

- GENERAL NOTES:**
1. CONTRACTOR SHALL NOTIFY THE VILLAGE OF ROSCOE AND OWNER'S REPRESENTATIVE 48 HOURS PRIOR TO START OF CONSTRUCTION. OWNERS REPRESENTATIVE SHALL BE NOTIFIED 24 HOURS IN ADVANCE OF RESUMING WORK AFTER STOPPAGE AND BEFORE ANY PAVING OR CONCRETE WORK. PAVING, WALKS OR CURB INSTALLED WITHOUT OWNER REPRESENTATIVE APPROVAL OF SUBGRADE, BASE COURSE, FORM OR STRING LINES MAY BE REJECTED FOR PAYMENT.
 2. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE FOLLOWING DOCUMENTS:
 - a. STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS (HEREIN AFTER CALLED "DOT SPECIFICATIONS")
 - b. SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS
 - c. CODE OF ORDINANCES FOR THE VILLAGE OF ROSCOE.
 3. ALL WORK SHALL BE GUARANTEED BY THE CONTRACTOR FOR ONE YEAR FROM DATE OF ACCEPTANCE BY OWNER AND VILLAGE OF ROSCOE.
 4. CONTRACTOR SHALL BE RESPONSIBLE FOR CONSTRUCTION MEANS, METHODS & JOBSITE SAFETY AND ANY AND ALL DAMAGE TO EXISTING FACILITIES RESULTING FROM THE PERFORMANCE OF WORK UNDER THIS CONTRACT. CONTRACTOR SHALL CARRY LIABILITY INSURANCE NAMING OWNER, ENGINEER, THE VILLAGE AND ITS CONSULTANTS AS ADDITIONAL INSURERS. MINIMUM INSURANCE COVERAGE SHALL BE IN ACCORDANCE WITH DOT STANDARDS OR APPROVED BY ENGINEER.
 5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY CONSTRUCTION PERMITS, INCLUDING, A RIGHT-OF-WAY PERMIT FROM ROSCOE TOWNSHIP, THE CONSTRUCTION AND EROSION CONTROL PERMIT FROM THE VILLAGE AND CONNECTION PERMITS FROM THE FOUR RIVERS SANITATION AUTHORITY (FRSA). IN ADDITION TO THESE DRAWINGS AND THE STANDARDS REFERENCE HEREIN, ALL WORK MUST BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THESE PERMITS.
 6. CONTRACTOR SHALL LOCATE ALL UTILITIES PRIOR TO START OF CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR UTILITY RELOCATIONS REQUIRED BY THE WORK IN THIS CONTRACT.
CONTACT JULIE AT 1-800-892-0123 OR DIAL 811
 8. THE STORM WATER DETENTION AREA WAS CONSTRUCTED DURING THE PLAT NO. 1 PROJECT, BUT THE OVERFLOW SPILLWAY WILL BE REVISED TO ACCOMMODATE THE PLAT NO. 2 DEVELOPMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING POSITIVE DRAINAGE OF THE SITE TO THE EXISTING DETENTION AREA AT THE END OF EACH WORKING DAY.
 9. ALL PAVEMENT AND CURB & GUTTER REMOVAL SHALL BE COMPLETED WITH A CLEAN VERTICAL SAW CUT.
 10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING A CONSTRUCTION SITE THAT IS SAFE FOR ALL MOTORISTS, CONSTRUCTION PERSONNEL, PEDESTRIANS AND NEARBY RESIDENTS.
 11. THIS PROJECT INVOLVES MINIMAL IMPACT TO TRAFFIC ON VILLAGE ROADWAYS. NO LANE OR ROAD CLOSURES ARE ALLOWED AS PART OF THIS PROJECT.
 12. CONSTRUCTION VEHICLES SHALL OPERATE CAREFULLY WHEN EXITING THE JOBSITE ONTO BRADLEY DRIVE OR RACCOON RUN.

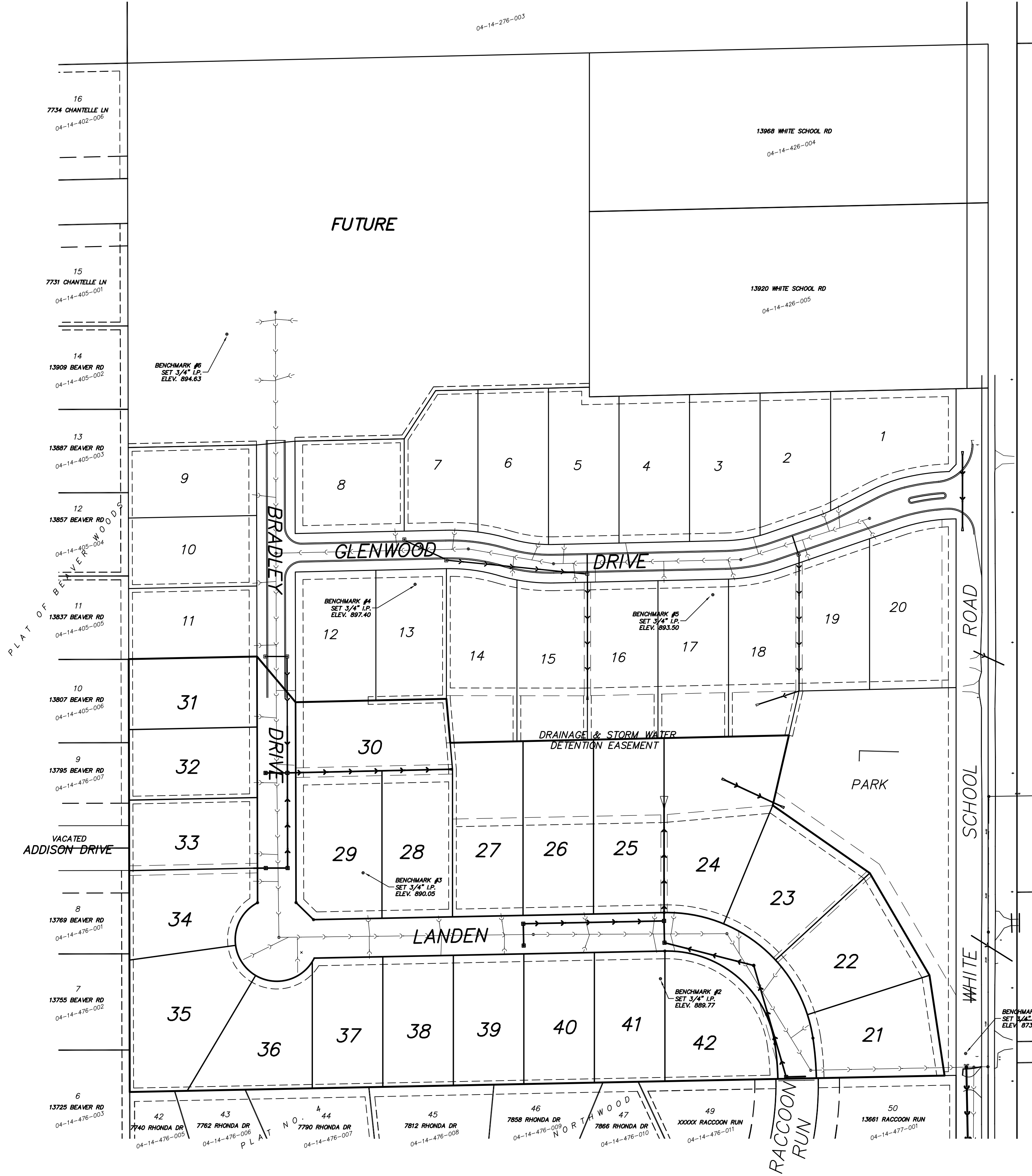
- EARTHWORK NOTES:**
1. LIMITED GEOTECHNICAL INFORMATION FOR THE PROPOSED IMPROVEMENT AREA IS AVAILABLE. CONTRACTORS SHALL CONTACT THE OWNER AT (779) 423-0548 TO REQUEST ACCESS TO THE SITE TO PERFORM TEST EXCAVATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE HIMSELF WITH THE EXISTING SOIL LOCATIONS PRIOR TO THE PREPARATION OF A CONSTRUCTION BID.
 2. TOPSOIL UNDER PAVEMENTS SHALL BE STRIPPED AND STOCKPILED ONSITE. INACTIVE STOCKPILES SHALL BE STABILIZED WITH EROSION CONTROL SEEDING AS OUTLINED IN THE EROSION CONTROL NOTES. TOPSOIL SHALL BE RE-Spread FOR SITE RESTORATION IN ALL LAWN AREAS OR USED IN THE CONSTRUCTION OF THE DETENTION AREA BERM AND OVERFLOW SPILLWAY. EXCESS TOPSOIL SHALL BE STORED ONSITE FOR FUTURE PROJECTS UNLESS OTHERWISE DIRECTED BY THE OWNER.
 3. SAND OR CLAY FILL MATERIAL IN EXCAVATED AREAS SHALL BE USED ONSITE AS ENGINEERED FILL UNDER ROADS, DRIVES OR BUILDINGS. INACTIVE STOCKPILES SHALL BE COVERED WITH 4" (MIN.) TOPSOIL AND STABILIZED WITH EROSION CONTROL SEEDING AS OUTLINED IN THE EROSION CONTROL NOTES. EXCESS FILL MATERIAL SHALL NOT BE HAULED OFFSITE UNLESS REQUESTED BY THE OWNER.
- FOUR RIVERS SANITATION AUTHORITY (FRSA) GENERAL NOTES:**
1. THE SANITARY SEWER WITHIN THE PROJECT LIMITS WAS INSTALLED AS PART OF THE PLAT NO. 1 CONSTRUCTION PROJECT, AND PUNCH LIST WORK WAS COMPLETED TO ALLOW CONNECTION OF THE SINGLE FAMILY HOMES CONSTRUCTED ON THE PLAT NO.1 LOTS.
 2. SANITARY SEWER WORK ON THIS PROJECT IS MINIMAL AND CONSISTS OF ADJUSTING THE EXISTING MANHOLE FRAMES AND LIDS WITHIN THE PROPOSED ROADWAY. THE FRSA CHIEF INSPECTOR, BEN CHRISTIANSEN SHALL BE CONTACTED AT (815) 209-7952 PRIOR TO ANY WORK ON THE EXISTING SANITARY SEWER. ALL SANITARY SEWER CONSTRUCTION PERFORMED IN THE ABSENCE OF A FRSA INSPECTOR WILL NOT BE ACCEPTED.
 3. THE SANITARY SEWER SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR WATER AND SEWER CONSTRUCTION IN THE STATE OF ILLINOIS, LATEST EDITION, AND THE REQUIREMENTS OF FRSA.
 4. NOTICE ALLOWING CONNECTIONS MUST BE ISSUED BY THE FRSA BEFORE INDIVIDUAL SERVICE CONNECTIONS WILL BE PERMITTED UNDER THE STANDARD FRSA SERVICE CONNECTION PERMIT PROCESS.
 5. THE PERMIT HOLDER IS RESPONSIBLE FOR THE SANITARY SEWER SYSTEM WORKMANSHIP AND MATERIALS FOR 2 YEARS AFTER THE FRSA ISSUED NOTICE OF ACCEPTING THE SEWER, AND FOR SANITARY SEWER TRENCH SETTLEMENT FOR A PERIOD OF THREE (3) YEARS AFTER COMPLETION OF THE PROJECT.
 6. THE SANITARY SEWER SERVICE FOR EACH LOT IN PLAT NO. 2 OF GLENWOOD ESTATES WAS INSTALLED AS PART OF THE PLAT NO. 1 IMPROVEMENT PROJECT. THE LOCATION AND DEPTH OF THE EXISTING PIPE HAS BEEN SHOWN BASED ON FRSA RECORD DRAWINGS. PRIOR TO THE START OF SINGLE FAMILY HOME CONSTRUCTION ON AN INDIVIDUAL LOT, THE EXISTING SANITARY SEWER SERVICE SHALL BE PHYSICALLY LOCATED. IF THE INVERT ELEVATION IS MORE THAN 0.25' HIGHER THAN SHOWN IN THESE DRAWINGS, THE ENGINEER SHALL BE CONTACTED IMMEDIATELY.

- WATER MAIN NOTES:**
1. ALL LOTS WILL BE SERVED BY A PRIVATE WELL TO BE INSTALLED DURING CONSTRUCTION OF THE SINGLE FAMILY HOME. THERE ARE NO WATER MAIN IMPROVEMENTS INVOLVED WITHIN THIS PROJECT.

- PAVEMENT NOTES:**
1. HOT MIX ASPHALT (HMA) PAVEMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH IDOT SECTION 406 AND MEET THE FOLLOWING DESCRIPTION.
HMA SURFACE COURSE: PO 64-22
HMA BINDER COURSE: PO 64-22
• 4% DESIGN AIR VOIDS AT N50
• MIXTURE COMPOSITION IL-9.5 OR SMA-9.5
• FRICTION AGGREGATE C
 2. NO HMA MIXES SHALL BE IDOT APPROVED
 3. NO HMA PAVING SHALL BE COMPLETED OVER DETERIORATED OR DIRTY AGGREGATE BASE COURSE OR HMA BINDER COURSE.
 3. PRIOR TO PLACEMENT OF THE AGGREGATE BASE COURSE, THE PROPERLY PREPARED SUBGRADE SHALL UNDERGO A DENSITY TEST PER IDOT ARTICLE 205.06 (ILLINOIS MODIFIED AASHTO T 310 DIRECT TRANSMISSION DENSITY). ALL UNSUITABLE AREAS SHALL BE UNDERCUT PER THE DETAIL.
 4. PRIOR TO PAVING OF THE HMA BINDER COURSE, THE PREPARED AGGREGATE BASE COURSE SHALL UNDERGO A PROOF ROLL TEST PER IDOT SECTION 351. UNSUITABLE AREAS SHALL BE UNDERCUT PER THE DETAIL.
 5. PRIOR TO PAVING ON PREPARED AGGREGATE BASES, A PRIME COAT SHALL BE APPLIED. PRIOR TO PAVING ON MILLED HMA AND NEW HMA BINDER COURSE, A TACK COAT SHALL BE APPLIED. THIS WORK SHALL BE COMPLETED IN ACCORDANCE WITH IDOT ARTICLE 406.05.
 6. CONCRETE CURB & GUTTER SHALL BE CONSTRUCTED PER IDOT SECTION 606.

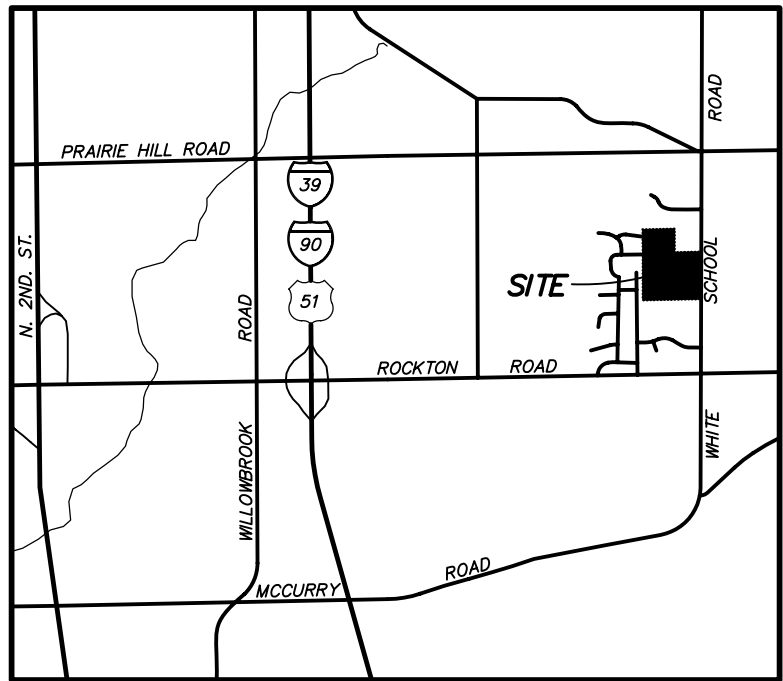
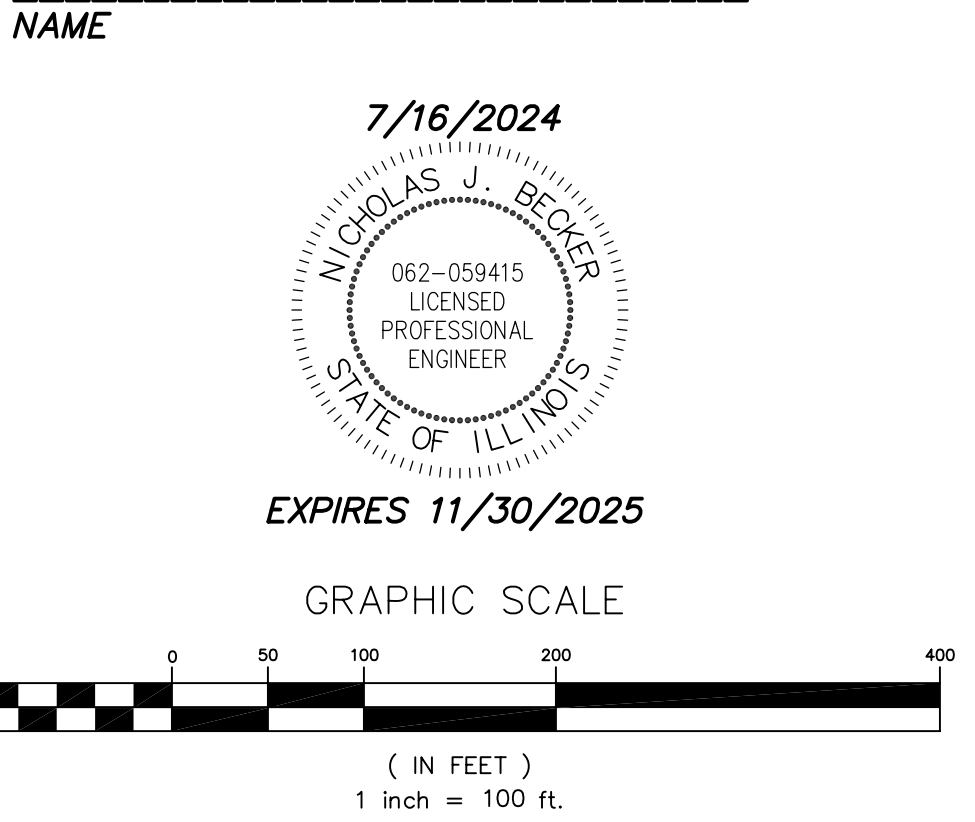
- EROSION CONTROL NOTES:**
1. ALL WORK SHALL BE IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL (IUM) GOVERNING EROSION AND SEDIMENT CONTROL AND THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP). NOTE THAT THE SWPPP IS A SEPARATE WRITTEN DOCUMENT THAT IS NOT A PART OF THIS PLAN SET.
 2. THE EROSION AND SEDIMENT CONTROL PLAN IS DYNAMIC AND CAN BE CHANGED TO MEET DIFFERING SITE CONDITIONS.
 3. THE STABILIZED CONSTRUCTION ENTRANCES, SILT FENCE AND PROTECTION OF EXISTING STORM INLETS / PIPE CULVERTS SHALL BE INSTALLED BEFORE THE START OF EARTHWORK OR OTHER GROUND DISTURBING ACTIVITIES.
 4. IN AREAS WHERE WORK HAS TEMPORARILY CEASED, SOIL STABILIZATION SHALL BE IMPLEMENTED. EROSION CONTROL SEEDING USED FOR TEMPORARY STABILIZATION SHALL BE COMPLETED IN ACCORDANCE WITH IUM PRACTICE STANDARD 965. MULCHING FOR SOIL STABILIZATION SHALL BE COMPLETED PER IUM PRACTICE STANDARD 875. SOIL STABILIZATION SHALL BE IMPLEMENTED AS SOON AS POSSIBLE AFTER SUSPENSION OF WORK.
 5. THE STABILIZED CONSTRUCTION ENTRANCE SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH IUM PRACTICE STANDARD 930. ALL CONSTRUCTION TRAFFIC SHALL ENTER/EXIT THE JOBSITE FROM THE STABILIZED CONSTRUCTION ENTRANCE. IF ADDITIONAL ACCESS POINTS ARE USED FOR THE JOBSITE INGRESS/EGRESS, ADDITIONAL STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED.
 6. DISTURBED GROUND IN LAWN / LANDSCAPE AREAS SHALL BE RESTORED WITH 6" (MIN.) TOPSOIL, LAWN SEED AND FERTILIZER IN ACCORDANCE WITH IDOT SECTIONS 211 AND 280. THE TOPSOIL SHALL CONSIST OF ONSITE MATERIAL THAT WAS STRIPPED IN ANTICIPATION OF PAVEMENT OR BUILDING CONSTRUCTION. SEED MIXTURES IN THE RIGHT-OF-WAY AREAS SHALL BE IDOT CLASS 1A (SALT TOLERANT LAWN). SEED MIXTURES ON PRIVATE PROPERTY, INCLUDING ANY DRAINAGE, DETENTION OR STORM SEWER EASEMENT SHALL BE IDOT CLASS 1B (LOW MAINTENANCE LAWN).
 7. HEAVY DUTY EROSION CONTROL BLANKET AND TURF REINFORCEMENT MAT SHALL BE INSTALLED FOR GROUND STABILIZATION AS SHOWN IN THE PLANS. HEAVY DUTY EROSION CONTROL BLANKET SHALL BE S150 OR SC150 BY NORTH AMERICAN GREEN. TURF REINFORCEMENT MAT (TRM) SHALL BE TRM C350 BY NORTH AMERICAN GREEN. ALL DISTURBED SOILS ARE TO BE COVERED WITH HEAVY DUTY EROSION CONTROL BLANKET OR TRM SHALL BE STABILIZED WITH STANDARD EROSION CONTROL BLANKET, CONFORMING TO PRODUCT DS75 BY NORTH AMERICAN GREEN. SUBSTITUTE PRODUCTS ARE ALLOWED IF APPROVED BY THE ENGINEER.
 8. AN INSPECTION OF ALL EROSION CONTROL AND SLOPE STABILIZATION MEASURES SHALL BE PERFORMED EVERY 7 DAYS AND AFTER EACH RAINFALL EVENT OF 0.5" OR MORE. ANY DEFICIENCIES NOTED DURING THIS INSPECTION SHALL BE REPAIRED AS DIRECTED BY THE EROSION CONTROL INSPECTOR. THE REPAIR TIME MAY VARY BASED ON THE URGENCY OF THE SITUATION, BUT SHALL NOT EXCEED 7 DAYS. DURING EXTENDED PERIODS WHEN THE JOBSITE IS INACTIVE DUE TO WINTER OR FROZEN GROUND CONDITIONS, EROSION CONTROL INSPECTIONS SHALL BE PERFORMED A MINIMUM OF ONE TIME PER MONTH, AND AFTER ANY RAINFALL GREATER THAN 0.5" OR EQUIVALENT SNOW MELT.
 9. ADJACENT ROADWAYS SHALL BE INSPECTED AT THE END OF EACH WORK DAY. PAVEMENT SHALL BE CLEANED OR SWEEPED AS NECESSARY TO PREVENT TRACK-OFF OF SEDIMENT.
 10. ROLLED EROSION CONTROL PRODUCTS SHALL BE INSTALLED AS TEMPORARY DITCH CHECKS AS SHOWN IN THE DRAWINGS AND DETAILS. THE DITCH CHECKS SHALL EXTEND ACROSS THE ENTIRE CROSS-SECTION OF THE DRAINAGE DITCH AND/OR SWALE.
 11. FILTERS SHALL BE INSTALLED IMMEDIATELY AFTER THE STORM INLETS HAVE BEEN INSTALLED. FOR SPECIAL INLETS WITH REAR OPENINGS, A FILTER SHALL BE INSTALLED ON BOTH SIDES OF THE STRUCTURE, FOR OPEN GRATE STORM MANHOLES, A DROP-IN FILTER SHALL BE USED.
 12. ALL TOPSOIL STOCKPILES SHALL HAVE SEDIMENT BARRIER PLACED AROUND IT ON THREE SIDES. THE FOURTH SIDE SHALL BE GRADED SO THAT FLOWS TOWARDS THE STOCKPILE OR RUNOFF FROM THE STOCKPILE WILL BE TRAPPED TO ALLOW SEDIMENT TO SETTLE OUT.
 13. THE CONTRACTOR SHALL CONSTRUCT AND MAINTAIN TEMPORARY CONCRETE WASHOUT FACILITIES PER IUM PRACTICE STANDARD 954. THE LOCATIONS OF THE WASHOUT FACILITIES SHOWN IN THE DRAWINGS ARE APPROXIMATE AND MAY BE RELOCATED AS NEEDED.
 14. ALL TEMPORARY EROSION CONTROL MEASURES SHALL REMAIN IN PLACE UNTIL GROWTH OF THE FINAL TURF PROVIDES AT LEAST 70 PERCENT COVERAGE AT ANY LOCATION ON THE CONSTRUCTION SITE. THE FINAL INSPECTION PRIOR TO REMOVING ALL TEMPORARY EROSION AND SEDIMENT CONTROLS. AFTER THE FINAL INSPECTION IS COMPLETE AND TEMPORARY EROSION CONTROL MEASURES HAVE BEEN REMOVED, THE OWNER SHALL BE RESPONSIBLE FOR SUBMITTING THE NOTICE OF TERMINATION FORM TO THE IEPA TO CLOSE-OUT THE NPDES PERMIT.

ROADWAY IMPROVEMENT PLANS FOR PLAT NO. 2 OF GLENWOOD ESTATES



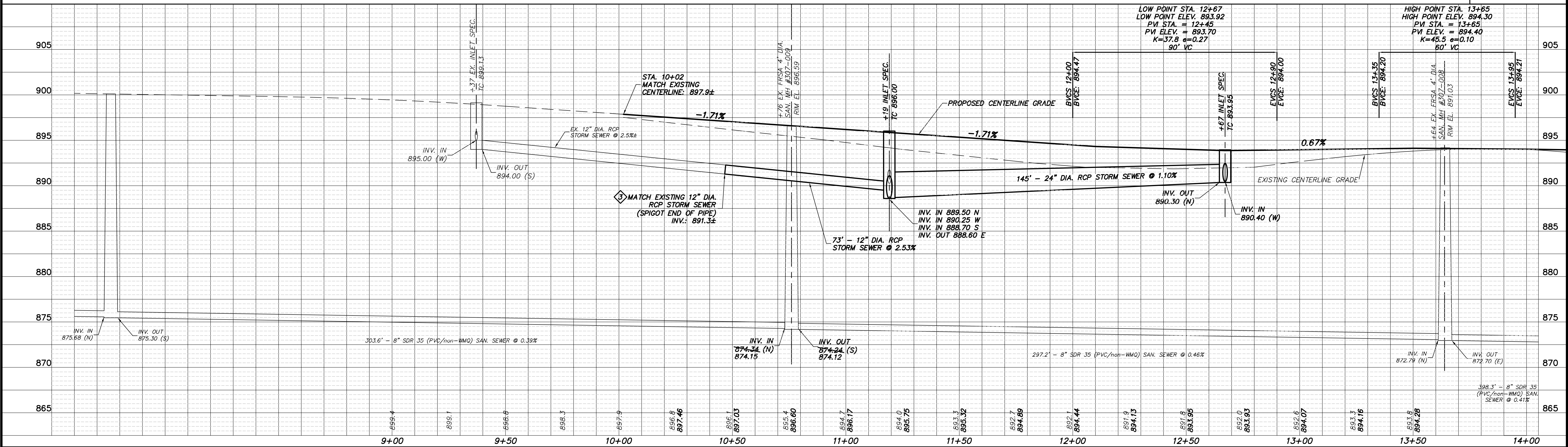
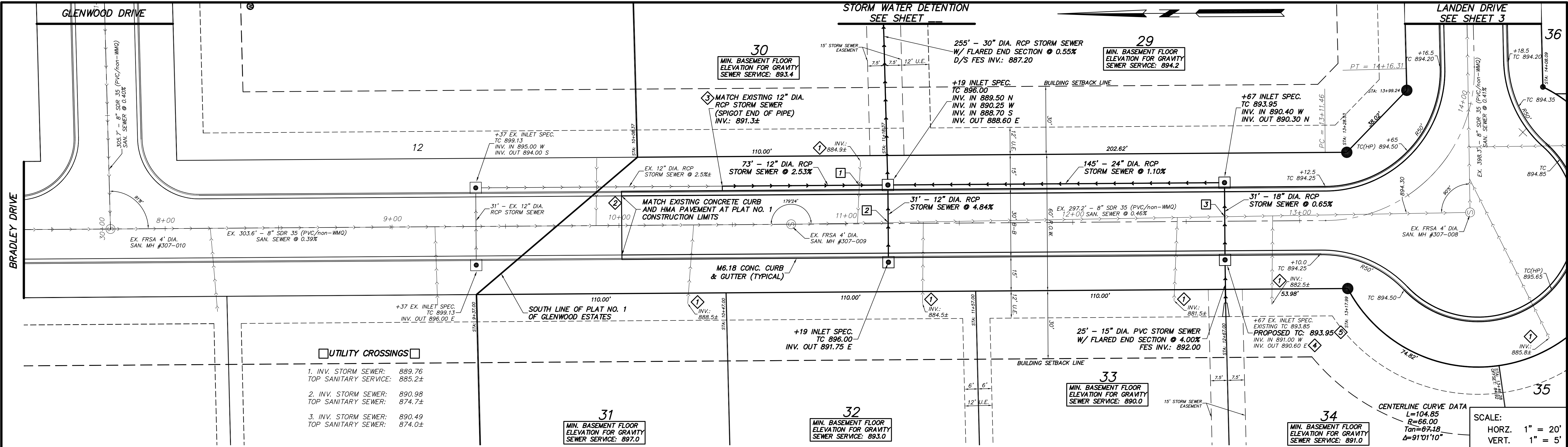
SHEET INDEX	
SHEETS	DESCRIPTION
1.	COVER SHEET
2.	BRADLEY DRIVE
3.	LANDEN DRIVE
4.	LANDEN DRIVE
5.	GRADING/EROSION PLAN
6.	STORM SEWER DETAIL
7.	EROSION CONTROL DETAIL

I HEREBY CERTIFY THAT THE ELEVATIONS SHOWN HEREON WERE BASED ON WINGIS MONUMENT 21.2N - 12.1E WHICH HAS A PUBLISHED ELEVATION OF 867.65 AND IS LOCATED NEAR THE INTERSECTION OF ROCKTON ROAD AND COUNTY LINE ROAD.



LOCATION MAP
PREPARED BY:
R.K. JOHNSON & ASSOCIATES, INC.
CONSULTING CIVIL ENGINEERS AND LAND SURVEYORS
1515 WINDSOR ROAD LOVES PARK, ILLINOIS 61111
(815) 633-5097 (815) 633-4593 (FAX)
ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004994
FEBRUARY 29, 2024 JOB NO. 14935-2
SHEET 1 OF 7
REVISED PER VILLAGE COMMENTS: 7-16-2024

CALL J.U.L.I.E. 1-800-892-0123
48 HOURS BEFORE YOU DIG.
EXCLUDING SAT., SUN. & HOLIDAYS



PLAN NOTES

- EXISTING 4" DIA. SDR 26 PVC (ASTM D-2241) SANITARY SEWER SERVICE @ 1.0% MIN. SLOPE. ALIGNMENT AND ELEVATION BASED ON RECORD DRAWING INFORMATION.
- MATCH EXISTING HMA PAVEMENT AND CONCRETE CURB AND GUTTER AT THE LIMITS OF THE PLAT NO. 1 CONSTRUCTION PHASE. CONTRACTOR SHALL SAW CUT AND REMOVE NARROW STRIP (1' APPROX.) EXISTING HMA SO THAT THE NEW ROADWAY CAN MEET THE EXISTING AT A CLEAN, VERTICAL BUTT JOINT.
- AT THE START OF THE PROJECT THE CONTRACTOR SHALL VERIFY THE INVERT ELEVATION AT THE DOWNSTREAM END OF THE EXISTING STORM SEWER. IF THE
- INVERT VARIES SIGNIFICANTLY FROM 891.30, THE ENGINEER SHALL BE CONTACTED IMMEDIATELY. THE SPIGOT END OF THE PIPE SHALL BE CLEANED AND PREPARED WITH MASTIC PRIOR TO INSTALLATION OF NEW RCP STORM SEWER.
- THE INVERT ELEVATION OF THE EXISTING SPECIAL INLET SHALL BE VERIFIED BY THE CONTRACTOR AT THE START OF THE PROJECT. IF THE ELEVATION VARIES SIGNIFICANTLY FROM 890.60, THE ENGINEER SHALL BE CONTACTED IMMEDIATELY.
- ADJUST STORM INLET TO FINAL GRADE PER IDOT SECTION 602.

PLAT NO. 2 OF GLENWOOD ESTATES
WINNEBAGO COUNTY, ILLINOIS

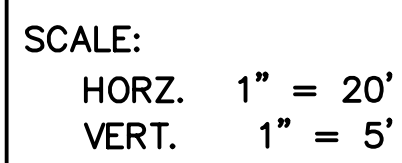
BRADLEY DRIVE
STA: 9+00 - 14+00

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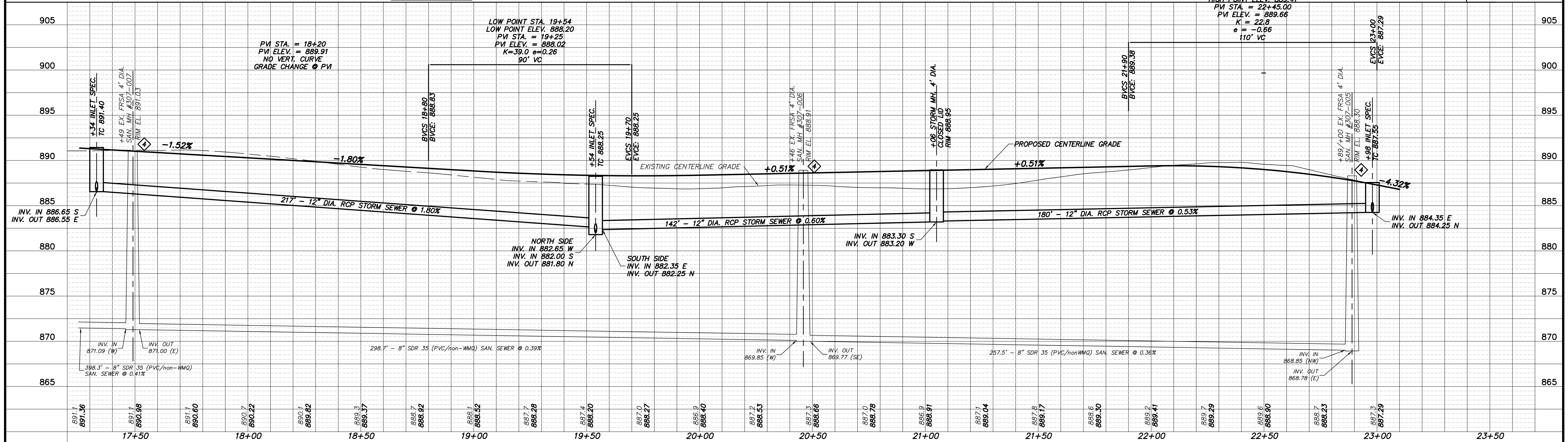
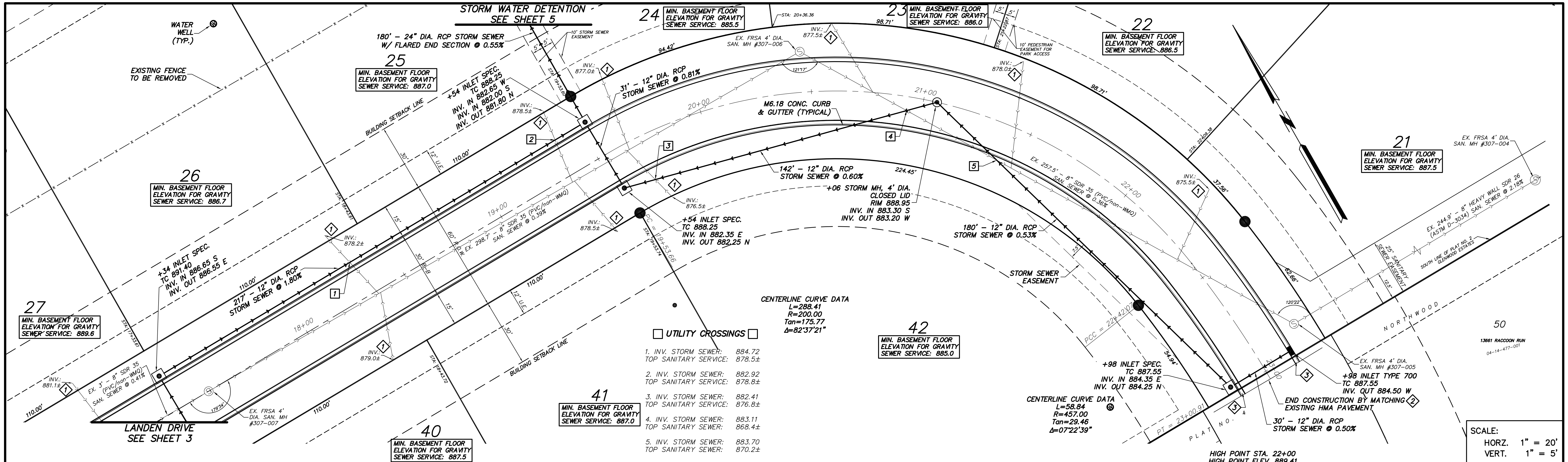
REVISIONS	
1	7/16/2024 PER VILLAGE COMMENTS

ISSUED FOR: 2/29/2024
ISSUE DATE: 2/29/2024
PROJECT NO.: 14935-2

2 OF 7



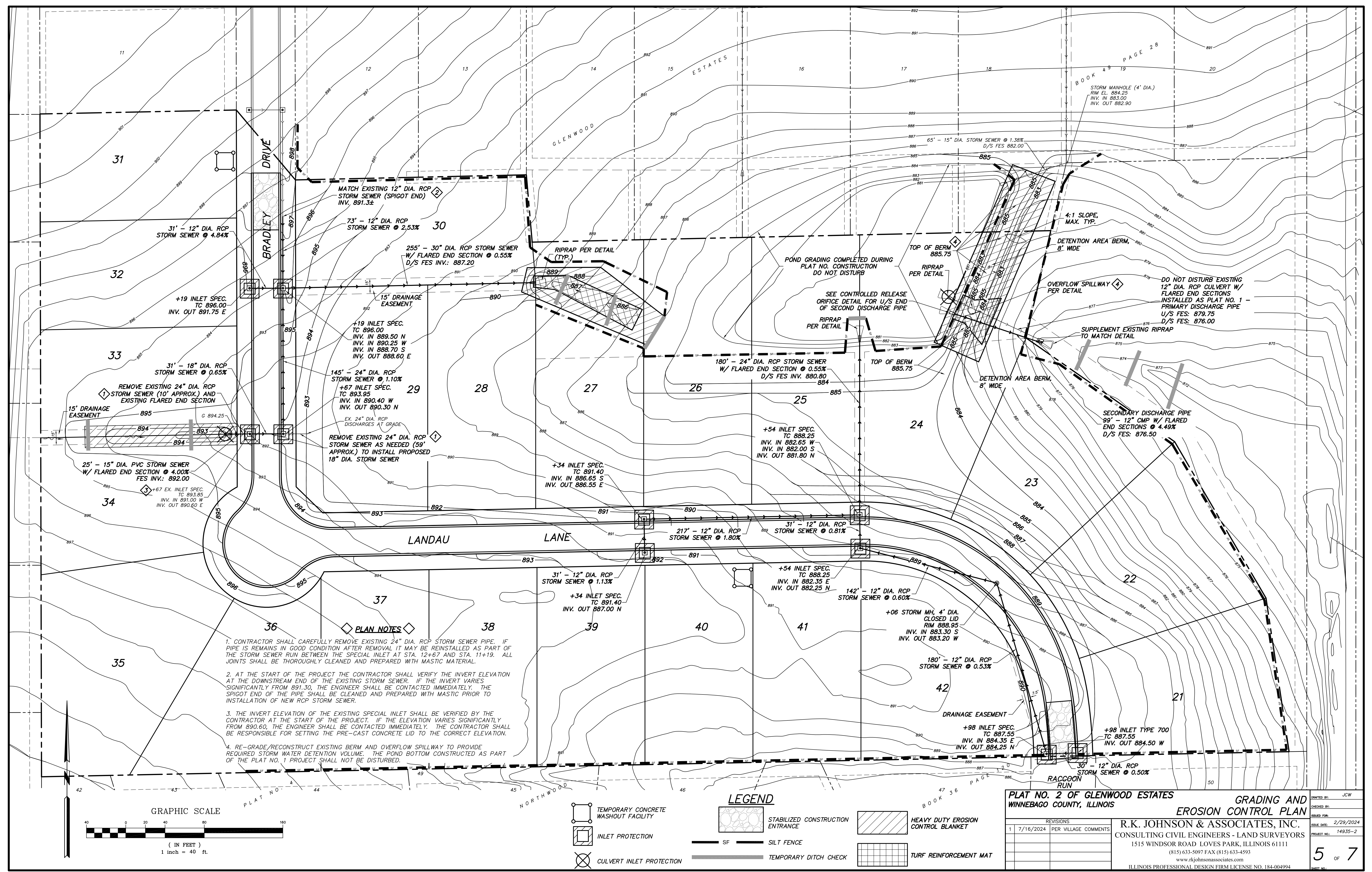
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|--|-----------|---|---|
| PLAT NO. 2 OF GLENWOOD ESTATES
WINNEBAGO COUNTY, ILLINOIS | | LANDEN DRIVE
STA: 13+00 - 19+00 | |
| REVISIONS | | R.K. JOHNSON & ASSOCIATES, INC. | |
| 1 | 7/16/2024 | PER VILLAGE COMMENTS | CONSULTING CIVIL ENGINEERS - LAND SURVEYORS
1515 WINDSOR ROAD LOVES PARK, ILLINOIS 61111
(815) 633-5097 FAX (815) 633-4593
www.rkjohnsonassociates.com
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| | | DRAFTED BY: JCW
CHECKED BY:
ISSUED FOR:
ISSUE DATE: 2/29/2024
PROJECT NO: 14935-2 | |
| | | OF 7 | |



- PLAN NOTES**
- EXISTING 4" DIA. SDR 26 PVC (ASTM D-2241) SANITARY SEWER SERVICE @ 1.0% MIN. SLOPE. ALIGNMENT AND ELEVATION BASED ON RECORD DRAWING INFORMATION.
 - MATCH EXISTING HMA PAVEMENT ON RACCOON RUN. CONTRACTOR SHALL SAW CUT AND REMOVE NARROW STRIP (1' APPROX.) EXISTING HMA SO THAT THE NEW ROADWAY CAN MEET THE EXISTING AT A CLEAN, VERTICAL BUTT JOINT. REMOVE EXISTING TEMPORARY TURN AROUND PAVEMENT AS REQUIRED BY THE VILLAGE.
 - TAPER CURB HEAD OVER 3'.
 - PRIOR TO HMA PAVING SANITARY MANHOLES SHALL BE ADJUSTED TO FINAL GRADE PER FRSA REQUIREMENTS.

PLAT NO. 2 OF GLENWOOD ESTATES WINNEBAGO COUNTY, ILLINOIS		LANDEN DRIVE STA: 17+25 - 23+01		DATE: 2/29/2024
REVISIONS		R.K. JOHNSON & ASSOCIATES, INC.		ISSUED FOR: 14935-2
1	7/16/2024 PER VILLAGE COMMENTS	CONSULTING CIVIL ENGINEERS - LAND SURVEYORS		PROJECT NO.: 14935-2
		1515 WINDSOR ROAD LOVES PARK, ILLINOIS 61111		4 OF 7
		(815) 633-5097 FAX (815) 633-4593		
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		ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004994		

PAGE 28
BOOK 49
STORM MANHOLE (4' DIA.)
RIM EL. 884.25
INV. IN 883.00
INV. OUT 882.90



- PLAN NOTES**
- CONTRACTOR SHALL CAREFULLY REMOVE EXISTING 24" DIA. RCP STORM SEWER PIPE. IF PIPE IS REMAINS IN GOOD CONDITION AFTER REMOVAL IT MAY BE REINSTALLED AS PART OF THE STORM SEWER RUN BETWEEN THE SPECIAL INLET AT STA. 12+67 AND STA. 11+19. ALL JOINTS SHALL BE THOROUGHLY CLEANED AND PREPARED WITH MASTIC MATERIAL.
 - AT THE START OF THE PROJECT THE CONTRACTOR SHALL VERIFY THE INVERT ELEVATION AT THE DOWNSTREAM END OF THE EXISTING STORM SEWER. IF THE INVERT VARIES SIGNIFICANTLY FROM 891.30, THE ENGINEER SHALL BE CONTACTED IMMEDIATELY. THE SPIGOT END OF THE PIPE SHALL BE CLEANED AND PREPARED WITH MASTIC PRIOR TO INSTALLATION OF NEW RCP STORM SEWER.
 - THE INVERT ELEVATION OF THE EXISTING SPECIAL INLET SHALL BE VERIFIED BY THE CONTRACTOR AT THE START OF THE PROJECT. IF THE ELEVATION VARIES SIGNIFICANTLY FROM 890.60, THE ENGINEER SHALL BE CONTACTED IMMEDIATELY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SETTING THE PRE-CAST CONCRETE LID TO THE CORRECT ELEVATION.
 - RE-GRADE/RECONSTRUCT EXISTING BERM AND OVERFLOW SPILLWAY TO PROVIDE REQUIRED STORM WATER DETENTION VOLUME. THE POND BOTTOM CONSTRUCTED AS PART OF THE PLAT NO. 1 PROJECT SHALL NOT BE DISTURBED.

LEGEND

- TEMPORARY CONCRETE WASHOUT FACILITY
- INLET PROTECTION
- CULVERT INLET PROTECTION
- STABILIZED CONSTRUCTION ENTRANCE
- SILT FENCE
- TEMPORARY DITCH CHECK
- HEAVY DUTY EROSION CONTROL BLANKET
- TURF REINFORCEMENT MAT

PLAT NO. 2 OF GLENWOOD ESTATES
WINNEBAGO COUNTY, ILLINOIS

GRADING AND EROSION CONTROL PLAN

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ISSUED FOR: 2/29/2024
PROJECT NO.: 14935-2
SHEET NO.: 5 OF 7

REVISIONS	
1	7/16/2024 PER VILLAGE COMMENTS

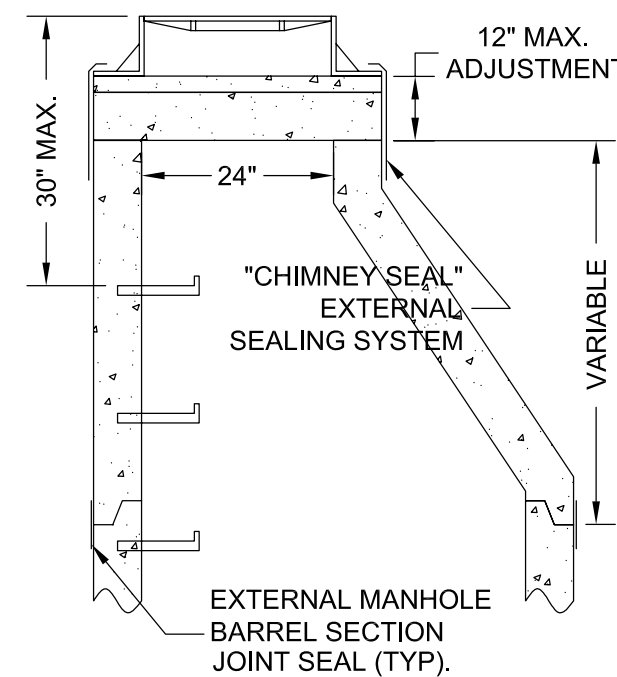
NOTES:

- 1.MANHOLE FRAMES & LIDS SHALL BE PER THE TABLE BELOW.
- 2.FOR MANHOLES CONNECTED TO MAINS 18" DIAMETER OR LARGER, OR FOR MANHOLES LOCATED IN FLOOD PRONE AREAS, FRAMES & LIDS SHALL BE THE BOLT DOWN TYPE.
- 3.ALLOWABLE TYPES OF ADJUSTING RINGS INCLUDE PRECAST CONCRETE (4" HEIGHT MIN.), & EXPANDED POLYPROPYLENE (EPP). THESE CAN BE USED IN CONJUNCTION WITH EACH OTHER, EXCEPT THAT A PRECAST RING SHALL NOT BE PLACED OVER AN EPP RING.
- 4.FOR PRECAST ADJUSTING RINGS, ALL ADJUSTING RING JOINTS AS WELL AS THE FRAME TO ADJUSTING RING JOINT SHALL BE SEALED WITH TWO 1" BEADS OF PRE-FORMED RUBBER BUTYL JOINT SEALANT. WHEN A FRAME REQUIRES PITCHING, EPP TAPER RINGS SHALL BE USED PER NOTE 5.
- 5.FOR EPP ADJUSTING RINGS, RINGS SHALL BE INSTALLED ACCORDING TO MANUFACTURER'S INSTRUCTIONS. WHEN A FRAME REQUIRES PITCHING, THE TOP RING SHALL BE A TAPERED ADJUSTMENT RING PER MANUFACTURER'S INSTRUCTIONS.
- 6.NO TARRING OR GROUTING IS ALLOWED ON THE INSIDE OF MANHOLE OR ADJUSTMENT JOINTS.
- 7.MAXIMUM MANHOLE ADJUSTMENT IS 12". MINIMUM ADJUSTMENT IS 4" UNLESS OFF-ROAD OR IN CURB & GUTTER ROADWAY.
- 8.MANHOLE FRAMES SHALL BE SET 1/4" MIN. TO 3/8" MAX. BELOW PAVED SURFACES, AND AT FINAL GRADE IN TURF AREAS.
- 9.WHEN ADJUSTING EXISTING MANHOLES, THE ENTIRE EXISTING ADJUSTMENT SHALL BE REMOVED AND REPLACED.
- 10.THE COMBINATION OF NEW ADJUSTING RINGS

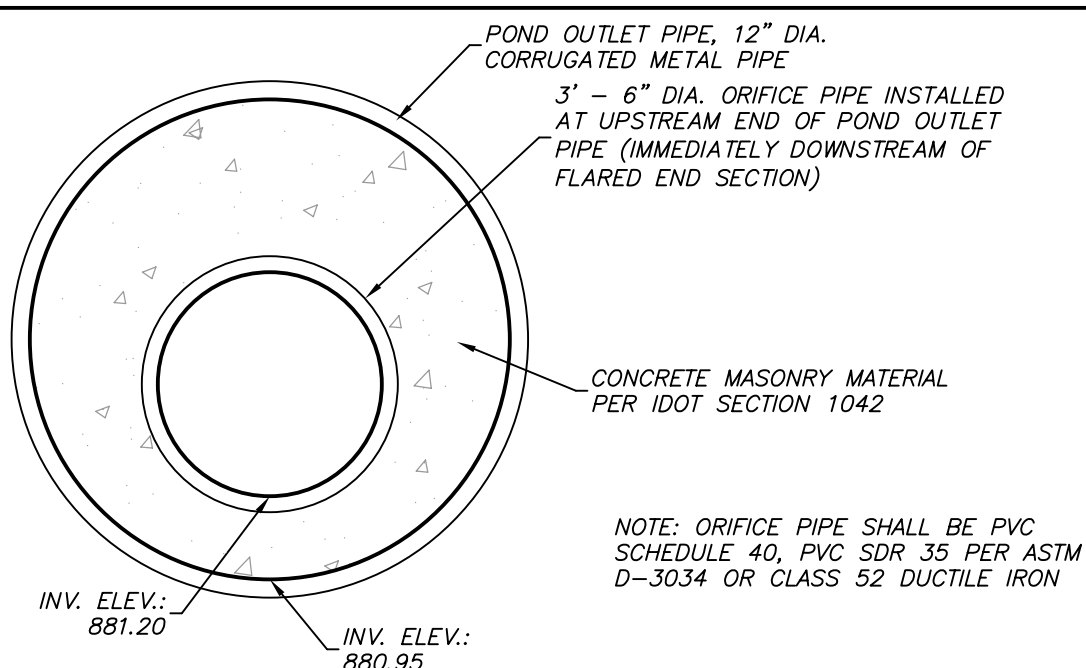
SHALL BE SUCH THAT THE MINIMUM NUMBER OF RINGS POSSIBLE ARE USED.

- 11.FLAT-TOPS ARE NOT PERMITTED ON 4" OR 5" DIA. MANHOLES.
- 12.REPLACEMENT OF EXISTING BARREL SECTIONS MAY BE REQUIRED TO MEET THE ABOVE REQUIREMENTS.
- 13.MANHOLE ADJUSTMENT INSERT RISER RINGS ARE NOT APPROVED FOR USE.

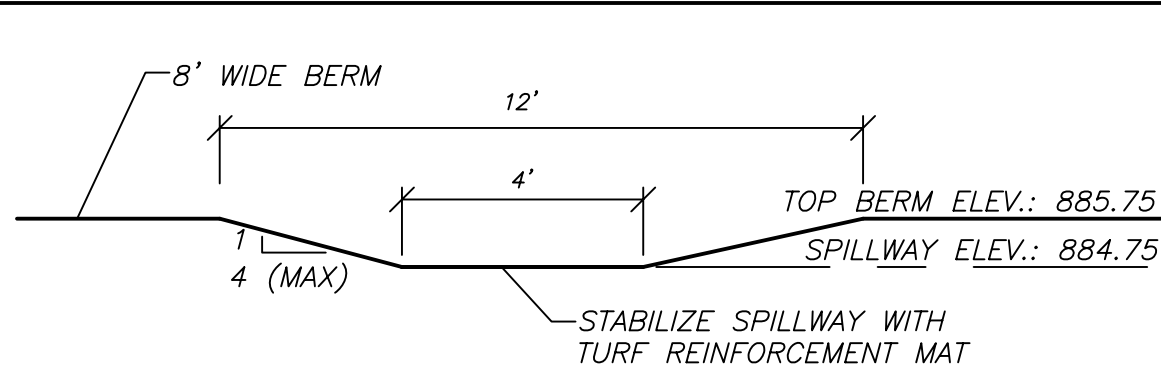
APPROVED FRAME & LID TABLE				
TYPE	NEENAH FRAME	NEENAH LID	EAST JORDAN FRAME	EAST JORDAN LID
REGULAR	1670-2004	R-1670-0358	00111711	00111732
LOW PROFILE	1670-2008	R-1670-0358	—	—
BOLT DOWN	1915JT08	—	—	—



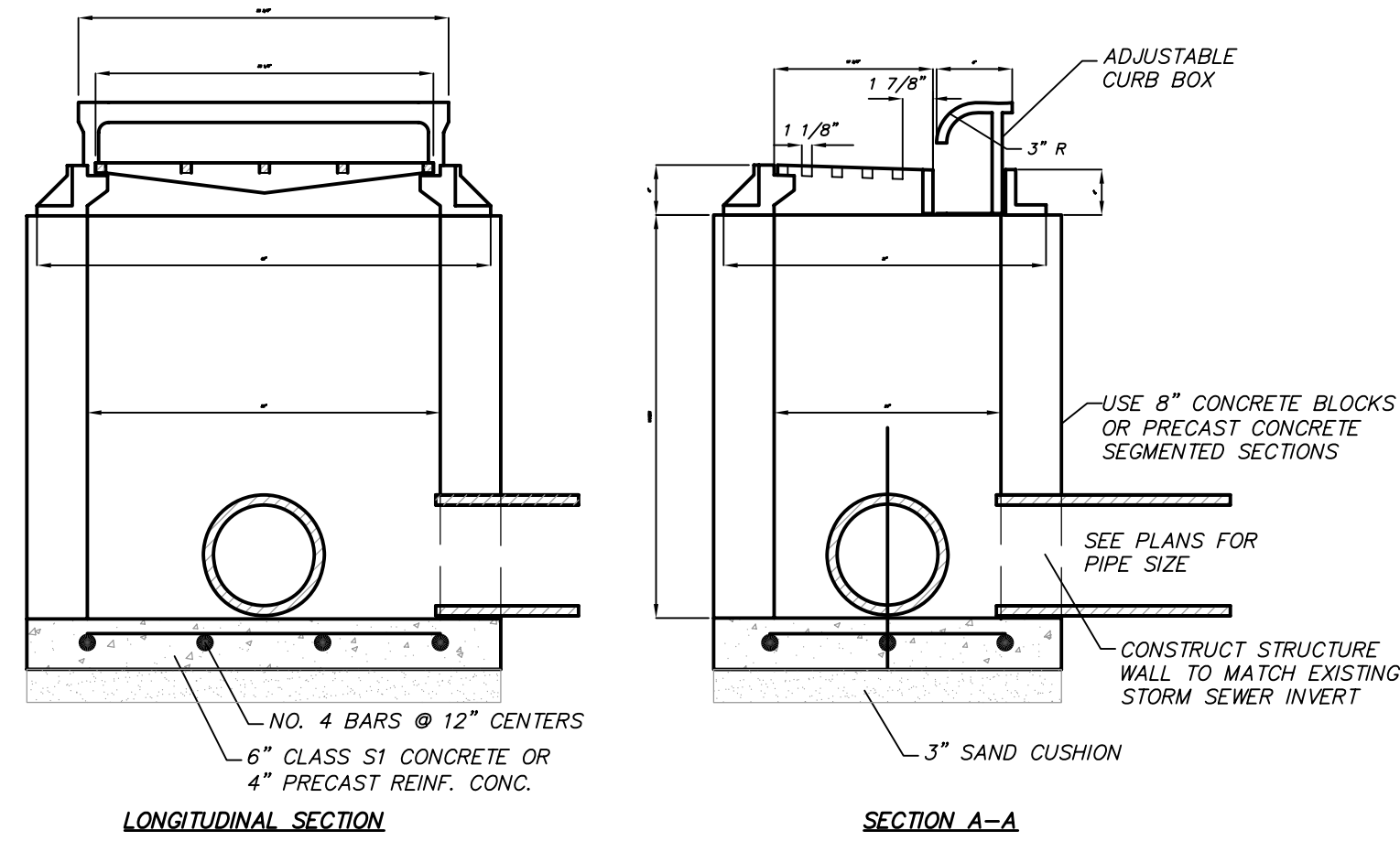
MANHOLE ADJUSTMENT DETAIL
(FOR ADJUSTMENT OF BOTH NEW & EXISTING MANHOLES)



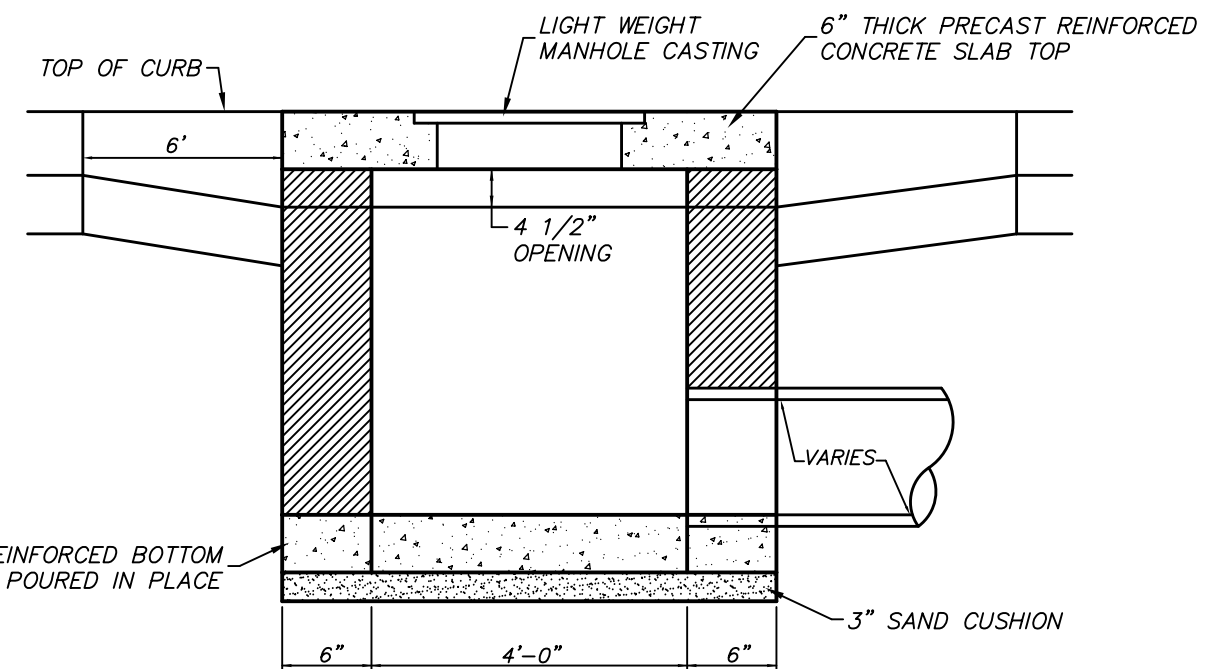
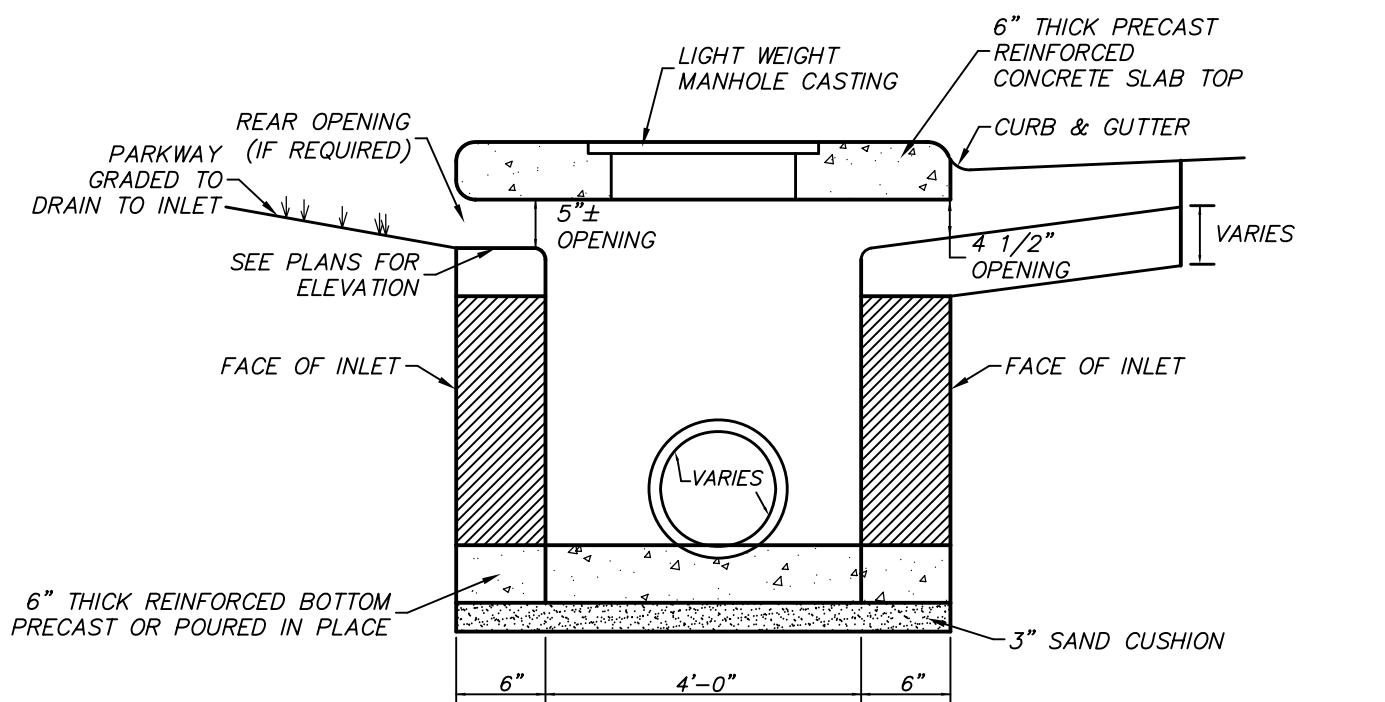
SECONDARY DISCHARGE PIPE CONTROLLED RELEASE ORIFICE



DETENTION AREA OVERFLOW SPILLWAY

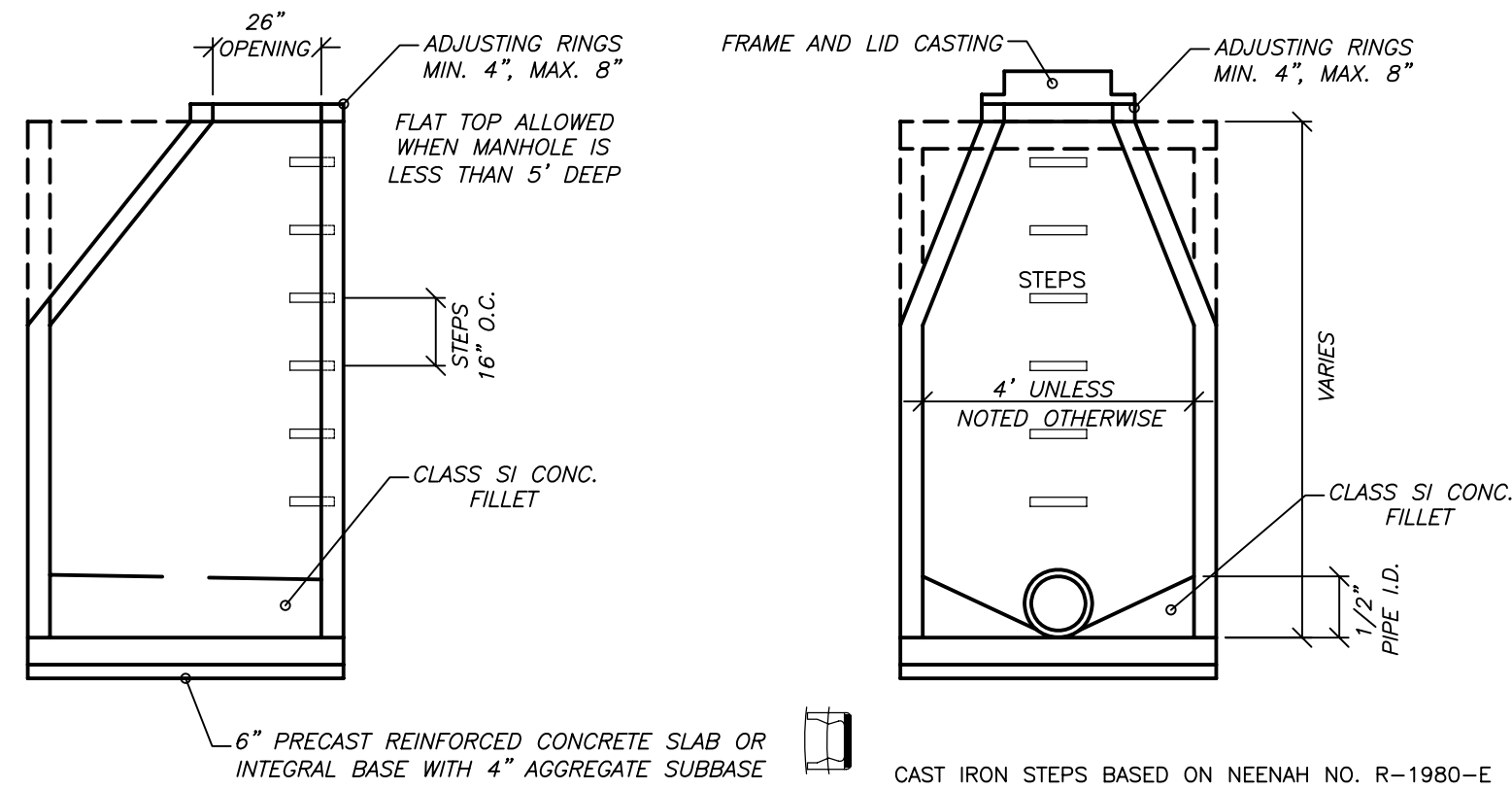


STANDARD INLET TYPE 700



- NOTES:**
1. ALL STORM SEWER MANHOLES AND INLETS SHALL BE PRECAST CONCRETE STRUCTURES.
 2. REINFORCEMENT SHALL BE IN ACCORDANCE WITH IDOT DISTRICT 2 STANDARD 79.4g.

INLET SPECIAL



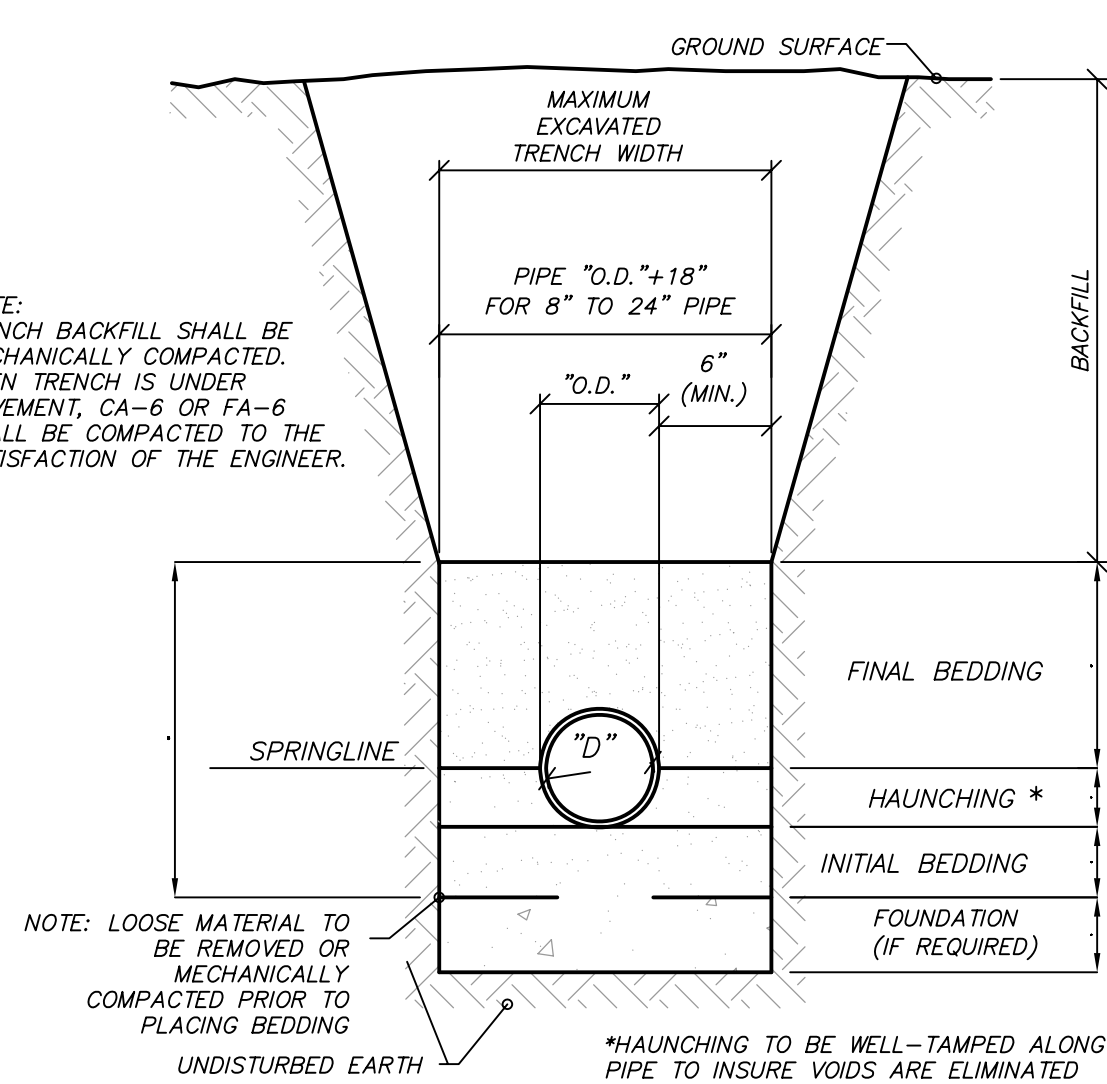
FRAME AND LID CASTINGS:
LID SHALL BE OPEN OR CLOSED AS INDICATED ON THE DRAWINGS. FRAME & OPEN LID SHALL BE NEENAH R-2502. FRAME & CLOSED LID SHALL BE NEENAH R-1772. STANDARD CURB BOX SHALL BE NEENAH R-3065 OR SIMILAR. SEAL FRAME TO MANHOLE WITH KENT-SEAL OR EQUAL. SET TOP OF CASTING FLUSH WITH PROPOSED GRADE OR AS INDICATED ON DRAWINGS.

MANHOLE WALL AND BOTTOM SLAB:
WALL THICKNESS AND REINFORCEMENT FOR MANHOLE BARREL SECTIONS, BOTTOM SLABS AND FLAT TOP SLABS SHALL MEET THE REQUIREMENTS OF IDOT STANDARD 602401-06 FOR 4" DIA. MANHOLE AND IDOT STANDARD 602402-02 FOR 5" DIA. MANHOLES. FOR 6" DIA. MANHOLES THE BARREL SECTION, BASE SLAB, RISER SECTION AND FLAT TOP SLAB SHALL MEET THE REQUIREMENTS OF IDOT STANDARD 602406-11.

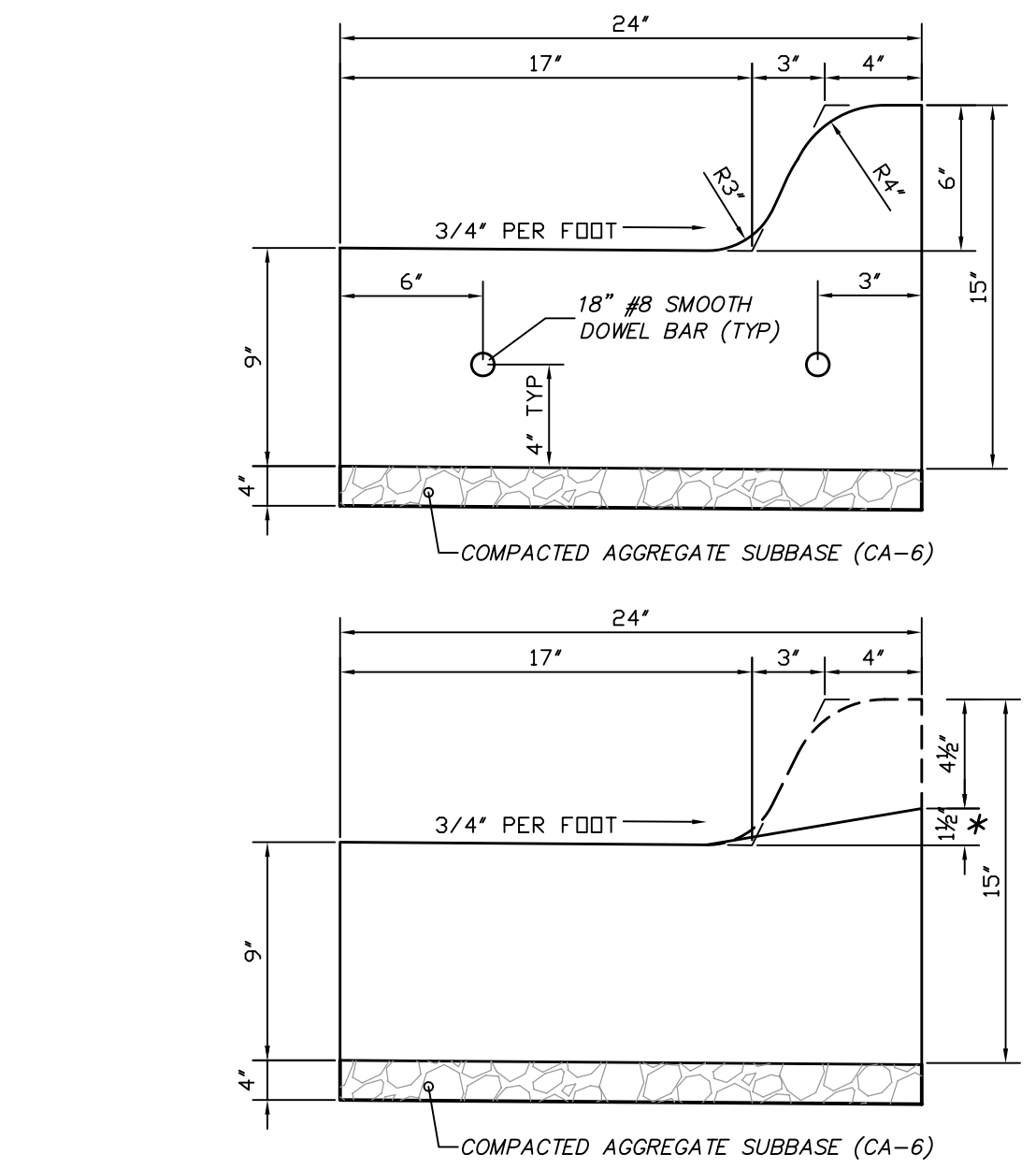
PRECAST REINFORCED CONCRETE SECTIONS SHALL MEET THE REQUIREMENTS OF ASTM C478.

CAST IRON STEPS:
CAST IRON STEPS SHALL BE GRAY IRON CONFORMING TO THE REQUIREMENTS OF ART. 1006.14 OF THE IDOT STANDARD SPECIFICATIONS. STEPS SHALL BE EMBEDDED INTO WALL A MINIMUM OF 76mm (3"). STEPS SHALL NOT BE EXTENDED ON THE OUTSIDE. STEPS SHALL BE OMITTED WHEN THE DEPTH IS LESS THAN 1.83m (6').

STORM MANHOLE

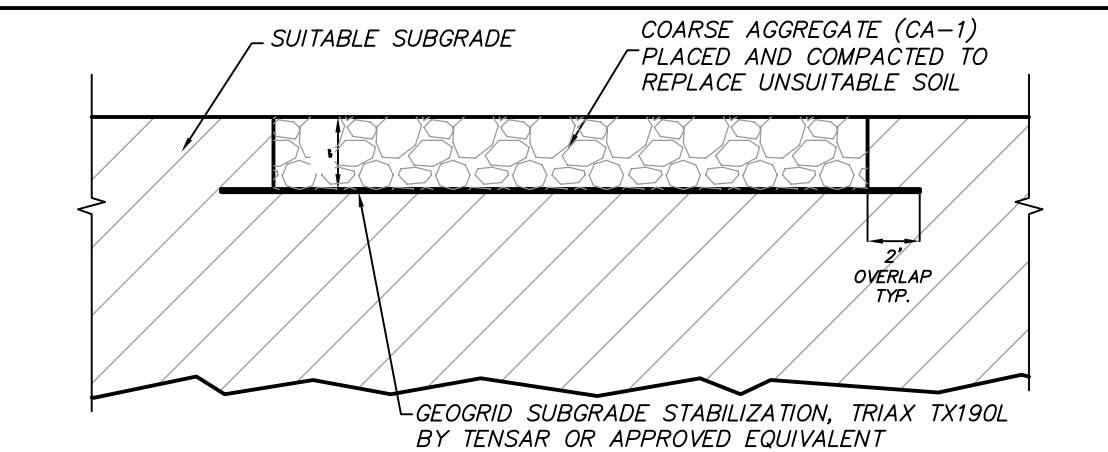


PIPE BEDDING & BACKFILL



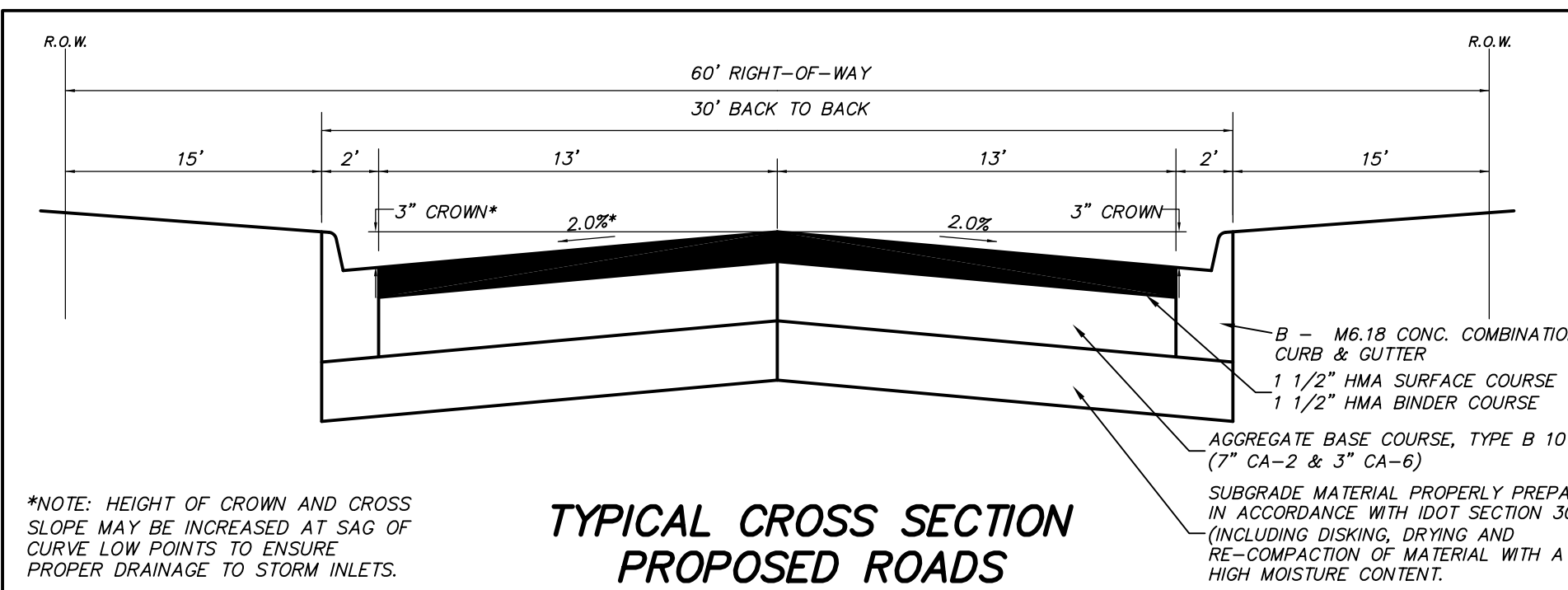
- NOTES:**
1. CONCRETE CURB & GUTTER SHALL BE CONSTRUCTED PER IDOT SECTION 606.
 2. "SPILL OUT" CURB SHALL BE CONSTRUCTED AS STANDARD CURB & GUTTER EXCEPT THE GUTTER SHALL DRAIN INTO THE ADJACENT PAVEMENT RATHER THAN TOWARDS THE CURB HEAD. SPILL OUT CURB SHALL BE INSTALLED WHERE EVER THE ADJACENT PAVEMENT SLOPES AWAY FROM THE GUTTER. EXPANSION JOINTS SHALL BE CONSTRUCTED WITH DOWEL BARS AND 3/4" PREFORMED EXPANSION JOINT CUT TO THE SHAPE OF THE CURB & GUTTER. EXPANSION JOINTS SHALL BE SET AT THE ENDS OF ALL CURVES AND AT 50' (MAX) INTERVALS.
 3. TRANSVERSE CONTROL JOINTS SHALL CONSIST OF A 1/8" SAW CUT AT A DEPTH OF 1". TRANSVERSE JOINTS SHALL BE SET AT 15' (MAX) INTERVALS.

COMBINATION CONCRETE CURB & GUTTER, TYPE M6.18



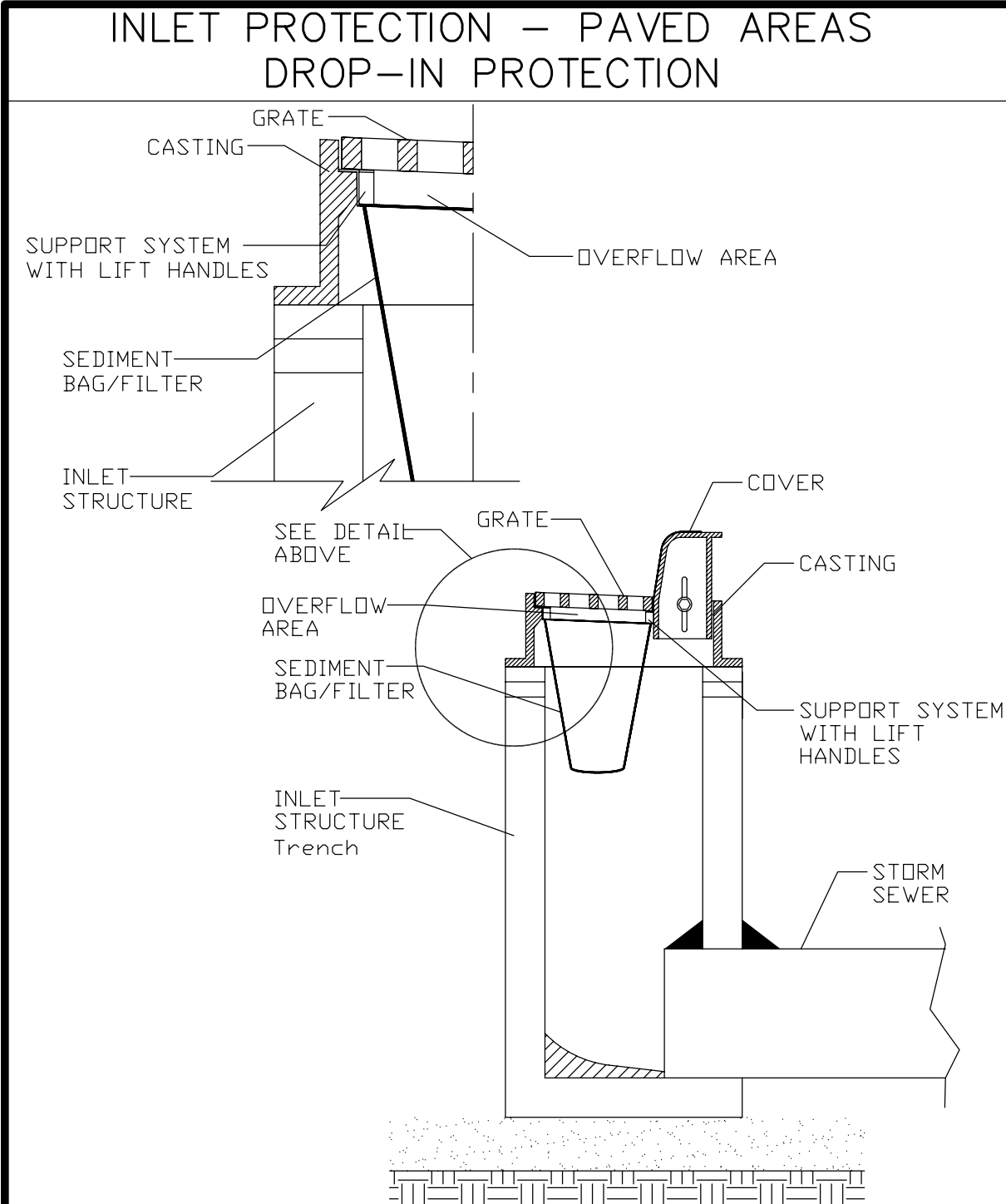
- *DEPTH OF AGGREGATE MAY BE ADJUSTED BASED ON THE RECOMMENDATION OF THE GEOTECHNICAL OR VILLAGE ENGINEER, BUT SHALL NOT BE LESS THAN 6".**
- NOTES:**
1. ALL EXCAVATED UNSUITABLE SUBGRADE SHALL BE USED AS FILL IN AN UNPAVED AREA OR PROPERLY DISPOSED OFFSITE.
 2. UNLESS OTHERWISE APPROVED BY THE ENGINEER, ALL SUBGRADE SHALL BE DISKED AND DRIED PER IDOT SECTION 301 BEFORE IT IS CONSIDERED UNSUITABLE.

SUBGRADE UNDER CUT DETAIL



TYPICAL CROSS SECTION PROPOSED ROADS

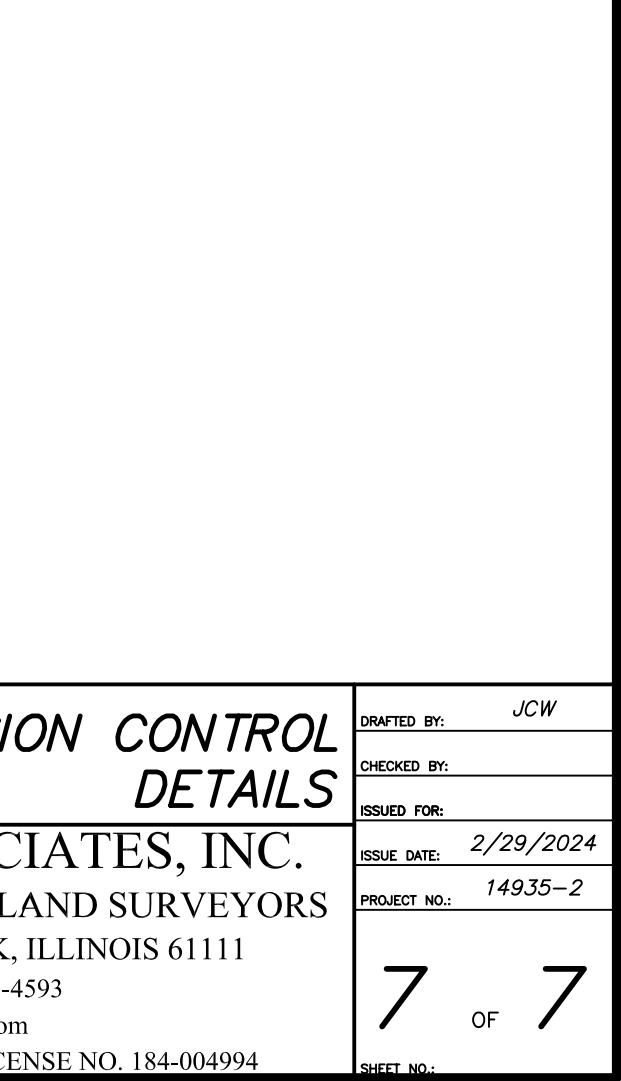
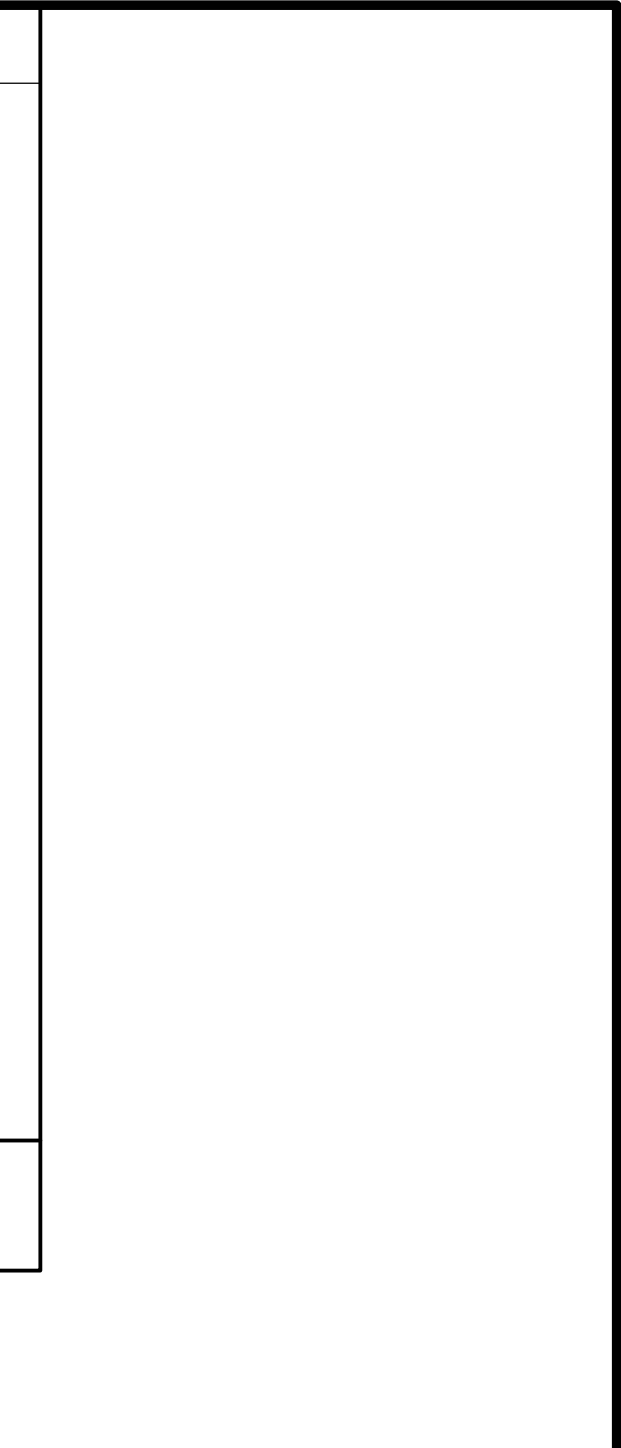
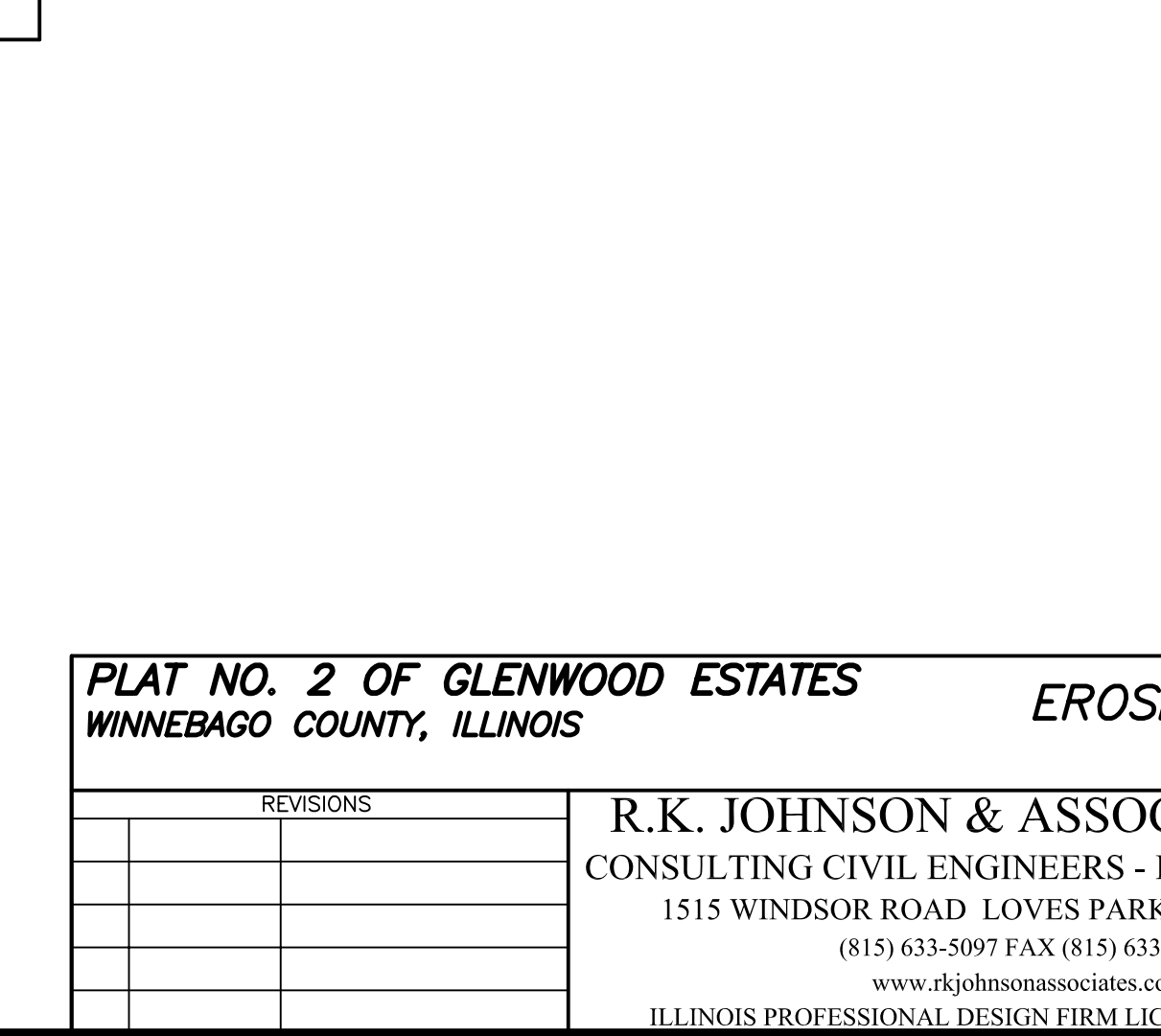
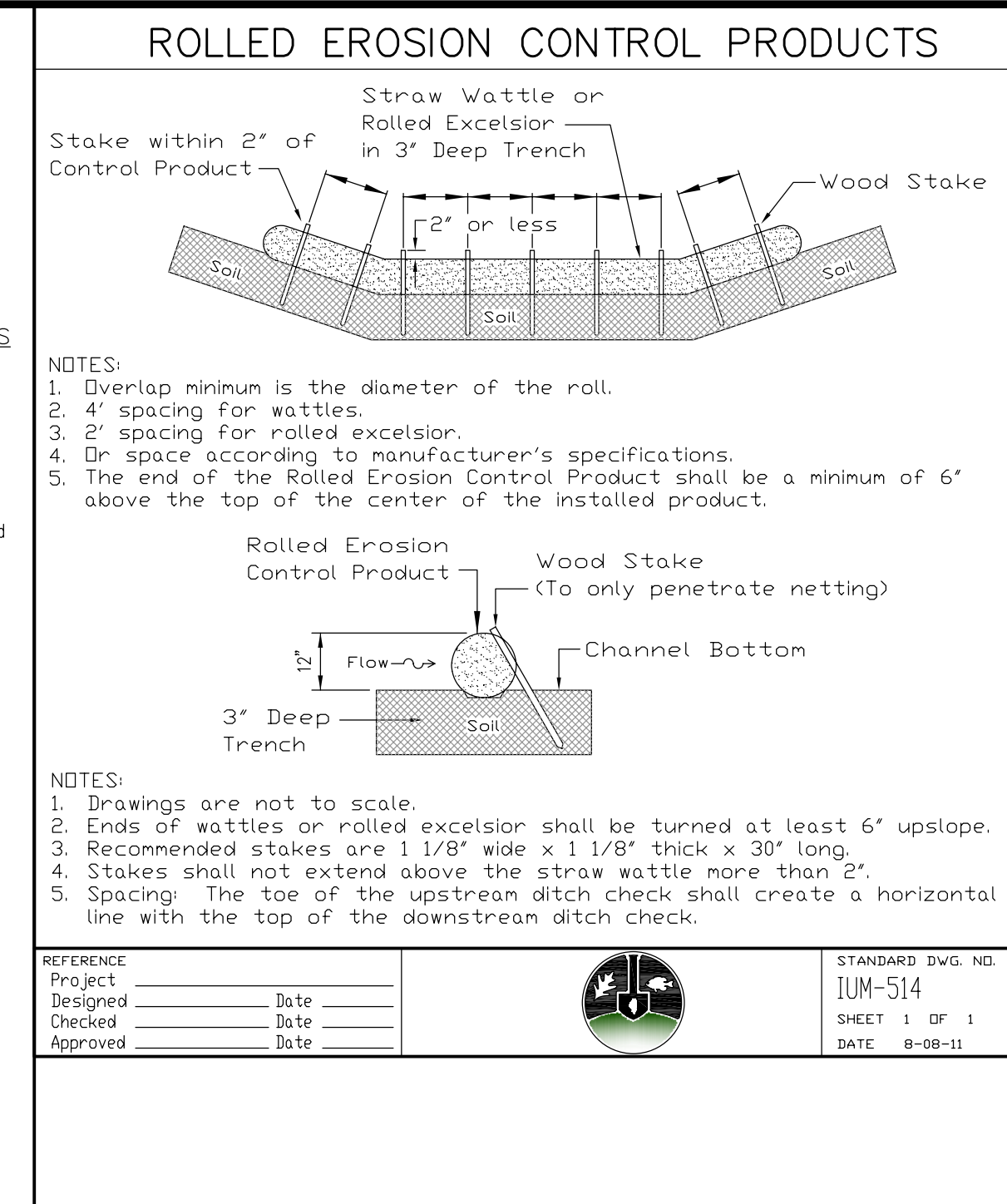
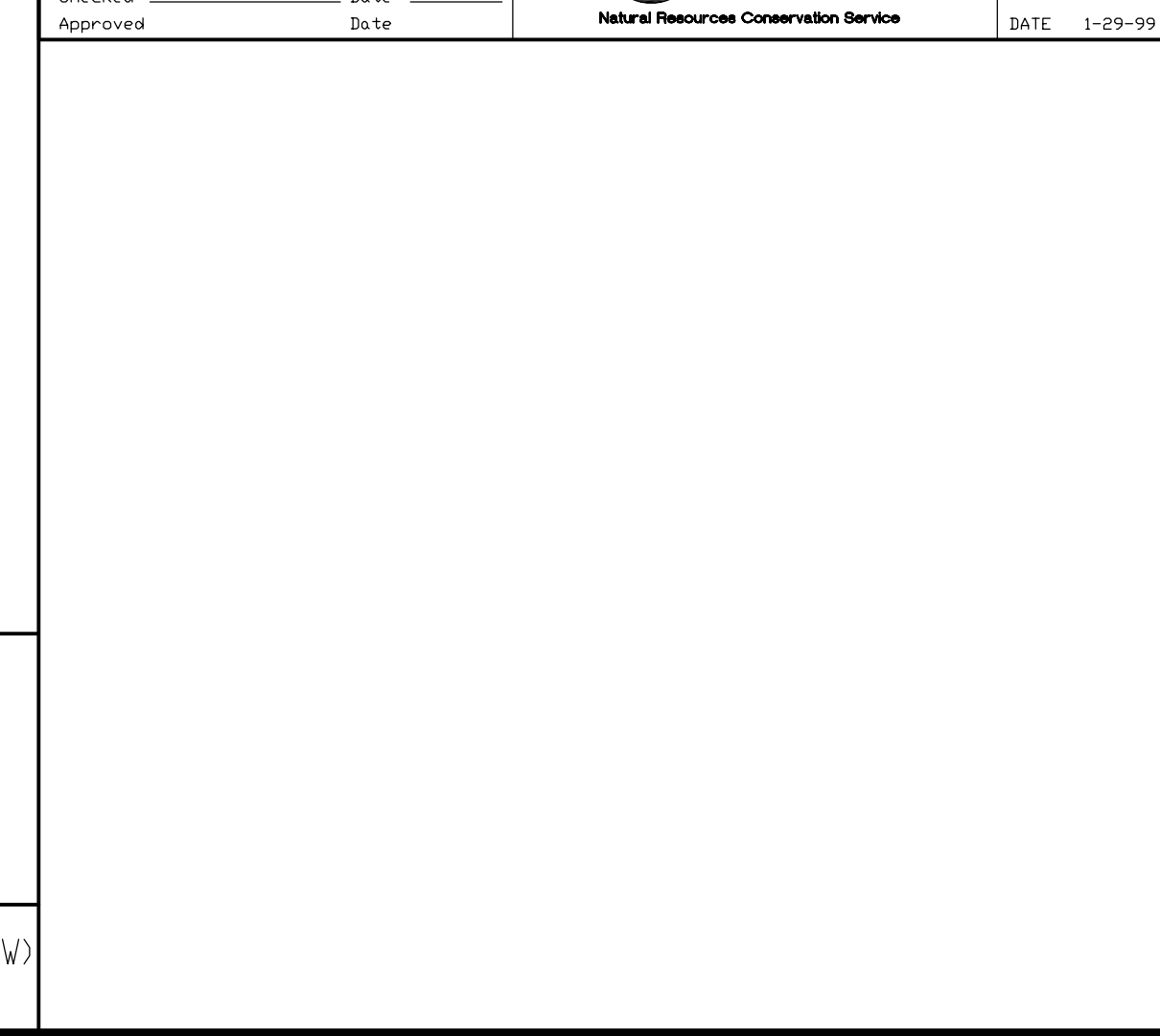
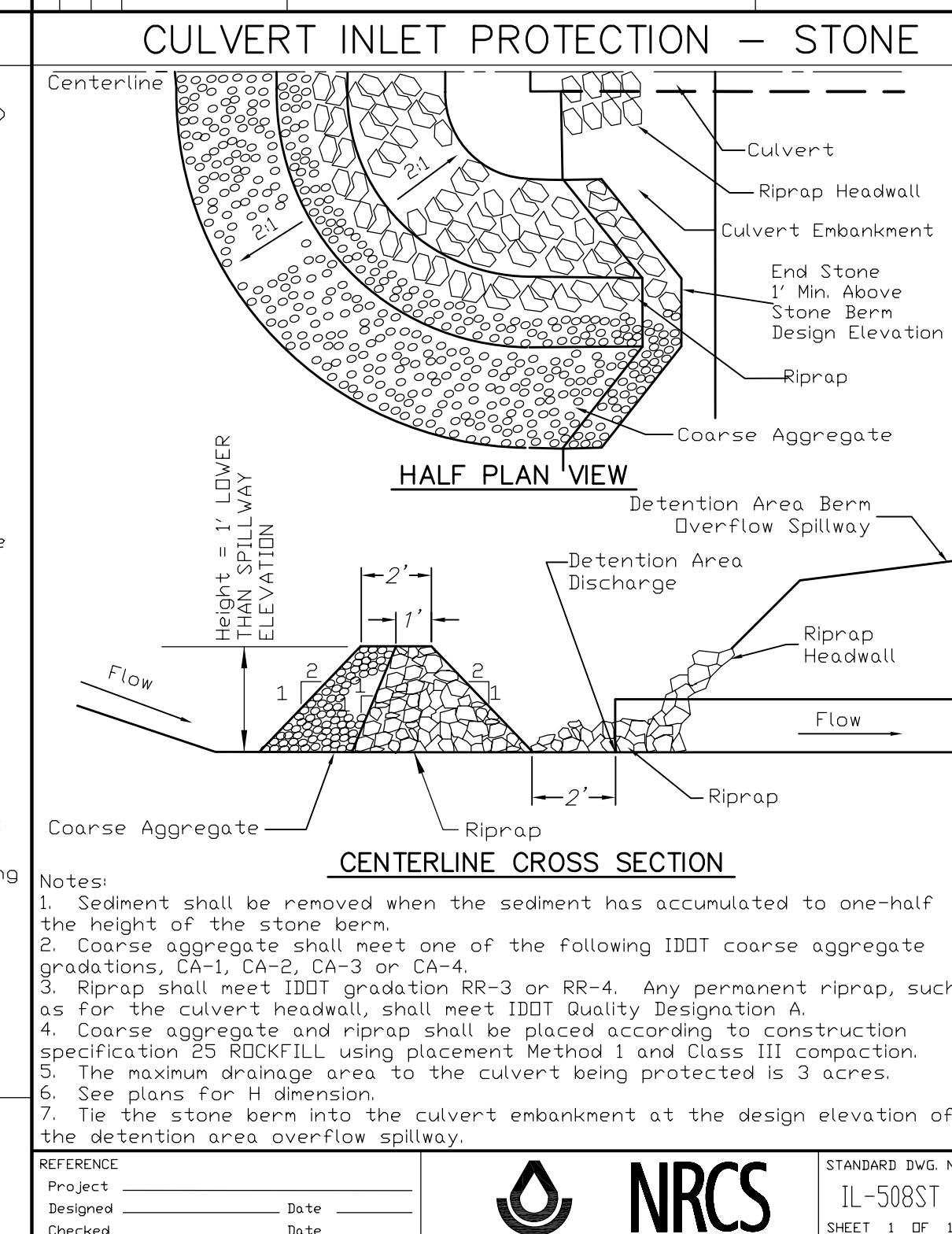
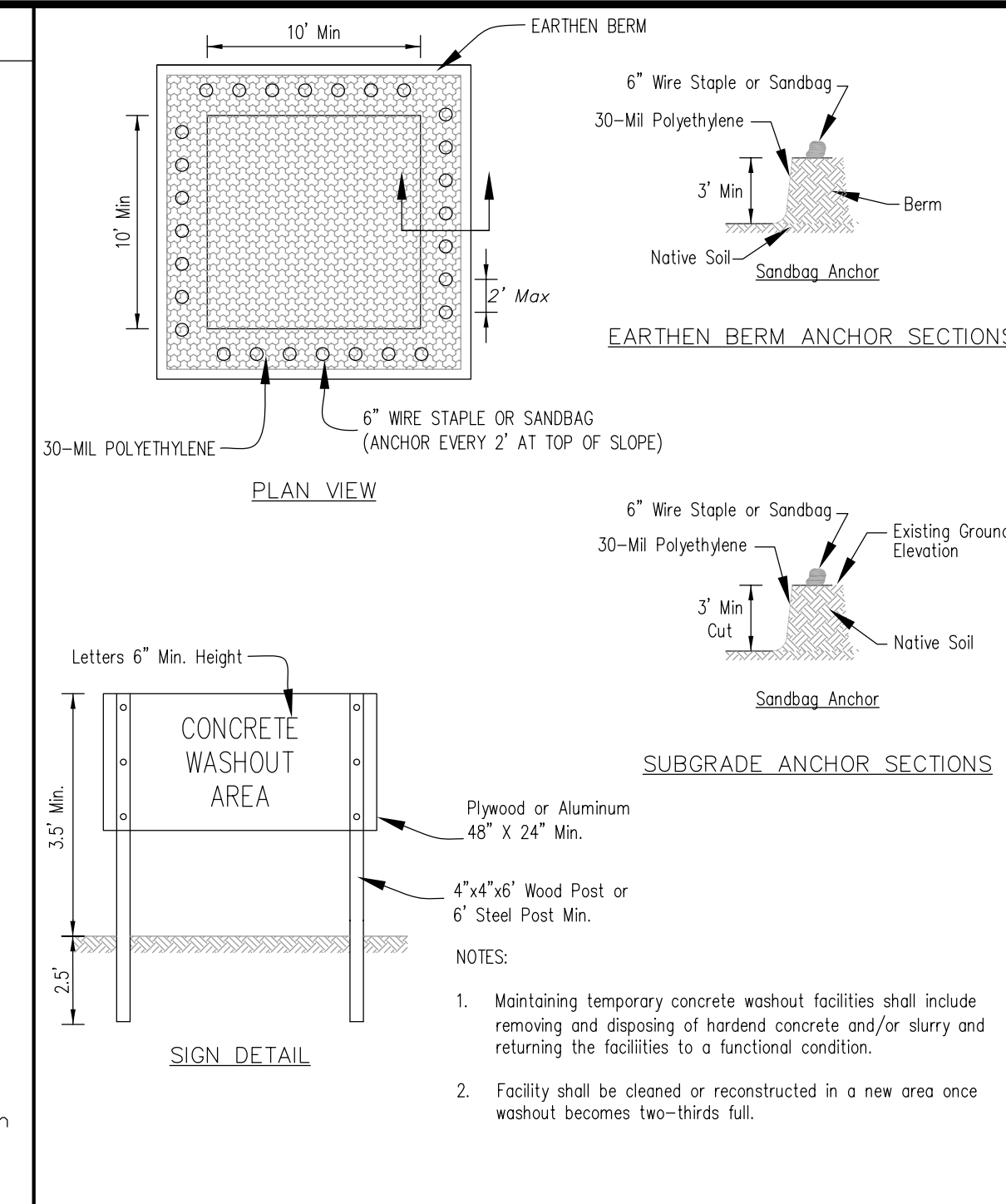
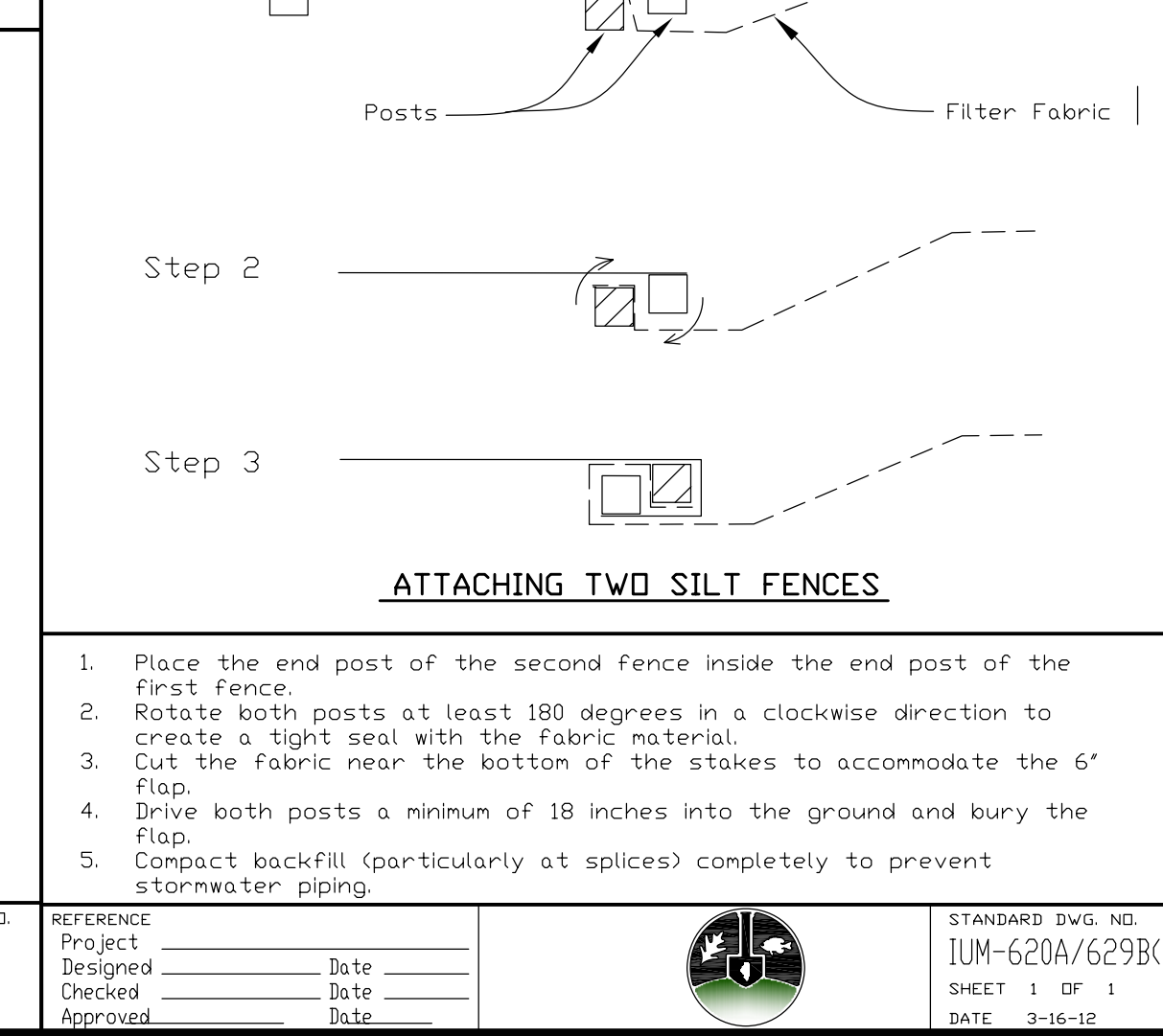
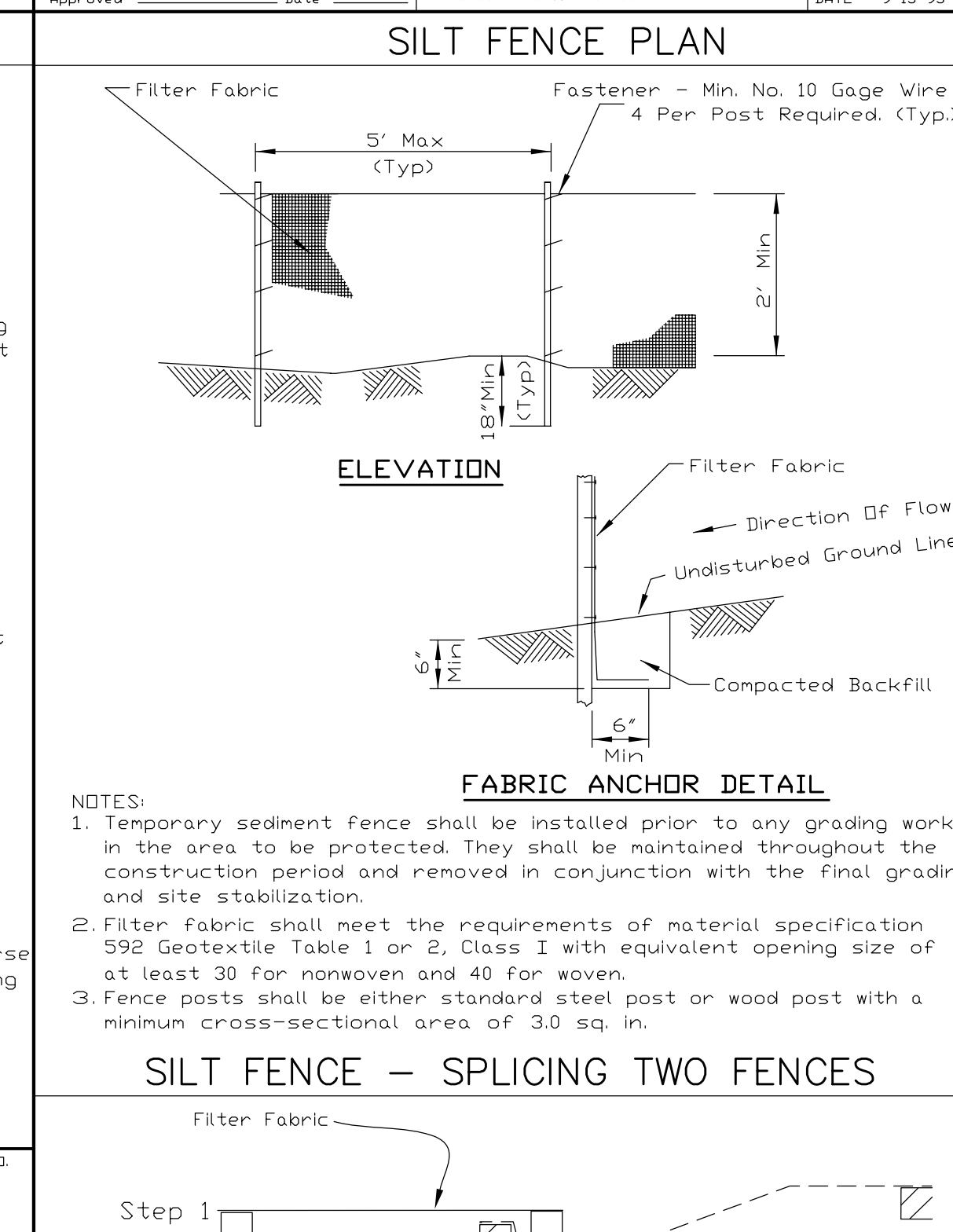
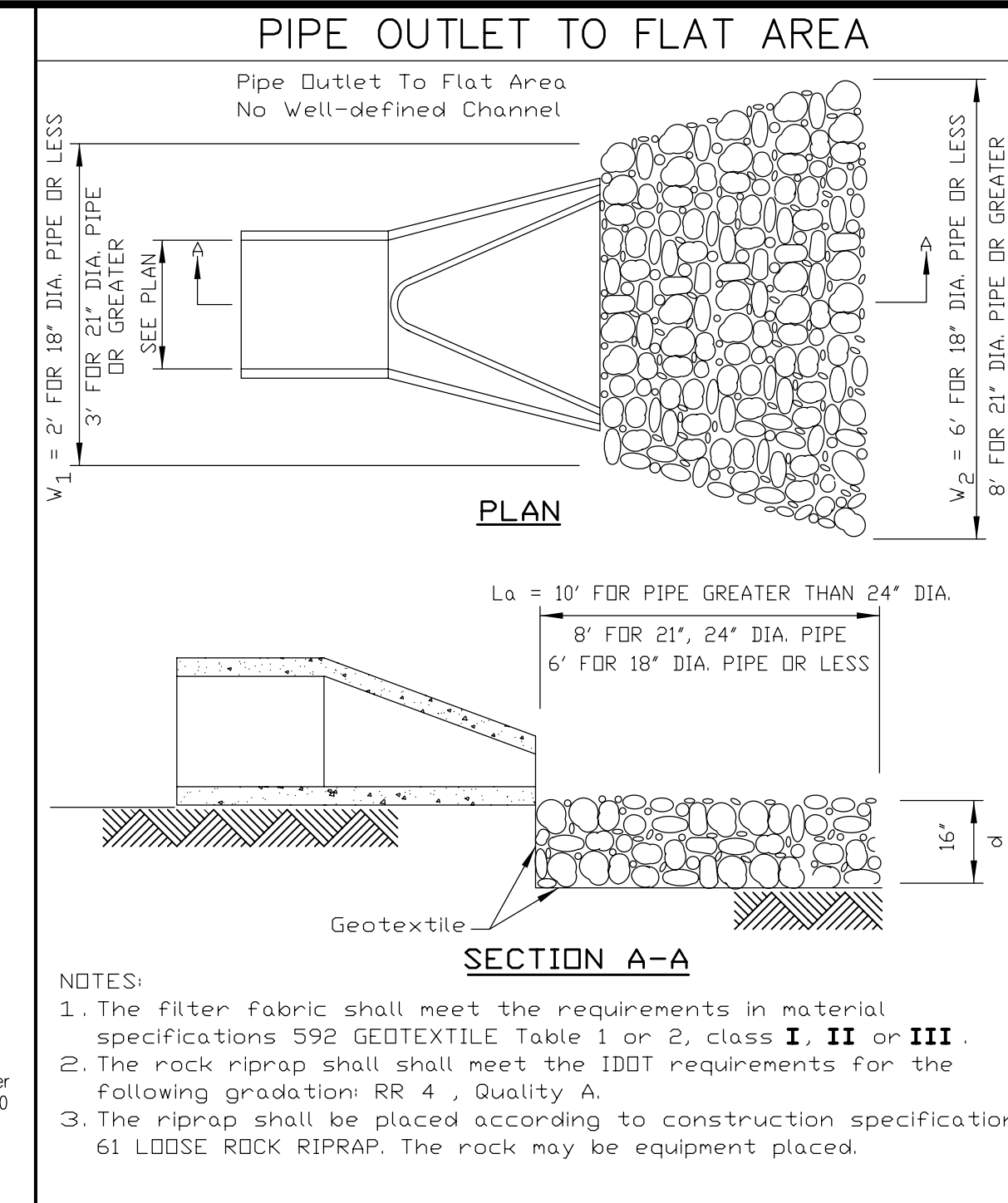
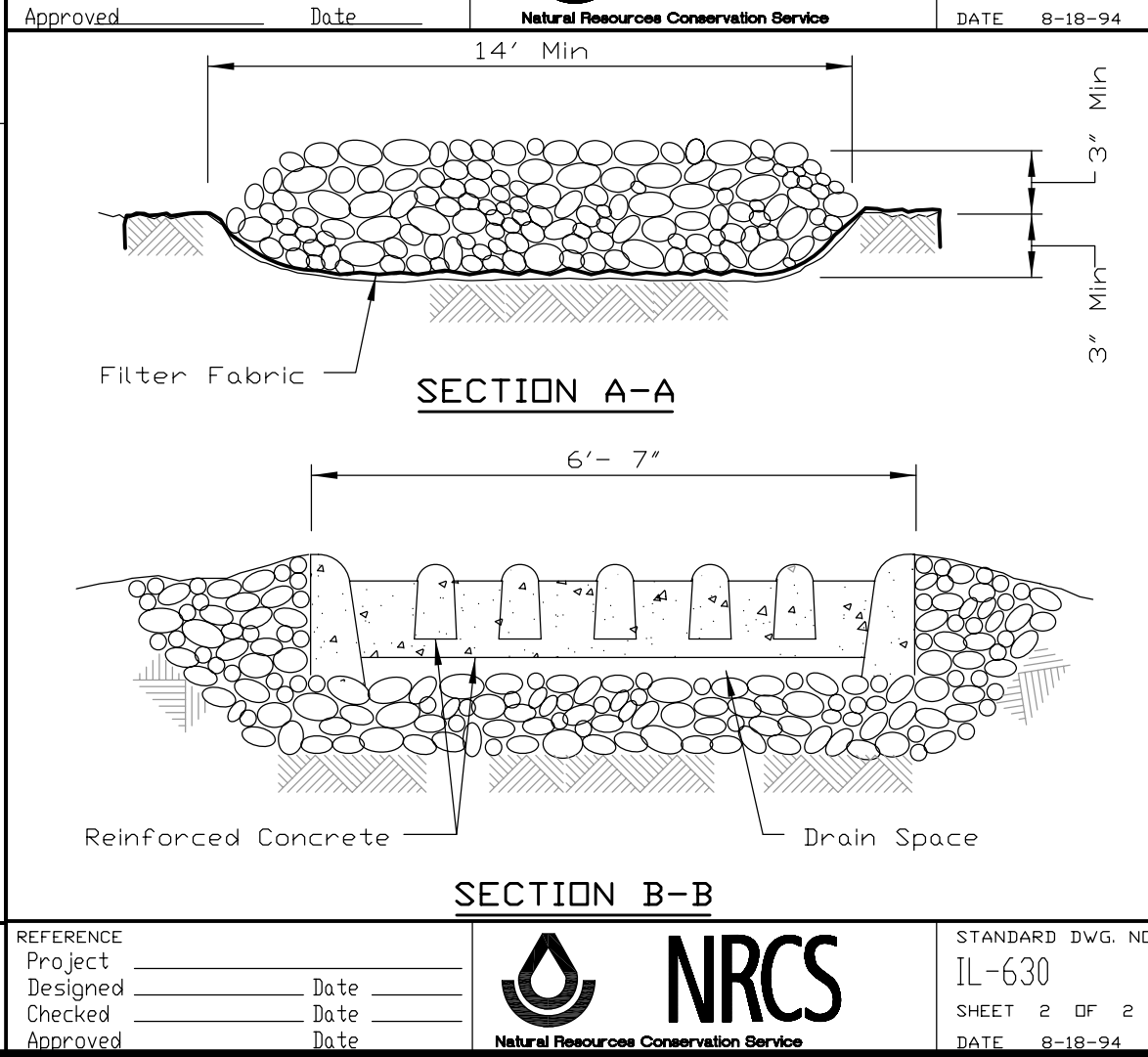
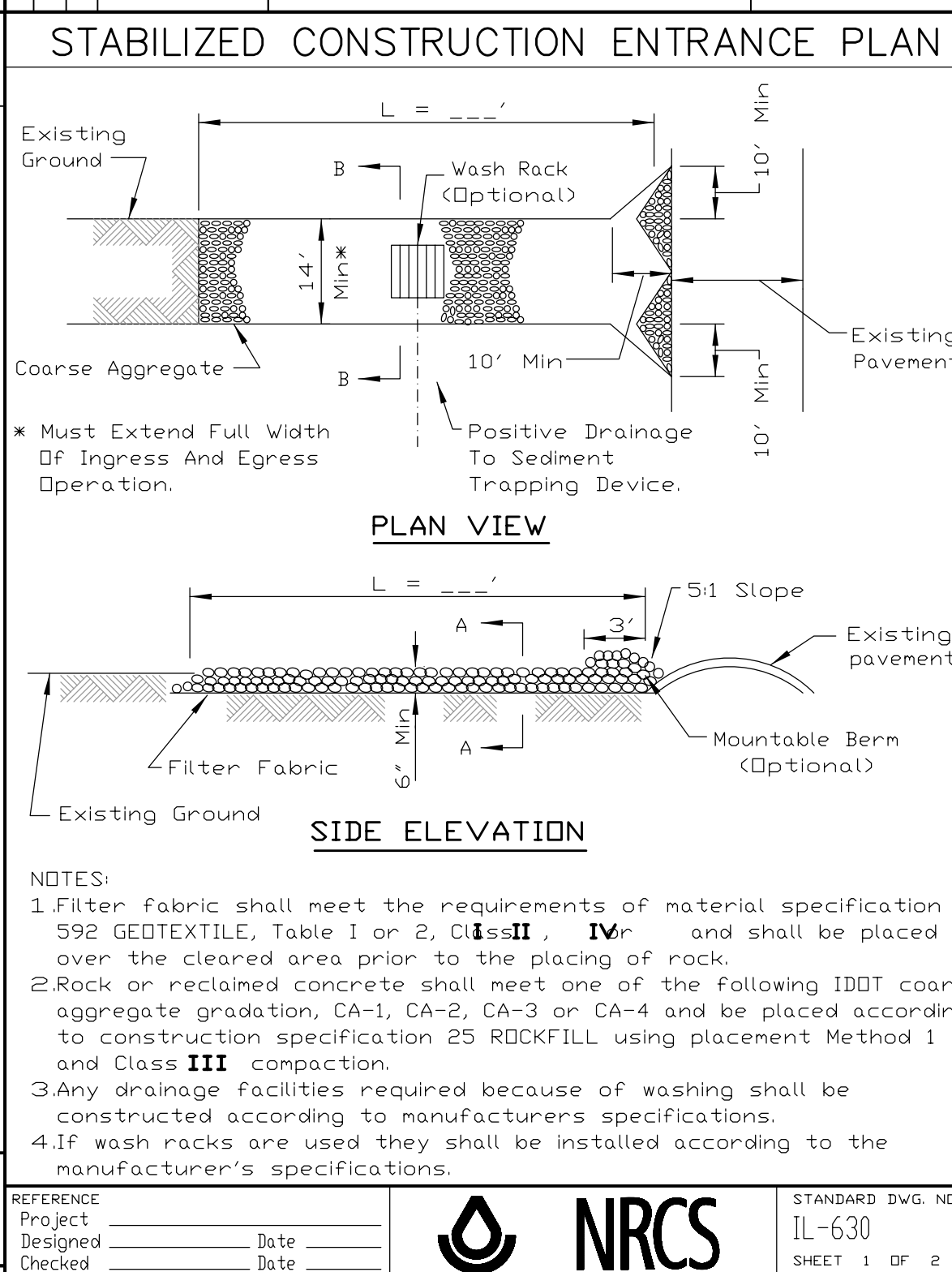
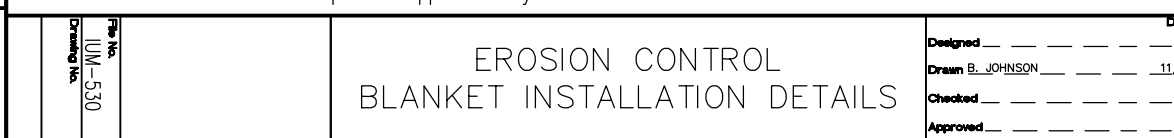
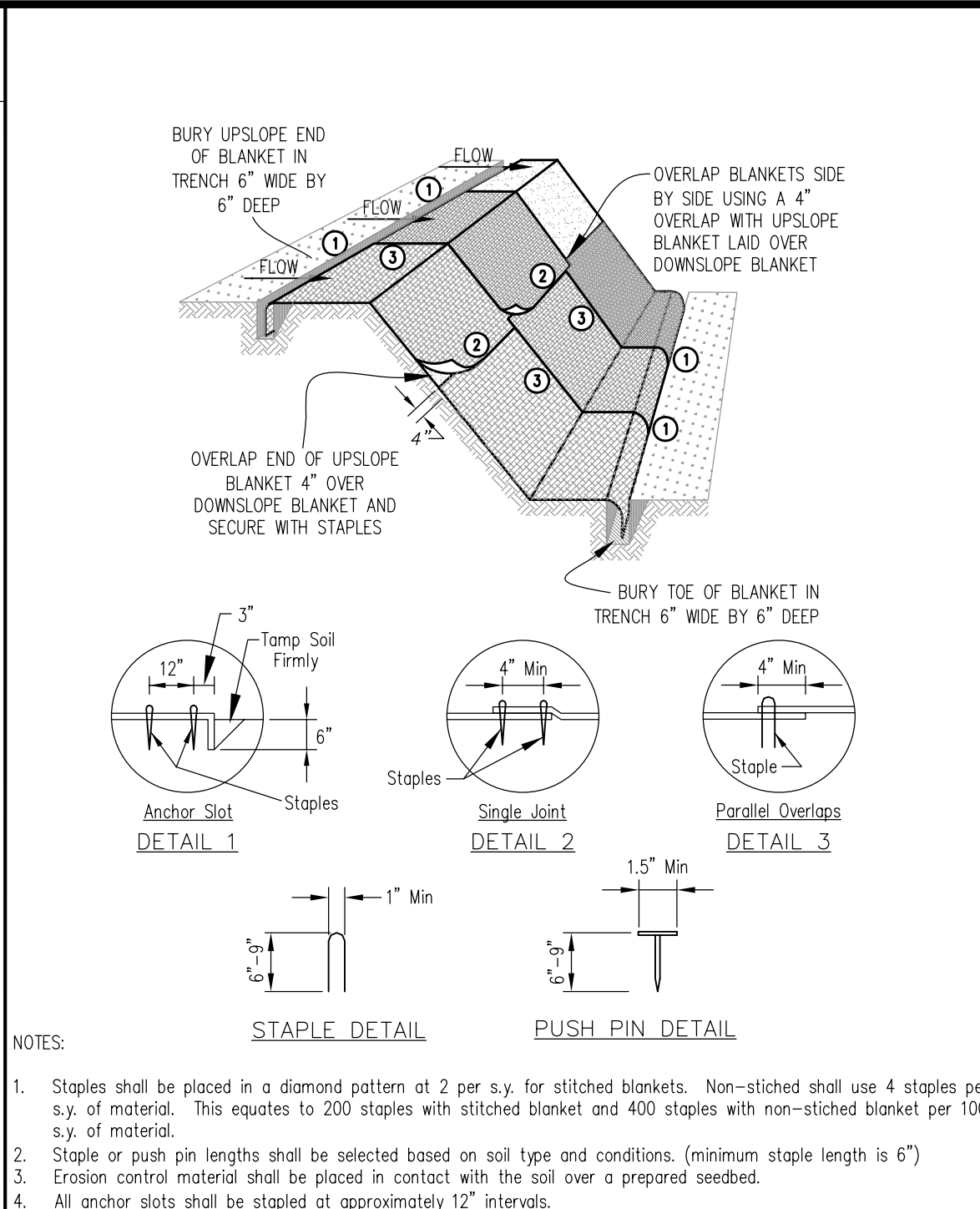
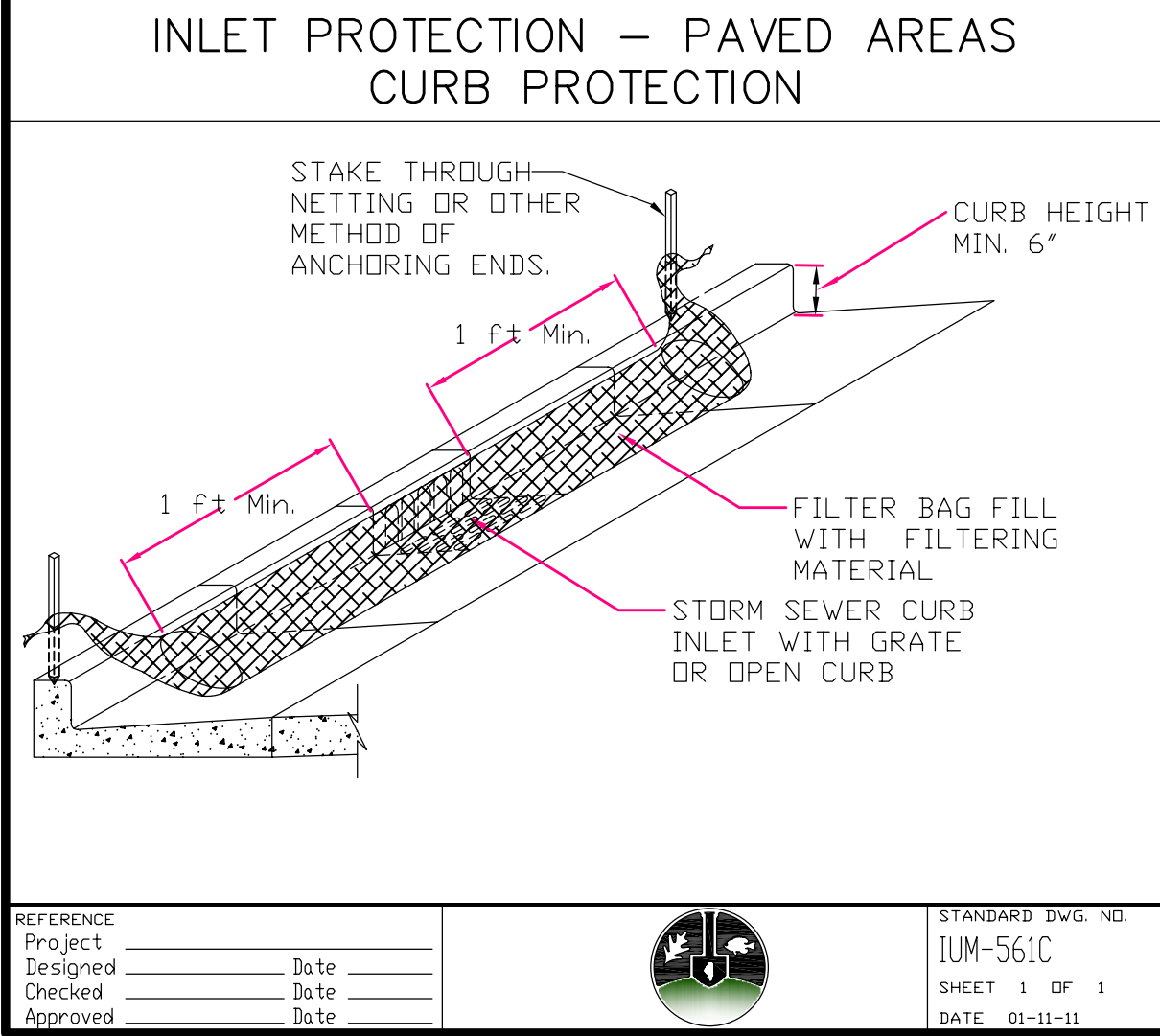
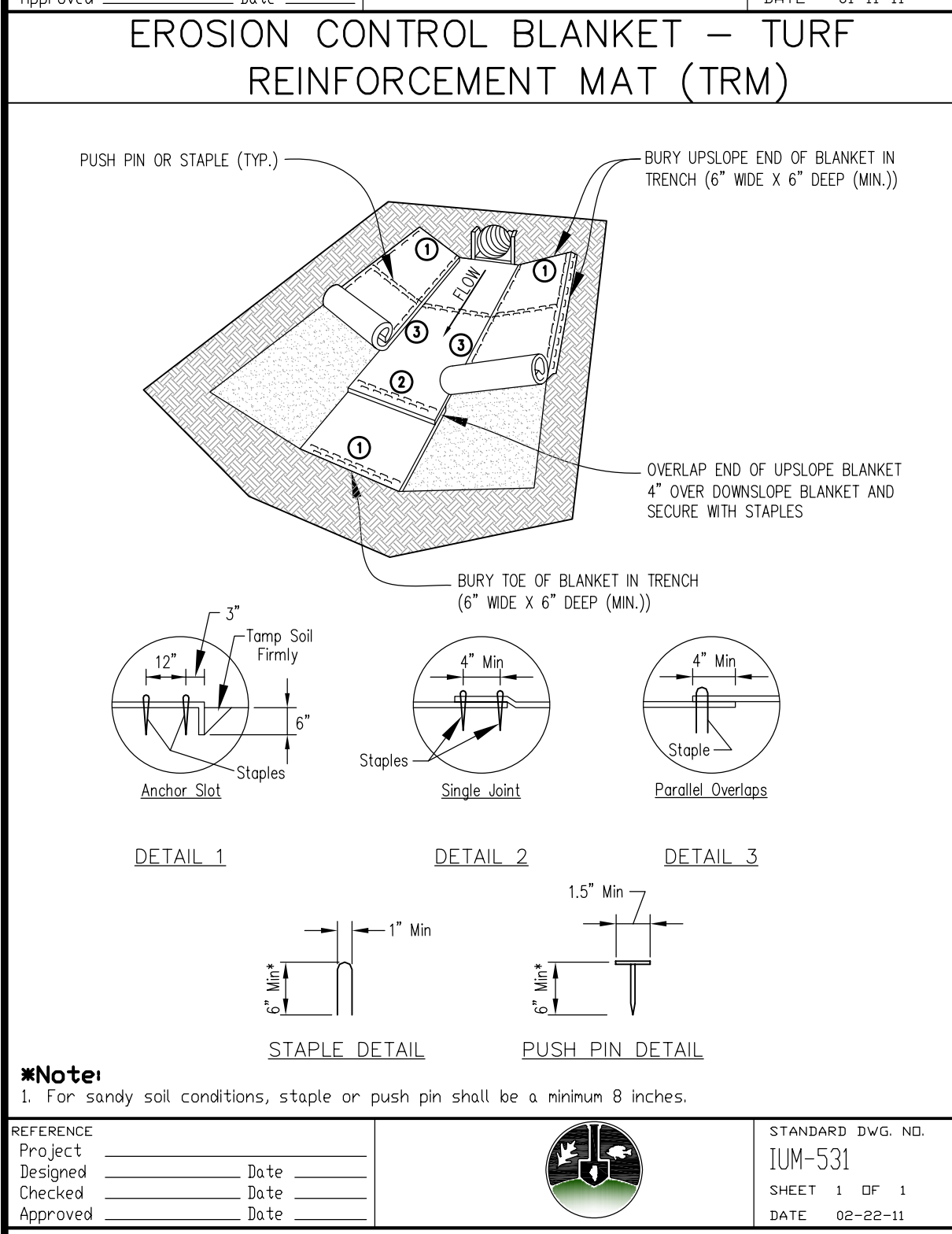
PLAT NO. 2 OF GLENWOOD ESTATES			DETAILS	
WINNEBAGO COUNTY, ILLINOIS				
REVISIONS 1 7/16/2024 PER VILLAGE COMMENTS			R.K. JOHNSON & ASSOCIATES, INC. CONSULTING CIVIL ENGINEERS - LAND SURVEYORS 1515 WINDSOR ROAD LOVES PARK, ILLINOIS 61111 (815) 633-5097 FAX (815) 633-4593 www.rkjohinsonassociates.com ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004994	
			CREATED BY: JCW CHECKED BY: ISSUED FOR: ISSUE DATE: 2/29/2024 PROJECT NO.: 14935-2 SHEET NO.: 6 OF 7	



REFERENCE	Project	Design	Check	Approved
	Date	Date	Date	Date



STANDARD DWG. NO.
IUM-561D
SHEET 1 OF 1
DATE 01-11-11



PLAT NO. 2 OF GLENWOOD ESTATES WINNEBAGO COUNTY, ILLINOIS		EROSION CONTROL DETAILS		CREATED BY: JCW
				CHECKED BY:
				ISSUED FOR:
				ISSUE DATE: 2/29/2024
				PROJECT NO.: 14935-2
				7 OF 7
				SHEET NO.

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