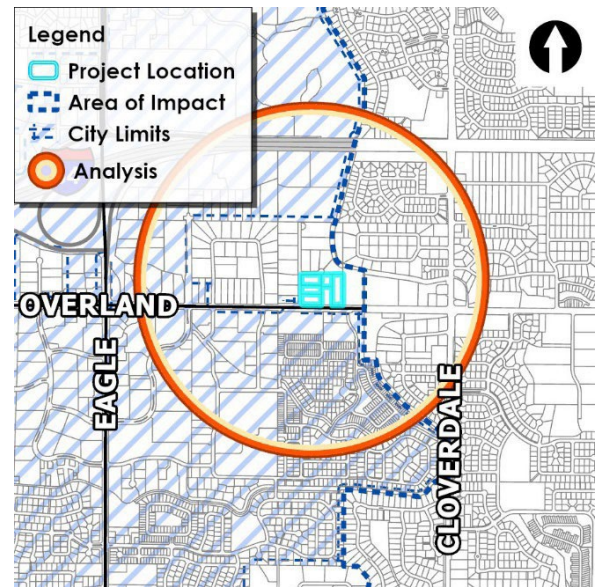


COMMUNITY DEVELOPMENT
DEPARTMENT REPORT



HEARING DATE: 10/3/2024
TO: Planning & Zoning Commission
FROM: Bill Parsons, Current Planning Supervisor 208-884-5533
shersh@meridiancity.org
APPLICANT: Jorre Delgado, JD Planning & Consulting
SUBJECT: H-2023-0070
Rolling Hill - AZ, CUP
LOCATION: 1560 Rolling Hill Drive (Parcel # R7555000032), 1520 Rolling Hill Drive (Parcel #R7555000041), 1480 Rolling Hill Drive (Parcel #R7555000050), 4270 Rolling Hill Drive (Parcel #R7555000015) and Parcel #R7555000025 in the SW 1/4 of the SE 1/4 of Section 16, T.3N., R.1E.



I. PROJECT OVERVIEW

A. Summary

Annexation of 6.90 acres of land from the R-1 zone in Ada County to the C-C (Community Business District district) zone to develop two (2) vertically integrated residential structures featuring 90 dwelling units and 20,134 square feet of commercial space; conditional use permit to develop one (1) multi-family building consisting of 64 dwelling units on 5.89 acres in the proposed C-C zone. **NOTE: The CUP is only required the multi-family portion of this development. Vertically integrated residential projects are listed as principally permitted uses in the C-C zone.**

B. Issues/Waivers

Staff acknowledges that this is a challenging project, given the undeveloped surrounding properties, including the out-parcel at the center of this site and the extension of the future collector roadway to the east. **Commission and Council should rely on all relevant information when determining if this is in the best interest of the City to annex this property.**

C. Recommendation

Staff recommendation: Approval

D. Decision

Select: Pending.

II. COMMUNITY METRICS

Table 1: Land Use

Description	Details	Map Ref.
Existing Land Use(s)	Single-family Residential and Vacant land	-
Proposed Land Use(s)	Multi-family Development & Vertically Integrated Residential	-
Existing/Proposed Zoning	Estate Residential (R1)/Community Business District (C-C)	V.A.2
Future Land Use Designation	Mixed Use-Regional (MU-R)	V.A.3

Table 2: Process Facts

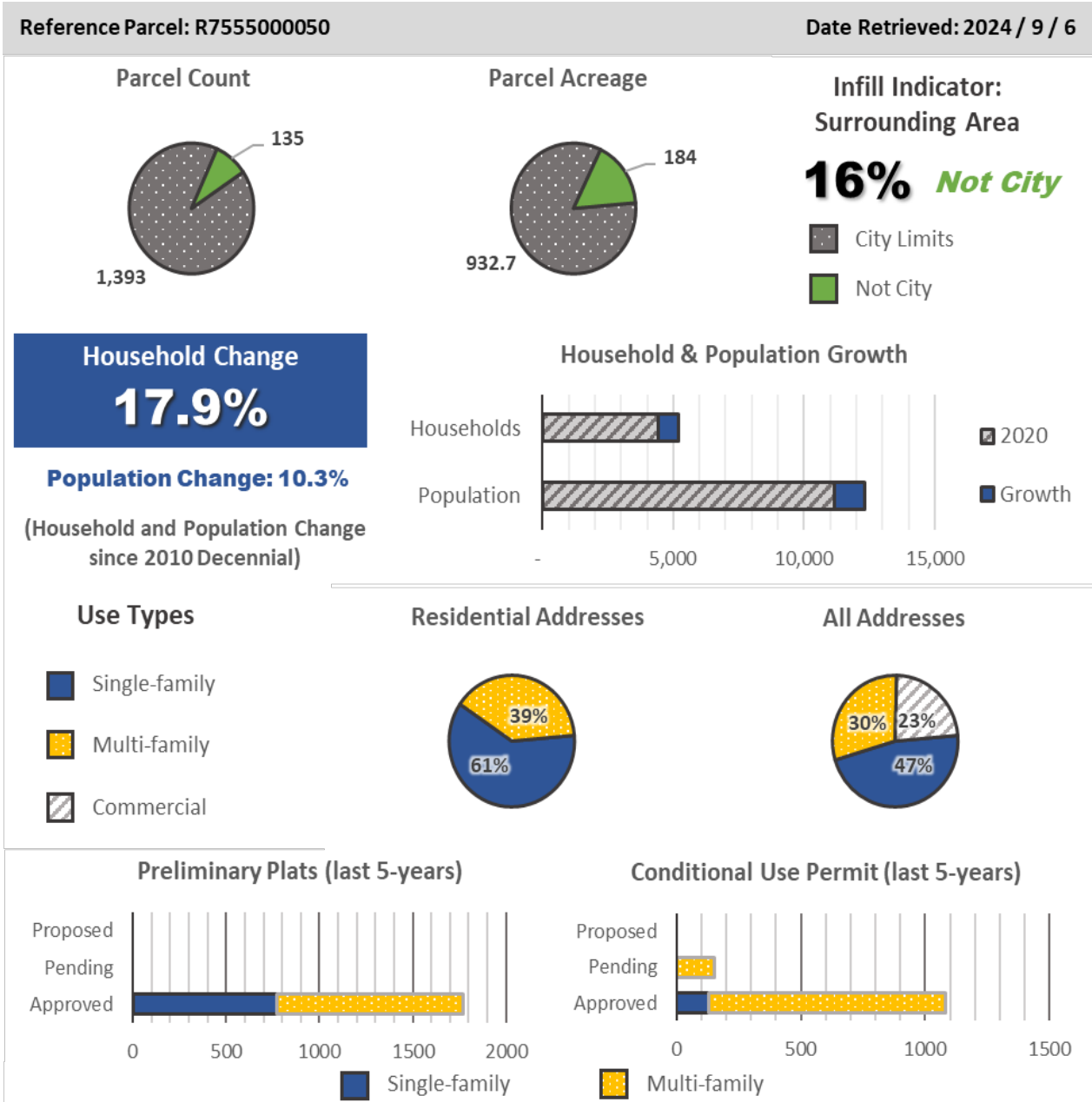
Description	Details
Preapplication Meeting date	Tuesday, November 14, 2023
Neighborhood Meeting	10/25/2023
Site posting date	09/23/2024

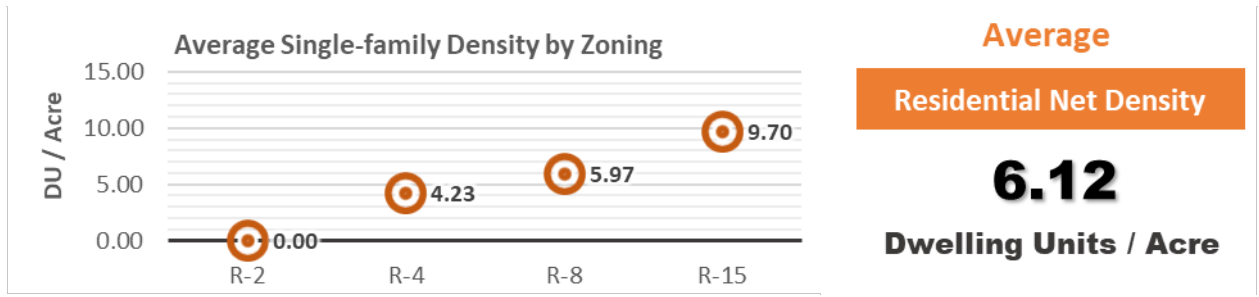
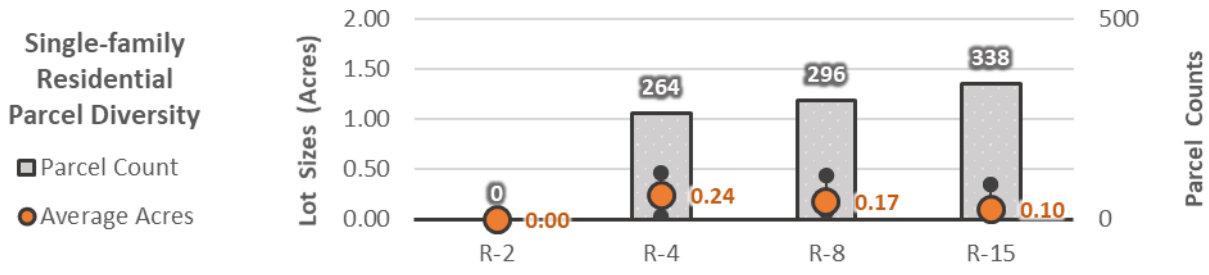
Table 3: Community Metrics

Agency / Element	Description / Issue	Reference
Ada County Highway District		III.K
<ul style="list-style-type: none"> • Comments Received 	Yes, Staff Report	-
<ul style="list-style-type: none"> • Commission Action Required 	No	-
<ul style="list-style-type: none"> • Access 	One access is proposed via S. Rolling Hill Drive/Local (existing) via Overland Road from the west; One access is proposed via E. View Circle/Collector (new proposed collector street) from the north; One access is proposed via the future collector to be built (extension of Movado Way) from the east.	-
<ul style="list-style-type: none"> • Traffic Level of Service 	See Figure 2 Below	-
ITD Comments Received	No	III.L
Meridian Fire	A full fire plan review shall be completed prior to construction.	III.C
Meridian Police	N/A	III.D
Meridian Public Works Wastewater	See Section IV B; Impacts or Concerns: No	III.B
Meridian Public Works Water	See Section IV B; Impacts or Concerns: No	III.B
School District(s)		III.J
<ul style="list-style-type: none"> • Distance 		-
<ul style="list-style-type: none"> • Capacity of Schools 		-
<ul style="list-style-type: none"> • Number of Students Enrolled 		-

See City/Agency Comments and Conditions Section and public record for all department/agency comments received.

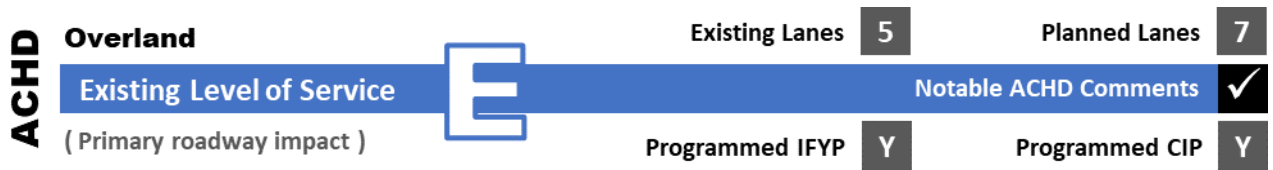
Figure 1: One-Mile Radius Existing Condition Metrics





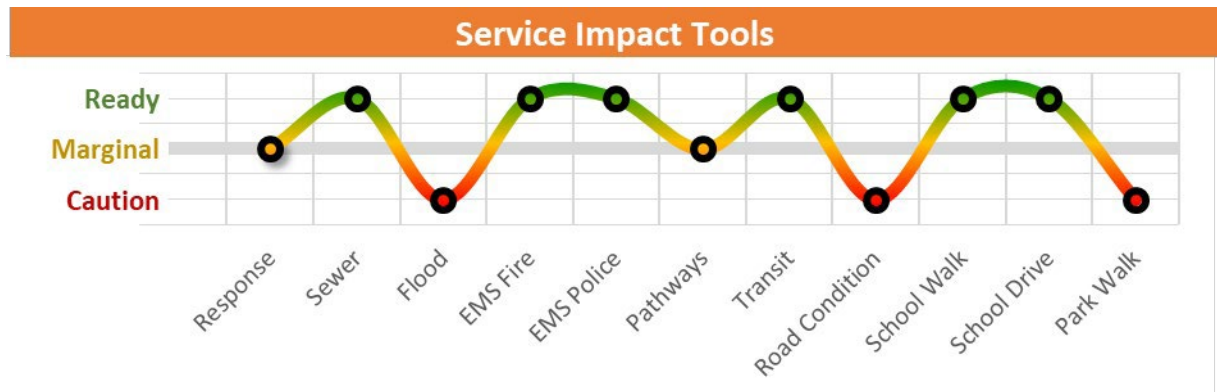
Notes: See VI. Additional Notes & Details for Staff Report Maps, Tables, and Charts.

Figure 2: ACHD Summary Metrics



Notes: See VI. Additional Notes & Details for Staff Report Maps, Tables, and Charts.

Figure 3: Service Impact Summary



Notes: See VI. Additional Notes & Details for Staff Report Maps, Tables, and Charts.

III. STAFF ANALYSIS

Comprehensive Plan and Unified Development Code (UDC)

A. General Overview

Land Use: The Future Land Use Map (FLUM) contained in the Comprehensive Plan designates this property as Mixed Use – Regional (MU-R).

The purpose of the MU-R designation is to provide a mix of employment, retail, and residential dwellings and public uses near major arterial intersections. The intent is to integrate a variety of uses together, including residential, and to avoid predominantly single-use developments such as a regional retail center with only restaurants and other commercial uses. Developments should be anchored by uses that have a regional draw with the appropriate supporting uses. The developments are encouraged to be designed consistent with the conceptual MU-R plan.

The site is currently surrounded by R1 and RUT residential county properties to the north, east, and west, with a few single-family residents remaining along Rolling Hill Drive & E. View Circle. Additionally, commercial space is located at the end of Rolling Hill Drive, including a Top Golf facility, commercial office, restaurants, and Eagle View Apartments comprising 396 units. To the south directly across Overland Road are Silverstone Apartments consisting of 312 residential units and Movado Village Apartments consisting of 50 residential units.

Furthermore, the property on the northwest corner of Rolling Hill Drive and Overland (abutting this project) is an active commercial roofing company located in the C-G zoning district.

The Applicant proposes to develop the site with two vertically integrated residential buildings, which include 20,134 square feet of commercial space and 90 residential dwelling units, and one (1) multi-family building consisting of 64 dwelling units (154 total units). The site is located near S. Eagle Rd. and E. Overland Rd., a major arterial intersection, and the Eagle Rd./I-84 interchange. The proposed commercial space is designed to offer the owner flexibility to divide the spaces as needed for future tenants, who will provide employment opportunities and services for residents with this development and the surrounding area. According to the Applicant's narrative, future tenants proposed for the commercial spaces will include boutique retail shops, restaurants, coffee shops, and medical and business offices. Additionally, Pedestrian walkways are proposed for interconnectivity within the overall site area. *A Mixed-Use Regional project should demonstrate a positive integration with the surrounding properties, offering a blend of residential, commercial, and recreational spaces that align with the overall regional development vision. The purpose of MU-R is to foster a sense of community and enhance the functionality of the area. However, a notable issue with the proposed development is the creation of a county enclave that lacks seamless integration with the project. Additionally, the disconnect with the property to the east presents several concerns. The development, as currently designed, seems to create a physical or functional boundary that isolates the eastern property, rather than encouraging a continuous flow between the properties including the surrounding area. This lack of integration may hinder interaction, disrupt cohesive infrastructure, and prevent coordinated land uses, causing a sense of isolation, particularly for the county enclave, from overall broader regional growth dynamics. Furthermore, with approximately 760 multi-family units already within the vicinity, Commission and Council should evaluate whether the City necessitates additional multi-family housing in this area.*

In reviewing development applications, the following items will be considered in all Mixed-Use areas, per the Comprehensive Plan (pg. 3-13, prior to the March 5, 2024 Revisions):
(Staff's analysis in italics)

- “A mixed-use project should include at least three types of land uses. Exceptions may be granted for smaller sites on a case-by-case basis. This land use is not intended for high density residential development alone.”

The proposed development includes a vertically integrated residential project and a multi-family residential development (i.e. apartment), which will add to the variety of uses in the larger MU-R designated area to the north and south consisting of office, retail, entertainment, restaurants, hotel uses, and multi-family residential.

- “Where appropriate, higher density and/or multi-family residential development is encouraged for projects with the potential to serve as employment destination centers and when the project is adjacent to US 20/26, SH-55, SH-16 or SH-69.”

The proposed multi-family high-density development should provide housing options in close proximity to nearby employment uses in the immediate area.

- “Mixed Use areas are typically developed under a master or conceptual plan; during an annexation or rezone request, a development agreement will typically be required for developments with a Mixed-Use designation.”

A site plan was submitted with the proposed annexation application for the subject property, which is located within the MU-R designation. It is recommended that a Development Agreement be included as a provision of annexation, tying future development to this plan and adhering to the general guidelines for mixed-use developments, specifically the MU-R designation.

- “In developments where multiple commercial and/or office buildings are proposed, the buildings should be arranged to create some form of common, usable area, such as a plaza green space.”

The site plan depicts a common open space area and plaza area adjacent to Building C that appears to meet this guideline. The common open space and plaza area are counted towards the open space requirements for the proposed multi-family development.

- “The site plan should depict a transitional use and/or landscaped buffering between commercial and existing low- or medium-density residential development.”

Multi-family residential and commercial uses are proposed on this site, which is adjacent to existing rural residential properties. The proposed development will serve as a transition and buffer to commercial uses on the west and the out parcel in the center of this development. Additionally, a 25-foot wide landscaped buffer is required along the northern and western boundaries of the site, where it borders existing residential areas, in accordance with the C-C zoning district regulations.

- “Community-serving facilities such as hospitals, clinics, churches, schools, parks, daycares, civic buildings, or public safety facilities are expected in larger mixed-use developments.”

No such uses are specifically proposed in this development – the tenants of the commercial spaces are unknown at this time; however, St. Luke’s hospital, daycare, and other medical offices are in the immediate area.

- “Supportive and proportional public and/or quasi-public spaces and places including but not limited to parks, plazas, outdoor gathering areas, open space, libraries, and schools are expected; outdoor seating areas at restaurants do not count.”

An outdoor plaza/gathering area and common grass area are depicted on the conceptual development plan between Building C on the western portion of the site. Details should be submitted with development of these buildings that demonstrate compliance with this guideline.

- “Mixed-use areas should be centered around spaces that are well-designed public and quasi-public centers of activity. Spaces should be activated and incorporate permanent design

elements and amenities that foster a wide variety of interests ranging from leisure to play. These areas should be thoughtfully integrated into the development and further placemaking opportunities considered.”

The proposed conceptual development plan includes a plaza/gathering area and a common grassy area between Building C on the western portion of the site. These areas are intended to serve as the heart of the development, providing a focal point for community interaction. The site/landscape plan features artistic bench seating adjacent to the green space, offering an area for leisure and recreation. Further amenities for the multi-family development include a pool and bike repair station. Additionally, a pedestrian circulation network is proposed around the perimeter of the overall development and throughout the site, enhancing pedestrian walkability within the development and to Overland Road only. Furthermore, a 10-foot pathway is required along Overland Road and the northern boundary adjacent to Five Mile Creek. However, there will be a gap along Overland Road in front of the out parcel in the center of this development, which is not preferred.

- “All mixed-use projects should be directly accessible to neighborhoods within the section by both vehicles and pedestrians.”

*The proposed development is directly accessible by vehicle to residents to the north via S. Rolling Hill Dr and E. View Circle, with a proposed connection across the canal. A future collector road, Movado Way, will bisect the northeast corner of this site. The Applicant has depicted cross-access to the County enclave in the center of the proposed project, and has illustrated how this parcel could be developed in the future. However, there are no pedestrian pathway stubs to this site from the adjacent County and commercial properties. Although the Applicant proposes a 10-foot pathway along Overland Road and a pathway along the northern boundary of this site adjacent to the Five Mile Creek, these pathways do not connect to any destinations and are stubbed to the surrounding properties. **The proposed pathways do not link the mixed-use project with nearby parks, schools, shopping areas, or other neighborhood amenities, except for the potential future commercial tenants in the proposed vertically integrated buildings. A mixed-use project should integrate seamlessly with the surrounding area to enhance connectivity, foster community, and improve the local environment. However, the proposed development isolates itself from the surrounding area and fails to provide integration or connectivity with its surroundings.***

S. Rolling Hill Dr. is currently a substandard street and lacks pedestrian facilities; ACHD is requiring off-site improvements with this application consisting of a sidewalk along one side of Rolling Hill and pavement widening. Furthermore, a portion of the future collector to be built on the east is contingent on the development of the properties to the east and north. According to ACHD’s Staff Report, the Applicant is required to construct View Circle as ½ of a 36-foot wide residential collector roadway with curb, gutter, and a 10-foot wide pathway as required by the Meridian Pathway Master Plan.

Staff recommends that the proposed pathway provided along the northern boundary adjacent to the Five Mile Creek be widened to 10-feet in width as required by the Meridian Master Pathways Plan.

- “Alleys and roadways should be used to transition from dissimilar land uses, and between residential densities and housing types.”

A 25-foot wide densely landscaped buffer and a driveway is proposed along the western and northern boundary of the site as a transition and buffer between existing rural residential properties and the proposed high-density multi-family residential development. A 5-foot landscape buffer is proposed along the eastern boundary of the site adjacent to the parking stalls. The Applicant illustrates an approximately 40-foot wide landscape buffer on the property to the east along with the layout for the potential future collector roadway. This is concerning because

this configuration could make it difficult to integrate with the surrounding properties as envisioned by the Plan.

- “Because of the parcel configuration within Old Town, development is not subject to the Mixed-Use standards listed herein.”

The subject property is not located in Old Town; therefore, this item is not applicable. In reviewing development applications, the following items will be considered in MU-R areas, per the Comprehensive Plan (pgs. 3-16 thru 3-17):

- Development should generally comply with the general guidelines for development in all Mixed-Use areas.

Staff’s analysis on the proposed project’s compliance with these guidelines is included above.

- Residential uses should comprise a minimum of 10% of the development area at gross densities ranging from 6 to 40 units/acre. There is neither a minimum nor maximum imposed on non-retail commercial uses such as office, clean industry, or entertainment uses. *The total development area consists of 5.89 acres; the multi-family residential and vertically integrated residential project consists of 5.89 acres in accord with this guideline. Multi-family apartments are proposed at a gross density of 26.15 units/acre, which falls within the desired density range.*

- Retail commercial uses should comprise a maximum of 50% of the development area.

A mix of non-residential commercial uses will be provided on 5% of the development area in accord with this guideline. Retail uses are expected to comprise only a small portion of the development.

Where the development proposes public and quasi-public uses to support the development, the developer may be eligible for additional area for retail development (beyond the allowed 50%), based on the ratios below:

- For land that is designated for a public use, such as a library or school, the developer is eligible for a 2:1 bonus. That is to say, if there is a one-acre library site planned and dedicated, the project would be eligible for two additional acres of retail development.
- For active open space or passive recreation areas, such as a park, tot-lot, or playfield, the developer is eligible for a 2:1 bonus. That is to say, if the park is 10 acres in area, the site would be eligible for 20 additional acres of retail development.
- For plazas that are integrated into a retail project, the developer would be eligible for a 6:1 bonus. Such plazas should provide a focal point (such as a fountain, statue, and water feature), seating areas, and some weather protection. That would mean that by providing a half-acre plaza, the developer would be eligible for three additional acres of retail development.

This guideline is not applicable as no public/quasi-public uses are proposed in the MU-R designated area on this site.

UNIFIED DEVELOPMENT CODE ANALYSIS (UDC)

Annexation: The proposed annexation is for 6.90-acres of land with a C-C (Community Business District) zoning district. The proposed use of the property includes multi-family residential apartments and a vertically integrated residential project. A multi-family development requires approval of a CUP in the C-C zoning district and is subject to the specific use standards outlined in UDC 11-4-3-27; a vertically integrated residential project is principally permitted in the C-C zoning district and is subject to the specific use standards outlined in UDC 11-4-3-41.

The proposed C-C zoning district is consistent with the associated MU-R FLUM designation as are the proposed uses.

The property is contiguous to City annexed land and is within the City’s Area of City Impact boundary. A legal description and exhibit map of the overall annexation area is included in Section VII.K.

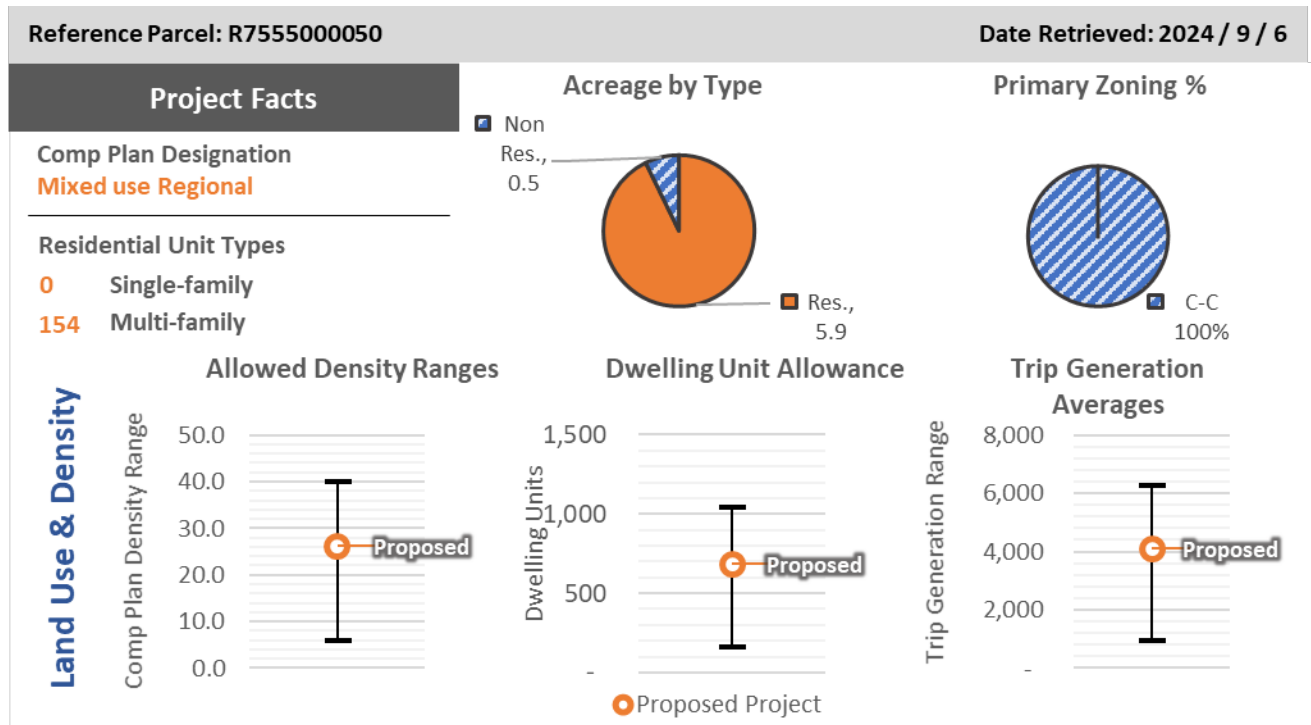
The City may require a development agreement (DA) in conjunction with an annexation pursuant to Idaho Code section 67-6511A. Staff recommends a DA is required with this application, containing the provisions noted in Section IV.G., as discussed herein.

Conditional Use Permit (CUP): A CUP is requested for a multi-family development comprising of one (1) multi-family building consisting of 64 dwelling units on 5.89 acres of land in the proposed C-C zoning district. Unit types consist of 94 1-bedroom and 60 2-bedroom units. The proposed gross density of the development is 26.15 units per acre, which is consistent with that desired in MU-R designated areas.

Table 4: Project Overview

Description	Details
History	None
Phasing Plan	No Phasing Plans are proposed
Residential Units	94 1-Bedroom units & 60 2-Bedroom units; 154 Multi-Family units
Open Space	5.89 Acres (256,568 SF)/10% = 25,657 SF/50% of Buffers = 8,488/250 SF Per Unit = 38,500 SF
Amenities	4 Amenities provided (Fitness Center, Plaza, Pool, Bike Repair Station)
Physical Features	Five Mile Creek runs along the northern boundary of the site
Acreage	6.90 acres Annexation; 5.89 Conditional Use Permit
Lots	5 Lots
Density	Gross Density 22.31/Net Density 26.15

Figure 4: Project Metrix



Notes: See VI. Additional Notes & Details for Staff Report Maps, Tables, and Charts.

B. Site Development and Use Analysis

1. Existing Structures/Site Improvements (UDC 11-1):

There are four (4) existing structures on this site (5 parcels total); the four (4) homes and accessory structures should be removed prior to development occurring on the property. *Additionally, the buildings currently reside on property lines. Staff recommends that the Applicant modify the property boundaries by submitting a Property Boundary Adjustment (PBA) application to the Planning Division, to be processed concurrently with the future CZC application.*

2. Proposed Use Analysis (UDC 11-2):

The proposed multi-family development comprises one (1) building (64 units), including a vertically integrated residential project in Building type B & C, totaling 20,134 square feet of commercial space featuring 90 dwelling units. The multi-family development requires a CUP, while the vertically integrated residential project is a principally permitted use in the C-C (Community Business district) zoning district, in accordance with the Allowed Uses listed in the Commercial Districts Table 11-2A-2. A multi-family development is subject to the specific standards in UDC 11-4-3-27 and a vertically integrated residential project is subject to the specific use standards in UDC 11-4-3- 41.

3. Specific Use Standards (UDC 11-4-3): 11-4-3-27. Multi-family development.

Multi-family developments with multiple properties shall be considered as one (1) property for the purpose of implementing the standards set forth in this section.

A. Purpose.

1. To implement the goals and policies of the Comprehensive Plan:
 - a. Plan for safe, attractive, and well-maintained neighborhoods that have ample open space, and generous amenities that provide varied lifestyle choices.
 - b. Require the design and construction of pathways connections, easy pedestrian and bicycle access to parks, safe routes to schools, and the incorporation of usable open
 - c. space with quality amenities as part of new multi-family residential and mixed-use developments.
2. To create multi-family housing that is safe and convenient and that enhances the quality of life of its residents.
 - a. To create quality buildings and designs for multi-family development that enhance the visual character of the community.
 - b. To create building and site design in multi-family development that is sensitive to and well-integrated with the surrounding neighborhood.
 - c. To create open space areas that contribute to the aesthetics of the community, provide an attractive setting for buildings, and provide safe, interesting outdoor spaces for residents.

B. Site design. (See Staff's analysis in bold)

1. Residential buildings shall provide a minimum setback of ten (10) feet unless a greater setback is otherwise required by this title and/or Title 10 of this Code. Building setbacks shall take into account windows, entrances, porches and patios, and how they impact adjacent properties. ***The Applicant has not provided setback measurements from the that the buildings may meet the setback in some areas. The Applicant should include the setback with the plans submitted with the CZC application.***
2. All on-site service areas, outdoor storage areas, waste storage, disposal facilities, and transformer and utility vaults shall be located in an area not visible from a public street, or shall be fully screened from view from a public street. ***The site plan depicts screened***

trash enclosures not visible from a public street; all proposed transformer/utility vaults and other service areas shall comply with this requirement.

3. A minimum of eighty (80) square feet of private, usable open space shall be provided for each unit. This requirement can be satisfied through porches, patios, decks, and/or enclosed yards. Landscaping, entryway and other accessways shall not count toward this requirement. In circumstances where strict adherence to such standard would create inconsistency with the purpose statements of this section, the Director may consider an alternative design proposal through the alternative compliance provisions as set forth in Section 11-5B-5 of this title. ***The floor plans submitted with this application depict patios and balconies; however, the plans do not include measurements for the square footage of the patio/balconies. The site plan indicates that a minimum of 50 sf of patio or /balcony space is provided for each dwelling unit. While the proposed 50 sf meets +the requirements for Buildings C & B (as part of the vertically integrated residential project), it does not meet the requirement for the multi-family Building A, which requires 80 sf of private open space. The Applicant may also request Alternative Compliance to allow the 50-square-foot patios, ensuring a consistent design throughout the development, as outlined in Section 11-5B-5 of the UDC. Staff recommends that the Applicant include the patio and balcony square footage measurements on the floor plans with the future CZC submittal or apply for Alternative Compliance as outlined in UDC 11-5B-5.***
4. For the purposes of this section, vehicular circulation areas, parking areas, and private usable open space shall not be considered common open space. ***The applicant has included landscaping adjacent to parking areas and landscape islands within the parking lot as part of the common open space calculations in the open space exhibit.***
5. No recreational vehicles, snowmobiles, boats or other personal recreation vehicles shall be stored on the site unless provided for in a separate, designated and screened area. ***The Applicant shall adhere to this standard.***
6. The parking shall meet the requirements set forth in Chapter 3, "Regulations Applying To All Districts", of this title.
7. Developments with twenty (20) units or more shall provide the following:
 - a. A property management office. ***Located in clubhouse in Building C***
 - b. A maintenance storage area. ***Located in clubhouse in Building C***
 - c. A central mailbox location, including provisions for parcel mail, that provide safe pedestrian and/or vehicular access. ***Located in clubhouse in Building C***
 - d. A directory and map of the development at an entrance or convenient location for those entering the development. ***Depict on site plan submitted with the future CZC application.***

The site plan submitted with the Certificate of Zoning Compliance application should depict the location of these items in accord with this standard.

C. Common open space design requirements.

1. The total baseline land area of all qualified common open space shall equal or exceed ten

(10) percent of the gross land area for multi-family developments of five (5) acres or more. When multi-family is approved concurrently with single-family, the minimum open space requirements in Section 11-3G-3 shall apply to the gross land area of entire development. ***Open Space requirements differ for vertically integrated residential projects and multi-family residential projects. Because the Applicant is requesting annexation for the subject site, Staff is evaluating the project as a whole and calculating the open space based on the multi-family requirements in the UDC. The site consists of 5.89 acres, requiring 0.589 acres (25,657 square feet) of baseline open space according to the standard. The Applicant is proposing 10 % of the baseline open space that meets this requirement.***

2. All common open space shall meet the following standards:
 - a. The development plan shall demonstrate that the open space has been integrated into the development as a priority and not for the use of land after all other elements of the development have been designed. Open space areas that has been given priority in the development design have:
 - (1) Direct pedestrian access;
 - (2) High visibility;
 - (3) Comply with Crime Prevention through Environmental Design (CTED) standards; and
 - (4) Support a range of leisure and play activities and uses. Irregular shaped, disconnected or isolated open spaces shall not meet this standard.
 - b. Open space shall be accessible and well connected throughout the development. This quality can be shown with open spaces that are centrally located within the development, accessible by pathway and visually accessible along collector streets or as a terminal view from a street.
 - c. The open space promotes the health and well-being of its residents. Open space shall support active and passive uses for recreation, social gathering and relaxation to serve the development.

Staff finds the proposed open space areas within the multi-family development comply with these standards by providing open space that is well connected, highly visible, and promotes health and well-being by supporting a range of leisure and play activities with the proposed amenities and general design of the open space. See the submitted landscape plan and rendering for a visual of compliance with this standard

3. Alternative compliance is available for the standards listed in subsections (C)1 and (C)2 above, if a project has a unique targeted demographic; utilizes other place-making design elements in Old-Town or mixed-use future land use designations with collectively integrated and shared open space areas.
4. All multi-family projects over twenty (20) units shall provide at least one (1) common grassy area integrated into the site design allowing for general activities by all ages. This area may be included in the minimum required open space total. Projects that provide safe access to adjacent public parks or parks under a common HOA, without crossing an arterial roadway, are exempt from this standard.

- a. Minimum size of common grassy area shall be at least five thousand (5,000) square feet in area. This area shall increase proportionately as the number of units increase and shall be commensurate to the size of the multi-family development as determined by the decision-making body. Where this area cannot be increased due to site constraints, it may be included elsewhere in the development.
- b. Alternative compliance is available for the standards listed in subsections (C)1 and (C)2 above, if a project has a unique targeted demographic; utilizes other place-making design elements in Old-Town or mixed-use future land use designations with collectively integrated and shared open space areas.

The submitted plans feature only one common grassy open space area of 5,000 square feet in size.

5. In addition to the baseline open space requirement, a minimum area of outdoor common open space shall be provided as follows:
 - a. One hundred fifty (150) square feet for each unit containing five hundred (500) or less square feet of living area. ***None of the units are below 500 square feet (s.f.) of living area.***
 - b. Two hundred fifty (250) square feet for each unit containing more than five hundred (500) square feet and up to one thousand two hundred (1,200) square feet of living area. ***There are 154 units proposed between 500 and 1,200 sf; therefore, a total of 0.88 acres (38,500 square feet) of common open space is required.***
 - c. Three hundred fifty (350) square feet for each unit containing more than one thousand two hundred (1,200) square feet of living area. ***None of the units are above 1,200 square feet of living area.***

At a minimum, a total of 64,157 s. (or 1.47-acres) of outdoor common open space is required to be provided in the proposed development. A total of approximately 63,000 square feet (or 1.45-acres) is proposed to be provided less than UDC standards. As mentioned above, the Applicant included open space adjacent to the parking and landscape islands within the parking lot, which are not usable for residents. Staff supports the slight decrease in the outdoor common open space provided since the vertically integrated residential project is not required to meet this requirement for 90 of the 154 residential units. Furthermore, the Applicant has submitted a plan that illustrates how the out-parcel in the center of this development could potentially be developed in the future. The site plan includes an additional 16 1-bedroom units and 16 2-bedroom units, which would require approximately an additional 9,157 square feet of outdoor common open space. A large green space is proposed to the north that appears to meet the overall requirement for future development. Overall, Staff finds that the proposed qualified open space complies with the standards in 11-4-3-27C.

6. Common open space shall be not less than four hundred (400) square feet in area, and shall have a minimum length and width dimension of twenty (20) feet. ***The Applicant included areas that do not meet this requirement as part of the common open space calculations. Staff has excluded these areas from the common open space count.***
7. In phased developments, common open space shall be provided in each phase of the development consistent with the requirements for the size and number of dwelling units. ***Staff is not aware of any phasing for the proposed multi-family project.***

8. Unless otherwise approved through the conditional use process, common open space areas shall not be adjacent to collector or arterial street buffers unless separated from the street by a berm or constructed barrier at least four (4) feet in height, with breaks in the berm or barrier to allow for pedestrian access. ***The Applicant is not proposing any common open space areas adjacent to E. Overland Road (arterial roadway).***

9. Buffer(s): One hundred (100) percent of the landscape buffer along collector streets and fifty (50) percent of the landscape buffer along arterial streets that meet the enhanced buffer requirements below may count towards the required baseline open space.
 - a. Enhanced landscaping as set forth in Article 11-3B, Landscaping Requirements;
 - b. Multi-use pathways;
 - c. Enhanced amenities with social interaction characteristics;
 - d. Enhanced context with the surroundings.
 - **Enhanced landscaping:** *A 35-foot landscape buffer is required along E. Overland Road, as it is designated as an entry corridor. The Applicant counts 50% of the landscape buffer along E. Overland Road (arterial roadway) towards the baseline open space requirement. The proposed landscape buffer features a dry stream bed design with boulders and shrubs north of the 10-foot wide detached pathway, with trees located on the south side adjacent to E. Overland Road. Staff finds that the proposed denser plantings meet the enhanced landscaping as outlined in UDC 11-3B-7. Staff recommends that the Applicant specify the type of lawn and dry creek rock in the plant schedule on the landscape plan.*
 - **Multi-use pathways (UDC 11-3A-5):** *Goals 4.04.01A and Goal 4.04.02 of the Comprehensive Plan emphasize the importance of ensuring that new development and subdivisions connect to the surrounding pathway system and link pathways to important pedestrian generators, environment features, historic landmarks, public facilities, Town Centers, and business districts. The Applicant proposes a pathway along Five Mile Creek on the northern portion of the site, with trees and shrubs lining both sides. As noted above, the pathway should be widened to 10-feet in width as required by the Meridian Master Pathways Plan. Additionally, a 10-foot wide pathway is proposed along E. Overland Road, providing access to key areas within the development. However, there will be a gap along Overland Road in front of the outparcel at the center of this development, which is not ideal. Although the pathways are designed for walking, jogging, cycling, or other forms of active transportation, the pathway along Five Mile Creek does not extend beyond this site. Despite these limitations, Staff finds that the Applicant meets the intent of this requirement.*
 - **Enhanced amenities with social interaction characteristics:** *The Applicant is proposing a plaza area and open grassy space adjacent to Building C, as well as a pool adjacent to Building A, to encourage social interaction among residents, future commercial tenants, and visitors. These amenities should be designed to be strategically placed to draw people in and create a sense of community. Staff believes that the Applicant meets the intent of this requirement.*
 - **Enhanced context with the surroundings:** *The Applicant has designed the buffer zones to complement the existing neighborhood and adjacent properties as much as*

possible. The proposed landscaping and amenities aim to create a smooth transition between different land uses, considering factors like views, access points, and neighborhood connectivity. As mentioned above, Staff acknowledges that this is a challenging project, given the undeveloped surrounding properties, including the out-parcel at the center of this site and the future collector roadway. Commission and Council should rely on all relevant information when determining if this is the right timing for this project.

D. Site development amenities.

1. All multi-family developments shall provide for quality of life, open space and recreation amenities to meet the particular needs of the residents as follows:
 - a. *Quality of life.*
 - (1) Clubhouse.
 - (2) Fitness facilities.
 - (3) Enclosed bike storage.
 - (4) Public art such as a statue.
 - (5) Dog park with waste station.
 - (6) Commercial outdoor kitchen.
 - (7) Fitness course.
 - (8) Enclosed storage
 - b. *Open space.*
 - (1) Community garden.
 - (2) Ponds or water features.
 - (3) Plaza.
 - (4) Picnic area including tables, benches, landscaping and a structure for shade.
 - c. *Recreation.*
 - (1) Pool.
 - (2) Walking trails.
 - (3) Children's play structures.
 - (4) Sports courts.
 - d. *Multi-modal amenity standards.*
 - (1) Bicycle repair station.
 - (2) Park and ride lot.
 - (3) Sheltered transit stop.
 - (4) Charging stations for electric vehicles.
2. The number of amenities shall depend on the size of multi-family development as follows:
 - a. For multi-family developments with less than twenty (20) units, two (2) amenities shall be provided from two (2) separate categories.
 - b. For multi-family development between twenty (20) and seventy-five (75) units, three (3) amenities shall be provided, with one (1) from each category.

- c. For multi-family development with seventy-five (75) units or more, four (4) amenities shall be provided, with at least one (1) from each category.
 - d. For multi-family developments with more than one hundred (100) units, the decision-making body shall require additional amenities commensurate to the size of the proposed development.
3. The decision-making body shall be authorized to consider other improvements in addition to those provided under this subsection D, provided that these improvements provide a similar level of amenity.

As noted above, Staff is evaluating the project as a whole and calculating the site amenities based on the multi-family requirements in the UDC. To satisfy the specific use standards for the proposed 154 multi-family units the decision-making body may deem if the proposed amenities for the multi-family development are sufficient. According to the submitted plans and narrative, the applicant believes they have included four qualifying amenities, each from a different category. The proposed amenities consist of the following:

- *Swimming pool*
- *Fitness center*
- *10-foot wide pathway that runs along the Ten Mile Creek*
- *Bike repair station*

All amenities are located within the central open space lot adjacent to Buildings A and C. Staff recommends that the Applicant add a picnic area that includes tables, benches, and a structure for shade in the open space grassy area by the pool to enhance recreational options for the residents. Overall, the proposed amenities satisfy the required standards.

E. Landscaping requirements.

1. Development shall meet the minimum landscaping requirements in accord with Chapter 3, "Regulations Applying To All Districts", of this Title.
 - **Landscape buffers along streets:** *Required street buffer widths are as follows: 35-foot along E. Overland Road (arterial roadway), 20-foot along S. Rolling Hill Drive (local street), 20-foot along E. View Circle (future collector – north), and 20-foot along N. Movado Way (future collector -east). The Applicant is depicting a 5-foot wide landscape buffer on the east side adjacent to the parking and illustrating an approximate 44-foot wide buffer with the new collector street on the property to the east. A calculations table is included on the landscape plan submitted with this application demonstrating compliance with the requirements outlined in UDC 11-3B-7.*
 - **Parking lot landscaping:** *The landscape plan appears to meet the requirements outlined in UDC 11-3B-8 for all perimeter and internal landscaping planters.*
 - **Landscape buffers to adjoining uses:** *The landscape plan appears to comply with the requirements outlined in UDC 11-3B-9.*
 - **Tree preservation UDC 11-3B-10:** *Mitigation shall be required for all existing trees four-inch caliper or greater that are removed from the site with equal replacement of the total calipers lost on site up to an amount of one hundred (100) percent replacement (Example: Two (2) ten-inch caliper trees removed may be mitigated with four 5-inch caliper trees, five (5) four-inch caliper trees, or seven (7) three-inch caliper trees). Deciduous specimen trees four-inch caliper or greater may*

count double towards total calipers lost, when planted at entryways, within common open space, and when used as focal elements in landscape design.

No mitigation is required in the following:

(1) Existing prohibited trees within the property boundary of the project;

(2) Existing dead, dying, or hazardous trees certified prior to removal by the City of Meridian Parks Department Arborist;

(3) Trees that are required to be removed by another governmental agency having jurisdiction over the project.

Staff recommends that the Applicant include tree mitigation notes on the Landscape plan submitted with the future CZC Application.

➤ **Pathway landscaping:** *The landscape plan appears to comply with the requirements outlined in UDC 11-3B-12.*

2. All street facing elevations shall have landscaping along their foundation. The foundation landscaping shall meet the following minimum standards:

a. The landscaped area shall be at least three (3) feet wide.

b. For every three (3) linear feet of foundation, an evergreen shrub having a minimum mature height of twenty-four (24) inches shall be planted.

c. Ground cover plants shall be planted in the remainder of the landscaped area.

The submitted landscape plans indicate that the Applicant is in compliance with this standard.

F. Maintenance and ownership responsibilities. All multi-family developments shall record legally binding documents that state the maintenance and ownership responsibilities for the management of the development, including, but not limited to, structures, parking, common areas, and other development features.

The applicant shall comply with this requirement and provide said document at the time of CZC submittal.

G. Police access under exigent circumstances. Multifamily developments with units that take access via secured common corridors shall install and maintain a keyless entry system, or suitable alternative, to provide police access to the common corridors under exigent circumstances. The keyless entry system or alternative shall be subject to review and approval by the Meridian Police Department.

The Applicant should work with the Police Dept. on a plan for emergency police access into each building entry point using a multi-technology keypad.

C. Design Standards Analysis

1. Existing structure and Site Design Standards (*Comp Plan, UDC 11-3A-19*):

A. Purpose.

1. Encourage site design that provides an interconnected network of walkways, pathways,

streets, and/or drive aisles that combined promote pedestrian and vehicular mobility within the development and connect to adjacent developments.

2. To facilitate the efficient movement of traffic into, out of, and through a site, protect pedestrian and bicycle users, establish an aesthetically consistent street presence and limit the visual impacts of large parking areas on a site.

The proposed multi-family and vertically integrated residential project incorporates a thoughtfully designed network of internal walkways, pathways, and drive aisles that enhance mobility and connectivity within the development. The site layout prioritizes pedestrian access, with pathways that link key areas within the development, including residential units, commercial spaces, and community amenities. The proposed pathway along E. Overland extends outward to connect with existing sidewalks between neighborhoods and the commercial development to the east. However, Staff finds that if the surrounding properties were developed, it would create a more seamless transition with defined pedestrian routes, further ensuring safety, and accessibility, and promoting a walkable, interconnected community environment.

The site layout includes well-placed access points and drive aisles that facilitate smooth vehicular movement, reducing congestion and providing clear entry and exit routes. Parking areas are strategically placed to minimize visual impact, incorporating landscape buffers, trees, and planters that maintain an attractive street presence.

Additionally, the Applicant has extended the drive aisles and pedestrian connections for cross-access to the out-parcel within the center of this development. Staff would like to see the drive aisles stubbed to this parcel line-up directly across from each other for easier vehicle maneuvering. Furthermore, Staff recommends that the Applicant revise the landscape plan to include cross-access to the outparcel to match the site plan.

Goal 2.01.01K of the Comprehensive Plan emphasizes strategically locating higher-density housing near key corridors with existing or planned transit routes. This site is located within a third of a mile from bus stops to the west at the northeast and southeast corners of Overland and Eagle Road (H-55), as well as at the southwest and northwest corners of Overland Road and Cloverdale. Additionally, a project to the east was recently approved by the City of Boise, which supports the addition of a new bus stop between the project site and Cloverdale. Furthermore, this development is near employment centers and commercial areas, reducing commute times and supporting a potential live-work environment in the future.

Goal 2.01.02D of the Comprehensive Plan emphasizes incorporating design elements that ensure a safe, attractive, and well-maintained neighborhood. The development includes ample open spaces, such as landscaped green areas, a plaza, and pool, which provides residents with spaces for recreation and relaxation. Amenities, such as walking paths, a fitness center, a bike repair station, and social gathering areas, are integrated to support a variety of lifestyles. Overall, the development, along with the future commercial tenant spaces, will promote an inclusive community environment that caters to diverse needs and preferences.

2. Parking (UDC 11-3C):

Off-street vehicle parking is required for the proposed multi-family dwellings and vertically integrated residential project as set forth in UDC Table 11-3C-6.

➤ Residential parking analysis

Based on (24) 2-bedroom units and (40) 1-bedroom units for the multi-family portion of the development, a minimum of 114 parking spaces are required, including six (6) spaces

for guest parking, with 64 of those in a covered carport or garage. Accessible parking is required in accord with ADA standards.

Based on (36) 2-bedroom units and (54) 1-bedroom units for the vertically integrated residential project portion of the development, a minimum of 108 parking spaces are required. Accessible parking is required in accord with ADA standards.

A total of 281 spaces are proposed, including 125 covered spaces, exceeding the minimum requirement (222 required). However, the Applicant is proposing eleven (11) compact stalls on the west side of the out-parcel. Staff recommends that the Applicant shift Building C and the associated parking 2 -feet to the west, as there is sufficient space within the 20-foot buffer required adjacent to Rolling Hill Drive.

➤ **Bicycle parking analysis**

Based on 281 vehicle parking spaces, a minimum of eleven (11) bicycle parking spaces should be provided in accord with the standards listed in UDC 11-3C-5C (30 bicycle space are provided). Five (5) bicycle racks capable of holding a minimum of six (6) bicycles, are provided throughout the development for residents and guests in accord with the standards listed in UDC 11-3C-5C.

3. Building Elevations (Comp Plan, Architectural Standards Manual):

Goals 5.01.02D and 5.01.02A of the Comprehensive Plan emphasize the importance of building design and landscaping in buffering, screening, beautifying, and integrating commercial, multifamily, and parking areas with existing neighborhoods. The schematic renderings for the proposed 4-story structures, detailed in Section V.J, reveal a design utilizing a mix of brick, metal siding, and stucco and fiber cement accents in a neutral color scheme.

The final design of all buildings must comply with the standards outlined in the Architectural Standards Manual. The applicant has revised the elevations to better align with the surrounding buildings and the city's architectural standards, reflecting a commitment to meeting the design and aesthetic requirements.

4. Fencing (UDC 11-3A-6, 11-3A-7):

All fencing is required to comply with the standards listed in UDC 11-3A-7. ***The Applicant is not proposing fencing for this project. However, Staff recommends that 6-foot non-scalable wrought iron fence be erected as a safety barrier in compliance with the International Swimming Pool and Spa Code, and provide a detail of the fencing on the plans submitted with the future CZC application.***

5. Parkways (Comp Plan, UDC 11-3A-17):

All sidewalks around buildings and serving public streets shall be a minimum of five (5) feet in width, except if detached sidewalks are provided on local public streets in residential subdivisions, the minimum sidewalk width may be reduced to four (4) feet. ***The Applicant has provided an internal pedestrian circulation plan that shows how the sidewalks meet these standards to provide adequate pedestrian access and safety throughout the development in Section VII.I. below. However, the Applicant did not include the dimensions of the sidewalks on the site plan. Staff recommends that the Applicant include the measurement of the sidewalks on the plans submitted with the future CZC application.***

The minimum width of parkways planted with Class II trees shall be eight (8) feet. The width can be measured from the back of curb where there is no likely expansion of the street section within the right-of-way; the parkway width shall exclude the width of the sidewalk. Class II trees are the preferred parkway trees. ***The plans submitted appear to comply with this***

requirement.

Goal 6.01.01J of the Comprehensive Plan emphasizes the importance of ensuring and encouraging new development to include buffered sidewalks, a sidewalk separated from the motor vehicle lane by a planter strip, especially on collector and arterial roadways. *The submitted plans incorporate detached sidewalks/pathways with landscape buffers proposed along the future collector, arterial roadway, and local street. The sidewalks are separated from motor vehicle lanes by landscape planter strips, enhancing pedestrian safety and comfort by providing a clear buffer between pedestrians and vehicles. The planter strips feature shrubs and trees that help to reduce noise and provide shade. Integrating sidewalks/pathways into the buffers promotes a walkable environment that aligns with the Comprehensive Plan's goal of creating safe, pedestrian-friendly streetscapes.*

D. Transportation Analysis

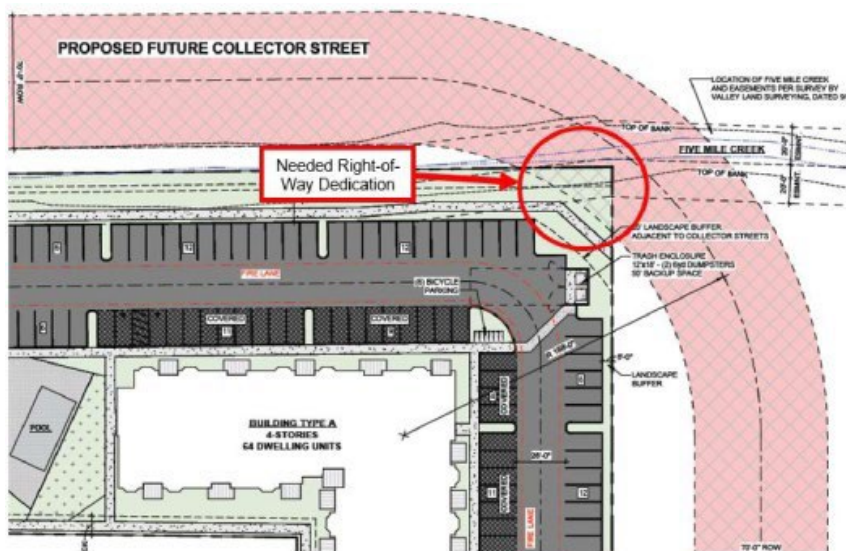
1. Access (Comp Plan, UDC 11-3A-3, UDC 11-3H-4):

Goals 6.01.01H and 6.01.02B of the Comprehensive Plan emphasize reducing the number of access points onto arterial streets through methods such as cross-access agreements, access management, and frontage or backage roads. These goals also stress the importance of enhancing local and collector street connectivity and incorporating pedestrian access connectors to link subdivisions and promote neighborhood connectivity within the community pathway system.

One full-access driveway is proposed via S. Rolling Hill Drive (local street) off E. Overland Road (arterial roadway). According to ACHD's Staff Report, the Applicant should construct Rolling Hill Drive as ½ of a 36-foot wide local roadway with curb, gutter, and 5-foot wide concrete sidewalk abutting the site.

One full-access driveway is proposed on E. View Circle (collector roadway). According to ACHD's Staff Report, the Applicant should construct View Circle as ½ of a 36-foot wide residential collector roadway with curb, gutter, and a 10-foot wide pathway as required by the Meridian Pathway Master Plan.

One full-access driveway is proposed via the future collector Street (Movado Way). According to ACHD's Staff Report, the Applicant should dedicate right-of way abutting the site's northeast property line to accommodate the future construction of a planned collector roadway (see figure below).



Given that both the subject property and the adjacent out-parcel at the center of this development are designated MU-R (Mixed Use-Regional) on the Future Land Use Map (FLUM), and there is no existing local street access, the Applicant, at the request of Staff, has proposed a cross-access/ingress-egress easement and driveways, including a pedestrian walkway, to facilitate future interconnectivity with this property in accordance with UDC 11-3A-3A.2. ACHD and Staff both request that the Applicant coordinate with the owner of the out-parcel surrounded by this site to close the existing driveways on Overland and provide access to the neighboring parcel internally through the site. If the driveways cannot be wholly closed, the Applicant should close the portions of the shared-access driveways that abut the site on Overland Road with an 8-foot wide planters' strip and a 10-foot wide multi-use pathway.

Staff recommends that the Applicant comply with ACHD's Site-Specific Conditions of Approval in Section III.K.

2. Sidewalks (UDC 11-3A-17):

All sidewalks around buildings and serving public streets shall be a minimum of five (5) feet in width, except if detached sidewalks are provided on local public streets in residential subdivisions, the minimum sidewalk width may be reduced to four (4) feet. *All internal sidewalks within the development appear to meet this requirement; however, the Applicant has not included the dimensions of the sidewalks on the site plan submitted. Staff recommends that the Applicant include the dimensions of the sidewalks on the plans submitted with the future CZC Application. There are existing 7-foot sidewalks abutting the site along Overland Road. The Applicant is proposing to construct an 8-foot wide planter strip with a 10-foot wide multi-use pathway abutting the site on Overland Road consistent with the City's Comprehensive Plan and Ada County Highway Districts Master Street Map and Roadways to Bikeways Master Plan.*

E. Services Analysis

1. Waterways (Comp Plan, UDC 11-3A-6):

Goal 4.05.01D and Goal 6.01.04B of the Comprehensive Plan emphasize the importance of improving and protecting creeks and other natural waterways throughout commercial, industrial and residential areas. Develop and implement agreements with irrigation districts and the Union Pacific Railroad to allow for bike/pedestrian pathways. *The Five Mile Creek runs along the northwest boundary of the site and is proposed to be preserved as a natural amenity in accordance with UDC 11-3A-6. The Applicant prioritizes the preservation of the existing creek by incorporating the creek into the site design as a natural amenity. The Applicant is proposing to protect and enhance Five Mile Creek by adding planters with trees and shrubs, on both sides of the creek. Additionally, the multi-use pathway is proposed to run parallel to the Creek.*

Along with a bridge for the drive aisle across that will connect to E. View Circle Drive, the Applicant should coordinate with Nampa Meridian Irrigation District (NMID) to ensure that the development adheres to best practices for waterway protection.

2. Pressurized Irrigation (UDC 11-3A-15):

Underground pressurized irrigation water is required to be provided as set forth in UDC 11-3A-15

3. Storm Drainage (UDC 11-3A-18):

The Ten Mile Creek bisects the southwest corner of this site and is proposed to be left open as an amenity for the development in accord with UDC 11-3A-6, which requires laterals to be piped unless improved as a water amenity or linear open space.

4. Utilities (Comp Plan, UDC 11-3A-21):

Goal 3.01.01F of the Comprehensive Plan emphasizes the importance of permitting new development only where urban services can be reasonably provided at the time of final approval and development is contiguous to the City.

Connection to City water and sewer services is required in accord with UDC 11-3A-21 and the Comprehensive Plan.

IV. CITY/AGENCY COMMENTS & CONDITIONS

F. Meridian Planning Division

Annexation & Zoning

1. A Development Agreement (DA) is required as a provision of annexation of this property. Prior to approval of the annexation ordinance, a DA shall be entered into between the City of Meridian, the property owner(s) at the time of annexation ordinance adoption, and the developer. **A Certificate of Zoning Compliance and Design Review Application will not be accepted until the Ordinance and Development Agreement are recorded.**

Currently, a fee of \$303.00 shall be paid by the Applicant to the Planning Division prior to commencement of the DA. The DA shall be signed by the property owner and returned to the Planning Division within six (6) months of the City Council approval of the annexation Findings. The DA shall, at minimum, incorporate the following provisions IF City Council determines annexation is in the best interest of the City:

- a. Future development of this site shall be consistent with the site plan, landscape plan, qualified open space, and qualified site amenities (i.e. swimming pool, fitness center, a 10-foot multiuse pathway along the Five Mile Creek, plaza area, and bike repair station); two (2) vertically integrated residential structures featuring 90 dwelling units and 20,134 square feet of commercial space along with one (1) multi-family building consisting of 64 dwelling units (for a total of 154 residential units); and conceptual building elevations and renderings as proposed in Section VII and the provisions contained herein.
- b. The Development shall comply with the Specific Use Standards for a multi-family development outlined in UDC 11-4-27 and a vertically integrated residential project as outlined in UDC 11-4-3-41.
- c. There are four (4) existing structures on this site (5 parcels total); the four (4) homes and accessory structures should be removed prior to City Engineer's signature on the final plat.

Conditional Use Permit (Multi-Family Development):

2. The trash enclosures shall contain receptacles to accommodate recycling.
3. Compliance with the specific use standards listed in UDC 11-4-3-27 for Multi-family Developments is required.
4. The site plan shall be revised as follows:
 - a. Depict a directory and map of the development at each of the entrances or in a convenient location for those entering the development in accord with UDC 11-4-3-27B.7d.
 - b. Include the building setbacks on the plans submitted with the future CZC application.
 - c. Align the drive aisles stubbed to the outparcel directly across from each other for easier vehicle maneuvering.

- d. Include the measurement of the sidewalks on the plans submitted with the future CZC application.
 - e. Include 80 square footage of patio and balcony space for the multi-family portion of the development on the floor plans with the future CZC submittal or apply for Alternative Compliance as outlined in UDC 11-5B-5.
 - f. Shift Building C and the associated parking 2 -feet to the west, to eliminate the compact parking stalls and provide standard 9 x 19 parking stalls.
 - g. Applicant shall submit a Property Boundary Adjustment (PBA) application to the Planning Division, to be processed concurrently with the future CZC application.
13. The landscape plan shall be revised as follows:
- a. The proposed pathway provided along the northern boundary adjacent to the Five Mile Creek shall be widened to 10-feet in width as required by the Meridian Master Pathways Plan.
 - b. Specify the type of lawn and dry creek rock in the plat schedule on the landscape plan submitted with the future CZC Application.
 - c. Add a picnic area that includes tables, benches, and a structure for shade in the open space grassy area by the pool to enhance recreational options for the residents.
 - d. Revise the landscape plan to include cross-access to the outparcel to match the site plan.
 - e. Install a 6-foot non-scalable wrought iron fence as a safety barrier around the pool, in compliance with the International Swimming Pool and Spa Code, and provide a detail of the fencing on the plans submitted with the future CZC application.
 - f. Include tree mitigation notes on the Landscape plan submitted with the future CZC Application.
 - g. A 10-foot wide detached sidewalk/multi-use pathway shall be along the northern boundary adjacent to the Five Mile Creek as required by the Meridian Master Pathways Plan.
14. A legally binding document shall be recorded that states the maintenance and ownership responsibilities for the management of the development, including, but not limited to, structures, parking, common areas, and other development features as set forth in UDC 11-4-3-27F. A copy of such shall be submitted to the Planning Division prior to issuance of the first Certificate of Occupancy within the development.
15. Multifamily developments with units that take access via secured common corridors shall install and maintain a keyless entry system, or suitable alternative, to provide police access to the common corridors under exigent circumstances as set forth in UDC 11-4-3-27G. The keyless entry system or alternative shall be subject to review and approval by the Meridian Police Department.
16. All structures are required to comply with the design standards listed in the Architectural Standards Manual.
17. **A Floodplain Permit is required; consult with the Floodplain Administrator prior to commencing development and to address timing, as there are inconsistencies in the FEMA Maps. Structures must be elevated to the flood protection level, and a Letter of Map Revision is required.**
18. A Certificate of Zoning Compliance application shall be submitted for the proposed use and site improvements that comply with all UDC conditions and the provisions contained herein.

19. The design of all future structures on this site, shall comply with the standards listed in the Architectural Standards Manual.
20. The Applicant shall comply with ACHD's Site-Specific Conditions of Approval.
21. The conditional use permit is valid for a maximum period of two (2) years unless otherwise approved by the City. During this time, the Applicant shall commence the use as permitted in accord with the conditions of approval, satisfy the requirements set forth in the conditions of approval, and acquire building permits and commence construction of permanent footings or structures on or in the ground as set forth in UDC 11-5B-6. A time extension may be requested as set forth in UDC 11-5B-6F.

G. Meridian Public Works

NON-PLAT CONDITIONS

Wastewater	
Distance to Sewer Services Sewer Shed Estimated Project Sewer ERU's WRRF Declining Balance Project Consistent with WW Master Plan/Facility Plan	Directly adjacent See application 14.68 MGD Yes
• Impacts/concerns	• Additional 10,200 gpd committed to model. • See Public Works Site Specific Conditions
Water	
Distance to Water Services Pressure Zone Estimated Project Water ERU's Water Quality Project Consistent with Water Master Plan Impacts/Concerns	Available at Site 4 See application None Yes See Public Works Site Specific Conditions

PUBLIC WORKS DEPARTMENT

Site Specific Conditions of Approval

1. Ensure no permanent structures (trees, bushes, buildings, carports, trash receptacle walls, fences, infiltration trenches, light poles, etc.) are built within the utility easement.
2. Provide sewer "to and through" to R7555000190, R7555000090, R7555000080, R7555000070, and R7555000060.
3. All cover parking needs to be clearly called out on the utility plan. Cover parking cannot be inside the required water or sewer easement(s).
4. Engineer to verify if there is a well onsite. If a well is located on the site it must be abandoned per regulatory requirements and proof of abandonment must be provided to the City. It may also be used for pressurized irrigation as either a primary source or secondary source.

General Conditions of Approval

1. Applicant shall coordinate water and sewer main size and routing with the Public Works Department.
2. Per Meridian City Code (MCC), the applicant shall be responsible to install sewer and water mains to and through this development. Applicant may be eligible for a reimbursement agreement for infrastructure enhancement per MCC 8-6-5.
3. The applicant shall provide easement(s) for all public water/sewer mains outside of public right of way (include all water services and hydrants). Sewer/water easement varies depending on sewer

depth. Sewer 0-20 ft deep require a 30 ft easement, 20-25 ft a 40 ft easement, and 25-30 ft a 45 ft easement. Ensure no permanent structures (trees, bushes, buildings, carports, trash receptacle walls, fences, infiltration trenches, light poles, etc.) are built within the utility easement. Submit an executed easement (on the form available from Public Works), a legal description prepared by an Idaho Licensed Professional Land Surveyor, which must include the area of the easement (marked EXHIBIT A) and an 8 1/2" x 11" map with bearings and distances (marked EXHIBIT B) for review. Both exhibits must be scaled, signed and dated by a Professional Land Surveyor. DO NOT RECORD.

4. The City of Meridian requires that pressurized irrigation systems be supplied by a year-round source of water (UDC 11-3B-6). The applicant should be required to use any existing surface or well water for the primary source. If a surface or well source is not available, a single-point connection to the culinary water system shall be required. If a single-point connection is utilized, the developer will be responsible for the payment of assessments for the common areas prior to receiving development plan approval.
5. Any structures that are allowed to remain shall be subject to evaluation and possible reassignment of street addressing to be in compliance with MCC.
6. All irrigation ditches, canals, laterals, or drains, exclusive of natural waterways, intersecting, crossing or laying adjacent and contiguous to the area being subdivided shall be addressed per UDC 11-3A-6. In performing such work, the applicant shall comply with Idaho Code 42-1207 and any other applicable law or regulation.
7. Any wells that will not continue to be used must be properly abandoned according to Idaho Well Construction Standards Rules administered by the Idaho Department of Water Resources. The Developer's Engineer shall provide a statement addressing whether there are any existing wells in the development, and if so, how they will continue to be used, or provide record of their abandonment.
8. Any existing septic systems within this project shall be removed from service per City Ordinance Section 9-1-4 and 9 4 8. Contact Central District Health for abandonment procedures and inspections (208)375-5211.
9. All improvements related to public life, safety and health shall be completed prior to occupancy of the structures.
10. Applicant shall be required to pay Public Works development plan review, and construction inspection fees, as determined during the plan review process, prior to the issuance of a plan approval letter.
11. It shall be the responsibility of the applicant to ensure that all development features comply with the Americans with Disabilities Act and the Fair Housing Act.
12. Applicant shall be responsible for application and compliance with any Section 404 Permitting that may be required by the Army Corps of Engineers.
13. Developer shall coordinate mailbox locations with the Meridian Post Office.
14. Compaction test results shall be submitted to the Meridian Building Department for all building pads receiving engineered backfill, where footing would sit atop fill material.
15. The design engineer shall be required to certify that the street centerline elevations are set a minimum of 3-feet above the highest established peak groundwater elevation. This is to ensure that the bottom elevation of the crawl spaces of homes is at least 1-foot above.
16. The applicants design engineer shall be responsible for inspection of all irrigation and/or drainage facility within this project that do not fall under the jurisdiction of an irrigation district or ACHD. The design engineer shall provide certification that the facilities have been installed in accordance with the approved design plans. This certification will be required before a certificate of occupancy is issued for any structures within the project.
17. At the completion of the project, the applicant shall be responsible to submit record drawings per the City of Meridian AutoCAD standards. These record drawings must be received and approved prior to the issuance of a certification of occupancy for any structures within the project.

18. A street light plan will need to be included in the civil construction plans. Street light plan requirements are listed in section 6-5 of the Improvement Standards for Street Lighting. A copy of the standards can be found at http://www.meridiancity.org/public_works.aspx?id=272.
19. The City of Meridian requires that the owner post to the City a performance surety in the amount of 125% of the total construction cost for all incomplete sewer, water and reuse infrastructure prior to final plat signature. This surety will be verified by a line item cost estimate provided by the owner to the City. The surety can be posted in the form of an irrevocable letter of credit, cash deposit or bond. Applicant must file an application for surety, which can be found on the Community Development Department website. Please contact Land Development Service for more information at 887-2211.
20. The City of Meridian requires that the owner post to the City a warranty surety in the amount of 20% of the total construction cost for all completed sewer, water and reuse infrastructure for duration of two years. This surety will be verified by a line item cost estimate provided by the owner to the City. The surety can be posted in the form of an irrevocable letter of credit, cash deposit or bond. Applicant must file an application for surety, which can be found on the Community Development Department website. Please contact Land Development Service for more information at 887-2211.

H. Meridian Fire Department

A full fire plan review shall be completed prior to construction.

I. Meridian Police Department

No comments received.

J. Meridian Park's Department



Reviewer: Kim Warren / Pathways Project Manager

Planner Assigned: Stacy Hersch

Project Name: Rolling Hills **File No:** H-2023-0050 – AZ, PP, **Date:** 5.24.2024

The following will be required for development of the proposed project:

CONDITIONS OF APPROVAL - PATHWAYS

1. The project developer shall design and construct multi-use pathways consistent with the location and specifications set forth in the Meridian Pathways Master Plan Map and Master Pathways Plan Document Chapter 3). Any proposed adjustments to pathway alignment shall be coordinated through the Pathways Project Manager. *Interactive Pathways Map*
2. Prior to final approval the applicant shall dedicate a public access easement for a 10' wide multi-use pathway parallel to and south of the Fivemile Creek (Drain) on the north side of the property. This pathway will also double as maintenance access to the Fivemile Drain for Nampa and Meridian Irrigation District.

Easements shall be a minimum of 14' wide (10' pathway + 2' shoulder each side). All pathways shall be located outside of irrigation district easements unless permission is specifically obtained from the governing irrigation district. Easement need only be dedicated for multi-use pathways that lie outside the public ROW.

*Use standard City template for public access easement. **Submit all easements online through Citizen's Access Portal.***
3. Construct multi-use pathways per paving section based on existing site conditions as recommended by project civil engineer in accord with UDC 11-3A-8 and 11-3B-12. **Prior to final approval the applicant's engineer shall provide written documentation that the pathway segment was constructed per the recommended specifications.**
4. The owner (or representative association) of the property affected by each public access easement shall have an ongoing obligation to maintain the multi-use pathway.
5. 6' high open vision fencing shall be installed between pathways and (live) water irrigation canals and laterals as detailed in the Meridian Pathways Master Plan, Chapter 3, page 3-5. All other fence details per UDC 11-3A-7. The Fivemile Pathway is NOT live water, so this condition does not apply.
6. Project developer shall be responsible for obtaining license agreement and other permission(s) as required for constructing within irrigation district easements.
7. Should any discrepancy exist between these conditions of approval and the requirements of the irrigation district, the developer shall work with Pathways Project Manager to achieve a pathway design that meets both City and irrigation district objectives. City requirements as stated shall not (nor are they intended to) override those of the irrigation district having jurisdiction over the project area.

Rev. 02.15.24

K. Community Planning Association of Southwest Idaho (COMPASS)

See public record

L. Irrigation Districts

1. Nampa & Meridian Irrigation District



ORGANIZED 1914
Nampa & Meridian Irrigation District

1503 FIRST STREET SOUTH
FAX #208-463-0092

NAMPA, IDAHO 83651-4395
nmid.org

OFFICE: Nampa 208-466-7861
SHOP: Nampa 208-466-0663

July 8, 2024

City Clerk's Office
City of Meridian
33 E. Broadway Avenue, Suite 102
Meridian, ID 83642-2619

RE: H-2023-0070/ Rolling Hill; 1560 1520 1480 S Rolling Hill & 4270 E Overland Rd

To Whom It May Concern:

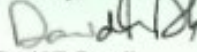
Nampa & Meridian Irrigation District (NMID) requires a filed Land Use Change Application to review prior to final platting.

All private laterals and waste ways must be protected. The Districts Five Mile Drain courses through the north boundary of this property. The Districts easement for the Five Mile Drain at this location is a minimum of forty feet (40') total, twenty feet (20') each side.

This easement must be protected. Any encroachment without a signed License Agreement and approved plan before construction is unacceptable.

All municipal surface drainage must be retained on site. If any municipal surface drainage leaves the site, NMID must review drainage plans. Developer must comply with Idaho Code 31-3805. Please feel free to contact me for further information.

Sincerely,


David T. Duvall
Asst. Water Superintendent
Nampa & Meridian Irrigation District
DTD /eol

Cc: Office/ file
S. Pardew
A. Wolfe
Applicant



APPROXIMATE IRRIGABLE ACRES
RIVER FLOW RIGHTS - 23,000
BOISE PROJECT RIGHTS - 40,000

M. Central District Health Department

See public record

N. Idaho Department of Environmental Quality (DEQ)

See public record

O. West Ada School District (WASD) or Other District/School

1. Kuna School District

See public record

2. West Ada School District

See public record

P. Ada County Highway District (ACHD)



Alexis Pickering, President
 Miranda Gold, Vice-President
 Jim Hansen, Commissioner
 Kent Goldthorpe, Commissioner
 Dave McKinney, Commissioner

Date: July 3, 2024

To: Jorre Delgado, via email

Staff Contact: KaraLeigh Troyer, Planner

Project Description: The applicant is requesting approval of an annexation and conditional use permit application for the development of a 154-unit multifamily development on 5.34-acres.

Trip Generation: This development is estimated to generate 897 vehicle trips per day and 71 vehicle trips per hour in the PM peak hour, based on the Institute of Transportation Engineers Trip Generation Manual, 11th edition.

Proposed Development Meets	
All ACHD Policies	
Requires Revisions to meet ACHD Policies	X

Traffic Impact Study	
Yes	
No	X
If yes, is mitigation required	

Area Roadway Level of Service	
Do area roadways meet ACHD's LOS Planning Thresholds?	
Yes	X
No	
Area roads will meet ACHD's LOS Planning Thresholds in the future with planned improvements?	
Yes	
No	

ACHD Planned Improvements	
IFYWP	X
CIP	X

Livable Street Performance Measures	
Pedestrian	LTS 1
Cyclist	LTS 2

Is Transit Available?	
Yes	X
No	

Comments:

connecting you to more

Ada County Highway District - 3775 Adams Street - Garden City, ID - 83714 - PH 208 387-6100 - FX 345-7650 - www.achdidaho.org



Project/File: **Rolling Hill / MER24-0058 / H-2023-0070**
This is an annexation and conditional use permit application for the development of an 154-unit multifamily development on 5.34-acres.

Lead Agency: City of Meridian

Site address: 1560 S Rolling Hill Drive

Staff Approval: July 3, 2024

Applicant: Jorre Delgado
JD Planning & Consulting
1109 W Main Street, Suite 700
Boise, ID 83702

Staff Contact: KaraLeigh Troyer, Planner
Phone: 387-6391
E-mail: ktroyer@achdidaho.org

Report Summary:

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ACHD Planned Improvements

1. Capital Improvements Plan (CIP)/ Integrated Five Year Work Plan (IFYWP):

- Cloverdale Road is scheduled in the IFYWP to be widened to 5-lanes from Victory Road to Overland Road in 2026.
- Rolling Hill Drive and Bridge #1218 is scheduled in the IFYWP to be replaced in 2027.
- Overland Road is listed in the CIP to be widened to 7-lanes from Cloverdale Road to Five Mile Road between 2036 and 2040.
- The intersection of Overland Road and Cloverdale Road is listed in the CIP to be widened to 7-lanes on the north leg, 7-lanes on the south, 8-lanes east, and 8-lanes on the west leg, and signalized between 2026 and 2030.
- The intersection of Overland Road and Five Mile Road is listed in the CIP to be widened to 7-lanes on the north leg, 7-lanes on the south, 8-lanes east, and 8-lanes on the west leg, and signalized between 2026 and 2030.

Level of Service Planning Thresholds

1. Condition of Area Roadways

Traffic Count is based on Vehicles per hour (VPH)

Roadway	Frontage	Functional Classification	PM Peak Hour Traffic Count	PM Peak Hour Level of Service
Overland Road	475-feet	Principal Arterial	1,028	Better than "E"
View Circle	323-feet	Collector	N/A	N/A
Rolling Hill Drive	464-feet	Local	N/A	N/A

* Acceptable level of service for a five-lane principal arterial is "E" (1,780 VPH).

* Acceptable level of service for a two-lane collector is "D" (425 VPH).

** ACHD does not set level of service thresholds for local roadways.

2. Average Daily Traffic Count (VDT)

Average daily traffic counts are based on ACHD's most current traffic counts.

- The average daily traffic count for Overland Road west of Cloverdale Road was 22,805 on October 21, 2020.
- There are no current average daily traffic counts for View Circle and Rolling Hill Drive.

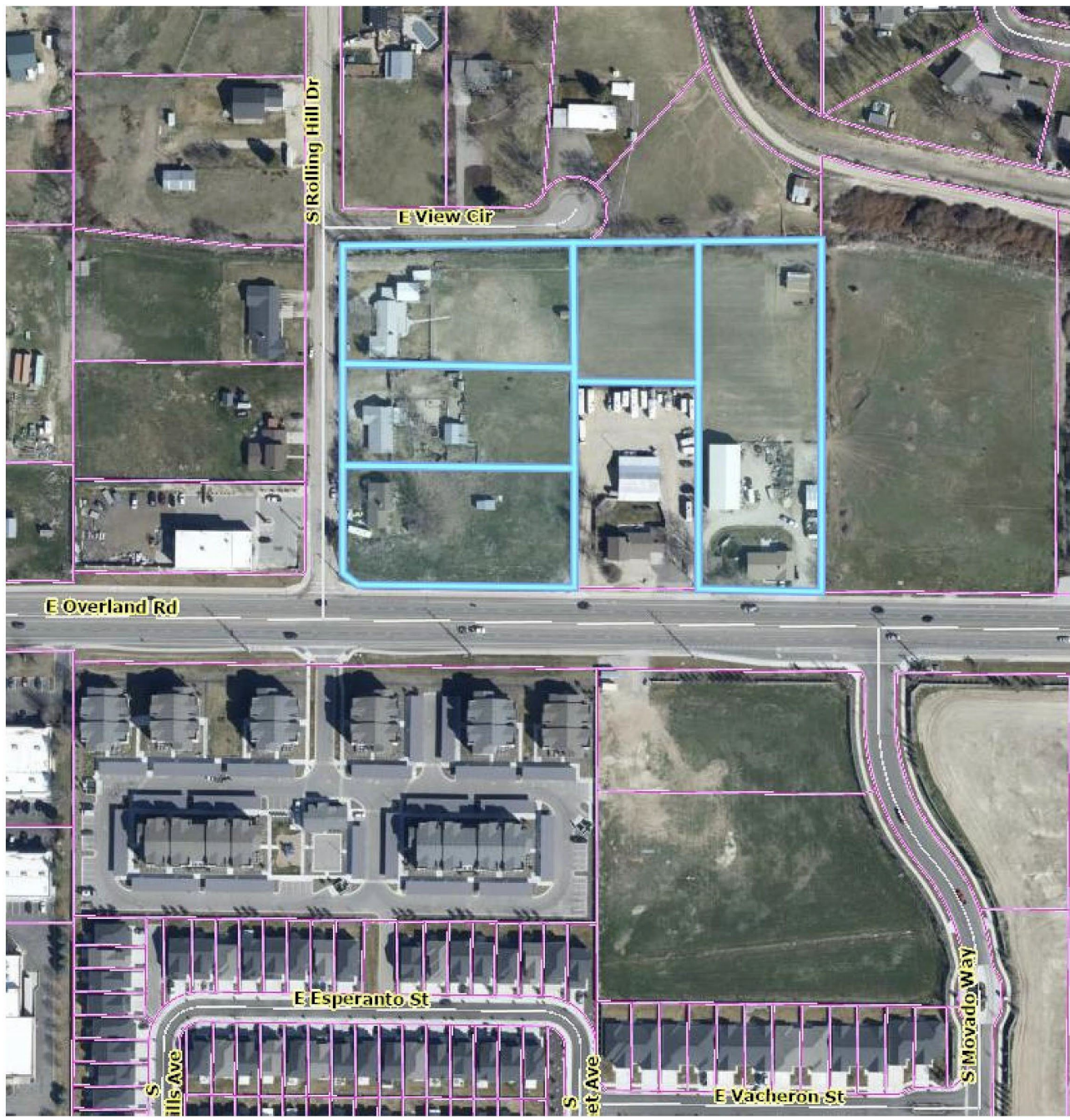
A. Site Specific Conditions of Approval

1. Repair or replace any damaged or deficient transportation facilities abutting the site on Overland Road consistent with the current version of PROWAG.
2. Construct an 8-foot wide planter strip and a 10-foot wide multi-use pathway abutting the site on Overland Road to tie into existing improvements on either side.
3. Dedicate right-of-way to total 62-feet from centerline on Overland Road.
4. Dedicate right-of-way to extend 2-feet behind back of sidewalks abutting the site.
5. Construct View Circle as $\frac{1}{2}$ of a 36-foot wide residential collector roadway with curb, gutter, and a minimum of 7-foot attached sidewalk or 5-foot wide detached sidewalk with an 8-foot wide planter strip.
6. Dedicate right-of-way abutting the site's northeast property line to accommodate the future construction of a planned collector roadway, as shown in site plan on page 8.
7. Construct Rolling Hill Drive as $\frac{1}{2}$ of a 36-foot wide local roadway with curb, gutter, and 5-foot wide concrete sidewalk abutting the site.
8. Install a sign at the terminus of View Circle stating, "THIS IS A DESIGNATED COLLECTOR ROADWAY. THIS STREET WILL BE EXTENDED AND WIDENED IN THE FUTURE."
9. Close 22-foot wide existing driveway abutting the site on Overland Road with an 8-foot wide planter's strip and a 10-foot wide multi-use pathway.
10. Coordinate with the owner of the parcel surrounded by the site to close the existing driveways on Overland Road and provide access to the neighboring parcel internally through the site.

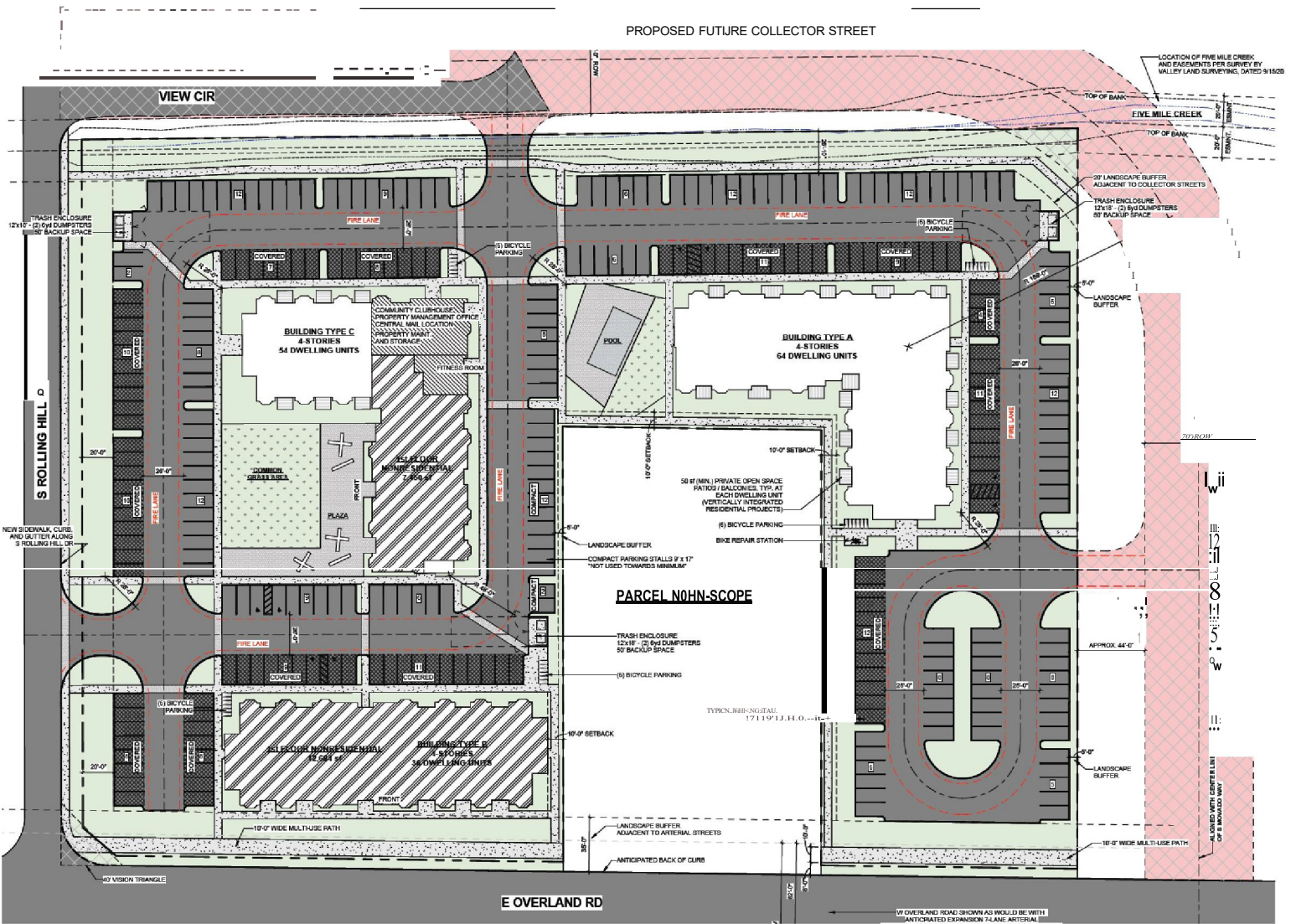
If the driveways cannot be wholly closed, the applicant shall close the portions of the shared-access driveways that abut the site on Overland Road with an 8-foot wide planter's strip and a 10-foot wide multi-use pathway.
11. Construct a 27-foot wide curb-return type driveway on View Circle located 302-feet east of Rolling Hill Drive.
12. Close the four existing unimproved driveways abutting the site on Rolling Hill Drive with curb, gutter, and 5-foot wide attached sidewalk.
13. Construct a 27-foot wide curb-return type driveway abutting the site on Rolling Hill Drive located 191-feet north of Overland Road.
14. Provide ACHD approved plans for the crossings of Fivemile Creek prior to the pre-construction meeting and final plat approval. Note: Timing of project plan submittals should take into account review times, lead time for precast members and potential roadway closures. To ensure construction prior to irrigation season, approval of the project plans must be attained by January 15th. The District retains the right to modify road closure approvals on any project based on the needs of the District. Construction of projects approved after January 15th may be postponed until after irrigation season is over in October. It is recommended that bridge submittals be submitted before the end of the current irrigation season to ensure the best time frame for construction is attained. Submittals will need to include the street section extending over the bridge to ensure the requirements of the roadway are met.
15. Other than the access specifically approved with this application, direct lot access is prohibited to Overland Road and View Circle.

- 16. Submit civil plans to ACHD Development Services for review and approval. The impact fee assessment will not be released until the civil plans are approved by ACHD.
- 17. There will be an impact fee that is assessed and due prior to issuance of any building permits. The assessed impact fee will be based on the impact fee ordinance that is in effect at that time. The impact fee assessment will not be released until the civil plans are approved by ACHD.
- 18. Comply with all Standard Conditions of Approval.

B. Vicinity Map



C. Site Plan



D. Findings for Consideration

1. Overland Road

- a. Existing Conditions:** Overland Road is improved with 5-travel lanes, vertical curb, gutter, and 7-foot wide sidewalk abutting the site. There is 110-feet of right-of-way for Overland Road (47-feet from centerline).
- b. Applicant Proposal:** The applicant is proposing to construct an 8-foot wide planter strip with a 10-foot wide multi-use pathway abutting the site on Overland Road.
- c. Staff Comments/Recommendations:** The applicant's proposal exceeds District policy and should be approved, as proposed. The applicant should be required to tie in the proposed sidewalk with the existing sidewalk on either side. The applicant should be required to dedicate right-of-way to total 62-feet from centerline on Overland Road.

The applicant should be required to repair or replace any damaged or deficient transportation facilities abutting the site on Overland Road consistent with the current version of PROWAG.

2. View Circle

- a. Existing Conditions:** View Circle is improved with 2-travel lanes and no curb, gutter or sidewalk abutting a portion of the site from Rolling Hill Drive running 350-feet east. View Circle is not constructed abutting the remainder of the site. View Circle terminates in a cul-de-sac turnaround. There is 50-feet of right-of-way for View Circle (24-feet from centerline).
- b. Applicant Proposal:** The applicant is not proposing any improvements to View Circle abutting the site.
- c. Staff Comments/Recommendations:** The applicant's proposal does not meet District policy and should not be approved, as proposed. The applicant should be required to construct View Circle as ½ of a 36-foot wide collector roadway with curb, gutter, and a minimum of 7-foot attached sidewalk or 5-foot wide detached sidewalk with an 8-foot wide planter strip from Rolling Hill Drive to the existing terminus of View Circle 350-feet east of Rolling Hill Drive.

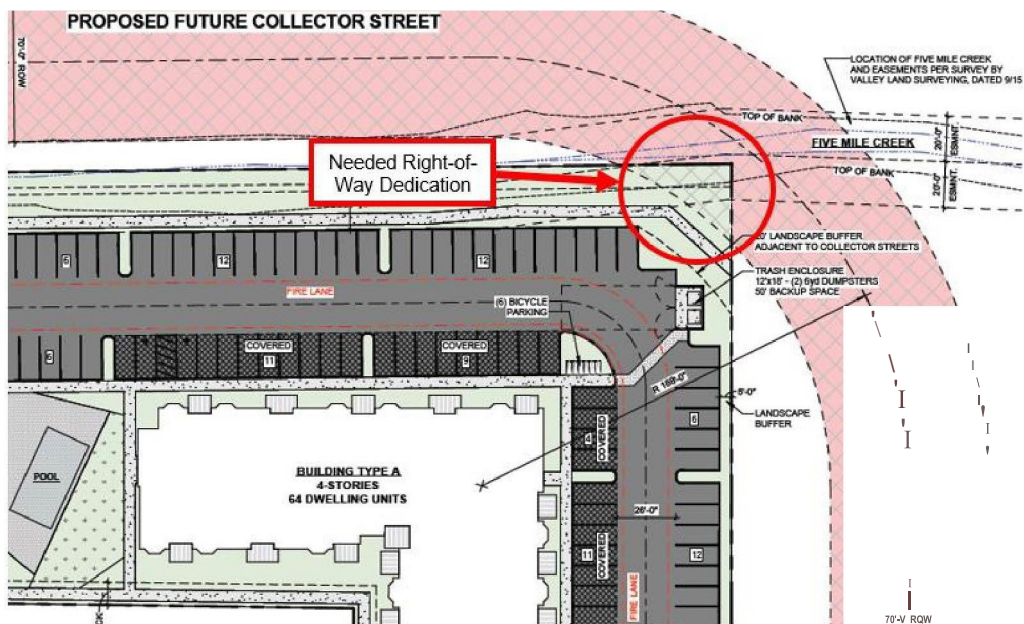
The applicant should be required to dedicate right-of-way to extend 2-feet behind back of sidewalk.

ACHD's MSM shows the extension of a collector roadway extending from Rolling Hills east to intersect Overland Road in alignment Movado Way.



*New collector shown in blue dashed line

The applicant has worked with the City of Meridian to determine an alignment of the new collector roadway, as shown below. To accommodate the future construction of the collector, the applicant should be required to dedicate right-of-way at the site's northeast property line, as shown on the site plan below.



3. Rolling Hill Drive

- a. **Existing Conditions:** Rolling Hill Drive is improved with 2-travel lanes and no curb, gutter or sidewalk abutting the site. There is 50-feet of right-of-way for Rolling Hill Drive (26-feet from centerline).
- b. **Applicant's Proposal:** The applicant is proposing to construct curb, gutter, and 5-foot wide attached sidewalk abutting the site on Rolling Hill Drive.
- c. **Staff Comments/Recommendations:** The applicant's proposal meets District policy and should be approved, as proposed. The applicant should be required to construct Rolling Hill Drive as ½ of a 36-foot wide local roadway with curb, gutter, and 5-foot wide concrete sidewalk abutting the site.

The applicant should be required to dedicate right-of-way to extend 2-feet behind back of sidewalk.

4. Stub Streets

- a. **Existing Conditions:** There are no existing stub streets to or from the site.
- b. **Applicant Proposal:** The applicant is not proposing any stub streets.
- c. **Staff Comments/Recommendations:** Because View Circle will be extended as a collector roadway in the future, the applicant should be required to install a sign at the terminus of the roadway stating, "THIS IS A DESIGNATED COLLECTOR ROADWAY. THIS STREET WILL BE EXTENDED AND WIDENED IN THE FUTURE."

5. Driveways

5.1 Overland Road

- a. **Existing Conditions:** There are 3 existing curb-cut type driveways abutting the site on Overland Road located as follows (measured centerline to centerline):
 - A 37-foot wide driveway located 117-feet west of Movado Way.
 - A 43-foot wide shared-access driveway located 247-feet west of Movado Way.
 - A 22-foot wide shared-access driveway located 332-feet east of Rolling Hill Drive.
- b. **Applicant's Proposal:** The applicant is proposing to close 22-foot wide existing driveway abutting the site on Overland Road with an 8-foot wide planter's strip and a 10-foot wide multi-use pathway.

The applicant is proposing to close the portions of the shared-access driveways that abut the site on Overland Road with an 8-foot wide planter's strip and a 10-foot wide multi-use pathway.

- c. **Staff Comments/Recommendations:** The applicant's proposal to close the 22-foot wide existing driveway meets District policy and should be approved, as proposed.

The applicant's proposal to partially close the existing shared-access driveways meets District policy. In accordance with District Cross Access Easements/Shared Access Policy 7202.4.1, staff recommends the applicant coordinate with the owner of the parcel surrounded by the site to close the existing driveways on Overland Road and provide access to the neighboring parcel internally through the site. The pathway should tie into the existing attached sidewalk on either side.

5.2 View Circle

- a. **Existing Conditions:** There are no existing driveway from the site onto View Circle.

- b. **Applicant's Proposal:** The applicant is proposing to construct a 27-foot wide curb-return type driveway on View Circle located 302-feet east of Rolling Hill Drive (measured centerline to centerline).
- c. **Staff Comments/Recommendations:** The applicant's proposal meets District policy and should be approved, as proposed.

5.3 Rolling Hill Drive

- a. **Existing Conditions:** There are 4 existing unimproved driveways abutting the site on Rolling Hill Drive located as follows (measured centerline to centerline):
 - A 16-foot wide driveway 50-feet south of View Circle.
 - A 19-foot wide driveway 109-feet south of View Circle.
 - A 35-foot wide driveway 244-feet south of View Circle.
 - A 21-foot wide driveway 140-feet north of Overland Road.
- b. **Applicant's Proposal:** The applicant is proposing to close the four existing unimproved driveways abutting the site on Rolling Hill Drive with curb, gutter, and 5-foot wide attached sidewalk.

The applicant is proposing to construct a 27-foot wide curb-return type driveway abutting the site on Rolling Hill Drive located 191-feet north of Overland Road.

- c. **Staff Comments/Recommendations:** The applicant's proposal meets District policy and should be approved, as proposed.

6. Bridge for Fivemile Creek Crossing

The District will require that the applicant submit the bridge plans for the crossing of the Fivemile Creek for review and approval prior to the pre-construction meeting and final plat approval. Note: Timing of project plan submittals should take into account review times, lead time for precast members and potential roadway closures. To ensure construction prior to irrigation season, approval of the project plans must be attained by January 15th. The District retains the right to modify road closure approvals on any project based on the needs of the District. Construction of projects approved after January 15th may be postponed until after irrigation season is over in October. It is recommended that bridge submittals be submitted before the end of the current irrigation season to ensure the best time frame for construction is attained. Submittals will need to include the street section extending over the bridge to ensure the requirements of the roadway are met.

7. Other Access

Overland Road is classified as a principal arterial roadway; View Circle is classified as a collector roadway. Other than the access specifically approved with this application, direct lot access is prohibited to these roadways.

E. Policy

1. Federal Accessibility Design Guidelines and Standards

District policy 7203.1.1 states that developers shall follow the current version of the U.S. Access Board's Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way (PROWAG), 36 CFR Part 1190, September 7, 2023; (Also see, <https://www.access-board.gov/prowag> and <https://www.access-board.gov/files/prowag/planning-and-design-for-alternatives.pdf> for additional information).

2. Minor Improvements

Minor Improvements Policy: District Policy 7203.3 states that minor improvements to existing Highways adjacent to a proposed development may be required. These needed transportation facilities are to correct deficiencies or replace deteriorated facilities. Included are sidewalk and/or bike lane construction or replacement (with physical buffers if missing and needed); construction of transitional sidewalk segments; crosswalk construction or replacement; curb and gutter construction or replacement; repair, replacement or expansion of curb extensions; replacement of unused driveways with curb, gutter, sidewalk, repair or addition of traffic calming or speed mitigation features; installation or reconstruction of pedestrian ramps; pavement repairs; signs, motor vehicle, pedestrian and bicycle traffic control devices; and other similar items. The current version of PROWAG will determine the applicable accessibility requirements for alterations and elements added to existing streets. ACHD staff is responsible for identifying the minor improvements that would be proportionate to the size and complexity of the development.

3. Livable Street Performance Measures

District policy 7203.8 states that ACHD has adopted performance measures (level of stress) for evaluating the experience of bicyclists and pedestrians. ACHD seeks to create a transportation network that serves all ages and abilities. Bike and pedestrian facilities built through development should achieve a bike and pedestrian level of traffic stress 1 or 2, with no new bike lane below a minimum of 5-feet.

4. Overland Road

Arterial Roadway Policy: District Policy 7205.2.1 states that the developer is responsible for improving all street frontages adjacent to the site regardless of whether or not access is taken to all of the adjacent streets. Improvements shall include transitional segments in accordance with the current version of PROWAG.

Master Street Map and Typology Policy: District Policy 7205.5 states that the design of improvements for arterials shall be in accordance with District standards, including the Master Street Map, Livable Streets Design Guide, and the most current ACHD Livable Streets Performance Measures Plan as adopted by the ACHD Commission. The developer or engineer should contact the District before starting any design.

Street Section and Right-of-Way Width Policy: District Policies 7205.2.1 & 7205.5.2 state that the standard 7-lane street section shall be 81-feet (back-of-curb to back-of-curb). This width typically accommodates three travel lanes in each direction, a continuous raised or landscaped median with intermittent turn lanes, and curbs and gutters. A 7-lane road shall also include a minimum 10-foot wide multi-use path outside the curb line on both sides with an 8-foot wide buffer from back-of-curb. Other Level 3 bike facility treatments as defined in the ACHD Bike Master Plan may be approved at the discretion of the ACHD Development Review Supervisor. The standard right-of-way width for a 7-lane arterial is 124-feet.

Right-of-Way Dedication: District Policy 7205.2 states that The District will provide compensation for additional right-of-way dedicated beyond the existing right-of-way along arterials listed as impact fee eligible in the adopted Capital Improvements Plan using available impact fee revenue in the Impact Fee Service Area.

No compensation will be provided for right-of-way on an arterial that is not listed as impact fee eligible in the Capital Improvements Plan.

The District may acquire additional right-of-way beyond the site-related needs to preserve a corridor for future capacity improvements, as provided in Section 7300.

Pedestrian Facilities: District Policy 7205.5.7 requires a concrete sidewalk at least 5-feet wide are required on both sides of all arterial streets. A parkway strip at least 8-feet wide between the back-of-curb and street edge of the sidewalk is required to provide increased safety and protection of pedestrians. Alternatively, on roadways identified for improvement in the Capital Improvement Plan, a minimum 10-foot wide multi-use path may be required. The path shall be placed in accordance with planned buildout in the Master Street Map with a minimum 8-foot wide planter strip as measured to the closest edge of the path. Street trees are encouraged between the pedestrian facility and the roadway when irrigation and maintenance will occur by the adjacent property owner or HOA through an approved license agreement. Consult the District's planter width policy if trees are to be placed within the parkway strip. In some instances, to match existing conditions, a minimum 7-foot wide sidewalk may be constructed next to the back-of-curb. ACHD Development Review staff will be responsible for determining the required facility. Vertical hardscape alternatives to street trees may be considered in the buffer space when street trees are not practicable.

Detached sidewalks and multi-use paths are encouraged and should be parallel to the adjacent roadway. Pedestrian facilities will only be allowed to deviate from a straight line parallel to the roadway when authorized by Development Review staff to accommodate site specific conditions (i.e., street trees, utilities, etc.).

Appropriate easements shall be provided if public pedestrian facilities are placed out of the right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the pedestrian facility. Pedestrian facilities shall either be located wholly within the public right-of-way or wholly within an easement.

Curb ramps or blended transitions shall be provided to connect the pedestrian access route at each pedestrian street crossing in accordance with the current version of PROWAG. Provide detectable warning surface in accordance with the current version of PROWAG.

Frontage Improvements Policy: District Policy 7205.2.1 states that the developer shall widen the pavement to a minimum of 17-feet from centerline plus a 3-foot wide gravel shoulder adjacent to the entire site. Curb, gutter and additional pavement widening may be required (See Section 7205.5.5).

ACHD Master Street Map: ACHD Policy Section 3111.1 requires the Master Street Map (MSM) guide the right-of-way acquisition, arterial street requirements, and specific roadway features required through development. This segment of Overland Road is designated in the MSM as a Mobility Arterial with 4-lanes and on-street bike lanes, a 99-foot street section within 124-feet of right-of-way.

5. View Circle

Collector Street Policy: District policy 7206.2.1 states that the developer is responsible for improving all collector frontages adjacent to the site or internal to the development as required below, regardless of whether access is taken to all of the adjacent streets. Improvements shall include transitional segments in accordance with ADA and the current version of PROWAG.

Master Street Map and Typologies Policy: District policy 7206.5 states that if the collector street is designated with a typology on the Master Street Map, that typology shall be considered for the required street improvements. If there is no typology listed in the Master Street Map, then standard street sections shall serve as the default. ACHD reserves the right to require bicycle and pedestrian facilities above those identified in the Livable Streets Design Guide and Master Street Map to ensure the safest facility possible based on current best practice.

Street Section and Right-of-Way Policy: District policy 7206.5.2 states that the standard right-of-way width for collector streets shall typically be 50 to 70-feet, depending on the location and width of the sidewalk or multi-use path and the location and use of the roadway. The right-of-way width may be reduced, with District approval, if the sidewalk or multi-use path is located within an easement; in which case the District will require a minimum right-of-way width that extends 2-feet behind the back-of-curb on each side.

The standard street section shall be 47-feet (back-of-curb to back-of-curb). This width typically accommodates a single travel lane in each direction, a continuous center left-turn lane, and bike lanes. If a multi-use path is determined to be the required treatment, the street section shall be reduced to 37-feet. If it is determined a continuous center left-turn lane is not appropriate and a path is installed, the street section may be reduced to 26-feet.

Residential Collector Policy: District policy 7206.5.2 states that the standard street section for a collector in a residential area shall be 36-feet (back-of-curb to back-of-curb). The District will consider a 33-foot or 29-foot street section with written fire department approval and taking into consideration the needs of the adjacent land use, the projected volumes, the need for bicycle lanes, and on-street parking.

At the discretion of Traffic Engineering staff, adjustments may be made to the street section, including removal of the continuous center turn lanes or modification to lane widths, where no driveways or intersections are present or to ensure adequate space for pathways and buffers.

Half Street Policy: District Policy 7206.2.2 required improvements shall consist of pavement widening to one-half the required width, including curb, gutter and concrete sidewalk (minimum 5-feet), plus 12-feet of additional pavement widening beyond the centerline established for the street to provide an adequate roadway surface, with the pavement crowned at the ultimate centerline. A 3-foot wide gravel shoulder and a borrow ditch sized to accommodate the roadway storm runoff shall be constructed on the unimproved side. Alternatively, on roadways identified as low-stress bikeways in ACHD's Bike Master Plan, a minimum 10-foot wide multi-use path may be required in lieu of sidewalks. ACHD Development Review staff will be responsible for determining the required facility. The path shall be built 8-feet behind the top back of curb as measured to the closest edge of the path. Street trees are encouraged between the pedestrian facility and the roadway when irrigation and maintenance will occur by the adjacent property owner or HOA through an approved license agreement. Vertical hardscape alternatives to street trees may be considered in the buffer space when street trees are not practicable.

Pedestrian Facilities: District policy 7206.5.6 requires a concrete sidewalk at least 5-feet wide to be constructed on both sides of all collector streets. A parkway strip at least 8-feet wide between the back-of-curb and street edge of the sidewalk is required to provide increased safety and protection of pedestrians. Consult the District's planter width policy if trees are to be placed within the parkway strip. Sidewalk constructed next to the back-of-curb shall be a minimum of 5-feet wide. Alternatively, on roadways identified as low-stress bikeways in ACHD's Bike Master Plan, a minimum 10-foot wide multi-use path may be required in lieu of sidewalks. ACHD Development Review staff will be responsible for determining the required facility. The path shall be built 8-feet behind the back-of-curb as measured to the closest edge of the path. Street trees are encouraged between the pedestrian facility and the roadway when irrigation and maintenance will occur by the adjacent property owner or HOA through an approved license agreement. Vertical hardscape alternatives to street trees may be considered in the buffer space when street trees are not practicable.

Pedestrian facilities should be parallel to the adjacent roadway. Pedestrian facilities will only be allowed to deviate from a straight line when authorized by Development Review staff to meet site specific conditions (i.e., street trees, utilities, etc.).

Appropriate easements shall be provided if public sidewalks or multi-use paths are placed out of the right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk or multi-use path. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

Curb ramps or blended transitions shall be provided to connect the pedestrian access route at each pedestrian street crossing in accordance with the current version of PROWAG. Provide detectable warning surface in accordance with the current version of PROWAG.

ACHD Master Street Map: ACHD Policy Section 3111.1 requires the Master Street Map (MSM) guide the right-of-way acquisition, collector street requirements, and specific roadway features required through development. This segment of View Circle is designated in the MSM as a Residential Collector with 2-lanes and on-street bike lanes, a 36-foot street section within 50-70 feet of right-of-way.

6. Rolling Hill Drive

Commercial Roadway Policy: District Policy 7208.2.1 states that the developer is responsible for improving all commercial street frontages adjacent to the site regardless of whether or not access is taken to all of the adjacent streets.

Street Section and Right-of-Way Policy: District Policy 7208.5 states that right-of-way widths for new commercial streets shall typically be 50 and 70-feet wide and that the standard street section will vary depending on the need for a center turn lane, bike lanes, volumes, percentage of truck traffic, and/or on-street parking.

- A 36-foot street section (back-of-curb to back-of-curb) will typically accommodate two travel lanes and on-street parking.
- A 40-foot street section (back-of-curb to back-of-curb) will typically accommodate two travel lanes and a center turn lane.
- A 46-foot street section (back-of-curb to back-of-curb) will typically accommodate two travel lanes and a center turn lane and bike lanes.

Sidewalk Policy: District Policy 7208.5.6 requires a concrete sidewalk at least 5-feet wide to be constructed on both sides of all commercial streets. If a separated sidewalk is proposed, a parkway strip at least 6-feet wide between the back-of-curb and street edge of the sidewalk is required to provide increased safety and protection of pedestrians. Consult the District's planter width policy if trees are to be placed within the parkway strip.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

Half Street Policy: District Policy 7208.2.2 required improvements shall consist of pavement widening to one-half the required width, including curb, gutter and concrete sidewalk (minimum 5-foot), plus 12-feet of additional pavement widening beyond the centerline established for the street to provide an adequate roadway surface, with the pavement crowned at the ultimate centerline. A 3-foot wide gravel shoulder and a borrow ditch sized to accommodate the roadway storm runoff shall be constructed on the unimproved side.

7. Stub Streets

Stub Street Policy: District policy 7206.2.4.3 (collector) states that stub streets will be required to provide circulation or to provide access to adjoining properties. Stub streets will conform with the requirements described in Section 7206.2.4 (collector), except a temporary cul-de-sac will not be required if the stub street has a length no greater than 150-feet. A sign shall be installed at the

terminus of the stub street stating that "THIS IS A DESIGNATED COLLECTOR ROADWAY. THIS STREET WILL BE EXTENDED AND WIDENED IN THE FUTURE."

In addition, stub streets must meet the following conditions:

- A stub street shall be designed to slope towards the nearest street intersection within the proposed development and drain surface water towards that intersection; unless an alternative storm drain system is approved by the District.
- The District may require appropriate covenants guaranteeing that the stub street will remain free of obstructions.

Temporary Dead End Streets Policy: District policy 7206.2.4.4 (collector) requires that the design and construction for cul-de-sac streets shall apply to temporary dead end streets. The temporary cul-de-sac shall be paved and shall be the dimensional requirements of a standard cul-de-sac. The developer shall grant a temporary turnaround easement to the District for those portions of the cul-de-sac which extend beyond the dedicated street right-of-way. In the instance where a temporary easement extends onto a buildable lot, the entire lot shall be encumbered by the easement and identified on the plat as a non-buildable lot until the street is extended.

8. Driveways

8.1 Overland Road

Access Points Policy: District Policy 7205.4.1 states that all access points associated with development applications shall be determined in accordance with the policies in this section and Section 7202. Access points shall be reviewed only for a development application that is being considered by the lead land use agency. Approved access points may be relocated and/or restricted in the future if the land use intensifies, changes, or the property redevelops.

Access Policy: District policy 7205.4.7 states that direct access to principal arterials is typically prohibited. If a property has frontage on more than one street, access shall be taken from the street having the lesser functional classification. If it is necessary to take access to the higher classified street due to a lack of frontage, the minimum allowable spacing shall be based on Table 1b under District policy 7205.4.7, unless a waiver for the access point has been approved by the District Commission. Driveways, when approved on a principal arterial shall operate as a right-in/right-out only, and the District will require the construction of a raised median to restrict the left turning movements.

Driveway Location Policy: District policy 7205.4.7 requires driveways located on principal arterial roadways to be located a minimum of 355-feet from the nearest intersection for a right-in/right-out only driveway. Full-access driveways are not allowed on principal arterial roadways.

Successive Driveways: District policy 7205.4.7 Table 1b, requires driveways located on principal arterial roadways with a speed limit of 40 MPH to align or offset a minimum of 400-feet from any existing or proposed driveway.

Driveway Width Policy: District policy 7205.4.8 restricts high-volume driveways (100 VTD or more) to a maximum width of 36-feet and low-volume driveways (less than 100 VTD) to a maximum width of 30-feet. Curb return type driveways with 30-foot radii will be required for high-volume driveways with 100 VTD or more. Curb return type driveways with 15-foot radii will be required for low-volume driveways with less than 100 VTD.

Driveway Paving Policy: Graveled driveways abutting public streets create maintenance problems due to gravel being tracked onto the roadway. In accordance with District policy, 7205.4.8, the applicant should be required to pave the driveway its full width and at least 30-feet into the site beyond the edge of pavement of the roadway and install pavement tapers in accordance with Table 2 under District Policy 7205.4.8.

Temporary Access Policy: District Policy 7202.4.2 identifies a temporary access as that which "is permitted for use until appropriate alternative access becomes available". Temporary access may be granted through a development agreement or similar method, and the developer shall be responsible for providing a financial guarantee for the future closure of the driveway.

Cross Access Easements/Shared Access Policy: District Policy 7202.4.1 states that cross access utilizes a single vehicular connection that serves two or more adjoining lots or parcels so that the driver does not need to re-enter the public street system.

8.2 View Circle

Access Policy: District Policy 7205.4.1 states that all access points associated with development applications shall be determined in accordance with the policies in this section and Section 7202. Access points shall be reviewed only for a development application that is being considered by the lead land use agency. Approved access points may be relocated and/or restricted in the future if the land use intensifies, changes, or the property redevelops.

District Policy 7206.1 states that the primary function of a collector is to intercept traffic from the local street system and carry that traffic to the nearest arterial. A secondary function is to service adjacent property. Access will be limited or controlled. Collectors may also be designated at bicycle and bus routes.

Driveway Location Policy (Stop Controlled Intersection): District policy 7206.4.4 requires driveways located on collector roadways near a STOP controlled intersection to be located outside of the area of influence; OR a minimum of 150-feet from the intersection, whichever is greater. Dimensions shall be measured from the centerline of the intersection to the centerline of the driveway.

Successive Driveways: District policy 7206.4.5 Table 1 requires driveways located on collector roadways with a speed limit of 25 MPH and daily traffic volumes greater than 100 VTD to align or offset a minimum of 245-feet from any existing or proposed driveway.

Driveway Width Policy: District policy 7206.4.6 restricts high-volume driveways (100 VTD or more) to a maximum width of 36-feet and low-volume driveways (less than 100 VTD) to a maximum width of 30-feet. Curb return type driveways with 30-foot radii will be required for high-volume driveways with 100 VTD or more. Curb return type driveways with 15-foot radii will be required for low-volume driveways with less than 100 VTD.

Driveway Paving Policy: Graveled driveways abutting public streets create maintenance problems due to gravel being tracked onto the roadway. In accordance with District policy, 7206.4.6, the applicant should be required to pave the driveway its full width and at least 30-feet into the site beyond the edge of pavement of the roadway and install pavement tapers in accordance with Table 2 under District Policy 7206.4.6.

8.3 Rolling Hill Drive

Driveway Location Policy: District policy 7208.4.1 requires driveways near intersections to be located a minimum of 75-feet (measured centerline-to-centerline) from the nearest local street intersection, and 150-feet from the nearest collector/arterial or arterial street intersection.

Successive Driveways: District Policy 7208.4.1 states that successive driveways away from an intersection shall have no minimum spacing requirements for access points along a local street, but the District does encourage shared access points where appropriate.

Driveway Width Policy: District policy 7208.4.3 restricts commercial driveways to a maximum width of 40-feet. Most commercial driveways will be constructed as curb-cut type facilities.

Driveway Paving Policy: Graveled driveways abutting public streets create maintenance problems due to gravel being tracked onto the roadway. In accordance with District policy,

7208.4.3, the applicant should be required to pave the driveway its full width and at least 30-feet into the site beyond the edge of pavement of the roadway.

Driveway Design Requirements: District policy 7208.4.3 states if an access point is to be gated, the gate or keypad (whichever is closer) shall be located a minimum of 50-feet from the near edge of the intersection and a turnaround shall be provided.

9. Traffic Calming

Speed Control and Traffic Calming Policy (Collectors): District policy 7206.3.8 states that collector streets should be designed to discourage speeds above 35 MPH and in a residential area, collector streets should be designed to discourage speeds above 30 MPH. The design of collector street systems should discourage excessive speeds by using passive design elements. If the design or layout of a development is anticipated to necessitate future traffic calming implementation by the District, then the District will require changes to the layout and/or the addition of passive design elements such as horizontal curves, bulb-outs, chokers, etc. The District will also consider texture changes to the roadway surface (i.e. stamped concrete) as a passive design element. These alternative methods may require a maintenance and/or license agreement.

10. Tree Planters

Tree Planter Policy: Tree Planter Policy: The District's Tree Planter Policy prohibits all trees in planters less than 8-feet in width without the installation of root barriers. Class II trees may be allowed in planters with a minimum width of 8-feet, and Class I and Class III trees may be allowed in planters with a minimum width of 10-feet.

11. Landscaping

Landscaping Policy: A license agreement is required for all landscaping proposed within ACHD right-of-way or easement areas. Trees shall be located no closer than 10-feet from all public storm drain facilities. Landscaping should be designed to eliminate site obstructions in the vision triangle at intersections. District Policy 5104.3.1 requires a 40-foot vision triangle and a 3-foot height restriction on all landscaping located at an uncontrolled intersection and a 50-foot offset from stop signs. Landscape plans are required with the submittal of civil plans and must meet all District requirements prior to signature of the final plat and/or approval of the civil plans.

12. Pathway Crossings: United States Access Board R304.5.1.2 Shared Use Paths. In shared use paths, the width of curb ramps runs and blended transitions shall be equal to the width of the shared use path.

AASHTO's Guidelines for the Development of Bicycle Facilities 5.3.5 Other Intersection Treatments: The opening of a shared use path at the roadway should be at least the same width as the shared use path itself. If a curb ramp is provided, the ramp should be the full width of the path, not including any flared sides if utilized. Detectable warnings should be placed across the full width of the ramp.

FHWA's "Designing Sidewalks and Trails for Access" (1999) reflected common ADA-related concepts: Chapter 6, Page 16-6: The width of the ramp should be at least as wide as the average width of the trail to improve safety for users who will be traveling at various speeds. In addition, the overall width of the trail should be increased, so the curb ramp can be slightly offset to the side. The increased width reduces conflict at the intersection by providing more space for users at the bottom of the ramp.

F. Standard Conditions of Approval

1. All proposed irrigation facilities shall be located outside of the ACHD right-of-way (including all easements). Any existing irrigation facilities shall be relocated outside of the ACHD right-of-way (including all easements).

2. Private Utilities including sewer or water systems are prohibited from being located within the ACHD right-of-way.
3. In accordance with District policy, 7203.3, the applicant may be required to update any existing non-compliant pedestrian improvements abutting the site to meet current Americans with Disabilities Act (ADA), Public Right-of-Way Accessibility Guidelines (PROWAG), ISPWC, or ACHD requirements . The applicant's engineer should provide documentation of compliance to District Development Review staff for review.
4. Replace any existing damaged curb, gutter and sidewalk and any that may be damaged during the construction of the proposed development. Contact Construction Services at 387-6280 (with file number) for details.
5. A license agreement and compliance with the District's Tree Planter policy is required for all landscaping proposed within ACHD right-of-way or easement areas.
6. All utility relocation costs associated with improving street frontages abutting the site shall be borne by the developer.
7. It is the responsibility of the applicant to verify all existing utilities within the right-of-way. The applicant at no cost to ACHD shall repair existing utilities damaged by the applicant. The applicant shall be required to call DIGLINE (1-811-342-1585) at least two full business days prior to breaking ground within ACHD right-of-way. The applicant shall contact ACHD Traffic Operations 387-6190 in the event any ACHD conduits (spare or filled) are compromised during any phase of construction.
8. Utility street cuts in pavement less than five years old are not allowed unless approved in writing by the District. Contact the District's Utility Coordinator at 387-6258 (with file numbers) for details.
9. All design and construction shall be in accordance with the ACHD Policy Manual, ISPWC Standards and approved supplements, Construction Services procedures and all applicable ACHD Standards unless specifically waived herein. An engineer registered in the State of Idaho shall prepare and certify all improvement plans.
10. Construction, use and property development shall be in conformance with all applicable requirements of ACHD prior to District approval for occupancy.
11. No change in the terms and conditions of this approval shall be valid unless they are in writing and signed by the applicant or the applicant's authorized representative and an authorized representative of ACHD. The burden shall be upon the applicant to obtain written confirmation of any change from ACHD.
12. If the site plan or use should change in the future, ACHD Planning Review will review the site plan and may require additional improvements to the transportation system at that time. Any change in the planned use of the property, which is the subject of this application, shall require the applicant to comply with ACHD Policy and Standard Conditions of Approval in place at that time unless a waiver/variance of the requirements or other legal relief is granted by the ACHD Commission.

G. Conclusions of Law

1. The proposed site plan is approved, if all of the Site Specific and Standard Conditions of Approval are satisfied.
2. ACHD requirements are intended to assure that the proposed use/development will not place an undue burden on the existing vehicular transportation system within the vicinity impacted by the proposed development.

Request for Appeal of Staff Decision

To request an appeal of a staff level decision, see District policy 7101.6.7 at <https://www.achdidaho.org/home/showpublisheddocument/452/638243231708370000>

Request for Reconsideration of Commission Action

To request reconsideration of a Commission Action, see District policy 1006.11 at <https://www.achdidaho.org/home/showpublisheddocument/452/638243231708370000>

Q. Idaho Transportation Department (ITD)

See public record

V. FINDINGS

R. Annexation and/or Rezone (UDC 11-5B-3E)

Upon recommendation from the commission, the council shall make a full investigation and shall, at the public hearing, review the application. In order to grant an annexation and/or rezone, the council shall make the following findings:

1. The map amendment complies with the applicable provisions of the comprehensive plan;

Staff finds the Applicant's request to annex the subject property with C-C zoning and develop with a multi-family development and a vertically integrated residential project on the site at the densities proposed is generally consistent with the Comprehensive Plan per the analysis in Section III.

2. The map amendment complies with the regulations outlined for the proposed district, specifically the purpose statement;

Staff finds the proposed map amendment to the C-C zoning district complies with the purpose statement of the commercial districts in that it will provide for the retail and service needs of the multi-family development and community in accord with the Comprehensive Plan.

3. The map amendment shall not be materially detrimental to the public health, safety, and welfare;

Staff finds the proposed map amendment should not be detrimental to the public health, safety and welfare as the proposed uses should be compatible with adjacent uses in the area.

4. The map amendment shall not result in an adverse impact upon the delivery of services by any political subdivision providing public services within the city including, but not limited to, school districts; and

Staff finds City services are available to be provided to this development. Comments were received from WASD on this application that are included above in Section VIII.L.

5. The annexation (as applicable) is in the best interest of city.

Staff finds the proposed annexation is in the best interest of the city if the Applicant complies with the recommended conditions.

S. Conditional Use (UDC 11-5B-6E)

The commission shall base its determination on the conditional use permit request upon the following:

1. That the site is large enough to accommodate the proposed use and meet all the dimensional and development regulations in the district in which the use is located.

Staff finds that the subject property is large enough to accommodate the proposed use and dimensional and development regulations of the C-C zoning districts (see Analysis, Section III. for more information).

2. That the proposed use will be harmonious with the Meridian comprehensive plan and in accord with the requirements of this title.

Staff finds that the proposed use is consistent with the future land use map designations of MU-R and is allowed as a conditional use in UDC Table 11-2B-2 in the C-C zoning districts.

3. That the design, construction, operation and maintenance will be compatible with other uses in the general neighborhood and with the existing or intended character of the general vicinity and that such use will not adversely change the essential character of the same area.

Staff finds the proposed design of the development, construction, operation and maintenance should be compatible with the mix of other uses planned for this area and with the intended character of the area and that such uses will not adversely change the area's character.

4. That the proposed use, if it complies with all conditions of the approval imposed, will not adversely affect other property in the vicinity.

Staff finds that if the applicant complies with the conditions outlined in this report, the proposed use will not adversely affect other properties in the area. The Commission and Council should weigh any public testimony provided to determine if the development will adversely affect other properties in the vicinity.

5. That the proposed use will be served adequately by essential public facilities and services such as highways, streets, schools, parks, police and fire protection, drainage structures, refuse disposal, water, and sewer.

Staff finds that essential public services are available to this property and that the use should be adequately served by these facilities.

6. That the proposed use will not create excessive additional costs for public facilities and services and will not be detrimental to the economic welfare of the community.

All public facilities and services are readily available for the subject site so staff finds that the proposed use will not be detrimental to the economic welfare of the community or create excessive additional costs for public facilities and services.

7. That the proposed use will not involve activities or processes, materials, equipment and conditions of operation that will be detrimental to any persons, property or the general welfare by reason of excessive production of traffic, noise, smoke, fumes, glare or odors.

Staff finds that although traffic is sure to increase in the vicinity with the addition of more residential units, the addition of the multifamily in this will not be detrimental to any persons, property or the general welfare by reason of excessive production of traffic, noise, smoke, fumes, glare or odors.

8. That the proposed use will not result in the destruction, loss or damage of a natural, scenic or historic feature considered to be of major importance.

Staff is unaware of any natural, scenic, or historic features within the development area, therefore, staff finds the proposed use should not result in damage to any such features.

VI. ACTION

A. Staff:

After reviewing the project for compliance with the city's comprehensive plan and unified development code, staff finds the project meets the intent and complies if the applicant adheres to the conditions outlined in Section IV per the Findings in Section V of the staff report

B. Commission:

Pending

C. City Council:

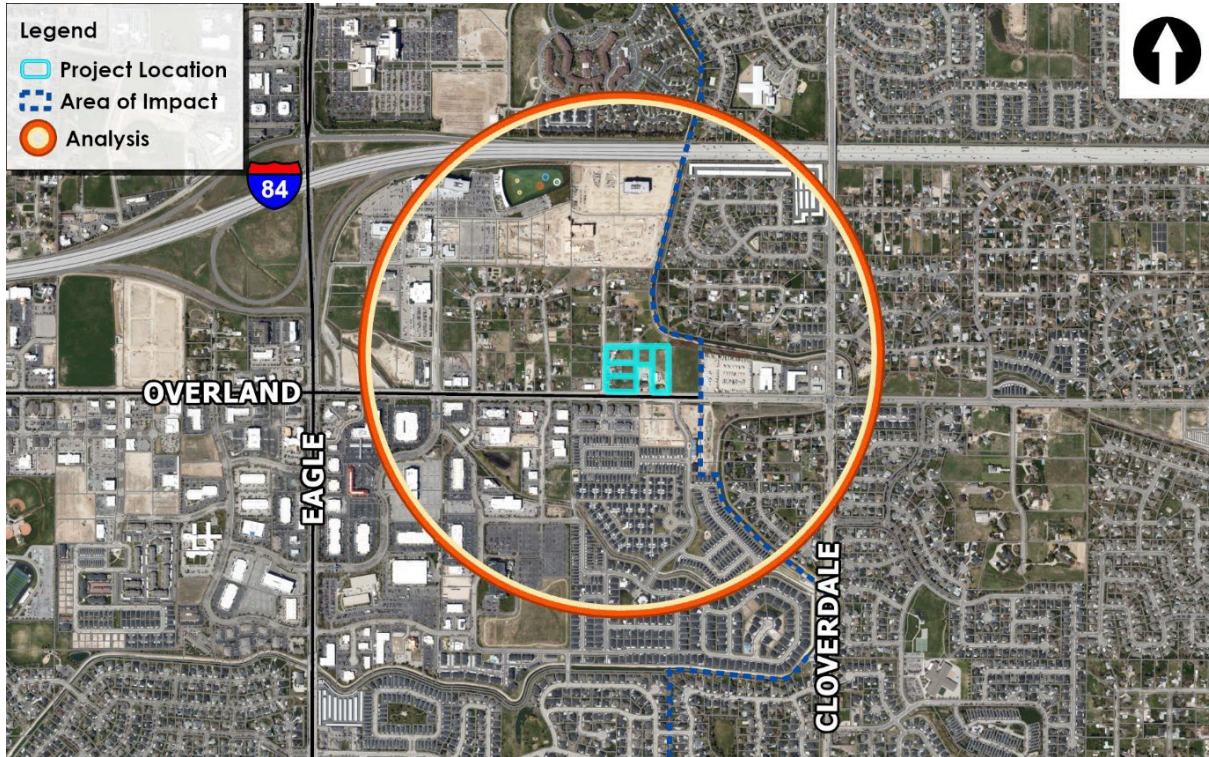
Action Pending.

VII. EXHIBITS

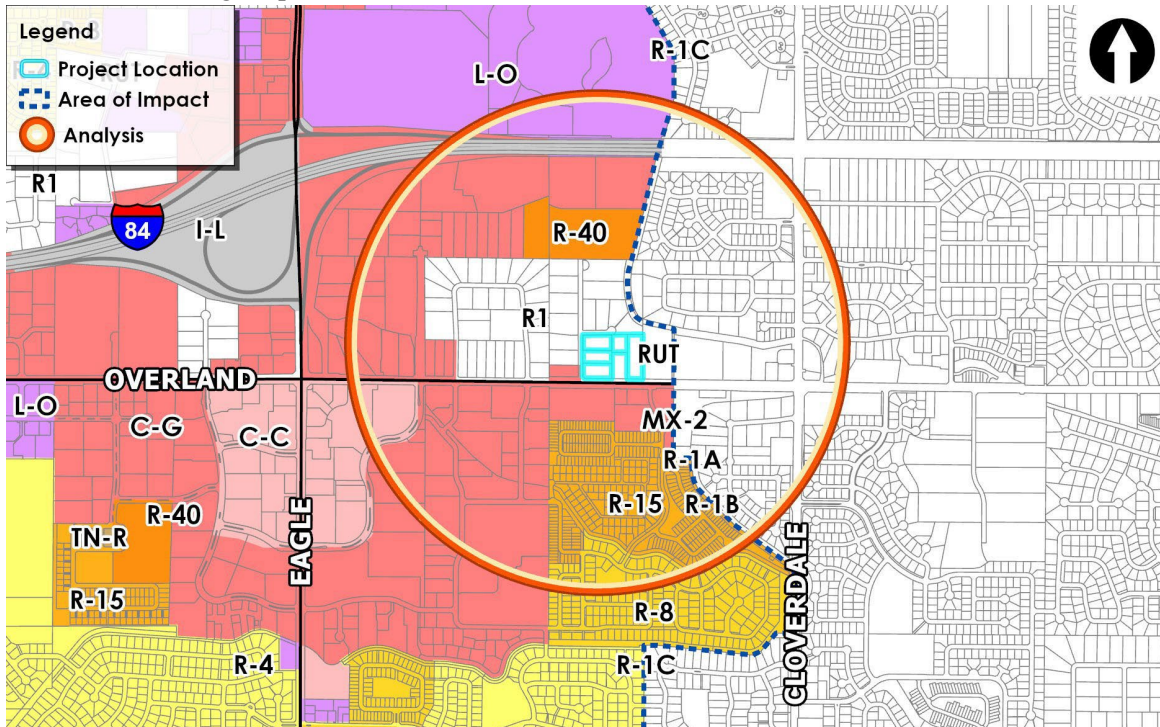
D. Project Area Maps

(link to [Project Overview](#))

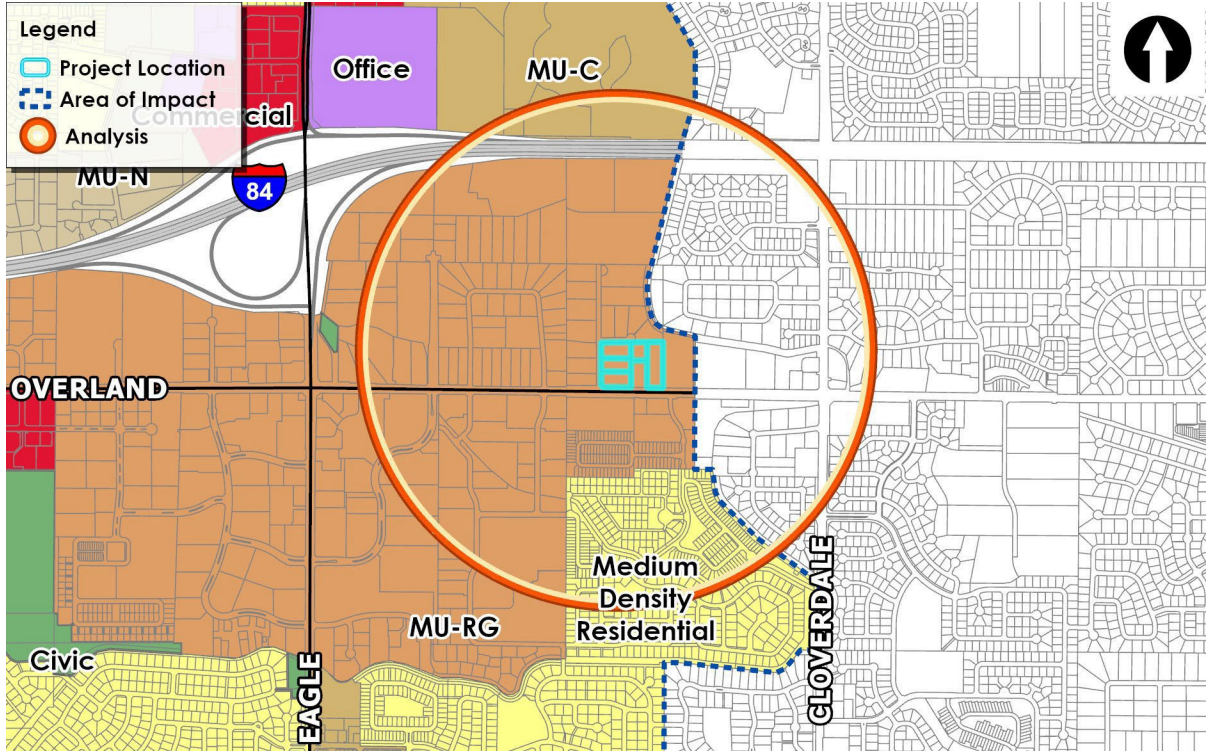
1. Aerial



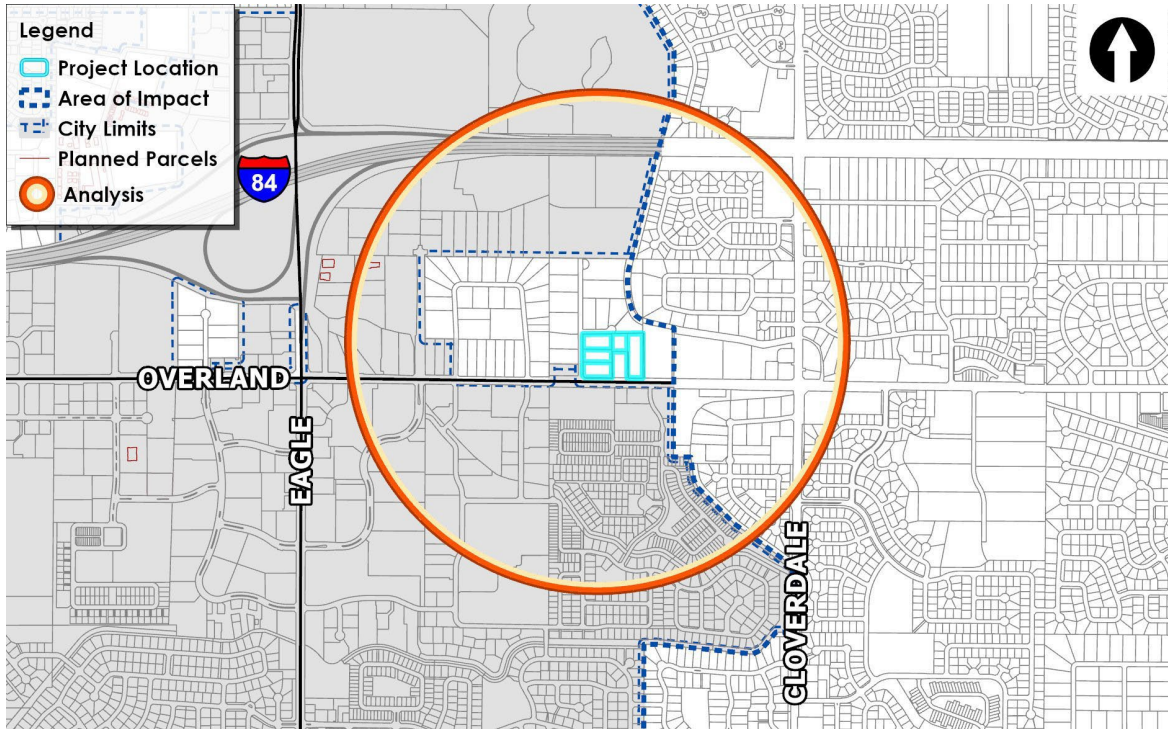
2. Zoning Map



3. Future Land Use



4. Planned Development Map



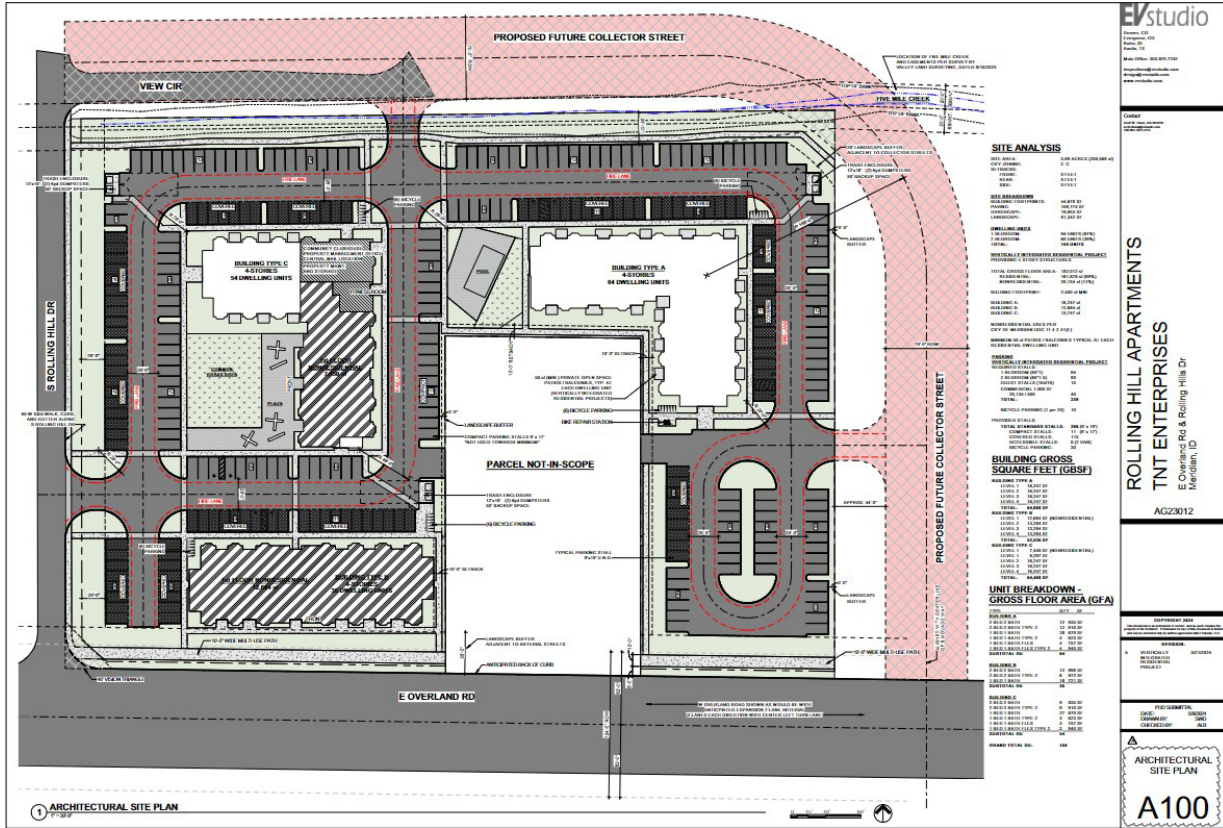
Service Accessibility Report

PARCEL R7555000190 SERVICE ACCESSIBILITY

Overall Score: 28	28th Percentile
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Criteria	Description	Indicator
Location	Within 1/2 mile of City Limits	YELLOW
Extension Sewer	Trunkshd mains < 500 ft. from parcel	GREEN
Floodplain	Within 100 yr floodplain & < 2 acres	RED
Emergency Services Fire	Response time < 5 min.	GREEN
Emergency Services Police	Meets response time goals most of the time	GREEN
Pathways	Within 1/4 mile of future pathways	YELLOW
Transit	Within 1/4 mile of current transit route	GREEN
Arterial Road Buildout Status	Ultimate configuration (# of lanes in master streets plan) matches existing (# of lanes)	GREEN
School Walking Proximity	From 1/2 to 1 mile walking	YELLOW
School Drivability	Either a High School or College within 2 miles OR a Middle or Elementary School within 1 mile driving (existing or future)	GREEN
Park Walkability	No park within walking distance by park type	RED

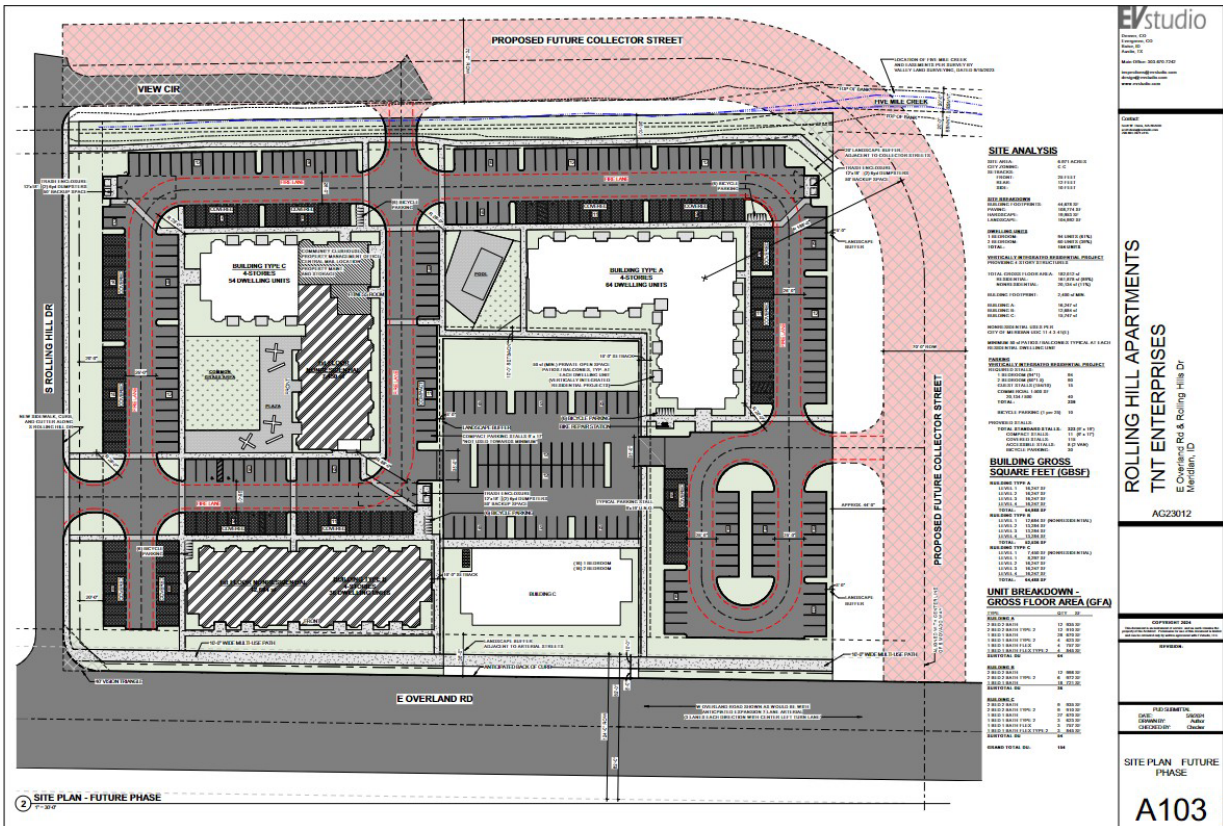
E. Site Plan (date: 5/21/2024)



EVstudio
 5000 E. Overland Rd & Rolling Hill Dr
 Meridian, ID
 AG23012

**ROLLING HILL APARTMENTS
 TNT ENTERPRISES**
 E Overland Rd & Rolling Hill Dr
 Meridian, ID
 AG23012

ARCHITECTURAL SITE PLAN
A100

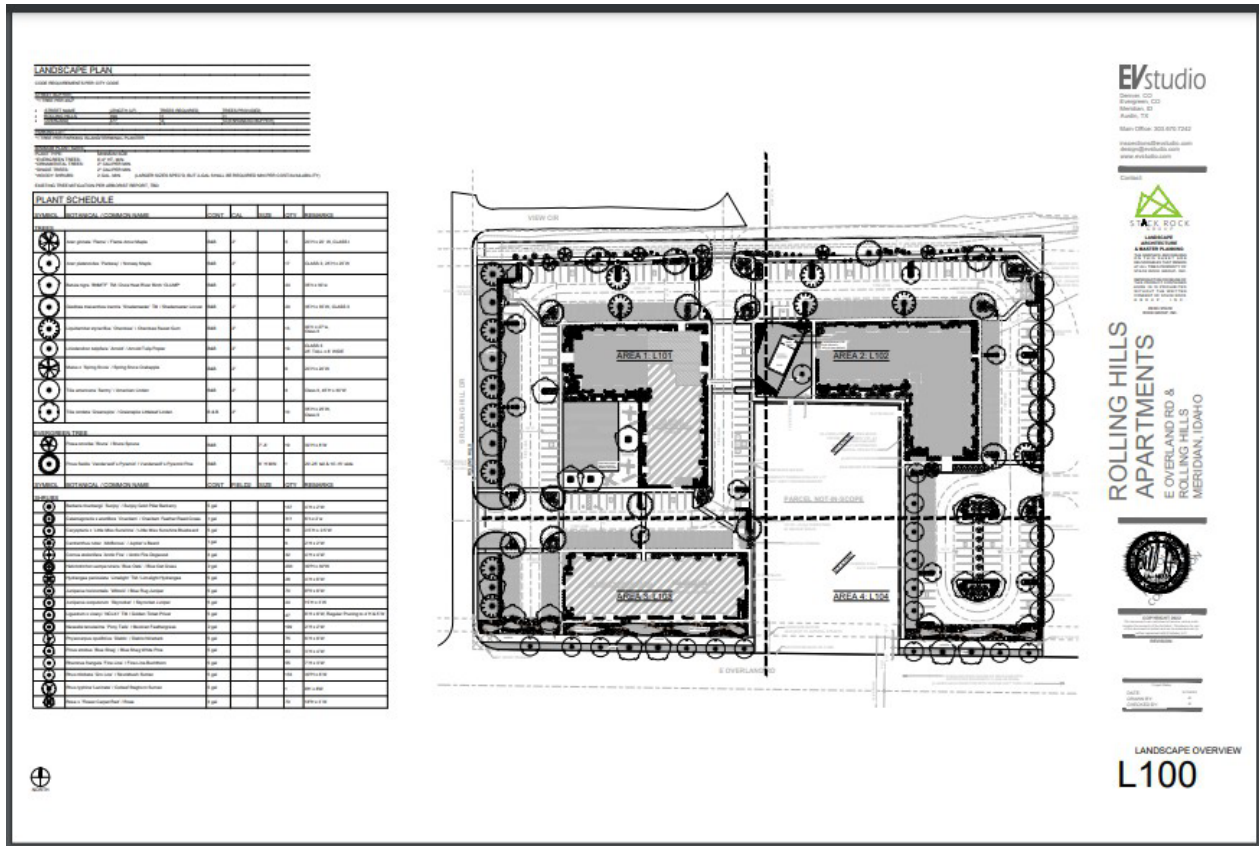


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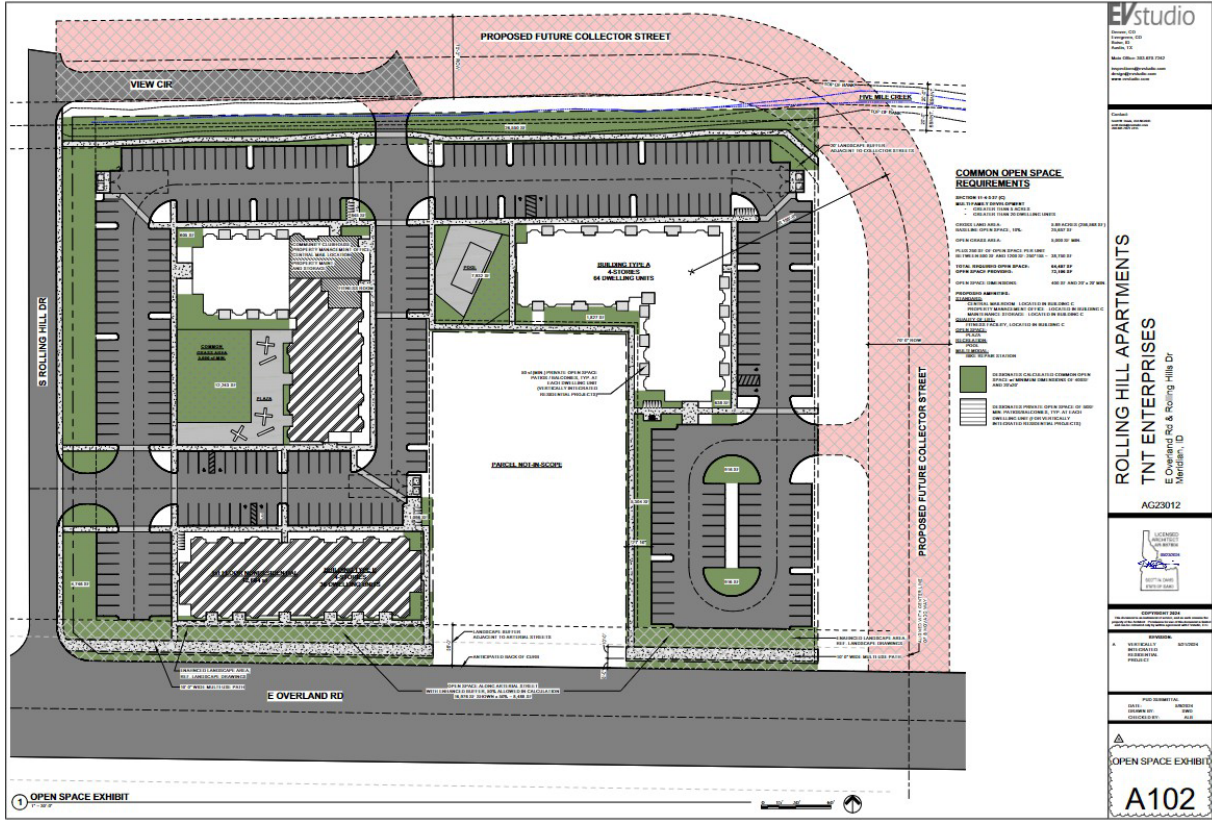
**ROLLING HILL APARTMENTS
 TNT ENTERPRISES**
 E Overland Rd & Rolling Hill Dr
 Meridian, ID
 AG23012

SITE PLAN - FUTURE PHASE
A103

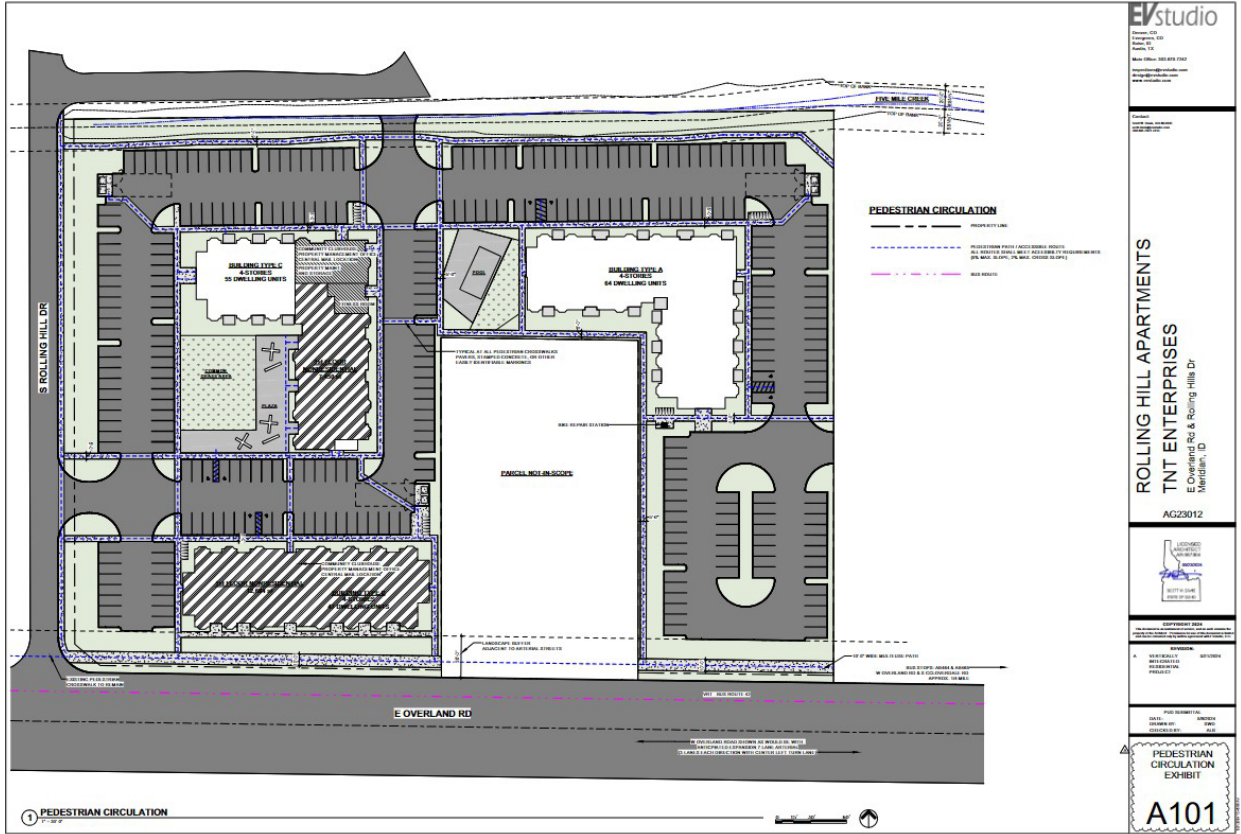
F. Landscape Plan (date: 5/23/2024)



G. Qualified Open Space Exhibit (date: 5/23/2024)



H. Pedestrian Circulation Plan (date: 5/23/2024)



I. Building Elevations (date: 5/23/2024)

MATERIALS LEGEND

1	2	3	4
EXTERIOR BLACK GRAY	CONCRETE BRICK	WHITE BRICKWORK	INTERIOR BLACK OF ROOF
5	6	7	8
9	10	11	12

EXTERIOR FINISHES	
1	CONCRETE BRICK
2	CONCRETE BRICK
3	CONCRETE BRICK
4	CONCRETE BRICK
5	CONCRETE BRICK
6	CONCRETE BRICK
7	CONCRETE BRICK
8	CONCRETE BRICK
9	CONCRETE BRICK
10	CONCRETE BRICK
11	CONCRETE BRICK
12	CONCRETE BRICK

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ROLLING HILL APARTMENTS
TNT ENTERPRISES
E. Overland Rd & Rolling Hills Dr
Meridian, ID

AG23012

2 BUILDING ELEVATION - SOUTH

MATERIALS LEGEND

1	2	3	4
EXTERIOR BLACK GRAY	CONCRETE BRICK	WHITE BRICKWORK	INTERIOR BLACK OF ROOF
5	6	7	8
9	10	11	12

EXTERIOR FINISHES	
1	CONCRETE BRICK
2	CONCRETE BRICK
3	CONCRETE BRICK
4	CONCRETE BRICK
5	CONCRETE BRICK
6	CONCRETE BRICK
7	CONCRETE BRICK
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11	CONCRETE BRICK
12	CONCRETE BRICK

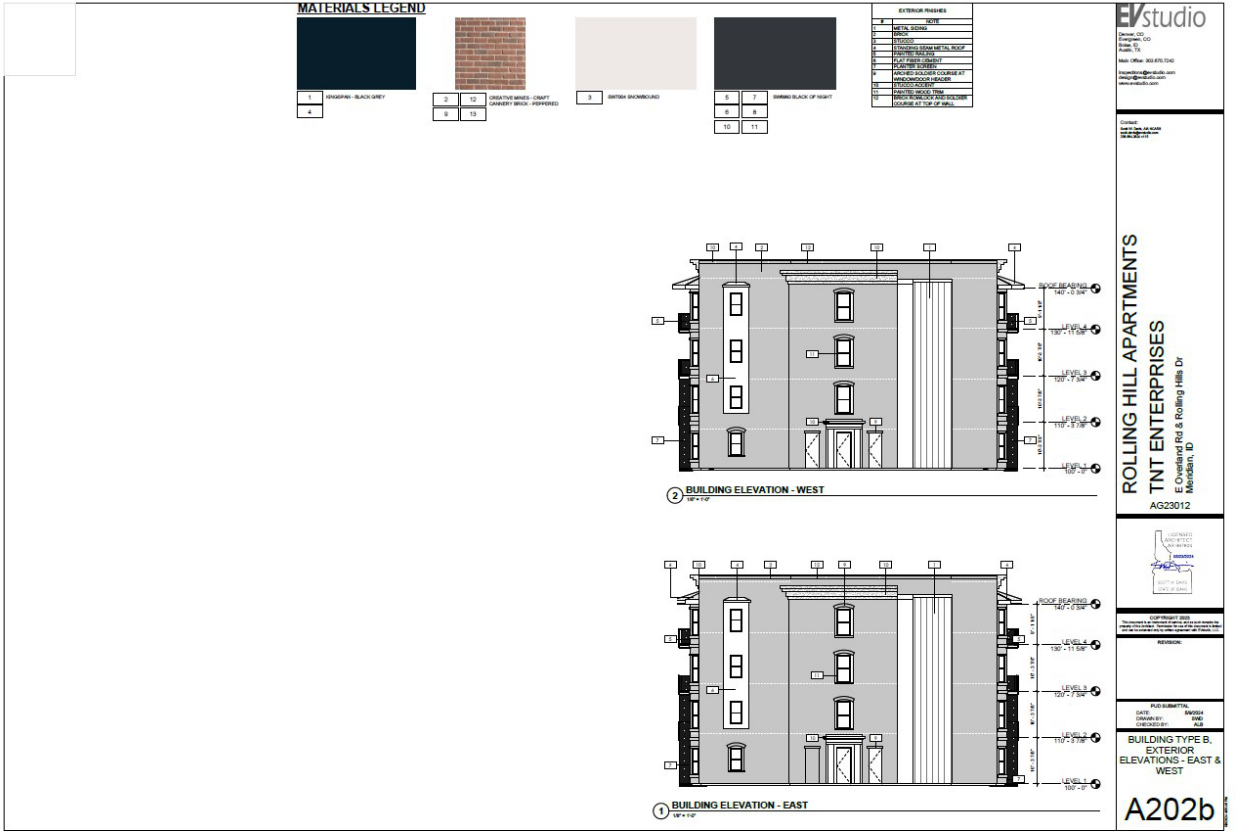
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TNE ENTERPRISES
E. Overland Rd & S Rolling Hills Dr
Meridian, ID

AG23012

2 BUILDING ELEVATION - SOUTH

1 BUILDING ELEVATION - NORTH





SITE RENDERING
LANDSCAPE AND SITE SERVICES ARE VARY (NOT LANDSCAPE)



TYPICAL BUILDING RENDERING



SITE RENDERING
LANDSCAPE AND SITE SERVICES ARE VARY (NOT LANDSCAPE)

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 www.evstudio.com

**ROLLING HILL APARTMENTS
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 Meridian, ID

AG23012

REVISIONS

DATE	BY	REVISION
08/14/2023	AG	REVISED BY

RENDERINGS

A901

J. Annexation Legal Description & Exhibit Map

EXHIBIT _____ ANNEXATION DESCRIPTION FOR ROLLING HILLS NO. 1

All of Lots 1, 3, 4 & 5 and a portion of Lot 2 in Block 1 of Rolling Hill Subdivision, according to the official plat thereof, filed in book 18 of Plats at pages 1202-2203, Ada County Records, lying within the SW 1/4 of the SE 1/4 of Section 16, Township 3 North, Range 1 East, Boise Meridian, located in the County of Ada, State of Idaho, being more particularly described as follows:

COMMENCING at the South 1/4 corner of said Section 16 from which the Southeast corner of said Section 16 bears South 89°13'12" East, 2676.93 feet; thence on the South line of said Section 16, South 89°13'12" East, 336.06 feet to the centerline of S. Rolling Hill Dr. and the **REAL POINT OF BEGINNING;**

thence on said centerline, North 0°06'35" West, 535.60 feet to the centerline of E. View Circle;

thence on said centerline, North 89°21'00" East, 275.85 feet;

thence continuing on said centerline 64.14 feet northeasterly on a curve to the left, having a radius of 100.00 feet, through a central angle of 36°45'00" and a long chord which bears North 70°51'14" East, 63.05 feet;

thence South 0°39'38" East, 45.00 feet lot corner common to said lots 2 & 5;

thence on the North line of said Lots 1 & 2, North 89°21'00" East, 333.54 feet to the Northeast corner of said Lot 1;

thence on the East line of said Lot 1 and the Southerly extension thereof, South 0°02'04" West, 527.28 feet to said South line of Section 16;

thence on said South line, North 89°13'12" West, 165.82 feet;

thence on the West line of Lot 1 and the Southerly extension thereof, North 0°01'59" East, 330.03 feet;

thence North 89°13'12" West, 167.29 feet to the East line of said Lot 4;

thence on the East line of said Lots 3 & 4 and the Southerly extension thereof, South 0°05'39" East, 330.04 feet to said South line;

thence on said South line, North 89°13'12" West, 335.77 feet to the **REAL POINT OF BEGINNING;**

Containing an approximate area of 6.90 Acres or 300,722 square feet, more or less.



