

MAIN STREET
PEDESTRIAN CROSSING
FOR
VILLAGE OF ROSCOE
ROSCOE, ILLINOIS

PROPOSED PEDESTRIAN CROSSING
BRIDGE STREET AND MAIN STREET
WINNEBAGO COUNTY

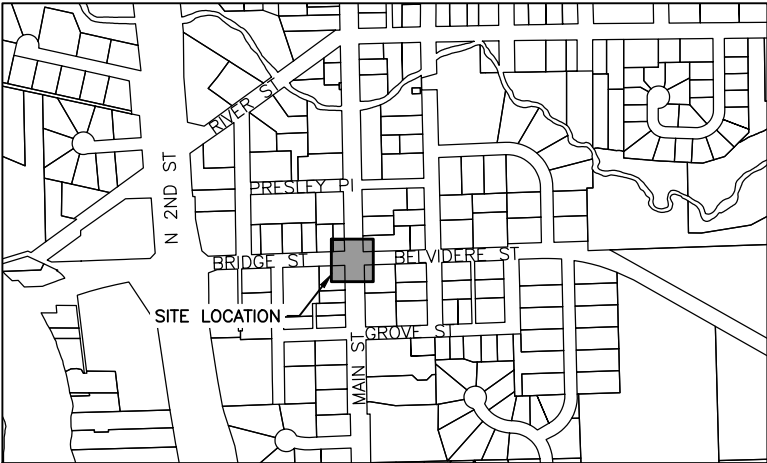
SEPTEMBER 2025



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UTILITIES	
UTILITY TYPE	COMMON NAME
WATER	NORTH PARK PUBLIC WATER
SEWER	FOUR RIVERS SANITATION AUTHORITY
ELECTRIC	COMMONWEALTH EDISON
TELEPHONE	CHARTER COMMUNICATIONS
GAS	NICOR
CABLE	CHARTER COMMUNICATIONS

(CONTRACTOR TO BE RESPONSIBLE FOR COORDINATING ANY ADJUSTMENTS TO BE MADE.)



LOCATION MAP



FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL

ILLINOIS

IOWA

WISCONSIN

ILLINOIS PROFESSIONAL DESIGN FIRM NUMBER: 184003525



9/8/2025
SIGNATURE DATE

FOR BID



ORIGINAL SET FOR PROJECT: 25-934		DATE CREATED: 9/8/2025
REVISIONS		
REV. NO.	DESCRIPTION	DATE

GENERAL NOTES

1. THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE MUNICIPAL CODE, CITY OF VILLAGE OF ROSCOE, ILLINOIS, CURRENT EDITION, THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", CURRENT EDITION, "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS," CURRENT EDITION, SPECIAL PROVISIONS AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", CURRENT EDITION. SIGN CONSTRUCTION AND PAVEMENT MARKINGS SHALL CONFORM TO THE REQUIREMENTS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", CURRENT EDITION.
2. IN THESE CONTRACT DOCUMENTS MENTION IS MADE OF THE "ENGINEER", WHICH SHALL MEAN FEHR GRAHAM OR THEIR DULY AUTHORIZED AGENT. IN THESE CONTRACT DOCUMENTS MENTION IS MADE OF THE "OWNER", WHICH SHALL MEAN VILLAGE OF ROSCOE, OR THEIR DULY AWARDED AGENT.
3. AS PART OF THE BIDDING PROCEDURE, THE CONTRACTOR SHALL VERIFY THAT THE QUANTITIES FOR PAY ITEMS, AS PRESENTED IN THESE PLAN DOCUMENTS, ARE SUBSTANTIALLY CORRECT. IF DISCREPANCIES ARE DETECTED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF THE DISCREPANCY PRIOR TO THE BID DATE.
4. QUANTITIES SHOWN ARE ESTIMATES FOR INFORMATION ONLY. PAYMENT WILL BE BASED ON ACTUAL QUANTITIES MEASURED IN THE FIELD OR ON PAYMENT LIMIT DETAILS.
5. THE CONTRACTOR SHALL BE PAID FOR MATERIALS AND EQUIPMENT SUCCESSFULLY INSTALLED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS AS MEASURED OR VERIFIED IN PLACE BY THE ENGINEER OR HIS AGENT.
6. IN CASE OF CONFLICT BETWEEN THE ABOVE MENTIONED SPECIFICATIONS, THE ENGINEER SHALL DETERMINE WHICH OF THE SPECIFICATIONS SHALL GOVERN. THE ENGINEER'S DECISION SHALL BE FINAL AND NO ADDITIONAL COMPENSATION SHALL BE AWARDED UNLESS APPROVED BY THE ENGINEER.
7. THE PROPOSED IMPROVEMENTS MUST BE CONSTRUCTED IN ACCORDANCE WITH THE ENGINEERING PLANS AS APPROVED BY THE OWNER. IMPROVEMENT REPRESENTATIONS AS SHOWN ON THESE PLANS, ARE AS ACCURATE AS POSSIBLE FROM THE INFORMATION AVAILABLE. HOWEVER SOME FIELD REVISIONS MAY BE REQUIRED TO ACCOMMODATE UNFORESEEN CIRCUMSTANCES. THE ENGINEER SHALL BE ADVISED OF ANY NECESSARY REVISIONS WITH SUFFICIENT LEAD TIME ALLOWED TO PROPERLY CONSIDER AND ACT UPON SAID REQUESTS. PROPER CONSTRUCTION TECHNIQUES MUST BE FOLLOWED IN CONSTRUCTING THOSE IMPROVEMENTS AS DETAILED IN THIS ENGINEERING PLAN.
8. THE ENGINEER SHALL HAVE THE AUTHORITY TO INSPECT, APPROVE OR REJECT THE WORKMANSHIP AND/OR MATERIALS WHICH GO TO MAKE UP IMPROVEMENTS AS DETAILED IN THESE PLANS AND SPECIFICATIONS.
9. GENERAL SAFETY PROVISION: TO PROVIDE DRIVERS WITH SAFE TRAVEL CONDITIONS DURING THE CONSTRUCTION PROJECT, AND TO PROVIDE SAFE WORKING CONDITIONS FOR ALL EMPLOYEES, THE RULES, REGULATIONS, AND CONDITIONS STATED BELOW WILL PREVAIL FOR THE DURATION OF THIS CONTRACT. ANY EMPLOYEE OF THE CONTRACTOR OR HIS SUBCONTRACTORS WHO REFUSES TO COMPLY WITH THESE GENERAL SAFETY PROVISIONS SHALL BE REMOVED FROM THE JOB SITE IN ACCORDANCE WITH STATE AND LOCAL REQUIREMENTS. THE CONTRACTOR AND ANY SUBCONTRACTORS RETAINED BY HIM SHALL COMPLY WITH THE STATE AND FEDERAL REQUIREMENTS OF THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970 (OSHA), JULY 1, 1987 AS IT RELATES TO CONTRACTOR'S OPERATIONS.
10. THE CONTRACTOR SHALL COMPLY WITH ALL STATE REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION. THE CONTRACTOR WILL NOT BE ALLOWED TO BUILD FIRES ON THE SITE.
11. THE SCALE SHOWN ON THE DRAWINGS APPLIES ONLY TO THE FULL SIZE PLANS NOT THE REDUCED SIZE PLANS.
12. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN DRAINAGE FLOWS AT ALL TIMES DURING THE PERFORMANCE OF THE WORK. METHODS USED BY THE CONTRACTOR SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. COST OF MAINTAINING DRAINAGE FLOWS SHALL BE INCIDENTAL TO THE CONTRACT.
13. WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED OR DISTURBED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS, MONUMENTS AND RIGHT-OF-WAY PINS UNTIL THE OWNER, AND AUTHORIZED SURVEYOR, OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DESTROYED BY HIS OPERATIONS. REPLACEMENT OF MONUMENTS WILL BE DETERMINED BY THE ENGINEER.
14. THE CONTRACTOR SHALL REMOVE, STORE, AND RELOCATE TO THE SATISFACTION OF THE ENGINEER ALL EXISTING SIGNAGE IN ACCORDANCE WITH STATE AND LOCAL REQUIREMENTS, AND CONSIDER THIS AS INCIDENTAL TO THE CONTRACT.
15. OUTSIDE THE EXISTING RIGHT-OF-WAY, THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATION NEAR ANY AND ALL EXISTING SIGNS OUTSIDE THE RIGHT-OF-WAY. ANY SIGNS REMOVED FOR CONSTRUCTION PURPOSES SHALL BE CAREFULLY REMOVED AND RE-ERECTED BY THE CONTRACTOR AT A LOCATION NEAREST TO THE ORIGINAL LOCATION, OR AT A LOCATION DETERMINED BY THE ENGINEER IN THE FIELD. REMOVAL AND RE-ERECTED SIGNS AND ANY DAMAGE DONE TO EXISTING SIGNS BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL EXPENSE TO THE OWNER.
16. ALL ITEMS SHALL INCLUDE ALL THE NECESSARY MATERIALS AND LABOR TO COMPLETE THE ITEM IN PLACE. MATERIALS AND LABOR NOT SPECIFICALLY IDENTIFIED SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
17. AT THE END OF EACH DAY, THE CONTRACTOR SHALL SECURE THE CONSTRUCTION WORK ZONE FROM POTENTIAL INTRUDERS.
18. THE CONTRACTOR SHALL FIELD VERIFY THE ELEVATIONS OF THE BENCHMARKS PRIOR TO COMMENCING WORK. THE CONTRACTOR SHALL ALSO FIELD VERIFY LOCATION, ELEVATION AND SIZE OF EXISTING UTILITIES, AND VERIFY PAVEMENT ELEVATIONS WHERE MATCHING INTO EXISTING WORK. THE CONTRACTOR SHALL FIELD VERIFY HORIZONTAL CONTROL BY REFERENCING SHOWN COORDINATES TO KNOWN PROPERTY LINES. NOTIFY ENGINEER OF DISCREPANCIES IN EITHER VERTICAL OR HORIZONTAL CONTROL PRIOR TO PROCEEDING WITH WORK.
19. THE CONTRACTOR SHALL CONTACT THE ENGINEER OF ANY ERRORS OR DISCREPANCIES WHICH MAY BE SUSPECTED IN LINES AND GRADES, AND SHALL NOT PROCEED WITH THE WORK UNTIL ALL LINES AND GRADES WHICH ARE BELIEVED TO BE IN ERROR HAVE BEEN VERIFIED OR CORRECTED BY THE ENGINEER OR HIS REPRESENTATIVE.
20. THE ENGINEER AND OWNER ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCE OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF THEIR WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.
21. ALL ITEMS TO BE REMOVED AND NOT DEFINED AS A PAY ITEM SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
22. ALL EXCESS EARTH EXCAVATION, EXCESS MATERIALS, OR OTHER REMOVED ITEMS SHALL BE HAULED OFF-SITE AT THE CONTRACTOR'S EXPENSE, UNLESS OTHERWISE APPROVED BY THE OWNER.
23. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL OBSTRUCTIONS, TREES, DEBRIS AND BRUSH AS DESIGNATED BY THE OWNER AND AS INDICATED ON THE PLANS. THIS WORK SHALL BE IN ACCORDANCE WITH SECTION 201 OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", CURRENT EDITION. ALL MATERIALS SHALL BE DISPOSED OF AT THE CONTRACTOR'S EXPENSE. DURING CONSTRUCTION, CARE SHALL BE TAKEN TO MINIMIZE DAMAGE TO THE EXISTING TREES AND LANDSCAPING. ONLY THOSE ITEMS DESIGNATED BY THE OWNER SHALL BE REMOVED.
24. ALL ROADWAY REMOVAL ITEMS SHALL CONFORM TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", CURRENT EDITION. ALL JOINTS BETWEEN THE PORTION REMOVED AND THAT LEFT IN PLACE SHALL BE SAWED TO SUCH A DEPTH THAT A CLEAN, NEAT EDGE WILL RESULT WITH NO SPALLING TO THE REMAINING PORTION. THE COST OF SAWING SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. ADDITIONAL SAWING OR RE-SAWING MAY BE REQUIRED AS DIRECTED BY THE ENGINEER WITH NO ADDITIONAL COMPENSATION BEING ALLOWED. THE COST OF SAWCUTTING THE EXISTING PAVEMENT SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

GENERAL NOTES CONTINUED

25. WHEN ARTIFICIAL LIGHTING IS UTILIZED DURING NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC, AS WELL AS ADJOINING RESIDENTIAL AREAS.
26. THE CONTRACTOR IS REQUIRED TO STAY WITHIN THE NOTED PROPERTY BOUNDARIES RIGHT-OF-WAY AND EASEMENTS AS SHOWN IN THE PLANS. ANY ADDITIONAL EASEMENTS SHALL BE SECURED BY THE CONTRACTOR AT NO EXTRA COST.
27. ANY AREAS DAMAGED OR DISTURBED DURING THE PROJECT AS A DIRECT OR INDIRECT RESULT OF CONTRACTOR OPERATIONS, SHALL BE RESTORED TO A CONDITION EQUAL TO OR BETTER THAN THE ORIGINAL CONDITION. THE COST OF SAID RESTORATION OR REPAIR SHALL BE BORNE TOTALLY BY THE CONTRACTOR, WITH NO EXTRA COMPENSATION BEING AWARDED UNDER THIS CONTRACT. THE RESPONSIBILITY FOR THE REPAIR OR REPLACEMENT OF ANY UTILITY, STRUCTURE, LANDSCAPING, ETC., DAMAGED OR DESTROYED BY THE CONTRACTOR DURING MOBILIZATION OR CONSTRUCTION SHALL BE BORNE SOLELY BY THE CONTRACTOR, WITH NO EXPENSE BEING CHARGED TO THE ENGINEER OR OWNER. PRIOR TO ACCEPTANCE OF THIS REPAIR OR REPLACEMENT, THE CONTRACTOR SHALL PRESENT THE OWNER WITH A "SIGNOFF LETTER", SIGNED BY A RESPONSIBLE OFFICIAL OF THE OWNER OF THE DAMAGED UTILITY STATING THAT THE REPAIR OR REPLACEMENT IS ACCEPTABLE.
- CONSTRUCTION STAKING
1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING THE PROPOSED IMPROVEMENTS.
- EROSION CONTROL NOTES
1. UNLESS OTHERWISE SPECIFIED, ALL EROSION AND SEDIMENT CONTROL MEASURES AND THEIR MAINTENANCE, CLEARING AND REMOVAL SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION.
2. THIS WORK SHALL CONFORM TO THE APPLICABLE STANDARDS FROM THE ILLINOIS URBAN MANUAL, THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATION, CURRENT EDITION, THE PROJECT SPECIFICATIONS, AND THE APPROPRIATE DETAILS.
3. THE CONTRACTOR SHALL IMPLEMENT THE EROSION AND SEDIMENT CONTROL MEASURES AS INDICATED ON THESE EROSION CONTROL PLANS.
4. THE CONTROLS SHALL BE INSTALLED AS DETAILED AND WHERE INDICATED ON THE EROSION CONTROL PLAN SHEETS AND AS DIRECTED BY THE INSPECTOR.
5. SITE ACTIVITIES SHOULD ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE PRACTISCABLE.
6. DISTURBED PORTIONS OF THE SITE SHALL BE STABILIZED (TEMPORARILY OR PERMANENTLY SEEDED, MULCHED, SODDED OR PAVED) AS SOON AS PRACTICABLE, BUT IN NO CASE MORE THAN 7 CALENDAR DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
7. UNTIL SUCH TIME AS THE PROJECT SITE REACHES FINAL STABILIZATION THE CONTRACTOR SHALL BE RESPONSIBLE TO ADJUST, REPAIR, OR REPLACE, ALL VEGETATION, EROSION CONTROLS, SEDIMENT CONTROLS, AND ANY OTHER PROTECTIVE MEASURES AS REQUIRED IN ORDER TO MAINTAIN THEIR INTENDED FUNCTION IN A GOOD AND EFFECTIVE OPERATING CONDITION.
8. EXCEPT FOR FLOWS FROM FIRE FIGHTING ACTIVITIES, SOURCES OF NON-STORM WATER EXPECTED DURING THE CONSTRUCTION PROCESS THAT MAY BE COMBINED WITH STORM WATER DISCHARGES ARE:
- A. FIRE HYDRANT FLUSHING
- B. WATERS USED TO WASH VEHICLES (DETERGENTS ARE NOT TO BE USED)
- C. WATERS USED TO CONTROL DUST
- D. POTABLE WATER FROM WATER MAIN FLUSHING
- E. LANDSCAPE IRRIGATION DRAINAGE
- F. UNCONTAMINATED GROUND WATER FROM DEWATERING EXCAVATED TRENCHES
- G. PAVEMENT WASH WATERS WHERE SPILLS OR LEAKS OF TOXIC OR HAZARDOUS MATERIALS HAVE NOT OCCURRED (UNLESS ALL SPILLED MATERIAL HAS BEEN REMOVED) ALSO, DETERGENTS ARE NOT TO BE USED
- H. UNCONTAMINATED AIR CONDITIONING CONDENSATE
- I. THE ABOVE NON-STORM DISCHARGES SHALL BE DIRECTED AWAY FROM UNPROTECTED, BARE, OR OTHERWISE UNSTABILIZED SOIL. THE CONTRACTOR SHALL FURTHER IMPLEMENT APPROPRIATE POLLUTION PREVENTION MEASURES TO ENSURE THAT ANY OF THE ABOVE DISCHARGES DO NOT CAUSE EROSION OR DEGRADE THE QUALITY OF RUNOFF FROM THE CONSTRUCTION SITE.
9. THE OWNER SHALL HAVE AUTHORIZATION TO DETERMINE THE ADEQUACY OF THE CONTRACTOR'S EROSION CONTROL EFFORTS. THE OWNER SHALL HAVE FULL AUTHORITY OVER THE GENERAL CONTRACTOR AND ANY SUBCONTRACTOR TO CAUSE POLLUTANT CONTROL MEASURES TO BE REPAIRED, MODIFIED, MAINTAINED, SUPPLEMENTED, OR WHATEVER ELSE IS NECESSARY IN ORDER TO ACHIEVE EFFECTIVE POLLUTANT CONTROL OR TO SUSPEND OR LIMIT THE CONTRACTORS OPERATIONS PENDING ADEQUATE PERFORMANCE.

10. PERIMETER EROSION BARRIER TO BE CONSTRUCTED OF SILT FENCE UNLESS NOTED OTHERWISE.
11. INLET PROTECTION SHALL BE A DANDY BAG, DANDY SACK, ROCSOC, OR APPROVED EQUAL.
12. EROSION CONTROL BLANKET SHALL BE OF NORTH AMERICAN GREEN DS75 OR APPROVED EQUAL.
13. A TEMPORARY CONCRETE WASHOUT FACILITY SHALL BE CONSTRUCTED AT A LOCATION APPROVED BY THE ENGINEER. WASHOUT FACILITY SHALL BE UTILIZED FOR ALL APPLICABLE OPERATIONS.
14. STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED, TO THE DIMENSIONS AS SHOWN, AT APPROVED LOCATIONS FOR THIS PROJECT. ALL CONSTRUCTION TRAFFIC MUST UTILIZE THE STABILIZED CONSTRUCTION ENTRANCES WHEN EXITING THE SITE. ALL COST FOR EROSION CONTROL AND RESTORATION WORK ASSOCIATED WITH THE APPROVED STABILIZED CONSTRUCTION ENTRANCES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
15. TEMPORARY EROSION CONTROL MEASURES INCLUDE TEMPORARY DITCH CHECKS, PERIMETER EROSION BARRIER, INLET AND PIPE PROTECTION, TEMPORARY SEEDING, AND ANY OTHER TEMPORARY EROSION CONTROL MEASURE NEEDED TO LIMIT THE AMOUNT OF SOIL EROSION AND SEDIMENTATION DURING CONSTRUCTION.
16. AT THE COMPLETION OF THE PROJECT, ALL TEMPORARY EROSION CONTROL ITEMS SHALL BE REMOVED FROM THE SITE, AND BECOME THE PROPERTY OF THE CONTRACTOR. CONTRACTOR MUST STABILIZE ANY AREA DISTURBED BY THE REMOVAL OF EROSION CONTROL ITEMS.
17. CONTRACTOR SHALL CLEAN ANY DEBRIS TRACKED OFFSITE DAILY.

SEEDING OF DISTURBED AREAS

1. THE FINAL TOP 6" INCHES OF SOIL IN ANY DISTURBANCE AREA MUST BE A COHESIVE SOIL CAPABLE OF SUPPORTING VEGETATION.
2. FERTILIZER HAVING AN ANALYSIS OF 10-10-10 SHALL BE APPLIED AT A RATE OF 90 LBS/ACRE TO ALL DISTURBED AREAS AND INCORPORATED INTO THE SEEDBED PRIOR TO SOWING THE SEED.
3. THE CONTRACTOR SHALL SEED AND STABILIZE ALL DISTURBED AREAS ADJACENT TO IMPROVEMENTS WITH SEEDING, IDOT CLASS 1A AND NAG DS75 EROSION CONTROL BLANKET OR APPROVED EQUAL IN ACCORDANCE WITH IDOT STANDARD SPECIFICATION OR AS APPROVED BY THE ENGINEER.

SEEDING OF DISTURBED AREAS CONTINUED

1. GUARANTEE: ALL SEEDED AREAS SHALL BE MAINTAINED AND MOWED FOR AT LEAST 30 DAYS AFTER GERMINATION. SCATTERED BARE SPOTS NO LARGER THAN TWO SQUARE FOOT WILL BE ALLOWED UP TO A MAXIMUM OF 5% OF ANY SEEDED AREA INCLUDING 30-DAY MAINTENANCE, MOWING AND WATERING AS NECESSARY.
2. THIS WORK SHALL CONFORM TO THE APPLICABLE STANDARDS FROM THE ILLINOIS URBAN MANUAL, THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATION SECTIONS, CURRENT EDITION, THE PROJECT SPECIFICATIONS, AND THE APPROPRIATE DETAILS.
3. RESTORATION – THE CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED DURING CONSTRUCTION OF THE IMPROVEMENTS AND RELATED APPURTENANCES OR AS PART OF ANY OF THEIR ACTIVITIES TO A CONDITION EQUAL TO OR BETTER THAN THE ORIGINAL CONDITION.
- UTILITIES
1. UTILITIES SHOWN ON THE PLANS ARE FOR ILLUSTRATIVE PURPOSES ONLY AND NO GUARANTEE OF THEIR ACCURACY IS MADE OR INFERRED. THE LOCATION OF EXISTING UTILITIES AS SHOWN ON THE DRAWINGS REPRESENT DATA RECEIVED FROM VARIOUS SOURCES. IT IS NOT GUARANTEED TO BE CORRECT OR ALL-INCLUSIVE. THE CONTRACTOR SHALL CONDUCT HIS OWN INVESTIGATION INTO THE LOCATION, SIZE, DEPTH AND NATURE OF ANY AND ALL EXISTING UTILITIES THAT MAY INTERFERE WITH THE WORK UNDER THIS CONTRACT. ANY EXISTING UTILITIES THAT ARE TO REMAIN IN SERVICE SHALL BE FULLY PROTECTED BY THE CONTRACTOR AND ANY DAMAGE CAUSED BY THE CONSTRUCTION OPERATIONS SHALL BE IMMEDIATELY REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE OWNER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ANY AND ALL UTILITY COMPANIES REGARDING ADJUSTMENTS NECESSARY. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE AND CONSIDERED INCIDENTAL TO THE PROJECT COST. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND, OVERHEAD, OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER OR THE OWNER OR REPLACED. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.
2. THE CONTRACTOR MUST VERIFY AND LOCATE ALL EXISTING UTILITIES ON OR ADJACENT TO THE SITE. PRIOR TO BEGINNING CONSTRUCTION ACTIVITIES, CONTACT J.U.L.I.E. AT 1-800-892-0123 (OR 811) FOR EXACT FIELD LOCATION OF UTILITIES. DAMAGE, AND THE COST THEREOF, TO ANY AND ALL UTILITIES SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. ANY AND ALL EXISTING UTILITIES SHOWN HEREON ARE APPROXIMATE. THE ENGINEER AND SURVEYOR ASSUMES NO RESPONSIBILITY FOR THE LOCATION OF THE EXISTING UTILITIES SHOWN HEREON.
3. IF THERE ARE ANY UTILITIES WHICH ARE NOT MEMBERS OF THE J.U.L.I.E. SYSTEM, THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR DETERMINING THIS AND REQUESTING SAID UTILITIES TO FIELD VERIFY AND MARK PERTINENT UTILITY LOCATIONS.
4. THE UTILITY LOCATIONS, DEPTHS, ETC. SHOWN ON THESE PLANS ARE APPROXIMATE ONLY, AND SHALL BE VERIFIED BY THE CONTRACTOR WITH ALL AFFECTED UTILITY COMPANIES PRIOR TO INITIATING CONSTRUCTION OPERATIONS; THE ENGINEER AND OWNER ASSUME NO RESPONSIBILITY FOR THE ADEQUACY, SUFFICIENCY OR EXACTNESS OF THESE UTILITY REPRESENTATIONS.
5. THE CONTRACTOR SHALL CONTACT THE NECESSARY UTILITY COMPANIES FOR ANY UTILITY RELOCATIONS. THE CONTRACTOR SHALL PAY FOR ALL COSTS ASSOCIATED WITH RELOCATION OF UTILITIES ON OR ADJACENT TO THE SUBJECT PROPERTY OR WITHIN THE ROAD RIGHT-OF-WAY.
6. TRENCH BACKFILL SHALL BE USED IN LOCATIONS WHERE THERE IS AN EXISTING OR PROPOSED PERMANENT SURFACE.
7. ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION OR HAVE THE POTENTIAL FOR CREATING FUTURE PROBLEMS SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE PROJECT AT AN APPROVED LOCATION OBTAINED BY THE CONTRACTOR, ACCORDING TO THE "STANDARD SPECIFICATIONS FOR WATER & SEWER CONSTRUCTION IN ILLINOIS", CURRENT EDITION, AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO EARTH EXCAVATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
8. ANY AND ALL FIELD TILES AND OR STORM SEWERS DAMAGED OR ENCOUNTERED DURING THE CONSTRUCTION ACTIVITIES SHALL BE REPAIRED, REPLACED AND/OR CONNECTED IMMEDIATELY BY THE CONTRACTOR. COST FOR SAID REPAIRS, REPLACEMENT, AND/OR CONNECTION SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC CONTROL

1. THE CONTRACTOR SHALL PROVIDE, INSTALL AND MAINTAIN ALL TRAFFIC CONTROL ITEMS NECESSARY FOR THE CONSTRUCTION OF ITEMS WITH IN THE ROAD RIGHT-OF-WAY. ALL WORK PERFORMED SHALL HAVE TRAFFIC CONTROL IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" AND OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", CURRENT EDITION.
2. ALL TRAFFIC CONTROL DEVICES USED FOR THE MAINTENANCE OF TRAFFIC SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS NECESSARY THROUGHOUT THE DURATION OF THE CONTRACT. ALL SIGNS SHALL BE FURNISHED, INSTALLED AND MAINTAINED BY THE CONTRACTOR. PAYMENT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
3. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN CONDITIONS MAY REQUIRE THE ENGINEER TO MODIFY THE LOCATION OF THE TRAFFIC CONTROL DEVICES. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND WITHIN 30 MINUTES FROM THE TIME OF NOTIFICATION BY THE ENGINEER TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION, IMPROVEMENT OR MODIFICATION OF THE MAINTENANCE OF TRAFFIC CONTROL DEVICES. DURING CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS TO PROTECT ADJACENT TRAFFIC LANES OPEN TO TRAFFIC FROM DEBRIS BEING BLOWN OR OTHERWISE REMOVED FROM THE CONSTRUCTION AREAS. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR KEEPING DEBRIS OFF THE ADJACENT TRAVELED LANE SURFACE. COST INCIDENTAL TO THE PROJECT.
4. THE CONTRACTOR SHALL SUBMIT MAINTENANCE OF TRAFFIC AND STAGING OF CONSTRUCTION PLANS FOR APPROVAL BY THE ENGINEER PRIOR TO COMMENCING WORK.
5. THE CONTRACTOR SHALL PERFORM THE WORK UNDER STAGE CONSTRUCTION IN THE EVENT THAT THE CONTRACTOR WILL NEED TO CLOSE PUBLIC ROADS, CONTRACTOR SHALL SUBMIT PROPOSED DETOUR ROUTE AND ASSOCIATED SIGNAGE TO THE ENGINEER PRIOR TO COMMENCING WORK.
6. TRAFFIC CONTROL DEVICES, STREET NAME SIGNS, AND PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH VILLAGE OF ROSCOE ORDINANCES AND THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES". LOCATIONS OF SIGNS AND MARKINGS SHALL BE SPECIFIED BY THE PLANS, AND/OR AS DIRECTED BY THE ENGINEER.
7. PROVIDE TO THE ENGINEER AND THE OWNER THE NAME AND PHONE NUMBER OF INDIVIDUALS RESPONSIBLE FOR MAINTAINING TRAFFIC CONTROL MEASURES DURING CONSTRUCTION. THIS INDIVIDUAL SHALL BE AVAILABLE TO CORRECT TRAFFIC CONTROL PROBLEMS 24 HOURS PER DAY.
8. THE CONTRACTOR SHALL NOTIFY THE POST OFFICE, POLICE DEPARTMENT, FIRE DEPARTMENT, 911 DISPATCH CENTER, ILLINOIS DEPARTMENT OF TRANSPORTATION, STATE POLICE, APPROPRIATE SCHOOL DISTRICT AND THE LOCAL AGENCY A MINIMUM OF 5 DAYS PRIOR TO CLOSING ANY PORTION OF THE STREET OR ALLEY.



ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525

ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, IL 61073

PROJECT AND LOCATION:
MAIN STREET PEDESTRIAN CROSSING
ROSCOE, ILLINOIS

DRAWN BY: JH
APPROVED BY: TN
DATE: 9/8/2025
SCALE: AS NOTED

REVISIONS		
REV. NO.	DESCRIPTION	DATE

DRAWING:
GENERAL NOTES

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















SHEET NUMBER:
2 of 13

ABBREVIATIONS






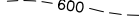





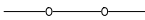



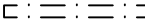











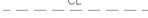
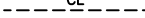




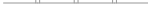
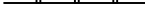

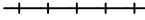









<	ANGLE
ABC	AGGREGATE BASE COURSE
AC	ACRE(S)
ACI	AMERICAN CONCRETE INSTITUTE
AGR	AGGREGATE
AISC	AMERICAN INSTITUTE OF STEEL CONSTRUCTION
ALT	ALTERNATE
ARCH	ARCHITECT
ASPH	ASPHALT
ASTM	AMERICAN SOCIETY OF TESTING AND MATERIALS
B	BALL VALVE
BFP	BACKFLOW PREVENTER
BIT	BITUMINOUS
BLDG	BUILDING
BLK	BLOCKING
BM	BENCHMARK
BOT	BOTTOM
BSMT	BASEMENT
BV	BUTTERFLY VALVE
B-B	BACK-TO-BACK OF CURB DIMENSION
CL or C	CENTERLINE
C to C	CENTER TO CENTER
C & G	CURB AND GUTTER
CF	CUBIC FEET
CHD	CHORD LENGTH
CI	CAST IRON PIPE
CHK	CHECK VALVE
CLR	CLEAR
CMP	CORRUGATED METAL PIPE
CMU	CONCRETE MASONRY UNIT
CTY	COUNTY
CONC	CONCRETE
CONT	CONTINUOUS
C-B	CENTERLINE TO BACK OF CURB DIMENSION
COORD	COORDINATE
CTRS	CORNER PIPING
CTRS	CENTERS
CY	CUBIC YARDS
CS	CORPORATION STOP
D	DEGREE OF CURVE
DEP	DEPRESSED
DET	DETAIL
DIAG	DIAGONAL
DIM	DIMENSION
DI	DUCTILE IRON PIPE
DN	DOWN
DNSTR	DOWNSTREAM
DP	DRAINAGE PIPE/STORM PIPE
DWG	DRAWING
E	EAST
EJ	EXPANSION JOINT
EL, ELEV	ELEVATION
EP	EDGE OF PAVEMENT
EQUIP	EQUIPMENT
EQUIV	EQUIVALENT
EW	EACH WAY
EXP	EXPANSION
EX, EXIST	EXISTING
EXT	EXTERIOR
E =	EXTERNAL DISTANCE
FD	FLOOR DRAIN
FDN	FOUNDATION
FE	FIELD ENTRANCE
FF	FINISH FLOOR
FIL	FILLET
FIN	FINISH
FL	FLOW LINE
FLR	FLOOR
FM	FORCE MAIN
FND	FOUND
FRMG	FRAMING
FTG	FOOTING
F-F	FACE TO FACE
GA	GAUGE
GI	GALVANIZED IRON PIPE
GRD	GRADE
GRS	GRATING SUPPORT
GRT	GROUT
GV	GAS VALVE
GYP	GYPSONUM
HSE	HOUSE
HC	HORIZONTAL CURVE
HMA	HOT MIX ASPHALT
HNGR	HANGER
HORIZ	HORIZONTAL
H.P.	HIGH POINT
HW	HOT WATER
HHW	HOT WATER HEATER
Δ =	CENTRAL ANGLE
I	MOMENT OF INERTIA
ID	INSIDE DIAMETER
INT	INTERIOR
INV	INVERT ELEVATION; BASED ON BENCH MARK DATUM
IR	IRON PIPE
JST	JOIST
L	LENGTH OF CURVE
LAT	LATERAL
LAV	LAVATORY
LF	LINEAL FEET
L.P.	LOW POINT
LT	LEFT OF SURVEY BASE LINE
MAX	MAXIMUM
ME	MATCH EXISTING
MH	MANHOLE
MIN	MINIMUM
MJ	MECHANICAL JOINT
MTL	METAL
N	NORTH
No. or #	NUMBER
NOM	NOMINAL
NTS	NOT TO SCALE
OC	ON CENTER
OD	OUTSIDE DIAMETER
OO	OUTSIDE TO OUTSIDE
OPNG	OPENING
OPP	OPPOSITE
PC	POINT OF CURVATURE
PCC	PORTLAND CEMENT CONCRETE
PCF	POUNDS PER CUBIC FOOT
PDP	PERFORATED DRAIN PIPE








PE	POLYETHYLENE PIPE
PI	POINT OF INTERSECTION
PL	PLATE
PLG	PLUG VALVE
PLP	POLYPROPYLENE PIPE
PLYWD	PLYWOOD
PM	PRINCIPAL MERIDIAN
PR	PRESSURE REGULATORS
PRC	POINT OF REVERSE CURVATURE
PRESS	PRESSURE
PR, PROP	PROPOSED
PRV	PRESSURE REDUCING VALVE
PSF	POUNDS PER SQUARE FOOT
PSI	POUNDS PER SQUARE INCH
PSL	PIPE SLEEVE
PT	POINT OF TANGENCY
PLG	PLUG VALVE
PVC	POLYVINYL CHLORIDE (PLASTIC) PIPE
R	RADIUS
RDCR	REDUCER
RCCP	REINFORCED CONCRETE CYLINDER PIPE
RCP	REINFORCED CONCRETE PIPE
RD	ROOF DRAIN
REINF	REINFORCING
REQD	REQUIRED
ROW	RIGHT OF WAY
RFTR	RAFTER
RND	ROUND
RR	RAILROAD
RRSP	RAILROAD SPIKE
RT	RIGHT
R&R	REMOVE AND REPLACE
S	SOUTH
SB	STREAM BED
SCHED	SCHEDULE
SEC	SECTION
SF	SQUARE FEET
SHR	SHOWER
SHT	SHEET
SHTG	SHEATHING
SP	SANITARY PIPE
SPA	SPACING OR SPACES
SPEC	SPECIFICATION
SQ	SQUARE
SS	SANITARY SERVICE
STA	STATION
STD	STANDARD
STL	STEEL
STRUCT	STRUCTURAL
SW	SIDEWALK
SY	SQUARE YARDS
SYM	SYMMETRICAL
TAN	TANGENT LENGTH
TBC	TOP BACK OF CURB
TBM	TEMPORARY BENCH MARK; BASED ON BENCHMARK DATUM
TD	TILE DRAIN
THK	THICK
TR	TREAD
TY	TYPE
TYP	TYPICAL
U.O.N.	UNLESS OTHERWISE NOTED
UP	UTILITY POLE
UPSTR	UPSTREAM
UR	URINAL
USGS	US GEOLOGICAL SURVEY
VC	VERTICAL CURVE
VCP	VITRIFIED CLAY PIPE
VERT	VERTICAL
VOL	VOLUME
VPC	VERTICAL POINT OF CURVATURE
VPI	VERTICAL POINT OF INTERSECTION
VPRC	VERTICAL POINT OF REVERSE CURVATURE
VPT	VERTICAL POINT OF TANGENCY
W	WEST
WC	WATER CLOSET
WF	WIDE FLANGE
WM	WATER MAIN
WMQ	WATER MAIN QUALITY
WV	WATER VALVE
WGT	WEIGHT
WP	WEATHER PROOF
WS	WATER SERVICE
WWF	WELDED WIRE FABRIC
W/	WITH
W/O	WITHOUT
XP	EXPLOSION PROOF

HATCH PATTERNS
























	EARTH - FILL		BRICK
	EARTH - UNDISTURBED		STEEL
	ROCK (GEOLOGICAL)		INSULATION (LOOSE/ BATT)
	STONE OR RIP RAP		INSULATION (RIGID)
	GRAVEL		WOOD (ROUGH)
	CONCRETE		WOOD (BLOCKING)
	CONCRETE BLOCK		WOOD (FINISH)
	CMU		DETECTABLE WARNING
	ASPHALT PAVEMENT		


























SYMBOLS













EXISTING	CIVIL	PROPOSED
EXISTING R.O.W.	RIGHT-OF-WAY LINE	PROPOSED R.O.W.
	PROPERTY LINE	
	CENTERLINE	
	SETBACK LINE	
	EASEMENT LINE	
	SECTION LINE	
	SECTION CORNER	
 N 1000.00 E 1000.00	COORDINATE POINT ON GRID SYSTEM	
● FND	FOUND OR SET PROPERTY PIN	○ SET
	RIGHT-OF-WAY MARKER	
	BENCHMARK	
	CONTOUR LINE	
	SPOT ELEVATION (AT ●)	
	FENCE LINE	
	SILT FENCE LINE	
	CURB AND GUTTER	
	TIP OUT CURB AND GUTTER	
	SAWCUT, LIMITS OF PAVEMENT REMOVAL & REPLACEMENT	
 X"	DECIDUOUS TREE W/ SIZE	 X"
 X"	CONIFEROUS TREE W/ SIZE	 X"
 X"	TREE STUMP	
	HEDGEROW	
	BUSH OR SHRUB	
	TREE LINE	
	CONSTRUCTION LIMIT LINE	
	SIGN (MULTIPLE POST, SINGLE POST, STREET SIGN)	
	SIGN (PYLON)	
	GUARD RAIL	
	RAILROAD TRACKS	
	BUILDING	
	MAILBOX	
	FLAGPOLE	
	BOLLARD	
	AIR CONDITIONER	

<u>EXISTING</u>	<u>MISC</u>	<u>PROPOSED</u>
 S.B. #XX	SOIL BORING LOCATION AND NUMBER	 S.B. #X
 MW #XX	MONITORING WELL	 MW #X
	REVISION NUMBER	
	OUTLINE OF DETAILED AREA	
	SECTION NUMBER _____	
	SHEET WHERE SHOWN _____	

EXISTING	SANITARY SEWER	PROPOSED
	SANITARY SEWER	
	SANITARY SEWER SERVICE	
	SANITARY SEWER FORCE MAIN	
	SANITARY CLEANOUT	CO ●
	SANITARY MANHOLE	●
	WYE FITTING	

EXISTING		PROPOSED
WSV	WATER SERVICE	WSV
W	WATER PIPE	W
	FIRE HYDRANT	
	YARD HYDRANT	
	WATER VALVE WITH BOX	
	CURB STOP W/CURB BOX	
	REDUCER	
	WATER VALVE VAULT	
	11.25" BEND	
	22.50" BEND	
	45" BEND	
	90" BEND	
	TEE	
	CAP	
	WATER METER	
	SPRINKLER HEAD	
	TRACER WIRE BOX	

EXISTING		PROPOSED
	STORM SEWER	
	DRAIN TILE	
	DITCH LINE (PAVED)	
	DITCH LINE (UNPAVED)	
	STORM MANHOLE	
	CATCH BASIN	
	STORM SEWER INLET	
	STORM SEWER INLET – BEHIND CURB	
	DOWNSPOUT	
	CULVERT AND SIZE	
	RCCP OR RCP EQRS (RCAP) END SECTION	
	METAL OR HDPE END SECTION	
	FLOW DIRECTION	

<u>EXISTING</u>	<u>PROPOSED</u>
EROSION CONTROL BLANKET	
TEMPORARY AND PERMANENT SEEDING AREA	
UNDISTURBED AREA	
STABILIZED CONSTRUCTION ENTRANCE	
SILT FENCE	
INLET PROTECTION	
TEMPORARY SEDIMENT TRAP	
CULVERT INLET PROTECTION	
ROCK OUTLET PROTECTION	
ROCK CHECK DAM — COURSE AGGREGATE	
ROCK CHECK DAM — RIP RAP	
DITCH CHECK	

EXISTING	UTILITY	PROPOSED
	FIBER OPTIC LINE	
	UNDERGROUND TV CABLE	
	CABLE TV RISER PEDESTAL	
	OVERHEAD UTILITY	
	UNDERGROUND ELECTRIC	
	ELECTRIC RISER PEDESTAL	
	ELECTRIC MANHOLE	
	UNDERGROUND TELEPHONE	
	TELEPHONE RISER PEDESTAL	
	TELEPHONE MANHOLE	
	UTILITY POLE	
	UTILITY POLE W/ METER	
	UTILITY POLE W/ TRANSFORMER	
	UTILITY POLE W/ LIGHT	
	UTILITY POLE WITH GUY WIRE AND ANCHOR	
	LIGHT (MAST MOUNTED)	
	LIGHT POLE (SINGLE FIXTURE)	
	YARD LIGHT	
	GAS MAIN	
	GAS METER	
	GAS VALVE	
	GAS STRUCTURE	
	TRANSFORMER	
	GENERATOR	

EXISTING	TRAFFIC RELATED	PROPOSED
	CONTROLLER	
	MAST ARM ASSEMBLY AND POLE	
	SIGNAL HEAD AND POST	
	SIGNAL HEAD	
	PEDESTRIAN HEAD	
	PEDESTRIAN PUSH-BUTTON	
	HAND HOLE	
	DOUBLE HAND HOLE	
	HAND HOLE OR JUNCTION BOX	
	HEAVY-DUTY HAND HOLE	
$(5' - 2'')$	EXISTING CONDUIT (LENGTH AND SIZE) PROP GALVANIZED STEEL OR PVC CONDUIT UPPER NUMERAL INDICATES LENGTH "T" INDICATES CONDUIT IN TRENCH "P" INDICATED CONDUIT PUSHED LOWER NUMERAL INDICATES SIZE AND TYPE	
	LUMINAIRE	
	ARROW - THROUGH, TURN LEFT	
	ARROW - THROUGH	
	ARROW - TURN LEFT	
	ARROW - TURN RIGHT	
	ONE DIRECTION TURN ONLY	
	HANDICAPPED PARKING STALL	
	TRAFFIC DETECTOR LOOP	
	TRAFFIC CONTROL BOX	



ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525

PLOT DATE: 9/8/25 © 2025 FFHR GRAHAM

ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, IL 61073

PROJECT AND LOCATION:
MAIN STREET PEDESTRIAN CROSSING
ROSCOE, ILLINOIS

DRAWN BY: JH
APPROVED BY: TN
DATE: 9/8/2025
SCALE: AS NOTED

REVISIONS		
REV. NO.	DESCRIPTION	DATE

DRAWING:
LEGEND

SET TYPE: FOR BID
G:\C3D\25\25-934\Plans\25-934 Plans.dwg

G:\C3D\25\25-934\Plans\25-934 Plans.dwg, LEGEND

JOB NUMBER:
25-934

SHEET NUMBER:
3 of 13

SUMMARY OF QUANTITIES

No.	Description	Quantity	Unit
1	HMA PAVEMENT REMOVAL	25.0	SY
2	COMBINATION CURB AND GUTTER REMOVAL	40.0	LF
3	SIDEWALK REMOVAL	655.0	SF
4	P.C.C. SIDEWALK, 4"	705.0	SF
5	COMBINATION CONCRETE CURB AND GUTTER, TYPE M6.24	40.0	LF
6	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	125.0	LF
7	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	110.0	LF
8	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	120.0	LF
9	THERMOPLASTIC PAVEMENT MARKING - LINE 24" -	30.0	LF
10	METHYL METHACRYLATE (MMA) PAVEMENT MARKING - LINE 4"	45.0	LF
11	METHYL METHACRYLATE (MMA) PAVEMENT MARKING - LINE 24"	25.0	LF
12	DETECTABLE WARNINGS	30.0	SF
13	EROSION AND SEDIMENT CONTROL	1.0	LS
14	RESTORATION	1.0	LS
15	CONCRETE WASHOUT BASIN	1.0	LS
16	TRAFFIC CONTROL & PROTECTION	1.0	LS
17	STOP SIGN RELOCATE	1.0	EA
18	CONSTRUCTION LAYOUT	1.0	LS



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REVISIONS		
REV. NO.	DESCRIPTION	DATE

DRAWING:

SUMMARY OF QUANTITIES

SET TYPE: FOR BID

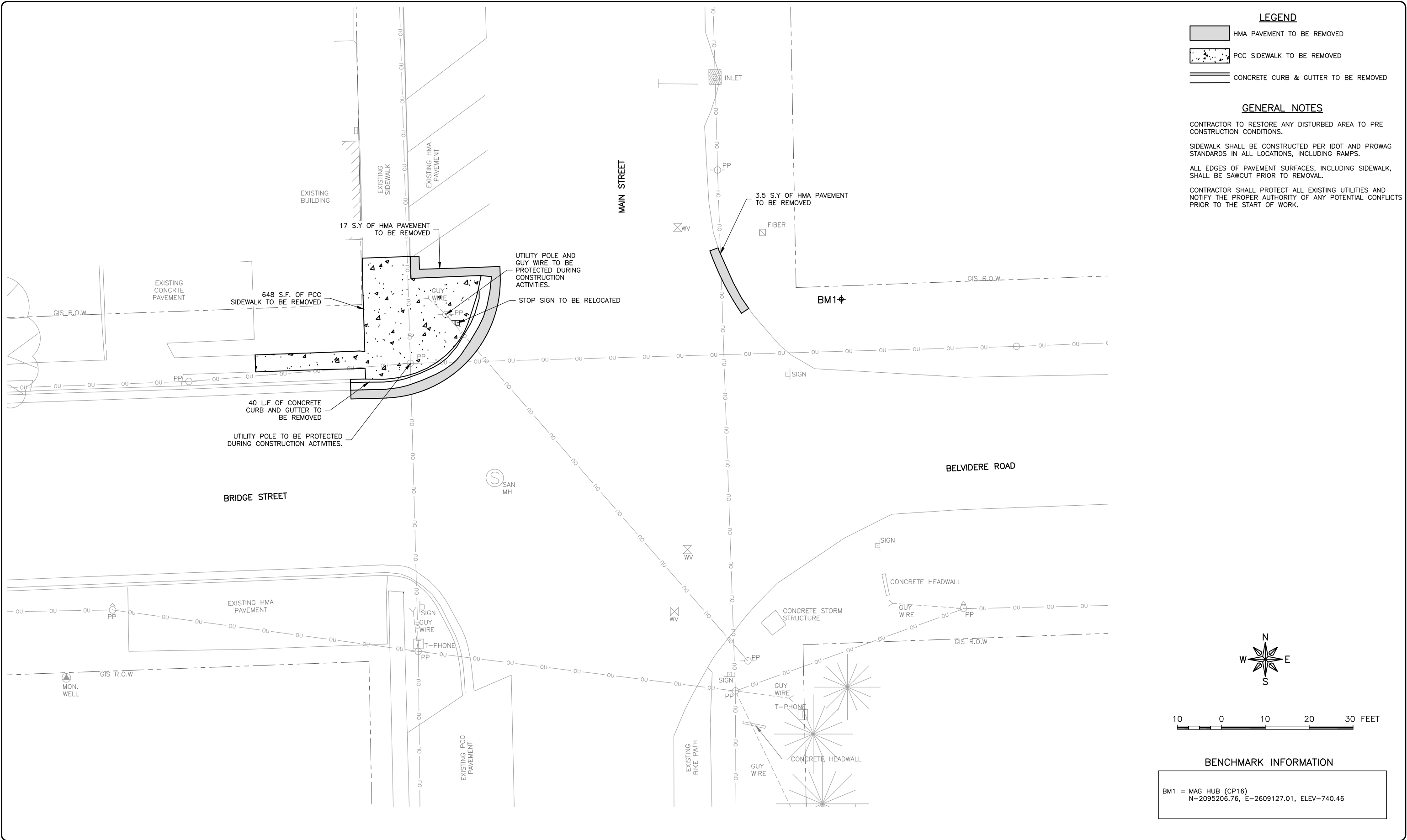
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JOB NUMBER:

25-934

SHEET NUMBER:

4 of 13



FEHR GRAHAM

ENGINEERING & ENVIRONMENTAL

ILLINOIS DESIGN FIRM NO. 184-003525

ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:

VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, IL 61073

PROJECT AND LOCATION:

MAIN STREET PEDESTRIAN CROSSING
ROSCOE, ILLINOIS

DRAWN BY: JH
APPROVED BY: TN
DATE: 9/8/2025
SCALE: AS NOTED

REVISIONS		
REV. NO.	DESCRIPTION	DATE

DRAWING:

BRIDGE STREET AND MAIN STREET REMOVAL
PLAN

SET TYPE: FOR BID

G:\C30\25\25-934\25-934 Design.dwg, removals

JOB NUMBER:

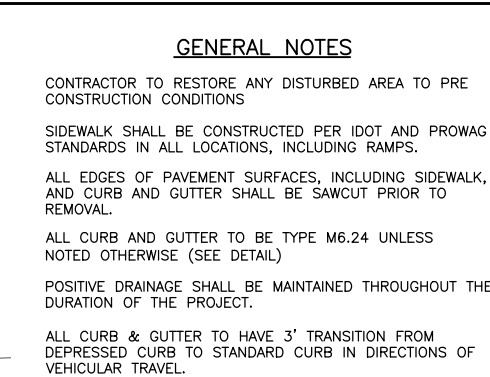
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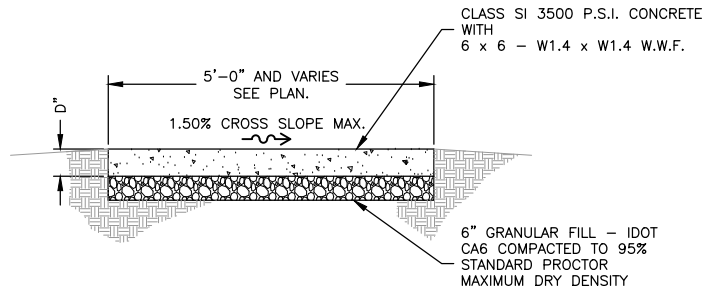
5 of 13

PLOT DATE: 9/8/25

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BM1 = MAG HUB (CP16)
N-2095206.76, E-2609127.01, ELEV-740.46



NOTE: WALK CONSTRUCTED OF CLASS SI 3500 PSI CONCRETE WITH 5 TO 8% AIR ENTRAINMENT, 3/4" MAX. AGGREGATE SIZE TO A MINIMUM THICKNESS OF 4". WALK TO BE FINISHED WITH A FLOAT, STEEL TROWEL AND BARN BROOM FINISH WITH TOOLED JOINTS AND EDGES AND CURING COMPOUND. FORMED JOINTS TO BE FINISHED WITH 1/4" RADIUS.

INSTALL 1/2" WIDE EXPANSION JOINTS WITH BITUMINOUS JOINT FILLER AT 30'-0" O.C. (EXTEND FULL DEPTH OF WALK).

SCORE TOOLED JOINTS, 1/2 D DEEP, AT 5'-0" O.C. UNLESS OTHERWISE NOTED. SEE SIDEWALK JOINT DETAILS.

INSTALL 1/2" WIDE BITUMINOUS ISOLATION JOINT WHERE SIDEWALK ABUTS A CURB, CONCRETE DRIVEWAY, STRUCTURE OR AT AN ADJACENT CURB JOINT (EXTEND FULL DEPTH OF WALK).

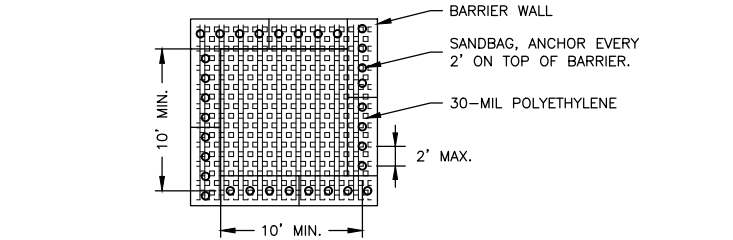
DEPTH D = 4" MIN. EXCEPT AT CURB RAMPS WHERE D = 6" MIN. OR AT DRIVEWAYS WHERE D = 6" MIN.

SIDEWALK RAMPS AT DRIVES TO BE CONSTRUCTED ACCORDING TO IDOT STANDARD No. 424001.

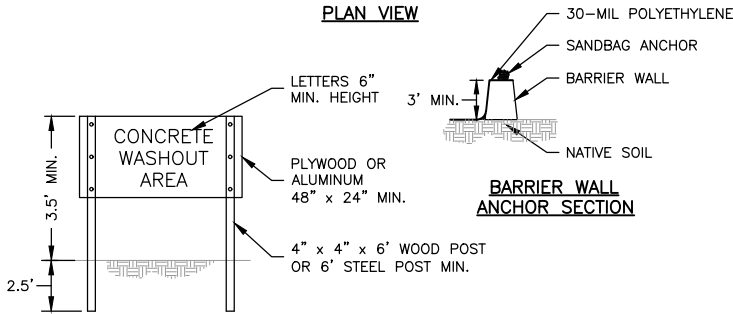
AGGREGATE BASE COURSE SHALL BE CONSIDERED INCIDENTAL TO THE SIDEWALK PAY ITEM.

EXCAVATION FOR NEW SIDEWALK SHALL BE CONSIDERED INCIDENTAL TO THE SIDEWALK PAY ITEM.

SIDEWALK CROSS SECTION DETAIL
N.T.S.



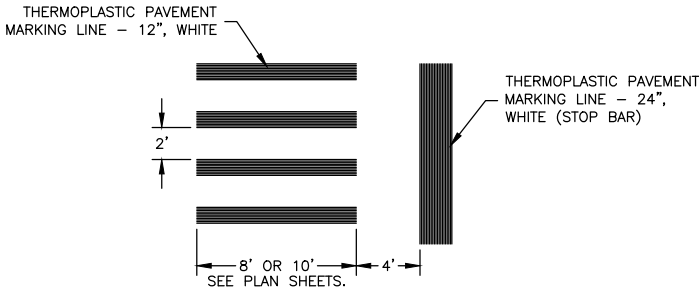
PLAN VIEW



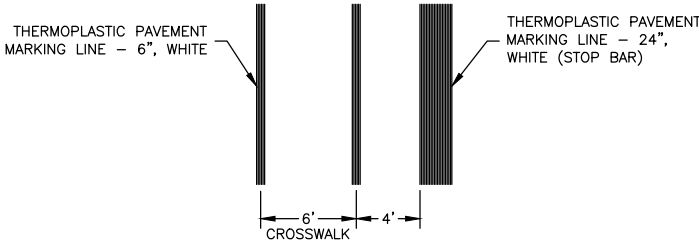
SIGN DETAIL

NOTE: MAINTAINING TEMPORARY CONCRETE WASHOUT FACILITIES SHALL INCLUDE REMOVING AND DISPOSING OF HARDENED CONCRETE AND/OR SLURRY AND RETURNING THE FACILITIES TO A FUNCTIONAL CONDITION. FACILITY SHALL BE CLEANED OR RECONSTRUCTED IN A NEW AREA ONCE WASHOUT BECOMES TWO-THIRDS FULL.

CONCRETE WASHOUT PIT DETAIL
N.T.S.

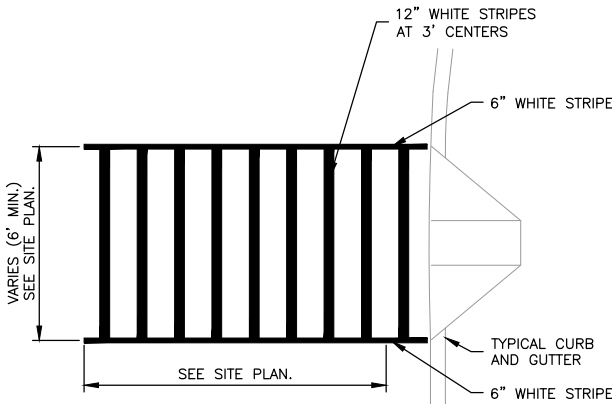


BIKE/PEDESTRIAN PATH CROSSING DETAIL



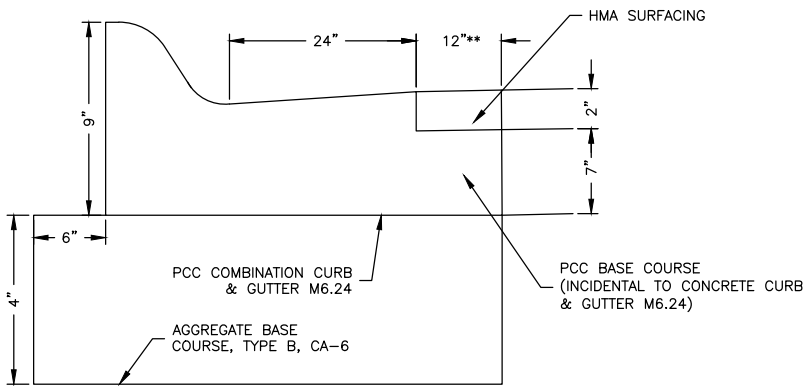
PEDESTRIAN CROSSWALK DETAIL

CROSSWALK PAVEMENT MARKING DETAIL
N.T.S.



NOTE: MARKINGS FOR STREET SHALL BE ACCORDING TO LOCAL CODE REQUIREMENTS AND AS OUTLINED IN SECTION 3B OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS".

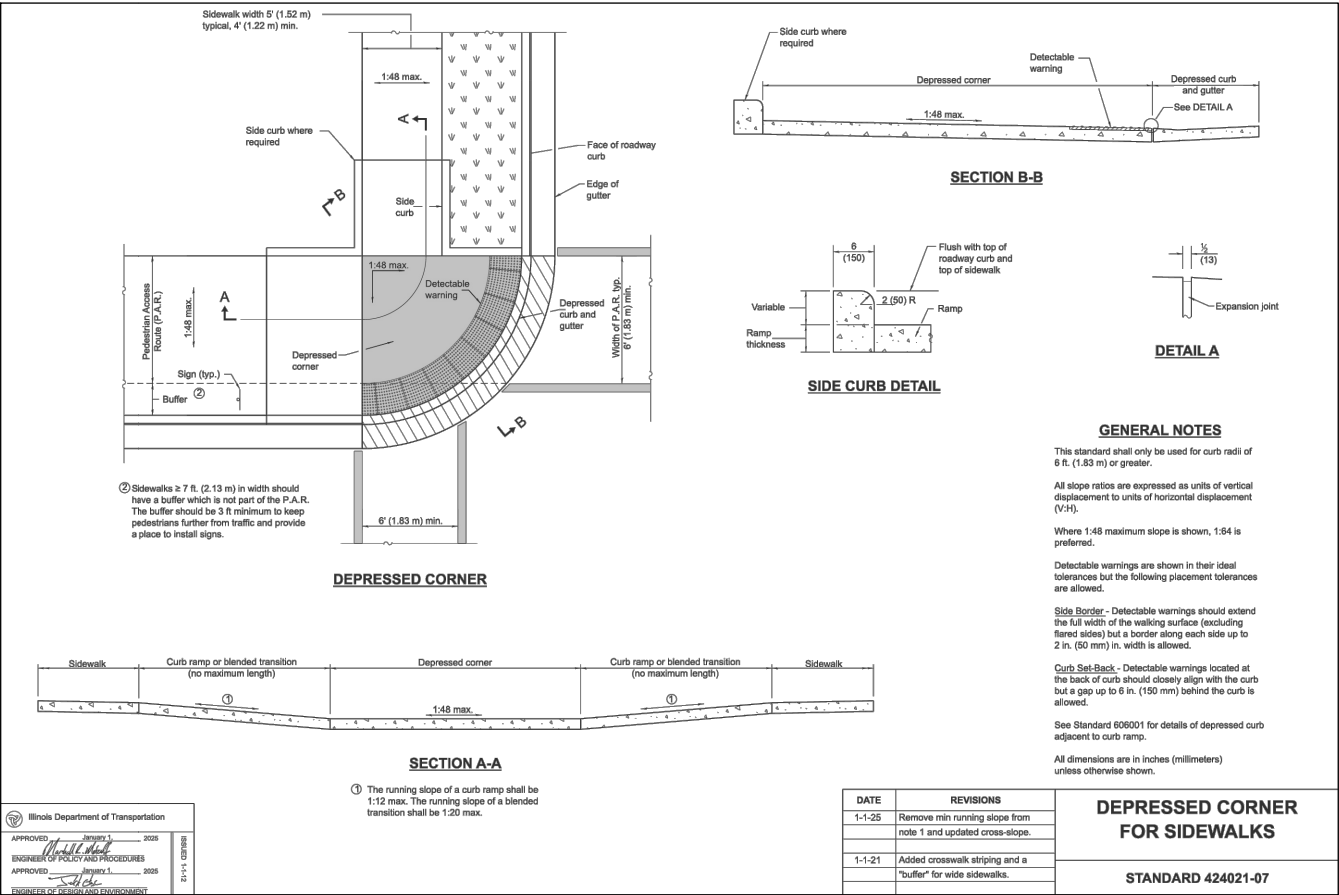
PEDESTRIAN CROSSWALK DETAIL
N.T.S.



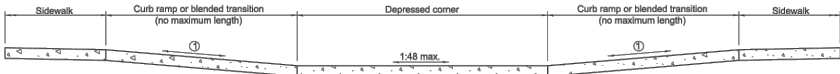
NOTE:
* 9" IS THE MINIMUM REQUIRED WHEN PCC BASE COURSE IN PLACE PER STD. 606001

** 12" MINIMUM, MAY VARY PER PLANS.
TIE BARS TO BE INSTALLED PER IDOT STD. 606001

PCC COMBINATION
CURB & GUTTER DETAIL TYPE M6.24 (HMA PAVEMENT)
N.T.S.



DEPRESSED CORNER



SECTION A-A

① The running slope of a curb ramp shall be 1:12 max. The running slope of a blended transition shall be 1:20 max.

Illinois Department of Transportation
APPROVED: January 1, 2025
ENGINEER OF POLICY AND PROCEDURES
APPROVED: January 1, 2025
ENGINEER OF DESIGN AND ENVIRONMENT

DATE	REVISIONS
1-1-25	Remove min running slope from note 1 and updated cross-slope.
1-1-21	Added crosswalk striping and a "buffer" for wide sidewalks.

DEPRESSED CORNER
FOR SIDEWALKS

STANDARD 424021-07

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL

ILLINOIS DESIGN FIRM NO. 184-003525

ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
VILLAGE OF ROSCOE
10631 MAIN STREET
ROSCOE, IL 61073

PROJECT AND LOCATION:
MAIN STREET PEDESTRIAN CROSSING
ROSCOE, ILLINOIS

DRAWN BY: JH
APPROVED BY: TN
DATE: 9/8/2025
SCALE: AS NOTED

REV. NO.	DESCRIPTION	DATE

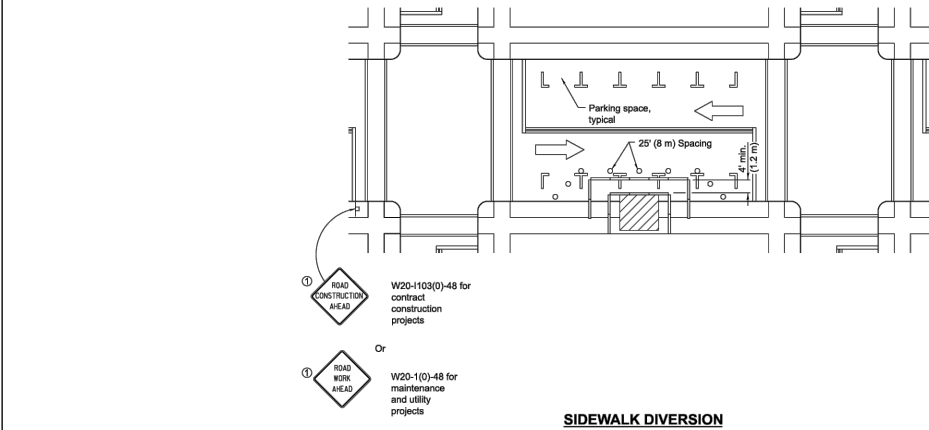
DRAWING:
DETAILS 1

SET TYPE: FOR BID

G:\C20\25\25-934\Plans\25-934 Plans.dwg, DETAILS

JOB NUMBER:
25-934

SHEET NUMBER:
8 of 13

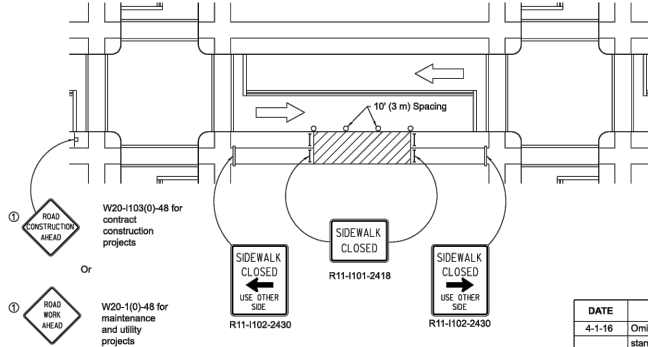


SYMBOLS

- Work area
- Sign on portable or permanent support
- Barricade or drum
- Cone, drum or barricade
- Type III barricade
- Detectable pedestrian channelizing barricade

Illinois Department of Transportation
APPROVED January 1, 2016
ENGINEER OF SAFETY ENGINEERING
APPROVED January 1, 2016
ENGINEER OF DESIGN AND ENVIRONMENT

SIDEWALK DIVERSION



SIDEWALK CLOSURE

① Omit whenever duplicated by road work traffic control.

GENERAL NOTES

This Standard is used where, at any time, pedestrian traffic must be rerouted due to work being performed.

This Standard must be used in conjunction with other Traffic Control & Protection Standards when roadway traffic is affected.

Temporary facilities shall be detectable and accessible.

The temporary pedestrian facilities shall be provided on the same side of the closed facilities whenever possible.

The SIDEWALK CLOSED / USE OTHER SIDE sign shall be placed at the nearest crosswalk or intersection to each end of the closure. Where the closure occurs at a corner, the signs shall be erected on the corners across the street from the closure. The SIDEWALK CLOSED signs shall be used at the ends of the actual closures.

Type III barricades and R11-2-4830 signs shall be positioned as shown in "ROAD CLOSED TO ALL TRAFFIC" detail on Standard 701901.

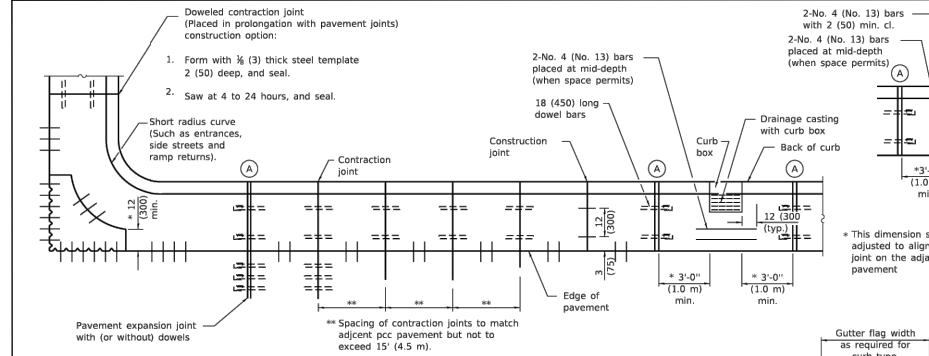
All dimensions are in inches (millimeters) unless otherwise shown.

SIDEWALK, CORNER OR CROSSWALK CLOSURE

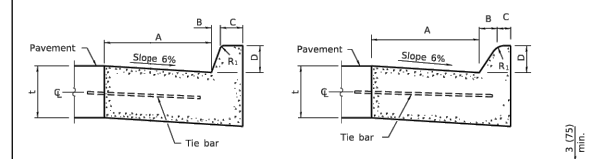
(Sheet 1 of 2)

STANDARD 701801-06

DATE	REVISIONS
4-1-16	Omitted orange safety fence from standard as this is covered in the standard spec.
1-1-12	Added SIDEWALK DIVERSION. Modified appearance of plan views. Renamed Standard.



ADJACENT TO PCC PAVEMENT OR PCC BASE COURSE



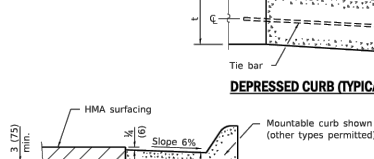
BARRIER CURB

MOUNTABLE CURB

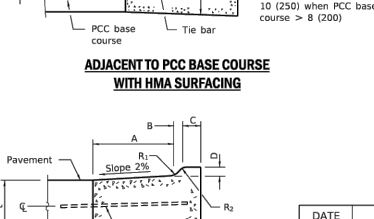
TYPE	A	B	C	D	R ₁
B-6.06	6	2	4	2	1
(B-15.15)	(150)	(25)	(100)	(50)	(125)
B-6.12	12	2	4	2	1
(B-15.30)	(300)	(25)	(100)	(50)	(125)
B-6.18	18	2	4	2	1
(B-15.45)	(450)	(25)	(100)	(50)	(125)
B-6.24	24	2	4	2	1
(B-15.60)	(600)	(25)	(100)	(50)	(125)
B-9.12	12	2	4	2	1
(B-22.30)	(300)	(50)	(125)	(25)	(25)
B-9.18	18	2	4	2	1
(B-22.45)	(450)	(50)	(125)	(25)	(25)
B-9.24	24	2	4	2	1
(B-22.60)	(600)	(50)	(125)	(25)	(25)

TYPE	A	B	C	D	R ₁	R ₂
M-2.06	6	2	4	2	3	2
(M-15.15)	(150)	(25)	(100)	(50)	(175)	(50)
M-2.12	12	2	4	2	3	2
(M-15.30)	(300)	(25)	(100)	(50)	(175)	(50)
M-2.06	6	2	4	2	3	2
(M-15.15)	(150)	(25)	(100)	(50)	(175)	(50)
M-2.12	12	2	4	2	3	2
(M-15.30)	(300)	(25)	(100)	(50)	(175)	(50)
M-4.18	18	4	3	4	3	NA
(M-10.45)	(450)	(100)	(75)	(100)	(75)	NA
M-4.12	12	4	3	4	3	NA
(M-10.30)	(300)	(100)	(75)	(100)	(75)	NA
M-4.18	18	4	3	4	3	NA
(M-10.45)	(450)	(100)	(75)	(100)	(75)	NA
M-4.24	24	4	3	4	3	NA
(M-10.60)	(600)	(100)	(75)	(100)	(75)	NA
M-6.06	6	2	4	2	6	2
(M-15.15)	(150)	(25)	(100)	(50)	(150)	(50)
M-6.12	12	2	4	2	6	2
(M-15.30)	(300)	(25)	(100)	(50)	(150)	(50)
M-6.18	18	2	4	2	6	2
(M-15.45)	(450)	(25)	(100)	(50)	(150)	(50)
M-6.24	24	2	4	2	6	2
(M-15.60)	(600)	(25)	(100)	(50)	(150)	(50)

ADJACENT TO PCC BASE COURSE WITH HMA SURFACING



ADJACENT TO PCC BASE COURSE WITH HMA SURFACING



ADJACENT TO PCC BASE COURSE WITH HMA SURFACING

ADJACENT TO PCC BASE COURSE WITH HMA SURFACING

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ADJACENT TO PCC BASE COURSE WITH HMA SURFACING

ADJACENT TO PCC BASE COURSE WITH HMA SURFACING

GENERAL NOTES

The bottom slope of combination curb and gutter constructed adjacent to pcc pavement shall be the same slope as the subbase or 6% when subbase is omitted.

t = Thickness of pavement.

Longitudinal joint tie bars shall be No. 6 (No. 19) at 36 (900) centers in accordance with details for longitudinal construction joint shown on Standard 420001.

A minimum clearance of 2 (50) between the end of the tie bar and the back of the curb shall be maintained.

The dowel bars shown in contraction joints will only be required for monolithic construction.

See Standard 606301 for details of corner islands.

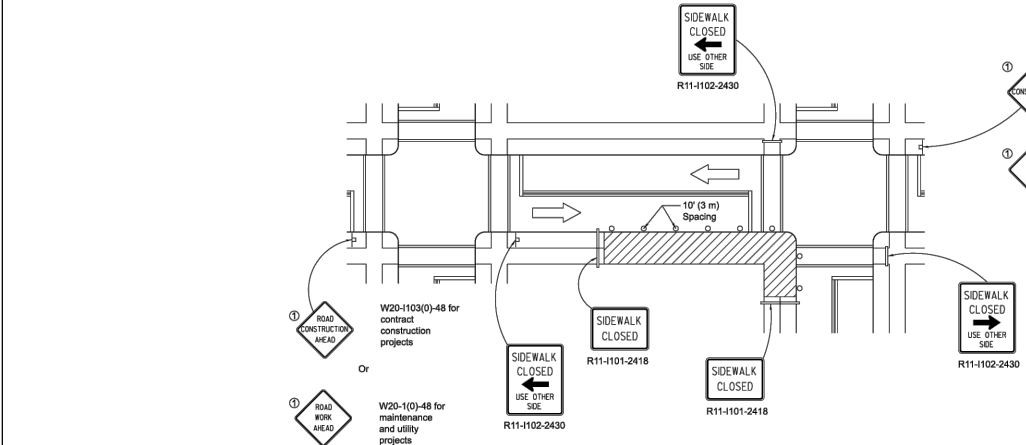
All dimensions are in inches (millimeters) unless otherwise shown.

CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER

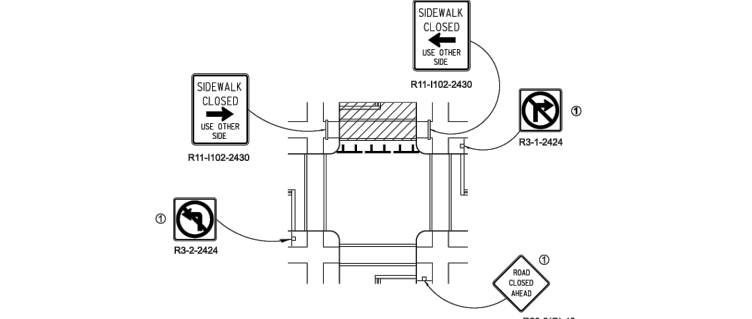
(Sheet 1 of 2)

STANDARD 606001-08

DATE	REVISIONS
1-1-22	Revised contraction joint spacing adjacent to pcc pavement.
1-1-18	Revised General Note for tie bar spacing to 36 (900) cts.



CORNER CLOSURE



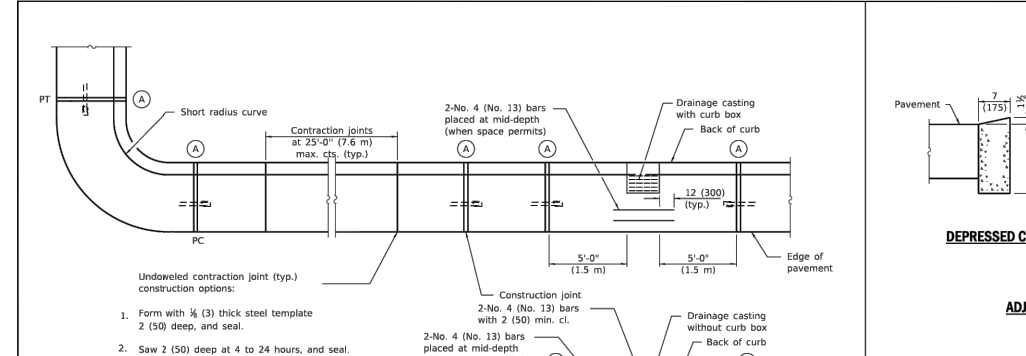
CROSSWALK CLOSURE

SIDEWALK, CORNER OR CROSSWALK CLOSURE

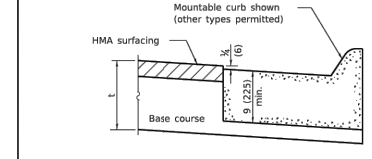
(Sheet 2 of 2)

STANDARD 701801-06

Illinois Department of Transportation
APPROVED January 1, 2016
ENGINEER OF SAFETY ENGINEERING
APPROVED January 1, 2016
ENGINEER OF DESIGN AND ENVIRONMENT



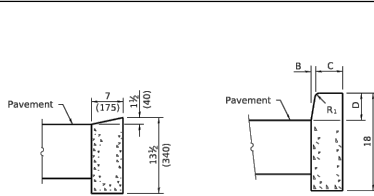
PLAN



ON DISTURBED SUBGRADE

ON UNDISTURBED SUBGRADE

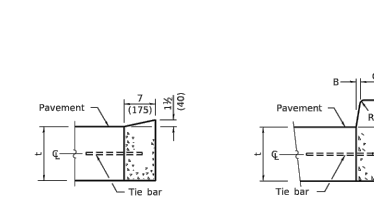
ADJACENT TO FLEXIBLE PAVEMENT



DEPRESSED CURB

BARRIER CURB

ADJACENT TO FLEXIBLE PAVEMENT



DEPRESSED CURB

BARRIER CURB

ADJACENT TO PCC PAVEMENT OR PCC BASE COURSE

CONCRETE CURB TYPE B

CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER

(Sheet 2 of 2)

STANDARD 606001-08

Illinois Department of Transportation
PASSED January 1, 2022
ENGINEER OF POLICY AND PROCEDURES
APPROVED January 1, 2022
ENGINEER OF DESIGN AND ENVIRONMENT

FEHR GRAHAM

ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525

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10631 MAIN STREET
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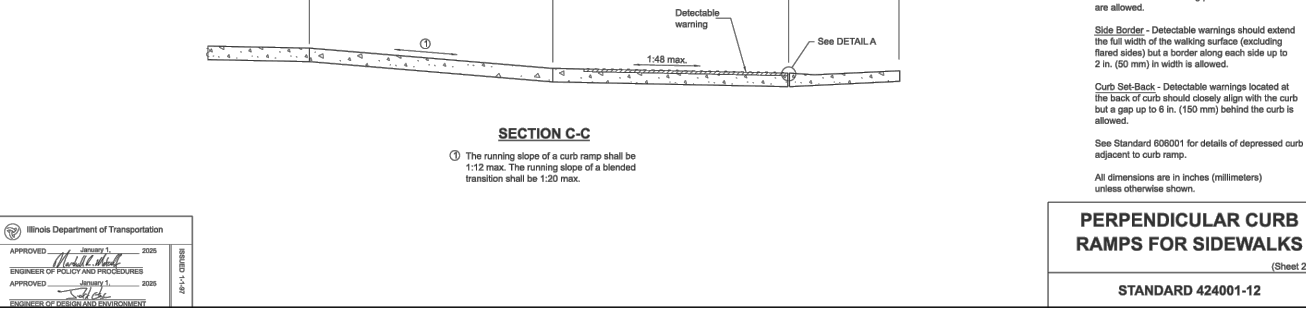
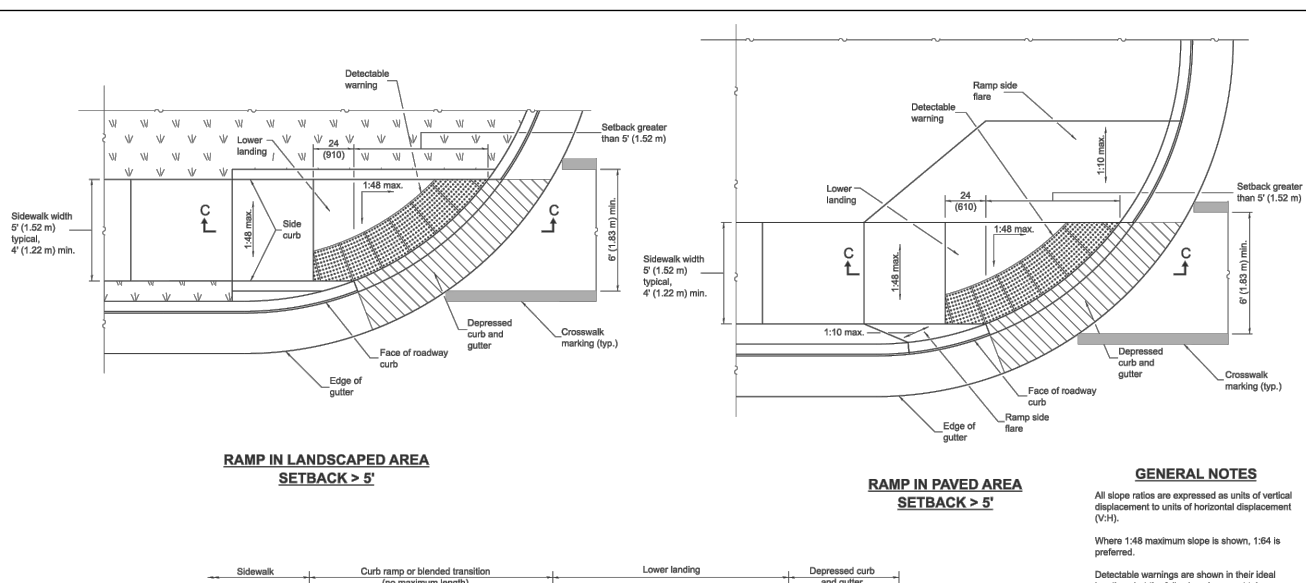
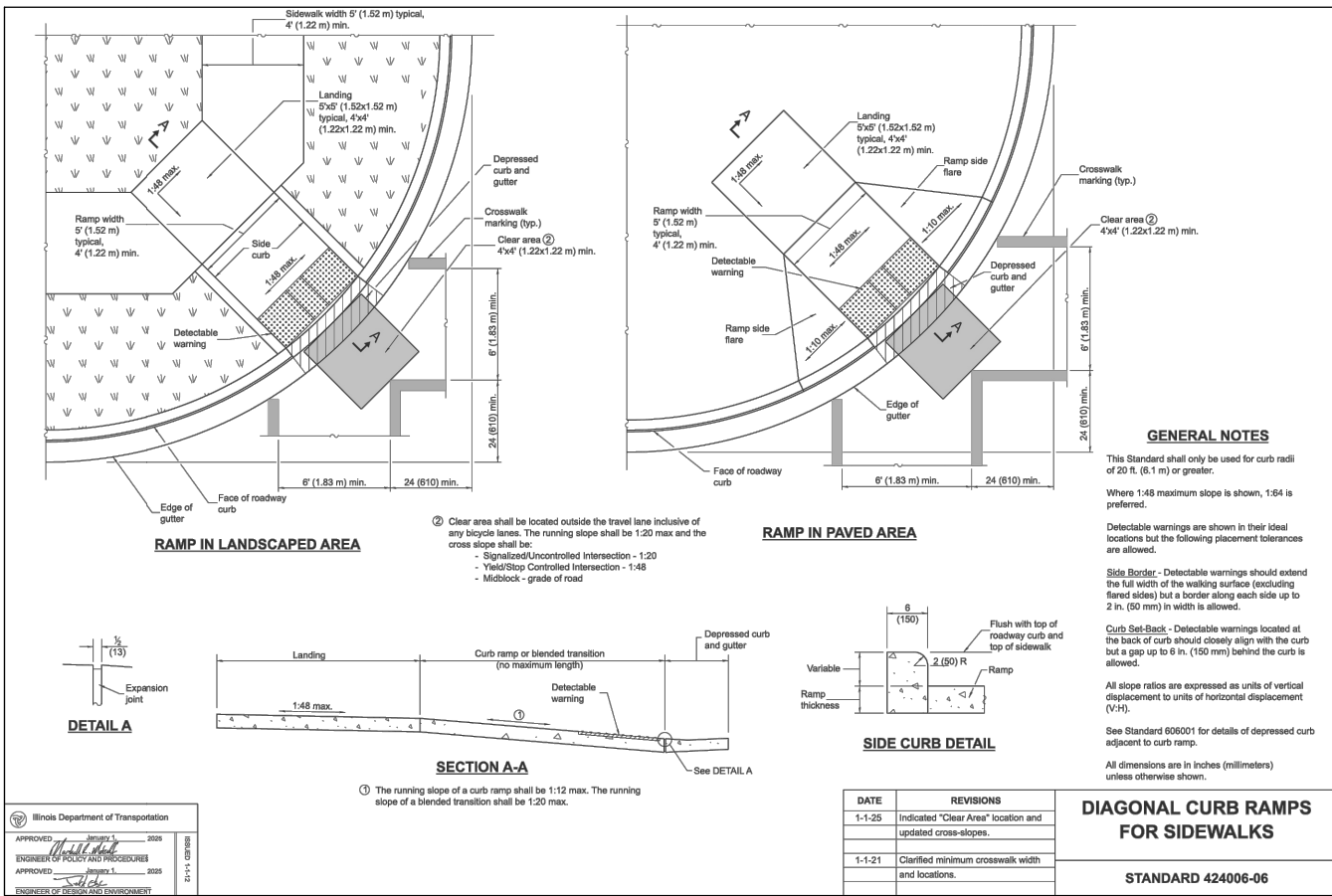
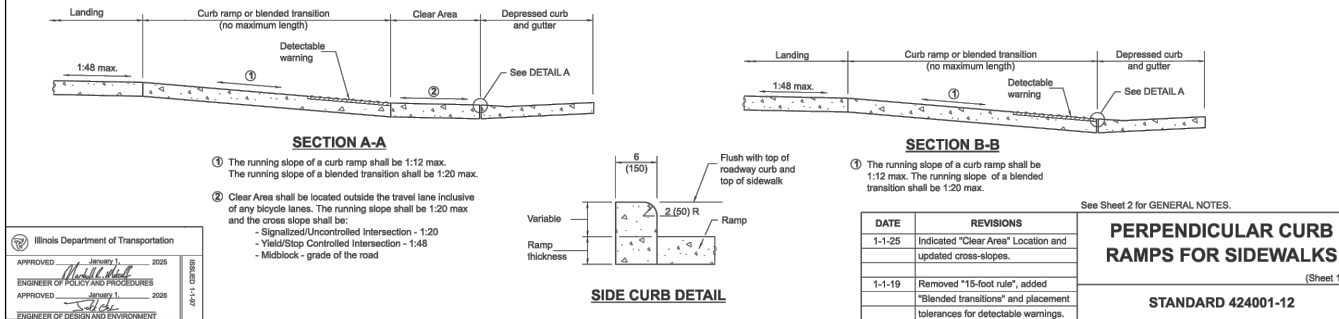
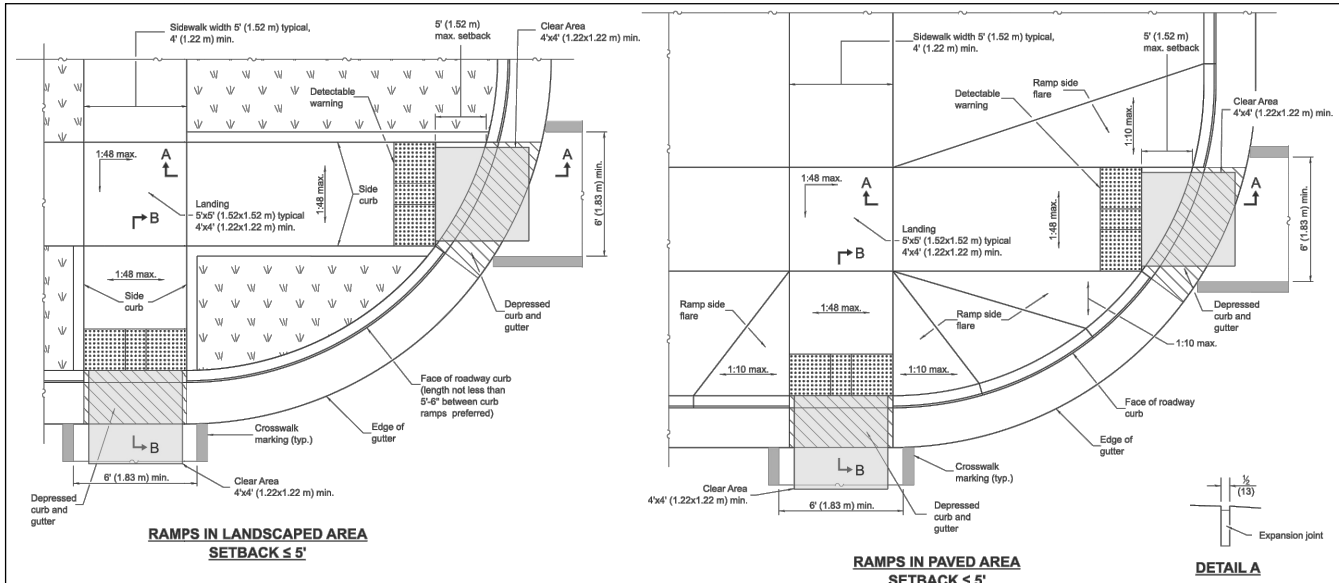
REV. NO.	DESCRIPTION	DATE

DRAWING:
DETAILS 2

SET TYPE: FOR BID
G:\C30\25\25-934\Plans\25-934 Plans.dwg, detail 1

JOB NUMBER:
25-934

SHEET NUMBER:
9 of 13



REVISIONS		
REV. NO.	DESCRIPTION	DATE

