

	AGGREGATE BASE	MW	MONITORING WELL
C	ASPHALT CONCRETE	N	NORTHING
CP	ASBESTOS CEMENT PIPE	NTS	NOT TO SCALE
CT&T	AMERICAN TELEPHONE & TELEGRAPH	OC	ON-CENTER
C	BEGIN CURVE, BOTTOM OF CURB	OH	OVERHEAD UTILITY LINE
W	BACK OF WALK	PB	PACIFIC BELL
	CURVE	PCC	PORTLAND CEMENT CONCRETE
&G	CURB & GUTTER	PE	POLYETHYLENE
DF	CONTROLLED DENSITY FILL	PG&E	PACIFIC GAS & ELECTRIC
I	CAST IRON	PIP	PROTECT IN PLACE
IL	CENTERLINE	PL	PLASTIC
MP	CORRUGATED METAL PIPE	POC	POINT OF CONNECTION
OM	COMMUNICATION	PT	POINT
ONC	CONCRETE	PVC	POLYVINYL CHLORIDE
Y	CUBIC YARD	R	RADIUS, RIGHT
I	DRAIN INLET	RC	RELATIVE COMPACTION
IFF	DIFFERENCE IN ELEVATION BETWEEN NEW & EX	RCP	REINFORCED CONCRETE PIPE
WY	DRIVEWAY	REHAB	REHABILITATION
	ELECTRICAL, EASTING	RT	RIGHT
C	EDGE OF CONCRETE, END CURVE	R/W	RIGHT OF WAY
P	EDGE OF PAVEMENT	S	SOUTH
X	EXISTING	SD	STORM DRAIN
DAC	FULL DEPTH ASPHALT CONCRETE	SDMH	STORM DRAIN MANHOLE
G	FINISH GRADE	SDR	STANDARD DIMENSION RATIO
H	FIRE HYDRANT	SL	STATION LINE, STREET LIGHT
L	FLOW LINE	SM	SURVEY MARKER
O	FIBER OPTICS	SS	SANITARY SEWER
S	FIRE SERVICE	SSCO	SANITARY SEWER CLEANOUT
W	FACE OF WALK	SSMH	SANITARY SEWER MANHOLE
G	GAS	STA	STATION
DB	GRADE BREAK	STD	STANDARD
DP	HIGH DENSITY POLYETHYLENE PIPE	SW	SIDEWALK
IP	HIGH POINT	TC	TOP OF CURB
IV	HIGH VOLTAGE	TEL	TELEPHONE, TELECOM
YD	FIRE HYDRANT	TFL	THEORETICAL FLOW LINE
NV	INVERT	TTC	THEORETICAL TOP OF CURB
RR	IRRIGATION	TYP	TYPICAL
P	JOINT POLE	UT	UTILITY
	LEFT, LINE, LENGTH	VCP	VITRIFIED CLAY PIPE
F	LINEAR FEET	W	WEST, WATER
P	LOW POINT	WTR	WATER
T	LEFT	WM	WATER METER
MH	MANHOLE	WV	WATER VALVE
ISC	MISCELLANEOUS	WW	WELDED WRAP
ION, M	MONUMENT		

ALL MATERIAL AND WORKMANSHIP SHALL FULLY CONFORM WITH THE SPECIFICATIONS, STANDARDS, AND ORDINANCES OF THE CITY OF LOS ALTOS. STANDARD SPECIFICATIONS AND DETAILS ARE AVAILABLE IN THE OFFICE OF THE CITY ENGINEER.

THE OFFICE OF PUBLIC WORKS INSPECTION SHALL BE NOTIFIED AT LEAST 72 HOURS IN ADVANCE OF ANY WORK.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE EXISTENCE AND LOCATION OF ALL UTILITIES. THE UNDERGROUND CONTRACTOR SHALL NOTIFY UTILITY COMPANIES AT LEAST 48 HOURS IN ADVANCE OF CONSTRUCTION TO FIELD LOCATE UTILITIES. CONTACT UNDERGROUND SERVICE ALERT AT 800-227-2600.

THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY FIELD CHANGES MADE WITHOUT WRITTEN AUTHORIZATION FROM THE CITY ENGINEER.

CONTRACTOR SHALL PROVIDE ADEQUATE TRAFFIC CONTROLS & SHALL SUBMIT A TRAFFIC CONTROL PLAN

ALL EXISTING UTILITIES AND PRIVATE IMPROVEMENTS THAT BECOME DAMAGED DURING CONSTRUCTION SHALL BE COMPLETELY RESTORED TO THE SATISFACTION OF THE CITY ENGINEER, AT CONTRACTOR'S SOLE EXPENSE.

THE CONTRACTOR AGREES THAT, IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY AND THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY AND NOT JUST DURING NORMAL WORKING HOURS.

EXCAVATIONS SHALL BE ADEQUATELY SHORED, BRACED AND SHEETED SO THAT THE EARTH WILL NOT SLIDE OR SETTLE. AND SO THAT ALL EXISTING IMPROVEMENTS OF ANY KIND WILL BE FULLY PROTECTED FROM DAMAGE. ANY DAMAGE RESULTING FROM A LACK OF ADEQUATE SHORING, BRACING OR SHEETING, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL MAKE NECESSARY REPAIRS OR RECONSTRUCTION AT CONTRACTOR OWN EXPENSE. WHERE THE EXCAVATION FOR A CONDUIT TRENCH, AND/OR STRUCTURE IS FOUR FEET OR MORE IN DEPTH, THE CONTRACTOR SHALL PROVIDE ADEQUATE SHEETING, SHORING AND BRACING OR EQUIVALENT METHOD, FOR THE PROTECTION OF LIFE, OR LIMB, WHICH SHALL CONFORM TO THE APPLICABLE CONSTRUCTION SAFETY ORDERS OF THE DIVISION OF INDUSTRIAL SAFETY OF THE STATE OF CALIFORNIA. THE CONTRACTOR SHALL ALWAYS COMPLY WITH OSHA REQUIREMENTS.

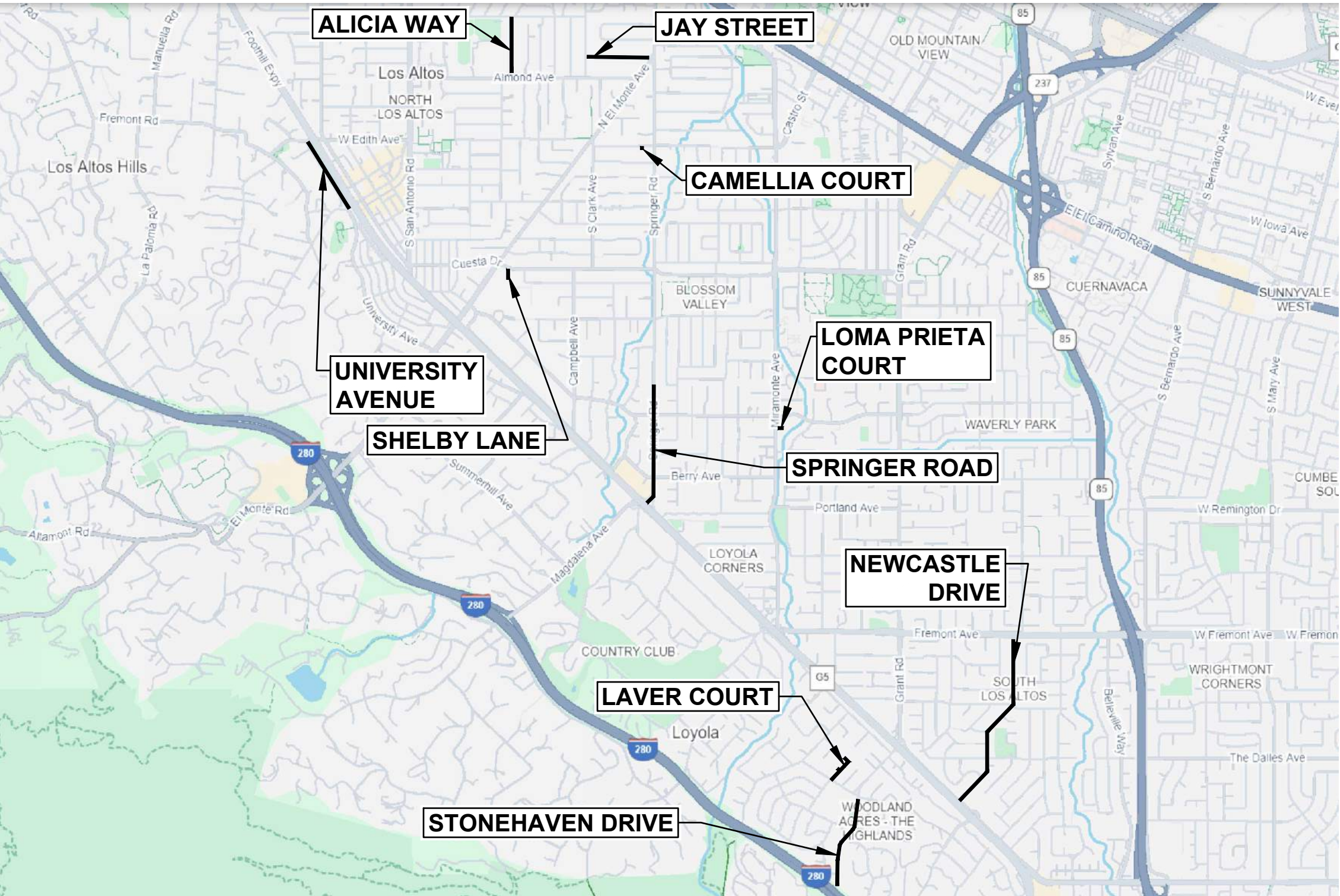
SHOULD IT APPEAR THAT THE WORK TO BE DONE, OR ANY MATTER RELATIVE THERETO, IS NOT SUFFICIENTLY DETAILED OR EXPLAINED ON THESE PLANS, THE CONTRACTOR SHALL CONTACT THE ENGINEER FOR SUCH FURTHER EXPLANATIONS AS MAY BE NECESSARY.

EXISTING UTILITIES SHALL BE MAINTAINED IN SERVICE AND IN PLACE BY THE CONTRACTOR DURING CONSTRUCTION UNLESS OTHERWISE SHOWN.

CONTRACTOR SHALL PROTECT ALL MONUMENTS.

ALL USA MARKINGS TO BE REMOVED AT END OF CONSTRUCTION.

NTS



NTS

<u>SHEET NUMBER</u>		<u>DESCRIPTION</u>
1	C1.1	COVER SHEET
2	C2.1	BLUEPRINT FOR A CLEAN BAY
3	C3.1	TYPICAL SECTIONS
4	C3.2	TYPICAL SECTIONS
5	C3.3	TYPICAL SECTIONS
6	C3.4	TYPICAL SECTIONS
7	C3.5	TYPICAL SECTIONS
8	C4.1	IMPROVEMENT PLAN - ALICIA WAY
9	C4.2	IMPROVEMENT PLAN - ALICIA WAY
10	C4.3	IMPROVEMENT PLAN - ALICIA WAY
11	C4.4	IMPROVEMENT PLAN - CAMELLIA COURT
12	C4.5	IMPROVEMENT PLAN - JAY STREET
13	C4.6	IMPROVEMENT PLAN - JAY STREET
14	C4.7	IMPROVEMENT PLAN - JAY STREET
15	C4.8	IMPROVEMENT PLAN - LAVER COURT
16	C4.9	IMPROVEMENT PLAN - LAVER COURT
17	C4.10	IMPROVEMENT PLAN - LOMA PRIETA COURT
18	C4.11	IMPROVEMENT PLAN - NEWCASTLE DRIVE
19	C4.12	IMPROVEMENT PLAN - NEWCASTLE DRIVE
20	C4.13	IMPROVEMENT PLAN - NEWCASTLE DRIVE
21	C4.14	IMPROVEMENT PLAN - NEWCASTLE DRIVE
22	C4.15	IMPROVEMENT PLAN - NEWCASTLE DRIVE
23	C4.16	IMPROVEMENT PLAN - NEWCASTLE DRIVE
24	C4.17	IMPROVEMENT PLAN - NEWCASTLE DRIVE
25	C4.18	IMPROVEMENT PLAN - SHELBY LANE
26	C4.19	IMPROVEMENT PLAN - SPRINGER ROAD
27	C4.20	IMPROVEMENT PLAN - SPRINGER ROAD
28	C4.21	IMPROVEMENT PLAN - SPRINGER ROAD
29	C4.22	IMPROVEMENT PLAN - SPRINGER ROAD
30	C4.23	IMPROVEMENT PLAN - SPRINGER ROAD
31	C4.24	IMPROVEMENT PLAN - STONEHAVEN DRIVE
32	C4.25	IMPROVEMENT PLAN - STONEHAVEN DRIVE
33	C4.26	IMPROVEMENT PLAN - STONEHAVEN DRIVE
34	C4.27	IMPROVEMENT PLAN - STONEHAVEN DRIVE
35	C4.28	IMPROVEMENT PLAN - UNIVERSITY AVENUE
36	C4.29	IMPROVEMENT PLAN - UNIVERSITY AVENUE
37	C4.30	IMPROVEMENT PLAN - UNIVERSITY AVENUE
38	C4.31	IMPROVEMENT PLAN - UNIVERSITY AVENUE

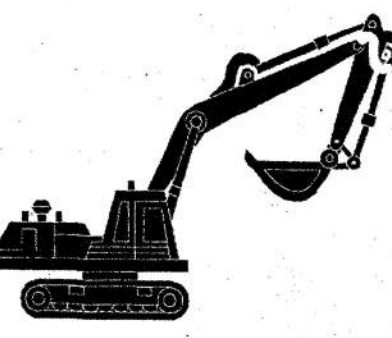
39	C5.1	CIVIL DETAILS
40	C5.2	CIVIL DETAILS
41	C5.3	CIVIL DETAILS
42	C5.4	CURB RAMP DETAILS
43	C5.5	CURB RAMP DETAILS
44	C5.6	CURB RAMP DETAILS
45	C5.7	CURB RAMP DETAILS
46	C5.8	CURB RAMP DETAILS
47	C5.9	CURB RAMP DETAILS
48	C5.10	CURB RAMP DETAILS
49	C5.11	CURB RAMP DETAILS
50	C5.12	CURB RAMP DETAILS



Call Two Working  
Days Before You  
Dig!

## Heavy Equipment Operation

**Best Management Practices for the Construction Industry**



**Doing the Job Right**

**Site Planning and Preventive Vehicle Maintenance**


- Maintain all vehicles and heavy equipment. Inspect frequently for and repair leaks.
- Perform major maintenance, repair jobs, and vehicle and equipment washing off site where cleanup is easier.
- If you must drain and replace motor oil, radiator coolant, or other fluids on site, use drip pans or drop cloths to catch drips and spills. Collect all spent fluids, store in separate containers, and properly dispose as hazardous waste (recycle whenever possible).
- Do not use diesel oil to lubricate equipment parts, or clean equipment. Use only water for any onsite cleaning.
- Cover exposed fifth wheel hitches and other oily or greasy equipment during rain events.

**Storm water Pollution from Heavy Equipment on Construction Sites**

Poorly maintained vehicles and heavy equipment that leak fuel, oil, antifreeze or other fluids on the construction site are common sources of storm drain pollution. Prevent spills and leaks by isolating equipment from runoff channels, and by watching for leaks and other maintenance problems. Remove construction equipment from the site as soon as possible.

## Landscaping, Gardening, and Pool Maintenance

**Best Management Practices for the Construction Industry**



**Doing the Right Job**

**General Business Practices**

- Protect stockpiles and landscaping materials from wind and rain by storing them under tarps or secured plastic sheeting.
- Store pesticides, fertilizers, and other chemicals indoors or in a shed or storage cabinet.
- Schedule grading and excavation projects during dry weather.
- Use temporary check dams or ditches to divert runoff away from storm drains.
- Protect storm drains with sandbags or other sediment controls.
- Re-vegetation is an excellent form of erosion control for any site.

**Landscaping/Garden Maintenance**


- Use pesticides sparingly, according to instructions on the label. Rinse empty containers, and use rinse water as product. Dispose of rinsed, empty containers in the trash. Dispose of unused pesticides as hazardous waste.
- Collect lawn and garden clippings, pruning waste, and tree trimmings. Chip if necessary, and compost.
- In communities with outside pick-up of yard waste, place clippings and pruning waste at the curb in approved bins or containers. Or, take to a landfill that composts yard waste. No outside pickup of yard waste is available for commercial properties.

**Storm Drain Pollution from Landscaping and Swimming Pool Maintenance**

Many landscaping activities expose soils and increase the likelihood that earth and garden chemicals will run off into the storm drains during irrigation or when it rains. Selecting pool water containing chlorine and copper-based algaecides should never be discharged to storm drains. These chemicals are toxic to aquatic life.

## General Construction And Site Supervision

**Best Management Practices For Construction**



**Doing The Job Right**

**General Principles**

- Keep an orderly site and ensure good housekeeping practices are used.
- Maintain equipment properly.
- Cover materials when they are not in use.
- Keep materials away from streets, storm drains and drainage channels.
- Ensure dust control water doesn't leave site or discharge to storm drains.

**Advance Planning To Prevent Pollution**

- Schedule excavation and grading activities for dry weather periods. To reduce soil erosion, plant temporary vegetation or place other erosion controls before rain begins. Use the Erosion and Sediment Control Manual, available from the Regional Water Quality Control Board, as a reference.
- Control the amount of runoff crossing your site (especially during excavation) by using berms or temporary or permanent drainage ditches to divert water flow around the site. Reduce storm water runoff velocities by constructing temporary check dams or berms where appropriate.
- Train your employees and subcontractors. Make these best management practices available to everyone who works on the construction site. Inform subcontractors about the storm water requirements and their own responsibilities.

**Good Housekeeping Practices**

- Designate one area of the site for auto parking, vehicle refueling, and routine equipment maintenance. The designated area should be well away from streams or storm drain inlets, berms if necessary. Make major repairs off site.
- Keep materials out of the rain - prevent runoff contamination at the source. Cover exposed piles of soil or construction materials with plastic sheeting or temporary rocks. Before it rains, sweep and remove materials from surfaces that drain to storm drains, creeks, or channels.
- Keep pollutants off exposed surfaces. Place treatable and recycling receptacles around the site to minimize litter.


**Storm Drain Pollution from Construction Activities**

Construction sites are common sources of storm water pollution. Materials and wastes that blow or wash into a storm drain, gutter, or street have a direct impact on local creeks and the Bay.

As a contractor, or site supervisor, owner or operator of a site, you may be responsible for any environmental damage caused by your subcontractors or employees.

## Roadwork and Paving

**Best Management Practices for the Construction Industry**



**Doing The Job Right**

**General Business Practices**

- Develop and implement erosion/sediment control plans for roadway embankments.
- Schedule excavation and grading work during dry weather.
- Check for and repair leaking equipment.
- Perform major equipment repairs at designated areas in the maintenance yard, where cleanup is easier. Avoid performing equipment repairs at construction sites.
- When refueling or when vehicle/equipment maintenance must be done on site, designate a location away from storm drains and creeks.
- Do not use diesel oil to lubricate equipment parts or clean equipment.
- Recycle used oil, concrete, broken asphalt, etc. whenever possible, or dispose of properly.

**During Construction**


- Avoid paving and seal coating in wet weather, or when rain is forecast, to prevent fresh materials from contacting stormwater runoff.
- Cover and seal catch basins and manholes when applying seal coat, slurry seal, fog seal, or similar materials.
- Protect drainage ways by using earth dikes, sand bags, or other controls to divert or trap and filter runoff.

**Storm Drain Pollution from Roadwork**

Road paving, surfacing, and pavement removal happen right in the street, where there are numerous opportunities for asphalt, saw-cut slurry, or excavated material to illegally enter storm drains. Extra planning is required to store and dispose of materials properly and guard against pollution of storm drains, creeks, and the Bay.

## Painting and Application of Solvents and Adhesives

**Best Management Practices for the Construction Industry**



**Doing The Job Right**

**Handling Paint Products**

- Keep all liquid paint products and wastes away from the gutter, street, and storm drains. Liquid residues from paints, thinners, solvents, glues, and cleaning fluids are hazardous wastes and must be disposed of at a hazardous waste collection facility (contact your local storm drain program listed on the back of this brochure).
- When thoroughly dry, empty paint cans, used brushes, rags, and drop cloths may be disposed of as garbage in a sanitary landfill. Empty, dry paint cans also may be recycled as metal.
- Wash water from painted buildings constructed before 1978 can contain high amounts of lead, even if paint chips are not present. Before you begin stripping paint or cleaning pre-1978 building exteriors with water under high pressure, test paint for lead by taking paint scrapings to a local laboratory. See Yellow Pages for a state-certified laboratory.
- If there is loose paint on the building, or if the paint tests positive for lead, block storm drains. Check with the wastewater treatment plant to the sanitary sewer. If you must send it offsite for disposal as hazardous waste.

**Paint Removal**

- Paint chips and dust from non-hazardous dry stripping and sand blasting may be swept up or collected in plastic drop cloths and disposed of as trash.
- Chemical paint stripping residue and chips and dust from marine paints or paints containing lead, mercury or tributyl tin must be disposed of as hazardous wastes. Lead based paint removal requires a state-certified contractor.
- When stripping or cleaning building exteriors with high-pressure water, block storm drains. Direct wash water onto a dirt area and spade into soil. Or, check with the local wastewater treatment authority to find out if you can collect (pump or vacuum) building cleaning water and dispose to the sanitary sewer. Sampling of the water may be required to assist the wastewater treatment authority in making its decision.

**Recycle/Reuse Leftover Paints**

**Whenever Possible**

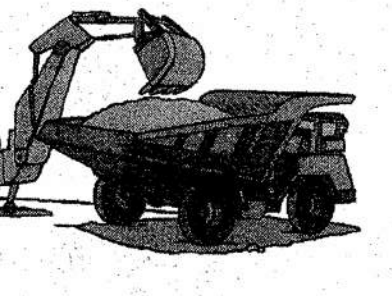
- Recycle or donate excess water-based (latex) paint, or return to supplier.
- Reuse leftover oil-based paint. Dispose of non-recyclable thinners, sludge and unwanted paint, as hazardous waste.
- Unopened cans of paint may be able to be returned to the paint vendor. Check with the vendor regarding its "buy-back" policy.

**Storm Drain Pollution from Paints, Solvents, and Adhesives**

All paints, solvents, and adhesives contain chemicals that are harmful to wildlife in local creeks, San Francisco Bay, and the Pacific Ocean. Toxic chemicals may come from liquid or solid products or from cleaning residues or rags. Paint material and wastes, adhesives and cleaning fluids should be recycled when possible, or disposed of properly to prevent these materials from flowing into storm drains and watercourses.

## Earth-Moving And Dewatering Activities

**Best Management Practices for the Construction Industry**



**Doing The Job Right**

**General Business Practices**

- Schedule excavation and grading work during dry weather.
- Perform major equipment repairs away from the job site.
- When refueling or vehicle/equipment maintenance must be done on site, designate a location away from storm drains.
- Do not use diesel oil to lubricate equipment parts, or clean equipment.

**Practices During Construction**

- Remove existing vegetation only when absolutely necessary. Plant temporary vegetation for erosion control on slopes or where construction is not immediately planned.
- Protect down slope drainage courses, streams, and storm drains with wattles, or temporary drainage swales. Use check dams or ditches to divert runoff around excavations. Refer to the Regional Water Quality Control Board's Erosion and Sediment Control Field Manual for proper erosion and sediment control measures.

**Storm Drain Pollution from Earth-Moving Activities and Dewatering**

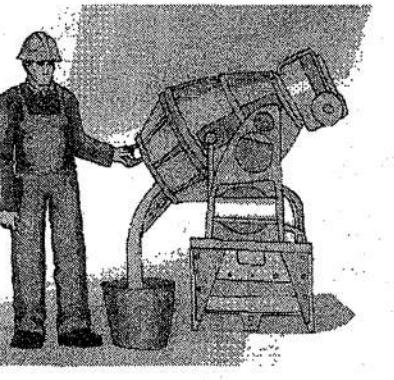
Soil excavation and grading operations loosen large amounts of soil that can flow or blow into storm drains when handled improperly. Sediments in runoff can clog storm drains, smother aquatic life, and destroy habitats in creeks and the Bay. Effective erosion control practices reduce the amount of runoff crossing a site and slow the flow with check dams or roughened ground surfaces.

**Best Management Practices for the Construction Industry**

- Bulldozer, back hoe, and grading machine operators.
- Dump truck drivers.
- Site supervisors.
- General contractors.
- Home builders.
- Developers.

## Fresh Concrete and Mortar Application

**Best Management Practices for the Construction Industry**



**Doing The Job Right**


**General Business Practices**

- Wash out concrete mixers only in designated wash-out areas in your yard, away from storm drains and waterways, where the water will flow into a temporary waste pit in a dirt area. Let water percolate through soil and dispose of settled, hardened concrete as garbage. Whenever possible, recycle washout by pumping back into mixers for reuse.
- Wash out chutes onto dirt areas at site that do not flow to streets or drains.
- Always store both dry and wet materials under cover, protected from rainfall and runoff and away from storm drains or waterways. Protect dry materials from wind.
- Secure bags of cement after they are open. Be sure to keep wind-blown cement powder away from streets, gutters, storm drains, rainfall, and runoff.
- Do not use diesel fuel as a lubricant on concrete forms, tools, or trailers.

**Storm Drain Pollution from Fresh Concrete and Mortar Applications**

Fresh concrete and cement-related mortar that wash into lakes, streams, or estuaries are toxic to fish and the aquatic environment. Disposing of these materials to the storm drains or creeks can block storm drains, cause serious problems, and is prohibited by law.

## Los Altos Municipal Code Requirements



**Los Altos Municipal Code Chapter 10.08.390 Non-storm water discharges**

A. Unlawful discharges. It shall be unlawful to discharge any domestic waste or industrial waste into storm drains, gutters, creeks, or San Francisco Bay. Unlawful discharges to storm drains shall include, but not be limited to, discharge from toilets; sinks; industrial processes; cooling systems; boilers; fabric cleaning; equipment cleaning; vehicle cleaning; construction activities, including, but not limited to, painting, paving, concrete placement, saw cutting and grading; swimming pools; spas; and fountains, unless specifically permitted by a discharge permit or unless exempted pursuant to guidelines published by the superintendent.

B. Threatened discharges. It shall be unlawful to cause hazardous materials, domestic waste, or industrial waste to be deposited in such a manner or location as to constitute a threatened discharge into storm drains, gutters, creeks or San Francisco Bay. A "threatened discharge" is a condition creating a substantial probability of harm, when the probability and potential extent of harm make it reasonably necessary to take immediate action to prevent, reduce or mitigate damages to persons, property or natural resources. Domestic or industrial wastes that are no longer contained in a pipe, tank or other container are considered to be threatened discharges unless they are actively being cleaned up.

**Los Altos Municipal Code Section 10.08.430 Requirements for construction operations.**

A. A spill response plan for hazardous waste, hazardous materials and uncontained construction materials shall be prepared and available at the construction sites for all projects where the proposed construction site is equal to or greater than one acre of disturbed soil and for any other projects for which the city engineer determines it is necessary to protect surface waters. Preparation of the plan shall be in accordance with guidelines published by the city engineer.

B. A storm water pollution prevention plan shall be prepared and available at the construction sites for all projects greater than one acre of disturbed soil and for any other projects for which the city engineer determines that a storm water management plan is necessary to protect surface waters. Preparation of the plan shall be in accordance with guidelines published by the city engineer.

C. Prior approval shall be obtained from the city engineer or designee to discharge water pumped from construction sites to the storm drain. The city engineer or designee may require gravity settling and filtration upon a determination that either or both would improve the water quality of the discharge. Contaminated groundwater or water that exceeds state or federal requirements for discharge to navigable waters may not be discharged to the storm drain. Such water may be discharged to the sewer, provided that the requirements of Section 10.08.240 are met and the approval of the superintendent is obtained prior to discharge.

D. No cleanup of construction debris from the streets shall result in the discharge of water to the storm drain system; nor shall any construction debris be deposited or allowed to be deposited in the storm drain system. (Prior code § 5-5.645)

**Criminal and judicial penalties can be assessed for non-compliance.**

## Preventing Pollution: It's Up to Us

In the Santa Clara Valley, storm drains transport water directly to local creeks and San Francisco Bay without treatment. Storm water pollution is a serious problem for wildlife dependent on our waterways and for the people who live near polluted streams or bay lands. Some common sources of this pollution include spilled oil, fuel, and fluids from vehicles and heavy equipment; construction debris; sediment created by erosion; landscaping runoff containing pesticides or weed killers; and materials such as used motor oil, antifreeze, and paint products that people pour or spill into a street or storm drain.

Thirteen valley municipalities have joined together with Santa Clara County and the Santa Clara Valley Water District to educate local residents and businesses and fight storm water pollution. To comply with this program, contractors must comply with the practices described in this drawing sheet.

**Spill Response Agencies**

DIAL 9-1-1

State Office of Emergency Services Warning Center (24 hours): 800-852-7550

Santa Clara County Environmental Health Services: (408) 299-6930

**Local Pollution Control Agencies**

County of Santa Clara Pollution Prevention Program: (408) 441-1195

County of Santa Clara Integrated Waste Management Program: (408) 441-1198

County of Santa Clara District Attorney Environmental Crimes Hotline: (408) 299-TIPS

Santa Clara County Recycling Hotline: 1-800-533-8414

Santa Clara Valley Water District: (408) 265-2600

Santa Clara Valley Water District Pollution Hotline: 1-888-510-5151

Regional Water Quality Control Board San Francisco Bay Region: (510) 622-2300

Palo Alto Regional Water Quality Control Plant: (650) 328-2598

Serving East Palo Alto Sanitary District, Los Altos, Los Altos Hills, Mountain View, Palo Alto, Stanford

**City of Los Altos**

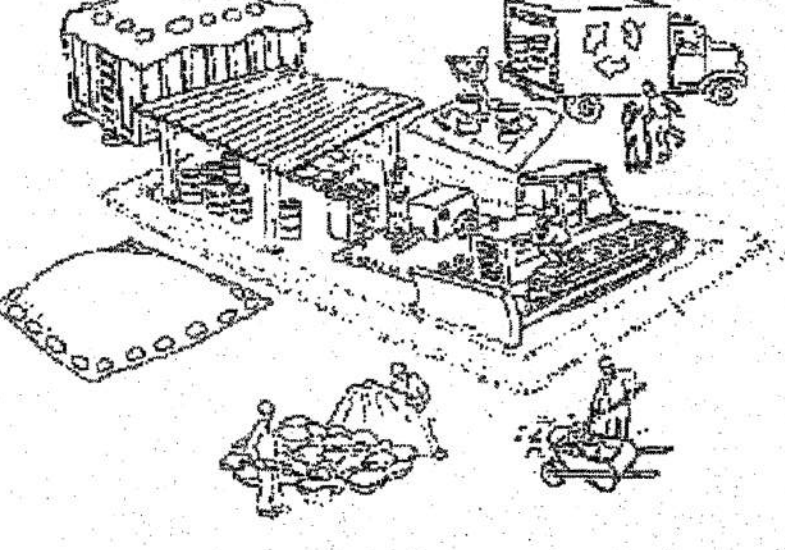
Building Department: (650) 947-2752

Engineering Department: (650) 947-2780

# Blueprint for a Clean Bay

Remember: The property owner and the contractor share ultimate responsibility for the activities that occur on a construction site. You may be held responsible for any environmental damage caused by your subcontractors or employees.

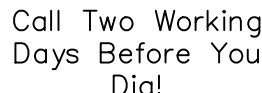
## Best Management Practices for the Construction Industry



**Santa Clara Urban Runoff Pollution Prevention Program**

DESIGNED BY: LARRY LIND	APPROVED BY: [Signature] CITY ENGINEER	CITY OF LOS ALTOS RCE	DATE: OCTOBER, 2003
DRAWN BY: VICTOR CHEN	CITY ENGINEER	SCALE: N.T.S.	
CHECKED BY: JIM GUSTAFSON	SHEET	OF	SHEETS
			DRAWING NO:





65% SUBMITTAL  
APRIL 07, 2025

TYPICAL SECTION	
LOS ANGELES COUNTY	CALIFORNIA

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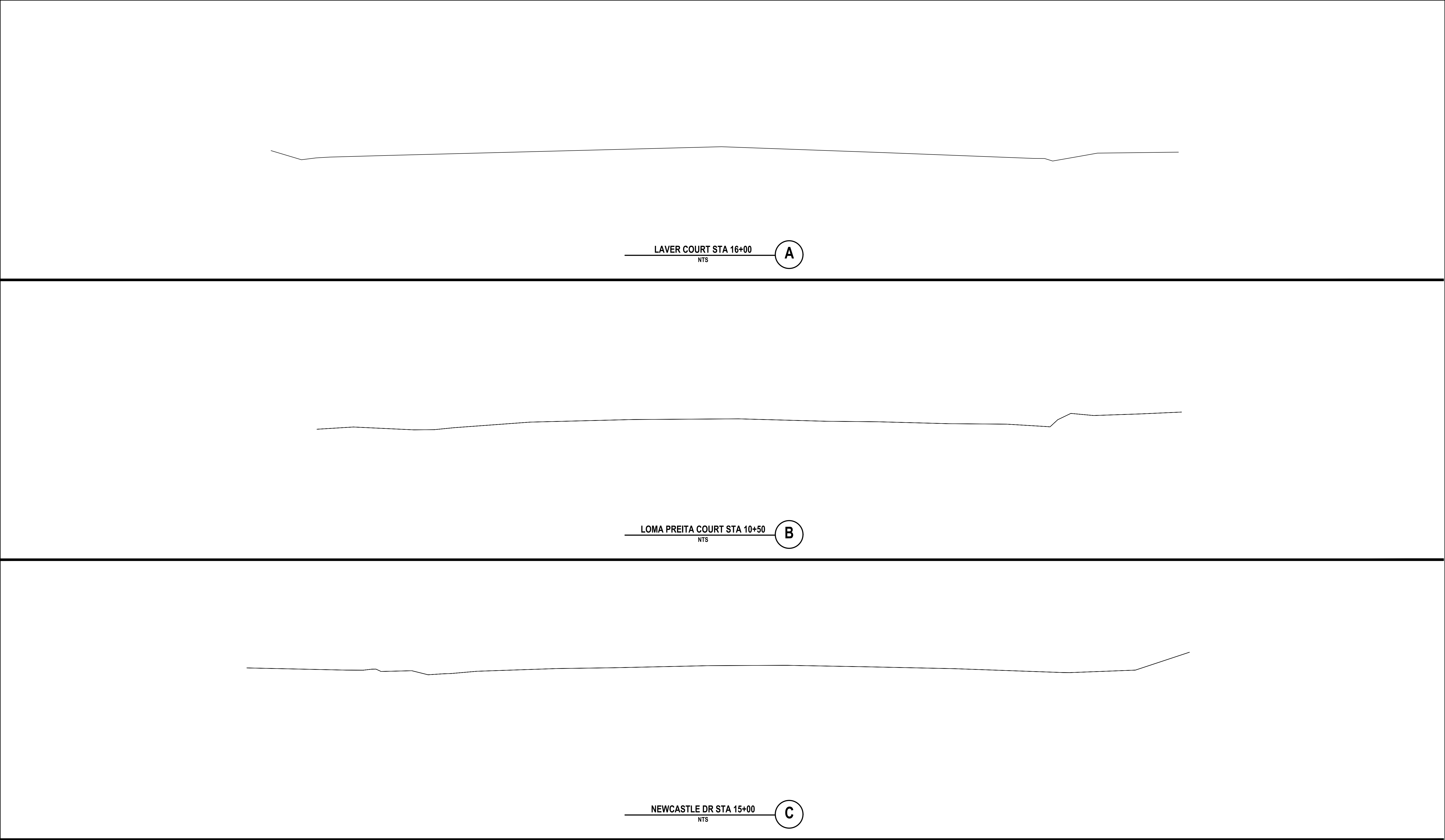
BY	DATE	REVISIONS

AMELLIA COURT STA 11+00

NTS

**B**

JAY STREET STA 16+00



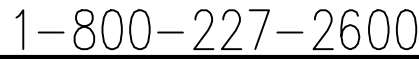
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APRIL 07, 2025

DWG NO. C3.2 4 OF 50 JOB NO. 250007	2024-2025 ANNUAL STREET RESURFACING PROJECT  TYPICAL SECTION	LOS ALTOS  SANTA CLARA COUNTY  CALIFORNIA	 CITY OF LOS ALTOS 1 NORTH SAN ANTONIO ROAD CALIFORNIA 94022							
BY			DATE	REVISIONS						





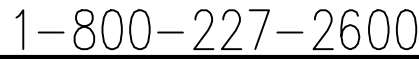
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**JOB NO.**

CAIFORNIA







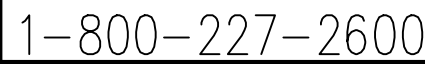
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250007

CALIFORNIA







65% SUBMITTAL  
APRIL 07, 2025

**D**

**C**

**B**

**A**



TYPICAL SECTION

SANTA CLARA COUNTY

CALIFORNIA

CITY OF LOS ALTOS  
11 NORTH SAN ANTONIO ROAD  
CALIFORNIA 94022



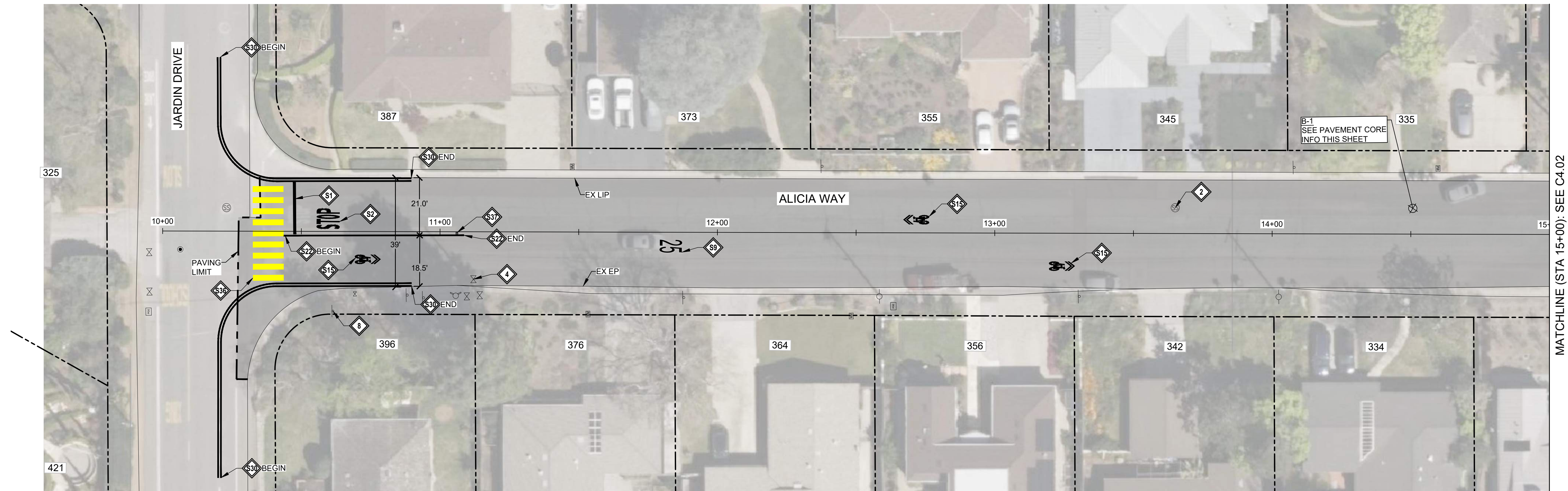
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BY	DATE
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## REVISIONS





## PAVEMENT REPAIR SITE - ALICIA WAY

[illegible]

Call Two Working  
Days Before You  
Dig!

1-800-227-2600

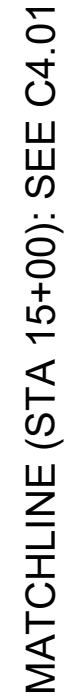
DWG NO.

C4.01

8 OF 50

**JOB NO.**



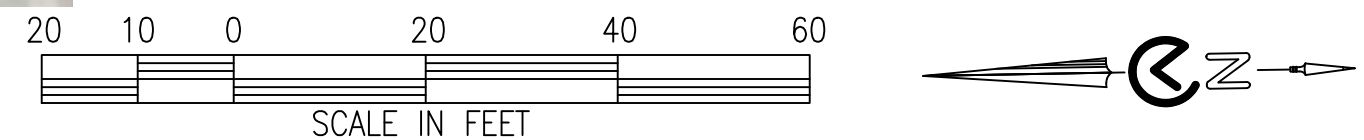




MATCHLINE (STA 21+00): SEE C4.03

65% SUBMITTAL  
APRIL 07, 2025

DWG NO.	2024-2025 ANNUAL STREET RESURFACING PROJECT											
C4.02	IMPROVEMENT PLAN - ALICIA WAY											
9 OF 50	15+00 to 21+00											
JOB NO.	LOS ALTOS	SANTA CLARA COUNTY	CALIFORNIA									
250007												





DWG NO.	2024-2025 ANNUAL STREET RESURFACING PROJECT	
C4.03	IMPROVEMENT PLAN - ALICIA WAY 21+00 to 23+39	
10 OF 50		
JOB NO. 250007	LOS ALTOS	SANTA CLARA COUNTY CALIFORNIA
 <p>CITY OF LOS ALTOS 1 NORTH SAN ANTONIO ROAD CALIFORNIA 94022</p>		
 <p>sanbell.com 925 • 685 • 4569</p>		
	BY	DATE
		REVISIONS



Call Two Working  
Days Before You  
Dig!



1-800-227-2600

CONSTRUCTION NOTES:

- 1 LOWER & RESTORE TO GRADE STORM MANHOLE
- 2 LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- 3 LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)
- 4 LOWER & RESTORE TO GRADE WATER VALVE
- 5 LOWER & RESTORE TO GRADE GAS VALVE
- 6 LOWER & RESTORE TO GRADE UTILITY BOX
- 7 REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR:
  - REFERENCE OUT PRIOR TO START OF CONSTRUCTION
  - FILE PROPER DOCUMENTATION SHOWING ORIGINAL AND REPLACEMENT MONUMENTS AFTER CONSTRUCTION

SEE DETAIL X CX.X
- 8 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
- 9 LOWER & RESTORE TO GRADE SEWER CLEANOUT

LEGEND:

- 3" AC GRIND & INLAY. SEE TYPICAL SECTIONS
- #

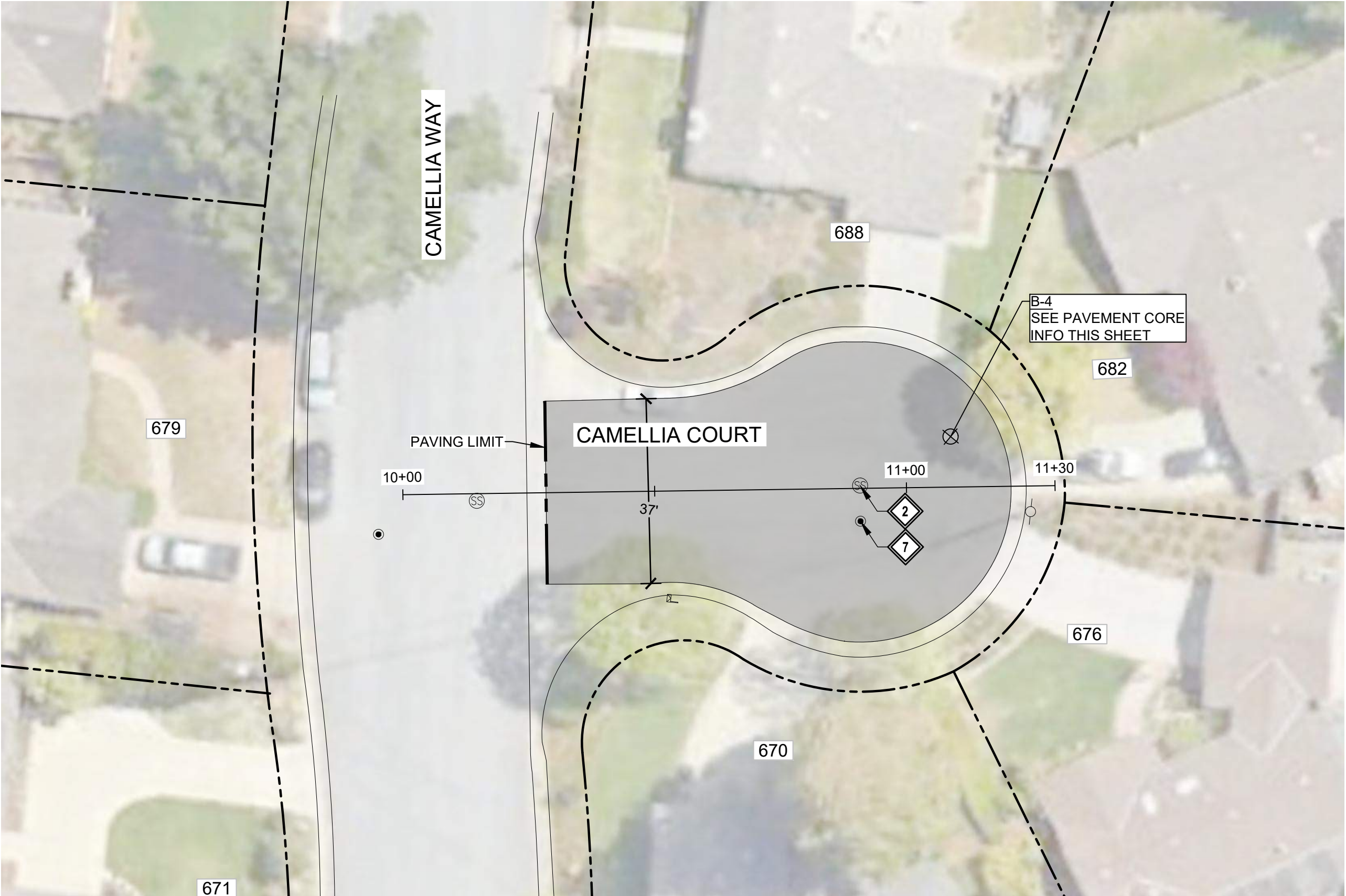
4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL
- X XX.X
- APPROXIMATE LIMIT OF WORK
- APPROXIMATE RIGHT OF WAY

PAVEMENT CORING LOG

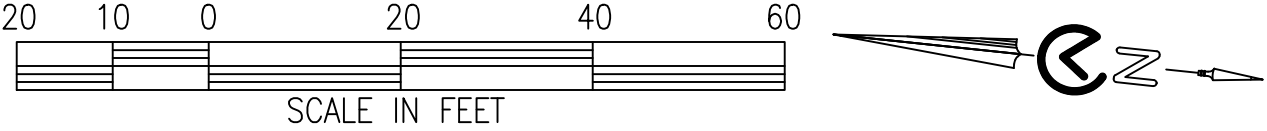
CORE #	SUBGRADE	AC (IN)	AB (IN)	FABRIC <sup>2</sup>
B-4	SILTY SAND / SAND WITH SILT (SM/SP)	3.5	5.0	N/A

PAVEMENT CORING NOTES

1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.



PAVEMENT REPAIR SITE - CAMELLIA COURT



GENERAL NOTES:

1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
2. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

PAVEMENT REPAIR QUANTITY NOTES

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

DWG NO.  
C4.04  
11 OF 50  
JOB NO.  
250007

65% SUBMITTAL  
APRIL 07, 2025

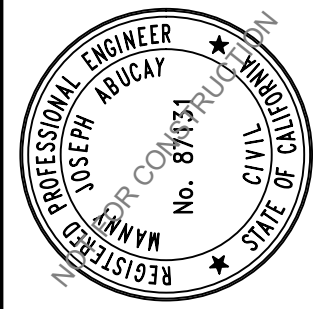
2024-2025 ANNUAL STREET RESURFACING PROJECT

IMPROVEMENT PLAN - CAMELLIA COURT  
10+00 to 11+30

SANTA CLARA COUNTY

LOS ALTOS

CALIFORNIA



CITY OF LOS ALTOS  
1 NORTH SAN ANTONIO ROAD  
CALIFORNIA 94022

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REVISIONS	BY	DATE









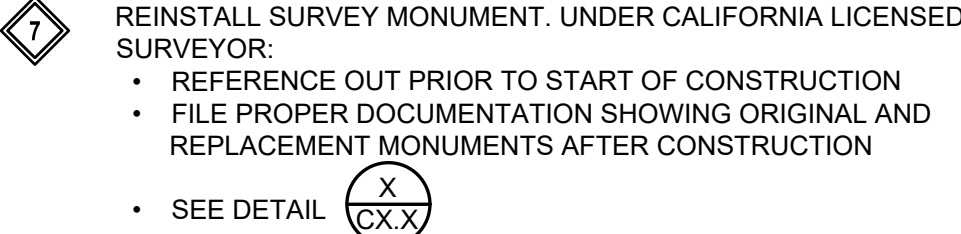

























CONSTRUCTION NOTES:

-  LOWER & RESTORE TO GRADE STORM MANHOLE
-  LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
-  LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)
-  LOWER & RESTORE TO GRADE WATER VALVE
-  LOWER & RESTORE TO GRADE GAS VALVE
-  LOWER & RESTORE TO GRADE UTILITY BOX
-  REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR:
  - REFERENCE OUT PRIOR TO START OF CONSTRUCTION
  - FILE PROPER DOCUMENTATION SHOWING ORIGINAL AND REPLACEMENT MONUMENTS AFTER CONSTRUCTION
  - SEE DETAIL 
-  INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
-  LOWER & RESTORE TO GRADE SEWER CLEANOUT

STRIPING NOTES:

-  WHITE LIMIT LINE (STOP LINE)
-  MARKING WORD "STOP"
-  MARKING WORD "AHEAD"
-  MARKING "25"
-  MARKING ARROW TYPE IV (R)
-  DETAIL 22
-  YELLOW CONTINENTAL CROSSWALK
-  BLUE HYDRANT MARKER
-  REPAINT WHITE CURB

LEGEND:

-  3" AC GRIND & INLAY. SEE TYPICAL SECTIONS
-  4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL 
-  APPROXIMATE LIMIT OF WORK
-  APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

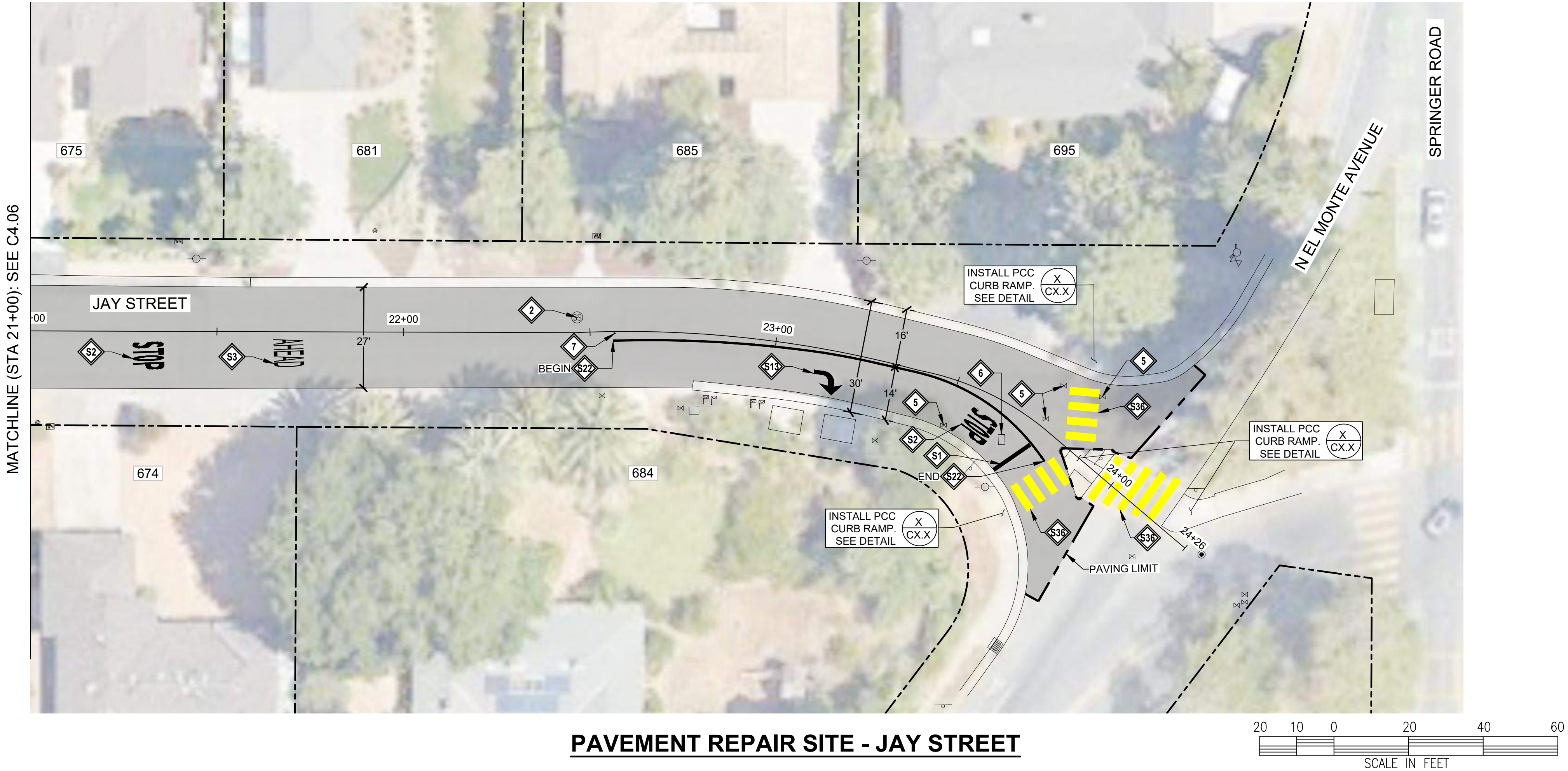
- SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

PAVEMENT REPAIR QUANTITY NOTES

- ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.







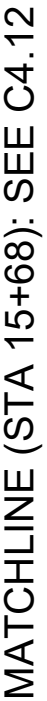














65% SUBMITTAL  
APRIL 07, 2025

DWG NO.	2024-2025 ANNUAL STREET RESURFACING PROJECT				CITY OF LOS ALTOS 1 NORTH SAN ANTONIO ROAD CALIFORNIA 94022		925 • 685 • 4569 sanbell.com	BY	DATE	REVISIONS		
C4.11	IMPROVEMENT PLAN - NEWCASTLE DRIVE											
18 OF 50	10+00 to 15+68											
JOB NO.	LOS ALTOS			SANTA CLARA COUNTY	CALIFORNIA							
250007												





















## PAVEMENT REPAIR SITE - NEWCASTLE DRIVE


### CONSTRUCTION NOTES:


- 1 LOWER & RESTORE TO GRADE STORM MANHOLE
- 2 LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
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  - REFERENCE OUT PRIOR TO START OF CONSTRUCTION
  - FILE PROPER DOCUMENTATION SHOWING ORIGINAL AND REPLACEMENT MONUMENTS AFTER CONSTRUCTION
- 8 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
- 9 LOWER & RESTORE TO GRADE SEWER CLEANOUT


STRIPING NOTES:


- |   |   |   |
|---|---|---|
|  | 51 WHITE LIMIT LINE (STOP LINE)                                     |   |
|  | 52 MARKING WORD "STOP"  |   |
|  | 53 MARKING WORD "AHEAD"   |   |
|  | 55 MARKING WORD "XING"  |   |
|  | 59 MARKING "25"   |   |
|  | 516 MARKING BIKE LANE SYMBOL WITH ARROW & GREEN BACKING. SEE DETAIL |  |
|  | 517 GREEN DASHED BIKE LANE. SEE DETAIL                              |    |
|  | 519 DETAIL 2  |   |
|  | 522 DETAIL 22   |   |
|  | 527 DETAIL 39   |   |
|  | 532 WHITE BASIC CROSSWALK   |   |
|  | 535 WHITE CONTINENTAL CROSSWALK                                     |   |
|  | 537 BLUE HYDRANT MARKER   |   |
|  | 539 MARKING WORD "PED"  |   |


LEGEND:

-  3" AC GRIND & INLAY. SEE TYPICAL SECTIONS

 4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL



 APPROXIMATE LIMIT OF WORK

 APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
2. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

## AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

## PAVEMENT REPAIR QUANTITY NOTES



9. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

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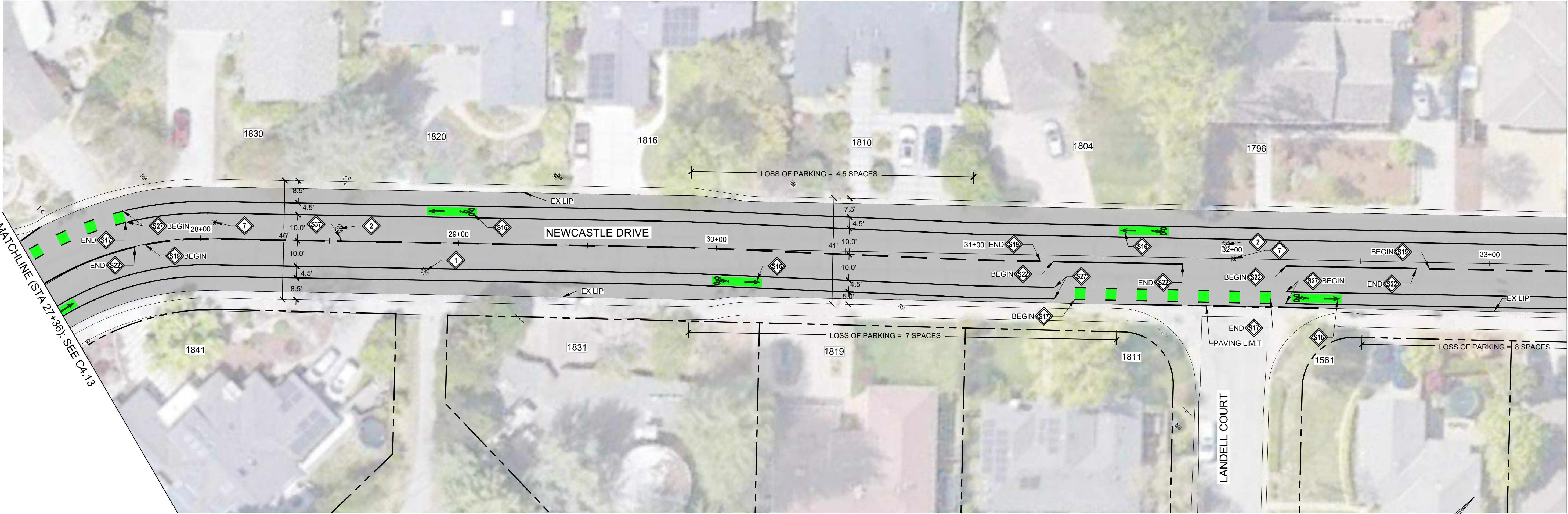
65% SUBMITTAL  
APRIL 07, 2025

DWG NO.	2024-2025 ANNUAL STREET RESURFACING PROJECT			<p>CITY OF LOS ALTOS 1 NORTH SAN ANTONIO ROAD CALIFORNIA 94022</p>	 <p>Intelligent Infrastructure Enduring Communities 925 • 685 • 4569 sanbell.com</p>							
C4.12	IMPROVEMENT PLAN - NEWCASTLE DRIVE 15+68 to 21+35											
19 OF 50												
JOB NO.	LOS ALTOS		SANTA CLARA COUNTY	CALIFORNIA		BY	DATE	REVISIONS				
250007												









PAVEMENT REPAIR SITE - NEWCASTLE DRIVE

CONSTRUCTION NOTES:

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- 8 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
- 9 LOWER & RESTORE TO GRADE SEWER CLEANOUT

STRIPING NOTES:

- S1 WHITE LIMIT LINE (STOP LINE)
- S2 MARKING WORD "STOP"
- S3 MARKING WORD "AHEAD"
- S5 MARKING WORD "XING"
- S9 MARKING "25"
- S16 MARKING BIKE LANE SYMBOL WITH ARROW & GREEN BACKING. SEE DETAIL
- S17 GREEN DASHED BIKE LANE. SEE DETAIL
- S19 DETAIL 2
- S22 DETAIL 22
- S27 DETAIL 39
- S32 WHITE BASIC CROSSWALK
- S35 WHITE CONTINENTAL CROSSWALK
- S37 BLUE HYDRANT MARKER
- S38 MARKING WORD "PED"

LEGEND:

- 3" AC GRIND & INLAY. SEE TYPICAL SECTIONS
- 4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL
- APPROXIMATE LIMIT OF WORK
- APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

PAVEMENT REPAIR QUANTITY NOTES

- ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

Call Two Working Days Before You Dig!



1-800-227-2600

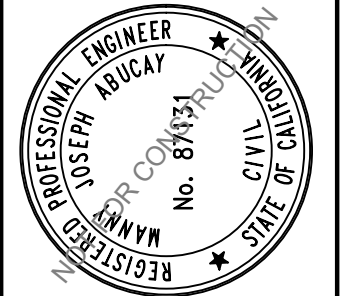
65% SUBMITTAL  
APRIL 07, 2025

MATCHLINE (STA 33+30): SEE C4.15

BY	DATE	REVISIONS



CITY OF LOS ALTOS  
1 NORTH SAN ANTONIO ROAD  
CALIFORNIA 94022

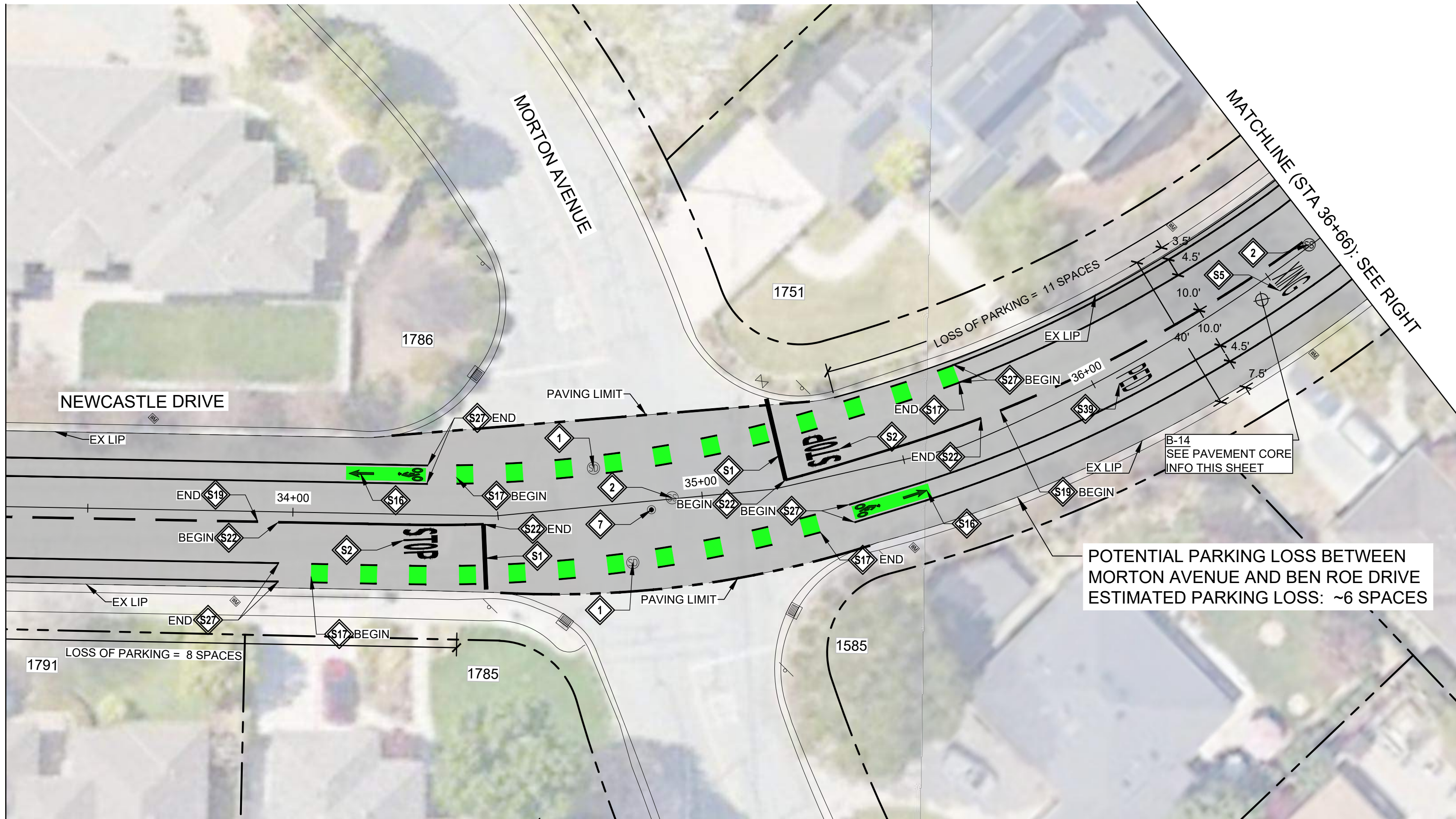


2024-2025 ANNUAL STREET RESURFACING PROJECT  
IMPROVEMENT PLAN - NEWCASTLE DRIVE  
27+36 to 33+30  
LOS ALTOS  
SANTA CLARA COUNTY  
CALIFORNIA

DWG NO. C4.14  
21 OF 50  
JOB NO. 250007

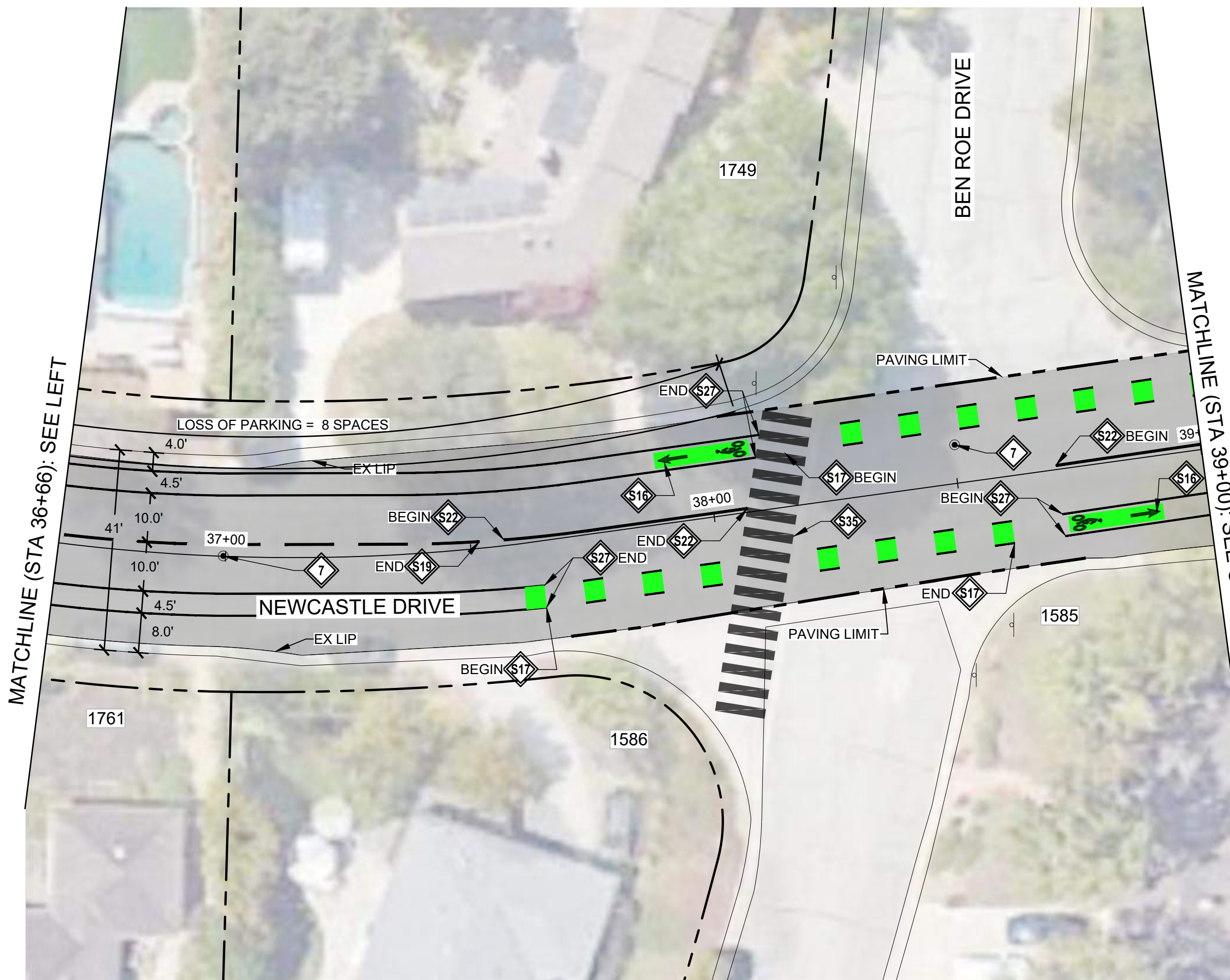


MATCHLINE (STA 33+30): SEE C4.14



POTENTIAL PARKING LOSS BETWEEN  
MORTON AVENUE AND BEN ROE DRIVE  
ESTIMATED PARKING LOSS: ~6 SPACES

### PAVEMENT REPAIR SITE - NEWCASTLE DRIVE



#### CONSTRUCTION NOTES:

- 1 LOWER & RESTORE TO GRADE STORM MANHOLE
- 2 LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- 3 LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)
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- 5 LOWER & RESTORE TO GRADE GAS VALVE
- 6 LOWER & RESTORE TO GRADE UTILITY BOX
- 7 REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR:
  - REFERENCE OUT PRIOR TO START OF CONSTRUCTION
  - FILE PROPER DOCUMENTATION SHOWING ORIGINAL AND REPLACEMENT MONUMENTS AFTER CONSTRUCTION
- 8 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
- 9 LOWER & RESTORE TO GRADE SEWER CLEANOUT

#### STRIPING NOTES:

- \$1 WHITE LIMIT LINE (STOP LINE)
- \$2 MARKING WORD "STOP"
- \$3 MARKING WORD "AHEAD"
- \$5 MARKING WORD "XING"
- \$9 MARKING "25"
- \$16 MARKING BIKE LANE SYMBOL WITH ARROW & GREEN BACKING. SEE DETAIL
- \$17 GREEN DASHED BIKE LANE. SEE DETAIL
- \$19 DETAIL 2
- \$22 DETAIL 22
- \$27 DETAIL 39
- \$32 WHITE BASIC CROSSWALK
- \$35 WHITE CONTINENTAL CROSSWALK
- \$37 BLUE HYDRANT MARKER
- \$38 MARKING WORD "PED"

#### LEGEND:

- 3" AC GRIND & INLAY. SEE TYPICAL SECTIONS
- 4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL
- APPROXIMATE LIMIT OF WORK
- APPROXIMATE RIGHT OF WAY

#### PAVEMENT CORING LOG

CORE #	SUBGRADE	AC (IN)	AB (IN)	FABRIC <sup>2</sup>
B-14	SAND WITH SILT AND GRAVEL (SW)	6.0	2.0	N/A

#### PAVEMENT CORING NOTES

1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

#### GENERAL NOTES:

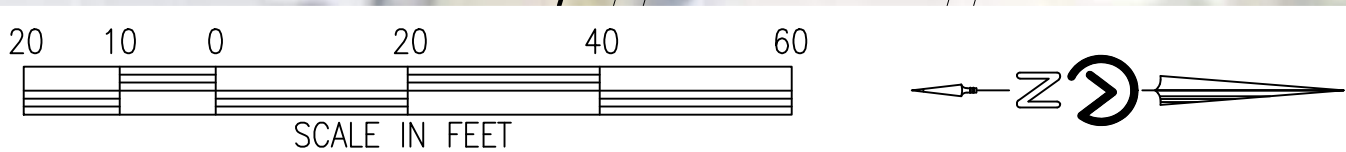
1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
2. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

#### AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

#### PAVEMENT REPAIR QUANTITY NOTES

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.



Call Two Working  
Days Before You  
Dig!



1-800-227-2600

65% SUBMITTAL  
APRIL 07, 2025

2024-2025 ANNUAL STREET RESURFACING PROJECT

IMPROVEMENT PLAN - NEWCASTLE DRIVE  
33+30 to 39+00

LOS ALTOS

CITY OF LOS ALTOS  
1 NORTH SAN ANTONIO ROAD  
CALIFORNIA 94022

Professional Engineer  
JOSEPH ARICAR  
No. 87131  
REGISTERED  
CIVIL  
STATE OF CA

2024-2025 ANNUAL STREET RESURFACING PROJECT

LOS ALTOS

SANTA CLARA COUNTY

CALIFORNIA

DWG NO.  
C4.15  
22 OF 50  
JOB NO.  
250007

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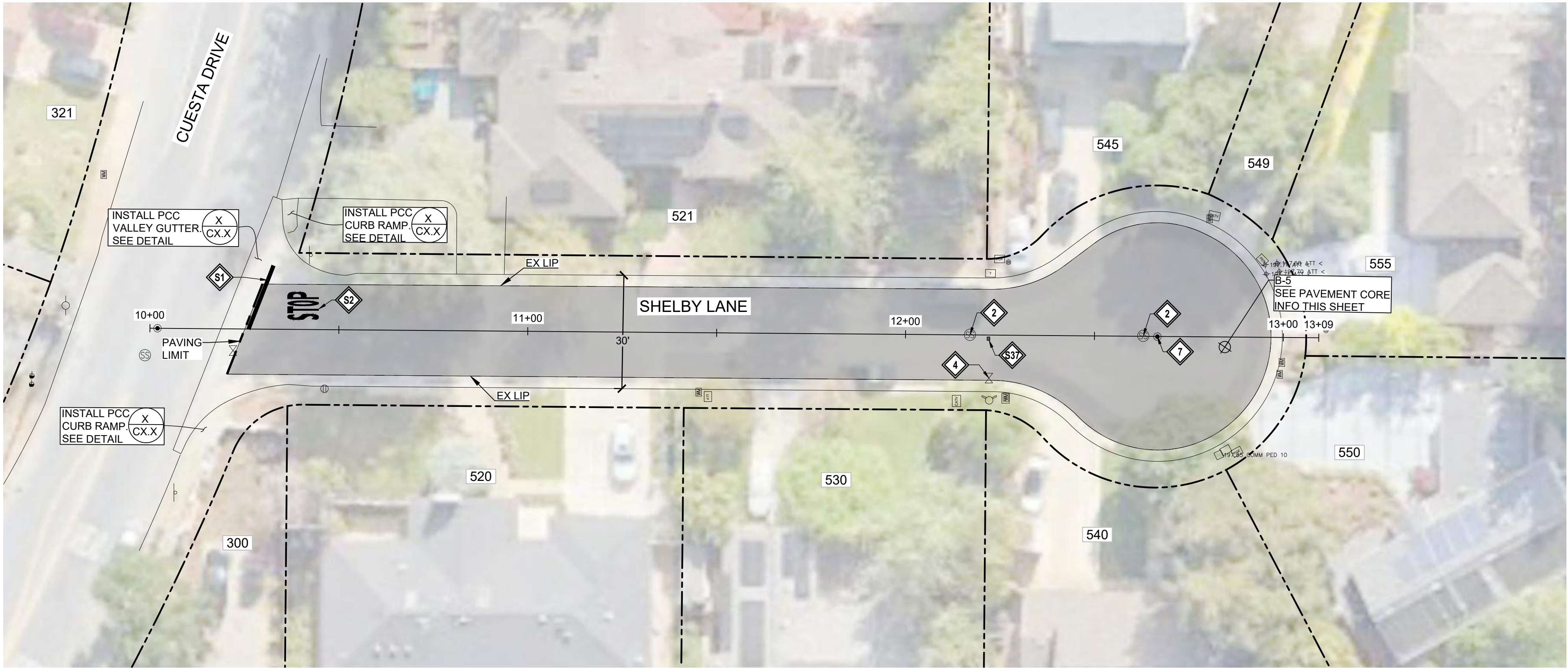












PAVEMENT REPAIR SITE - SHELBY LANE

CONSTRUCTION NOTES:

- 1 LOWER & RESTORE TO GRADE STORM MANHOLE
- 2 LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- 3 LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)
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  - REFERENCE OUT PRIOR TO START OF CONSTRUCTION
  - FILE PROPER DOCUMENTATION SHOWING ORIGINAL AND REPLACEMENT MONUMENTS AFTER CONSTRUCTION
- 8 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
- 9 LOWER & RESTORE TO GRADE SEWER CLEANOUT

STRIPING NOTES:

- S1 WHITE LIMIT LINE (STOP LINE)
- S2 WHITE "STOP" MARKING
- S37 BLUE HYDRANT MARKER

LEGEND:

- 3" AC GRIND & INLAY. SEE TYPICAL SECTIONS
- 4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL
- APPROXIMATE LIMIT OF WORK
- APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

PAVEMENT REPAIR QUANTITY NOTES

- ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING LOG

CORE #	SUBGRADE	AC (IN)	AB (IN)	FABRIC <sup>2</sup>
B-5	SILTY SAND (SM)	1.0	3.0	N/A

PAVEMENT CORING NOTES

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Call Two Working Days Before You Dig!



1-800-227-2600

65% SUBMITTAL  
APRIL 07, 2025

2024-2025 ANNUAL STREET RESURFACING PROJECT

IMPROVEMENT PLAN - ALICIA WAY

10+00 to 13+09

LOS ALTOS

SANTA CLARA COUNTY

CALIFORNIA

DWG NO. C4.18

25 OF 50

JOB NO. 250007

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CITY OF LOS ALTOS

1 NORTH SAN ANTONIO ROAD

CALIFORNIA 94022

REVISIONS

DATE

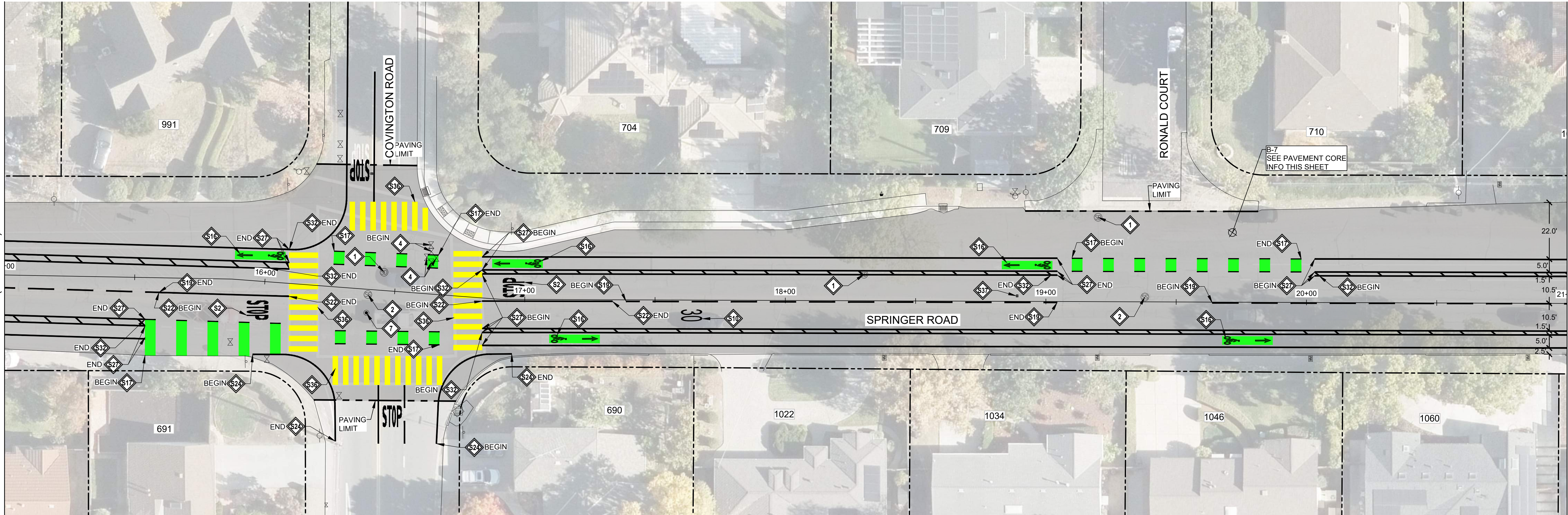
BY





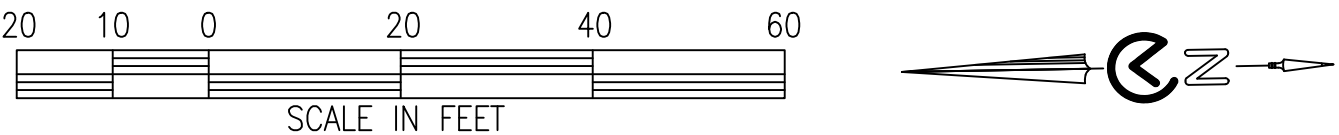


MATCHLINE (STA 15+00): SEE C4.19



MATCHLINE (STA 21+00): SEE C4.21

## SPRINGER ROAD REPAIR



### CONSTRUCTION NOTES:

- 1 LOWER & RESTORE TO GRADE STORM MANHOLE
- 2 LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- 3 LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)
- 4 LOWER & RESTORE TO GRADE WATER VALVE
- 5 LOWER & RESTORE TO GRADE GAS VALVE
- 6 LOWER & RESTORE TO GRADE UTILITY BOX
- 7 REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR:
  - REFERENCE OUT PRIOR TO START OF CONSTRUCTION
  - FILE PROPER DOCUMENTATION SHOWING ORIGINAL AND REPLACEMENT MONUMENTS AFTER CONSTRUCTION
- 8 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
- 9 LOWER & RESTORE TO GRADE SEWER CLEANOUT

### STRIPING NOTES:

- \$1 WHITE LIMIT LINE (STOP LINE)
- \$2 MARKING WORD "STOP"
- \$3 MARKING WORD "AHEAD"
- \$4 MARKING WORD "ONLY"
- \$10 MARKING "30"
- \$11 MARKING ARROW TYPE I
- \$12 MARKING ARROW TYPE IV (L)
- \$13 MARKING ARROW TYPE IV (R)
- \$15 MARKING SHARROW
- \$16 MARKING BIKE LANE SYMBOL. SEE DETAIL
- \$17 GREEN DASHED BIKE LANE. SEE DETAIL
- \$18 GREEN BACK
- \$19 DETAIL 2
- \$20 DETAIL 8
- \$21 DETAIL 15

### STRIPING NOTES:

- \$22 DETAIL 22
- \$23 DETAIL 25
- \$24 DETAIL 27B
- \$25 DETAIL 37B
- \$26 DETAIL 38
- \$27 DETAIL 39
- \$28 DETAIL 39A
- \$29 DETAIL 40
- \$31 6" DIAGONAL YELLOW STRIPE
- \$32 4" DIAGONAL WHITE STRIPE
- \$33 WHITE LADDER CROSSWALK
- \$36 YELLOW CONTINENTAL CROSSWALK
- \$37 BLUE HYDRANT MARKER
- \$44 36" HIGH YELLOW FLEXIBLE CHANNELIZER
- \$45 MARKING ARROW TYPE VII

### LEGEND:

- 3" AC GRIND & INLAY. SEE TYPICAL SECTIONS
- 4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL
- APPROXIMATE LIMIT OF WORK
- APPROXIMATE RIGHT OF WAY

### PAVEMENT CORING LOG

CORE #	SUBGRADE	AC (IN)	AB (IN)	FABRIC <sup>2</sup>
B-7	SAND WITH GRAVEL / GRAVELLY SAND (SWG/C)	5.0	3.0	1.5" DEPTH

### PAVEMENT CORING NOTES

1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

### GENERAL NOTES:

1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
2. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

### AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

### PAVEMENT REPAIR QUANTITY NOTES

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

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APRIL 07, 2025

2024-2025 ANNUAL STREET RESURFACING PROJECT

STREET IMPROVEMENT PLAN - SPRINGER ROAD  
15+00 to 21+00

DWG NO. C4.20  
27 OF 50  
JOB NO. 250007

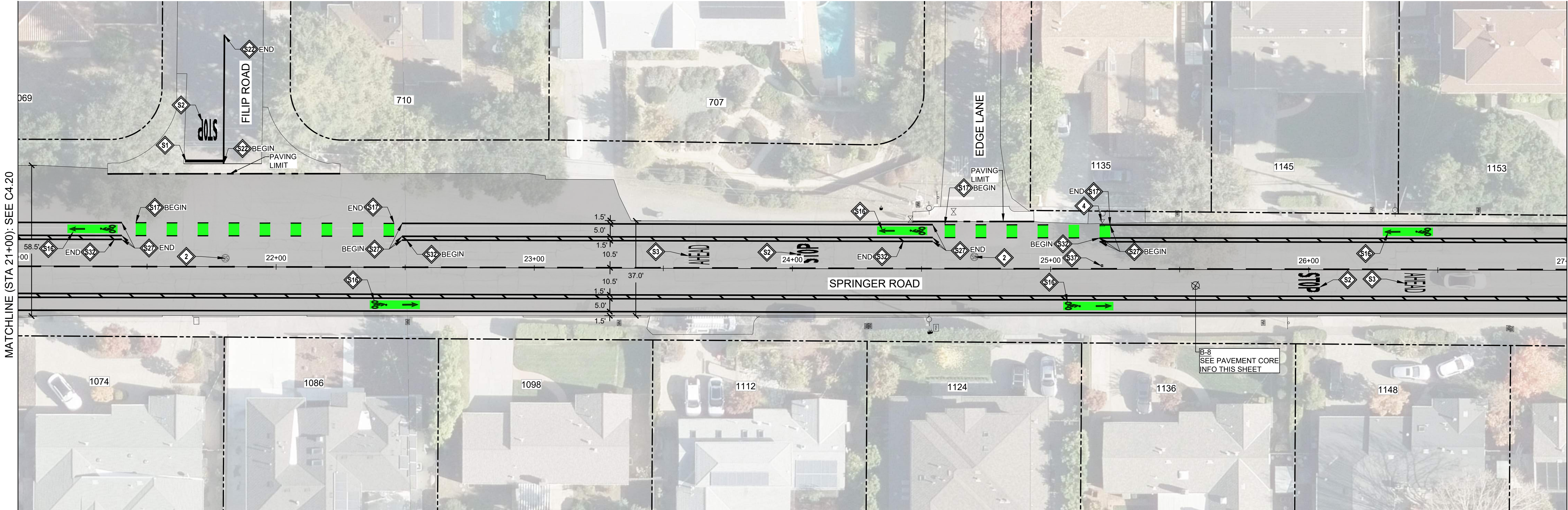
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1 NORTH SAN ANTONIO ROAD  
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## SPRINGER ROAD REPAIR

### CONSTRUCTION NOTES:

- 1 LOWER & RESTORE TO GRADE STORM MANHOLE
- 2 LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- 3 LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)
- 4 LOWER & RESTORE TO GRADE WATER VALVE
- 5 LOWER & RESTORE TO GRADE GAS VALVE
- 6 LOWER & RESTORE TO GRADE UTILITY BOX
- 7 REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR:
  - REFERENCE OUT PRIOR TO START OF CONSTRUCTION
  - FILE PROPER DOCUMENTATION SHOWING ORIGINAL AND REPLACEMENT MONUMENTS AFTER CONSTRUCTION
- 8 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
- 9 LOWER & RESTORE TO GRADE SEWER CLEANOUT

### STRIPING NOTES:

- S1 WHITE LIMIT LINE (STOP LINE)
- S2 MARKING WORD "STOP"
- S3 MARKING WORD "AHEAD"
- S4 MARKING WORD "ONLY"
- S10 MARKING "30"
- S11 MARKING ARROW TYPE I
- S12 MARKING ARROW TYPE IV (L)
- S13 MARKING ARROW TYPE IV (R)
- S15 MARKING SHARROW
- S16 MARKING BIKE LANE SYMBOL. SEE DETAIL
- S17 GREEN DASHED BIKE LANE. SEE DETAIL
- S18 GREEN BACK
- S19 DETAIL 2
- S20 DETAIL 8
- S21 DETAIL 15

### STRIPING NOTES:

- S22 DETAIL 22
- S23 DETAIL 25
- S24 DETAIL 27B
- S25 DETAIL 37B
- S26 DETAIL 38
- S27 DETAIL 39
- S28 DETAIL 39A
- S29 DETAIL 40
- S31 6" DIAGONAL YELLOW STRIPE
- S32 4" DIAGONAL WHITE STRIPE
- S33 WHITE LADDER CROSSWALK
- S36 YELLOW CONTINENTAL CROSSWALK
- S37 BLUE HYDRANT MARKER
- S44 36" HIGH YELLOW FLEXIBLE CHANNELIZER
- S45 MARKING ARROW TYPE VII

### LEGEND:

- 3" AC GRIND & INLAY. SEE TYPICAL SECTIONS
- 4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL
- APPROXIMATE LIMIT OF WORK
- APPROXIMATE RIGHT OF WAY

### PAVEMENT CORING LOG

CORE #	SUBGRADE	AC (IN)	AB (IN)	FABRIC <sup>2</sup>
B-8	SAND WITH GRAVEL (SP/SW)	7.0	0.0	N/A

### PAVEMENT CORING NOTES

- PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
- PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

### GENERAL NOTES:

- SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

### AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

### PAVEMENT REPAIR QUANTITY NOTES

- ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

MATCHLINE (STA 21+00): SEE C4.20

MATCHLINE (STA 27+00): SEE C4.22

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2024-2025 ANNUAL STREET RESURFACING PROJECT

STREET IMPROVEMENT PLAN - SPRINGER ROAD  
21+00 TO 27+00

DWG NO. C4.21  
28 OF 50  
JOB NO. 250007

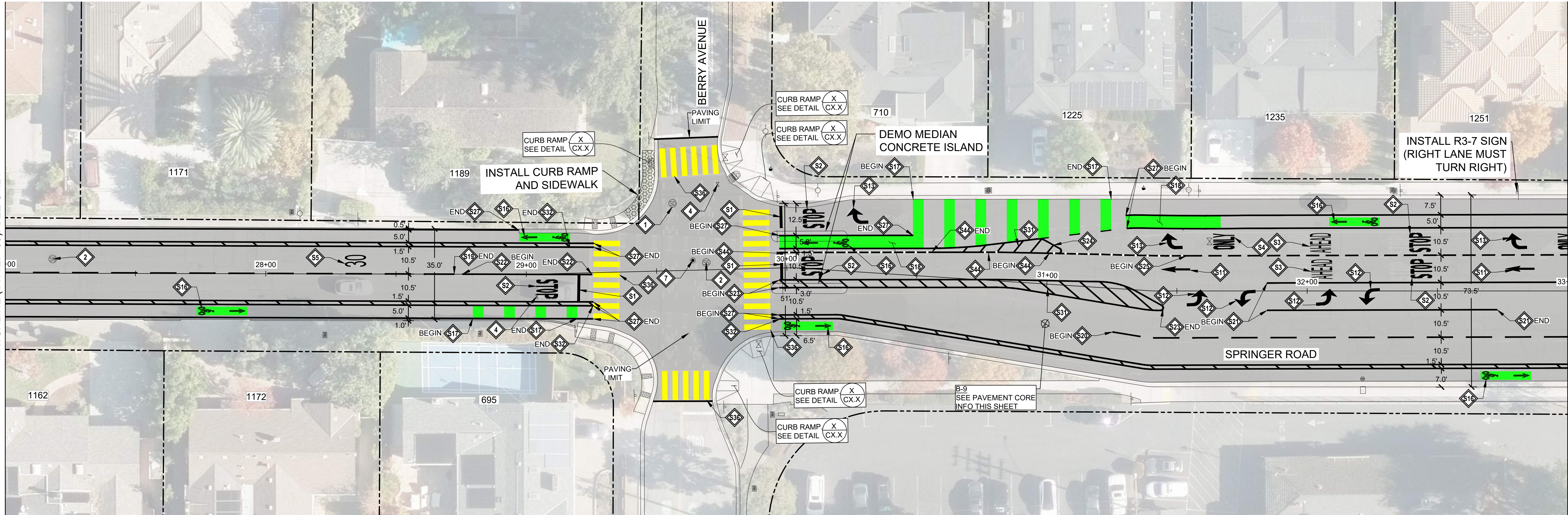
CITY OF LOS ALTOS  
1 NORTH SAN ANTONIO ROAD  
CALIFORNIA 94022

SANTA CLARA COUNTY  
LOS ALTOS

REVISIONS

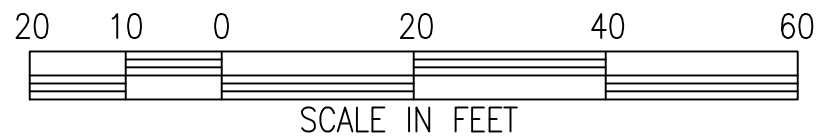


MATCHLINE (STA 27+00): SEE C4.21



MATCHLINE (STA 33+00): SEE C4.23

## SPRINGER ROAD REPAIR



### CONSTRUCTION NOTES:

- 1 LOWER & RESTORE TO GRADE STORM MANHOLE
- 2 LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- 3 LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)
- 4 LOWER & RESTORE TO GRADE WATER VALVE
- 5 LOWER & RESTORE TO GRADE GAS VALVE
- 6 LOWER & RESTORE TO GRADE UTILITY BOX
- 7 REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR:
  - REFERENCE OUT PRIOR TO START OF CONSTRUCTION
  - FILE PROPER DOCUMENTATION SHOWING ORIGINAL AND REPLACEMENT MONUMENTS AFTER CONSTRUCTION
- 8 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
- 9 LOWER & RESTORE TO GRADE SEWER CLEANOUT

### STRIPING NOTES:

- \$1 WHITE LIMIT LINE (STOP LINE)
- \$2 MARKING WORD "STOP"
- \$3 MARKING WORD "AHEAD"
- \$4 MARKING WORD "ONLY"
- \$10 MARKING "30"
- \$11 MARKING ARROW TYPE I
- \$12 MARKING ARROW TYPE IV (L)
- \$13 MARKING ARROW TYPE IV (R)
- \$15 MARKING SHARROW
- \$16 MARKING BIKE LANE SYMBOL. SEE DETAIL
- \$17 GREEN DASHED BIKE LANE. SEE DETAIL
- \$18 GREEN BACK
- \$19 DETAIL 2
- \$20 DETAIL 8
- \$21 DETAIL 15

### STRIPING NOTES:

- \$22 DETAIL 22
- \$23 DETAIL 25
- \$24 DETAIL 27B
- \$25 DETAIL 37B
- \$26 DETAIL 38
- \$27 DETAIL 39
- \$28 DETAIL 39A
- \$29 DETAIL 40
- \$31 6" DIAGONAL YELLOW STRIPE
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- \$33 WHITE LADDER CROSSWALK
- \$36 YELLOW CONTINENTAL CROSSWALK
- \$37 BLUE HYDRANT MARKER
- \$44 36" HIGH YELLOW FLEXIBLE CHANNELIZER
- \$45 MARKING ARROW TYPE VII

### LEGEND:

- 3" AC GRIND & INLAY. SEE TYPICAL SECTIONS
- 4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL
- APPROXIMATE LIMIT OF WORK
- APPROXIMATE RIGHT OF WAY

### PAVEMENT CORING LOG

CORE #	SUBGRADE	AC (IN)	AB (IN)	FABRIC <sup>2</sup>
B-9	SAND WITH GRAVEL (SP/SW)	6.0	0.0	N/A

### PAVEMENT CORING NOTES

1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
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### GENERAL NOTES:

1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
2. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

### AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

### PAVEMENT REPAIR QUANTITY NOTES

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

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2024-2025 ANNUAL STREET RESURFACING PROJECT

STREET IMPROVEMENT PLAN - SPRINGER ROAD  
27+00 TO 33+00

DWG NO. C4.22  
29 OF 50  
JOB NO. 250007

CITY OF LOS ALTOS  
1 NORTH SAN ANTONIO ROAD  
CALIFORNIA 94022

CALIFORNIA

SANTA CLARA COUNTY

LOS ALTOS

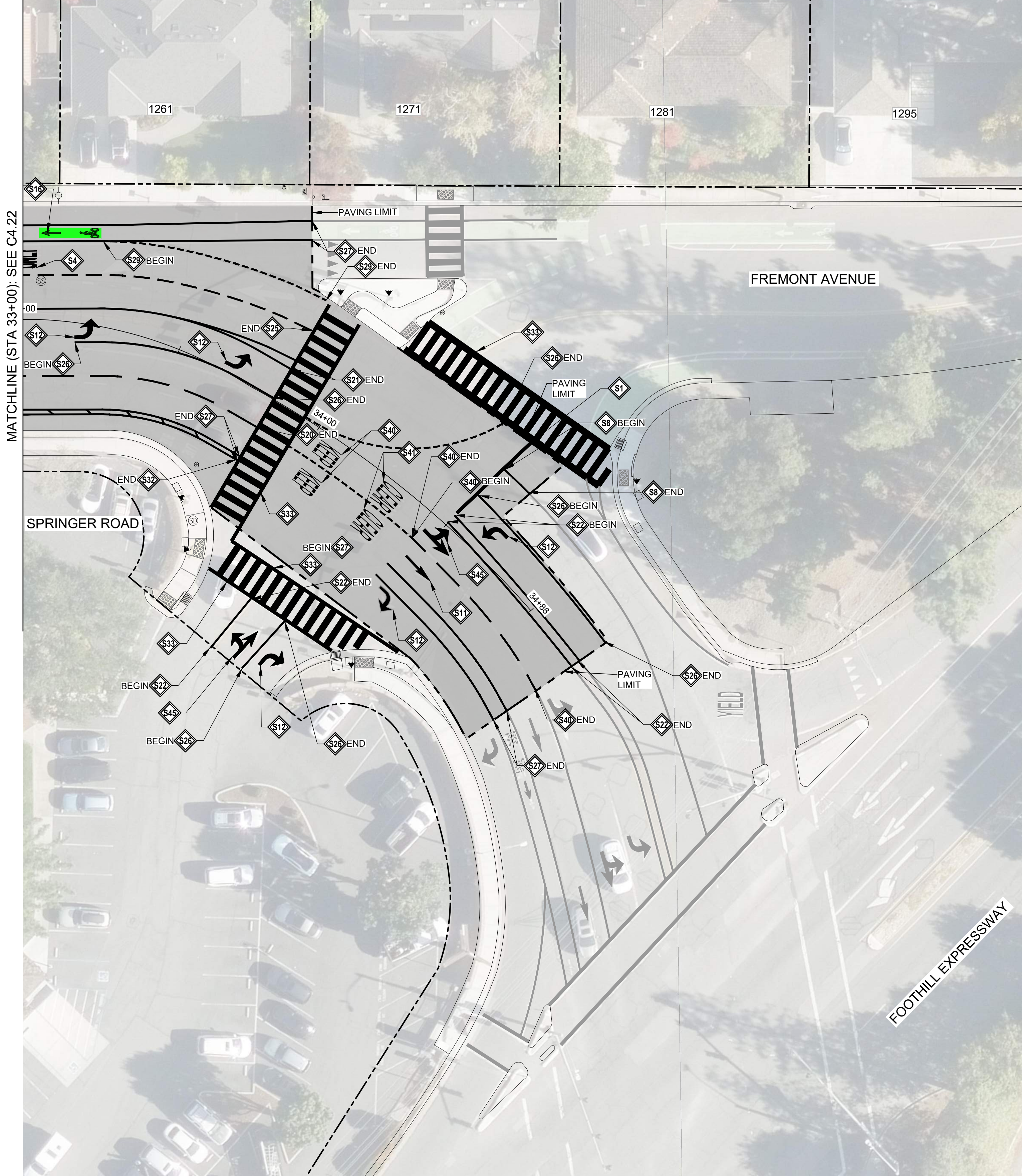
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LEGEND:

- 3" AC GRIND & INLAY. SEE TYPICAL SECTIONS
- 4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL
- APPROXIMATE LIMIT OF WORK
- APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

PAVEMENT REPAIR QUANTITY NOTES

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

CONSTRUCTION NOTES:

- LOWER & RESTORE TO GRADE STORM MANHOLE
- LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
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- LOWER & RESTORE TO GRADE WATER VALVE
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- LOWER & RESTORE TO GRADE UTILITY BOX
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  - REFERENCE OUT PRIOR TO START OF CONSTRUCTION
  - FILE PROPER DOCUMENTATION SHOWING ORIGINAL AND REPLACEMENT MONUMENTS AFTER CONSTRUCTION
  - SEE DETAIL
- INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
- LOWER & RESTORE TO GRADE SEWER CLEANOUT

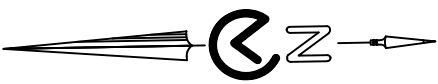
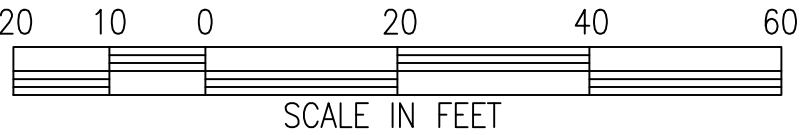
STRIPING NOTES:

- WHITE LIMIT LINE (STOP LINE)
- MARKING WORD "STOP"
- MARKING WORD "AHEAD"
- MARKING WORD "ONLY"
- MARKING "30"
- MARKING ARROW TYPE I
- MARKING ARROW TYPE IV (L)
- MARKING ARROW TYPE IV (R)
- MARKING SHARROW
- MARKING BIKE LANE SYMBOL. SEE DETAIL
- GREEN DASHED BIKE LANE. SEE DETAIL
- GREEN BACK
- DETAIL 2
- DETAIL 8
- DETAIL 15

STRIPING NOTES:

- DETAIL 22
- DETAIL 25
- DETAIL 27B
- DETAIL 37B
- DETAIL 38
- DETAIL 39
- DETAIL 39A
- DETAIL 40
- 6" DIAGONAL YELLOW STRIPE
- 4" DIAGONAL WHITE STRIPE
- WHITE LADDER CROSSWALK
- YELLOW CONTINENTAL CROSSWALK
- BLUE HYDRANT MARKER
- 36" HIGH YELLOW FLEXIBLE CHANNELIZER
- MARKING ARROW TYPE VII

SPRINGER ROAD REPAIR



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2024-2025 ANNUAL STREET RESURFACING PROJECT

STREET IMPROVEMENT PLAN - SPRINGER ROAD

33+00 to 64+88

LOS ALTOS

SANTA CLARA COUNTY

CALIFORNIA

DWG NO.

C4.23

30 OF 50

JOB NO.

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CITY OF LOS ALTOS

1 NORTH SAN ANTONIO ROAD

CALIFORNIA 94022

REGISTERED PROFESSIONAL ENGINEER

JOSEPH A. ARICAR

NO. 87131

CIVIL

STATE OF CALIFORNIA

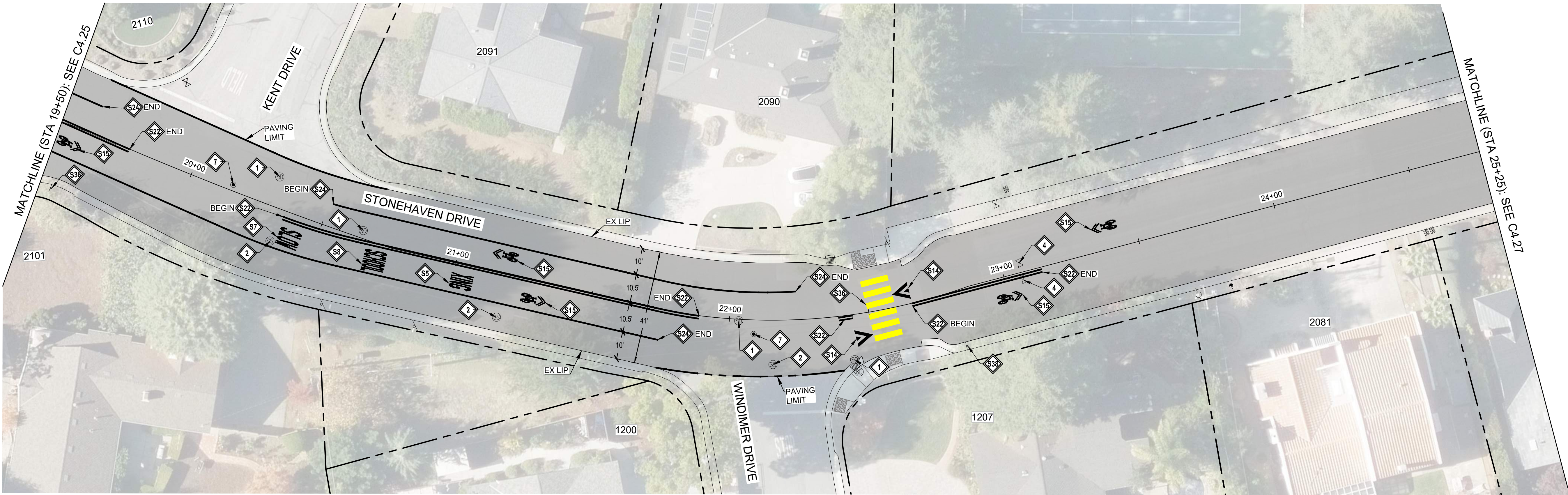




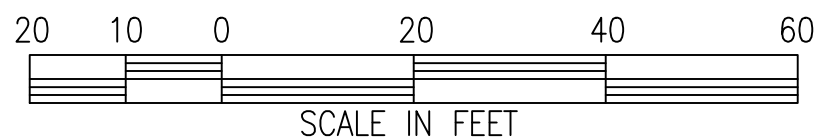








PAVEMENT REPAIR SITE - STONEHAVEN DRIVE



CONSTRUCTION NOTES:

- 1 LOWER & RESTORE TO GRADE STORM MANHOLE
- 2 LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
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  - FILE PROPER DOCUMENTATION SHOWING ORIGINAL AND REPLACEMENT MONUMENTS AFTER CONSTRUCTION
- 8 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
- 9 LOWER & RESTORE TO GRADE SEWER CLEANOUT

STRIPING NOTES:

- S1 WHITE LIMIT LINE (STOP LINE)
- S2 MARKING WORD "STOP"
- S3 MARKING WORD "AHEAD"
- S5 MARKING WORD "XING"
- S6 MARKING WORD "YIELD"
- S7 MARKING WORD "SLOW"
- S8 MARKING WORD "SCHOOL"
- S14 SPEED BUMP MARKING
- S15 SHARROW MARKING
- S22 DETAIL 22
- S24 DETAIL 27B
- S36 YELLOW CONTINENTAL CROSSWALK
- S37 BLUE HYDRANT MARKER
- S38 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL

LEGEND:

- 3" AC GRIND & INLAY. SEE TYPICAL SECTIONS
- 4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL
- APPROXIMATE LIMIT OF WORK
- APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

PAVEMENT REPAIR QUANTITY NOTES

- ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

Call Two Working Days Before You Dig!



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APRIL 07, 2025

2024-2025 ANNUAL STREET RESURFACING PROJECT

IMPROVEMENT PLAN - STONEHAVEN DRIVE

20+00 TO 25+25

LOS ALTOS

SANTA CLARA COUNTY

CALIFORNIA

DWG NO.

C4.26

33 OF 50

JOB NO.

250007

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CONSTRUCTION NOTES:

- 1 LOWER & RESTORE TO GRADE STORM MANHOLE
- 2 LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
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  - FILE PROPER DOCUMENTATION SHOWING ORIGINAL AND REPLACEMENT MONUMENTS AFTER CONSTRUCTION
- 8 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
- 9 LOWER & RESTORE TO GRADE SEWER CLEANOUT

STRIPING NOTES:

- \$1 WHITE LIMIT LINE (STOP LINE)
- \$2 MARKING WORD "STOP"
- \$3 MARKING WORD "AHEAD"
- \$5 MARKING WORD "XING"
- \$6 MARKING WORD "YIELD"
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- \$14 SPEED BUMP MARKING
- \$15 SHARROW MARKING
- \$22 DETAIL 22
- \$24 DETAIL 27B
- \$36 YELLOW CONTINENTAL CROSSWALK
- \$37 BLUE HYDRANT MARKER
- \$38 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL

LEGEND:

- 3" AC GRIND & INLAY. SEE TYPICAL SECTIONS
- 4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL
- APPROXIMATE LIMIT OF WORK
- APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

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- A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

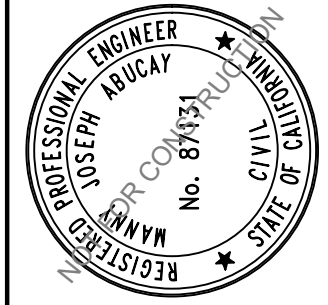
PAVEMENT REPAIR QUANTITY NOTES

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65% SUBMITTAL  
APRIL 07, 2025

DWG NO.  
C4.19  
26 OF 50  
JOB NO.  
250007

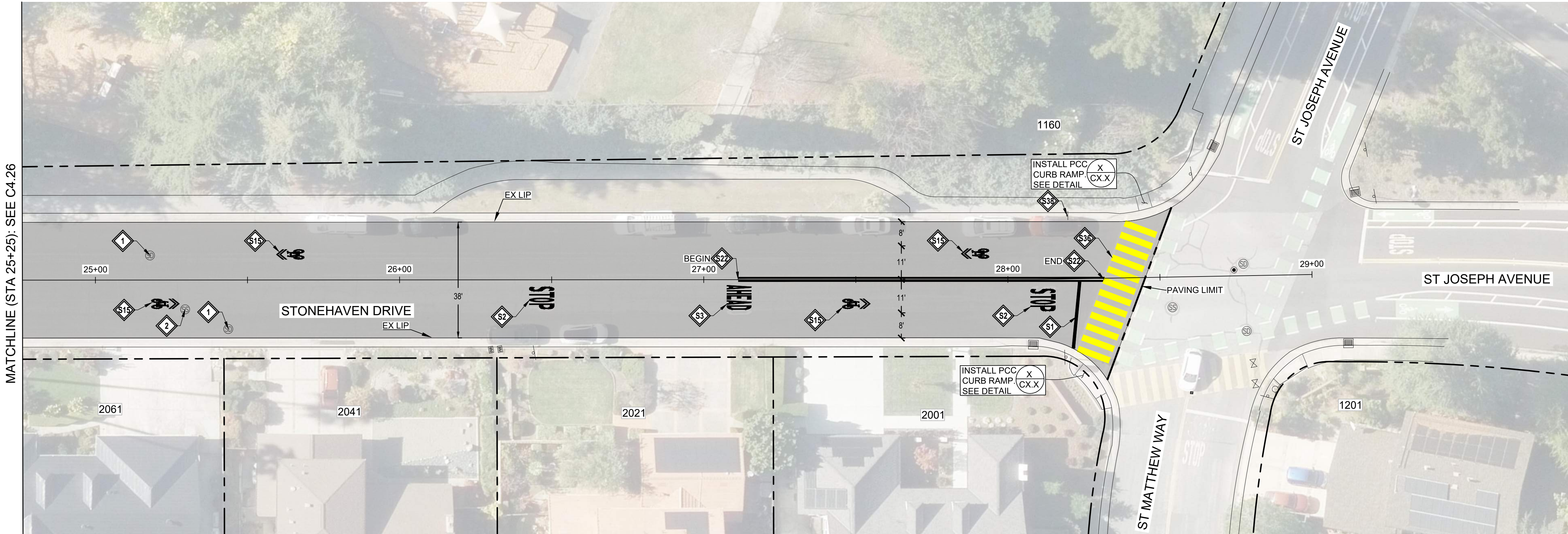
2024-2025 ANNUAL STREET RESURFACING PROJECT  
IMPROVEMENT PLAN - STONEHAVEN DRIVE  
25+25 TO 29+79  
LOS ALTOS  
SANTA CLARA COUNTY  
CALIFORNIA



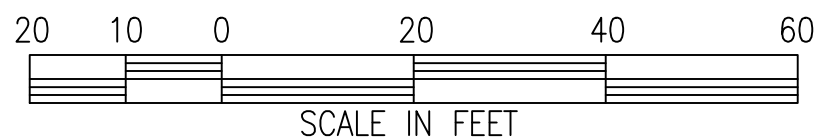
CITY OF LOS ALTOS  
1 NORTH SAN ANTONIO ROAD  
CALIFORNIA 94022

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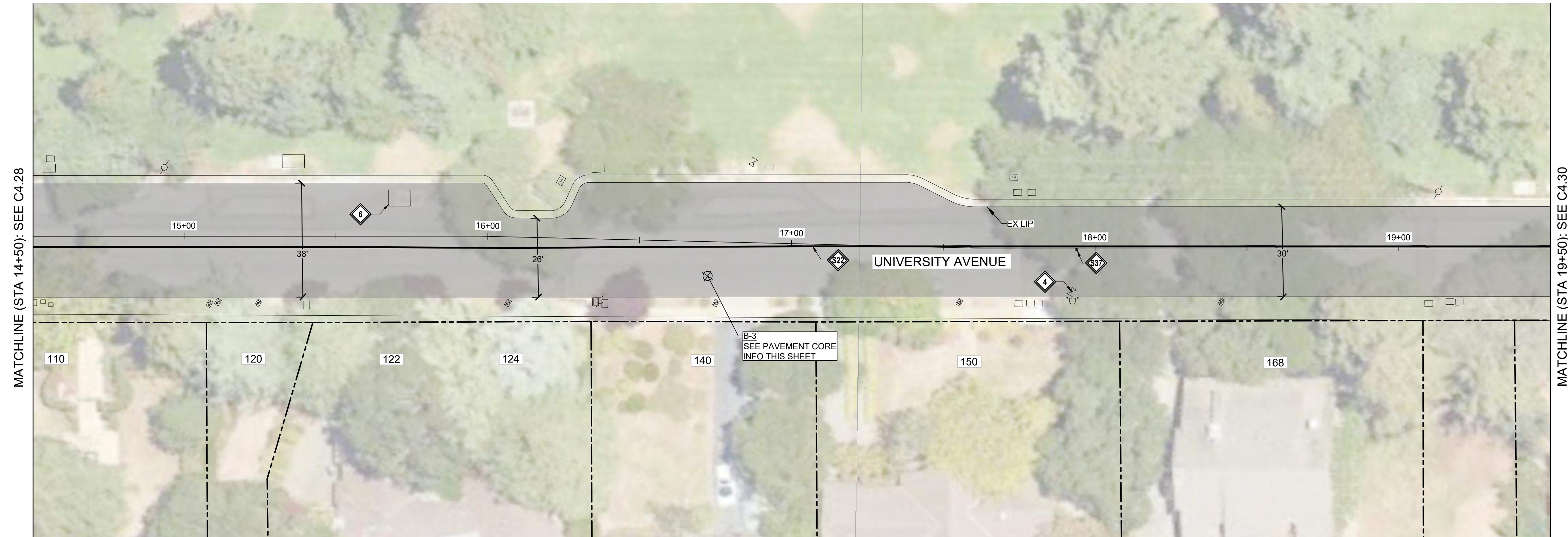
PAVEMENT REPAIR SITE - STONEHAVEN DRIVE





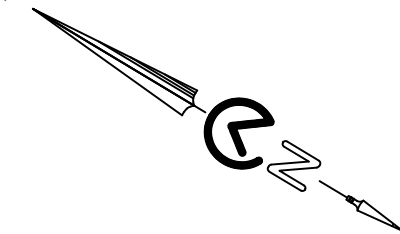






MATCHLINE (STA 19+50): SEE C4.30

## PAVEMENT REPAIR SITE - UNIVERSITY AVENUE



CONSTRUCTION NOTES:

STRIPING NOTES:

LEGEND:

GENERAL NOTES:

### AC BASE REPAIR ALLOCATION QUANTITIES

### PAVEMENT REPAIR QUANTITY NOTES

## PAVEMENT CORING LOG

### PAVEMENT CORING NOTES

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JOB NO. 250007	LOS ALTOS		SANTA CLARA COUNTY		CALIFORNIA	
	IMPROVEMENT PLAN - UNIVERSITY AVENUE 14+50 to 19+50		2024-2025 ANNUAL STREET RESURFACING PROJECT			
36 OF 50	C4.29					
DWG NO.						
<div><div><div>CITY OF LOS ALTOS 1 NORTH SAN ANTONIO ROAD CALIFORNIA 94022</div><div><div>PROFESSIONAL ENGINEER JOSEFA ARAYA No. 81751 CIVIL STATE OF CALIFORNIA</div><div>REGISTERED MEMBER COMPANY 1400</div></div></div></div>						

















CONSTRUCTION NOTES:

- **1**
- LOWER & RESTORE TO GRADE STORM MANHOLE
- **2**
- LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- **3**
- LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)
- **4**
- LOWER & RESTORE TO GRADE WATER VALVE
- **5**
- LOWER & RESTORE TO GRADE GAS VALVE
- **6**
- LOWER & RESTORE TO GRADE UTILITY BOX
- **7**
- REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR:
  - REFERENCE OUT PRIOR TO START OF CONSTRUCTION
  - FILE PROPER DOCUMENTATION SHOWING ORIGINAL AND REPLACEMENT MONUMENTS AFTER CONSTRUCTION


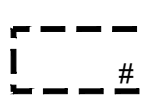





  - SEE DETAIL
- **8**
- INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL
- **9**
- LOWER & RESTORE TO GRADE SEWER CLEANOUT

STRIPING NOTES:

- **S1**
- WHITE LIMIT LINE (STOP LINE)
- **S2**
- MARKING WORD "STOP"
- **S9**
- MARKING "25"
- **S22**
- DETAIL 22
- **S33**
- WHITE LADDER CROSSWALK
- **S37**
- BLUE HYDRANT MARKER
- **S42**
- REPAINT RED CURB

LEGEND:

- **3" AC GRIND & INLAY. SEE TYPICAL SECTIONS**
- **4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL**


- **APPROXIMATE LIMIT OF WORK**
- **APPROXIMATE RIGHT OF WAY**

GENERAL NOTES:

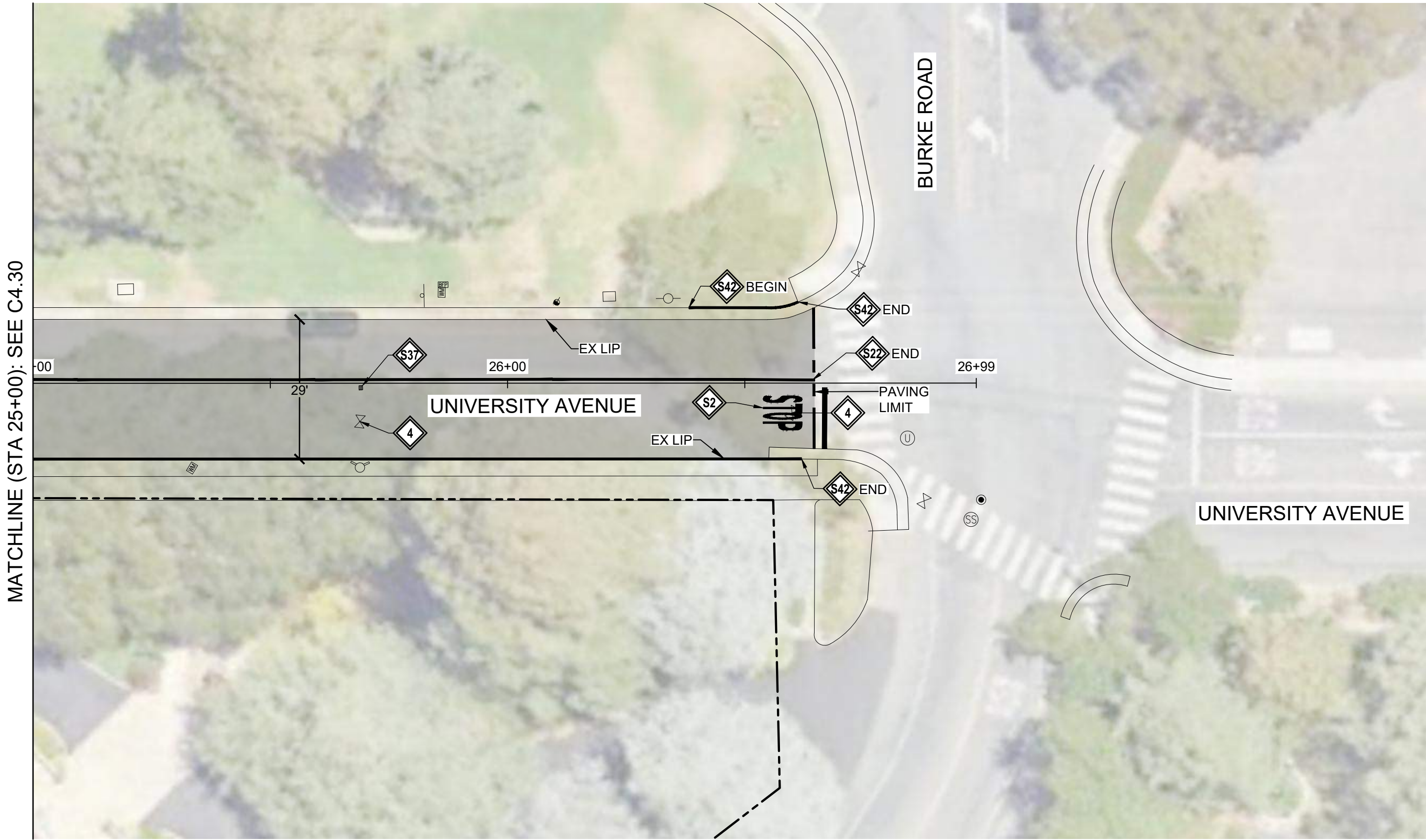
- SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

AC BASE REPAIR ALLOCATION QUANTITIES

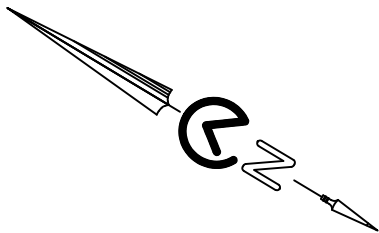
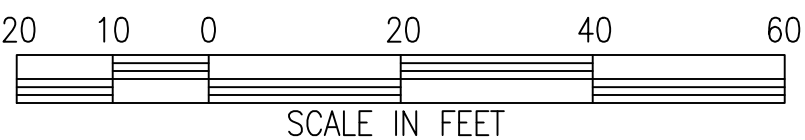
AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

PAVEMENT REPAIR QUANTITY NOTES

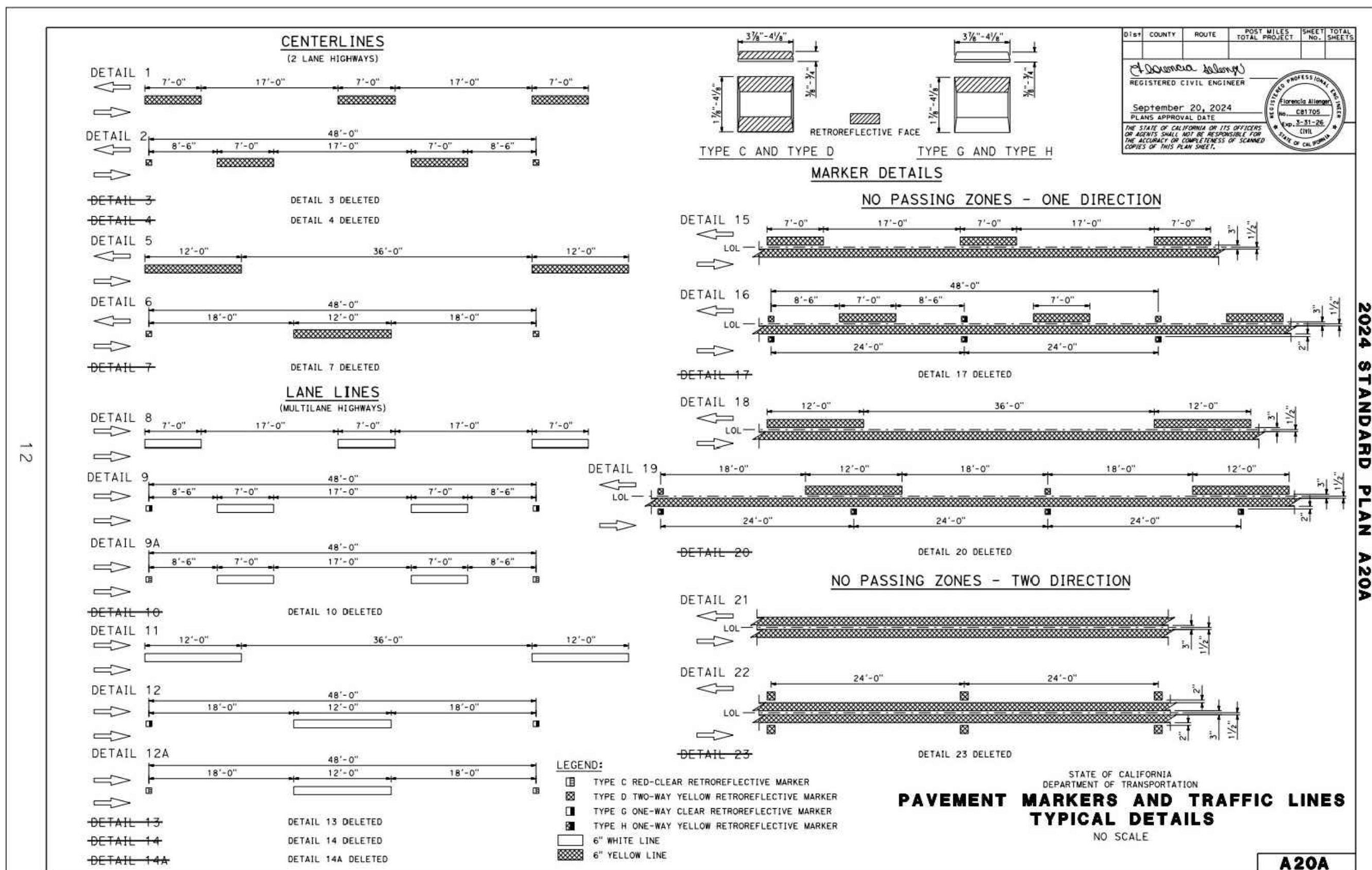
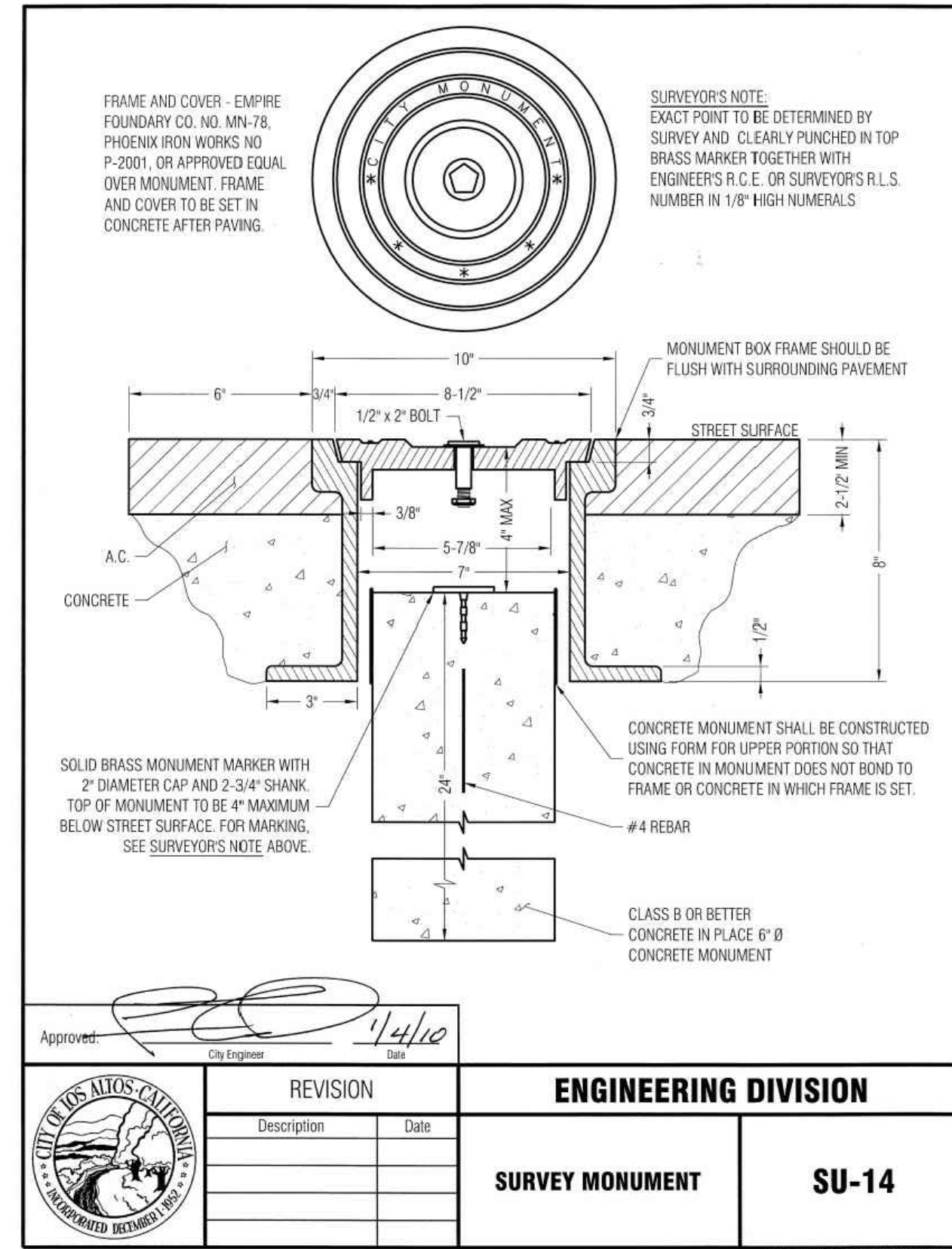
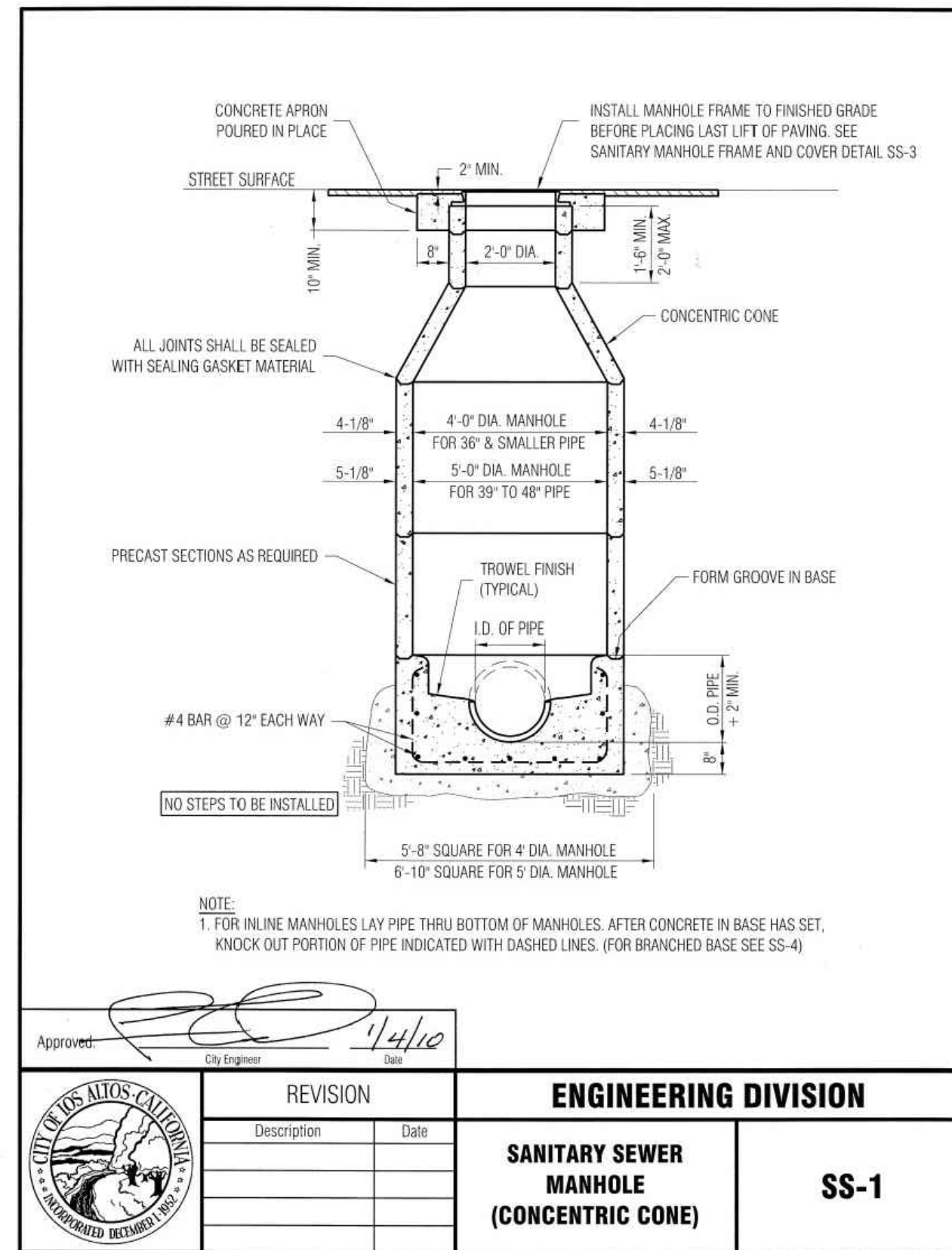
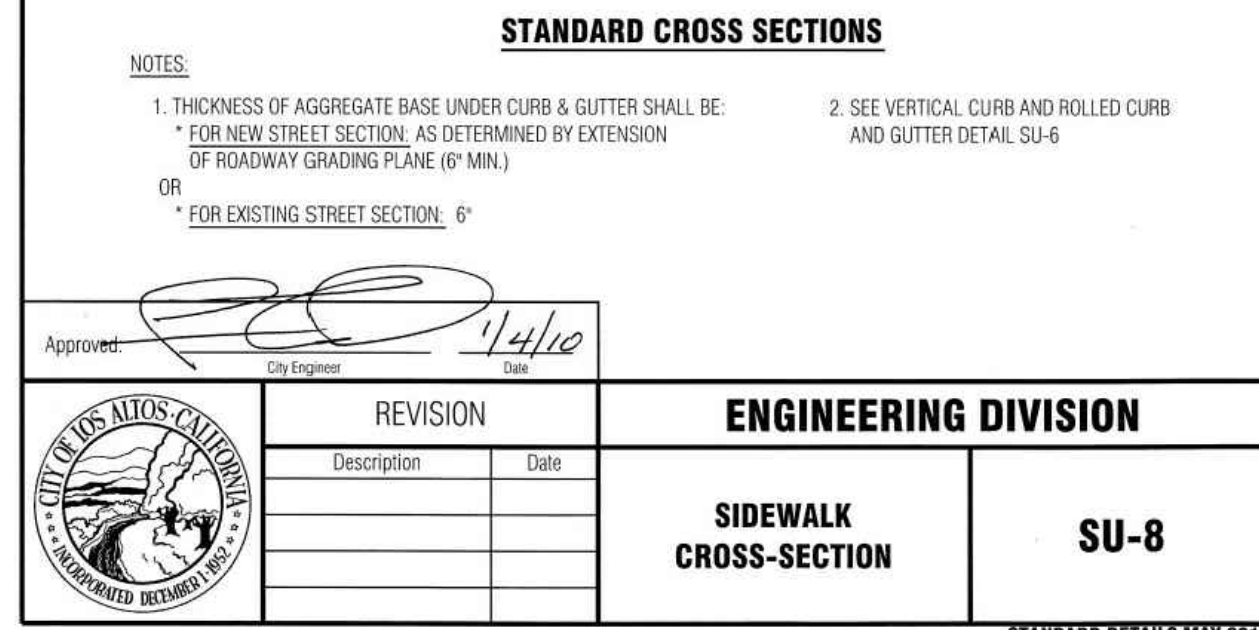
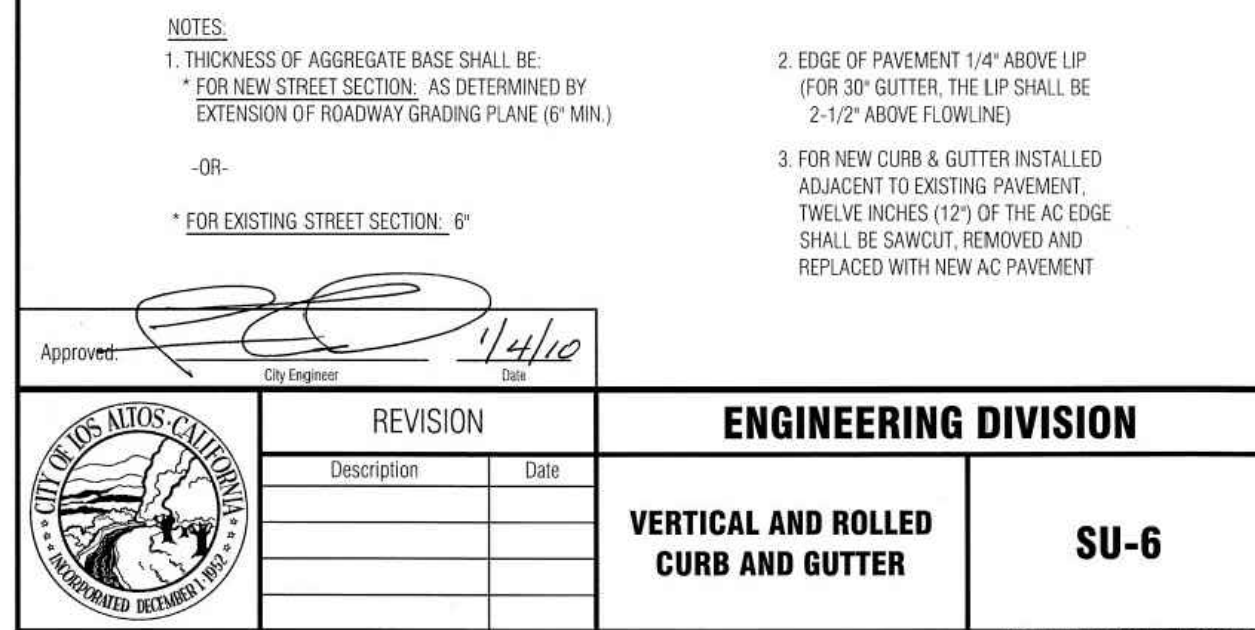
- ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.



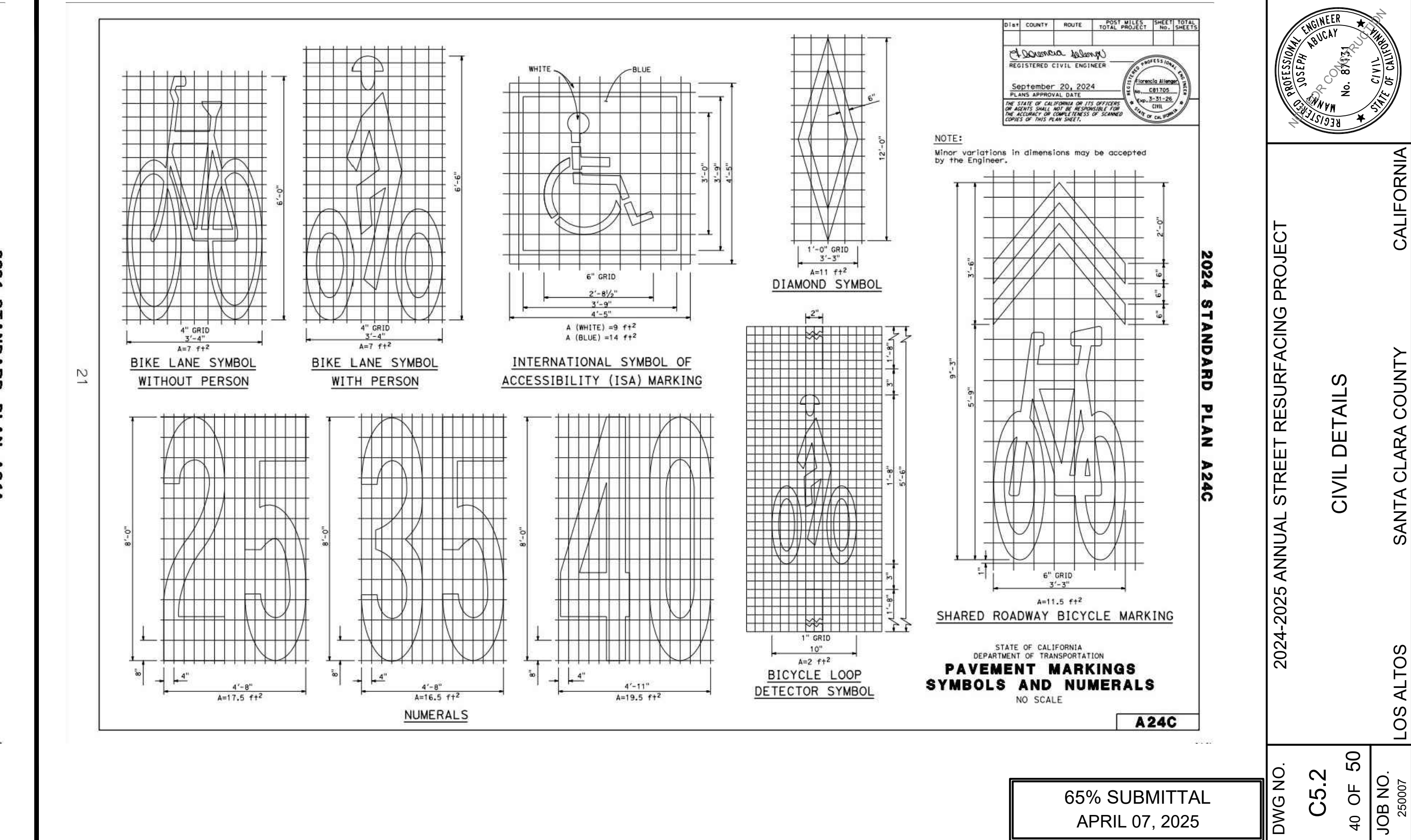
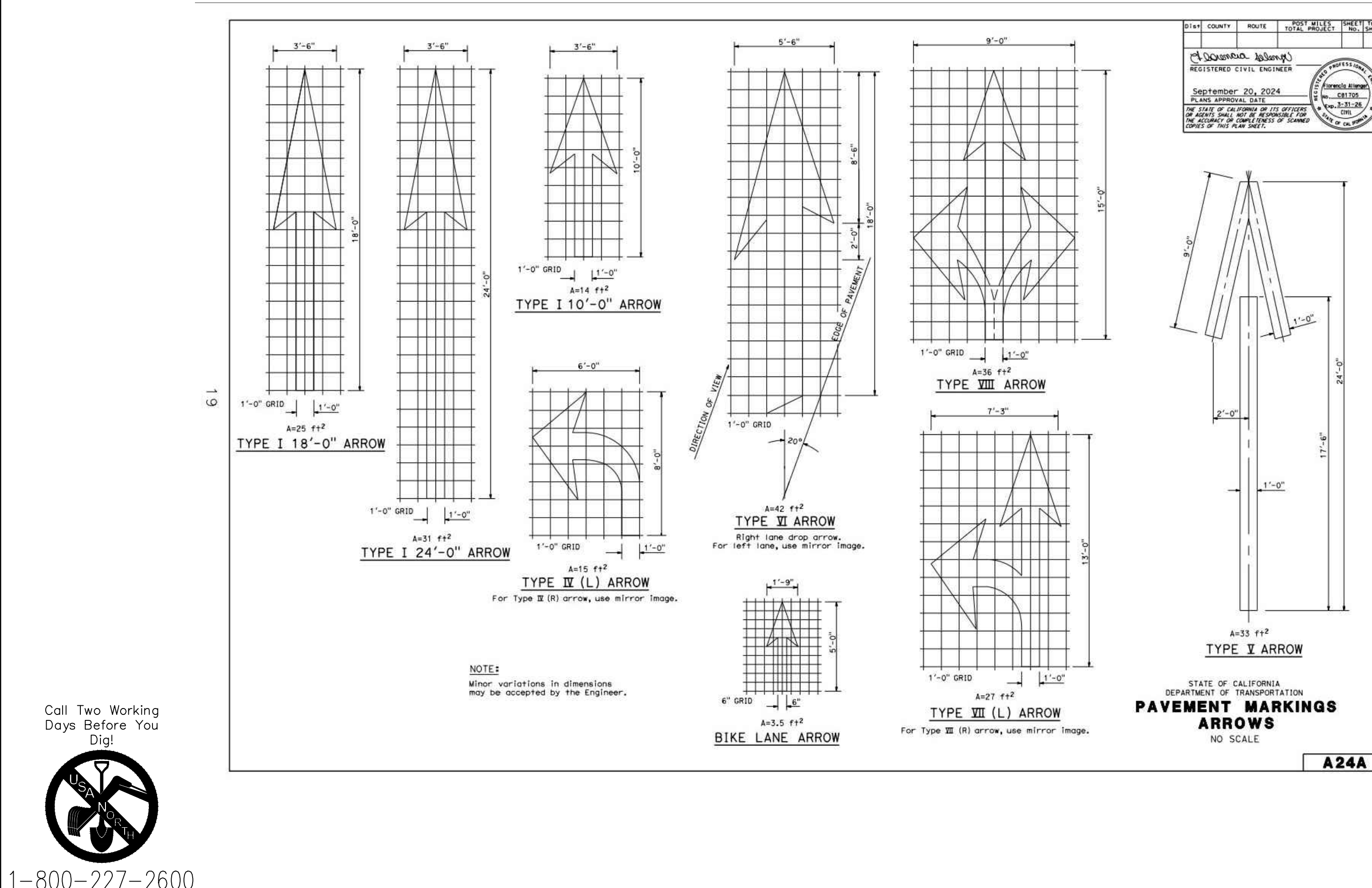
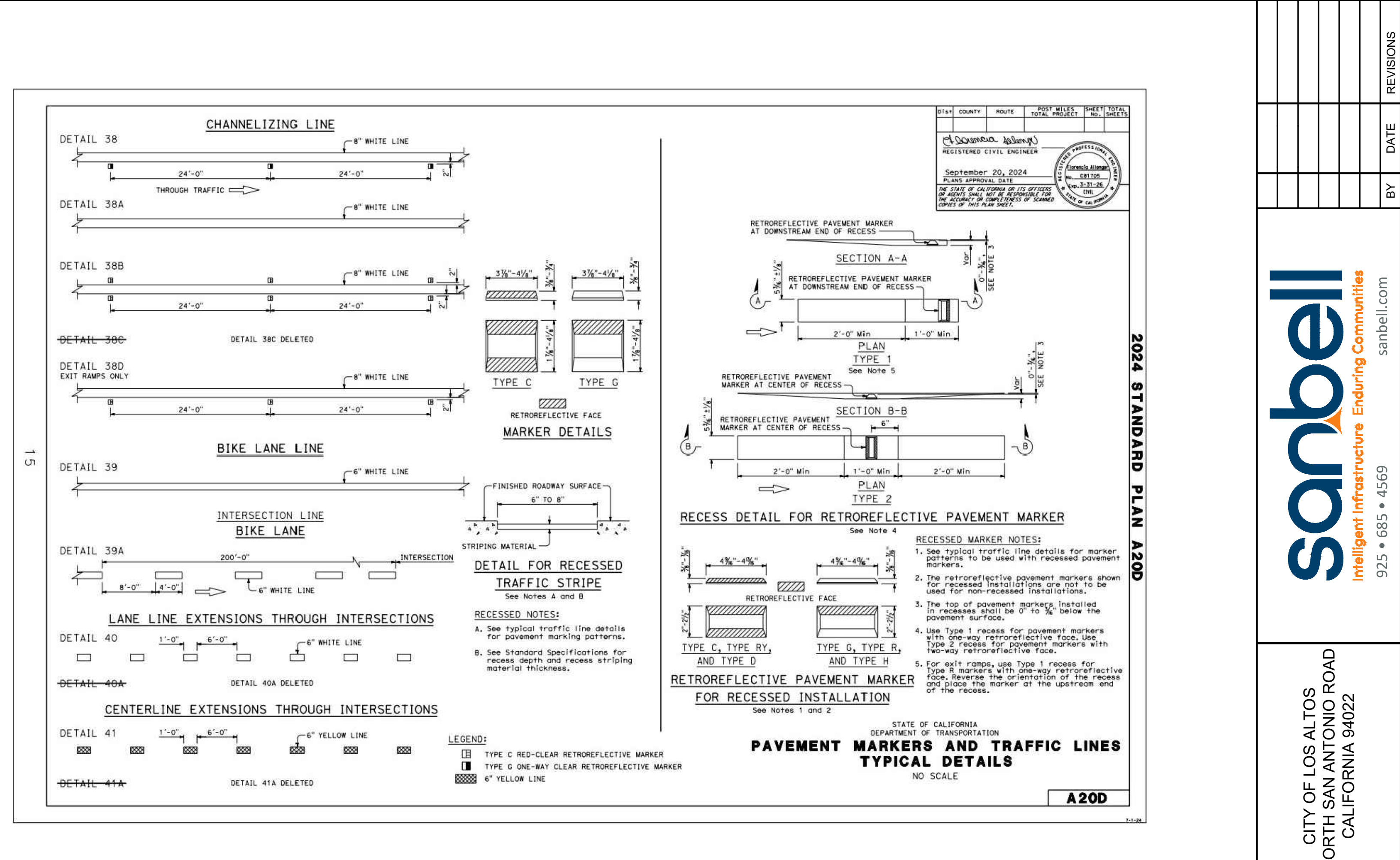
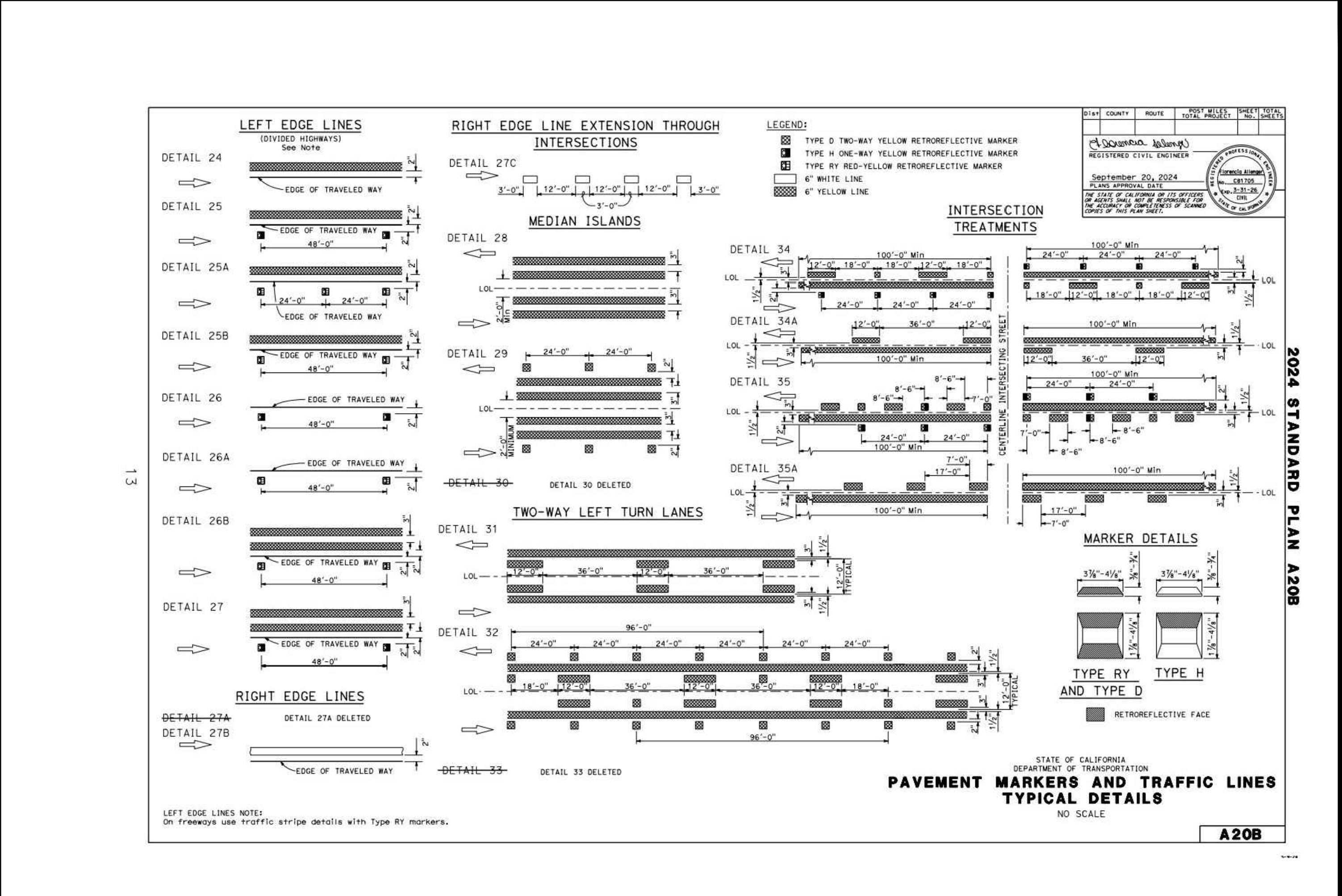
PAVEMENT REPAIR SITE - UNIVERSITY AVENUE











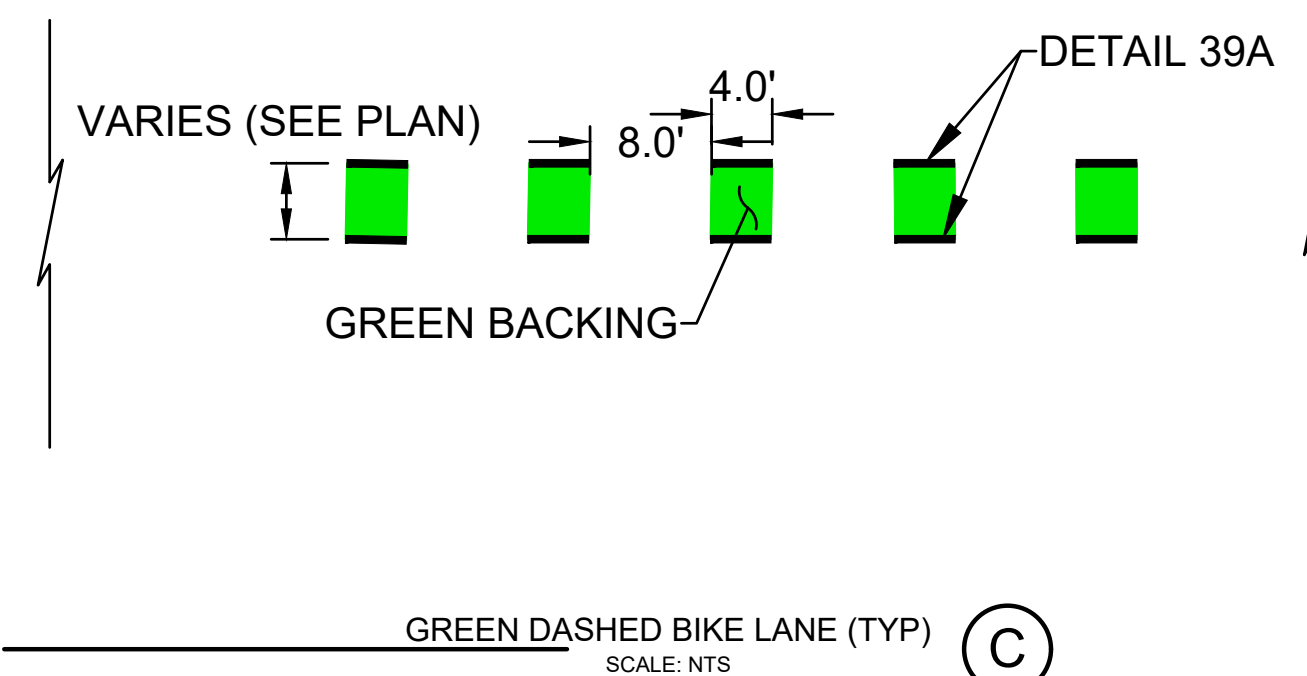
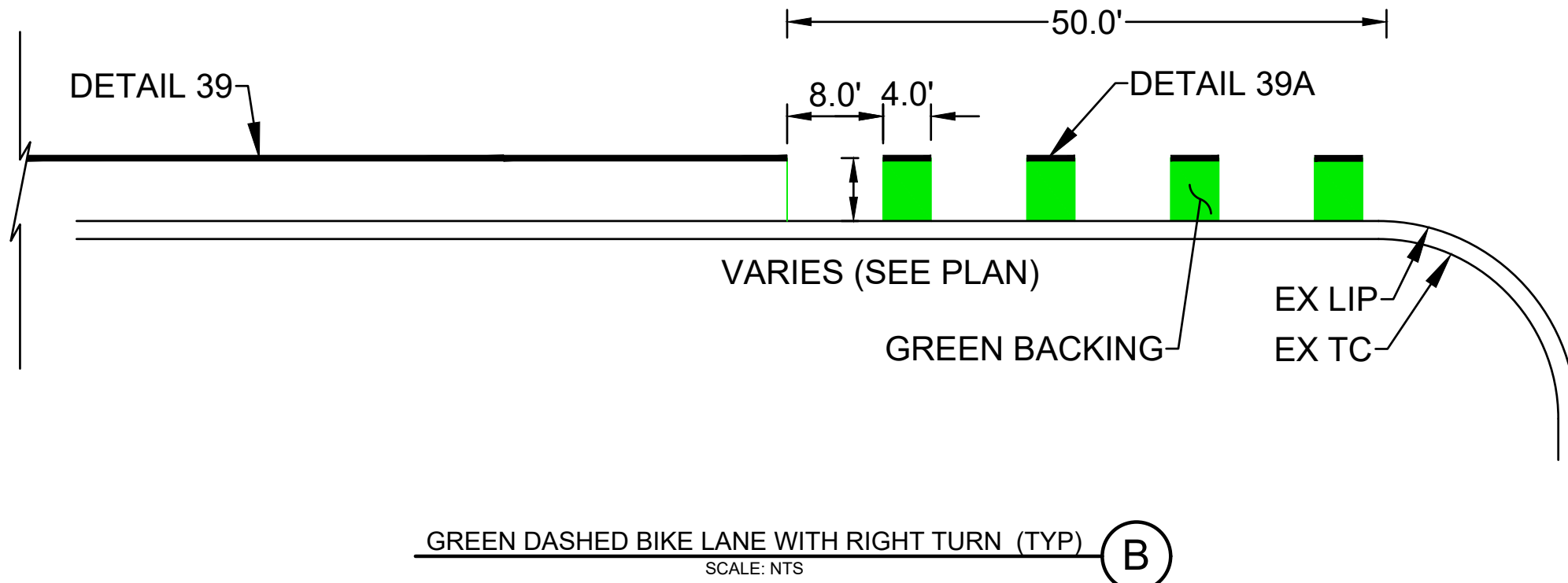
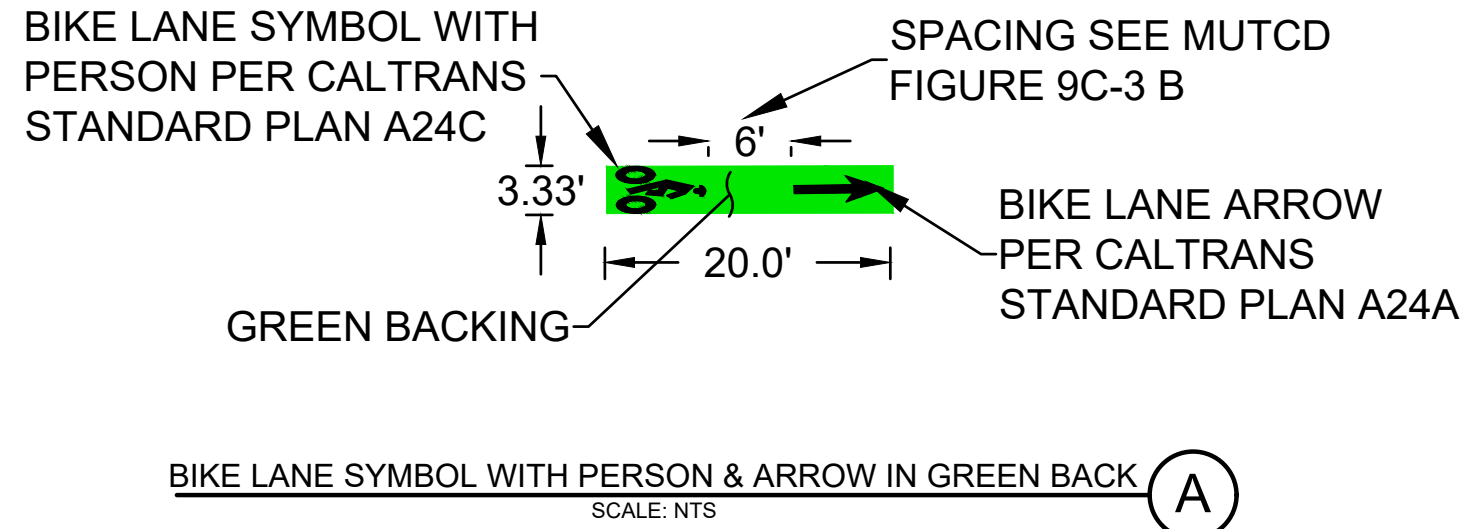




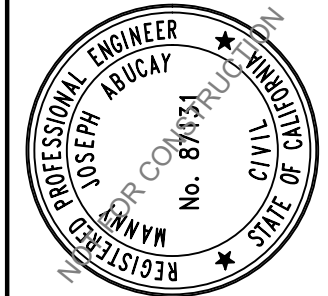
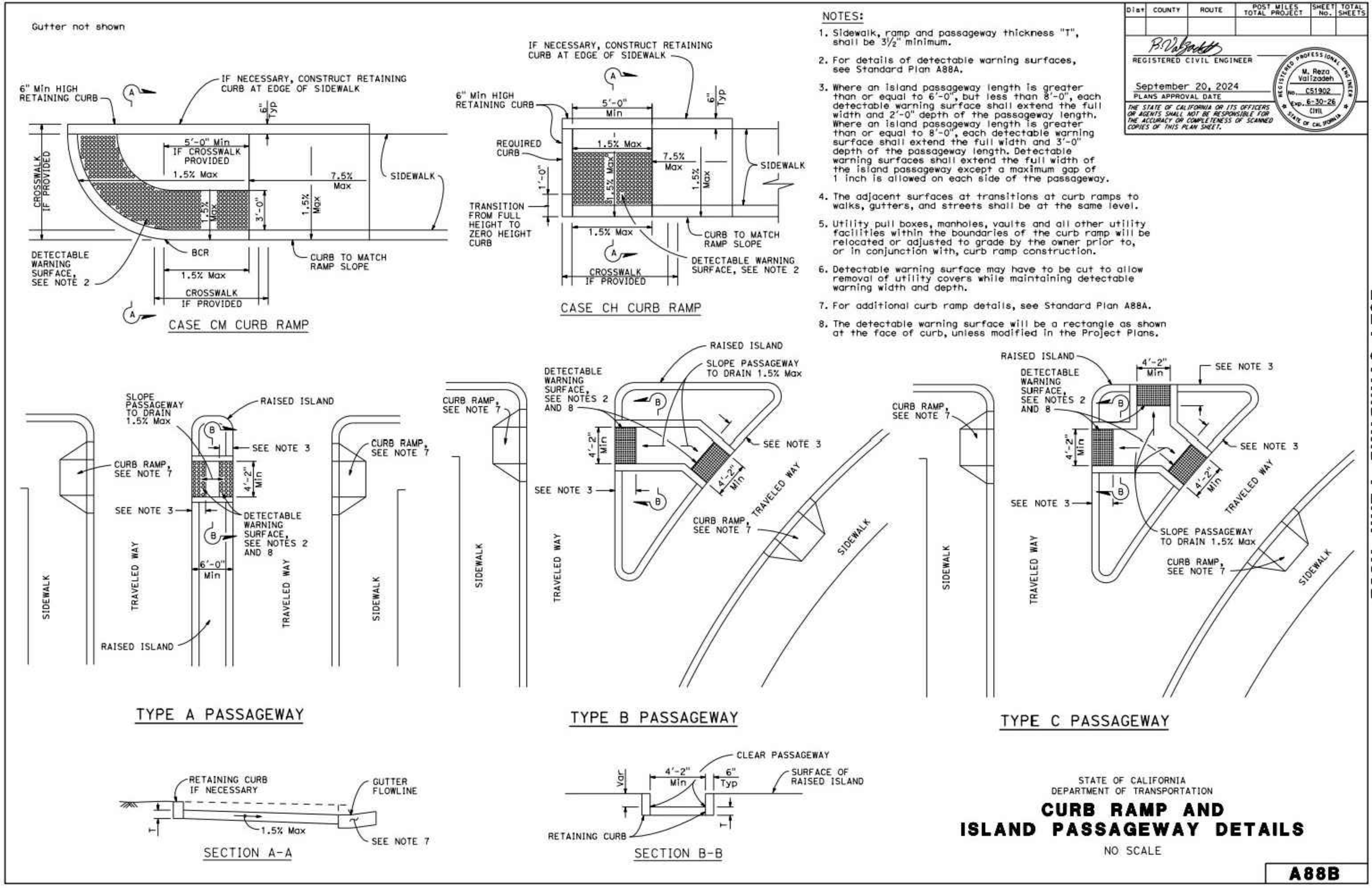




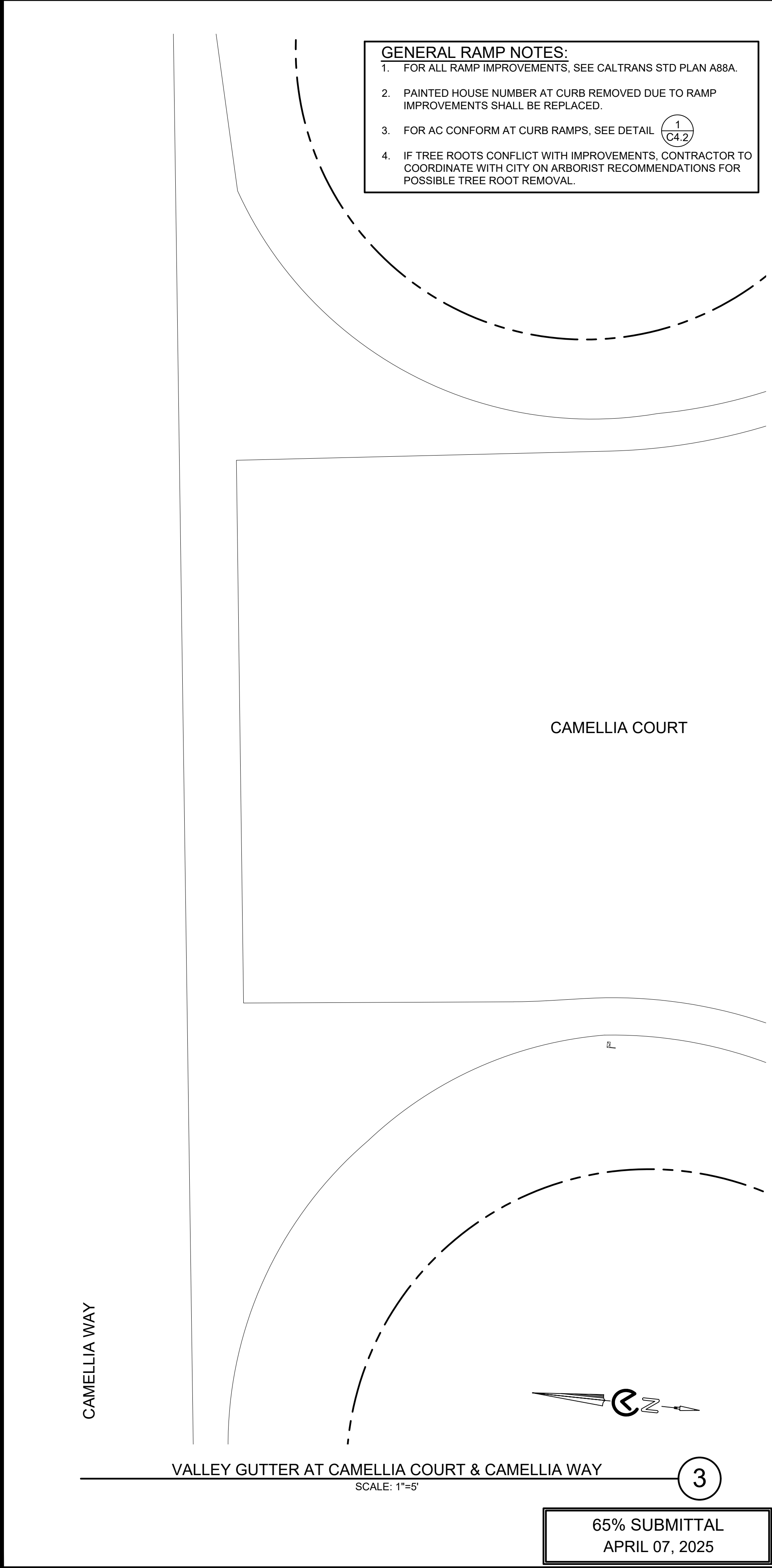
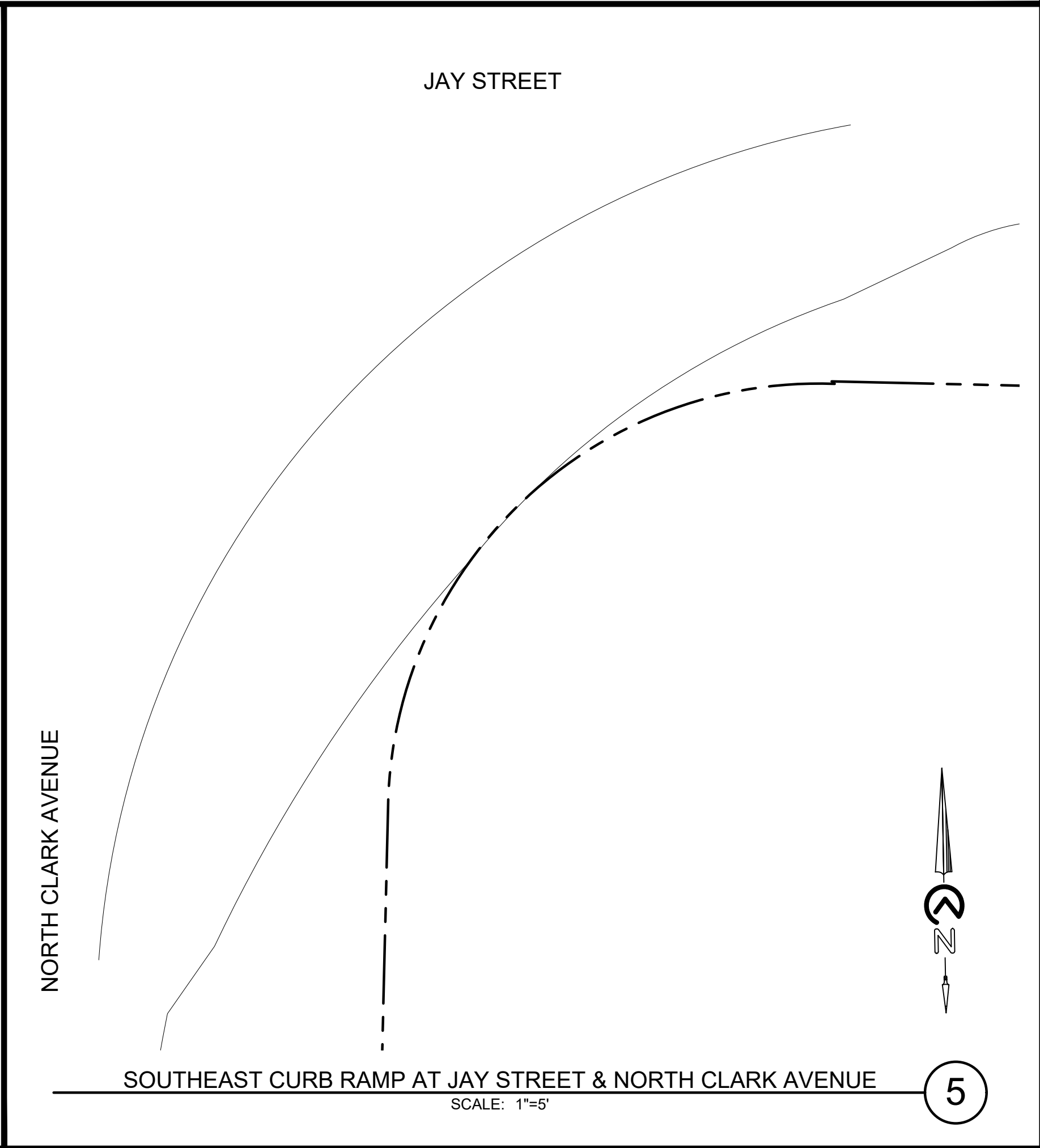
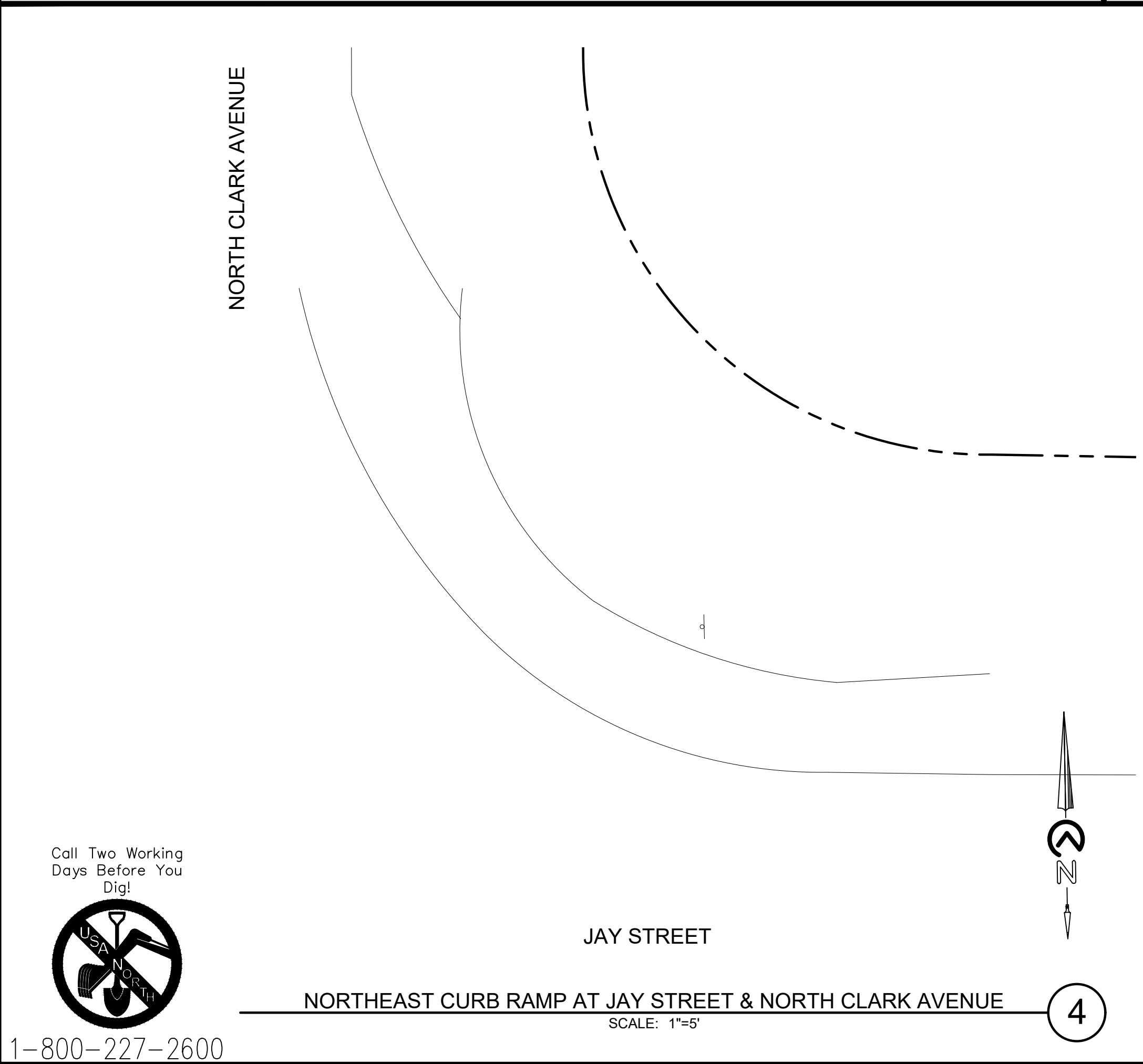
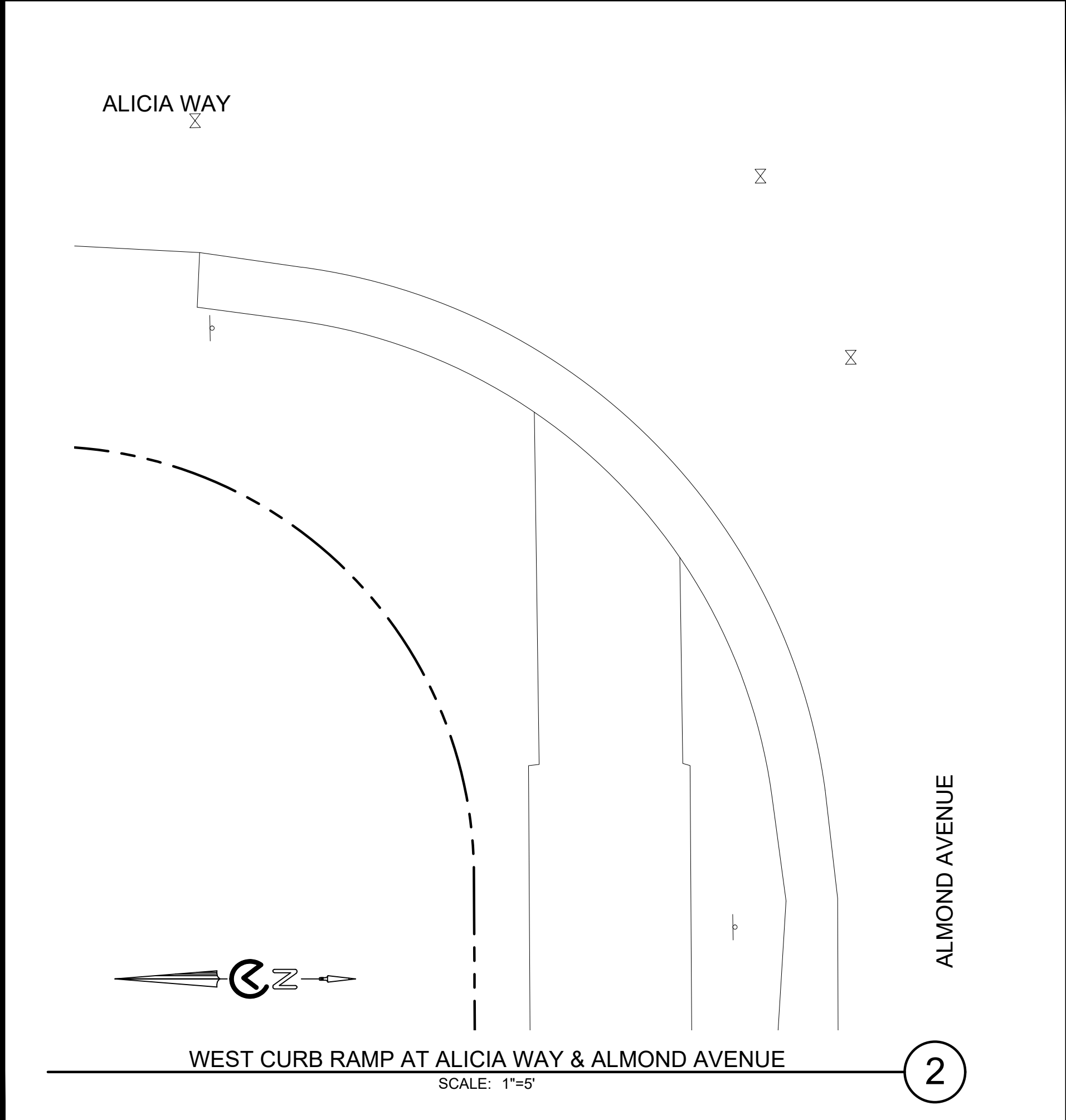
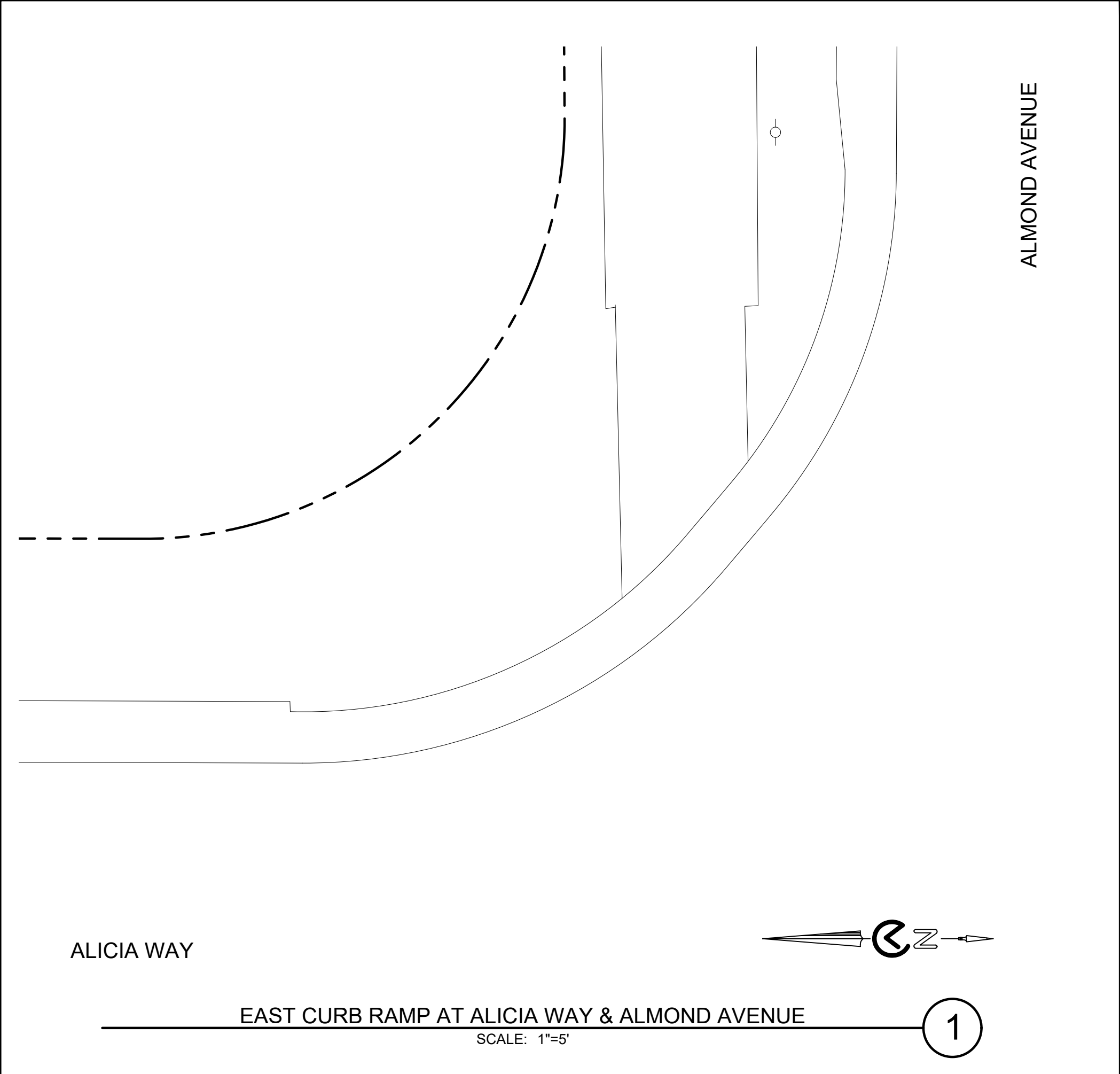
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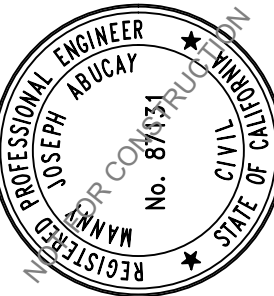




- GENERAL RAMP NOTES:**
- FOR ALL RAMP IMPROVEMENTS, SEE CALTRANS STD PLAN A88A.
  - PAINTED HOUSE NUMBER AT CURB REMOVED DUE TO RAMP IMPROVEMENTS SHALL BE REPLACED.
  - FOR AC CONFORM AT CURB RAMPS, SEE DETAIL 
  - IF TREE ROOTS CONFLICT WITH IMPROVEMENTS, CONTRACTOR TO COORDINATE WITH CITY ON ARBORIST RECOMMENDATIONS FOR POSSIBLE TREE ROOT REMOVAL.

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DWG NO.	C5.5	2024-2025 ANNUAL STREET RESURFACING PROJECT		CITY OF LOS ALTOS 1 NORTH SAN ANTONIO ROAD CALIFORNIA 94022	
JOB NO.	43 OF 50	CIVIL DETAILS		LOS ALTOS	SANTA CLARA COUNTY
250007				DATE	REVISIONS
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SPRINGER ROAD

1

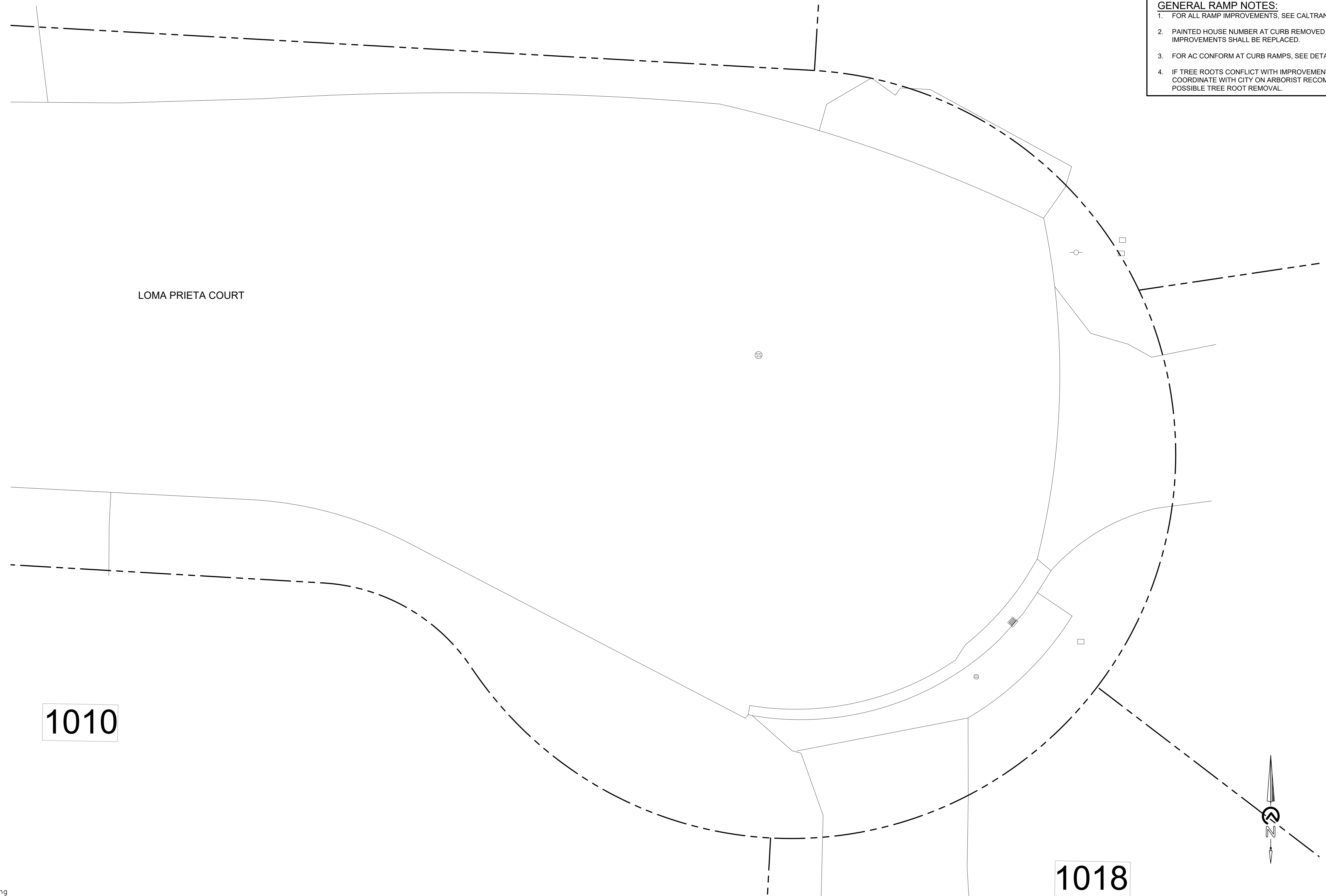
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DWG NO.	2024-2025 ANNUAL STREET RESURFACING PROJECT		CITY OF LOS ALTOS 1 NORTH SAN ANTONIO ROAD CALIFORNIA 94022		 <b>Intelligent Infrastructure</b> Enduring Communities 925 • 685 • 4569 sanbell.com				
C5.6									
44 OF 50									
JOB NO.	250007		 PROFESSIONAL ENGINEER JOSEPH ABUGAR No. 87131 REGISTERED MARCH 2004 CIVIL STATE OF CALIFORNIA				BY	DATE	REVISIONS
	LOS ALTOS		SANTA CLARA COUNTY		CALIFORNIA				










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LOMA PRIETA COURT

1010

1018



### CURB AND GUTTER AT LOMA PRIETA COURT

SCALE: 1"=5'

1



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## 2024-2025 ANNUAL STREET RESURFACING PROJECT

## CIVIL DETAILS

SANTA CLARA COUNTY

CALIFORNIA

CITY OF LOS ALTOS  
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C5.8

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NEWCASTLE DRIVE

1785


MORTON AVENUE

### SOUTH CURB RAMP AT NEWCASTLE DRIVE & MORTON AVENUE

SCALE: 1"=5'

1

GENERAL RAMP NOTES:

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CUESTA DRIVE

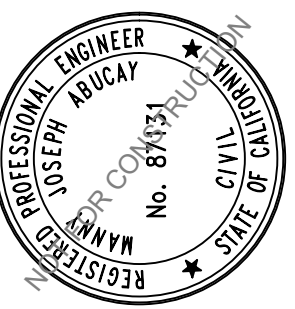
SHELBY LANE

## CURB RAMPS AT SHELBY LANE & CUESTA DRIVE

SCALE: 1"=5'

(2)

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## 2024-2025 ANNUAL STREET RESURFACING PROJECT

## CIVIL DETAILS

SANTA CLARA COUNTY

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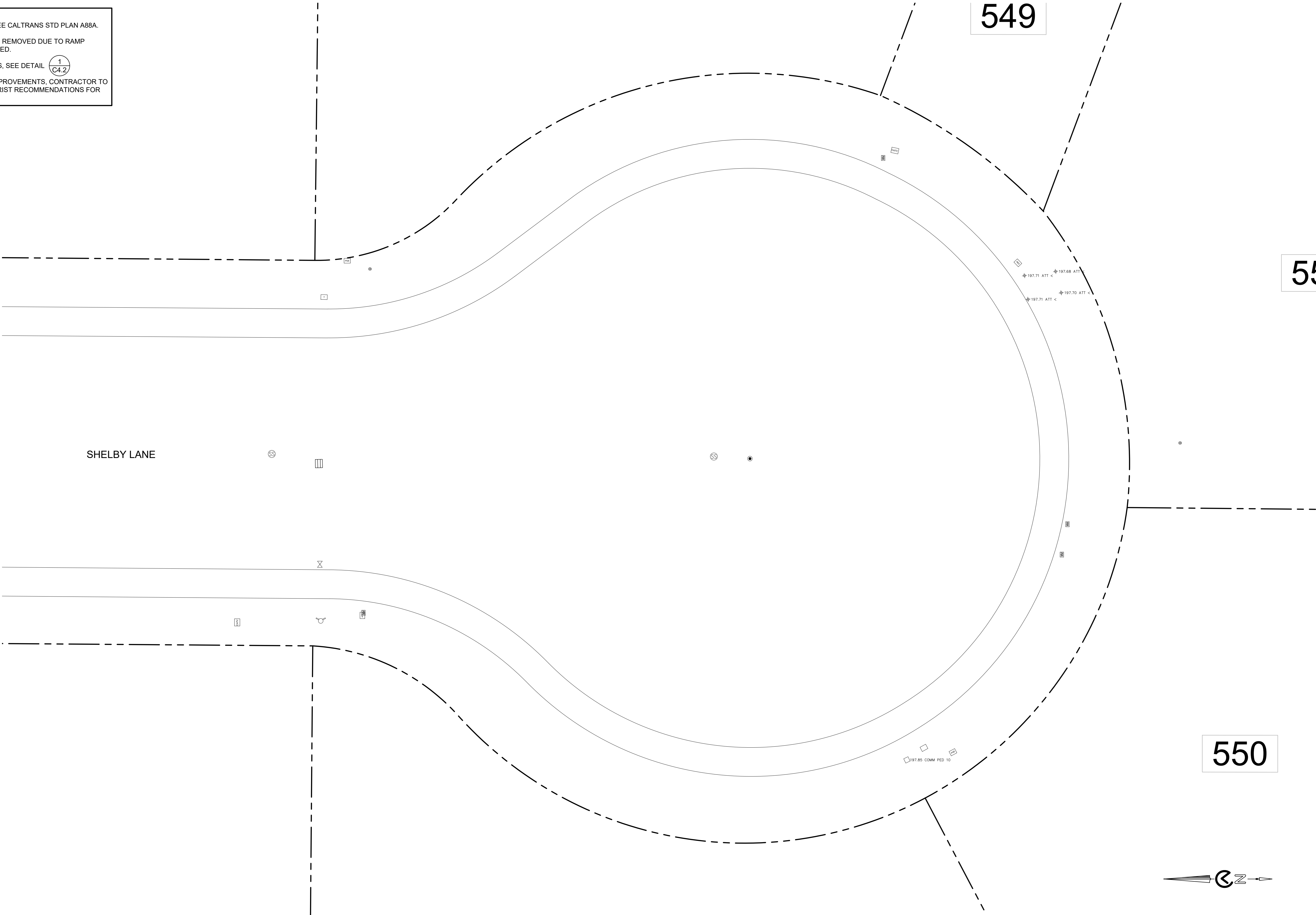
C5.9

JOB NO.  
250007



- GENERAL RAMP NOTES:**
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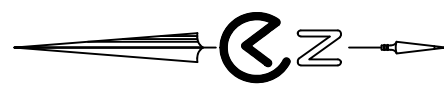
1  
C4.2
  - IF TREE ROOTS CONFLICT WITH IMPROVEMENTS, CONTRACTOR TO COORDINATE WITH CITY ON ARBORIST RECOMMENDATIONS FOR POSSIBLE TREE ROOT REMOVAL.



549

550

550



CURB AND GUTTER AT SHELBY LANE  
SCALE: 1"=5'

1

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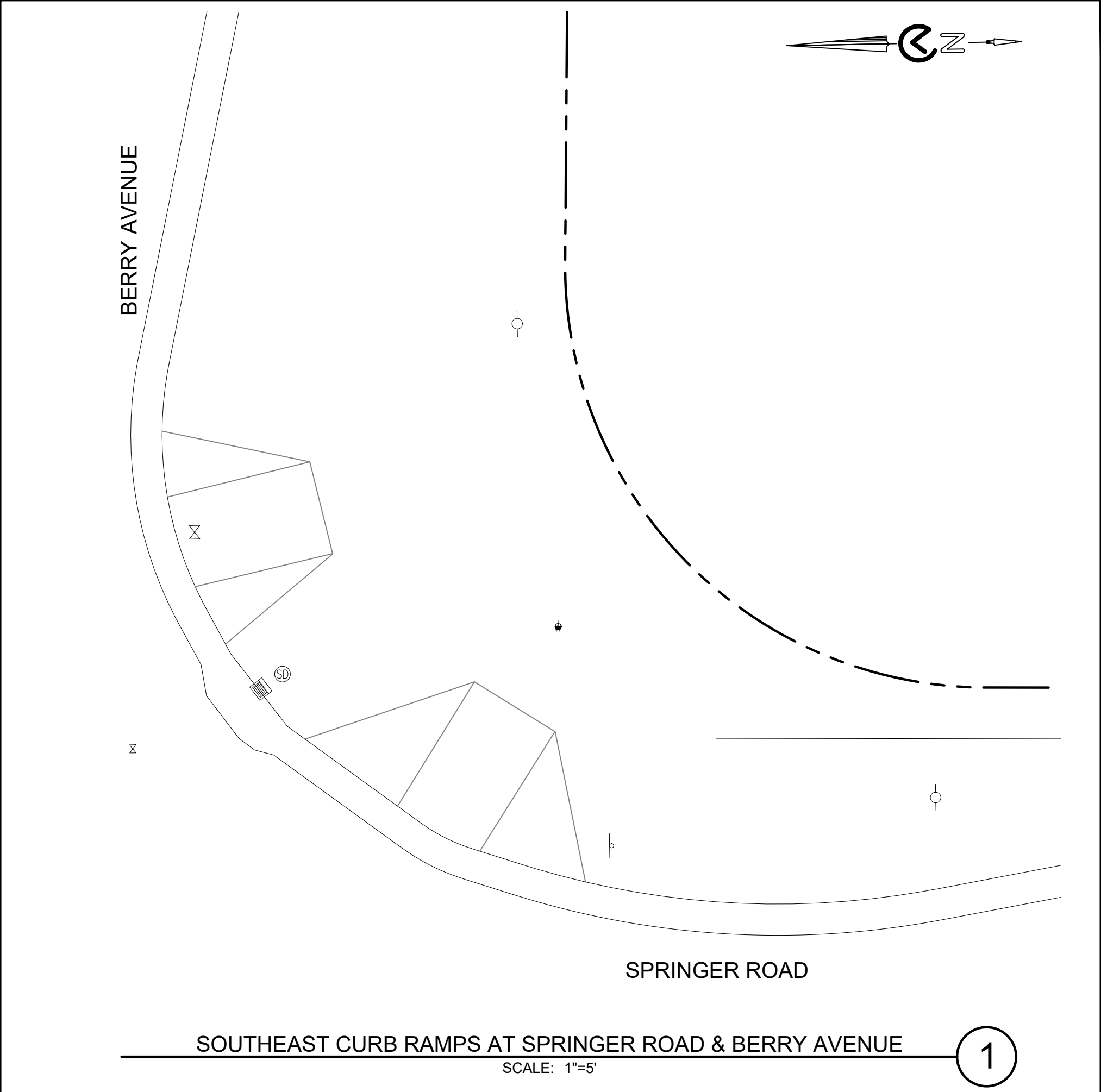


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DWG NO.	C5.10	JOB NO.	250007
48 OF 50			
2024-2025 ANNUAL STREET RESURFACING PROJECT			
CIVIL DETAILS			
LOS ALTOS	SANTA CLARA COUNTY	CALIFORNIA	
CITY OF LOS ALTOS 1 NORTH SAN ANTONIO ROAD CALIFORNIA 94022			
	BY	DATE	REVISIONS

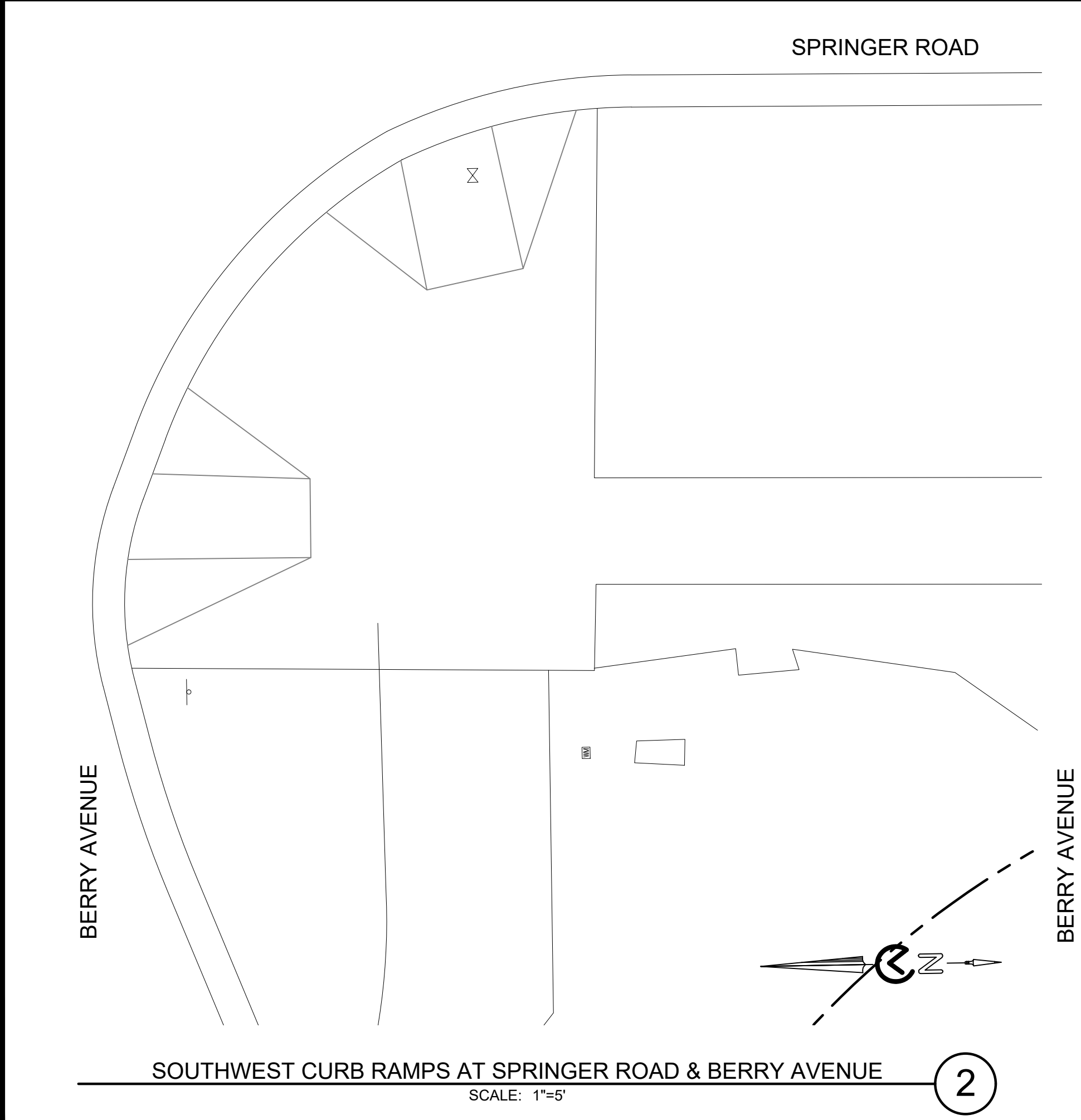




SOUTHEAST CURB RAMPS AT SPRINGER ROAD & BERRY AVENUE

SCALE: 1"=5'

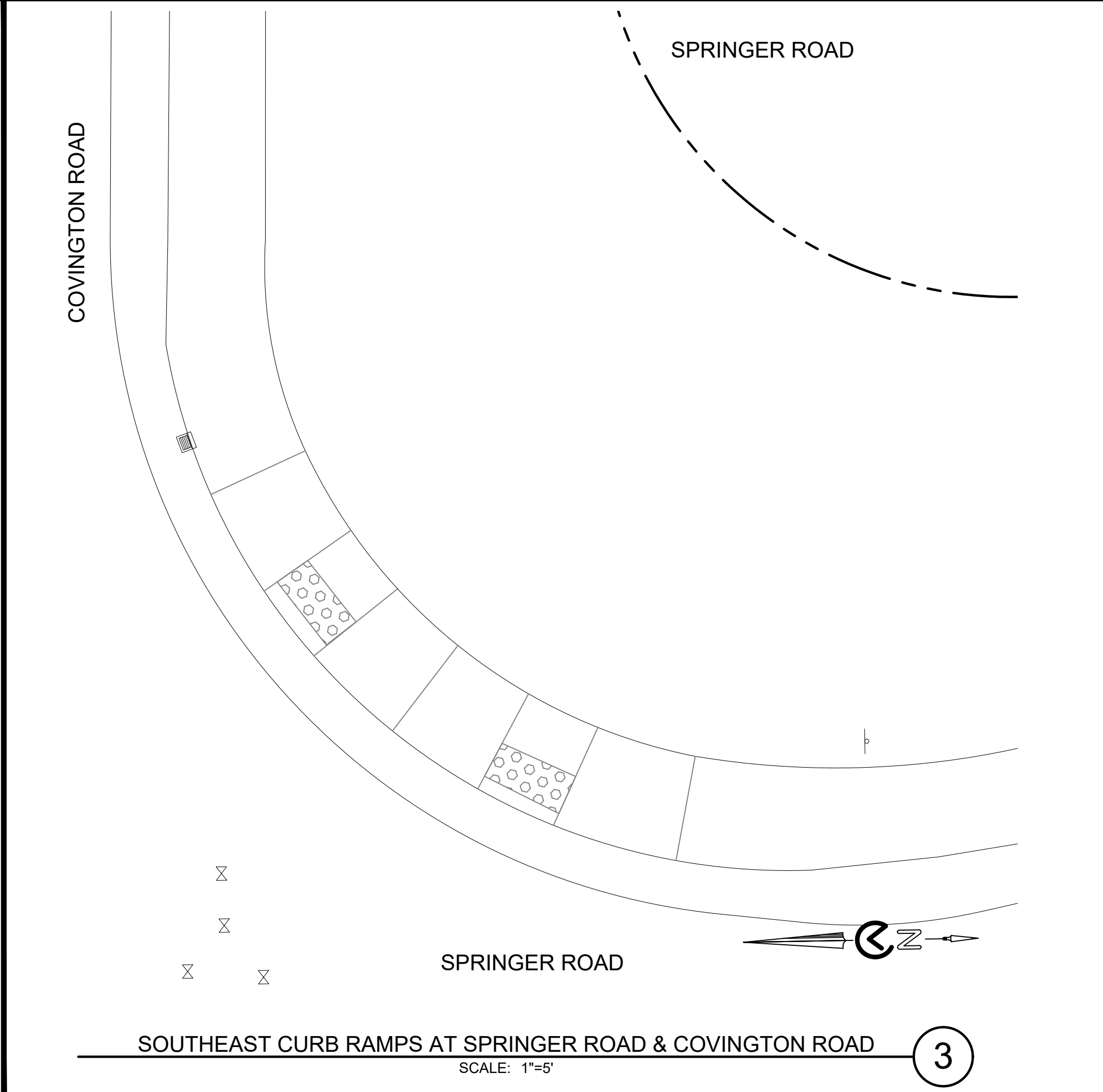
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SOUTHWEST CURB RAMPS AT SPRINGER ROAD & BERRY AVENUE

SCALE: 1"=5'

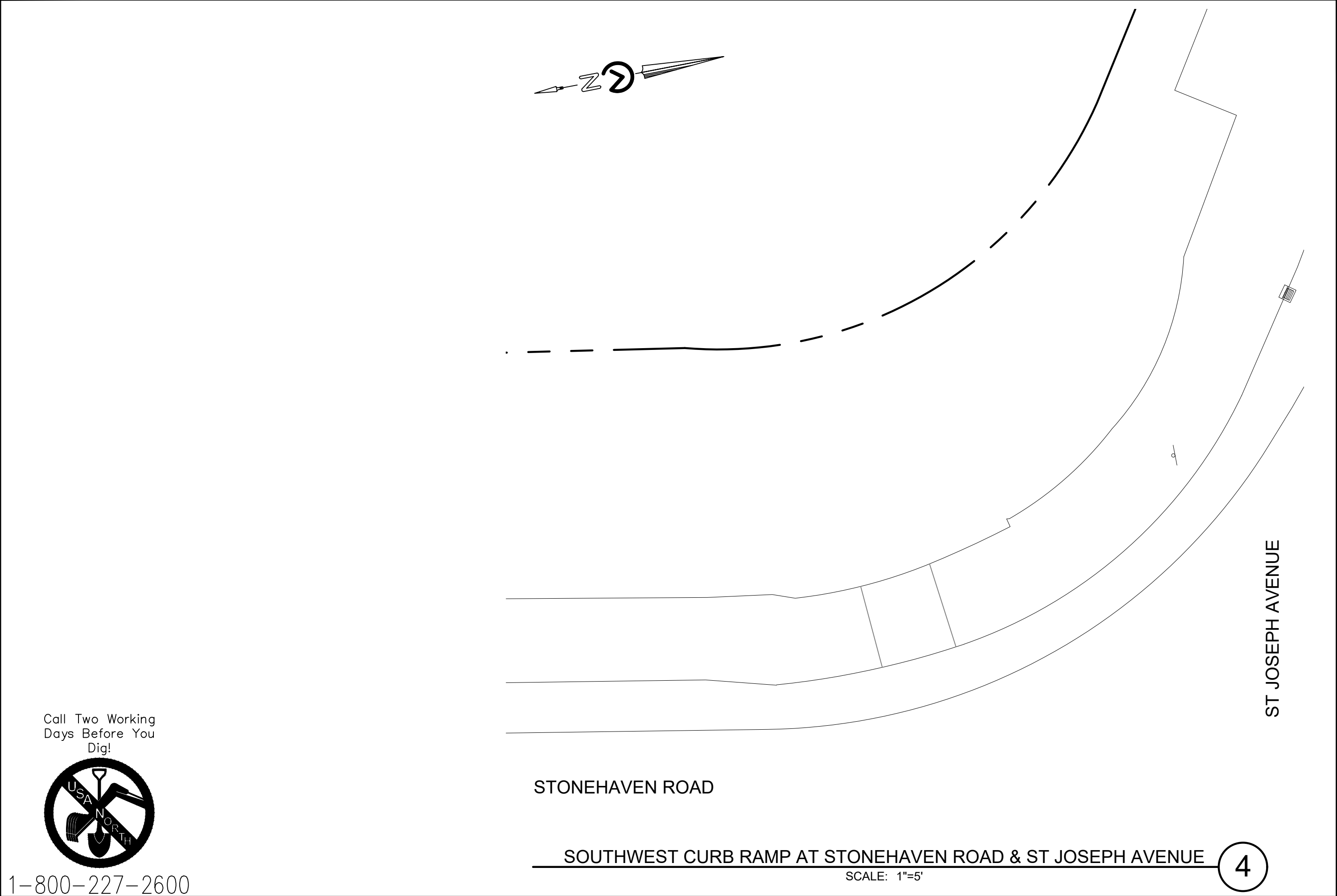
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SOUTHEAST CURB RAMPS AT SPRINGER ROAD & COVINGTON ROAD

SCALE: 1"=5'

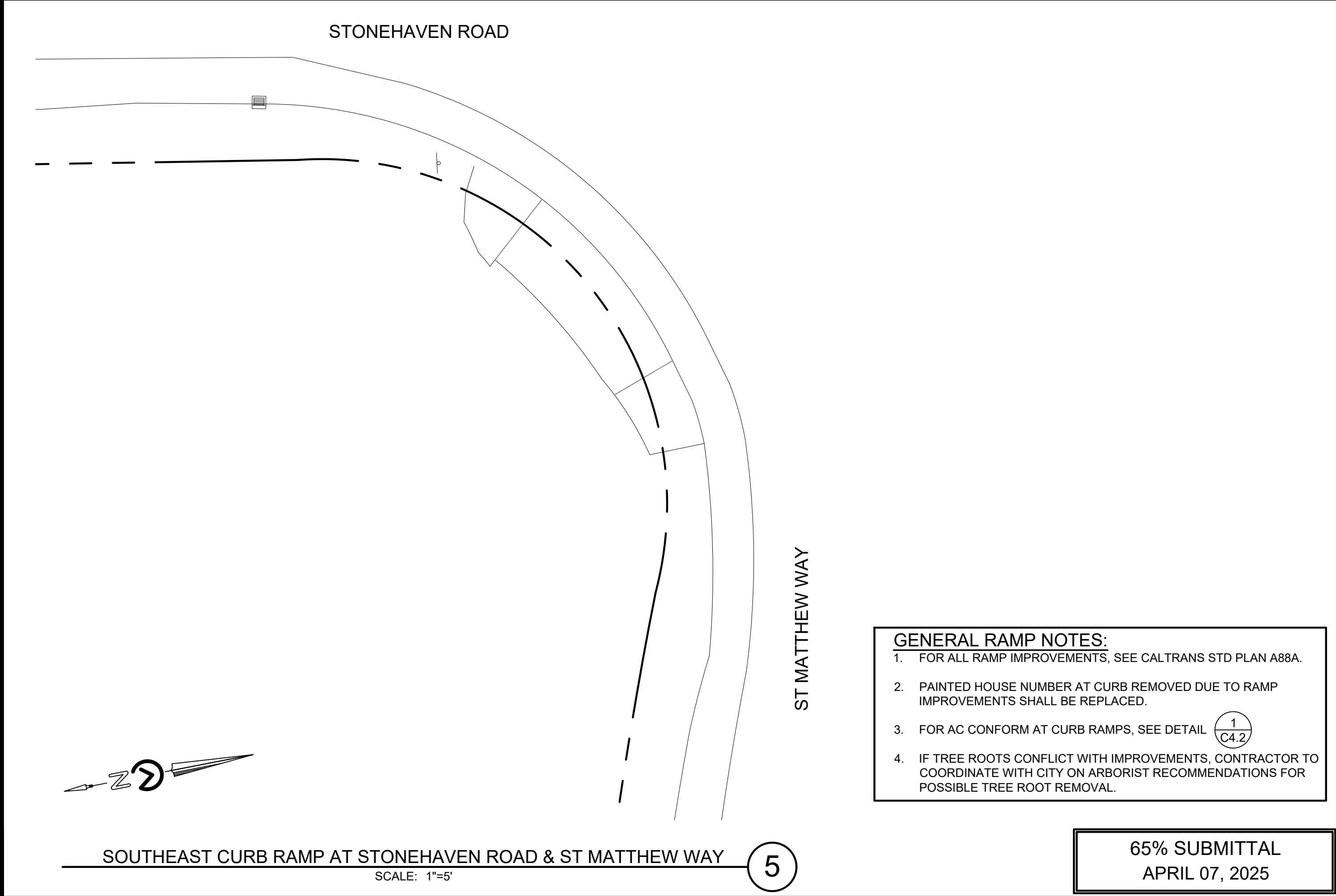
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SOUTHWEST CURB RAMP AT STONEHAVEN ROAD & ST JOSEPH AVENUE

SCALE: 1"=5'


4



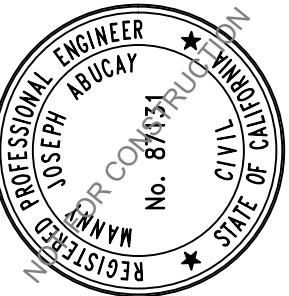

SOUTHEAST CURB RAMP AT STONEHAVEN ROAD & ST MATTHEW WAY

SCALE: 1"=5'

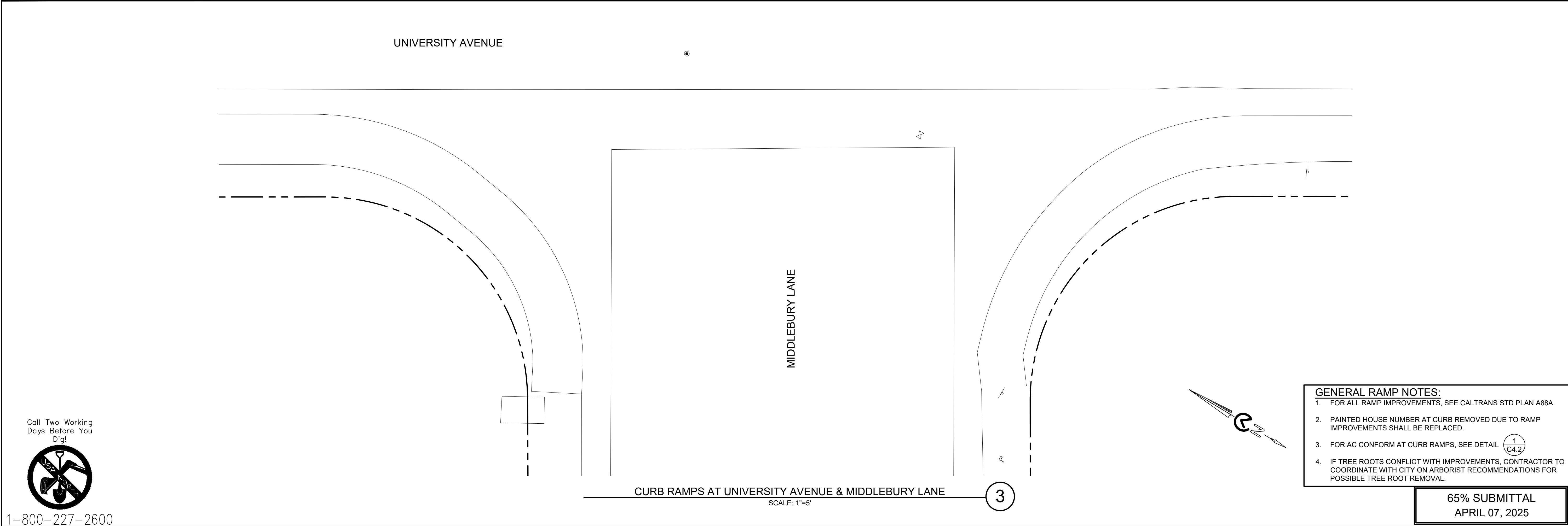
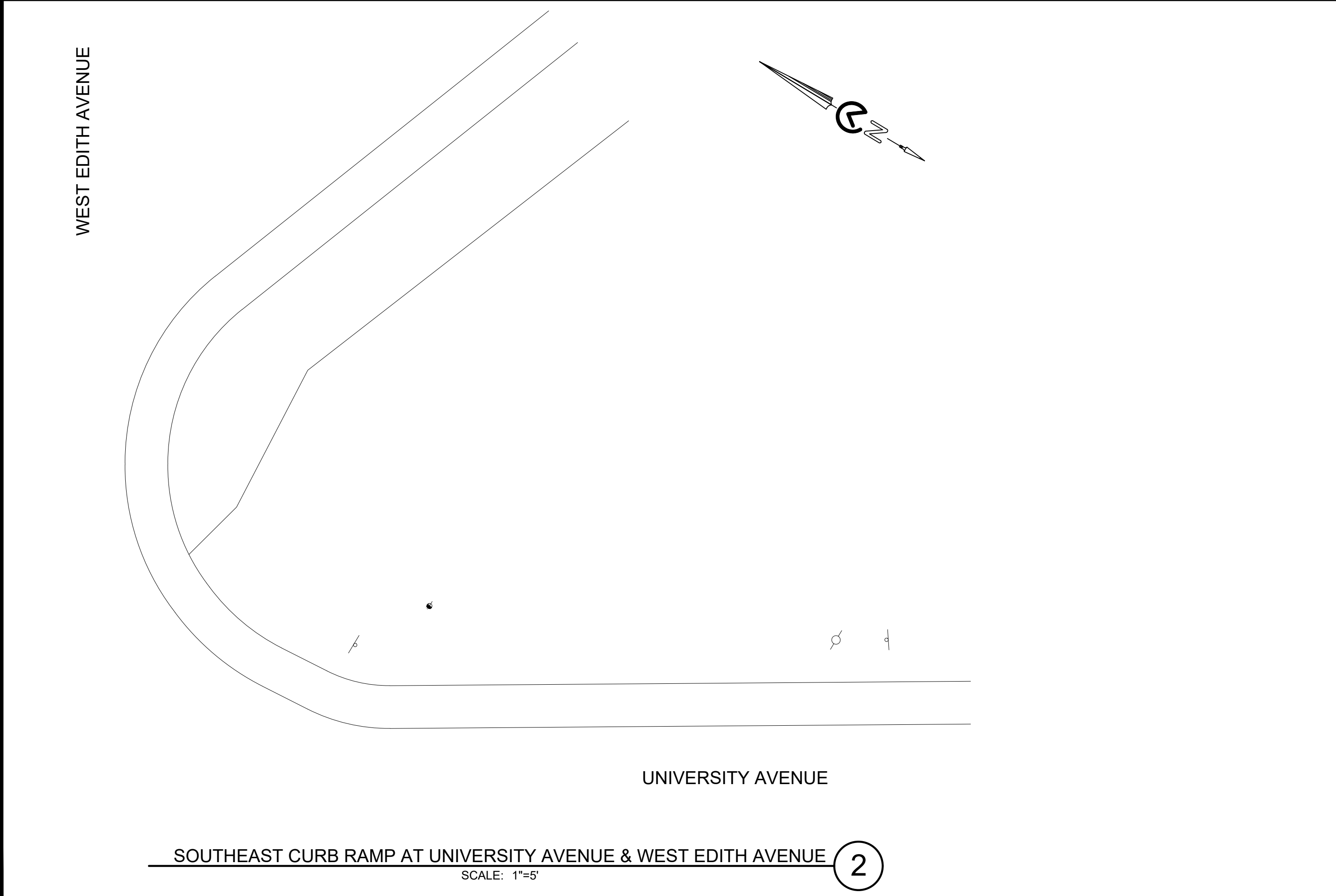
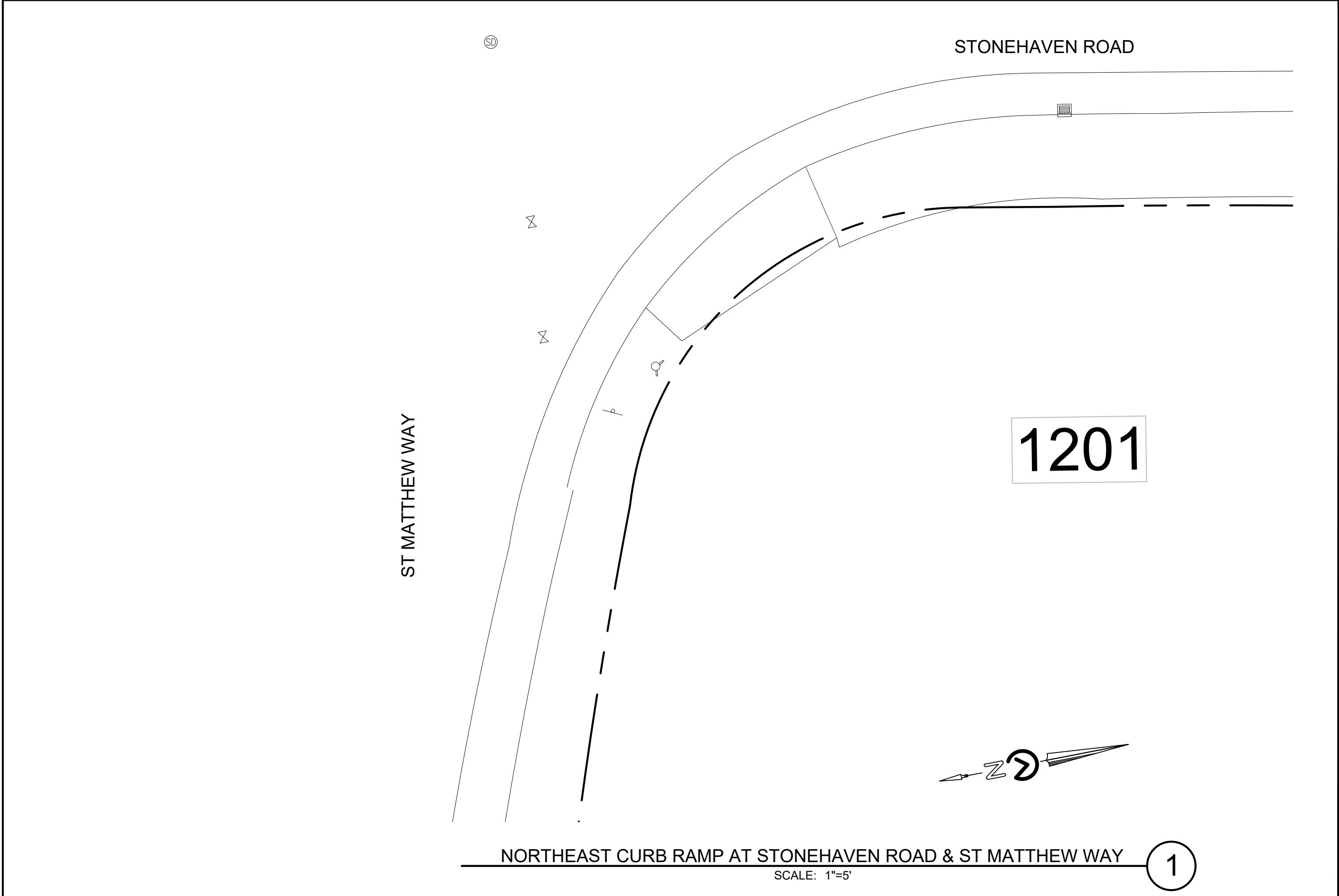
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- GENERAL RAMP NOTES:**
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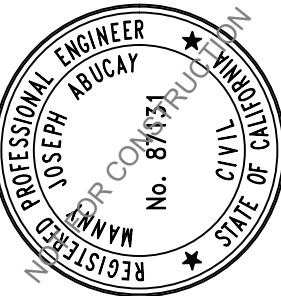

DWG NO.	C5.11	49 OF 50	JOB NO. 250007	LOS ALTOS	SANTA CLARA COUNTY	CALIFORNIA	2024-2025 ANNUAL STREET RESURFACING PROJECT	CIVIL DETAILS	CITY OF LOS ALTOS 1 NORTH SAN ANTONIO ROAD CALIFORNIA 94022			REVISIONS		
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C4.2
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DWG NO.	C5.12	JOB NO.	250007
50 OF 50			
2024-2025 ANNUAL STREET RESURFACING PROJECT			
CIVIL DETAILS			
LOS ALTOS	SANTA CLARA COUNTY	CALIFORNIA	
<div><div></div><div><div>CITY OF LOS ALTOS 1 NORTH SAN ANTONIO ROAD CALIFORNIA 94022</div><div><div>Intelligent Infrastructure Enduring Communities 925 • 685 • 4569 sanbell.com</div></div></div></div>			
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