THE ENGINEER. FABRIC SHALL BE INSTALLED IN ACCORDANCE WITH ARTICLE 210 OF THE IDOT STANDARD SPECIFICATIONS. THE COARSE AGGREGATE SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR SUBBASE GRANULAR MATERIAL, TYPE B. THE EXCAVATION AND DISPOSAL OF THE UNSUITABLE MATERIAL SHALL BE CONSIDERED INCIDENTAL TO SUBBASE GRANULAR MATERIAL, TYPE B. STABILIZING FABRIC SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR GEOTECHNICAL FABRIC FOR GROUND STABILIZATION.

EXCAVATION/EARTHWORK

1. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATION NEAR ANY AND ALL EXISTING ITEMS WHICH ARE NOT INDICATED TO BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED AT NO ADDITIONAL EXPENSE TO THE OWNER.

2. ROCK IS NOT ANTICIPATED TO BE ENCOUNTERED.

3. ALL EXCAVATIONS FOR STRUCTURES AND PIPE SHALL BE KEPT DEWATERED DURING CONSTRUCTION UNTIL BACKFILL IS IN PLACE. DURING DEWATERING OPERATIONS, WATER SHALL BE PUMPED INTO SEDIMENT BASINS OR SILT TRAPS. (COST INCIDENTAL)

4. EARTH EXCAVATION SHALL CONFORM TO SECTION 202 OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", CURRENT EDITION. THIS WORK SHALL INCLUDE THE EXCAVATION OF ALL MATERIALS TO DESIGN SUBGRADE ELEVATIONS INDICATED IN THE PLANS.

5. SHEETING AND SHORING SHALL BE CONSIDERED INCIDENTAL TO CONTRACT IF REQUIRED.

6. WHENEVER THE CONTRACTOR WORKS NEAR EXISTING FACILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS DURING TRENCHING OPERATIONS, HE WILL BE REQUIRED TO HAND TRENCH IN THAT AREA IN ORDER NOT TO DAMAGE THESE FACILITIES. PUSH HOLES AND SEARCH HOLES THAT ARE DUG BY THE CONTRACTOR SHALL BE BACKFILLED BY TAMPING THE EXCAVATED MATERIAL BACK IN PLACE TO KEEP SETTLEMENT TO A MINIMUM. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

7. EMBANKMENT WORK SHALL CONSIST OF THE CONSTRUCTION OF EMBANKMENTS BY DEPOSITING, PLACING AND COMPACTING EARTH, STONE, GRAVEL OR OTHER MATERIALS OF ACCEPTABLE QUALITY ABOVE THE NATURAL GROUND OR OTHER SURFACE IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", CURRENT EDITION.

8. IF SUFFICIENT TOPSOIL IS NOT PRESENT, THE CONTRACTOR SHALL SPREAD FURNISHED TOPSOIL SO AS TO MEET THE REQUIREMENTS OF THE CONTRACT. FURNISHED TOPSOIL SHALL ONLY BE USED WITH APPROVAL BY THE ENGINEER. THIS FURNISHED TOPSOIL SHALL BE PAID FOR AS FURNISHED TOPSOIL IN PLACE, DEPTH SPECIFIED.

SUMMARY OF QUANTITIES

No.	Description	Quantity	Unit
1	EARTH EXCAVATION	85.0	CY
2	PAVEMENT REMOVAL, VARIABLE DEPTH	17,080.0	SY
3	DRIVEWAY PAVEMENT REMOVAL	650.0	SY
4	TREE REMOVAL	1.0	LS
5	STORM SEWER PIPE REMOVAL	70.0	FT
6	STORM SEWER MANHOLE TO BE REMOVED, COMPLETE	1.0	EA
7	SHAPING AND GRADING ROADWAY	17,080	SY
8	AGREGATE BASE COURSE, TYPE B, VARIES	160.0	SY
9	HOT-MIX ASPHALT, SURFACE COURSE, IL-9.5, MIX "D", N50, 3" (SINGLE LIFT)	3,250.0	TON
10	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	650.0	SY
11	BITUMINOUS MATERIALS (PRIME COAT)	38,450.0	LB
12	MANHOLE TO BE ADJUSTED, COMPLETE	14.0	EA
13	INLETS TO BE ADJUSTED WITH NEW 2' X 3' FRAME AND GRATE	3.0	EA
14	STORM SEWER, CLASS B, TYPE 1, 12" CMP	70.0	FT
15	METAL FLARED END SECTIONS 12"	1.0	EA
16	METAL FLARED END SECTIONS 15"	3.0	EA
17	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	100.0	FT
18	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	135.0	FT
19	RESTORATION	1.0	LS
20	EROSION AND SEDIMENT CONTROL	1.0	LS
21	TRAFFIC CONTROL & PROTECTION	1.0	LS

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10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

REV. NO.	DESCRIPTION	DATE

DRAWING: GENERAL NOTES

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JOB NUMBER: 25-694

SHEET NUMBER: 3 of 24

PLOT DATE: 5/15/25

ABBREVIATIONS

<	ANGLE
ABC	AGGREGATE BASE COURSE
AC	ACRE(S)
ACI	AMERICAN CONCRETE INSTITUTE
AGR	AGGREGATE
AISC	AMERICAN INSTITUTE OF STEEL CONSTRUCTION
ALT	ALTERNATE
ARCH	ARCHITECT
ASPH	ASPHALT
ASTM	AMERICAN SOCIETY OF TESTING AND MATERIALS
B	BALL VALVE
BFP	BACKFLOW PREVENTER
BIT	BITUMINOUS
BLDG	BUILDING
BLK	BLOCKING
BM	BENCHMARK
BOT	BOTTOM
BSMT	BASEMENT
BV	BUTTERFLY VALVE
B-B	BACK-TO-BACK OF CURB DIMENSION
CL or ℄	CENTERLINE
C TO C	CENTER TO CENTER
C & G	CURB AND GUTTER
CF	CUBIC FEET
CHD	CHORD LENGTH
CI	CAST IRON PIPE
CHK	CHECK VALVE
CLR	CLEAR
CMP	CORRUGATED METAL PIPE
CMU	CONCRETE MASONRY UNIT
CTY	COUNTY
CONC	CONCRETE
CONT	CONTINUOUS
C-B	CENTERLINE TO BACK OF CURB DIMENSION
COORD	COORDINATE
CU	COPPER PIPING
CTRS	CENTERS
CY	CUBIC YARDS
CS	CORPORATION STOP
D	DEGREE OF CURVE
DEP	DEPRESSED
DET	DETAIL
DIAG	DIAGONAL
DIM	DIMENSION
DI	DUCTILE IRON PIPE
DN	DOWN
DNSTR	DOWNSTREAM
DP	DRAINAGE PIPE/STORM PIPE
DWG	DRAWING
E	EAST
EJ	EXPANSION JOINT
EL, ELEV	ELEVATION
EP	EDGE OF PAVEMENT
EQUIP	EQUIPMENT
EQUIV	EQUIVALENT
EW	EACH WAY
EXP	EXPANSION
EX, EXIST	EXISTING
EXT	EXTERIOR
E =	EXTERNAL DISTANCE
FD	FLOOR DRAIN
FDN	FOUNDATION
FE	FIELD ENTRANCE
FF	FINISH FLOOR
FIL	FILLET
FIN	FINISH
FL	FLOW LINE
FLR	FLOOR
FM	FORCE MAIN
FND	FOUND
FRMG	FRAMING
FTG	FOOTING
F-F	FACE TO FACE
GA	GAUGE
GI	GALVANIZED IRON PIPE
GRD	GRADE
GRS	GRATING SUPPORT
GRT	GROUT
GV	GAS VALVE
GYP	GYPSUM
HSE	HOUSE
HC	HORIZONTAL CURVE
HMA	HOT MIX ASPHALT
HNGR	HANGER
HORIZ	HORIZONTAL
H.P.	HIGH POINT
HW	HOT WATER
HWH	HOT WATER HEATER
Δ =	CENTRAL ANGLE
I	MOMENT OF INERTIA
ID	INSIDE DIAMETER
INT	INTERIOR
INV	INVERT ELEVATION; BASED ON BENCH MARK DATUM
IP	IRON PIPE
JST	JOIST
L	LENGTH OF CURVE
LAT	LATERAL
LAV	LAVATORY
LF	LINEAL FEET
L.P.	LOW POINT
LT	LEFT OF SURVEY BASE LINE
MAX	MAXIMUM
ME	MATCH EXISTING
MH	MANHOLE
MIN	MINIMUM
MJ	MECHANICAL JOINT
MTL	METAL
N	NORTH
No. OR #	NUMBER
NOM	NOMINAL
NTS	NOT TO SCALE
OC	ON CENTER
OD	OUTSIDE DIAMETER
OO	OUTSIDE TO OUTSIDE
OPNG	OPENING
OPP	OPPOSITE
PC	POINT OF CURVATURE
PCC	PORTLAND CEMENT CONCRETE
PCF	POUNDS PER CUBIC FOOT
PDP	PERFORATED DRAIN PIPE
PE	POLYETHYLENE PIPE
PI	POINT OF INTERSECTION
PL	PLATE
PLG	PLUG VALVE
PLP	POLYPROPYLENE PIPE
PLYWD	PLYWOOD
PM	PRINCIPAL MERIDIAN
PR	PRESSURE REGULATORS
PRC	POINT OF REVERSE CURVATURE
PRESS	PRESSURE
PR, PROP	PROPOSED
PRV	PRESSURE REDUCING VALVE
PSF	POUNDS PER SQUARE FOOT
PSI	POUNDS PER SQUARE INCH
PSL	PIPE SLEEVE
PT	POINT OF TANGENCY
PLG	PLUG VALVE
PVC	POLYVINYL CHLORIDE (PLASTIC) PIPE
R	RADIUS
RDCR	REDUCER
RCCP	REINFORCED CONCRETE CYLINDER PIPE
RCP	REINFORCED CONCRETE PIPE
RD	ROOF DRAIN
REINF	REINFORCING
REQD	REQUIRED
ROW	RIGHT OF WAY
RFTR	RAFTER
RND	ROUND
RR	RAILROAD
RRSP	RAILROAD SPIKE
RT	RIGHT
R&R	REMOVE AND REPLACE
S	SOUTH
SB	STREAM BED
SCHED	SCHEDULE
SEC	SECTION
SF	SQUARE FEET
SHR	SHOWER
SHT	SHEET
SHTG	SHEATHING
SP	SANITARY PIPE
SPA	SPACING OR SPACES
SPEC	SPECIFICATION
SQ	SQUARE
SS	SANITARY SERVICE
STA	STATION
STD	STANDARD
STL	STEEL
STRUCT	STRUCTURAL
SW	SIDEWALK
SY	SQUARE YARDS
SYM	SYMMETRICAL
TAN	TANGENT LENGTH
TBC	TOP BACK OF CURB
TBM	TEMPORARY BENCH MARK; BASED ON BENCHMARK DATUM
TD	TILE DRAIN
THK	THICK
TR	TREAD
TY	TYPE
TYP	TYPICAL
U.O.N.	UNLESS OTHERWISE NOTED
UP	UTILITY POLE
UPSTR	UPSTREAM
UR	URINAL
USGS	US GEOLOGICAL SURVEY
VC	VERTICAL CURVE
VCP	VITRIFIED CLAY PIPE
VERT	VERTICAL
VOL	VOLUME
VPC	VERTICAL POINT OF CURVATURE
VPI	VERTICAL POINT OF INTERSECTION
VPRC	VERTICAL POINT OF REVERSE CURVATURE
VPT	VERTICAL POINT OF TANGENCY
W	WEST
WC	WATER CLOSET
WF	WIDE FLANGE
WM	WATER MAIN
WMQ	WATER MAIN QUALITY
WV	WATER VALVE
WGT	WEIGHT
WP	WEATHER PROOF
WS	WATER SERVICE
WWF	WELDED WIRE FABRIC
W/	WITH
W/O	WITHOUT
XP	EXPLOSION PROOF

HATCH PATTERNS

- EARTH – FILL
- EARTH – UNDISTURBED
- ROCK (GEOLOGICAL)
- STONE OR RIP RAP
- GRAVEL
- CONCRETE
- CONCRETE BLOCK
- CMU
- ASPHALT PAVEMENT
- BRICK
- STEEL
- INSULATION (LOOSE/ BATT)
- INSULATION (RIGID)
- WOOD (ROUGH)
- WOOD (BLOCKING)
- WOOD (FINISH)
- DETECTABLE WARNING

SYMBOLS

CIVIL

EXISTING		PROPOSED
EXISTING R.O.W.	RIGHT-OF-WAY LINE	PROPOSED R.O.W.
	PROPERTY LINE	
	CENTERLINE	
	SETBACK LINE	
	EASEMENT LINE	
	SECTION LINE	
N 1000.00 E 1000.00	SECTION CORNER / COORDINATE POINT ON GRID SYSTEM	
FND	FOUND OR SET PROPERTY PIN	SET
	RIGHT-OF-WAY MARKER	
	BENCHMARK	
600	CONTOUR LINE	600
000.00 FG	SPOT ELEVATION (AT●)	000.00 FG
X X	FENCE LINE	X X
	SILT FENCE LINE	
	CURB AND GUTTER	
	TIP OUT CURB AND GUTTER	
	SAWCUT, LIMITS OF PAVEMENT REMOVAL & REPLACEMENT	
X"	DECIDUOUS TREE W/ SIZE	X"
X"	CONIFEROUS TREE W/ SIZE	X"
X"	TREE STUMP	
	HEDGEROW	
	BUSH OR SHRUB	
	TREE LINE	
CL	CONSTRUCTION LIMIT LINE	CL
	SIGN (MULTIPLE POST, SINGLE POST, STREET SIGN)	
	SIGN (PYLON)	
	GUARD RAIL	
	RAILROAD TRACKS	
	BUILDING	
	MAILBOX	
	FLAGPOLE	
	BOLLARD	
AC	AIR CONDITIONER	

MISC

EXISTING		PROPOSED
S.B. #XX	SOIL BORING LOCATION AND NUMBER	S.B. #XX
MW #XX	MONITORING WELL	MW #XX
	REVISION NUMBER	
	OUTLINE OF DETAILED AREA	
	SECTION NUMBER / SHEET WHERE SHOWN	

SANITARY SEWER

EXISTING		PROPOSED
SAN	SANITARY SEWER	SAN
SSV SSV	SANITARY SEWER SERVICE	SSV SSV
FM	SANITARY SEWER FORCE MAIN	FM
	SANITARY CLEANOUT	CO
S	SANITARY MANHOLE	
	WYE FITTING	

WATER

EXISTING		PROPOSED
WSV WSV	WATER SERVICE	WSV WSV
W W	WATER PIPE	W W
	FIRE HYDRANT	
	YARD HYDRANT	
	WATER VALVE WITH BOX	
	CURB STOP W/CURB BOX	
	REDUCER	
W	WATER VALVE VAULT	W
	11.25° BEND	
	22.50° BEND	
	45° BEND	
	90° BEND	
	TEE	
	CAP	
M	WATER METER	M
	SPRINKLER HEAD	
	TRACER WIRE BOX	

STORM SEWER

EXISTING		PROPOSED
ST ST	STORM SEWER	ST ST
DT DT	DRAIN TILE	DT DT
	DITCH LINE (PAVED)	
	DITCH LINE (UNPAVED)	
D	STORM MANHOLE	
	CATCH BASIN	
	STORM SEWER INLET	
	STORM SEWER INLET – BEHIND CURB	
	DOWNSPOUT	
X" TYPE	CULVERT AND SIZE	X" TYPE
	RCCP OR RCP EQRS (RCAP) END SECTION	
	METAL OR HDPE END SECTION	
	FLOW DIRECTION	

EROSION CONTROL

EXISTING		PROPOSED
	EROSION CONTROL BLANKET	
	TEMPORARY AND PERMANENT SEEDING AREA	
	UNDISTURBED AREA	
	STABILIZED CONSTRUCTION ENTRANCE	
	SILT FENCE	
	INLET PROTECTION	
	TEMPORARY SEDIMENT TRAP	
	CULVERT INLET PROTECTION	
	ROCK OUTLET PROTECTION	
	ROCK CHECK DAM – COURSE AGGREGATE	
	ROCK CHECK DAM – RIP RAP	
	DITCH CHECK	

UTILITY

EXISTING		PROPOSED
FO FO	FIBER OPTIC LINE	FO FO
TV TV	UNDERGROUND TV CABLE	TV TV
TV	CABLE TV RISER PEDESTAL	TV
OU	OVERHEAD UTILITY	OU
UE UE	UNDERGROUND ELECTRIC	UE UE
E	ELECTRIC RISER PEDESTAL	E
(E)	ELECTRIC MANHOLE	(E)
T T	UNDERGROUND TELEPHONE	T T
T	TELEPHONE RISER PEDESTAL	T
(T)	TELEPHONE MANHOLE	(T)
	UTILITY POLE	
	UTILITY POLE W/ METER	
	UTILITY POLE W/ TRANSFORMER	
	UTILITY POLE W/ LIGHT	
	UTILITY POLE WITH GUY WIRE AND ANCHOR	
	LIGHT (MAST MOUNTED)	
	LIGHT POLE (SINGLE FIXTURE)	
	YARD LIGHT	
G G	GAS MAIN	G G
M	GAS METER	M
	GAS VALVE	
G	GAS STRUCTURE	G
XFM	TRANSFORMER	
XEN	GENERATOR	

TRAFFIC RELATED

EXISTING		PROPOSED
	CONTROLLER	
	MAST ARM ASSEMBLY AND POLE	
	SIGNAL HEAD AND POST	
	SIGNAL HEAD	
	PEDESTRIAN HEAD	
	PEDESTRIAN PUSH-BUTTON	
	HAND HOLE	
	DOUBLE HAND HOLE	
	HAND HOLE OR JUNCTION BOX	
	HEAVY-DUTY HAND HOLE	
(5' – 2")	EXISTING CONDUIT (LENGTH AND SIZE) PROP GALVANIZED STEEL OR PVC CONDUIT UPPER NUMERAL INDICATES LENGTH "T" INDICATES CONDUIT IN TRENCH "P" INDICATED CONDUIT PUSHED LOWER NUMERAL INDICATES SIZE AND TYPE	5' T 2" GS-PVC
	LUMINAIRE	
	ARROW – THROUGH, TURN LEFT	
	ARROW – THROUGH	
	ARROW – TURN LEFT	
	ARROW – TURN RIGHT	
ONLY	ONE DIRECTION TURN ONLY	ONLY
	HANDICAPPED PARKING STALL	
	TRAFFIC DETECTOR LOOP	
	TRAFFIC CONTROL BOX	

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ILLINOIS DESIGN FIRM NO. 184-003525

ILLINOIS
IOWA
WISCONSIN

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OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

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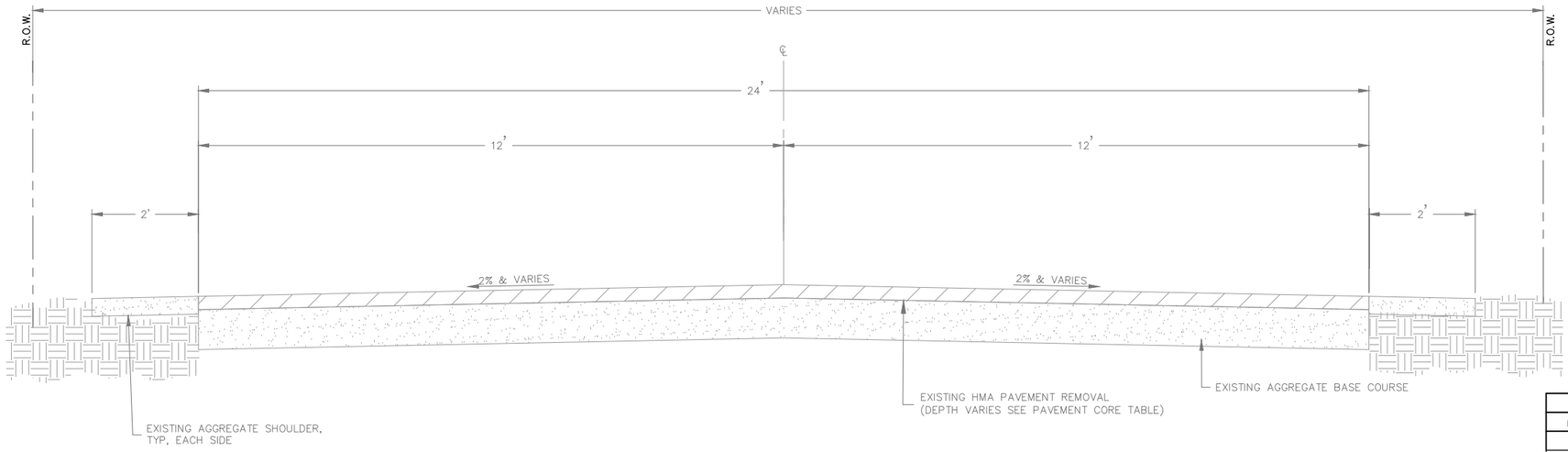
JOB NUMBER: 25-694

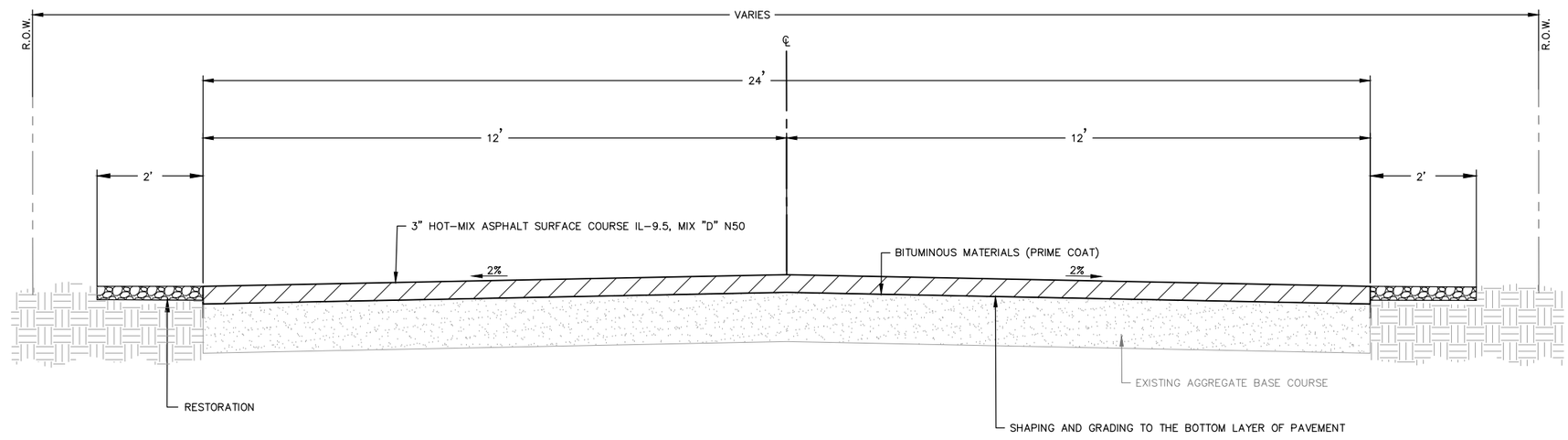
SHEET NUMBER: 4 of 24



EXISTING HODGES RUN TYPICAL SECTION

STA 100+50 TO STA 113+50
N.T.S.

PAVEMENT CORE TABLE			
NO.	1st Lift Thickness (in)	2nd Lift Thickness (in)	Aggregate Thickness (in)
1	1.5	N/A	6"
2	2.0	N/A	10.5"
3	1.5	0.75	8.5"
4	3.5	1.5	6.5"
5	2.0	N/A	8"
6	1.5	N/A	9"



PROPOSED HODGES RUN TYPICAL SECTION

STA 100+50 TO STA 113+50
N.T.S.

BITUMINOUS MATERIALS (PRIME) SHALL BE APPLIED AT:

- 0.25 LBS/SF OVER AGGREGATE BASE COURSE

MIX TABLE

USE	SURFACE
AC	PG64-22
MIX	IL-95
FRIC AGG	D
VOIDS	4.0 @ N50

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DRAWING: TYPICAL SECTION

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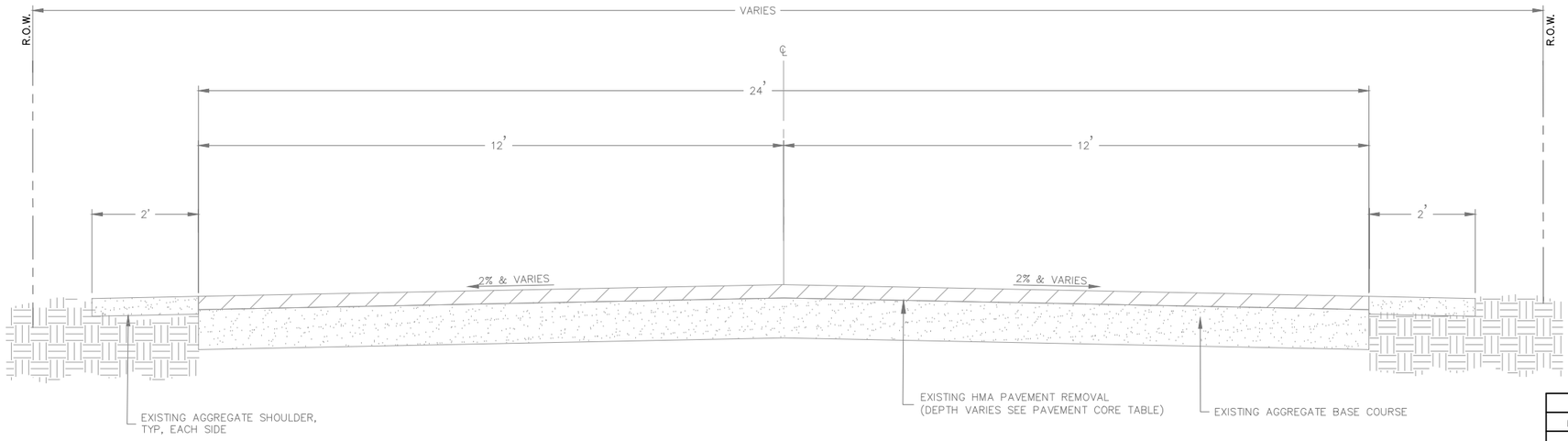
JOB NUMBER: 25-694

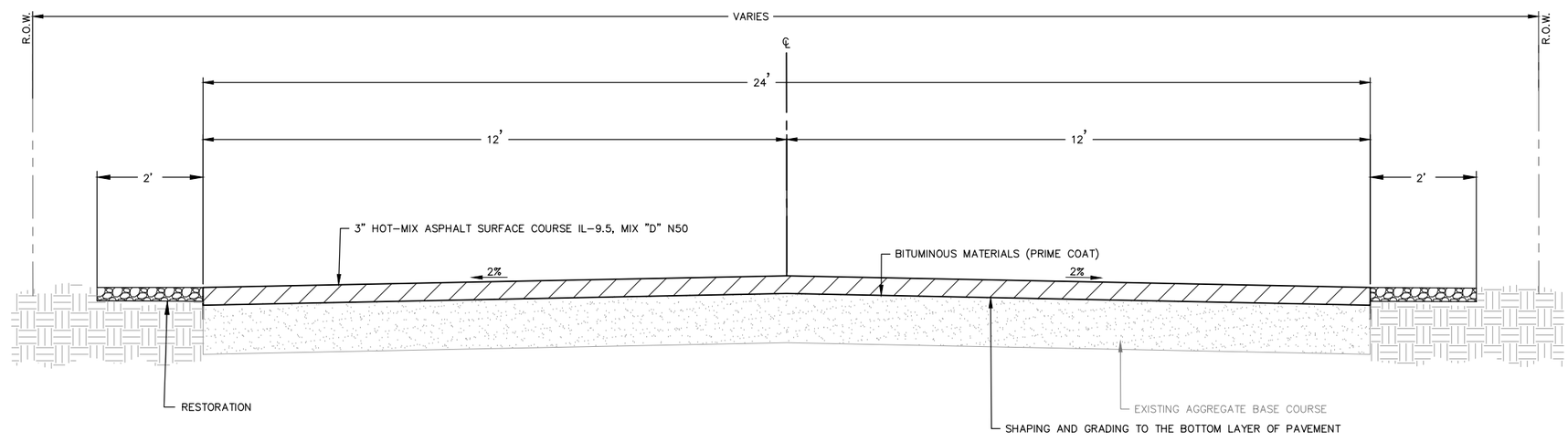
SHEET NUMBER: 5 of 24



EXISTING PRAIRIE WIND PL TYPICAL SECTION

STA 200+50 TO STA 205+50
N.T.S.

PAVEMENT CORE TABLE			
NO.	1st Lift Thickness (in)	2nd Lift Thickness (in)	Aggregate Thickness (in)
1	1.5	N/A	6"
2	2.0	N/A	10.5"
3	1.5	0.75	8.5"
4	3.5	1.5	6.5"
5	2.0	N/A	8"
6	1.5	N/A	9"



BITUMINOUS MATERIALS (PRIME) SHALL BE APPLIED AT:

- 0.25 LBS/SF OVER AGGREGATE BASE COURSE

MIX TABLE

USE	SURFACE
AC	PG64-22
MIX	IL-95
FRIC AGG	D
VOIDS	4.0 @ N50

PROPOSED PRAIRIE WIND PL TYPICAL SECTION

STA 200+50 TO STA 205+50
N.T.S.

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ILLINOIS DESIGN FIRM NO. 184-003525
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DRAWING:
TYPICAL SECTION

SET TYPE: PRELIMINARY
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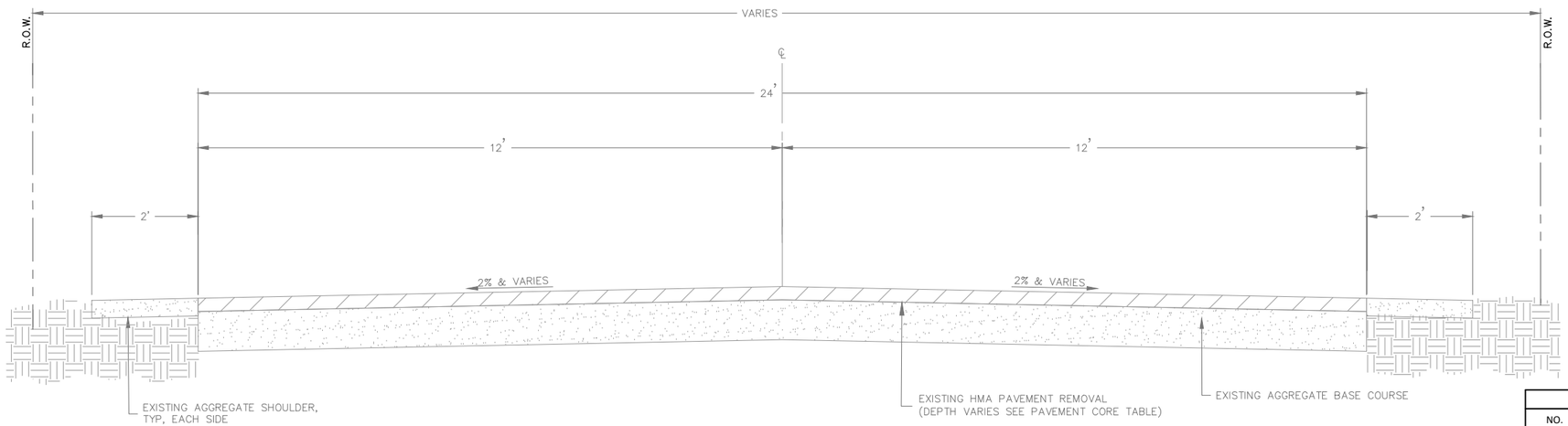
25-694

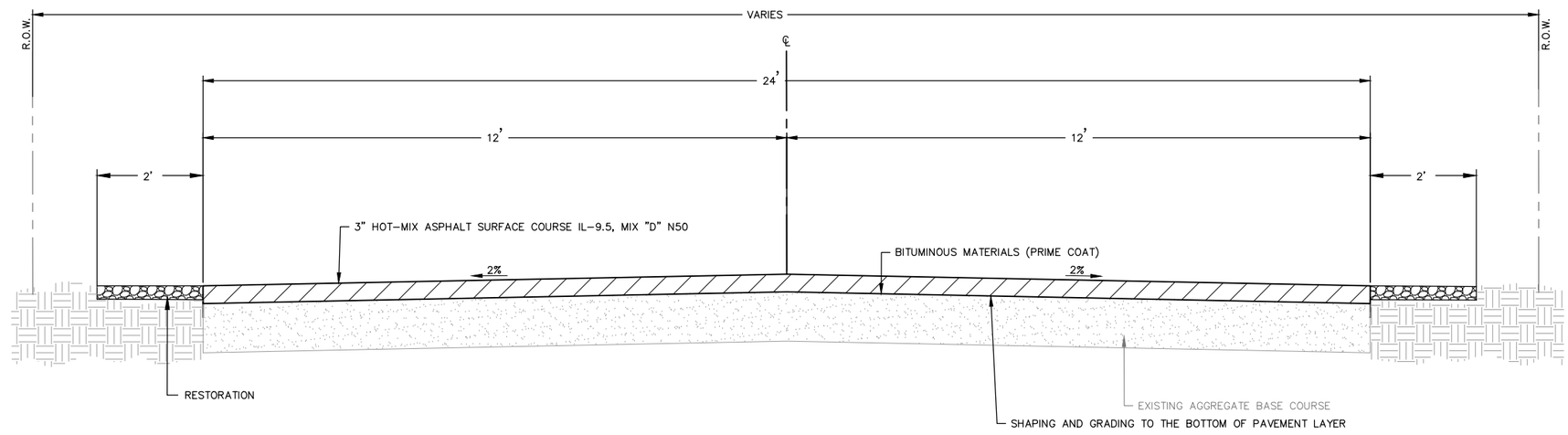
SHEET NUMBER:
6 of 24



EXISTING HAZEL CLOSE TYPICAL SECTION

STA 300+50 TO STA 301+50
N.T.S.

PAVEMENT CORE TABLE			
NO.	1st Lift Thickness (in)	2nd Lift Thickness (in)	Aggregate Thickness (in)
1	1.5	N/A	6"
2	2.0	N/A	10.5"
3	1.5	0.75	8.5"
4	3.5	1.5	6.5"
5	2.0	N/A	8"
6	1.5	N/A	9"



PROPOSED HAZEL CLOSE TYPICAL SECTION

STA 300+50 TO STA 301+50
N.T.S.

BITUMINOUS MATERIALS (PRIME) SHALL BE APPLIED AT:

- 0.25 LBS/SF OVER AGGREGATE BASE COURSE

MIX TABLE

USE	SURFACE
AC	PG64-22
MIX	IL-95
FRIC AGG	D
VOIDS	4.0 @ N50

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

PLOT DATE: 5/15/25

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

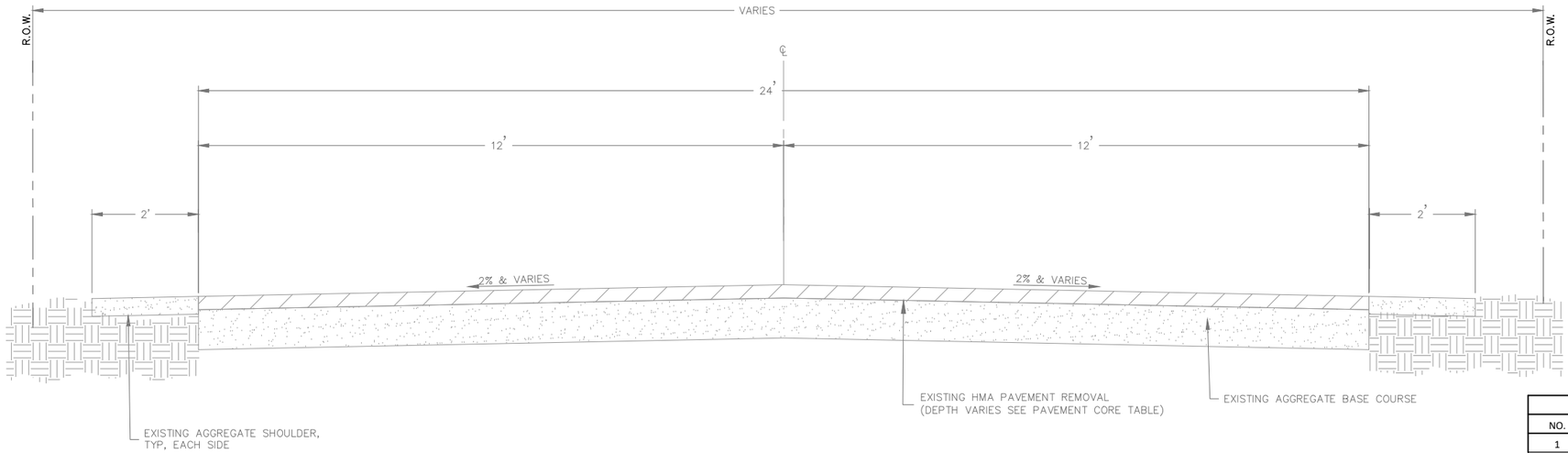
REV. NO.	DESCRIPTION	DATE

DRAWING:
TYPICAL SECTION

SET TYPE: PRELIMINARY
G:\C3D\25\25-694B\Plans\25-694 Plans.dwg, Typ 3

JOB NUMBER:
25-694

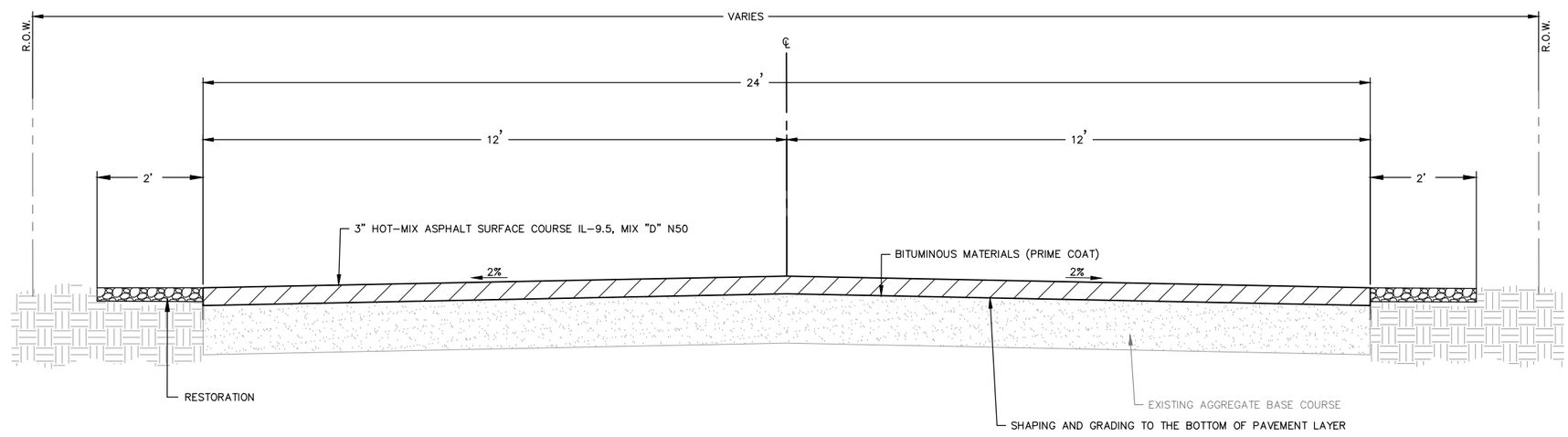
SHEET NUMBER:
7 of 24



EXISTING HODGES WAY TYPICAL SECTION

STA 401+00 TO STA 401+20
N.T.S.

PAVEMENT CORE TABLE			
NO.	1st Lift Thickness (in)	2nd Lift Thickness (in)	Aggregate Thickness (in)
1	1.5	N/A	6"
2	2.0	N/A	10.5"
3	1.5	0.75	8.5"
4	3.5	1.5	6.5"
5	2.0	N/A	8"
6	1.5	N/A	9"



BITUMINOUS MATERIALS (PRIME) SHALL BE APPLIED AT:

- 0.25 LBS/SF OVER AGGREGATE BASE COURSE

MIX TABLE

USE	SURFACE
AC	PG64-22
MIX	IL-95
FRIC AGG	D
VOIDS	4.0 @ N50

PROPOSED HODGES WAY TYPICAL SECTION

STA 401+00 TO STA 401+20
N.T.S.

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

PLOT DATE: 5/15/25

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

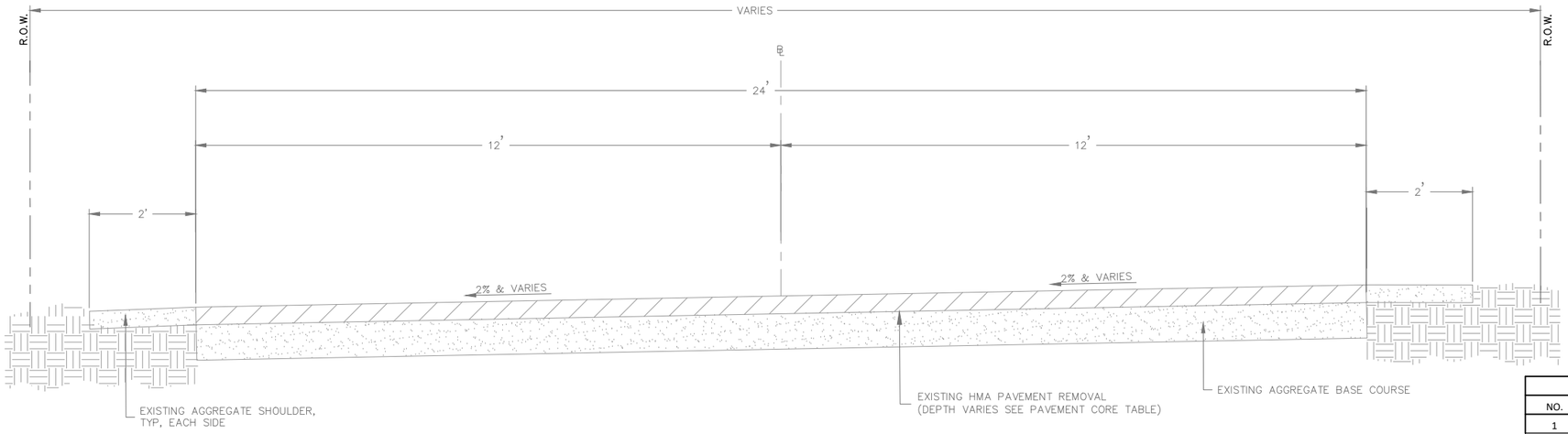
REV. NO.	DESCRIPTION	DATE

DRAWING:
TYPICAL SECTION

SET TYPE: PRELIMINARY
G:\C3D\25\25-694B\Plans\25-694 Plans.dwg, Typ 4

JOB NUMBER:
25-694

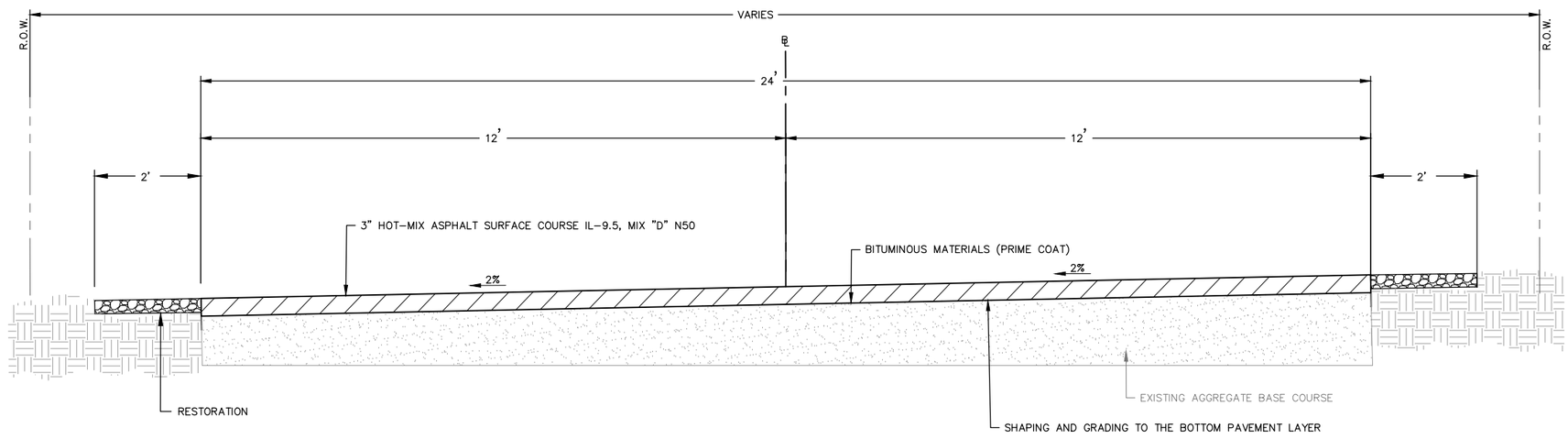
SHEET NUMBER:
8 of 24



EXISTING PARKWAY DRIVE TYPICAL SECTION

STA 500+50 TO STA 505+49.55
N.T.S.

PAVEMENT CORE TABLE			
NO.	1st Lift Thickness (in)	2nd Lift Thickness (in)	Aggregate Thickness (in)
1	1.5	N/A	6"
2	2.0	N/A	10.5"
3	1.5	0.75	8.5"
4	3.5	1.5	6.5"
5	2.0	N/A	8"
6	1.5	N/A	9"



BITUMINOUS MATERIALS (PRIME) SHALL BE APPLIED AT:

- 0.25 LBS/SF OVER AGGREGATE BASE COURSE

MIX TABLE

USE	SURFACE
AC	PG64-22
MIX	IL-95
FRIC AGG	D
VOIDS	4.0 @ N50

PROPOSED PARKWAY DRIVE TYPICAL SECTION

STA 500+50 TO STA 505+49.55
N.T.S.

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

REV. NO.	DESCRIPTION	DATE

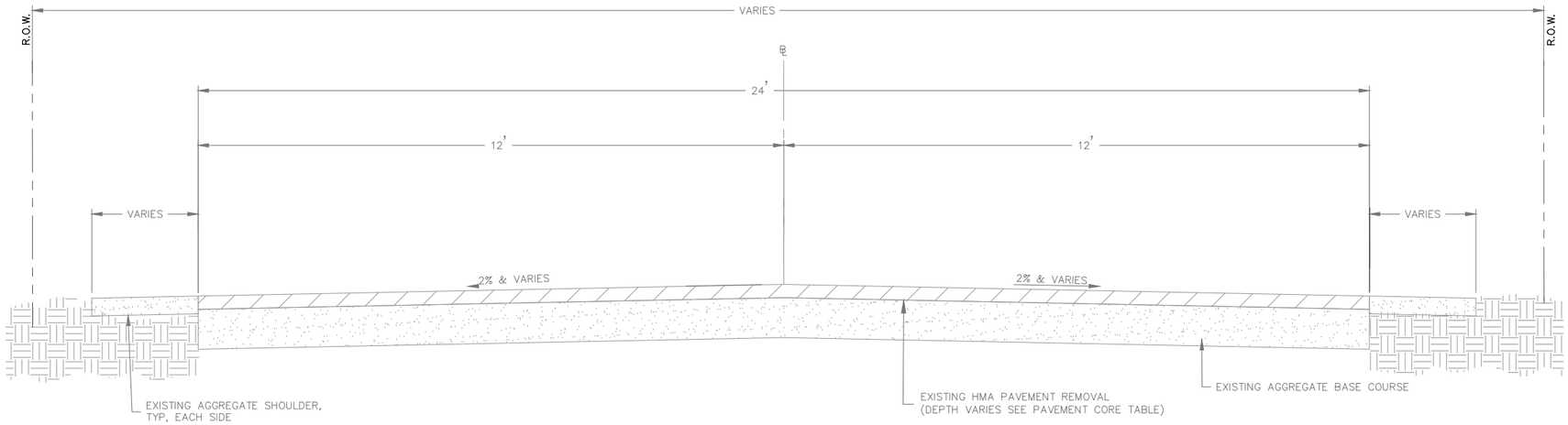
DRAWING:
TYPICAL SECTION

SET TYPE: PRELIMINARY
G:\C30\25\25-694B\Plans\25-694 Plans.dwg, Typ 5

JOB NUMBER:
25-694

SHEET NUMBER:
9 of 24

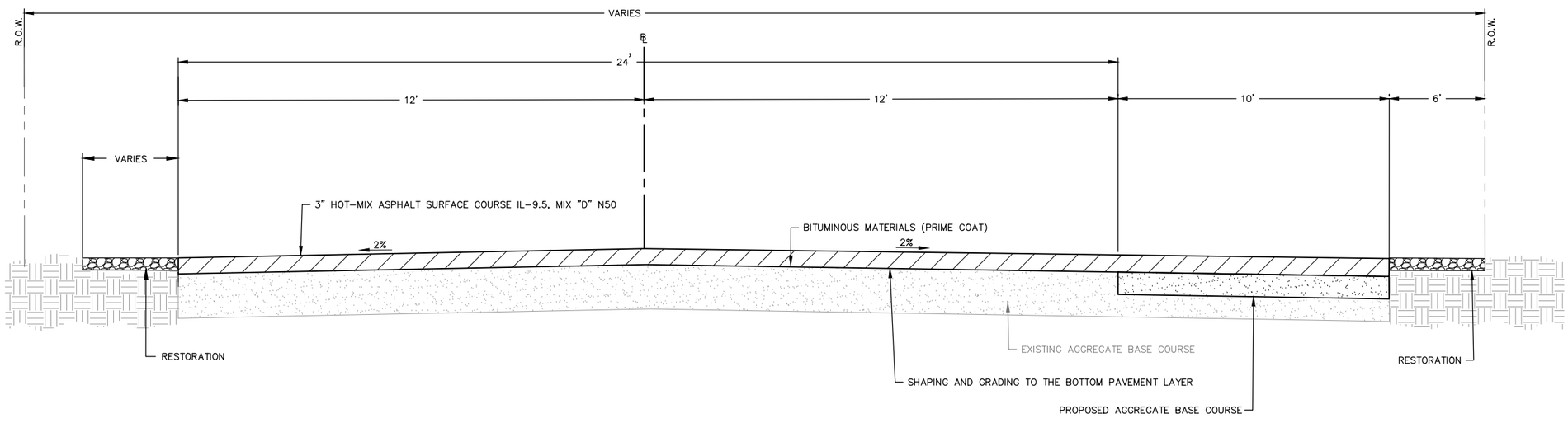
PLOT DATE: 5/15/25



EXISTING PARKWAY DRIVE TYPICAL SECTION

STA 505+49.44 TO STA 507+05
N.T.S.

PAVEMENT CORE TABLE			
NO.	1st Lift Thickness (in)	2nd Lift Thickness (in)	Aggregate Thickness (in)
1	1.5	N/A	6"
2	2.0	N/A	10.5"
3	1.5	0.75	8.5"
4	3.5	1.5	6.5"
5	2.0	N/A	8"
6	1.5	N/A	9"



BITUMINOUS MATERIALS (PRIME) SHALL BE APPLIED AT:

- 0.25 LBS/SF OVER AGGREGATE BASE COURSE

MIX TABLE

USE	SURFACE
AC	PG64-22
MIX	IL-95
FRIC AGG	D
VOIDS	4.0 @ N50

PROPOSED PARKWAY DRIVE TYPICAL SECTION

STA 505+49.44 TO STA 507+05
N.T.S.

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

PLOT DATE: 5/15/25

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

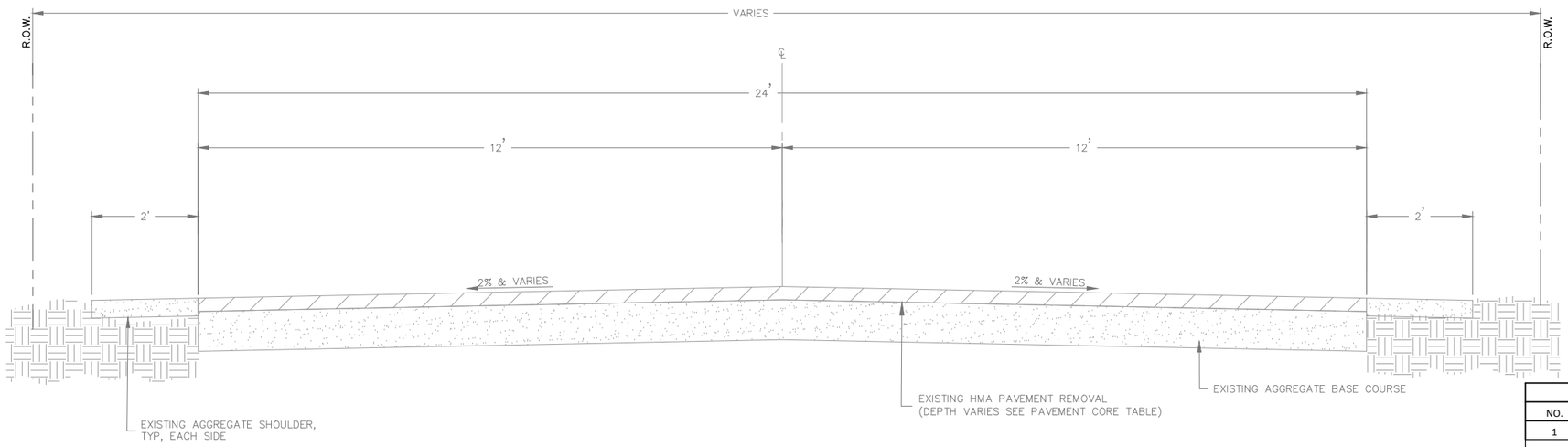
REV. NO.	DESCRIPTION	DATE

DRAWING:
TYPICAL SECTION

SET TYPE: PRELIMINARY
G:\C3D\25\25-6948\Plans\25-694 Plans.dwg, Typ 6

JOB NUMBER:
25-694

SHEET NUMBER:
10 of 24



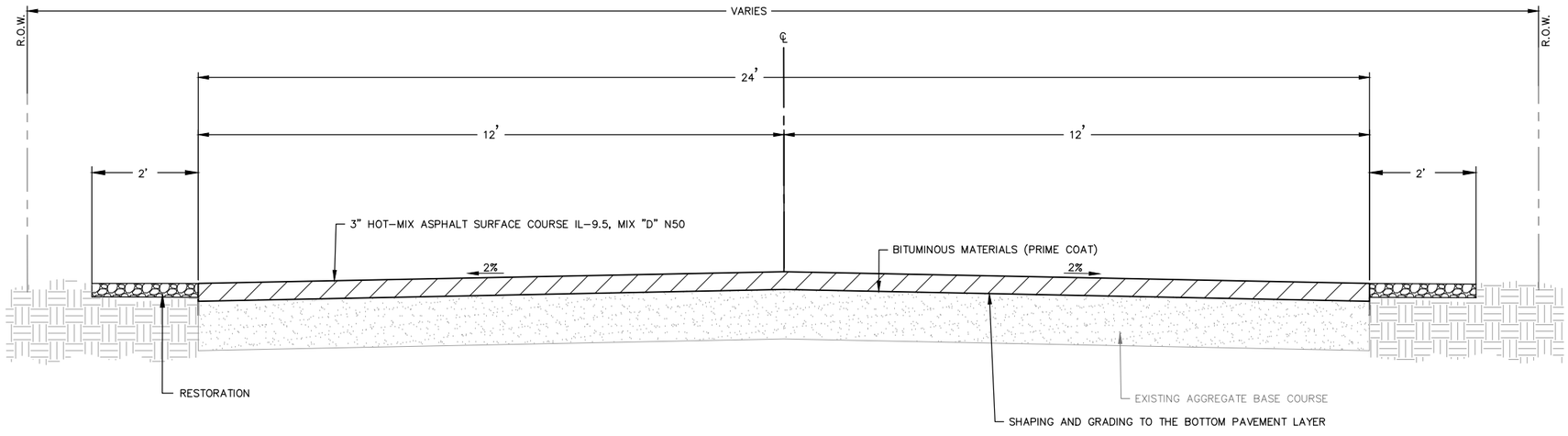
EXISTING ANDREWS DRIVE TYPICAL SECTION

STA 600+50 TO STA 610+29
STA 800+21 TO STA 805+22
N.T.S.

PAVEMENT CORE TABLE			
NO.	1st Lift Thickness (in)	2nd Lift Thickness (in)	Aggregate Thickness (in)
1	1.5	N/A	6"
2	2.0	N/A	10.5"
3	1.5	0.75	8.5"
4	3.5	1.5	6.5"
5	2.0	N/A	8"
6	1.5	N/A	9"

BITUMINOUS MATERIALS (PRIME) SHALL BE APPLIED AT:

- 0.25 LBS/SF OVER AGGREGATE BASE COURSE



PROPOSED ANDREWS DRIVE TYPICAL SECTION

STA 600+50 TO STA 610+29
STA 800+21 TO STA 805+22
N.T.S.

MIX TABLE

USE	SURFACE
AC	PG64-22
MIX	IL-95
FRIC AGG	D
VOIDS	4.0 @ N50

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

PLOT DATE: 5/15/25

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

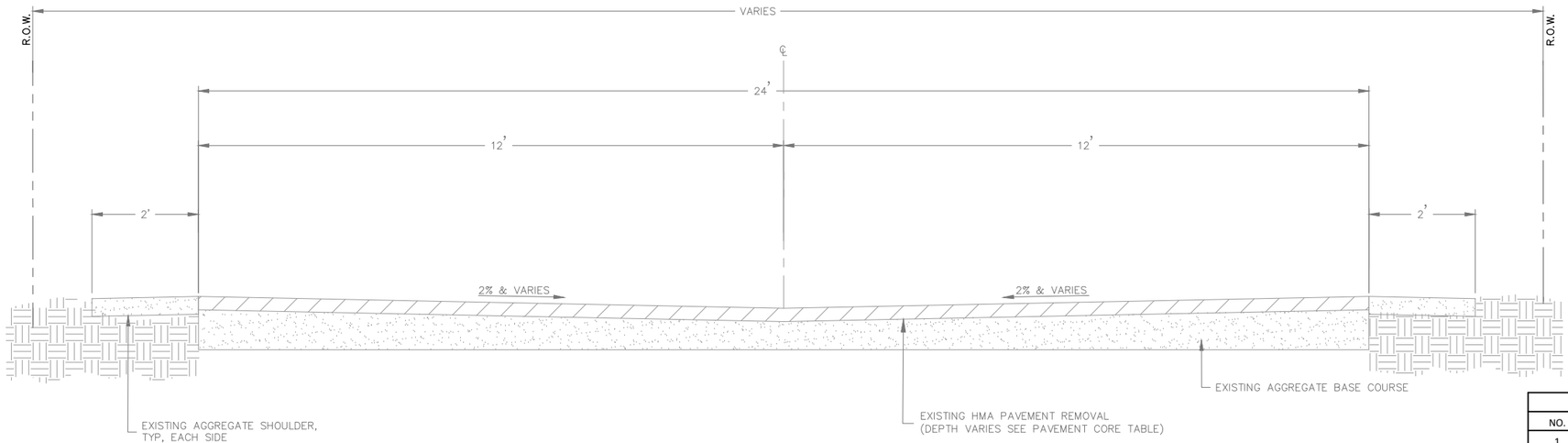
REV. NO.	DESCRIPTION	DATE

DRAWING: TYPICAL SECTION

SET TYPE: PRELIMINARY
G:\C3D\25\25-694B\Plans\25-694 Plans.dwg, Typ 7

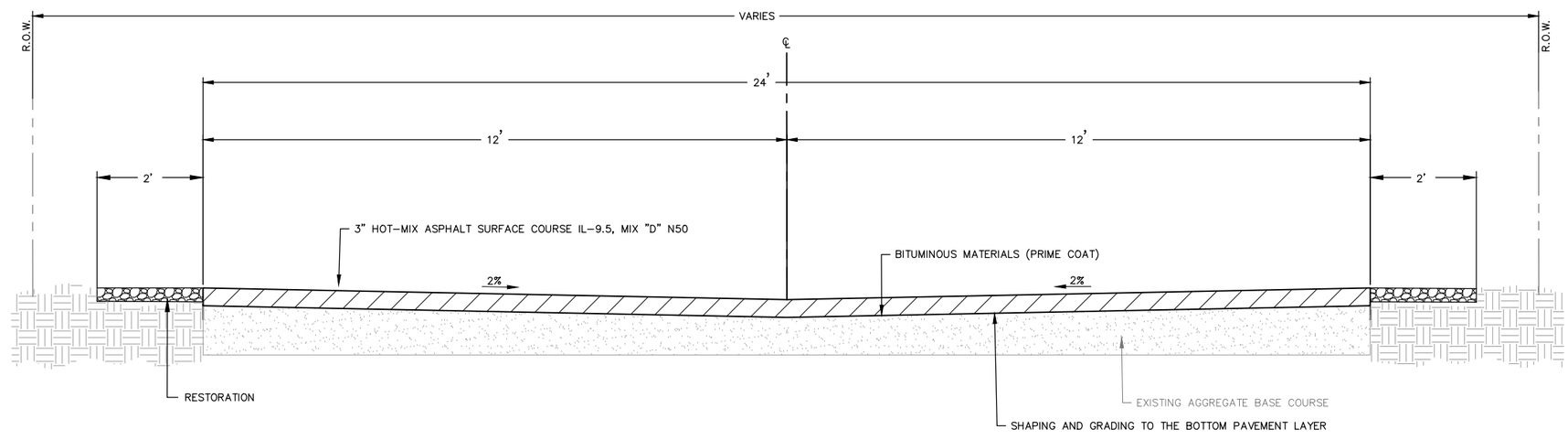
JOB NUMBER: 25-694

SHEET NUMBER: 11 of 24



EXISTING TER MAAT COUR TYPICAL SECTION
STA 700+50 TO STA 705+00
N.T.S.

PAVEMENT CORE TABLE			
NO.	1st Lift Thickness (in)	2nd Lift Thickness (in)	Aggregate Thickness (in)
1	1.5	N/A	6"
2	2.0	N/A	10.5"
3	1.5	0.75	8.5"
4	3.5	1.5	6.5"
5	2.0	N/A	8"
6	1.5	N/A	9"



PROPOSED TER MAAT COURT TYPICAL SECTION
STA 700+50 TO STA 705+00
N.T.S.

BITUMINOUS MATERIALS (PRIME) SHALL BE APPLIED AT:

- 0.25 LBS/SF OVER AGGREGATE BASE COURSE

MIX TABLE

USE	SURFACE
AC	PG64-22
MIX	IL-95
FRIC AGG	D
VOIDS	4.0 @ N50

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

REV. NO.	DESCRIPTION	DATE

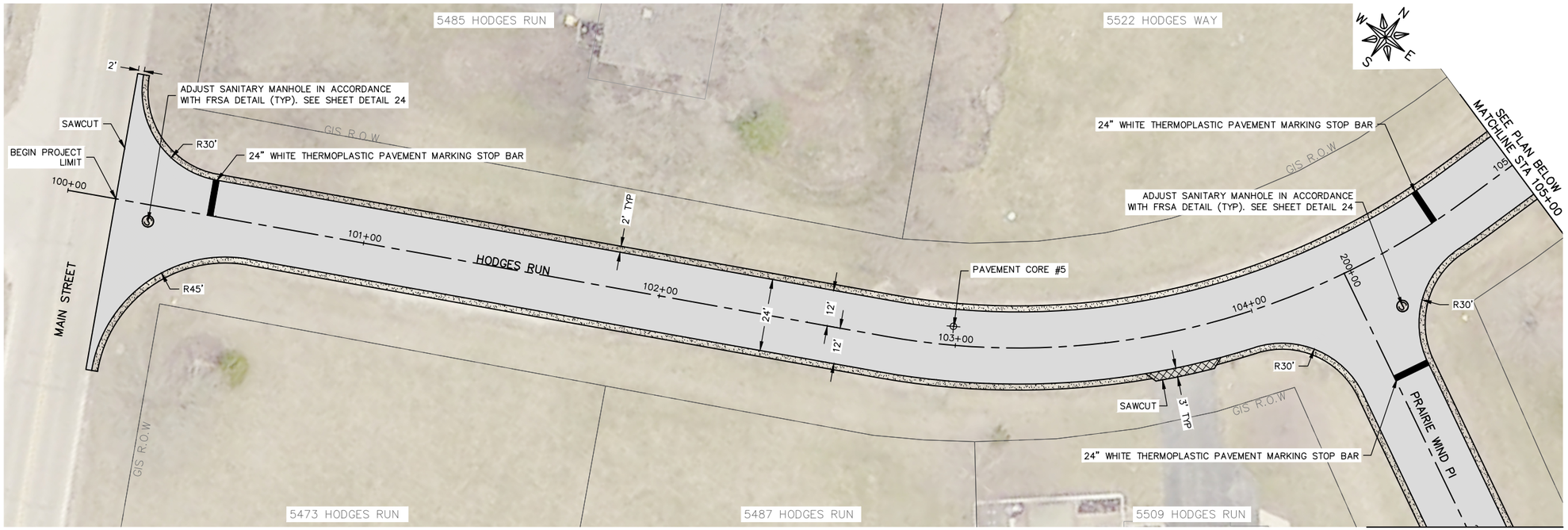
DRAWING:
TYPICAL SECTION

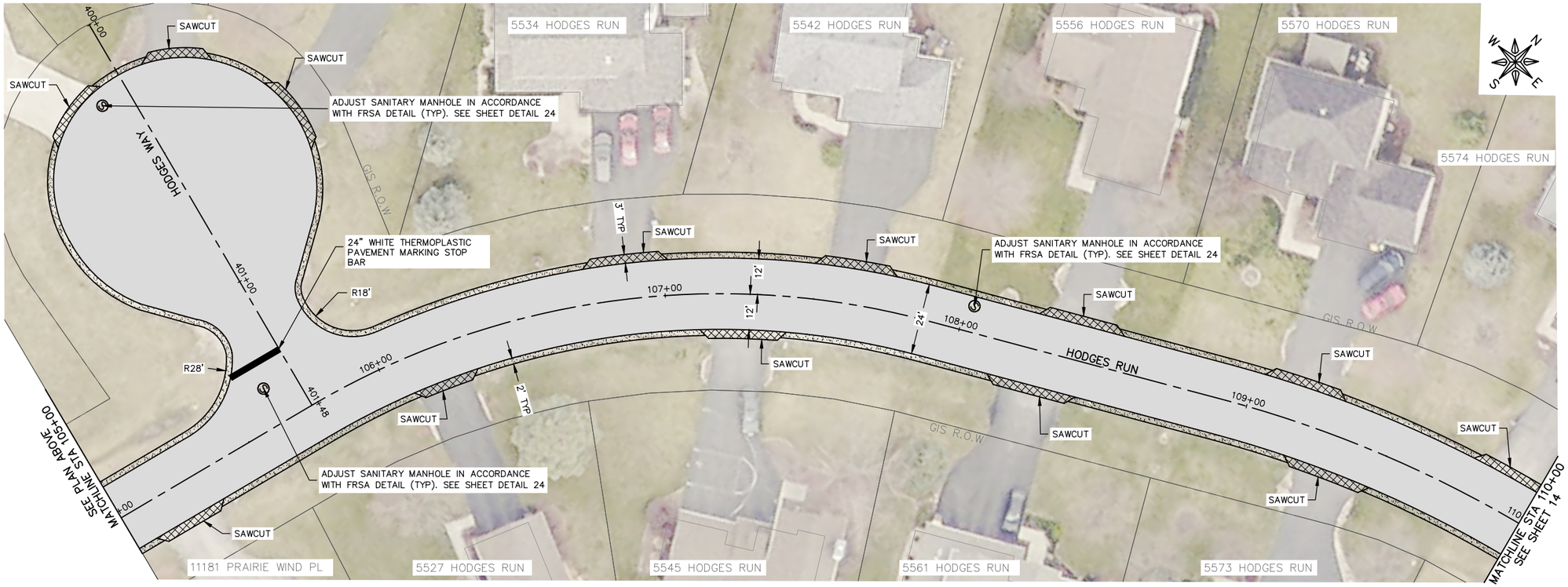
SET TYPE: PRELIMINARY
G:\C3D\25\25-694B\Plans\25-694 Plans.dwg, Typ 8

JOB NUMBER:
25-694

SHEET NUMBER:
12 of 24

PLOT DATE: 5/15/25





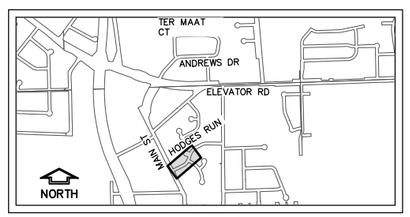
LEGEND

- EARTH EXCAVATION, AGGREGATE BASE AND HMA PAVEMENT, 3"
- HMA PAVEMENT REMOVAL & REPLACEMENT. SEE TYPICAL SECTION FOR DETAILS.
- HMA DRIVEWAY REMOVAL & REPLACEMENT, 3"
- AGGREGATE SHOULDER REMOVAL
- TURF RESTORATION
- PAVEMENT CORE SAMPLE

GENERAL NOTE:

CONTRACTOR TO RESTORE ANY DISTURBED AREAS TO PRE-CONSTRUCTION CONDITIONS.

20 0 20 40 60 FEET



KEY MAP

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

PLOT DATE: 5/15/25

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

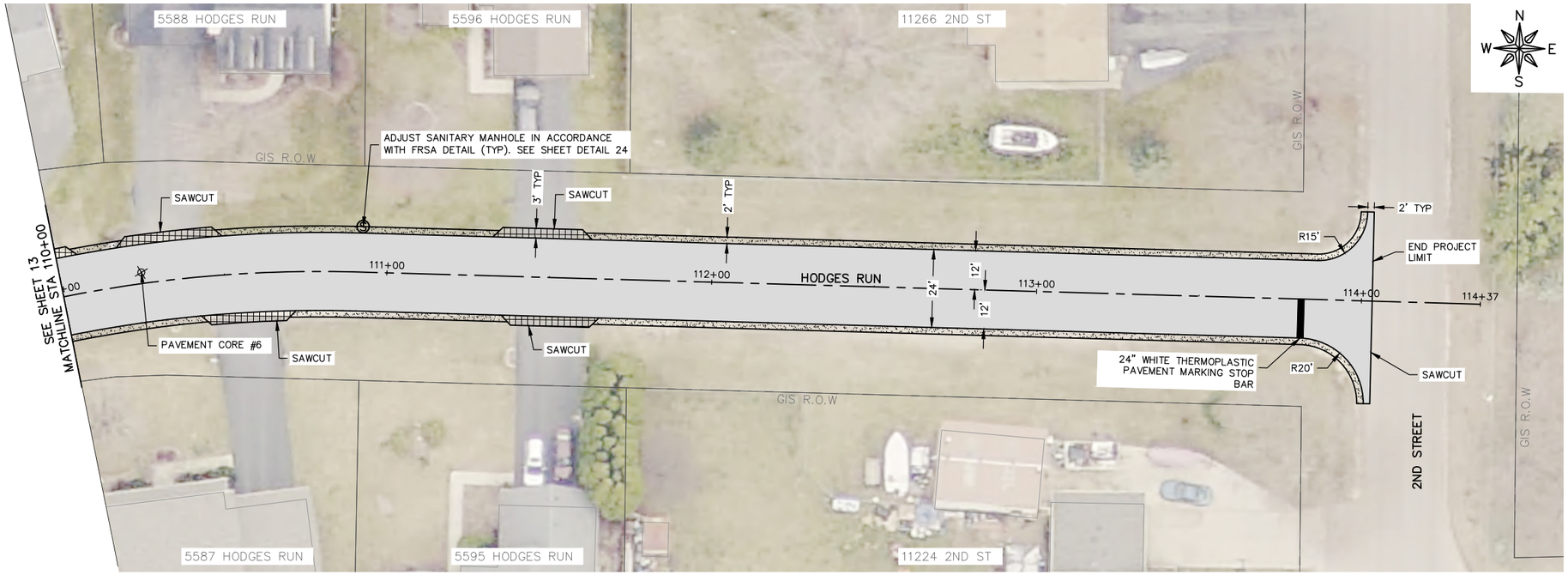
REV. NO.	DESCRIPTION	DATE

DRAWING: HODGES RUN STA 100+00 TO STA 110+00

SET TYPE: PRELIMINARY
G:\C30\25\25-694B\25-694 Design.dwg, P&P1

JOB NUMBER: 25-694

SHEET NUMBER: 13 of 24



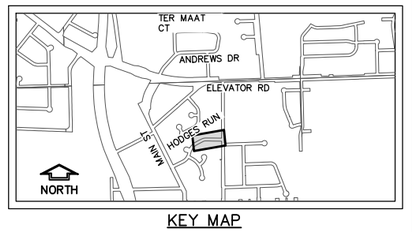
LEGEND

- EARTH EXCAVATION, AGGREGATE BASE AND HMA PAVEMENT, 3"
- HMA PAVEMENT REMOVAL & REPLACEMENT. SEE TYPICAL SECTION FOR DETAILS.
- HMA DRIVEWAY REMOVAL & REPLACEMENT, 3"
- AGGREGATE SHOULDER REMOVAL
- TURF RESTORATION
- PAVEMENT CORE SAMPLE

GENERAL NOTE:

CONTRACTOR TO RESTORE ANY DISTURBED AREAS TO PRE-CONSTRUCTION CONDITIONS.

20 0 20 40 60 FEET



KEY MAP

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

PLOT DATE: 5/15/25

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

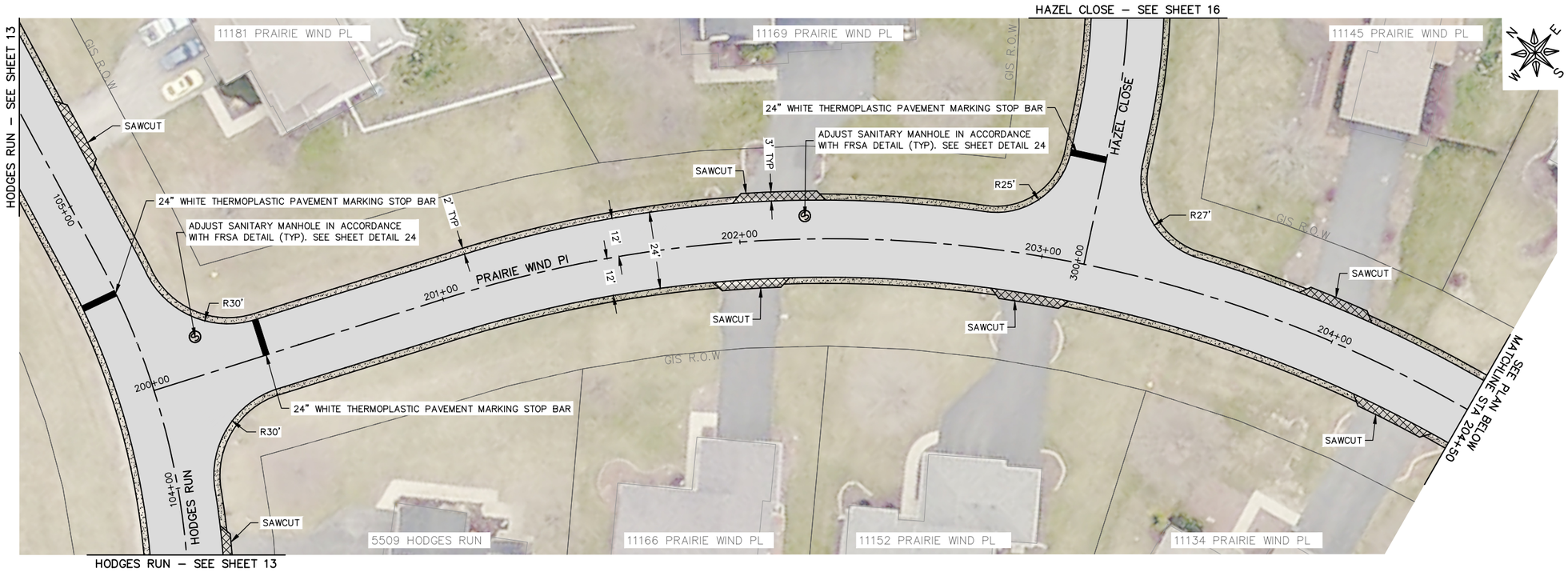
REV. NO.	DESCRIPTION	DATE

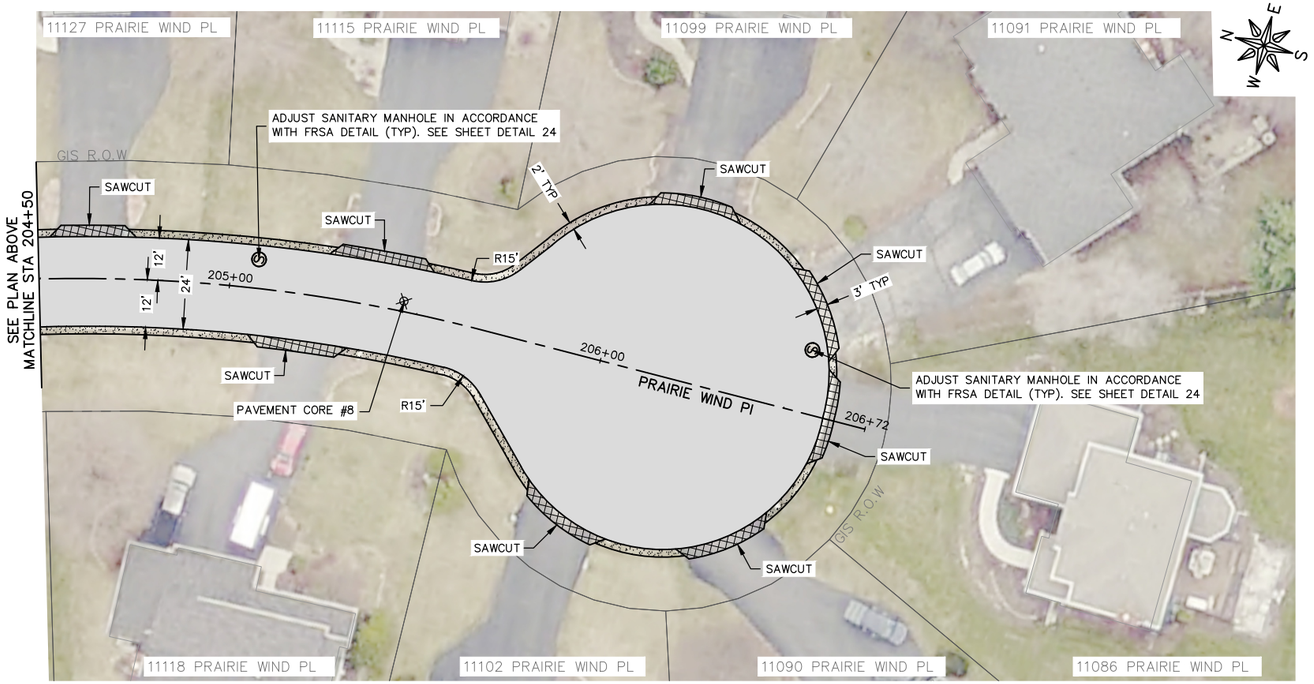
DRAWING:
HODGES RUN STA 110+00 TO STA 140+37

SET TYPE: PRELIMINARY
G:\C3D\25\25-694B\25-694 Design.dwg, P&P2

JOB NUMBER:
25-694

SHEET NUMBER:
14 of 24





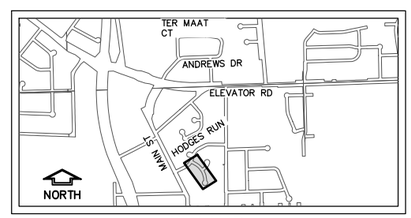
LEGEND

- EARTH EXCAVATION, AGGREGATE BASE AND HMA PAVEMENT, 3"
- HMA PAVEMENT REMOVAL & REPLACEMENT. SEE TYPICAL SECTION FOR DETAILS.
- HMA DRIVEWAY REMOVAL & REPLACEMENT, 3"
- AGGREGATE SHOULDER REMOVAL
- TURF RESTORATION
- PAVEMENT CORE SAMPLE

GENERAL NOTE:

CONTRACTOR TO RESTORE ANY DISTURBED AREAS TO PRE–CONSTRUCTION CONDITIONS.

20 0 20 40 60 FEET



KEY MAP

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184–003525
ILLINOIS
IOWA
WISCONSIN

PLOT DATE: 5/15/25

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

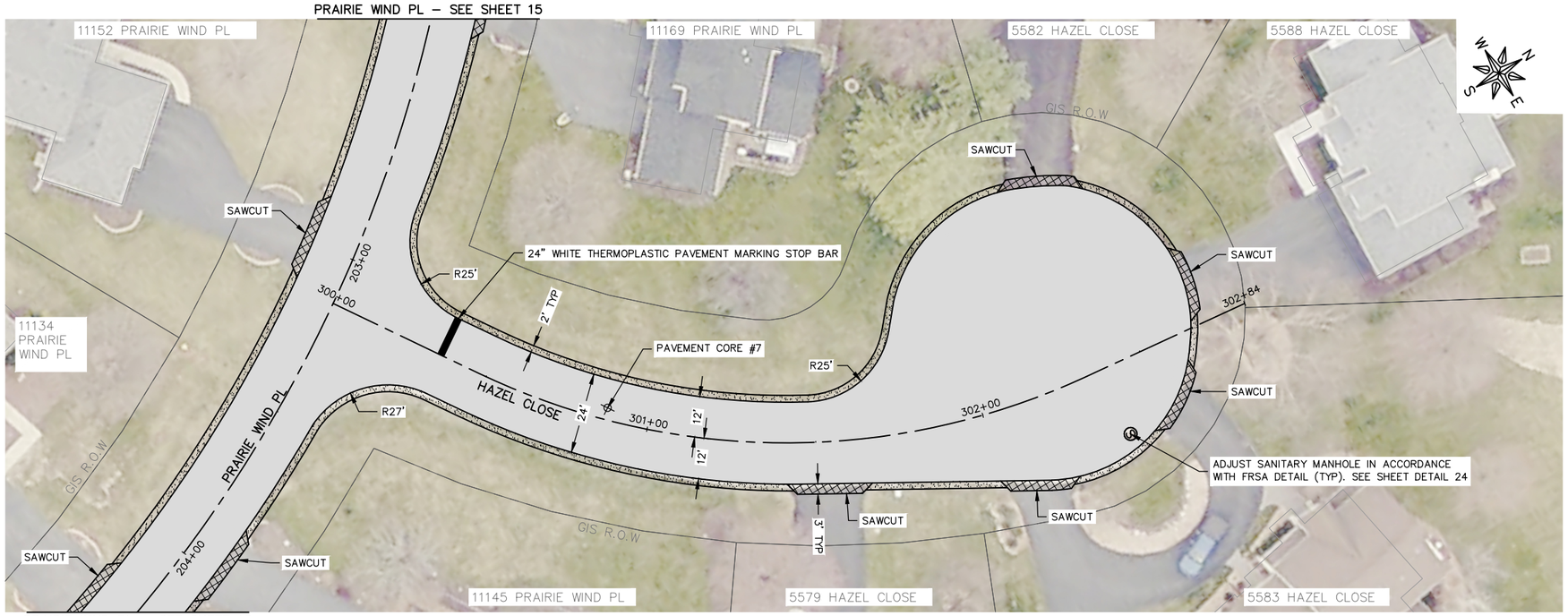
REV. NO.	DESCRIPTION	DATE

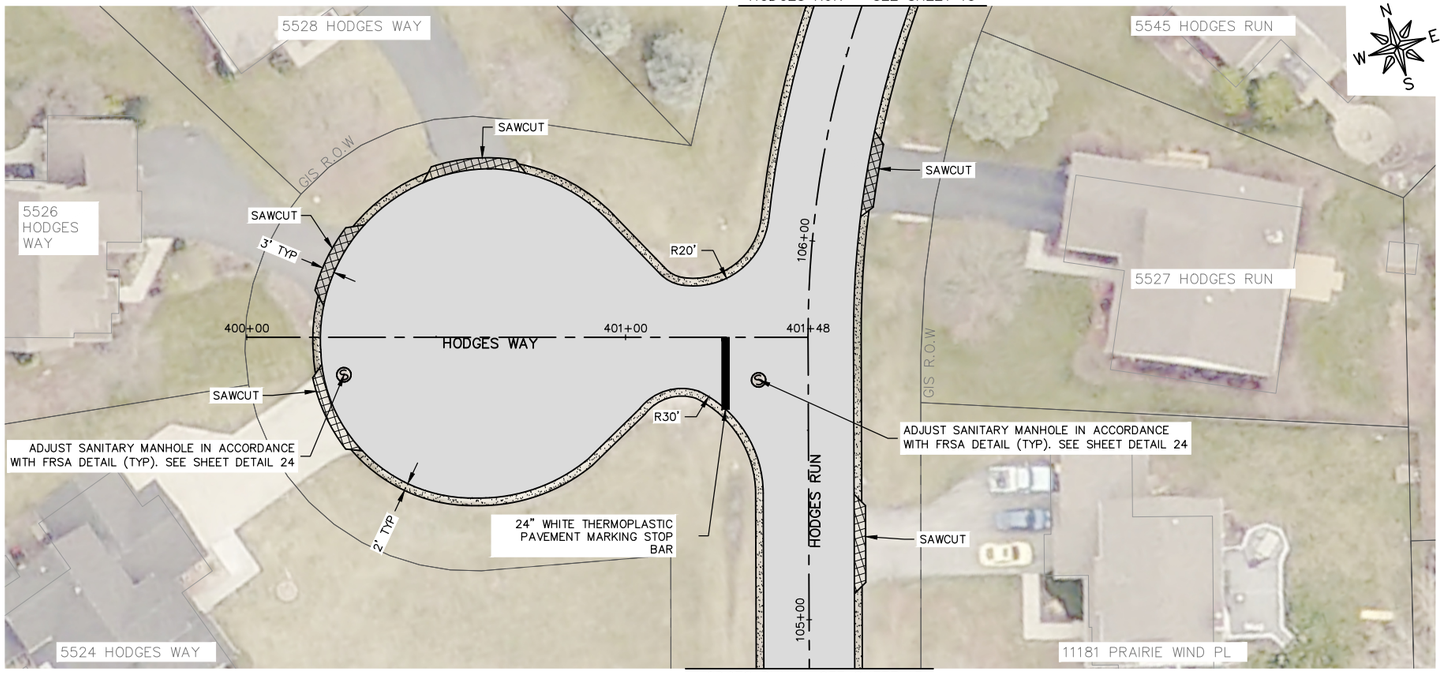
DRAWING:
PRAIRIE WIND PL STA 200+00 TO STA 206+72

SET TYPE: PRELIMINARY
G:\C3D\25\25-694B\25-694 Design.dwg, P&P3

JOB NUMBER: 25–694

SHEET NUMBER: 15 of 24





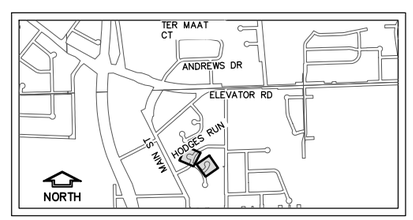
LEGEND

- EARTH EXCAVATION, AGGREGATE BASE AND HMA PAVEMENT, 3"
- HMA PAVEMENT REMOVAL & REPLACEMENT. SEE TYPICAL SECTION FOR DETAILS.
- HMA DRIVEWAY REMOVAL & REPLACEMENT, 3"
- AGGREGATE SHOULDER REMOVAL
- TURF RESTORATION
- PAVEMENT CORE SAMPLE

GENERAL NOTE:

CONTRACTOR TO RESTORE ANY DISTURBED AREAS TO PRE-CONSTRUCTION CONDITIONS.

20 0 20 40 60 FEET



KEY MAP

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

PLOT DATE: 5/15/25

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

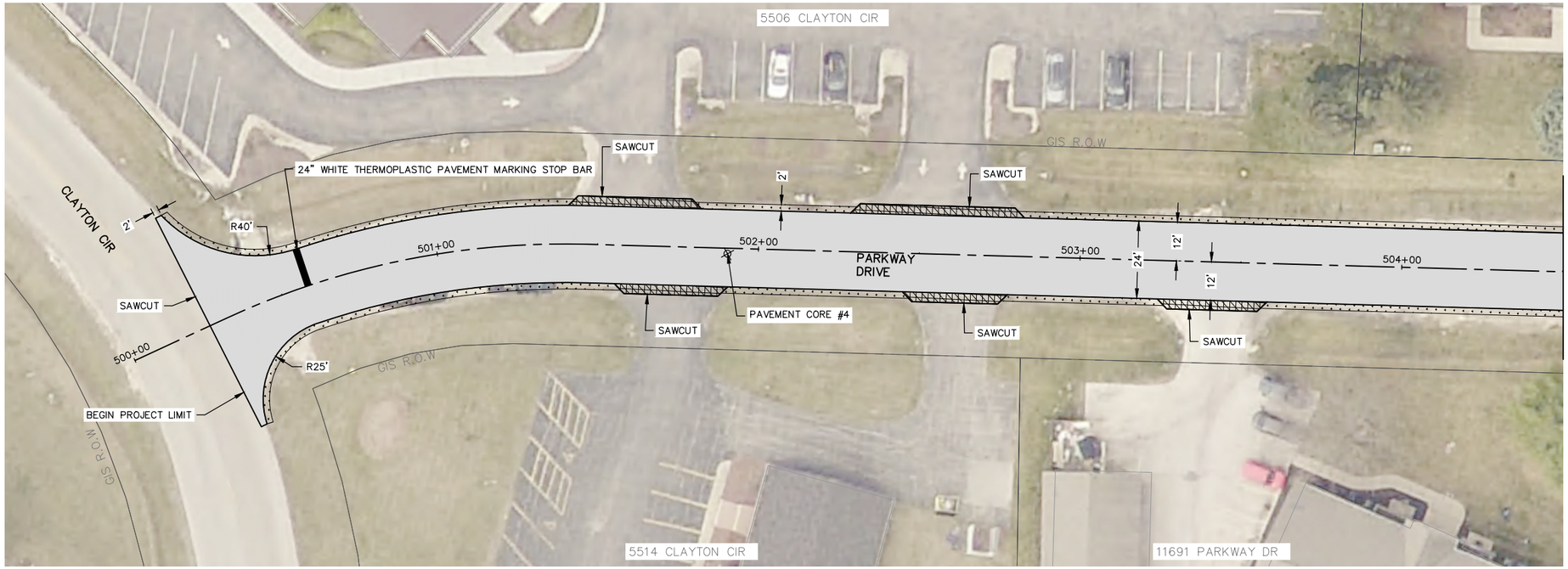
REV. NO.	DESCRIPTION	DATE

DRAWING:
HAZEL CLOSE STA 300+00 TO STA 302+84 & HODGES WAY STA 400+00 TO STA 401+48

SET TYPE: PRELIMINARY
G:\C30\25\25-694\25-694 Design.dwg, PAF4

JOB NUMBER: 25-694

SHEET NUMBER: 16 of 24

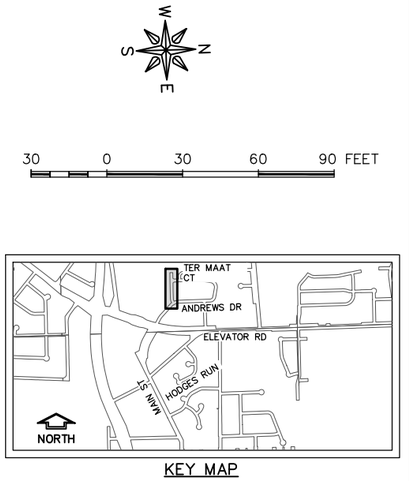


LEGEND

- EARTH EXCAVATION, AGGREGATE BASE AND HMA PAVEMENT, 3"
- HMA PAVEMENT REMOVAL & REPLACEMENT. SEE TYPICAL SECTION FOR DETAILS.
- HMA DRIVEWAY REMOVAL & REPLACEMENT, 3"
- AGGREGATE SHOULDER REMOVAL
- TURF RESTORATION
- PAVEMENT CORE SAMPLE

GENERAL NOTE:

CONTRACTOR TO RESTORE ANY DISTURBED AREAS TO PRE-CONSTRUCTION CONDITIONS.



30 0 30 60 90 FEET

KEY MAP

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525

ILLINOIS
IOWA
WISCONSIN

PLOT DATE: 5/15/25

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

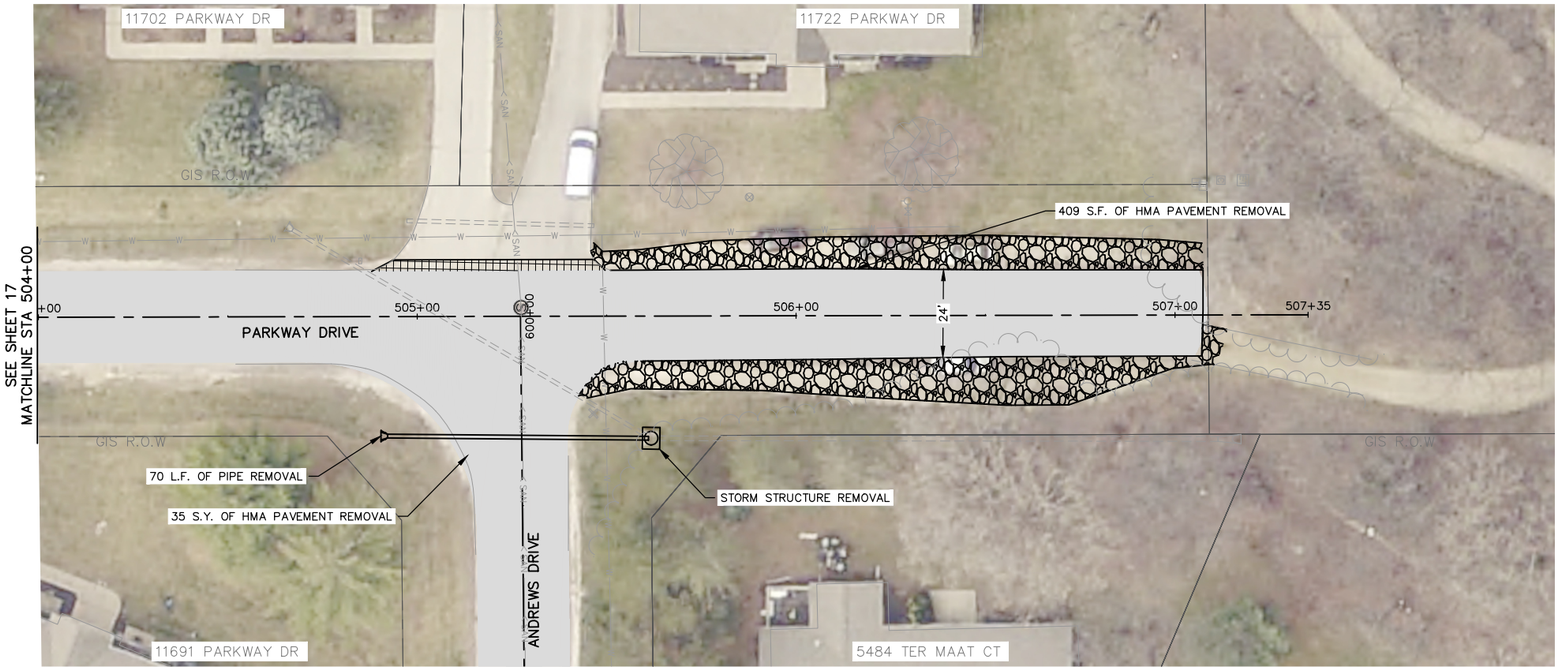
REV. NO.	DESCRIPTION	DATE

DRAWING:
PARKWAY DRIVE STA 500+00 TO STA 504+00

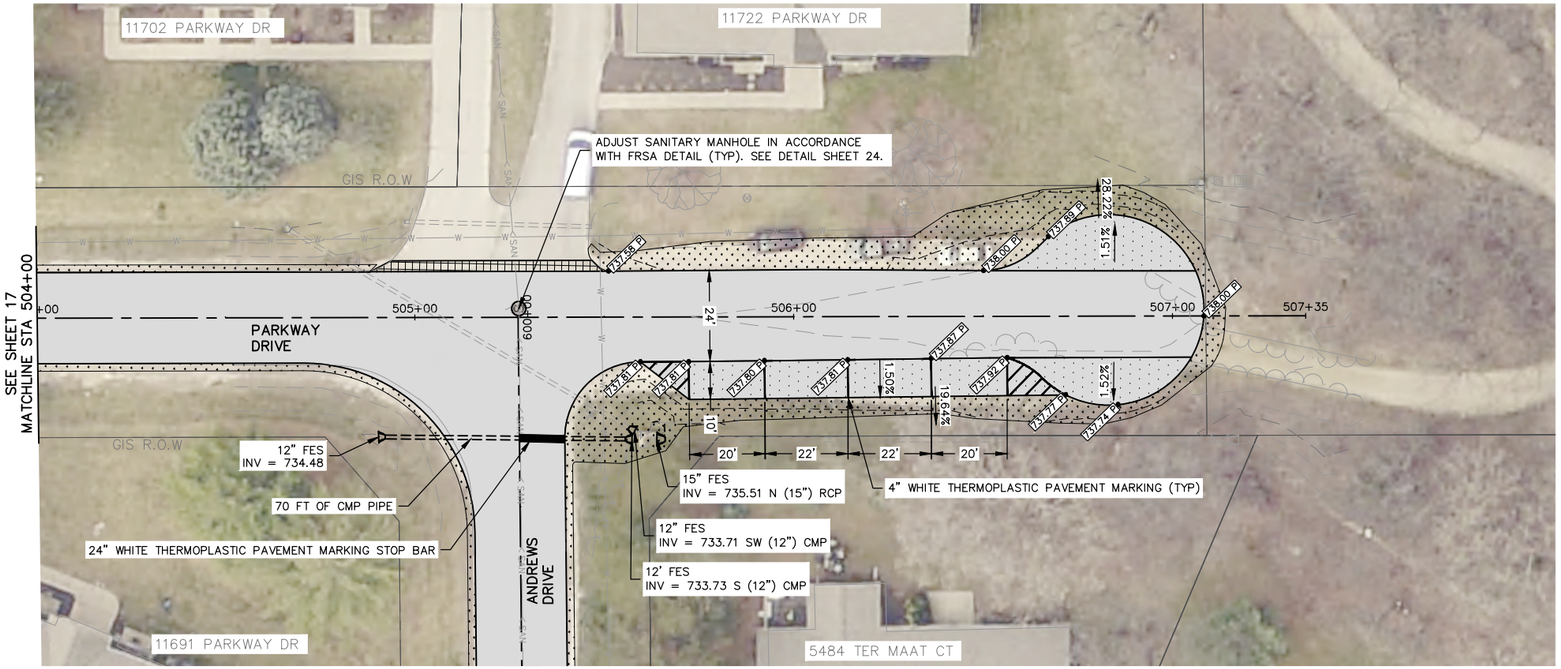
SET TYPE: PRELIMINARY
G:\C3D\25\25-694B\25-694 Design.dwg, P&P5

JOB NUMBER: 25-694

SHEET NUMBER: 17 of 24



REMOVAL PLAN



SITE PLAN

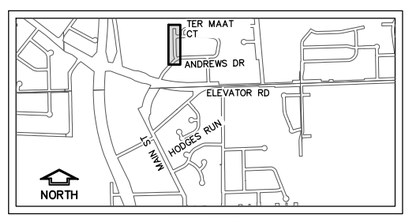
LEGEND

- EARTH EXCAVATION, AGGREGATE BASE AND HMA PAVEMENT, 3"
- HMA PAVEMENT REMOVAL & REPLACEMENT. SEE TYPICAL SECTION FOR DETAILS.
- HMA DRIVEWAY REMOVAL & REPLACEMENT, 3"
- AGGREGATE SHOULDER REMOVAL
- TURF RESTORATION
- PAVEMENT CORE SAMPLE

GENERAL NOTE:

CONTRACTOR TO RESTORE ANY DISTURBED AREAS TO PRE-CONSTRUCTION CONDITIONS.

20 0 20 40 60 FEET



KEY MAP

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525

ILLINOIS
IOWA
WISCONSIN

PLOT DATE: 5/15/25

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

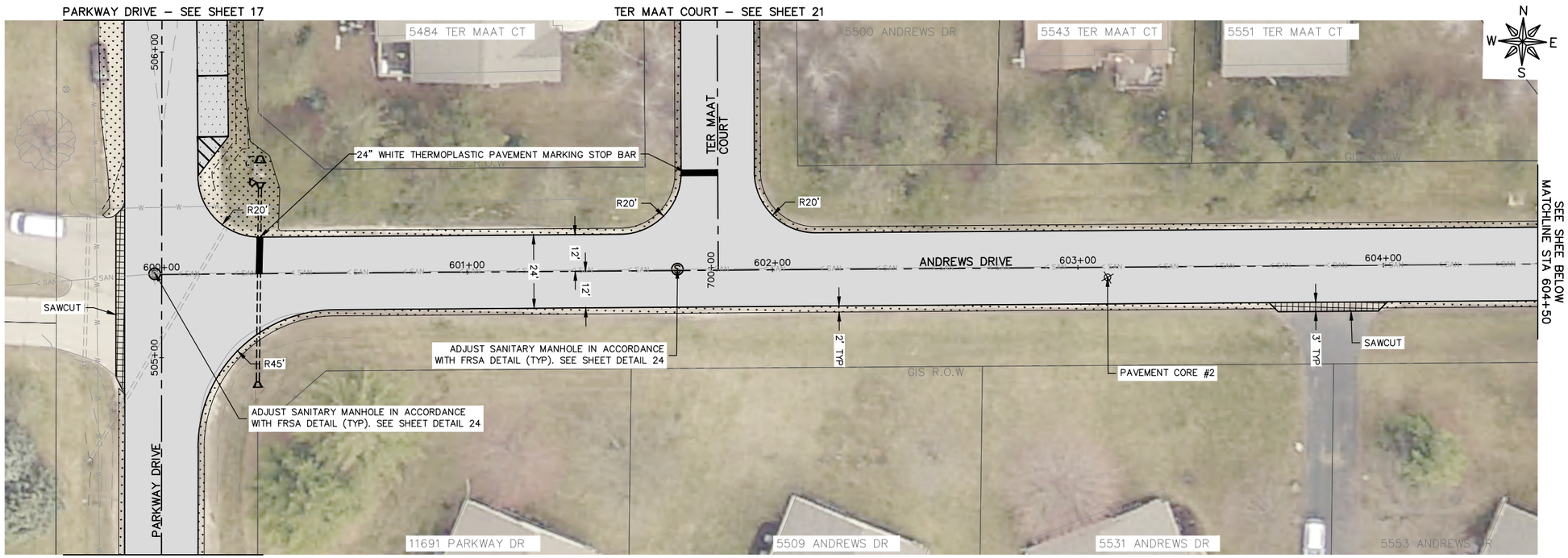
REVISIONS		
REV. NO.	DESCRIPTION	DATE

DRAWING:
PARKWAY DRIVE STA 504+00 TO STA 507+35

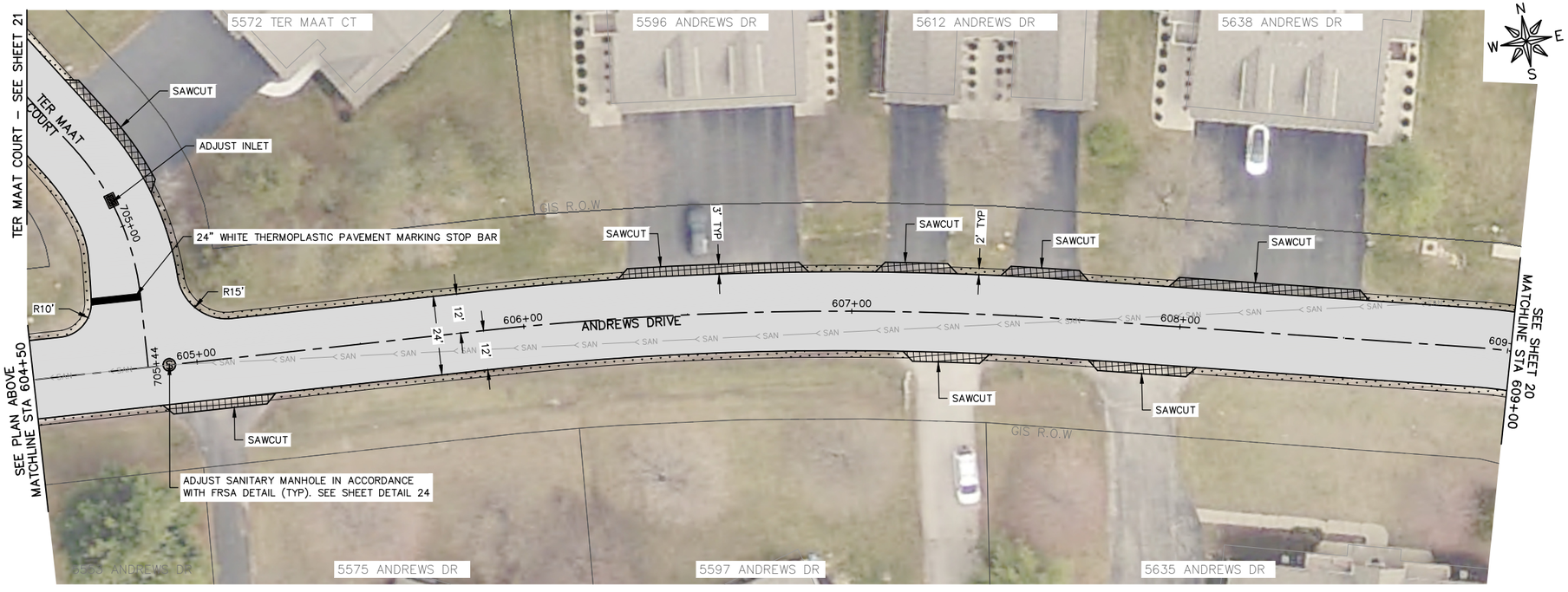
SET TYPE: PRELIMINARY
G:\C3D\25\25-694B\25-694 Design.dwg, P&P5 (2)

JOB NUMBER: 25-694

SHEET NUMBER: 18 of 24



PARKWAY DRIVE – SEE SHEET 17



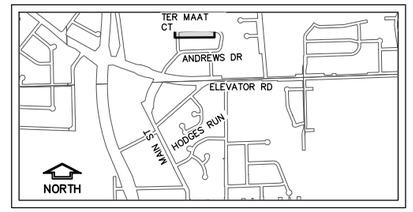
LEGEND

- EARTH EXCAVATION, AGGREGATE BASE AND HMA PAVEMENT, 3"
- HMA PAVEMENT REMOVAL & REPLACEMENT. SEE TYPICAL SECTION FOR DETAILS.
- HMA DRIVEWAY REMOVAL & REPLACEMENT, 3"
- AGGREGATE SHOULDER REMOVAL
- TURF RESTORATION
- PAVEMENT CORE SAMPLE

GENERAL NOTE:

CONTRACTOR TO RESTORE ANY DISTURBED AREAS TO PRE–CONSTRUCTION CONDITIONS.

20 0 20 40 60 FEET



KEY MAP

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184–003525

ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

REV. NO.	DESCRIPTION	DATE

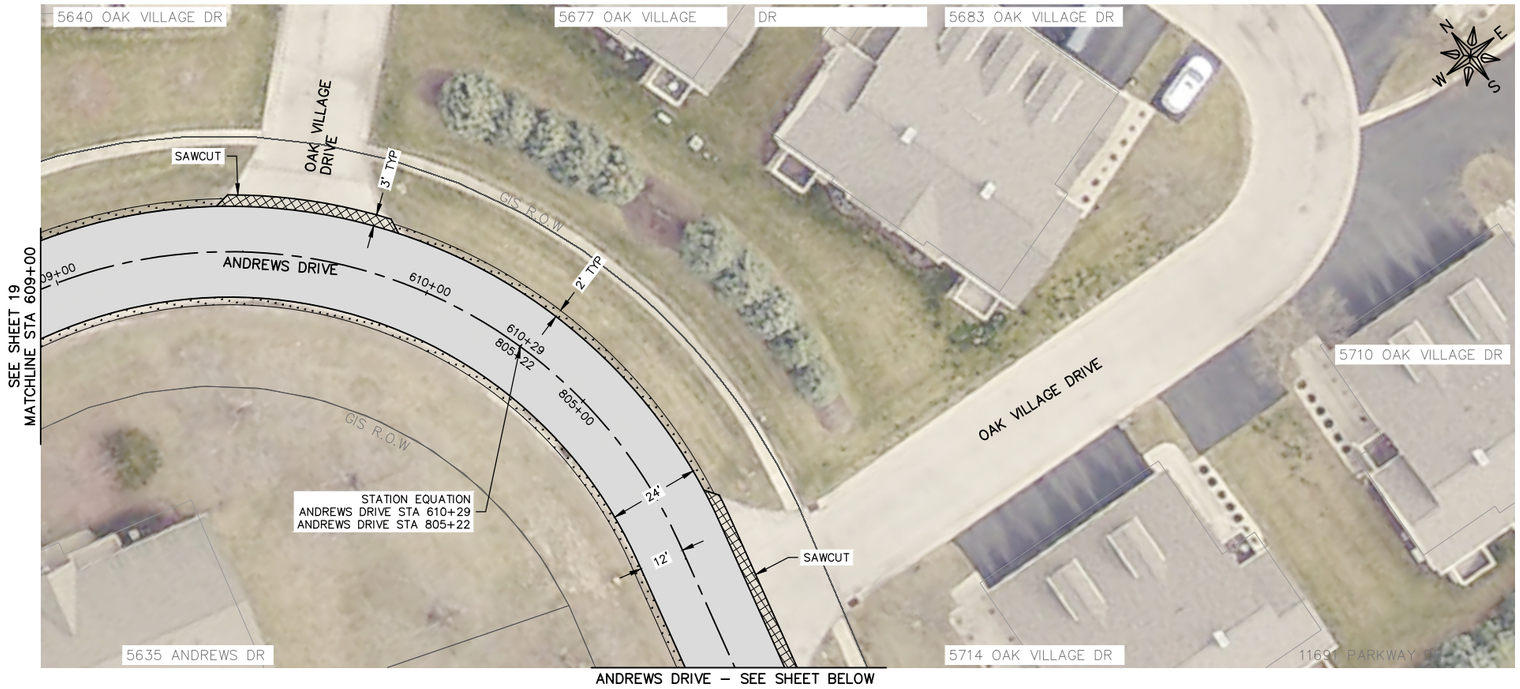
DRAWING:
ANDREWS DRIVE STA 600+00 TO STA 609+00

SET TYPE: PRELIMINARY
G:\C3D\25\25-694B\25-694 Design.dwg, P&P6

JOB NUMBER: 25–694

SHEET NUMBER: 19 of 24

PLOT DATE: 5/15/25



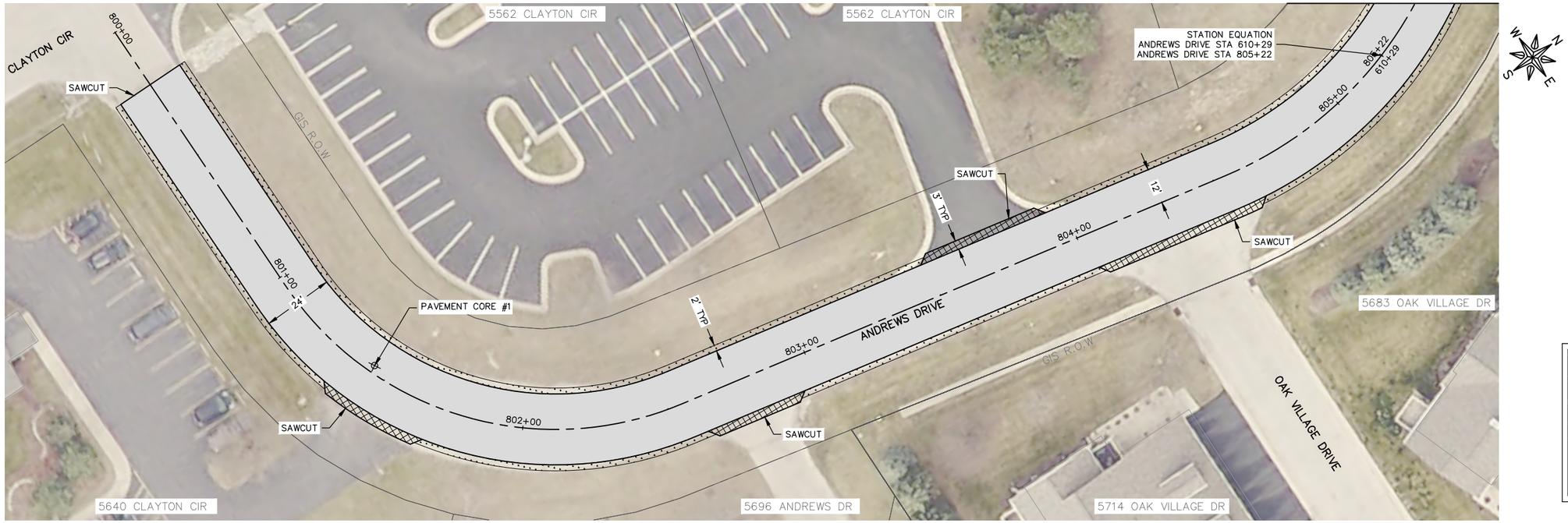
LEGEND

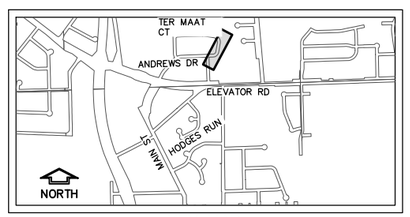
- EARTH EXCAVATION, AGGREGATE BASE AND HMA PAVEMENT, 3"
- HMA PAVEMENT REMOVAL & REPLACEMENT. SEE TYPICAL SECTION FOR DETAILS.
- HMA DRIVEWAY REMOVAL & REPLACEMENT, 3"
- AGGREGATE SHOULDER REMOVAL
- TURF RESTORATION
- PAVEMENT CORE SAMPLE

GENERAL NOTE:

CONTRACTOR TO RESTORE ANY DISTURBED AREAS TO PRE-CONSTRUCTION CONDITIONS.

20 0 20 40 60 FEET





KEY MAP

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

PLOT DATE: 5/15/25

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

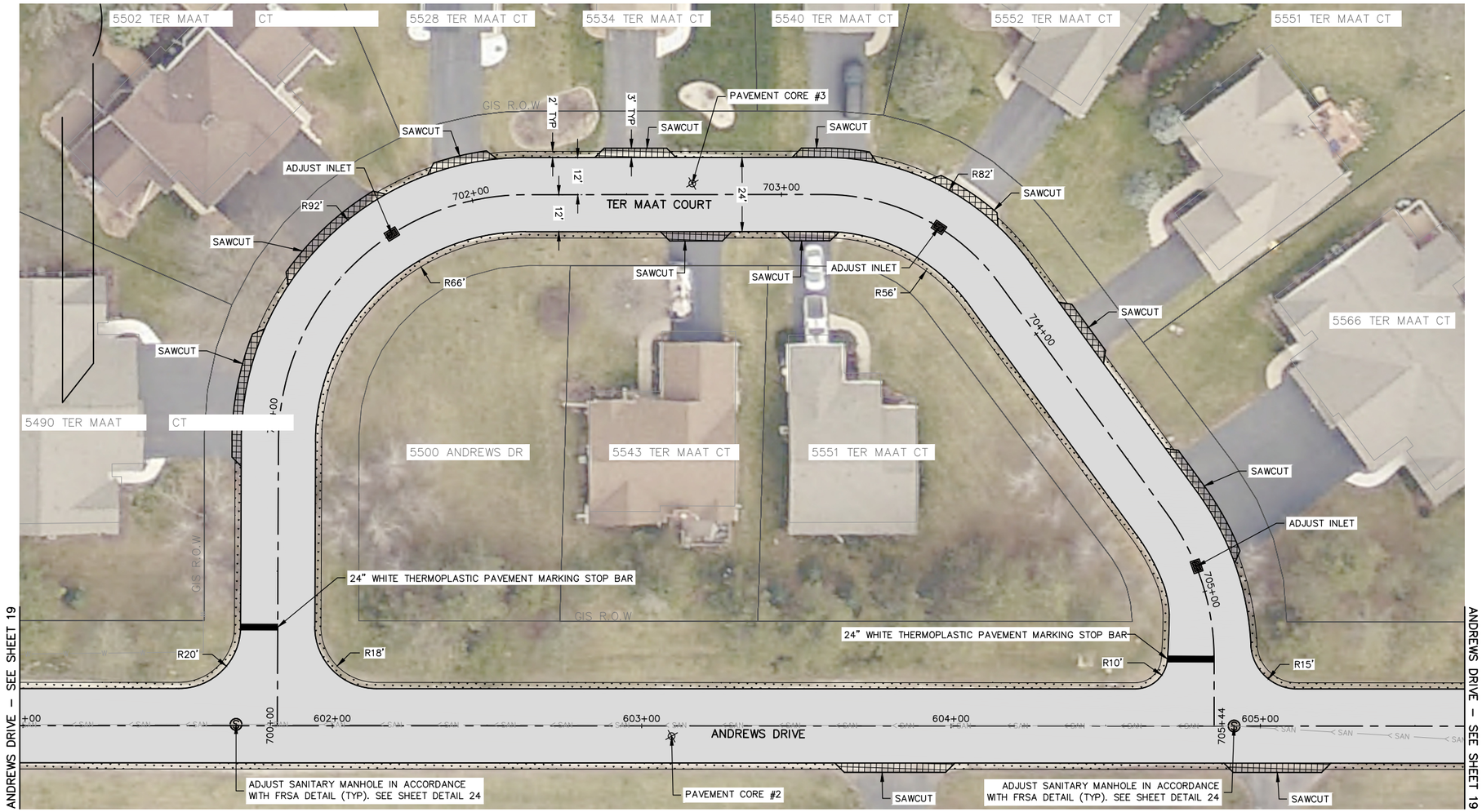
REVISIONS		
REV. NO.	DESCRIPTION	DATE

DRAWING:
ANDREWS DRIVE STA 609+00 TO STA 610+29 & ANDREWS DRIVE STA 800+00 TO STA 805+22

SET TYPE: PRELIMINARY
G:\C3D\25\25-694B\25-694 Design.dwg, P&P7

JOB NUMBER: 25-694

SHEET NUMBER: 20 of 24



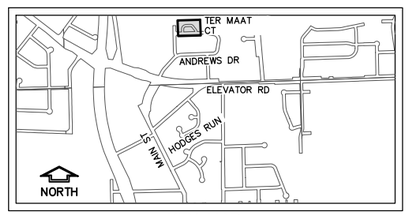
LEGEND

- EARTH EXCAVATION, AGGREGATE BASE AND HMA PAVEMENT, 3"
- HMA PAVEMENT REMOVAL & REPLACEMENT. SEE TYPICAL SECTION FOR DETAILS.
- HMA DRIVEWAY REMOVAL & REPLACEMENT, 3"
- AGGREGATE SHOULDER REMOVAL
- TURF RESTORATION
- PAVEMENT CORE SAMPLE

GENERAL NOTE:

CONTRACTOR TO RESTORE ANY DISTURBED AREAS TO PRE–CONSTRUCTION CONDITIONS.

20 0 20 40 60 FEET



KEY MAP

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184–003525

ILLINOIS
IOWA
WISCONSIN

PLOT DATE: 5/15/25

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

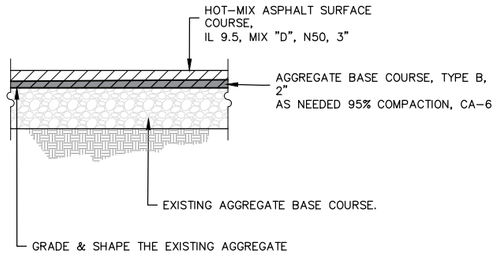
REV. NO.	DESCRIPTION	DATE

DRAWING:
TER MAAT COURT STA 700+00 TO STA 705+44

SET TYPE: PRELIMINARY
G:\C3D\25\25-694B\25-694 Design.dwg, P&P8

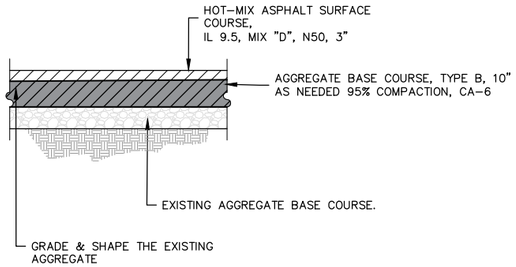
JOB NUMBER:
25–694

SHEET NUMBER:
21 of 24



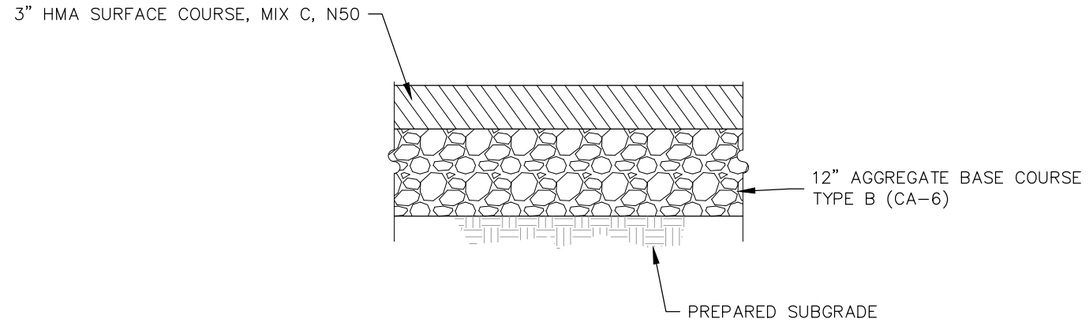
NOTE: PAVEMENT SECTION SHOULD COPY TYPICAL SECTION AT FRONT OF PLAN SET.

HMA PAVEMENT SECTION
N.T.S.



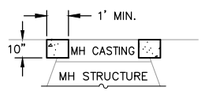
NOTE: PAVEMENT SECTION SHOULD COPY TYPICAL SECTION AT FRONT OF PLAN SET.

PARKWAY DRIVE WIDENING & TURNAROUND HMA PAVEMENT SECTION
N.T.S.

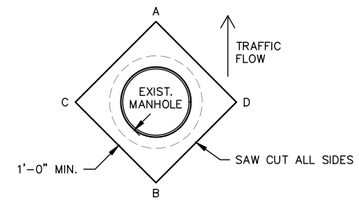


NOTE: AGGREGATE BASE SHALL BE CONSIDERED INCIDENTAL TO HMA DRIVEWAY PAVEMENT, VARIABLE DEPTH

HMA DRIVEWAY PAVEMENT, VARIABLE DEPTH DETAIL
N.T.S.



SECTION



NOTE: THE CONTRACTOR SHALL MARK THE LOCATION OF EACH MANHOLE PRIOR TO REMOVING THE CASTING, SO THAT IT CAN BE RELOCATED AFTER PAVING OPERATIONS HAVE BEEN COMPLETED.

MANHOLE CASTING SHALL BE REMOVED PRIOR TO GRINDING OPERATIONS ALONG WITH ANY DEFECTIVE ADJUSTMENT RINGS. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE EXISTING MANHOLE CASTING PRIOR TO BEING REPLACED. IF A BROKEN CASTING IS FOUND DURING THE REMOVAL PROCESS IT SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER TO DECIDE IF IT NEEDS TO BE REPLACED.

ONCE THE MANHOLE CASTING AND ANY BROKEN ADJUSTMENT RINGS HAVE BEEN REMOVED, A STEEL PLATE SHALL BE PLACED OVER THE MANHOLE OPENING TO PREVENT DEBRIS FROM GETTING INTO THE MANHOLE STRUCTURE. THE CONTRACTOR IS RESPONSIBLE FOR PREVENTING DEBRIS FROM BEING DEPOSITED IN THE STRUCTURE.

A STEEL PLATE SHALL BE USED TO COVER THE OPENING OF THE MANHOLE AND AGGREGATE BASE COURSE PLACED OVER TOP TO MATCH EXISTING ROAD SURFACE SO THAT THE OPENING DOES NOT CREATE A HAZARD FOR THE MOTORING PUBLIC.

ONCE THE GRINDING AND PAVING OPERATIONS HAVE BEEN COMPLETED, THE CONTRACTOR SHALL RELOCATE THE CENTER OF EACH MANHOLE CASTING AND SAW CUT A SQUARE OPENING SET AT A DIAGONAL, WITH POINTS OF DIAGONAL ABOUT 1.5' FROM EDGE OF MANHOLE CASTING.

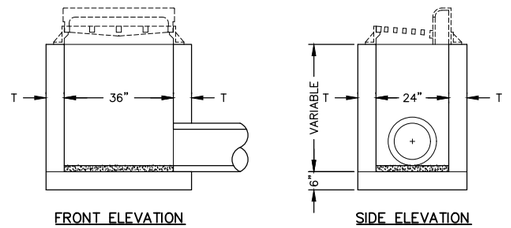
REMOVE THE PAVEMENT MATERIAL AND ADJUST MANHOLE TO WITHIN 1/4" OF TOP OF NEW PAVEMENT USING ADJUSTMENT RINGS AND MANHOLE CASTINGS, MAKING SURE THE CORNERS OF THE DIAGONAL HAVE BEEN CLEANED AND HAVE A VERTICAL EDGE.

FILL THE VOID WITH MINIMUM OF 10" OF CONCRETE. THIS WORK SHALL MEET THE REQUIREMENTS OF ARTICLE 442.02 NOTE #1 AND ARTICLE 701.17 (E)(2) AND (3) OF "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", LATEST EDITION, AND BE VIBRATED DURING INSTALLATION.

TROWEL OFF CONCRETE FLUSH WITH NEW PAVEMENT AND BROOM FINISH WITH DIRECTION OF TRAFFIC FLOW.

ONCE MANHOLES HAVE BEEN ADJUSTED, THE CONTRACTOR SHALL OPEN EACH MANHOLE TO CLEAN OUT ANY DEBRIS.

MANHOLE ADJUSTMENT DETAIL
N.T.S.



FRONT ELEVATION

SIDE ELEVATION

MATERIALS	T
CONCRETE MASONRY UNITS	5"
BUILDING BRICK GRADE S.W. FROM CLAY OR SHALE	8"
MONOLITHIC CONCRETE	6"
CONCRETE BUILDING BRICK GRADE A	8"

NOTE: THE BOTTOM SHALL BE CONSTRUCTED WITH CLASS SI CONCRETE.

INLET BOTTOM SHALL BE SLOPED 1" PER FOOT TO OUTLET PIPE.

THE INLET SHALL BE FURNISHED WITH A FRAME AND GRATE CURB TYPE NEENAH R-3246™ OR APPROVED EQUAL; OR FOR DRIVEWAY INSTALLATION NEENAH R-3290-A™ OR APPROVED EQUAL.

COST OF FURNISHING AND SETTING TO BE INCLUDED IN THE CONTRACT UNIT PRICE FOR INLET.

2' x 3' INLET DETAIL
N.T.S.

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

REV. NO.	DESCRIPTION	DATE

DRAWING: CIVIL DETAILS

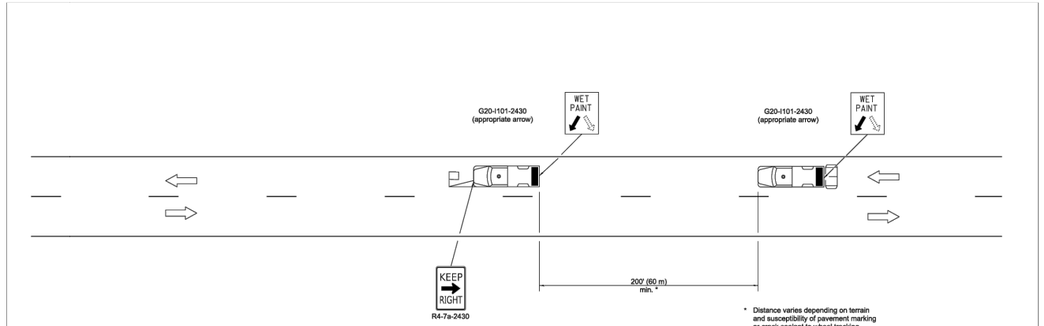
SET TYPE: PRELIMINARY

G:\C30\25\25-6948\Plans\25-694 Plans.dwg, Civil Details

JOB NUMBER: 25-694

SHEET NUMBER: 22 of 24

PLOT DATE: 5/15/25



TYPICAL APPLICATIONS

Landscaping work
Utility work
Pavement marking
Weed spraying
Roadometer measurements
Debris cleanup
Crack pouring

SYMBOLS

Arrow board (Hazard Mode only)

Truck with headlights, emergency flashers and flashing amber light. (visible from all directions)

18 x 18 (450x450) min. orange flag (use when guide wheel is used)

Truck mounted attenuator

GENERAL NOTES

This Standard is used where any vehicle, equipment, workers or their activities will require a continuous moving operation where the average speed is greater than 3 mph (5 km/h).

For shoulder operations not encroaching on the pavement, use DETAIL A, Standard 701426.

All dimensions are in inches (millimeters) unless otherwise shown.

Illinois Department of Transportation

APPROVED January 1, 2009
ENGINEER OF OPERATIONS

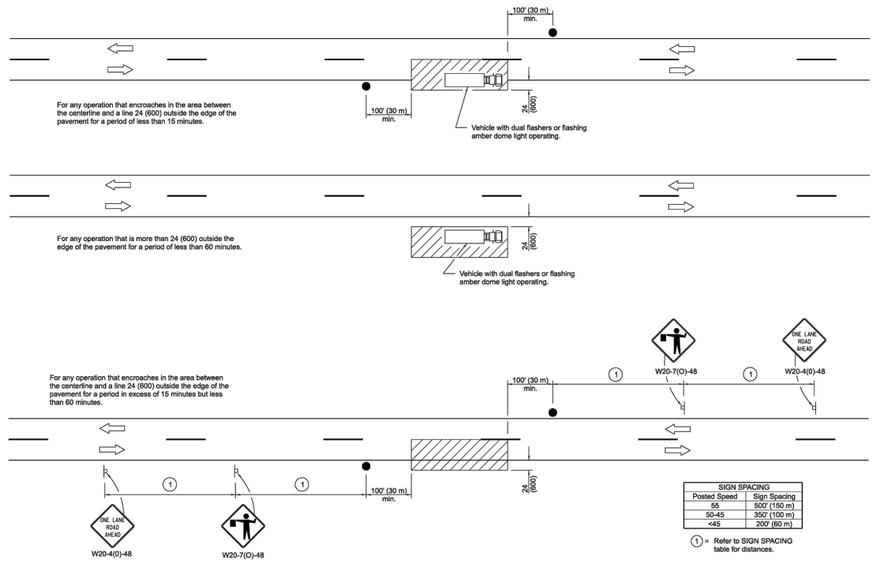
APPROVED January 1, 2009
ENGINEER OF DESIGN AND ENVIRONMENT

BDE 801 (D) - 1 of 1

DATE	REVISIONS
1-1-09	Switched units to English (metric).
	Omitted Pass With Care sign.
1-1-00	Eliminated speed restrictions
	in Standard title.

LANE CLOSURE 2L, 2W MOVING OPERATIONS- DAY ONLY

STANDARD 701311-03



SIGN SPACING	
Posted Speed	Sign Spacing
55	500' (150 m)
50-45	350' (100 m)
<45	200' (60 m)

TYPICAL APPLICATIONS

Marking patches
Field survey
String line
Utility operations
Cleaning up debris on pavement

SYMBOLS

Work area

Sign on portable or permanent support

Flagger with traffic control sign

All dimensions are in inches (millimeters) unless otherwise shown.

Illinois Department of Transportation

APPROVED January 1, 2011
ENGINEER OF SAFETY ENGINEERING

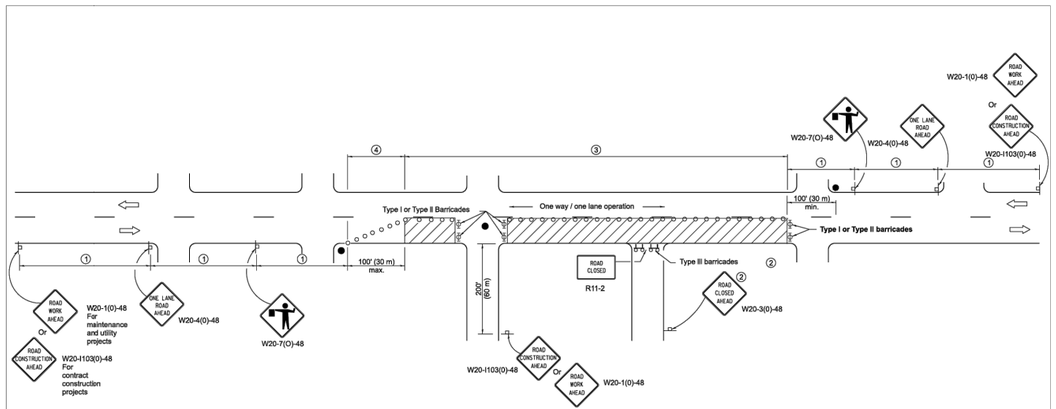
APPROVED January 1, 2011
ENGINEER OF DESIGN AND ENVIRONMENT

BDE 801 (D) - 1 of 1

DATE	REVISIONS
1-1-11	Revised flagger sign.
1-1-09	Switched units to English (metric).

LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS

STANDARD 701301-04



SIGN SPACING	
Posted Speed	Sign Spacing
55	500' (150 m)
50-45	350' (100 m)
<45	200' (60 m)

SYMBOLS

Work area

Cone, drum or barricade (not required for moving operations)

Sign on portable or permanent support

Flagger with traffic control sign

Barricade or drum with flashing light

Type III barricade with flashing lights

1. Refer to SIGN SPACING TABLE for distances.
2. For approved sideroad closures.
3. Cones at 25' (8 m) centers for 250' (75 m). Additional cones may be placed at 50' (15 m) centers. When drums or Type I or Type II barricades are used, the interval between devices may be doubled.
4. Cones, drums or barricades at 20' (6 m) centers.

GENERAL NOTES

This Standard is used where at any time, day or night, any vehicle, equipment, workers or their activities encroach on the pavement requiring the closure of one traffic lane in an urban area.

All dimensions are in inches (millimeters) unless otherwise shown.

Illinois Department of Transportation

APPROVED January 1, 2011
ENGINEER OF SAFETY ENGINEERING

APPROVED January 1, 2011
ENGINEER OF DESIGN AND ENVIRONMENT

BDE 801 (D) - 1 of 1

DATE	REVISIONS
1-1-11	Revised flagger sign.
1-1-09	Switched units to English (metric).
	Corrected sign No.'s.

URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED

STANDARD 701501-06

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL
STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

REV. NO.	DESCRIPTION	DATE

DRAWING: IDOT DETAILS

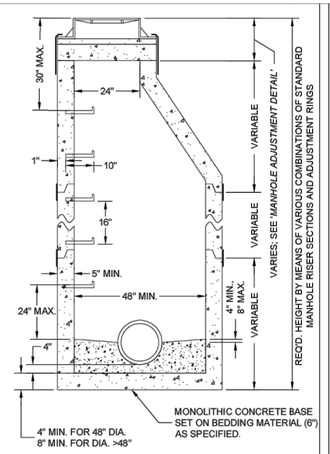
SET TYPE: PRELIMINARY
G:\C30\25\25-6948\Plans\25-694 Plans.dwg, IDOT Details

JOB NUMBER: 25-694

SHEET NUMBER: 23 of 24

PLOT DATE: 5/15/25

NOTES:
1. THE MAX. DROP FROM THE INVERT OF ANY PIPE TO THE CONCRETE CHANNEL UNDER THAT PIPE SHALL BE 8".
2. MANHOLES LOCATED OUTSIDE OF PUBLIC RIGHT-OF-WAY SHALL BE MARKED WITH A STEEL FENCE POST AS DIRECTED.
3. ALL NEW MANHOLES SHALL BE VACUUM TESTED PER A.S.T.M. C-1244 PRIOR TO ACCEPTANCE.
4. ALL BARREL JOINTS SHALL BE SEALED WITH 3½" x ⅜" PRE-FORMED RUBBER BUTYL JOINT SEALANT ON THE LOWER SHIPLAP.
5. ALL BARREL JOINTS SHALL BE SEALED WITH AN EXTERNAL BARREL SEAL CENTERED ON THE JOINT (MAR MAC MACWRAP, OR APPROVED EQUAL)
6. MANHOLE STRUCTURE SHALL BE CONSTRUCTED OF PRECAST REINFORCED CONCRETE MANHOLE RISER SECTIONS IN ACCORDANCE WITH A.S.T.M. C478-90 OR THE LATEST DESIGNATION.
7. PRECAST FLAT TOPS ARE NOT APPROVED FOR USE.
8. SEE *'MANHOLE ADJUSTMENT DETAIL'* FOR ADJUSTMENT REQUIREMENTS.
9. PIPE CONNECTIONS TO NEW MANHOLES SHALL BE MADE BY MEANS OF EITHER RUBBER GASKET SEAL (A-LOK OR APPROVED EQUAL) CONFORMING TO ASTM C-923 CAST INTEGRALLY IN MANHOLE WALL, OR RUBBER GASKET SEAL AND STAINLESS STEEL CLAMP (PSX SERIES SIX OR APPROVED EQUAL) CONFORMING TO ASTM C-923. FOR PIPE CONNECTIONS WITH A DEPTH OF >20 FT., A RUBBER GASKET SEAL (A-LOK OR APPROVED EQUAL) CONFORMING TO ASTM C-923 CAST INTEGRALLY IN MANHOLE WALL SHALL BE USED.
10. PIPE CONNECTIONS TO EXISTING MANHOLES SHALL BE MADE BY MEANS OF CORE DRILLING MANHOLE WALL AND INSTALLING RUBBER GASKET SEAL AND STAINLESS STEEL CLAMP (PSX SERIES SIX OR APPROVED EQUAL) CONFORMING TO ASTM C-923.
11. THE MAXIMUM DISTANCE FROM ANY INLET PIPE INVERT TO THE OUTLET PIPE INVERT SHALL BE 2'. DISTANCES GREATER THAN 2' WILL REQUIRE AN INSIDE DROP CONNECTION PER *'INSIDE DROP CONNECTION DETAIL'.*
12. MANHOLE STEPS SHALL BE NEENAH R-1982-F OR M.A. IND. PS-1 OR APPROVED EQUAL INSTALLED AT 16" CENTERS, ORIENTED ABOVE THE OUTLET PIPE UNLESS OTHERWISE SPECIFIED. FOR MANHOLES WITH INSIDE DROP ASSEMBLIES, STEPS IN THE MANHOLE BASE SECTION SHALL BE INSTALLED IN THE FIELD AND NOT CAST IN PLACE. IN THIS CASE, THE ORIENTATION OF THE CONE SHALL BE AS DIRECTED BY FRSA.
13. THE TOP HALF OF INLET & OUTLET PIPES SHALL BE CUT FLUSH WITH THE MANHOLE WALL. HYDRAULIC CEMENT (XYPEX PATCH N PLUG OR APPROVED EQUAL) SHALL BE PLACED IN THE ENTIRE VOID BETWEEN PIPES AND MANHOLE OPENINGS.



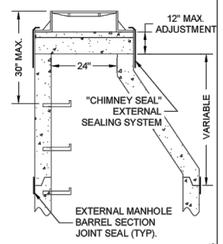
STANDARD MANHOLE DETAIL
('*MANHOLE ADJUSTMENT DETAIL*' SHALL APPLY)

rev. 5/1/23

NOTES:
1. MANHOLE FRAMES & LIDS SHALL BE PER THE TABLE BELOW.
2. FOR MANHOLES CONNECTED TO MAINS 18" DIAMETER OR LARGER, OR FOR MANHOLES LOCATED IN FLOOD PRONE AREAS, FRAMES & LIDS SHALL BE THE BOLT DOWN TYPE.
3. ALLOWABLE TYPES OF ADJUSTING RINGS INCLUDE PRECAST CONCRETE (4" HEIGHT MIN.), & EXPANDED POLYPROPYLENE (EPP). THESE CAN BE USED IN CONJUNCTION WITH EACH OTHER, EXCEPT THAT A PRECAST RING SHALL NOT BE PLACED OVER AN EPP RING.
4. FOR PRECAST ADJUSTING RINGS, ALL ADJUSTING RING JOINTS AS WELL AS THE FRAME TO ADJUSTING RING JOINT SHALL BE SEALED WITH TWO 1" BEADS OF PRE-FORMED RUBBER BUTYL JOINT SEALANT. WHEN A FRAME REQUIRES PITCHING, EPP TAPER RINGS SHALL BE USED PER NOTE 5.
5. FOR EPP ADJUSTING RINGS, RINGS SHALL BE INSTALLED ACCORDING TO MANUFACTURER'S INSTRUCTIONS. WHEN A FRAME REQUIRES PITCHING, THE TOP RING SHALL BE A TAPERED ADJUSTMENT RING PER MANUFACTURER'S INSTRUCTIONS.
6. NO TARRING OR GROUTING IS ALLOWED ON THE INSIDE OF MANHOLE OR ADJUSTMENT JOINTS.
7. MAXIMUM MANHOLE ADJUSTMENT IS 12". MINIMUM ADJUSTMENT IS 4" UNLESS OFF-ROAD OR IN CURB & GUTTER ROADWAY.
8. MANHOLE FRAMES SHALL BE SET ¼" MIN. TO ⅝" MAX. BELOW PAVED SURFACES, AND AT FINAL GRADE IN TURF AREAS.
9. WHEN ADJUSTING EXISTING MANHOLES, THE ENTIRE EXISTING ADJUSTMENT SHALL BE REMOVED AND REPLACED.
10. THE COMBINATION OF NEW ADJUSTING RINGS SHALL BE SUCH THAT THE MINIMUM NUMBER OF RINGS POSSIBLE ARE USED.
11. FLAT-TOPS ARE NOT PERMITTED ON 4' OR 5' DIA. MANHOLES.
12. REPLACEMENT OF EXISTING BARREL SECTIONS MAY BE REQUIRED TO MEET THE ABOVE REQUIREMENTS.
13. MANHOLE ADJUSTMENT INSERT RISER RINGS ARE NOT APPROVED FOR USE.

APPROVED FRAME & LID TABLE

TYPE	NEENAH FRAME	NEENAH LID	EAST JORDAN FRAME	EAST JORDAN LID
REGULAR	1670-2004	R-1670-0358	00111711	00111732
LOW PROFILE	1670-2008	R-1670-0358	---	---
BOLT DOWN	1915JT08		---	---



MANHOLE ADJUSTMENT DETAIL
(FOR ADJUSTMENT OF BOTH NEW & EXISTING MANHOLES)

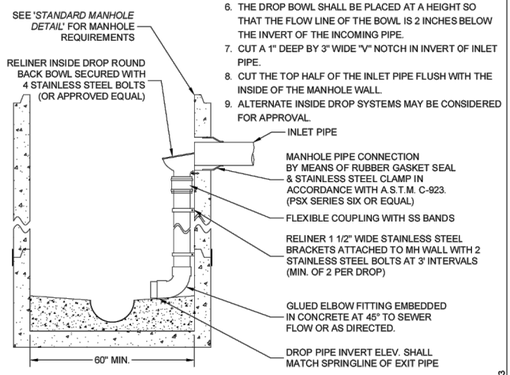
rev. 5/1/23

DROP BOWL & DROP PIPE SIZING TABLE

INLET PIPE DIA.	DROP PIPE DIA. (MIN.)
4-6 INCH	4 INCH
8 INCH	6 INCH
10 INCH	8 INCH
>10 INCH	*

* PER MFG. OR AS DIRECTED BY FRSA

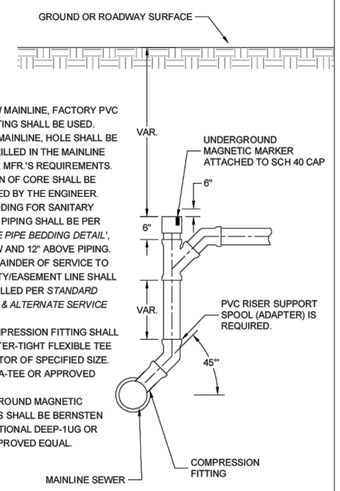
NOTES:
1. INSIDE DROP TYPE MANHOLES SHALL BE 5' MIN. INSIDE DIA.
2. ALL INSIDE DROP CONNECTIONS FOR SERVICES AND COLLECTOR SEWERS SHALL USE THE DROP AS MFG. BY RELINER-DURAN INC., OR EQUAL.
3. SEE TABLE FOR DROP BOWL AND DROP PIPE SIZES.
4. ALL INSIDE DROP PIPING SHALL BE PVC SDR35 ASTM-D3034.
5. ATTACH THE ROUND BACK DROP BOWL AND EACH CLAMPING BRACKET TO THE MANHOLE WALL WITH ⅜" x 1" MIN. STAINLESS STEEL BOLTS AND EPOXY IMPREGNATED LUGS PER MFR.'S RECOMMENDATIONS.
6. THE DROP BOWL SHALL BE PLACED AT A HEIGHT SO THAT THE FLOW LINE OF THE BOWL IS 2 INCHES BELOW THE INVERT OF THE INCOMING PIPE.
7. CUT A 1" DEEP BY 3" WIDE "V" NOTCH IN INVERT OF INLET PIPE.
8. CUT THE TOP HALF OF THE INLET PIPE FLUSH WITH THE INSIDE OF THE MANHOLE WALL.
9. ALTERNATE INSIDE DROP SYSTEMS MAY BE CONSIDERED FOR APPROVAL.



INSIDE DROP CONNECTION DETAIL
('*STANDARD MANHOLE DETAIL*' SHALL APPLY)

rev. 5/1/23

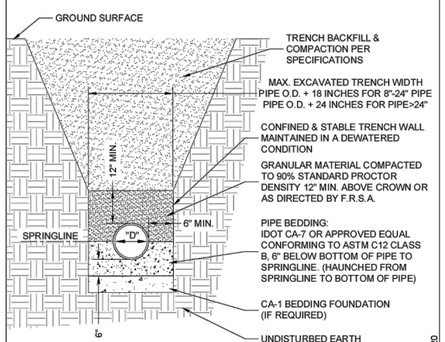
NOTES:
1. FOR NEW MAINLINE, FACTORY PVC WYE FITTING SHALL BE USED.
2. FOR EX. MAINLINE, HOLE SHALL BE CORE DRILLED IN THE MAINLINE PIPE PER MFR.'S REQUIREMENTS. LOCATION OF CORE SHALL BE APPROVED BY THE ENGINEER.
3. PIPE BEDDING FOR SANITARY SERVICE PIPING SHALL BE PER *'FLEXIBLE PIPE BEDDING DETAIL'*, 6" BELOW AND 12" ABOVE PIPING.
4. THE REMAINDER OF SERVICE TO PROPERTY/EASEMENT LINE SHALL BE INSTALLED PER *STANDARD SERVICE & ALTERNATE SERVICE DETAIL.*
5. THE COMPRESSION FITTING SHALL BE A WATER-TIGHT FLEXIBLE TEE CONNECTOR OF SPECIFIED SIZE. (INSERT-A-TEE OR APPROVED EQUAL).
6. UNDERGROUND MAGNETIC MARKERS SHALL BE BERNSTEN INTERNATIONAL DEEP-1UG OR FRSA APPROVED EQUAL.



VERTICAL SERVICE RISER DETAIL
(FOR MAINLINE DIA. 8" - 18"; CONNECTION TO >18" MAIN PROHIBITED)

rev. 5/1/23

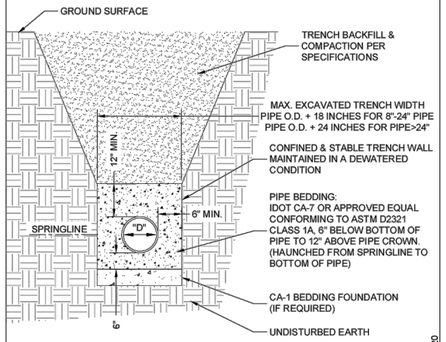
NOTES:
1. REASONABLE CARE SHALL BE USED WHEN BACKFILLING OVER SEWER. NO MATERIALS SUCH AS ROCKS OR BOULDERS SHALL BE PLACED WITHIN 24" OF THE CROWN OF THE PIPE. NO MATERIAL LARGER THAN 8" DIA. SHALL BE USED IN THE BACKFILL.
2. LOOSE MATERIAL SHALL BE REMOVED OR COMPACTED PRIOR TO PLACING PIPE BEDDING.
3. BEDDING SHALL BE WELL HAUNCHED ALONG PIPE TO ENSURE VOIDS ARE ELIMINATED.



RIGID PIPE BEDDING DETAIL

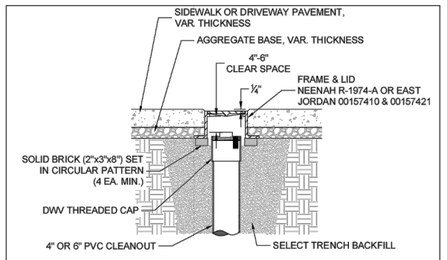
rev. 3/11/20

NOTES:
1. REASONABLE CARE SHALL BE USED WHEN BACKFILLING OVER SEWER. NO MATERIALS SUCH AS ROCKS OR BOULDERS SHALL BE PLACED WITHIN 24" OF THE CROWN OF THE PIPE. NO MATERIAL LARGER THAN 8" DIA. SHALL BE USED IN THE BACKFILL.
2. LOOSE MATERIAL TO BE REMOVED OR COMPACTED PRIOR TO PLACING PIPE BEDDING.
3. BEDDING SHALL BE WELL HAUNCHED ALONG PIPE TO ENSURE VOIDS ARE ELIMINATED.



FLEXIBLE PIPE BEDDING DETAIL

rev. 3/11/20

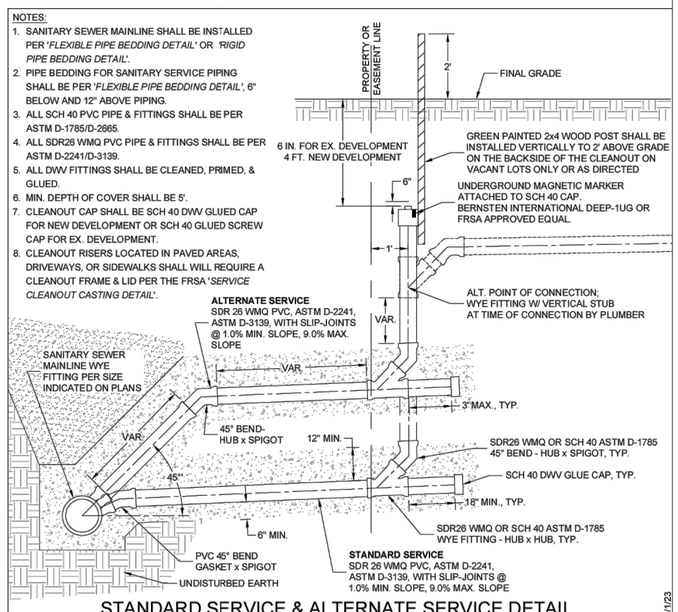


NOTES:
1. SERVICE CLEANOUT CASTINGS ARE REQUIRED FOR ALL CLEANOUTS LOCATED IN PAVED AREAS, DRIVEWAYS, OR SIDEWALKS.
2. THE LOCATION OF THE CLEANOUT SHALL BE APPROVED BY FOUR RIVERS SANITATION AUTHORITY (FRSA).
3. THE FRAME SHALL BE SET ON BRICKS THAT ARE PLACED IN A CIRCULAR PATTERN THE ENTIRE CIRCUMFERENCE OF THE FRAME. THE BRICKS SHALL BE SET ON COMPACTED TRENCH BACKFILL.
4. THE FRAME SHALL BE SET TO AN ELEVATION THAT PROVIDES 4"-6" CLEAR SPACE BETWEEN THE TOP OF THE CLEANOUT CAP AND THE BOTTOM OF THE CASTING LID.
5. THE CONTRACTOR SHALL ENSURE THAT THE CLEANOUT CAP CAN BE UNSCREWED AND REMOVED AND REPLACED WITHOUT HINDRANCE.
6. THE FRAME SHALL BE SET ¼" MIN. TO ⅜" MAX. BELOW FINAL PAVEMENT ELEVATION.

SERVICE CLEANOUT CASTING DETAIL

rev. 6/1/22

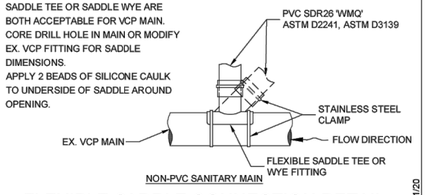
NOTES:
1. SANITARY SEWER MAINLINE SHALL BE INSTALLED PER *'FLEXIBLE PIPE BEDDING DETAIL'* OR *'RIGID PIPE BEDDING DETAIL'.*
2. PIPE BEDDING FOR SANITARY SERVICE PIPING SHALL BE PER *'FLEXIBLE PIPE BEDDING DETAIL'*, 6" BELOW AND 12" ABOVE PIPING.
3. ALL SCH 40 PVC PIPE & FITTINGS SHALL BE PER ASTM D-1785/D-2665.
4. ALL SDR26 WMQ PVC PIPE & FITTINGS SHALL BE PER ASTM D-2241/D-3139.
5. ALL DWV FITTINGS SHALL BE CLEANED, PRIMED, & GLUED.
6. MIN. DEPTH OF COVER SHALL BE 5'.
7. CLEANOUT CAP SHALL BE SCH 40 DWV GLUED CAP FOR NEW DEVELOPMENT OR SCH 40 GLUED SCREW CAP FOR EX. DEVELOPMENT.
8. CLEANOUT RISERS LOCATED IN PAVED AREAS, DRIVEWAYS, OR SIDEWALKS SHALL WILL REQUIRE A CLEANOUT FRAME & LID PER THE FRSA *'SERVICE CLEANOUT CASTING DETAIL'.*



STANDARD SERVICE & ALTERNATE SERVICE DETAIL
(FOR MAINLINE DIA. 8" - 18"; CONNECTION TO >18" MAIN PROHIBITED)

rev. 5/1/23

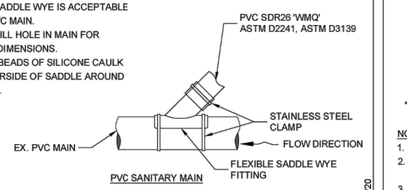
NOTES:
1. SADDLE TEE OR SADDLE WYE ARE BOTH ACCEPTABLE FOR VCP MAIN.
2. CORE DRILL HOLE IN MAIN OR MODIFY EX. VCP FITTING FOR SADDLE DIMENSIONS.
3. APPLY 2 BEADS OF SILICONE CAULK TO UNDERSIDE OF SADDLE AROUND OPENING.



FLEXIBLE SADDLE CONNECTION DETAIL
(FOR SERVICE CONNECTIONS TO NON-PVC SANITARY MAINS LESS THAN 18" DIA.)

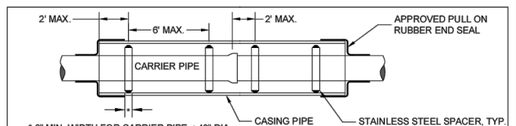
rev. 3/11/20

NOTES:
1. ONLY A SADDLE WYE IS ACCEPTABLE FOR A PVC MAIN.
2. CORE DRILL HOLE IN MAIN FOR SADDLE DIMENSIONS.
3. APPLY 2 BEADS OF SILICONE CAULK TO UNDERSIDE OF SADDLE AROUND OPENING.



FLEXIBLE SADDLE CONNECTION DETAIL
(FOR SERVICE CONNECTIONS TO PVC SANITARY MAINS LESS THAN 18" DIA.)

rev. 3/11/20



* 8" MIN. WIDTH FOR CARRIER PIPE < 48" DIA.
12" MIN. WIDTH FOR CARRIER PIPE ≥ 48" DIA.

NOTES:
1. CASING END SEALS & SPACERS SHALL BE AS MFD. BY CASCADE MFG., OR APPROVED EQUAL.
2. FOR FLEXIBLE CARRIER PIPE, SPACING OF SPACERS TO BE AS SHOWN, OR PER MFR.'S RECOMMENDATION.
3. FOR RIGID CARRIER PIPE, SPACING SHALL BE PER MFR.'S RECOMMENDATION.

CASING & SPACER DETAIL

rev. 5/15/18

FOUR RIVERS SANITATION AUTHORITY
(FRSA) STANDARD DETAIL SHEET
(NOT TO SCALE)
3501 KISHWAUKEE ST.
ROCKFORD, ILLINOIS 61109 PH. (815) 387-7660

ISSUE 05/01/2023

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, ILLINOIS 61073

PROJECT AND LOCATION:
ROSCOE 2025 RESIDENTIAL STREET PROGRAM
ROSCOE, ILLINOIS

DRAWN BY: JB
APPROVED BY: BB
DATE: 05/15/2025
SCALE: AS NOTED

REVISIONS

REV. NO.	DESCRIPTION	DATE

DRAWING: FRSA DETAILS
SET TYPE: PRELIMINARY
G:\C30\25\25-6948\Plans\25-694 Plans.dwg, FRSA Details

JOB NUMBER: 25-694
SHEET NUMBER: 24 of 24

PLOT DATE: 5/15/25