



Reservoir Trailhead/ Glen Park
Parking Kiosk Compliance
Enforcement

Draft Proposal

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Background and Justification

In early 2022, the town of Palmer Lake installed a parking kiosk at the entrance to the Reservoir Trailhead. The use of the parking areas down Old Carriage, and parts of Lover's Lane, require the user to pay \$5.30 for a day pass for parking. In this process a person uses a self-pay kiosk, then takes the printed ticket back to their vehicle where they are required to display it on their dashboard verifying their purchase. These purchases directly support the funding of the Parks Commission in town.

Currently there is an average daily usage of 34 paid tickets per day, or approximately \$1,234.90. Starting April 1st, 2022, the PLPD began to gather information regarding the unpaid and or incorrect usage of the parking areas. During these checks, which occurred at various times throughout each day of the week, an officer would make a single round through the areas mentioned above and record data. It was discovered there were on average 47 occurrences per week, or approximately \$249.10 in revenue, where the driver did not pay for their vehicle parking on the kiosk. There was also an average of 8 occurrences per week where the user incorrectly entered their vehicle information in the kiosk.

From 04/10/2022 – 05/09/2022, the parking kiosk recorded 1,019 tickets purchased, or approximately \$5,400.70 in revenue.

PLPD monitoring of the kiosk and parking area showed the kiosk was improperly used, or not used at all 163 times, approximately 16% of the time, accounting for a loss of \$863.90 in parking revenue. Please see Figure 1 below.

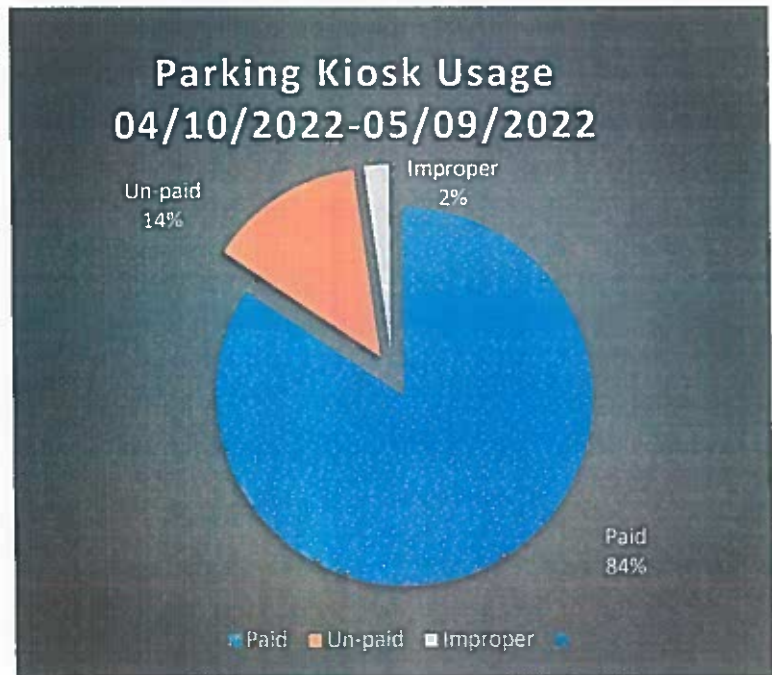


Figure 1

Speculation: During the final three days of the review period there was also a 16% increase in ticket sales from the week before. This may be due in large part to the improving weather and warming temperature. It is estimated the town's population increases to nearly 6,000 during the busy summer season, approximately a 100% increase. With this in mind it is fair to assume the usage of the trails and parking areas will also double.

Speculation: Assuming this increase in population increases the usage of the paid parking areas, it can be extrapolated that so to will the number of non-paying users of the area. It is estimated that up to \$1,727.80 in revenue could be lost per 30 day period. Please see Figure 2 below.

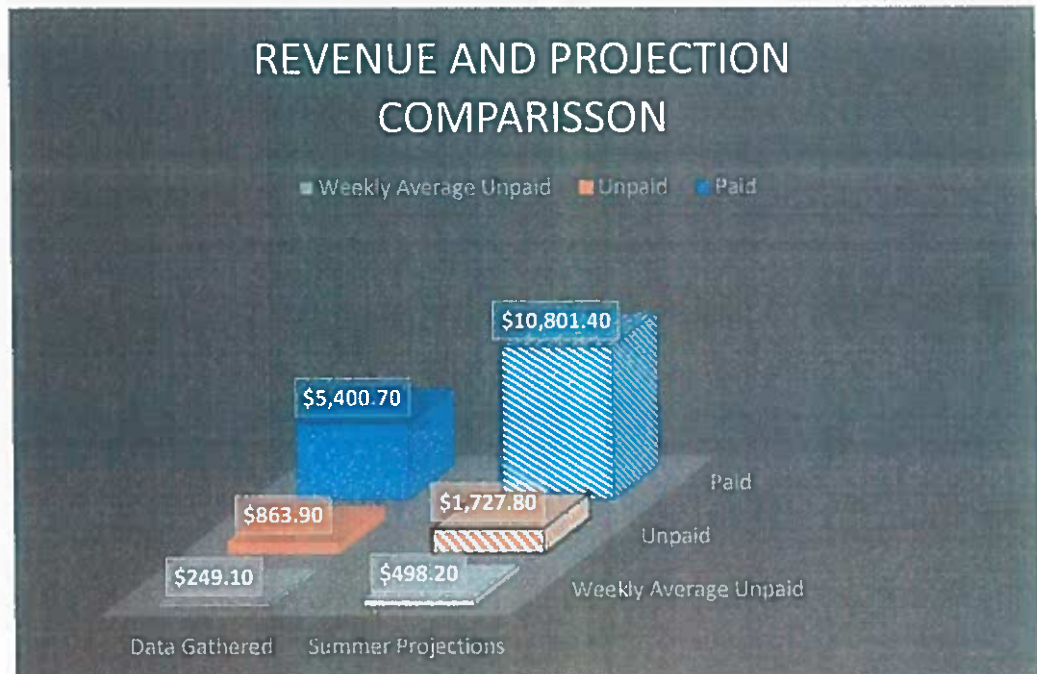


Figure 2

The PLPD is being requested to run enforcement operations through the paid parking areas. During the data gathering from PLPD officers the highest single accumulation of parking violations resulted in 17 vehicles failing to pay, and 6 improperly ticketed vehicles.

The typical officer can initiate and issue a municipal summons for a violation in approximately 7 minutes, assuming there is no driver information needed. With this in mind, and understanding multiple vehicles will result in less time required for the transmission of data, if the officer were to write all 17 vehicles a summons for failing to pay for their parking, a \$50.00+\$19.00 municipal violation, and warnings to the improperly recorded vehicles, it would take approximately 116 minutes to complete, removing that officer from other law enforcement operations for that time.

Analysis: Source data gathered at various times on various days indicates approximately 16% of the time, the kiosk is not being used properly or at all representing a loss in funding and revenue for the Parks Commission. In an effort to be able to enforce the new parking regulations, while

not removing officers from their other town wide responsibilities, it is reasonable and prudent that the Palmer Lake Police Department under the direction of Town Management take action beyond standard patrol operations through the means of the S.T.E.P., or Selected Traffic Enforcement Program to reduce the number of non-complying drivers by selected and specialized parking enforcement targeting the Reservoir Trailhead, Glen Park and Tennis Court parking areas.

Scope and Implementation

The Selected Traffic Enforcement Program consists of placing additional officers on detail assignment for the exclusive purpose of enforcing provisions of the Model Traffic Code of Colorado, adopted under authority of the Palmer Lake Board of Trustees and within the jurisdiction of the town. Officers assigned will be limited in shift numbers and duration and expected to produce enforcement results at acceptable levels, as defined by the chief of police in coordination with the town administrator. Revenue generated from written citations issued under STEP will financially and independently support program costs, with revenue in excess of costs being shared by the town's general fund and a new police traffic safety budget (see revenue/expenditure projection section for complete details).

- A. Coordination: The chief of police will designate a program coordinator, which coordinator will be responsible for assigning STEP shifts, tracking performance, and reporting program details periodically through the chain of command.
- B. Assignments: STEP shifts for parking enforcement will be 4 hours in length and limited to two shift per week, Saturdays and Sundays from 1100 hours to 1500 hours. This limitation is necessary to avoid negative public perception and avoid the discouragement/disruption of town visitor commerce and recreation. Officers may work the 4 hours on a day off, or prior to, or after their assigned patrol shift.
- C. Patrol Coverage: Officers engaged in a STEP parking enforcement shift will not be utilized to take general calls for service. This will allow a clear separation of duties and allow the STEP officer to focus on the parking enforcement specific to this program.
- D. Compensation: STEP Officers will be paid at an approximate time and a half their base hourly wage. This increased rate will provide monetary incentive necessary for the program to be worked on a regular and consistent basis, ensuring program effectiveness in its goal to lower traffic accidents as well as revenue generated. This time will be tracked by the "STEP" tab on the payroll timesheet and subject to approval by the officer's supervisor.

- E. Performance: Officers will be selected and scheduled on a volunteer basis by the program coordinator. Officers assigned to work STEP parking enforcement will be responsible for maintaining a high-performance and production standard during their shift. Officers will be discouraged from issuing warnings in place of citations while on STEP parking enforcement assignment unless extenuating circumstances can be articulated.
- F. Focused Enforcement: Officers will concentrate their enforcement efforts on violations in the three previously mentioned paid parking areas. They will be assigned the use of the PLPD ATV and be required to complete at least two full passes through the parking areas. Between passes, the officer will make a patrol of the reservoir trail area to ensure compliance with Municipal Code referencing the reservoir, and if necessary issue citations for violations therein.

Revenue/Expenditure Projection

The following program revenue projection is based on estimates of likely officer citation production numbers and the average penalty assessment fine of citations issued for parking violations. It is accounting for enforcement each weekend during peak usage times, June through September. The number of citations likely produced is based on previously referenced data gathering averaging 12 citations on Saturdays, and 7 on Sundays. Expenditures anticipated include an estimate of officer salary and an increase in ATV fuel expense. Please see Figure 3 below.

Total Annual Revenue Generated: \$22,287.00 – calculated by multiplying an estimated 19 citations issued per series of 2 4-hour STEP shifts each weekend over a 17-week peak enforcement period, at total fine amount of \$69, which is the fine for *MTC 1401 - Parking Where Signs Prohibited*.

Total Officer Salary: \$3,965.76 – for purposes of this draft estimate, the hourly rate would be \$29.16/hour (\$19.44 x 1.5). This rate is multiplied by 8 hours per week (limitation) by 17 weeks per year.

Total Estimated Fuel Expense: \$191.25 – calculated 45 total extra gallons of fuel required per year. .33 gallons per hour expended, mostly in idle, multiplied by the total annual STEP parking enforcement hours worked (136) of 85 octane gasoline priced at \$4.25 per gallon, the estimate for average summer gasoline prices per Gasbuddy. Average patrol miles: 7.8 per shift.

Total Miscellaneous Expenses: \$100.00 – calculated at approximately \$2.94 per shift for the cost of mobile printer unit paper, ink, and other incidental expenses for 34 total 4-hour

shifts over the 17-week peak enforcement period.

Monies available after expenses: \$18,029.99. The remaining amount after program costs would be evenly split (**\$9,014.99**) between the town general fund and a new police traffic safety item, to be utilized by the chief of police in coordination and under direction of the town administrator/clerk to fill departmental traffic safety needs of PLPD.

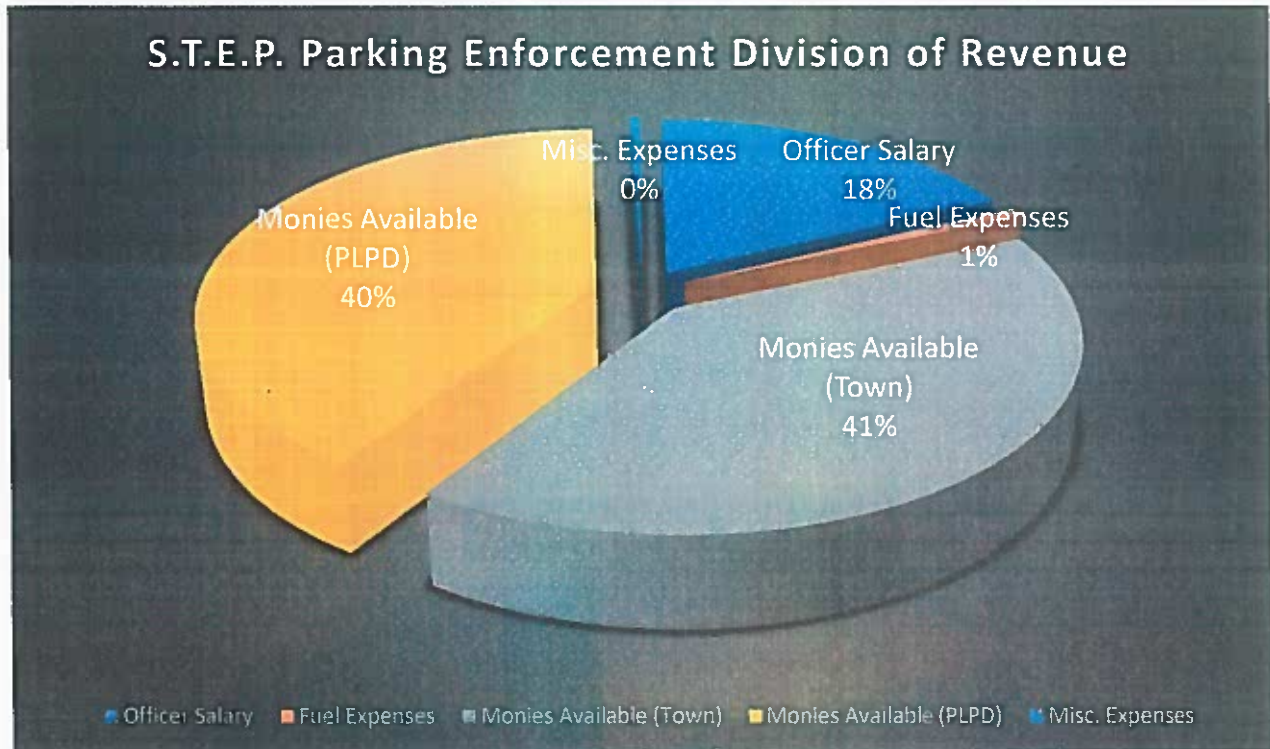


Figure 3