ABBREVIATIONS:

AD	DREVIATIONS.		
AB	AGGREGATE BASE	MW	MONITORING WELL
AC	ASPHALT CONCRETE	N	NORTHING
ACP	ASBESTOS CEMENT PIPE	NTS	NOT TO SCALE
AT&T	AMERICAN TELEPHONE & TELEGRAPH	OC	ON-CENTER
BC	BEGIN CURVE, BOTTOM OF CURB	OH	OVERHEAD UTILITY LINE
BW	BACK OF WALK	PB	PACIFIC BELL
С	CURVE	PCC	PORTLAND CEMENT CONCRETE
C&G	CURB & GUTTER	PE	POLYETHYLENE
CDF	CONTROLLED DENSITY FILL	PG&E	PACIFIC GAS & ELECTRIC
CI	CAST IRON	PIP	PROTECT IN PLACE
CL	CENTERLINE	PL	PLASTIC
CMP	CORRUGATED METAL PIPE	POC	POINT OF CONNECTION
COM	COMMUNICATION	PT	POINT
CONC	CONCRETE	PVC	POLYVINYL CHLORIDE
CY	CUBIC YARD	R	RADIUS, RIGHT
DI	DRAIN INLET	RC	RELATIVE COMPACTION
DIFF	DIFFERENCE IN ELEVATION BETWEEN NEW & EX	RCP	REINFORCED CONCRETE PIPE
DWY	DRIVEWAY		REHABILITATION
E	ELECTRICAL, EASTING	RT	RIGHT
EC	EDGE OF CONCRETE, END CURVE	R/W	RIGHT OF WAY
EP	EDGE OF PAVEMENT	S	SOUTH
EX	EXISTING	SD	STORM DRAIN
FDAC	FULL DEPTH ASPHALT CONCRETE	SDMH	
FG	FINISH GRADE	SDR	STANDARD DIMENSION RATIO
FH	FIRE HYDRANT	SL	STATION LINE, STREET LIGHT
FL	FLOW LINE	SM	SURVEY MARKER
FO	FIBER OPTICS	SS	SANITARY SEWER
FS	FIRE SERVICE	SSCO	SANITARY SEWER CLEANOUT
FW	FACE OF WALK	SSMH	SANITARY SEWER MANHOLE
G	GAS	STA	STATION
GB	GRADE BREAK	STD	STANDARD
HDPE	HIGH DENSITY POLYETHYLENE PIPE	SW	SIDEWALK
HP	HIGH POINT	TC	TOP OF CURB
HV	HIGH VOLTAGE	TEL	TELEPHONE, TELECOM
HYD	FIRE HYDRANT	TFL	THEORETICAL FLOW LINE
INV	INVERT	TTC	THEORETICAL TOP OF CURB
IRR	IRRIGATION	TYP	TYPICAL

GENERAL NOTES:

MANHOLE

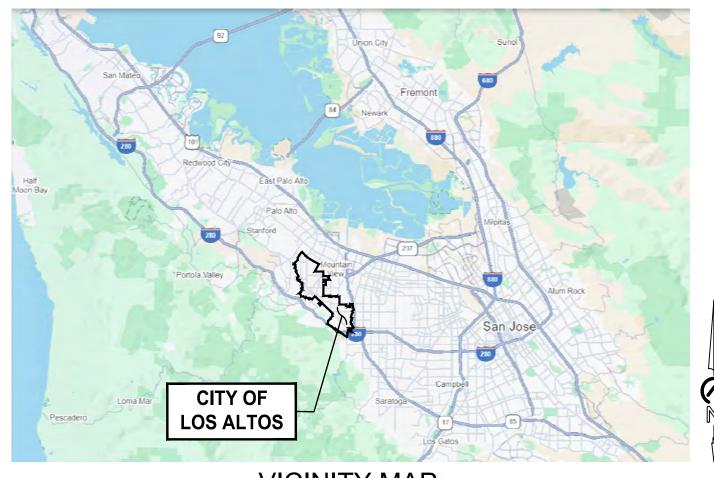
MON, M MONUMENT

- ALL MATERIAL AND WORKMANSHIP SHALL FULLY CONFORM WITH THE SPECIFICATIONS, STANDARDS, AND ORDINANCES OF THE CITY OF LOS ALTOS. STANDARD SPECIFICATIONS AND DETAILS ARE AVAILABLE IN THE OFFICE OF THE CITY ENGINEER.
- 2. THE OFFICE OF PUBLIC WORKS INSPECTION SHALL BE NOTIFIED AT LEAST 72 HOURS IN ADVANCE OF ANY WORK.
- 3. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE EXISTENCE AND LOCATION OF ALL UTILITIES. THE UNDERGROUND CONTRACTOR SHALL NOTIFY UTILITY COMPANIES AT LEAST 48 HOURS IN ADVANCE OF CONSTRUCTION TO FIELD LOCATE UTILITIES. CONTACT UNDERGROUND SERVICE ALERT AT 800-227-2600.
- 4. THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY FIELD CHANGES MADE WITHOUT WRITTEN AUTHORIZATION FROM THE CITY ENGINEER.
- 5. CONTRACTOR SHALL PROVIDE ADEQUATE TRAFFIC CONTROLS & SHALL SUBMIT A TRAFFIC CONTROL PLAN
- 6. ALL EXISTING UTILITIES AND PRIVATE IMPROVEMENTS THAT BECOME DAMAGED DURING CONSTRUCTION SHALL BE COMPLETELY RESTORED TO THE SATISFACTION OF THE CITY ENGINEER, AT CONTRACTOR'S SOLE EXPENSE.
- 7. THE CONTRACTOR AGREES THAT, IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY AND THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY AND NOT JUST DURING NORMAL WORKING HOURS.
- 8. EXCAVATIONS SHALL BE ADEQUATELY SHORED, BRACED AND SHEETED SO THAT THE EARTH WILL NOT SLIDE OR SETTLE AND SO THAT ALL EXISTING IMPROVEMENTS OF ANY KIND WILL BE FULLY PROTECTED FROM DAMAGE. ANY DAMAGE RESULTING FROM A LACK OF ADEQUATE SHORING, BRACING OR SHEETING, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL MAKE NECESSARY REPAIRS OR RECONSTRUCTION AT CONTRACTOR OWN EXPENSE. WHERE THE EXCAVATION FOR A CONDUIT TRENCH, AND/OR STRUCTURE IS FOUR FEET OR MORE IN DEPTH, THE CONTRACTOR SHALL PROVIDE ADEQUATE SHEETING, SHORING AND BRACING OR EQUIVALENT METHOD, FOR THE PROTECTION OF LIFE, OR LIMB, WHICH SHALL CONFORM TO THE APPLICABLE CONSTRUCTION SAFETY ORDERS OF THE DIVISION OF INDUSTRIAL SAFETY OF THE STATE OF CALIFORNIA. THE CONTRACTOR SHALL ALWAYS COMPLY WITH OSHA REQUIREMENTS.
- 9. SHOULD IT APPEAR THAT THE WORK TO BE DONE, OR ANY MATTER RELATIVE THERETO, IS NOT SUFFICIENTLY DETAILED OR EXPLAINED ON THESE PLANS, THE CONTRACTOR SHALL CONTACT THE ENGINEER FOR SUCH FURTHER EXPLANATIONS AS MAY BE NECESSARY.
- 10. EXISTING UTILITIES SHALL BE MAINTAINED IN SERVICE AND IN PLACE BY THE CONTRACTOR DURING CONSTRUCTION UNLESS OTHERWISE SHOWN.
- 11. CONTRACTOR SHALL PROTECT ALL MONUMENTS.
- 12. ALL USA MARKINGS TO BE REMOVED AT END OF CONSTRUCTION.

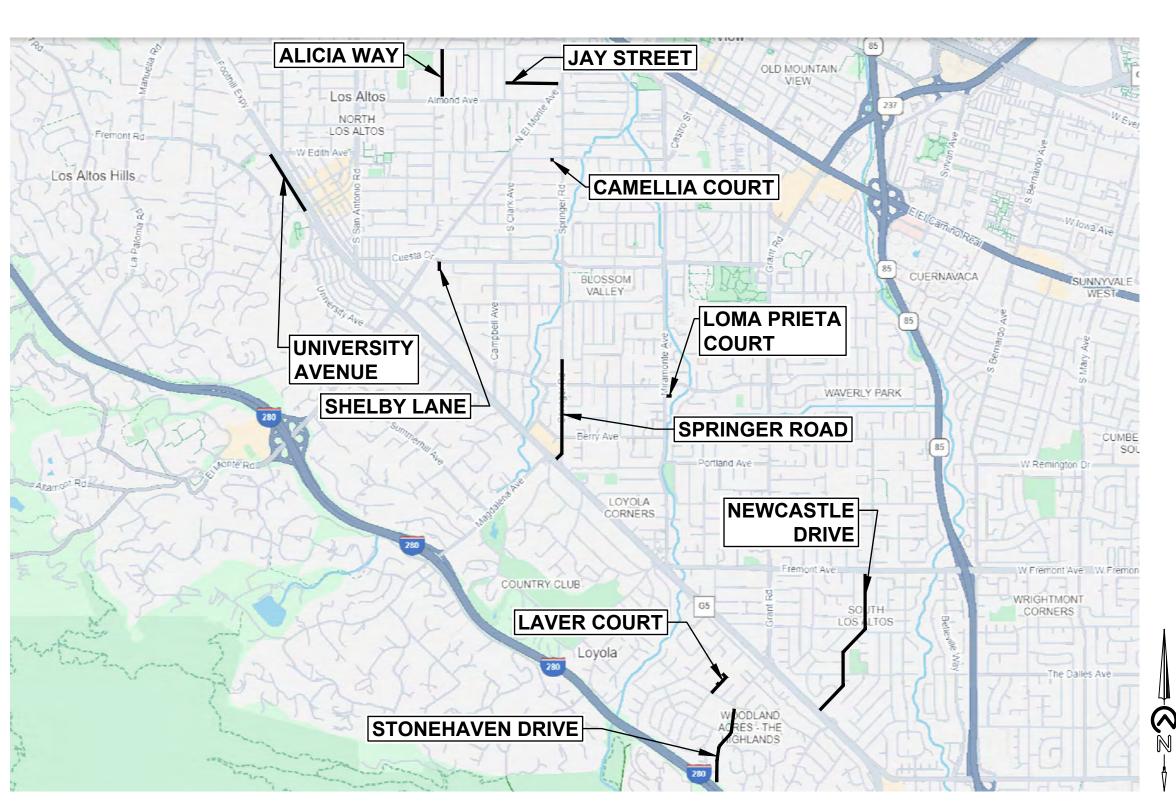
Call Two Working
Days Before You
Dig!



CITY OF LOS ALTOS 2024-2025 ANNUAL STREET RESURFACING PROJECT (TS-01001)



VICINITY MAP:



LOCATION MAP:

NTS

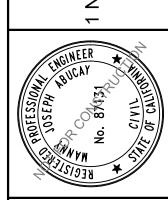
SHEET INDEX:

SHEET NUMBER	DESCRIPTION
1 C1.1	COVER SHEET
2 C2.1	BLUEPRINT FOR A CLEAN BAY
3 C3.1 4 C3.2 5 C3.3 6 C3.4 7 C3.5	TYPICAL SECTIONS TYPICAL SECTIONS TYPICAL SECTIONS TYPICAL SECTIONS TYPICAL SECTIONS
8	IMPROVEMENT PLAN - ALICIA WAY IMPROVEMENT PLAN - ALICIA WAY IMPROVEMENT PLAN - ALICIA WAY IMPROVEMENT PLAN - CAMELLIA COURT IMPROVEMENT PLAN - JAY STREET IMPROVEMENT PLAN - LAVER COURT IMPROVEMENT PLAN - LOMA PRIETA COURT IMPROVEMENT PLAN - LOMA PRIETA COURT IMPROVEMENT PLAN - NEWCASTLE DRIVE IMPROVEMENT PLAN - SPRINGER ROAD IMPROVEMENT PLAN - STONEHAVEN DRIVE IMPROVEMENT PLAN - UNIVERSITY AVENUE
39	CIVIL DETAILS CIVIL DETAILS CIVIL DETAILS CIVIL DETAILS CURB RAMP DETAILS

BY DATE REVISIONS



CITY OF LOS ALTOS RTH SAN ANTONIO ROAE CALIFORNIA 94022



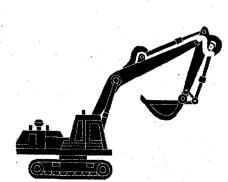
CALIFORNI

OVER SHEET

COVER SH

C1.1

90% SUBMITTAL MAY 16, 2025



Best Management Practices for the

- · Vehicle and equipment operators
- General contractor Home builders

Developers

rom Heavy Equipment on **Construction Sites**

Poorly maintained vehicles and heavy equipment that leak fuel, oil, antifreeze or other luids on the construction site are common sources of storm drain pollution. Prevent spil and leaks by isolating equipment from runol channels, and by watching for leaks and other maintenance problems. Remove construction

Doing The Right Job

General Business Practices

or secured plastic sheeting.

Schedule grading and excavation projects

Doing the Job Right

Site Planning and Preventive Vehicle

- ☐ Maintain all vehicles and heavy equipment Inspect frequently for and repair leaks.
- Perform major maintenance, repair jobs, and vehicle and equipment washing off site where cleanup is easier.
- If you must drain and replace motor oil, radiator coolant, or other fluids on site, use drip pans o drop cloths to catch drips and spills. Collect all properly dispose as hazardous waste (recycle
- ☐ Do not use diesel oil to lubricate equipment parts, or clean equipment. Use only water for
- Cover exposed fifth wheel hitches and other oily or greasy equipment during rain events.

Storm water Pollution

equipment from the site as soon as possible

Roadwork

Clean up spills immediately when they

☐ Sweep up spilled dry materials

immediately. Never attempt to "wash

Use as little water as possible for dust

control. Ensure water used doesn't

☐ Clean up spills on dirt areas by digging

up and properly disposing of

leave silt or discharge to storm drains

them away" with water, or bury them

Paving ■ Never hose down "dirty" pavement or impermeable surfaces where fluids have Best Management Practices for the spilled. Use dry cleanup methods Construction Industry absorbent materials, cat litter, and/or ans) whenever possible and properl dispose of absorbent materials.



Report significant spills to the appropriate local spill response

- Best Management Practices for the Road crews If the spill poses a significant hazard to
- human health and safety, property or the environment, you must also report it to the State Office of Emergency
- - Driveway/sidewalk/parking lot construction Seal coat contractors

Construction inspectors

General contractors

Home builders

Developers

Operators of grading equipment, paving

Road paving, surfacing, and pavement remova happen right in the street, where there are Inumerous opportunities for asphalt, saw-cut slurry or excavated material to illegally enter storm drains Extra planning is required to store and dispose of materials properly and guard against pollution of

Never wash excess material from exposed- aggregate concrete or similar treatments into a street or storm drain **General Business Practices** Collect and recycle, or dispose to di

- Develop and implement erosion/sediment control plans for roadway embankments. Cover stockpiles (asphalt, sand, etc.) ☐ Schedule excavation and grading work during
- and other construction materials with plastic tarps. Protect from rainfall and prevent runoff with temporary roofs of Check for and repair leaking equipment. lastic sheets and berms.

for dust control.

Asphalt/Concrete Removal

Avoid creating excess dust when

breaking asphalt or concrete.

☐ When making saw outs, use as little

water as possible. Shovel or vacuum

Cover or protect storm drain inlets

luring saw-cutting. Sweep up, and

properly dispose of, all residues

Sweep, never hose down streets to

clean up tracked dirt. Use a street

vacuumed liquor in storm drains.

☐ Never clean brushes or rinse paint

drain. French drain, or stream.

For water-based paints, paint out

containers into a street, gutter, storm

brushes to the extent possible, and rinse

into a drain that goes to the sanitary

sewer. Never pour paint down a storm

☐ For oil-based paints, paint out brushes to

saw-cut slurry and remove from the site.

sweeper or vacuum truck. Do not dump

properly dispose of contaminated soil

Avoid over-application by water trucks

- ☐ Perform major equipment repairs at designated Park paving machines over drip pans or areas in your maintenance yard, where absorbent material (cloth, rags, etc.) to cleanup is easier. Avoid performing equipment catch drips when not in use.
- repairs at construction sites. Clean up all spills and leaks using "drv" ☐ When refueling or when vehicle/equipment methods (with absorbent materials maintenance must be done on site, designate a location away from storm drains and creeks.
- Do not use diesel oil to lubricate equipment Collect and recycle or appropriately parts or clean equipment dispose of excess abrasive gravel or Recycle used oil, concrete, broken asphalt, etc. whenever possible, or dispose of properly:

During Construction

Doing The Job Right

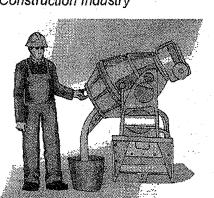
- Avoid paving and seal coating in wet weather, or when rain is forecast, to prevent fresh materials from contacting stormwater runoff
- Cover and seal catch basins and manholes After breaking up old pavement, be sure when applying seal coat, slurry seal, fog seal to remove all chunks and pieces. Make sure broken pavement does not come in contact with rainfall or runoff
- Protect drainage ways by using earth dikes

Storm Drain Pollution from Roadwork

and Mortar **Application**

Best Management Practices for the Construction Industry

Fresh Concrete



Best Management Practices for the

- Masons and bricklavers
- Sidewalk construction crews Patio construction workers
- Construction inspectors General contractors
- Home builders
- Developers
- Concrete delivery/pumping workers

General Business Practices

Doing The Job Right

- ☐ Wash out concrete mixers only in designated wash-out areas in your vard, away from storm drains and waterways, where the water will flow into a temporary waste pit in a dirt area. Let water percolate through soil and dispose of settled, hardened concrete as garbage. Whenever possible, recycle washout to
- pumping back into mixers for reuse. ☐ Wash out chutes onto dirt areas at site that do
- not flow to streets or drains. Always store both dry and wet materials unde cover, protected from rainfall and runoff and away from storm drains or waterways. Protect
- Secure bags of cement after they are open. Be sure to keep wind-blown cement powder away from streets, gutters, storm drains, rainfall, and
- Do not use diesel fuel as a lubricant on concrete forms, tools, or trailers.

Storm Drain Pollution from Fresh Concrete and Mortar Applications

Fresh concrete and cement-related mortars that wash into lakes, streams, or estuaries are toxic to fish and the aquatic environment. Disposing of these materials to the storm drains or creeks can block storm drains, causes serious problems, and is

During Construction Preventing Pollution: Don't mix up more fresh concrete or It's Up to Us cement than you will use in a two-hou

In the Santa Clara Valley, storm drains transport water directly to local creeks and San Francisco Bay without treatment. Storm water pollution is a serious problem for wildlife dependent on our waterways and for the people who live near polluted streams or bay lands. Some common sources of this pollution include spilled oil, fuel, and fluids from vehicles and heavy equipment; construction debris; sediment created by erosion; landscaping runoff containing pesticides or weed killers; and materials such as used motor oil, antifreeze, and paint products that people

Thirteen valley municipalities have joined together with Santa Clara County and the Santa Clara Valley Water District to educate local residents and businesses and fight storm water pollution. TO comply with this program, contractors most comply with the practices described this drawing sheet.

pour or spill into a street or storm drain.

Spill Response Agencies DIAL 9-1-1

State Office of Emergency Services Warning 800-852-7550 Center (24 hours): Santa Clara County Environmental Health (408) 299-6930

<u>Local Pollution Control</u>

County of Santa Clara Pollution Prevention (408) 441-1195

County of Santa Clara Integrated Waste Management Program: (408) 441-1198

Environmental Crimes Hotline (408) 299-TIPS

Santa Clara County 1-800-533-8414 Recycling Hotline:

(408) 265-2600 Santa Clara Valley Water District Pollution

Regional Water Quality Control Board San Francisco Bay Region: (510) 622-2300

Palo Alto Regional Water Quality (650) 329-2598

(650) 947-2752 Engineering Department: (650) 947-2780

Best Management Practices for the Construction Industry

Pool Maintenance

Gardening, and

Landscaping,



Best Management Practices for the

- Landscapers
- Gardeners Swimming pool/spa service and repair
- General contractors
- Home builders

Homeowners

Swimming Pool Maintenance increase the likelihood that earth and garden chemicals will run off into the storm drains during irrigation or when it rains. Swimming pool water containing chlorine and copper-based algaecides should never be discharged to storm drains. These

Do not blow or rake leaves, etc. into the street, or place yard waste in gutters or or ☐ Protect stockpiles and landscaping materials dirt shoulders, unless you are piling them for recycling (allowed by San Jose and from wind and rain by storing them under tarps unincorporated County only). Sweep up any leaves, litter or residue in gutters or or ☐ Store pesticides, fertilizers, and other chemicals indoors or in a shed or storage

Use temporary check dams or ditches to diver the flow line to any storm drain. Pool/Fountain/Spa Maintenance Protect storm drains with sandbags or other

Re-vegetation is an excellent form of erosion When it's time to drain a pool, spa, or fountain please be sure to call your local wastewater reatment plant before you start for further Landscaping/Garden Maintenance Use pesticides sparingly, according to instructions on the label. Rinse empty waste (such as acid wash). Discharge flow containers, and use rinse water as produc shall not exceed 100 gallon per minute Dispose of rinsed, empty containers in the

Filter Cleaning

Never clean a filter in the street or near a

diatomaceous earth filters onto a dirt area

storm drain. Rinse cartridge and

of spent diatomaceous earth in the

If there is no suitable dirt area, call you

local wastewater treatment plant for

or rinse water to the sanitary sewer.

instructions on discharging filter backwas

Draining Pools Or Spas

- trash. Dispose of unused pesticides as ☐ Never discharge pool or spa water to a street or storm drain; discharge to a Collect lawn and garden clippings, pruning sanitary sewer cleanout.
- waste, and tree trimmings. Chip if necessary, If possible, when emptying a pool or spa let chlorine dissipate for a few days and In communities with curbside pick-up of yard then recycle/reuse water by draining it waste, place clippings and pruning waste at the gradually onto a landscaped area. curb in approved bags or containers. Or, take Do not use copper-based algaecides to a landfill that composts yard waste. No curbside pickup of yard waste is available for alternatives, such as sodium bromide

Storm Drain Pollution

From Landscaping and

Painting and **Application of** Solvents and In San Jose, leave yard waste for curbside recycling pickup in piles in the street, 18 Adhesives nches from the curb and completely out o

Best Management Practices for the Construction Industry



- Paperhangers
- Home builders Developers



Best Management Practices for the

- Floor covering installers General contractors

Doing The Job Right **Handling Paint Products**

- Keep all liquid paint products and wastes away from the gutter, street, and storm drains. Liquid residues from paints, thinners solvents, glues, and cleaning fluids are hazardous wastes and must be disposed of at a hazardous waste collection facility (contac your local stormwater program listed on the back of this brochure).
- the extent possible and clean with thinner When thoroughly dry, empty paint cans, used or solvent in a proper container. Filter and reuse thinners and solvents. Dispose of brushes, rags, and drop cloths may be excess liquids and residue as hazardous disposed of as garbage in a sanitary landfill. Empty, dry paint cans also may be recycled as Paint Removal ☐ Wash water from painted buildings constructed Paint chips and dust from non-hazardous before 1978 can contain high amounts of lead,
- begin stripping paint or cleaning pre-1978 building exteriors with water under high pressure, test paint for lead by taking paint scrapings to a local laboratory. See Yellov Pages for a state-certified laboratory. ☐ If there is loose paint on the building, or if the paint tests positive for lead, block storm drains Check with the wastewater treatment plant to

Storm Drain Pollution from Paints, Solvents, and Adhesives

determine whether you may discharge water to

the sanitary sewer, or if you must send it offsite

All paints, solvents, and adhesives contain chemicals that are harmful to wildlife in local reeks, San Francisco Bay, and the Pacific Ocean products or from cleaning residues or rags. Paint material and wastes, adhesives and cleaning fluids should be recycled when possible, or disposed of properly to prevent these materials from flowing nto storm drains and watercourses.

dry stripping and sand blasting may be even if paint chips are not present. Before you swept up or collected in plastic drop cloths and disposed of as trash.

Chemical paint stripping residue and chips and dust from marine paints or paints containing lead, mercury or tributyl tin must be disposed of as hazardous wastes Lead based paint removal requires a state-certified contractor. When stripping or cleaning building

storm drains. Direct wash water onto a dirt area and spade into soil. Or, check with the local wastewater treatment authority to find out if you can collect (mop or vacuum) building cleaning water and dispose to the sanitary sewer. Sampling of the water may be required to assist the wastewater treatment authority in making its decision

exteriors with high-pressure water, block

Recycle/Reuse Leftover Paints Whenever Possible (latex) paint, or return to supplier. Reuse leftover oil-based paint. Dispose

of non-recyclable thinners, sludge and unwanted paint, as hazardous waste. Unopened cans of paint may be able to be returned to the paint vendor. Check with

the vendor regarding its "buy-back" policy.

of the plan shall be in accordance with guidelines published by the city engineer A storm water pollution prevention plan shall be prepared and available at the construction sites for all projects greater than one

Prior approval shall be obtained from the city engineer or designee to discharge water pumped from construction sites to the storm drain. The city engineer or designee may require gravity settling and filtration upon a determination that either or both would improve the water quality of the discharge. Contaminated groundwater or water that exceeds state or federal requirements for is may not be discharged to the storm drain. Such water may be discharged to the sewer, provider that the requirements of Section 10.08.240 are met and the approval of the superintendent is obtained prior to discharge. D. No cleanup of construction debris from the streets shall result in the discharge of water to the storm drain system; nor shall any

construction debris be deposited or allowed to be deposited in the storm drain system. (Prior code § 5-5.643) Criminal and judicial penalties can be assessed for non-compliance.

Best Management

Practices for the

☐ Set up and operate small mixers on

☐ When cleaning up after driveway or

Protect applications of fresh concrete

the street or storm drain.

the material has dried.

☐ Wash down exposed aggregate

tarps or heavy plastic drop cloths.

sidewalk construction, wash fines onto

concrete only when the wash water car

(1) flow onto a dirt area; (2) drain onto a

pumped and disposed of properly; or (3)

he vacuumed from a catchment created

necessary, divert runoff with temporary

berms. Make sure runoff does not reach

by blocking a storm drain inlet. If

When breaking up pavement, be sure to

pick up all the pieces and dispose of

small amounts of excess dry concrete

street, storm drains, drainage ditches, or

properly. Recycle large chunks of

Never bury waste material. Dispose of

grout, and mortar in the trash.

Never dispose of washout into the

broken concrete at a landfill.

dirt areas, not down the driveway or into

Los Altos Municipal Code Requirements

A. Unlawful discharges. It shall be unlawful to discharge any domestic waste or industrial waste into storm drains, gutters, creeks, o San Francisco Bay. Unlawful discharges to storm drains shall include, but not be limited to, discharge from toilets; sinks; industrial processes; cooling systems; boilers; fabric cleaning; equipment cleaning; vehicle cleaning; construction activities, including, but not limited to, painting, paving, concrete placement, saw cutting and grading; swimming pools; spas; and fountains, unless specificall permitted by a discharge permit or unless exempted pursuant to guidelines published by the superintendent Threatened discharges. It shall be unlawful to cause hazardous materials, domestic waste, or industrial waste to be deposited in

such a manner or location as to constitute a threatened discharge into storm drains, gutters, creeks or San Francisco Bay. A "threatened discharge" is a condition creating a substantial probability of harm, when the probability and potential extent of harm make it reasonably necessary to take immediate action to prevent, reduce or mitigate damages to persons, property or natural resources. Domestic or industrial wastes that are no longer contained in a pipe, tank or other container are considered to be threatened discharges unless they are actively being cleaned up.

Los Altos Municipal Code Section 10.08.430 Requirements for construction operations.

Los Altos Municipal Code Chapter 10.08.390 Non-storm water discharges

- A. A spill response plan for hazardous waste, hazardous materials and uncontained construction materials shall be prepared and available at the construction sites for all projects where the proposed construction site is equal to or greater than one acre of disturbed soil and for any other projects for which the city engineer determines is necessary to protect surface waters. Preparation
- acre of disturbed soil and for any other projects for which the city engineer determines that a storm water management plan is necessary to protect surface waters. Preparation of the plan shall be in accordance with guidelines published by the city engineer.

Agencies

County of Santa Clara District Attorney

Santa Clara Valley Water

1-888-510-5151

Serving East Palo Alto Sanitary District, Los Altos, Los Altos Hills, Mountain View, Palo Alto, Stanford

City of Los Altos

Building Department:

General Construction And Site Supervision

Best Management Practices For Construction

Inspectors

Home builders



General contractors Site supervisors

Storm Drain Pollution from **Construction Activities** Construction sites are common sources of storn water pollution. Materials and wastes that blow wash into a storm drain, gutter, or street have a direct impact on local creeks and the Bay. As a contractor, or site supervisor, owner o operator of a site, you may be responsible for vironmental damage caused by your stractors or employees.

<u>Doing The Job Right</u> General Principals

- Keep an orderly site and ensure good ousekeeping practices are used.
- ☐ Keep materials away from streets, storm drains and drainage channels ☐ Ensure dust control water doesn't leave site or Advance Planning To Prevent Pollution

☐ Schedule excavation and grading activities for

dry weather periods. To reduce soil erosion

- plant temporary vegetation or place other Erosion and Sediment Control Manual, available from the Regional Water Quality Control Board, Control the amount of runoff crossing your site (especially during excavation!) by using berms or temporary or permanent drainage ditches to divert water flow around the site. Reduce storm
- check dams or berms where appropriate. Train your employees and subcontractors. lake these best management practices construction site. Inform subcontractors about the storm water requirements and their own 300d Housekeeping Practices
- maintenance. The designated area should be well away from streams or storm drain inlets Keep materials out of the rain - prevent runoff contamination at the source. Cover exposed piles of soil or construction materials with plast sheeting or temporary roofs. Before it rains, drain to storm drains, creeks, or channels

Keep pollutants off exposed surfaces.

Place trashcans and recycling receptacle

Designate one area of the site for auto parking.

vehicle refueling, and routine equipment

Clean up leaks, drips and other spills immediately so they do not contaminat soil or groundwater or leave residue on paved surfaces. Use dry cleanup method: whenever possible. If you must use water, Maintain equipment properly. use just enough to keep the dust down. Cover materials when they are not in use. Cover and maintain dumpsters. Check frequently for leaks. Place dumpsters under

- secured around the outside of the dumpster. Never clean out a dumpster b hosing it down on the construction site. Set portable toilets away from storm drains. Make sure portable toilets are in good working order. Check frequently for leaks ials/Waste Handling
- ☐ Practice Source Reduction minimize waste when you order materials. Order only the amount you need to finish the job. Use recyclable materials whenever possible. Arrange for pick-up of recyclable naterials such as concrete, asphalt, scrap metal, solvents, degreasers, cleared vegetation, paper, rock, and vehicle maintenance materials such as used oil antifreeze, batteries, and tires.

roofs or cover with tarps or plastic sheeting

Dispose of all wastes properly. Many construction materials and wastes, including solvents, water-based paints vehicle fluids, broken asphalt and concrete wood, and cleared vegetation can be recycled. Materials that cannot be recycled must be taken to an appropriate landfill or disposed of as hazardous waste. Never bury waste materials or leave them in the street or near a creek or stream bed.

In addition to local building permits, you

will need to obtain coverage under the State's General Construction Activity

site disturbs one acre or more. Obtain

nformation from the Regional Water

n water Permit if your constructi

Earth-Moving Dewatering **Activities**

Best Management Practices for the Construction Industry



Best Management Practices for the

· Buildozer, back hoe, and grading machine · Dump truck drivers Site supervisors General contractors

Home builders

Developers

Doing The Job Right General Business Practices Schedule excavation and grading work during

Perform major equipment repairs away from the ☐ When refueling or vehicle/equipment maintenance must be done on site, designate a

location away from storm drains

parts, or clean equipment.

Do not use diesel oil to lubricate equipment

Practices During Construction Remove existing vegetation only when vegetation for erosion control on slopes or where construction is not immediately planned. Protect down slope drainage courses, streams, and storm drains with wattles, or temporary

Erosion and Sediment Control Field Manual fo proper erosion and sediment control Storm Drain Pollution

drainage swales. Use check dams or ditches

to divert runoff around excavations. Refer to

the Regional Water Quality Control Board's

Soil excavation and grading operations loosen large amounts of soil that can flow or blow into storn drains when handled improperly. Sediments in runoff destroy habitats in creeks and the Bay. Effective erosion control practices reduce the amount of runoff crossing a site and slow the flow with check dams or

from Earth-Moving Activities and Dewatering

Contaminated groundwater is a common problem in the Santa Clara Valley. Depending on soil types and site history, groundwater pumped from construction sites may be contaminated with toxics (such as oil or solvents) or laden with sediments. Any of these pollutants can harm wildlife in creeks or the Bay, or interfere with wastewater treatment plant operation Discharging sediment-laden water from a dewatering site into any water of the state without treatment is prohibited.

Cover stockpiles and excavated soil with secured tarps or plastic sheeting. **Dewatering Operations** . Check for Toxic Pollutants Check for odors, discoloration, or an oil

Call your local wastewater treatment agency and ask whether the groundwate must be tested. ☐ If contamination is suspected, have the water tested by a certified laboratory. Depending on the test results, you may be

to the storm drain (if no sediments

sheen on groundwater.

be required to collect and haul pumped groundwater offsite for treatment and disposal at an appropriate treatment Check for Sediment Levels If the water is clear, the pumping time in

less than 24 hours, and the flow rate is

allowed to discharge pumped groundwater

less than 20 gallons per minute, you may pump water to the street or storm drain If the pumping time is more than 24 hour and the flow rate greater than 20 gpm, call your local wastewater treatment plan for guidance. If the water is not clear, solids must be filtered or settled out by pumping to a settling tank prior to discharge. Options

Pumping through a perforated pipe

sunk part way into a small pit filled

Pumping from a bucket placed below

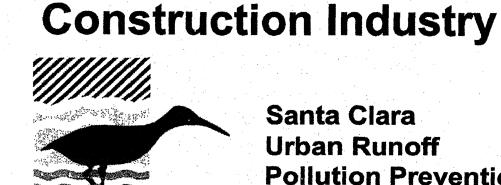
Pumping through a filtering device

water level using a submersible pump

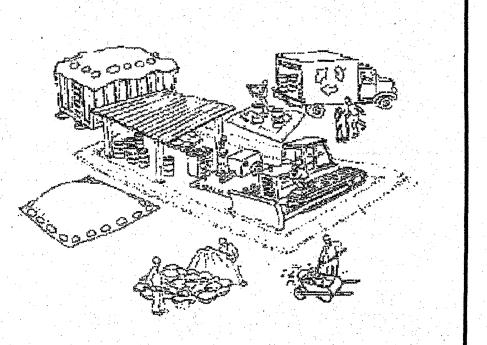
such as a swimming pool filter or filter fabric wrapped around end of suction When discharging to a storm grain, protect the inlet using a barrier of burlap bags filled with drain rock, or cover inlet wit filter fabric anchored under the grate. OR pump water through a grassy swale prior

Blueprint for a Clean Bay

Remember: The property owner and the contractor share ultimate responsibility for the activities that occur on a construction site. You may be held responsible for any environmental damage caused by your subcontractors or employees.



Santa Clara **Urban Runoff Pollution Prevention Program**



LARRY LIND	APPROVED BY. CITY OF LOS ALTOS	OCTOBER, 2003
DRAWN BY: VICTOR CHEN	CITY ENGINEER R.C.E.	SCALE: N.T.S.
CHECKED BY: JIM GUSTAFSON	SHEET OF SHEETS	DRAWING NO:
		1

Days Before You

Call Two Working

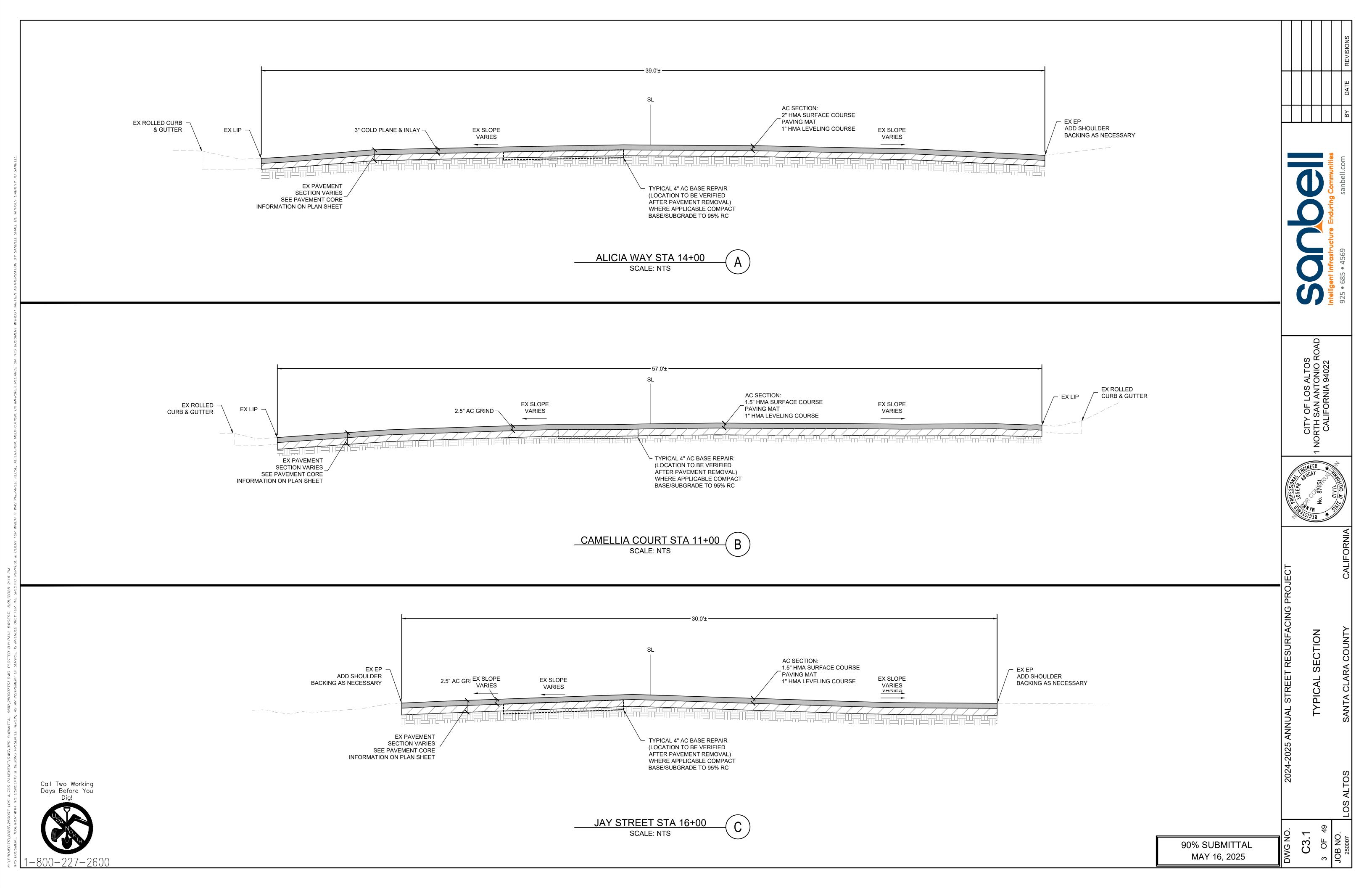


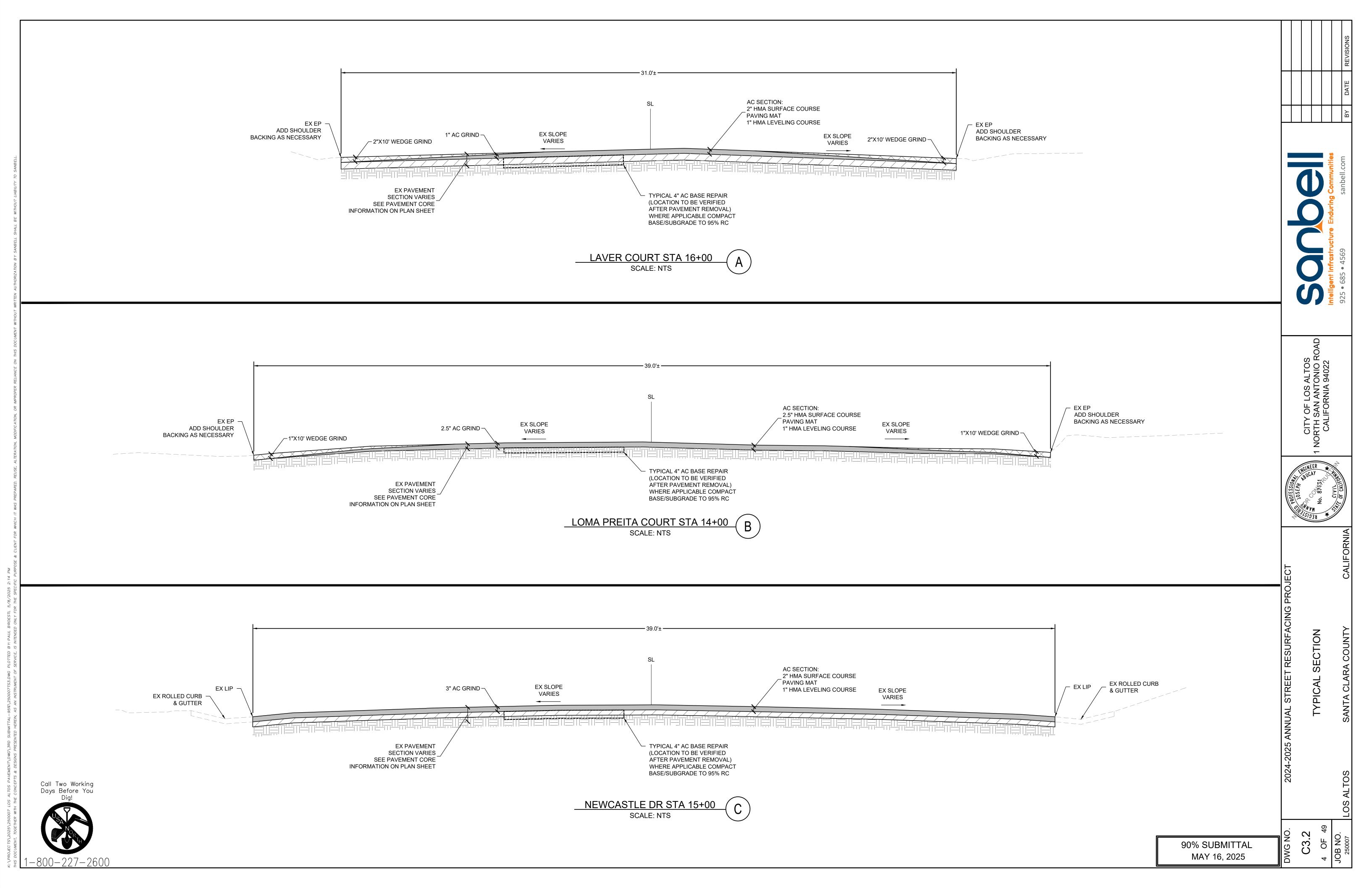
65% SUBMITTAL APRIL 07, 2025

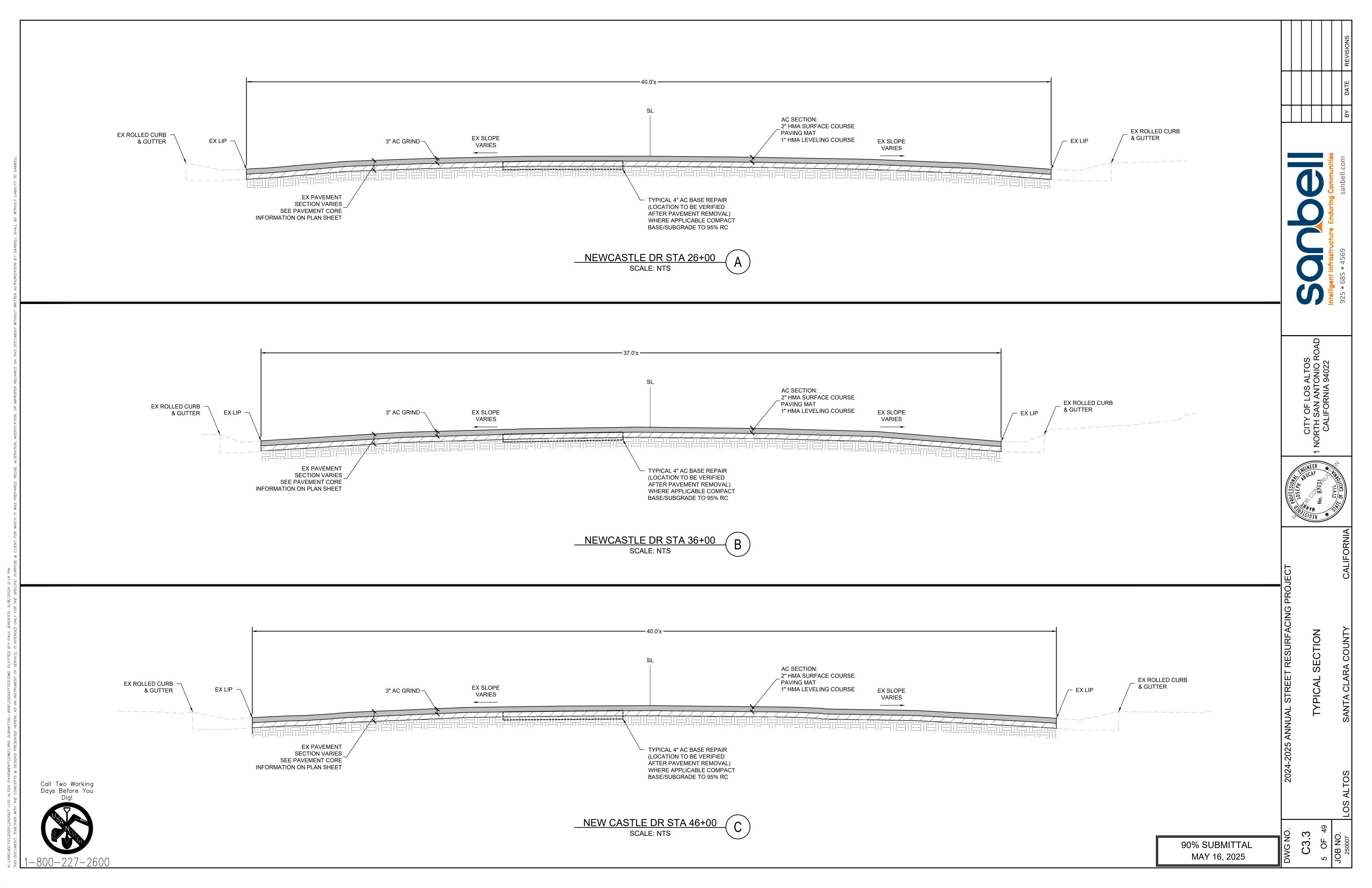
BAY

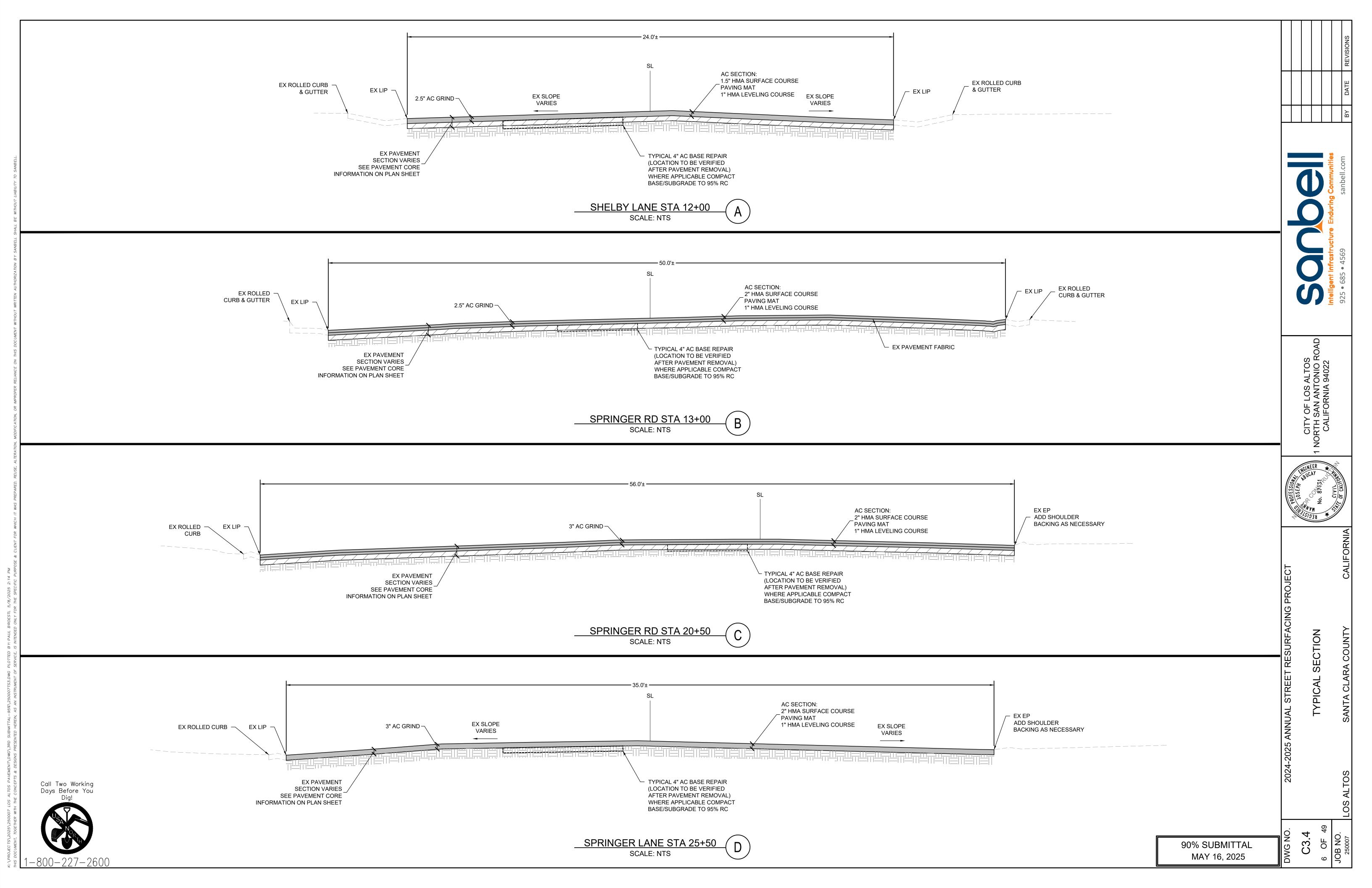
OR

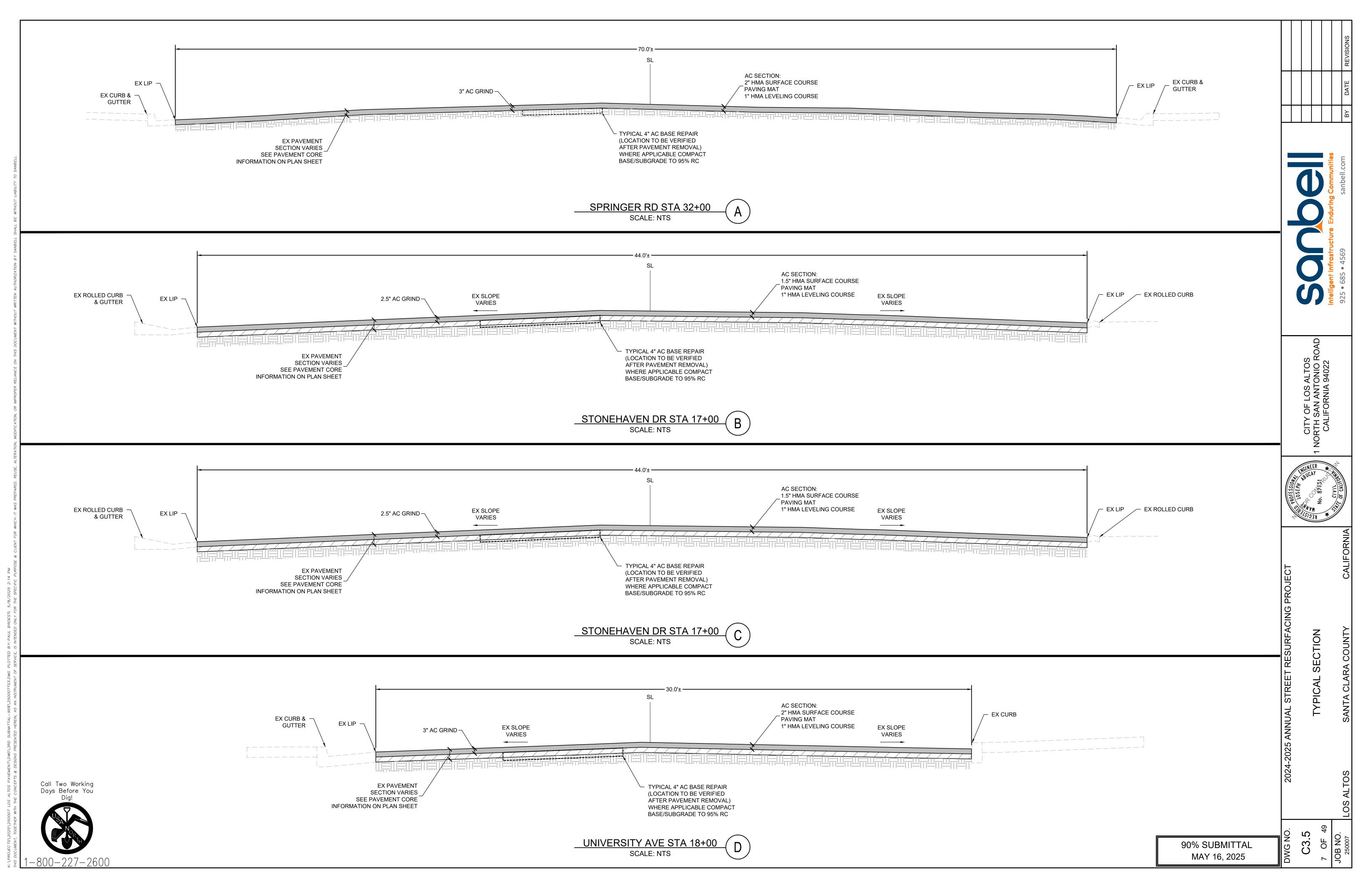
C2.

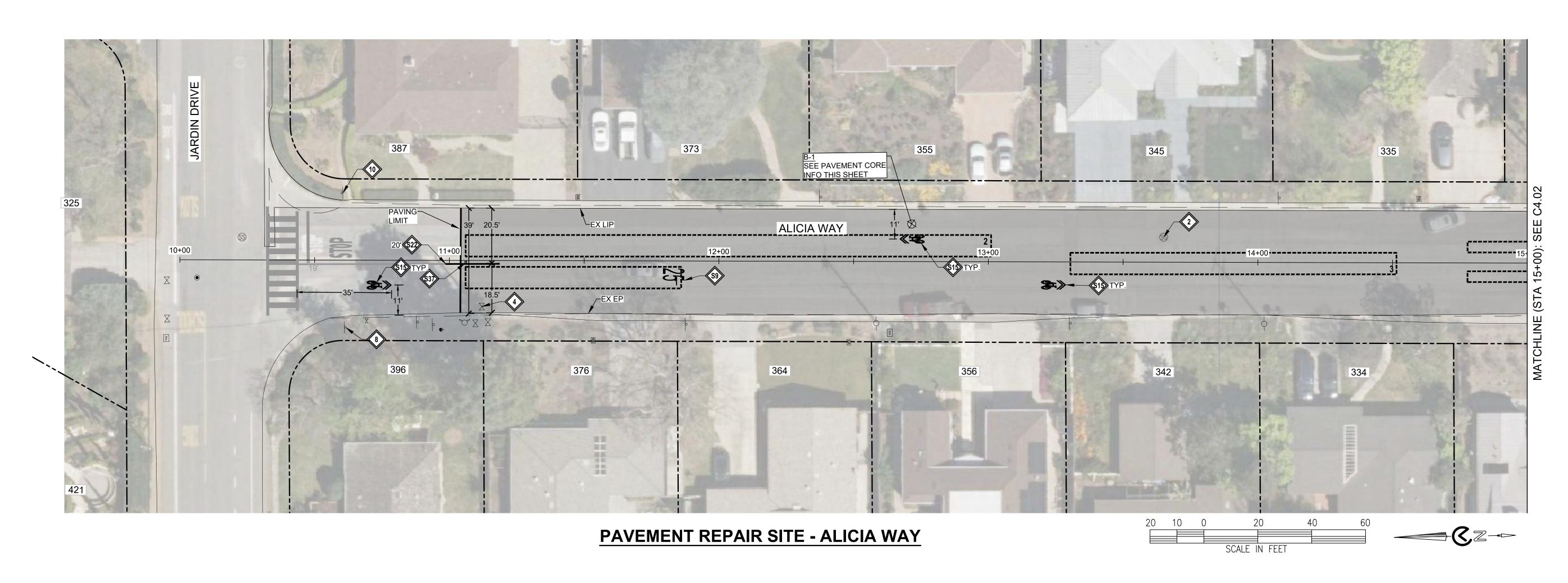












- LOWER & RESTORE TO GRADE STORM MANHOLE
- LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)
- LOWER & RESTORE TO GRADE WATER VALVE
- LOWER & RESTORE TO GRADE GAS VALVE
- LOWER & RESTORE TO GRADE UTILITY BOX
- REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED • SEE DETAIL (C5.1)
- INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL $\begin{pmatrix} X \\ CX.X \end{pmatrix}$
- LOWER & RESTORE TO GRADE SEWER CLEANOUT
- INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. SEE DETAIL X

STRIPING NOTES:

- S1 WHITE LIMIT LINE (STOP LINE)

- MARKING "25"
- MARKING SHARROW
- GREEN DASHED BIKE LANE. SEE DETAIL (B,C)
- DETAIL 22
- DETAIL 39A
- YELLOW LADDER CROSSWALK
- BLUE HYDRANT MARKER
- REPAINT RED CURB
- 36" HIGH WHITE FLEXIBLE CHANNELIZER

LEGEND:



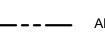


4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL 5



CORE#

APPROXIMATE LIMIT OF WORK



APPROXIMATE RIGHT OF WAY

SUBGRADE CLAYEY SAND / SILTY SAND (SC/SM)

AC (IN) AB (IN) FABRIC ²

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMEN
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	640.0
2	1,560.0
3	968.0
SUBTOTAL AREA (SF)	3168.0
TOTAL AREA (SF)	12,715.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING NOTES

PAVEMENT CORING LOG

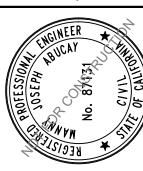
- 1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL
- EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
- 2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

Call Two Working Days Before You



90% SUBMITTAL MAY 16, 2025

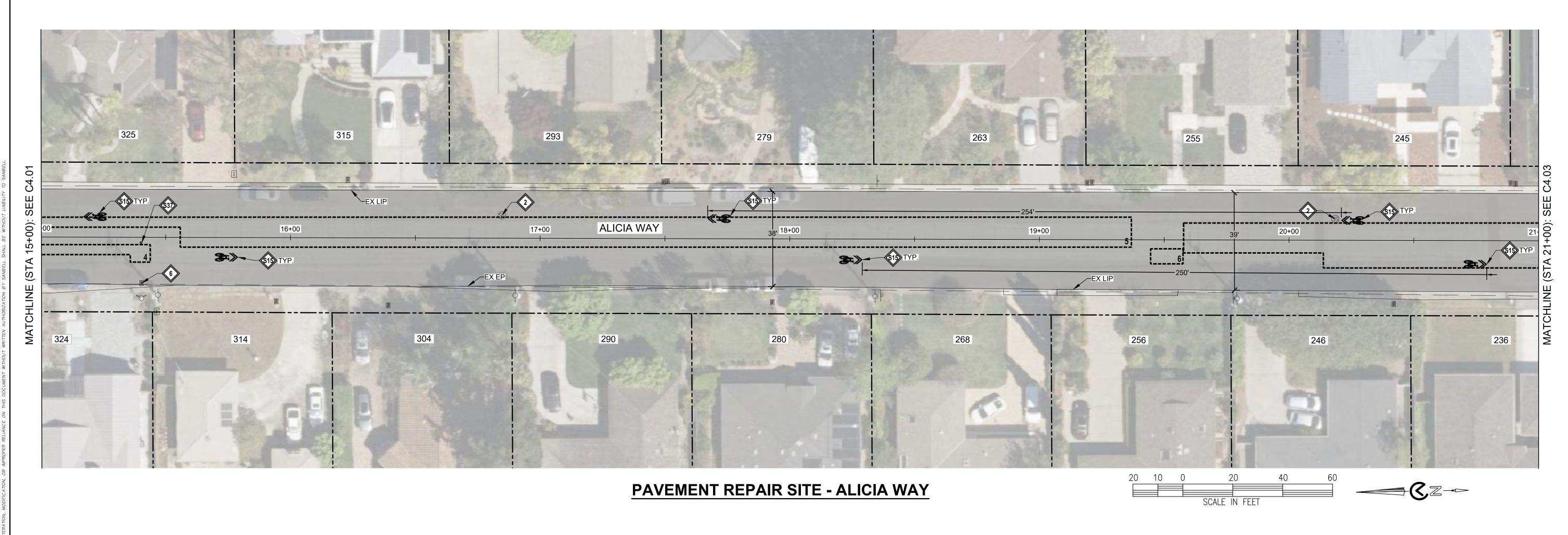




PLAN O to 15+

ENT F 0+00

0. 9F



LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)

LOWER & RESTORE TO GRADE WATER VALVE

LOWER & RESTORE TO GRADE GAS VALVE

LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED

• SEE DETAIL 4 C5.1 INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL $\begin{pmatrix} X \\ CX.X \end{pmatrix}$

LOWER & RESTORE TO GRADE SEWER CLEANOUT

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. SEE DETAIL X

STRIPING NOTES:

S1 WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

MARKING "25"

MARKING SHARROW

GREEN DASHED BIKE LANE. SEE DETAIL (B,C) (C4.3)

DETAIL 22

DETAIL 39A

YELLOW LADDER CROSSWALK

BLUE HYDRANT MARKER

REPAINT RED CURB

36" HIGH WHITE FLEXIBLE CHANNELIZER

LEGEND:

1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL 5

APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
4	288.0
5	4,884.0
6	78.0
SUBTOTAL AREA (SF)	5250.0
TOTAL AREA (SF)	12,715.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

> PLAN 5 to 21+ ENT F 5+00

> > OF

4.02

90% SUBMITTAL MAY 16, 2025



AC BASE REPAIR ALLOCATION QUANTITIES

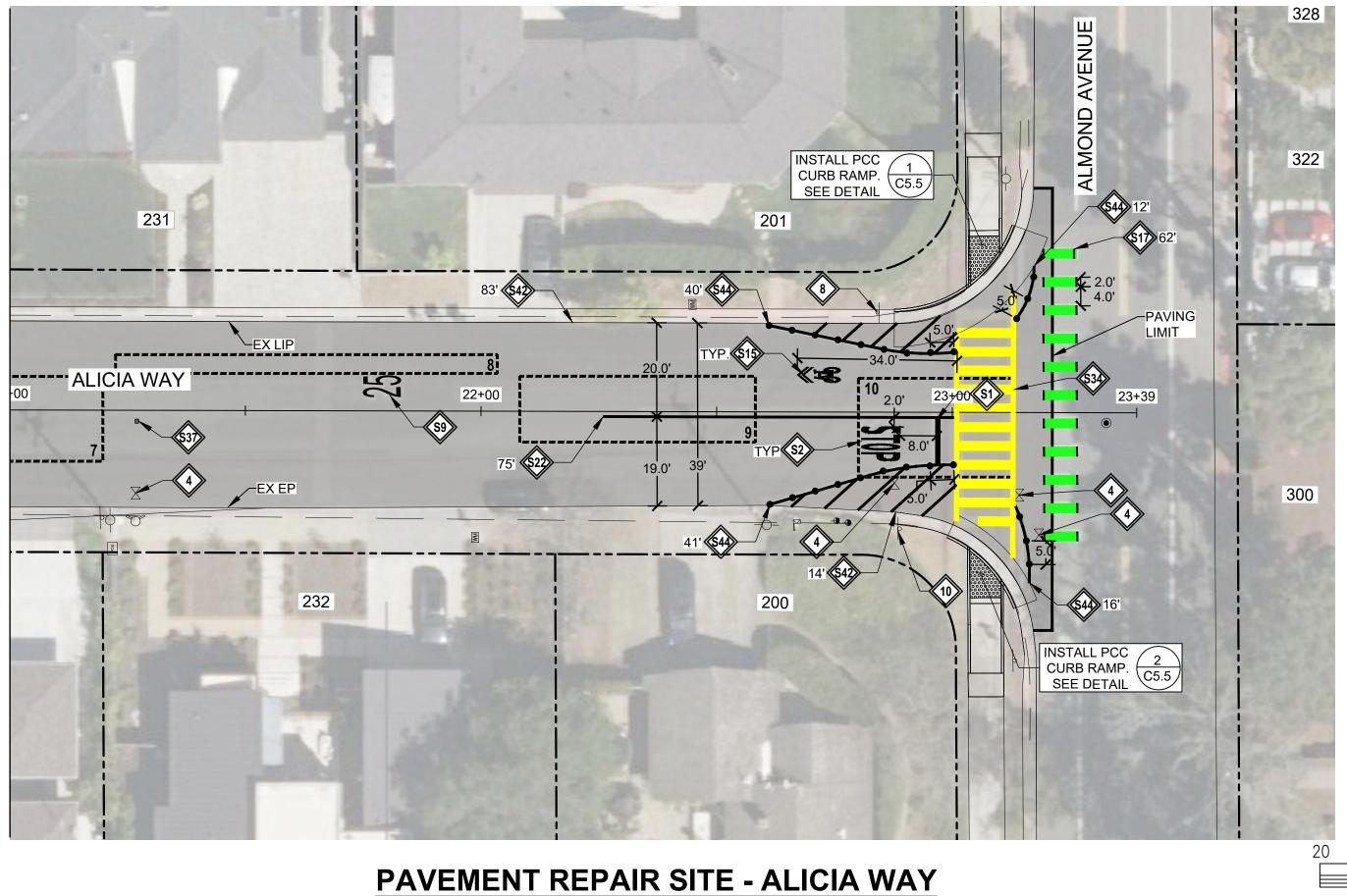
AC PAVEMENT REPAIR #

693.0 SUBTOTAL AREA (SF) 4297.0 TOTAL AREA (SF) 12,715.0

AREA (SF)

700.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.



CONSTRUCTION NOTES:

LOWER & RESTORE TO GRADE STORM MANHOLE

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)

LOWER & RESTORE TO GRADE WATER VALVE

LOWER & RESTORE TO GRADE GAS VALVE

LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED • SEE DETAIL 4 C5.1

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST.

INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL $\begin{pmatrix} X \\ CX.X \end{pmatrix}$

LOWER & RESTORE TO GRADE SEWER CLEANOUT

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

MARKING "25"

MARKING SHARROW

GREEN DASHED BIKE LANE. SEE DETAIL (B,C) (C4.3)

DETAIL 22

DETAIL 39A

YELLOW LADDER CROSSWALK

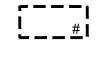
BLUE HYDRANT MARKER

REPAINT RED CURB

36" HIGH WHITE FLEXIBLE CHANNELIZER

LEGEND:

1" LEVELING COURSE + PAVING MAT + 2" INLAY.



4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET.

SEE TYPICAL SECTIONS

SEE DETAIL 5

APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS. 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF
- THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

Call Two Working

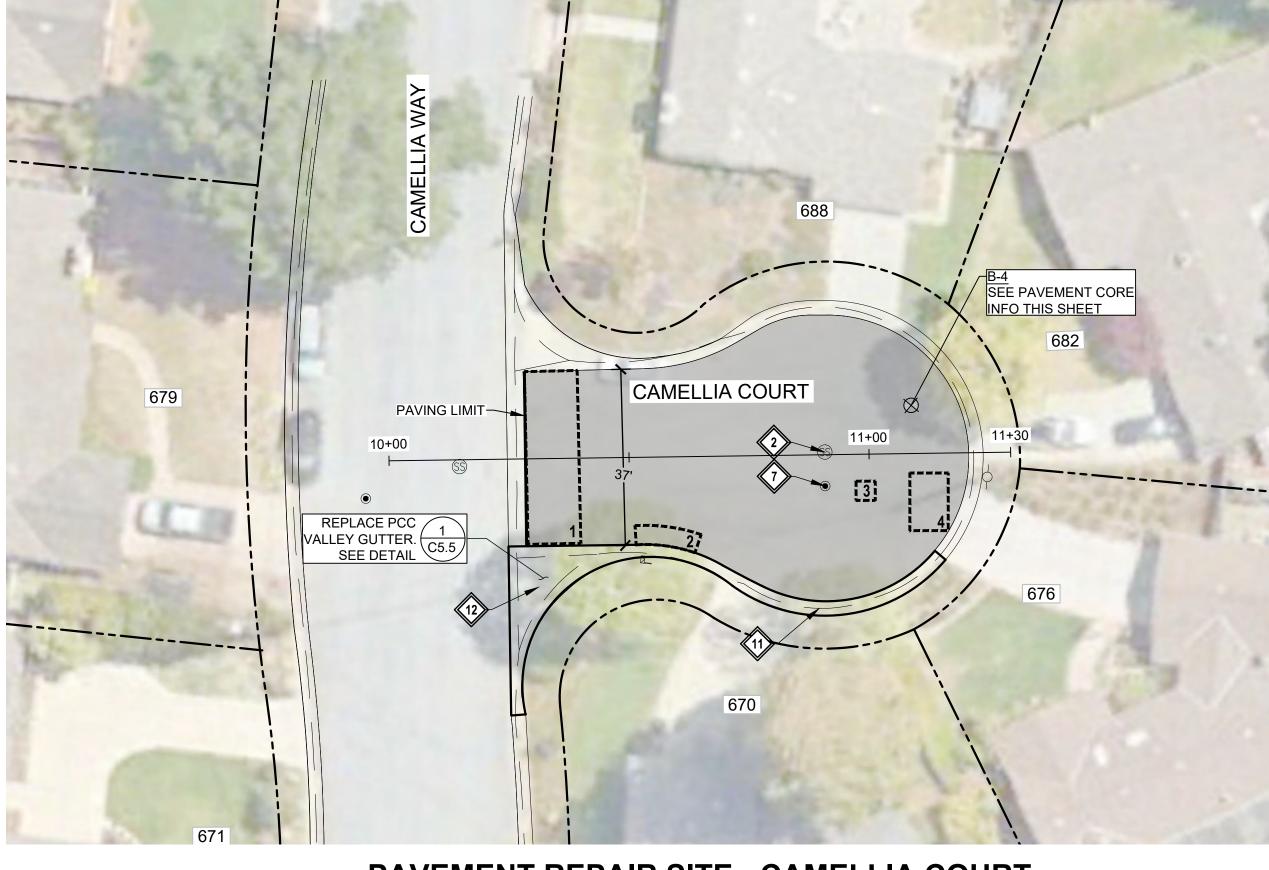
SEE DETAIL X



90% SUBMITTAL MAY 16, 2025

/EMENT PLAN -21+00 to 23+

4.03



PAVEMENT REPAIR SITE - CAMELLIA COURT

LEGEND:

1" LEVELING COURSE + PAVING MAT + 1 1/2" INLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
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AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	396.0
2	53.0
3	16.0
4	96.0
SUBTOTAL AREA (SF)	561.0
TOTAL AREA (SF)	561.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

Call Two Working Days Before You



90% SUBMITTAL MAY 16, 2025

PAVEMENT CORING LOG AC (IN) AB (IN) FABRIC 2 CORE# SUBGRADE SILTY SAND / SAND WITH SILT (SM/SP) **PAVEMENT CORING NOTES** 1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS. 2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS

CONSTRUCTION NOTES:

WORK, & REMOVED AFTER)

SURVEYOR:
• SEE DETAIL

(C5.1)

SEE DETAIL : X

REMOVE & REPLACE.

• SEE DETAIL:

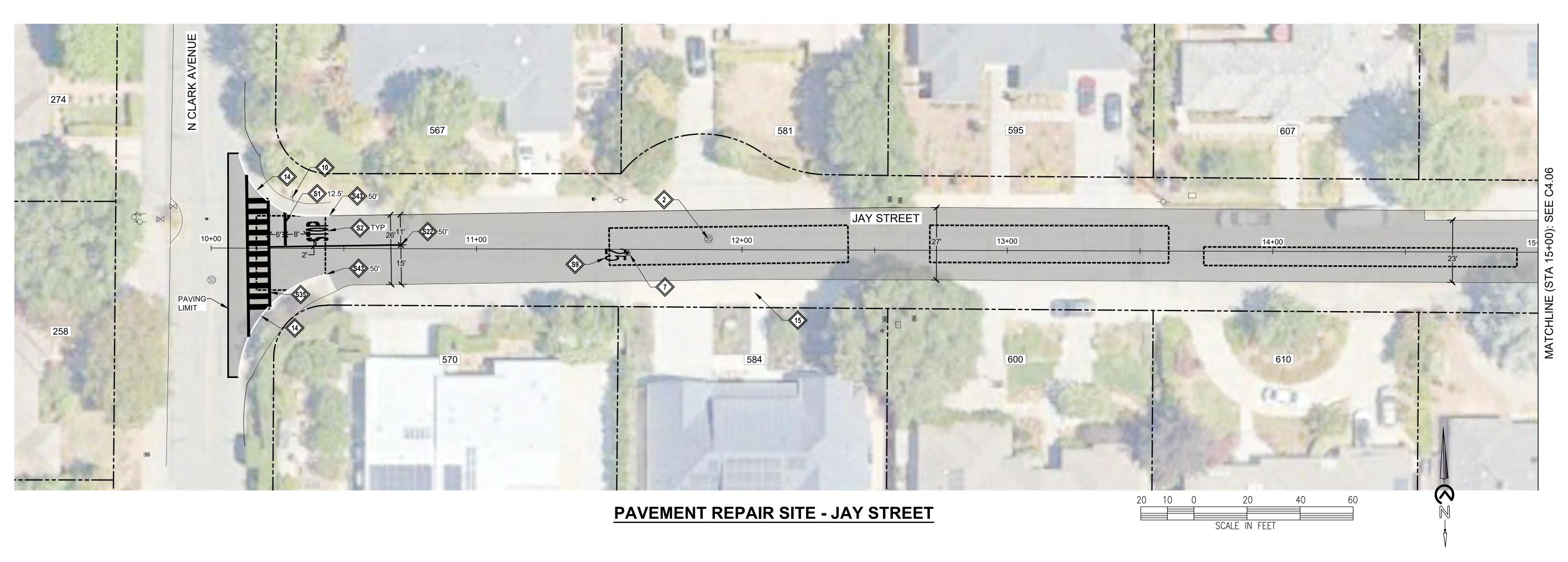
SURVEYOR:

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED

REMOVE & REPLACE PCC ROLLED CURB AND GUTTER.

REMOVE & REPLACE PCC VALLEY GUTTER AND APPRON.



LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE GAS VALVE

6 LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : 4

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. • SEE DETAIL : X

PROTECT IN PLACE EXISTING AC BERM

PROTECT IN PLACE EXISTING BRICK PAVERS

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

(\$3) MARKING WORD "AHEAD"

MARKING "25"

MARKING ARROW TYPE IV (R)

DETAIL 22

YELLOW LADDER CROSSWALK

BLUE HYDRANT MARKER

REPAINT WHITE CURB

LEGEND:

1" LEVELING COURSE + PAVING MAT + 1 1/2" INLAY. SEE TYPICAL SECTIONS



4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X



APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
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AC BASE REPAIR ALLOCATION QUANTITIES

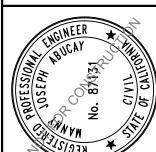
AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0
·	

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

Call Two Working Days Before You

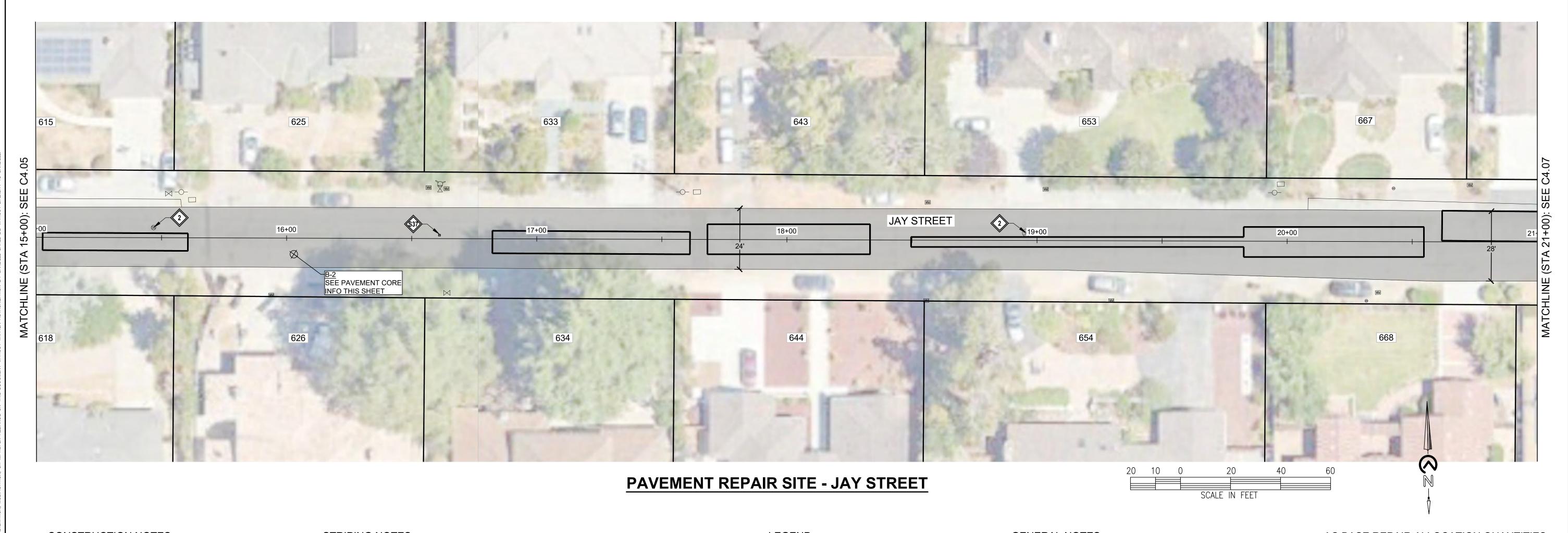


90% SUBMITTAL MAY 16, 2025



JA 00 /EMENT PLAN -10+00 to 15+

4.05 OF



LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE GAS VALVE

LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED • SEE DETAIL : 4

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. SEE DETAIL : X

PROTECT IN PLACE EXISTING AC BERM

PROTECT IN PLACE EXISTING BRICK PAVERS

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

MARKING WORD "AHEAD"

MARKING "25"

MARKING ARROW TYPE IV (R)

DETAIL 22

YELLOW LADDER CROSSWALK

BLUE HYDRANT MARKER

REPAINT WHITE CURB

LEGEND:

1" LEVELING COURSE + PAVING MAT + 1 1/2" INLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

— – – APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
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AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING LOG

		AC (IN) AB (IN)	CORE # SUBGRADE	CORE #
B-2 CLAYEY SAND (SC) 5.0	2.0 N/A	5.0 2.0	B-2 CLAYEY SAND (SC)	B-2

PAVEMENT CORING NOTES

- 1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
- 2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

Call Two Working

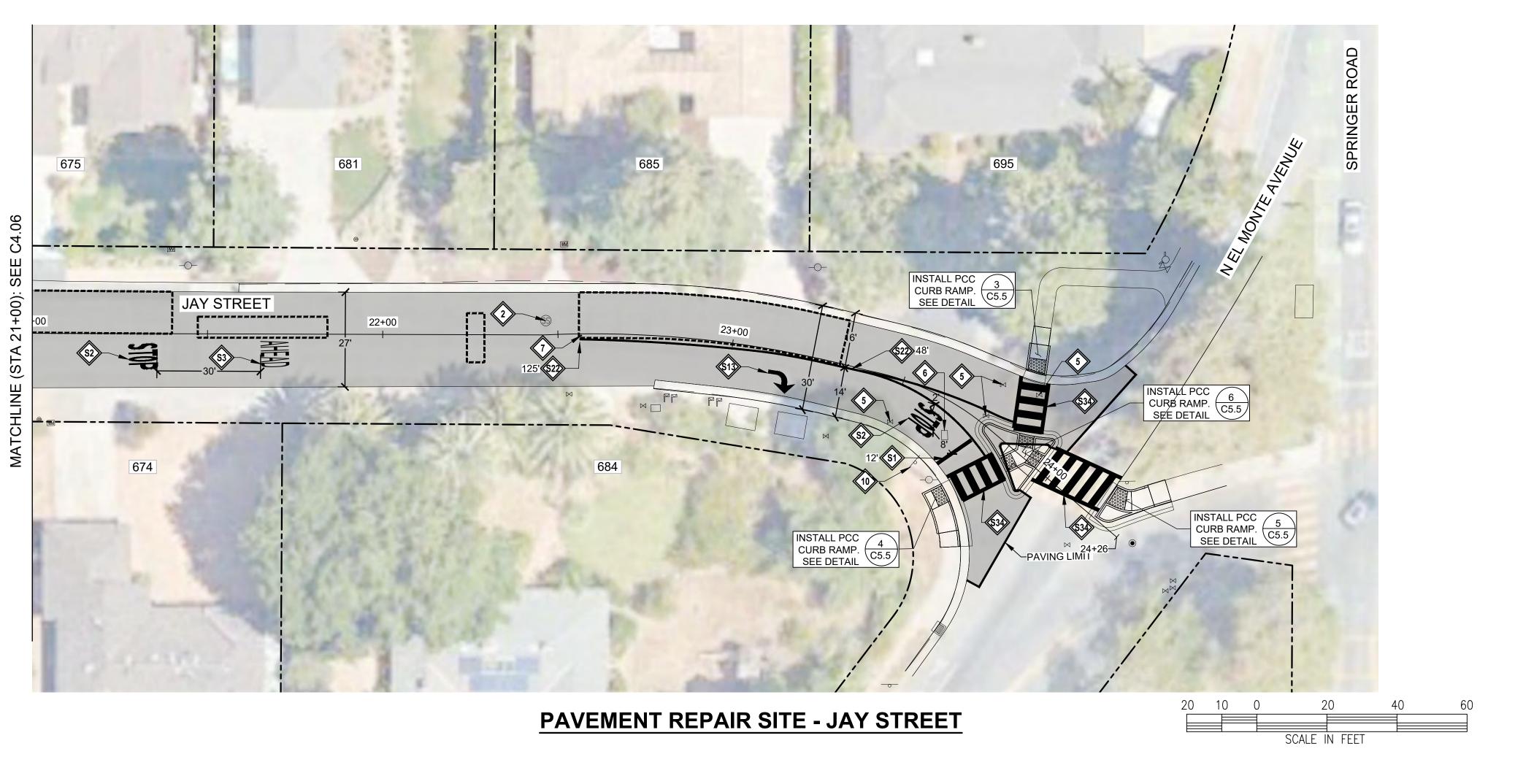


90% SUBMITTAL MAY 16, 2025

4.06

JA 00

/EMENT PLAN -15+00 to 21+



LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE GAS VALVE

LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : 4 C5.1

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. SEE DETAIL : X

PROTECT IN PLACE EXISTING AC BERM

PROTECT IN PLACE EXISTING BRICK PAVERS

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

MARKING WORD "AHEAD"

S9 MARKING "25"

MARKING ARROW TYPE IV (R)

DETAIL 22

(\$3) YELLOW LADDER CROSSWALK

BLUE HYDRANT MARKER

REPAINT WHITE CURB

LEGEND:

2.5" AC COLDPLANE 1" LEVELING COURSE + PAVING MAT + 1 1/2" INLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.

2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.

3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.

5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

Call Two Working Days Before You

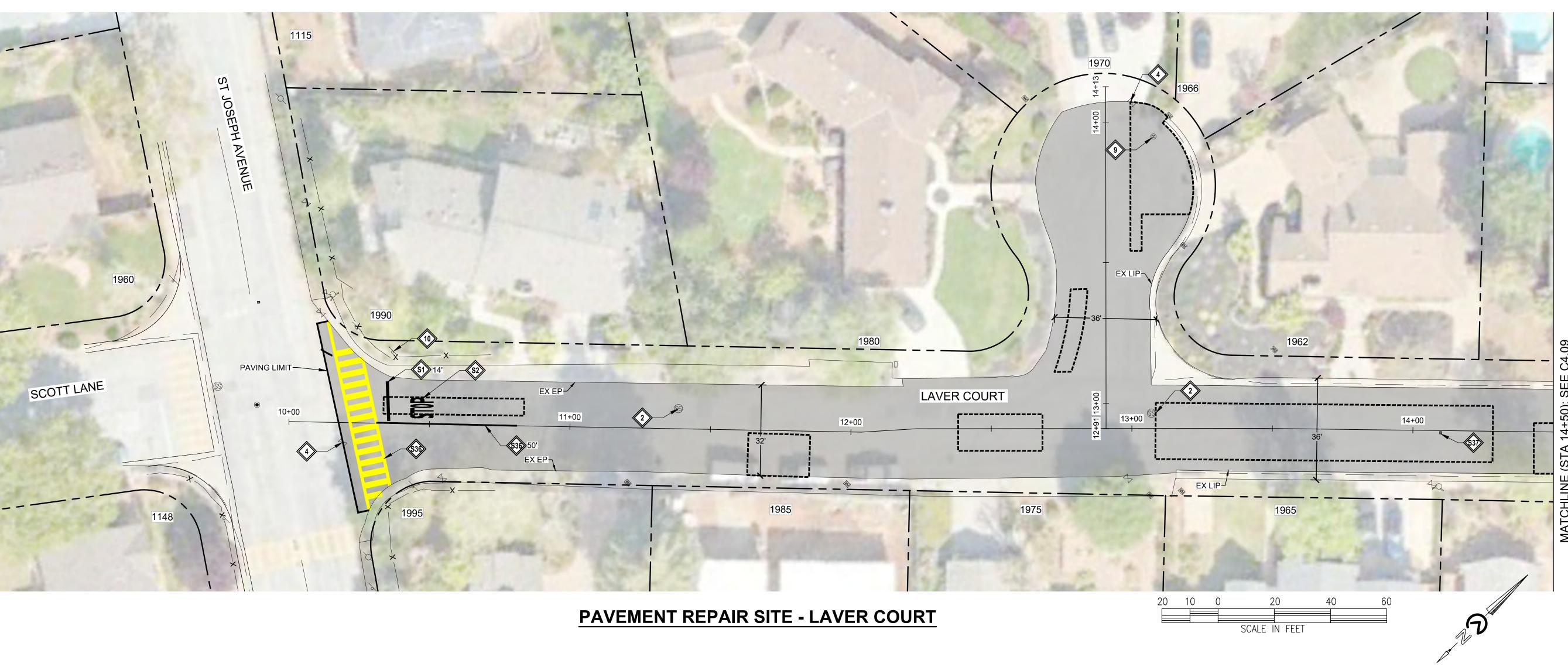


90% SUBMITTAL MAY 16, 2025

4.07

STREE JA, 26

/EMENT PLAN -21+00 to 24+



LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE WATER VALVE

LOWER & RESTORE TO GRADE GAS VALVE

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : 4

LOWER & RESTORE TO GRADE SEWER CLEANOUT

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. SEE DETAIL : X

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

S2 MARKING WORD "STOP"

DETAIL 27B

YELLOW LADDER CROSSWALK \$3 BLUE HYDRANT MARKER

LEGEND:

1" AC COLDPLANE, 2" AC COLDPLANE GRIND 1" LEVELING COURSE + PAVING MAT + 2" OVERLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

— – – — APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)	
1	0.0	
2	0.0	
3	0.0	
4	0.0	
5	0.0	
6	0.0	
SUBTOTAL AREA (SF)	0.0	

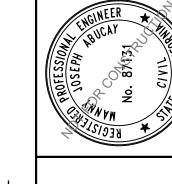
1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

Call Two Working



90% SUBMITTAL MAY 16, 2025

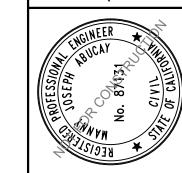
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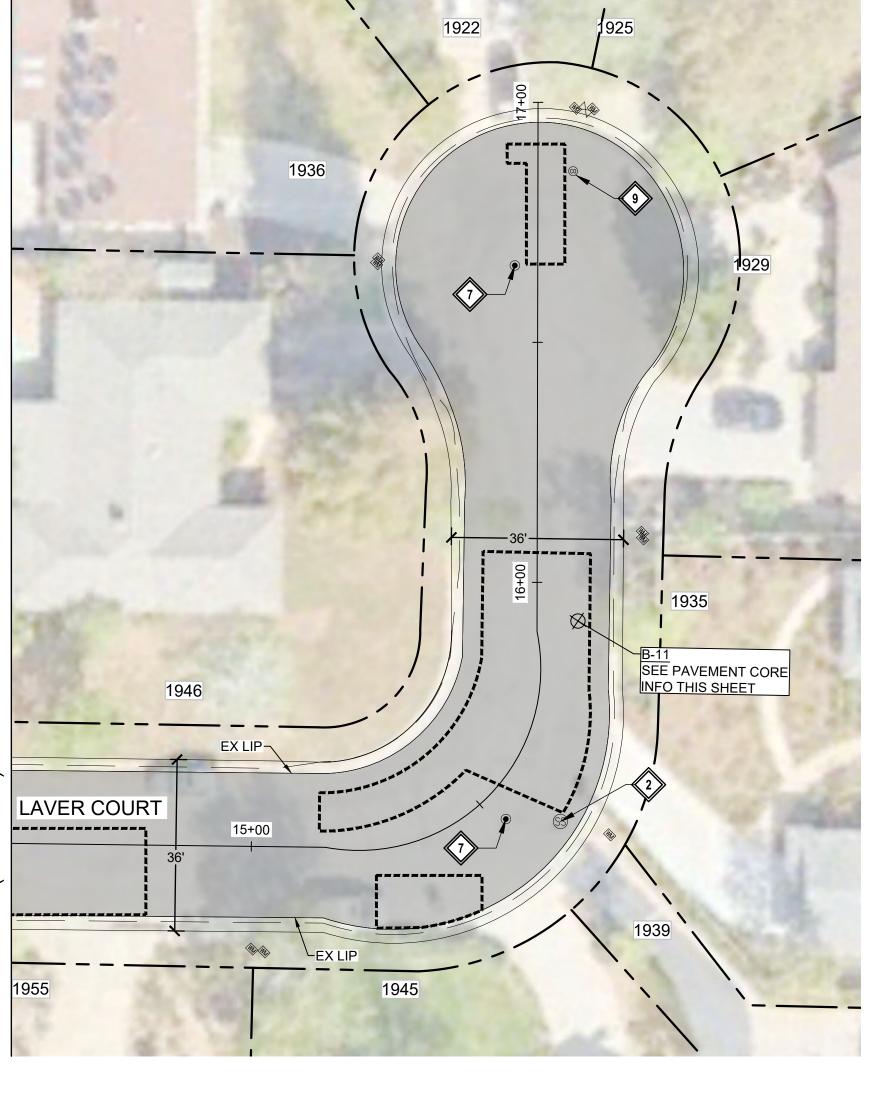


AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

PAVEMENT REPAIR QUANTITY NOTES

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.



PAVEMENT REPAIR SITE - LAVER COURT

CONSTRUCTION NOTES:

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE WATER VALVE

LOWER & RESTORE TO GRADE GAS VALVE

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED • SEE DETAIL : 4
C5.1

LOWER & RESTORE TO GRADE SEWER CLEANOUT

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST SEE DETAIL : X
CX.X

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

S24 DETAIL 27B

YELLOW LADDER CROSSWALK

BLUE HYDRANT MARKER

LEGEND:

 SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS. 1" AC COLDPLANE, 2" AC COLDPLANE WEDGE 1" LEVELING COURSE + PAVING MAT + 2" OVERLAY. SEE TYPICAL SECTIONS

GENERAL NOTES:

INLAY/OVERLAY

2. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY

PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

PAVEMENT CORING LOG

CORE#	SUBGRADE	AC (IN)	AB (IN)	FABRIC ²
B-11	CLAY WITH SAND AND GRAVEL (CL/CH)	3.0	4.0	N/A

PAVEMENT CORING NOTES

1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.

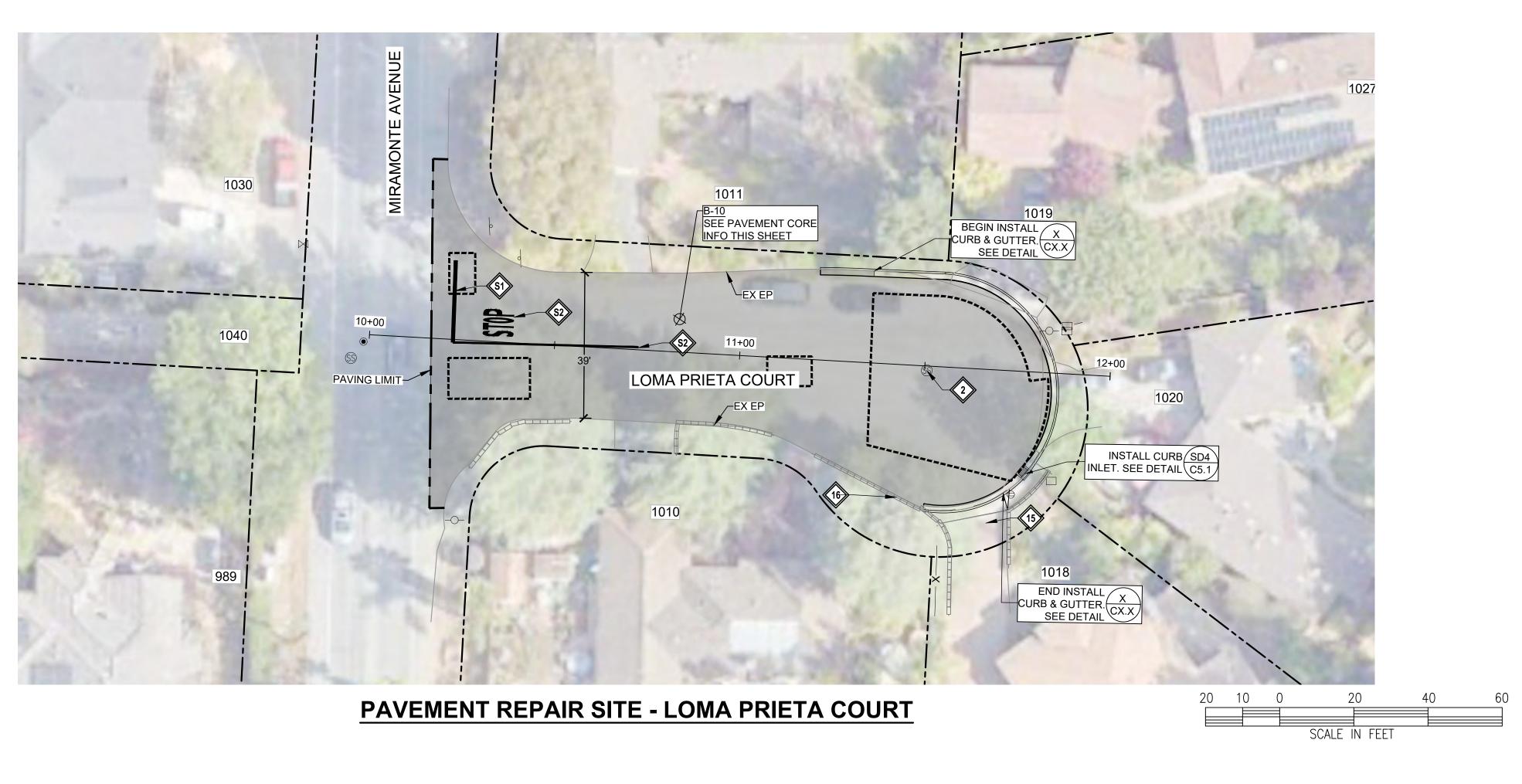
2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

Call Two Working



90% SUBMITTAL MAY 16, 2025

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CONSTRUCTION NOTES:

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

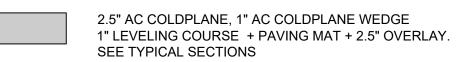
PROTECT IN PLACE EXISTING BRICK PAVERS

PROTECT IN PLACE EXISTING BRICK WALL

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

LEGEND:



4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

— – – — APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING LOG

CORE#	SUBGRADE	AC (IN)	AB (IN)	FABRIC ²
B-10	CLAYEY SAND (SC)	2.5	0.0	N/A

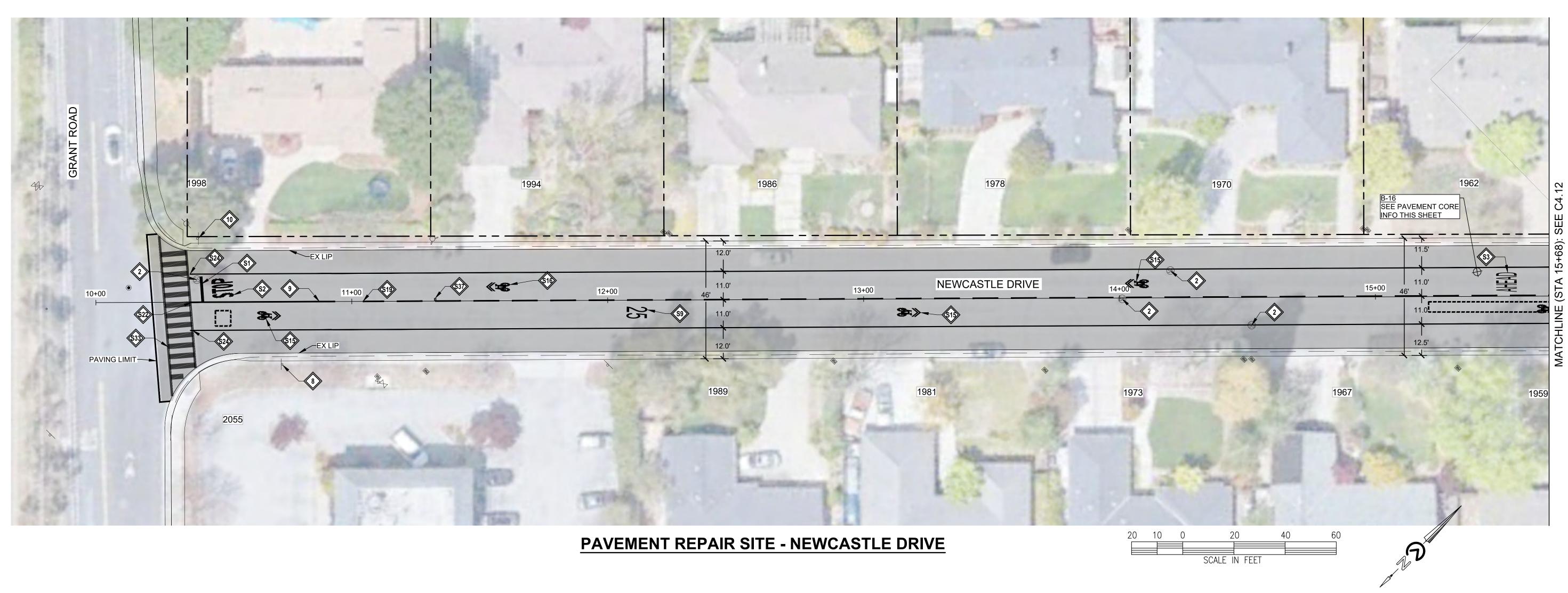
PAVEMENT CORING NOTES

- 1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL
- EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
- 2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

Call Two Working



90% SUBMITTAL MAY 16, 2025



LOWER & RESTORE TO GRADE STORM MANHOLE

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : (C5.1)

INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL : X

LOWER & RESTORE TO GRADE SEWER CLEANOUT

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. SEE DETAIL : X
CX.X

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

\$3 MARKING WORD "AHEAD"

S5 MARKING WORD "XING"

S9 MARKING "25"

MARKING SHARROW

GREEN DASHED BIKE LANE. SEE DETAIL $\begin{pmatrix} B,C\\C4.3 \end{pmatrix}$

DETAIL 2

DETAIL 22

DETAIL 27B

\$33 WHITE LADDER CROSSWALK

BLUE HYDRANT MARKER

MARKING WORD "PED"

LEGEND:

3" AC COLDPLANE 1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY
- MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS. 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF
- THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING LOG

CORE#	SUBGRADE	AC (IN)	AB (IN)	FABRIC ²
B-16	SILTY SAND WITH GRAVEL (SM)	5.0	2.5	N/A

PAVEMENT CORING NOTES

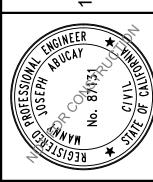
- 1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
- 2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

Call Two Working

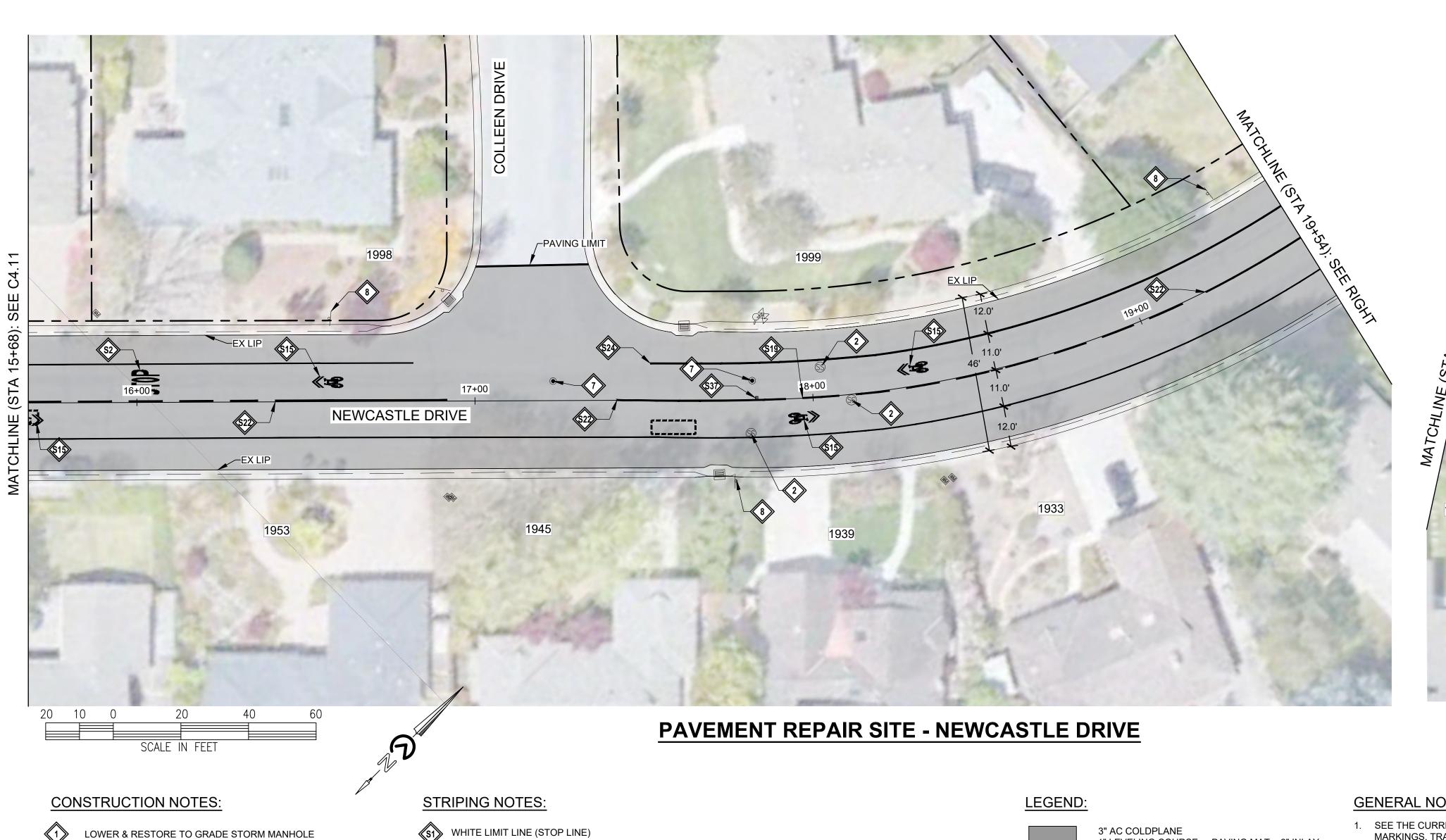


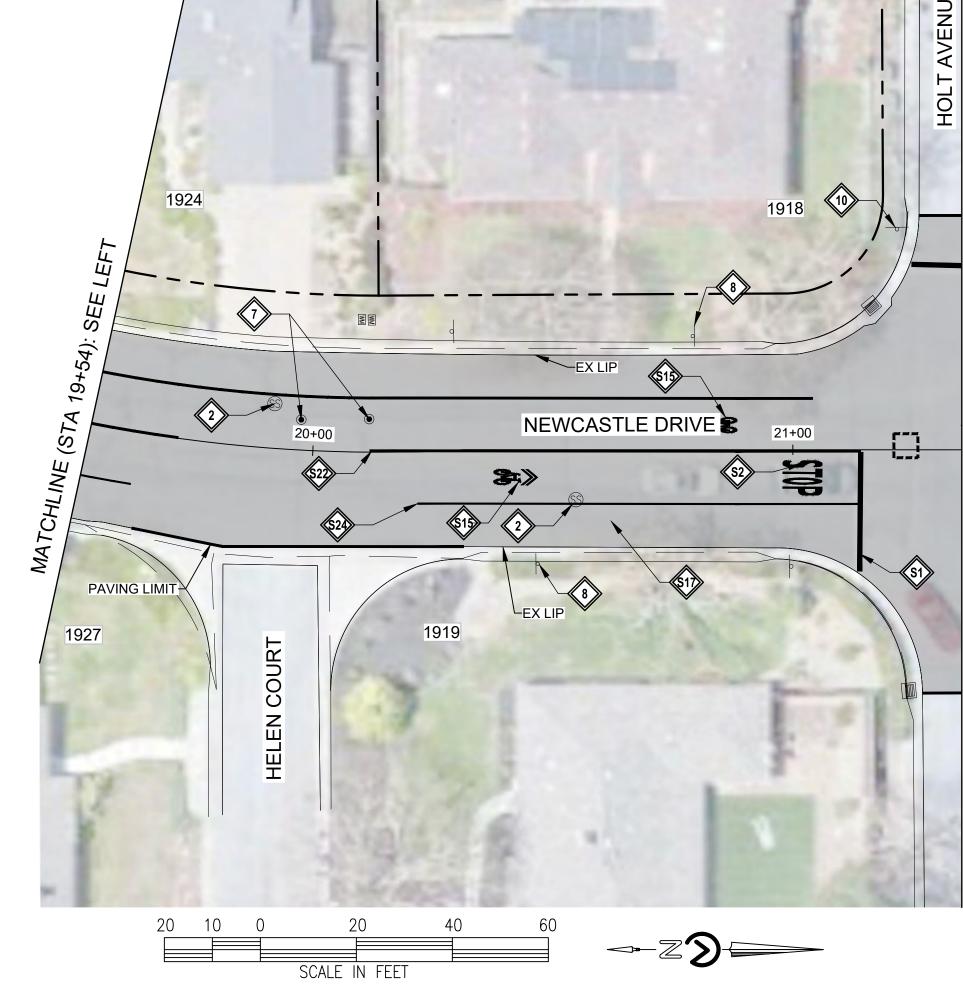
90% SUBMITTAL MAY 16, 2025

2



NEW 15+68





LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : 4

INSTALL W11-1 & W16-1 SIGNS ON NEW POST. • SEE DETAIL : X

LOWER & RESTORE TO GRADE SEWER CLEANOUT

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. SEE DETAIL : X
CX.X

MARKING WORD "STOP"

(\$3) MARKING WORD "AHEAD"

MARKING WORD "XING"

MARKING "25"

MARKING SHARROW

DETAIL 22

DETAIL 27B

S19 DETAIL 2

WHITE LADDER CROSSWALK

BLUE HYDRANT MARKER

MARKING WORD "PED"

1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS



4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

— - - — APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.

2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.

3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.

5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

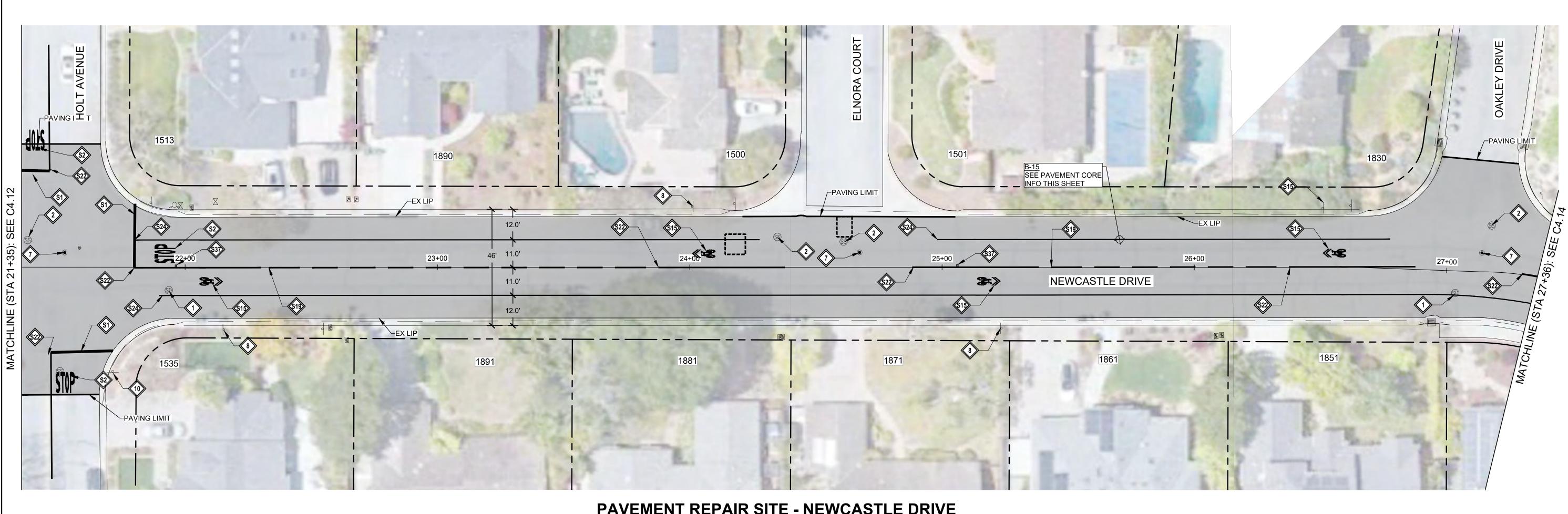
Call Two Working



90% SUBMITTAL MAY 16, 2025

NEWC 21+35

C4.12



PAVEMENT REPAIR SITE - NEWCASTLE DRIVE

CONSTRUCTION NOTES:

LOWER & RESTORE TO GRADE STORM MANHOLE

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : (C5.1)

INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL : X

LOWER & RESTORE TO GRADE SEWER CLEANOUT

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. • SEE DETAIL : X

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

(\$3) MARKING WORD "AHEAD"

MARKING WORD "XING"

S9 MARKING "25"

MARKING SHARROW

S22 DETAIL 22

S19 DETAIL 2

DETAIL 27B

WHITE LADDER CROSSWALK

BLUE HYDRANT MARKER

MARKING WORD "PED"

■ The state of th

LEGEND:

3" AC COLDPLANE 1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT

SCALE IN FEET

- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING LOG

CORE#	SUBGRADE	AC (IN)	AB (IN)	FABRIC ²
B-15	SILTY CLAY (SC/CL)	4.0	4.0	N/A

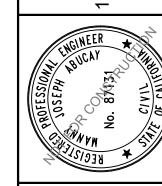
PAVEMENT CORING NOTES

- 1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
- 2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

Call Two Working

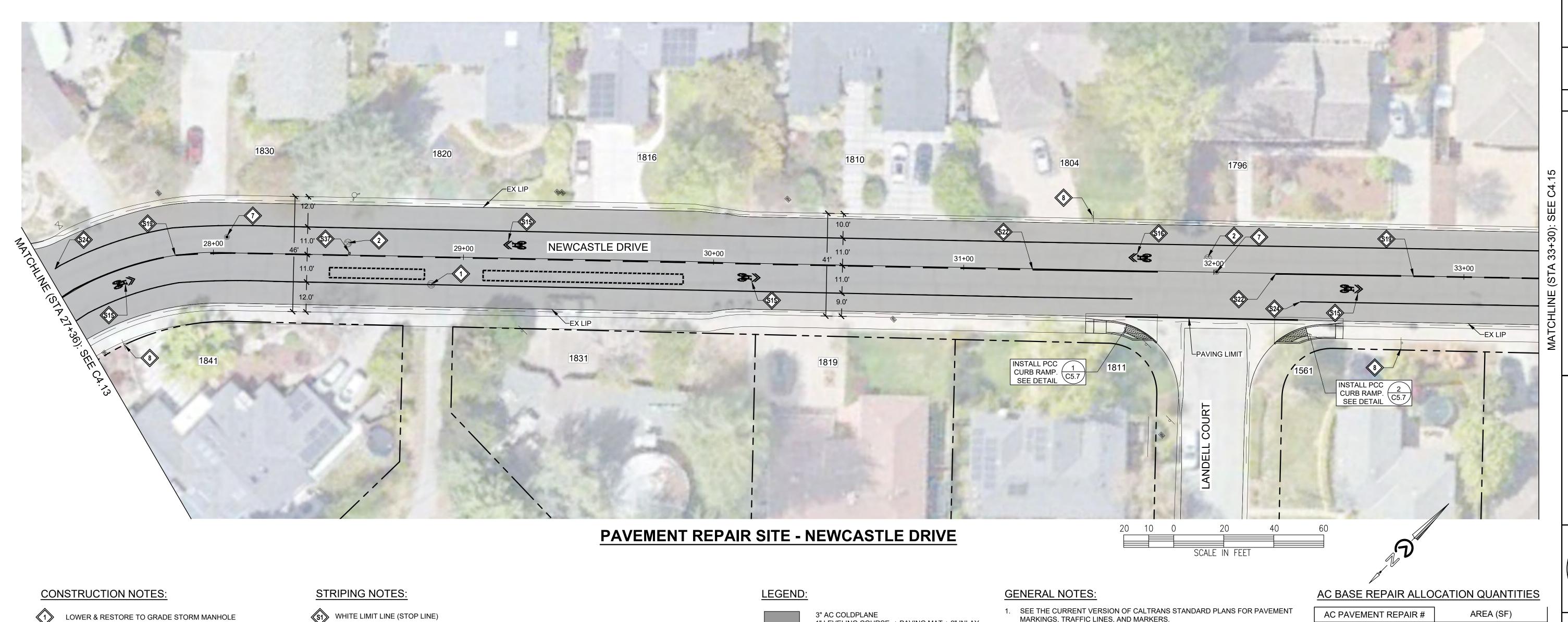


90% SUBMITTAL MAY 16, 2025



NEW 27+36 PLAN - 1+35 to

C4.13



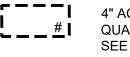
- LOWER & RESTORE TO GRADE STORM MANHOLE
- LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : (C5.1)
- INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL : X
- LOWER & RESTORE TO GRADE SEWER CLEANOUT
- INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. SEE DETAIL : X
 CX.X

- MARKING WORD "STOP"
- \$3 MARKING WORD "AHEAD"
- (\$5) MARKING WORD "XING"
- MARKING "25"
- MARKING SHARROW
- S19 DETAIL 2
- DETAIL 27B

DETAIL 22

- WHITE LADDER CROSSWALK
- BLUE HYDRANT MARKER
- MARKING WORD "PED"





4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

—— — APPROXIMATE RIGHT OF WAY

- MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

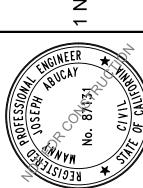
AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

Call Two Working

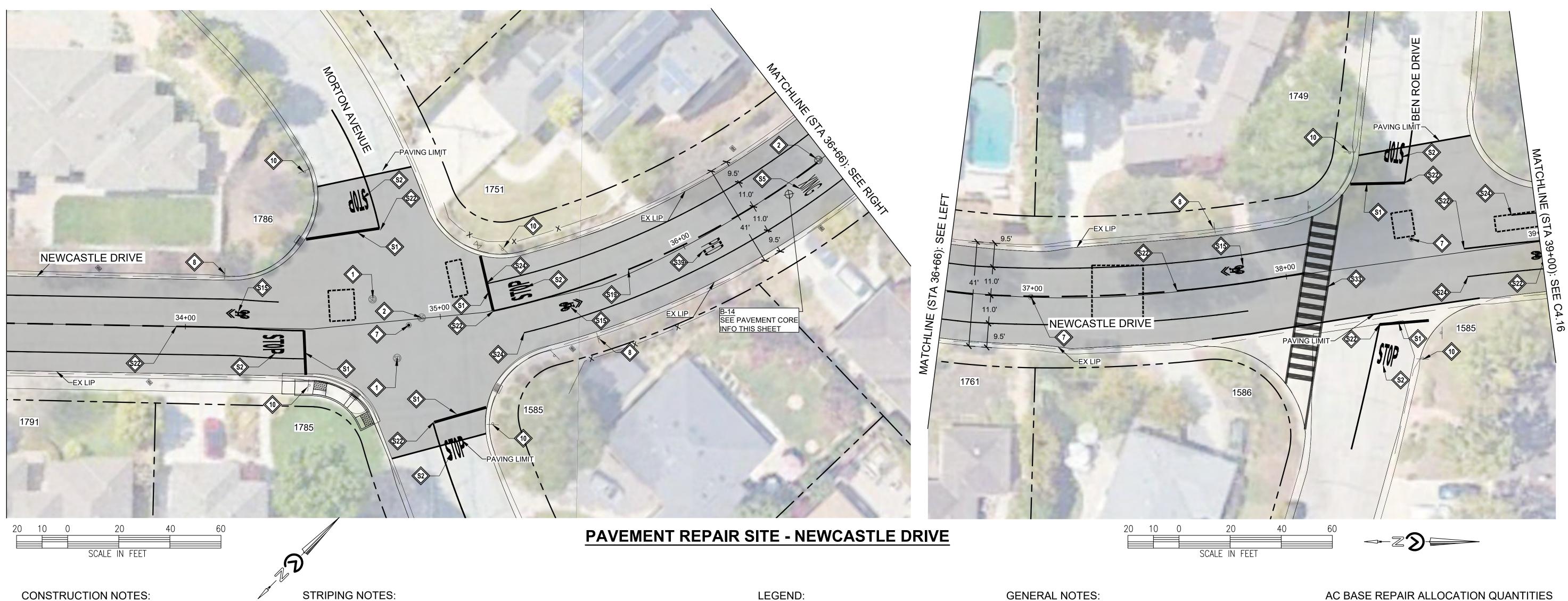


90% SUBMITTAL MAY 16, 2025



NEW(33+30

PLAN - 7+36 to



LOWER & RESTORE TO GRADE STORM MANHOLE

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : (C5.1)

INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL : X

LOWER & RESTORE TO GRADE SEWER CLEANOUT

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. SEE DETAIL : X
CX.X

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

\$3 MARKING WORD "AHEAD"

S5 MARKING WORD "XING"

S9 MARKING "25"

MARKING SHARROW

S22 DETAIL 22

S19 DETAIL 2

DETAIL 27B

WHITE LADDER CROSSWALK

BLUE HYDRANT MARKER

\$39 MARKING WORD "PED"

3" AC COLDPLANE 1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

—— — APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY
- MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING LOG

CORE#	SUBGRADE	AC (IN)	AB (IN)	FABRIC ²
B-14	SAND WITH SILT AND GRAVEL (SW)	6.0	2.0	N/A

PAVEMENT CORING NOTES

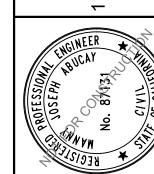
- 1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
- 2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

Call Two Working

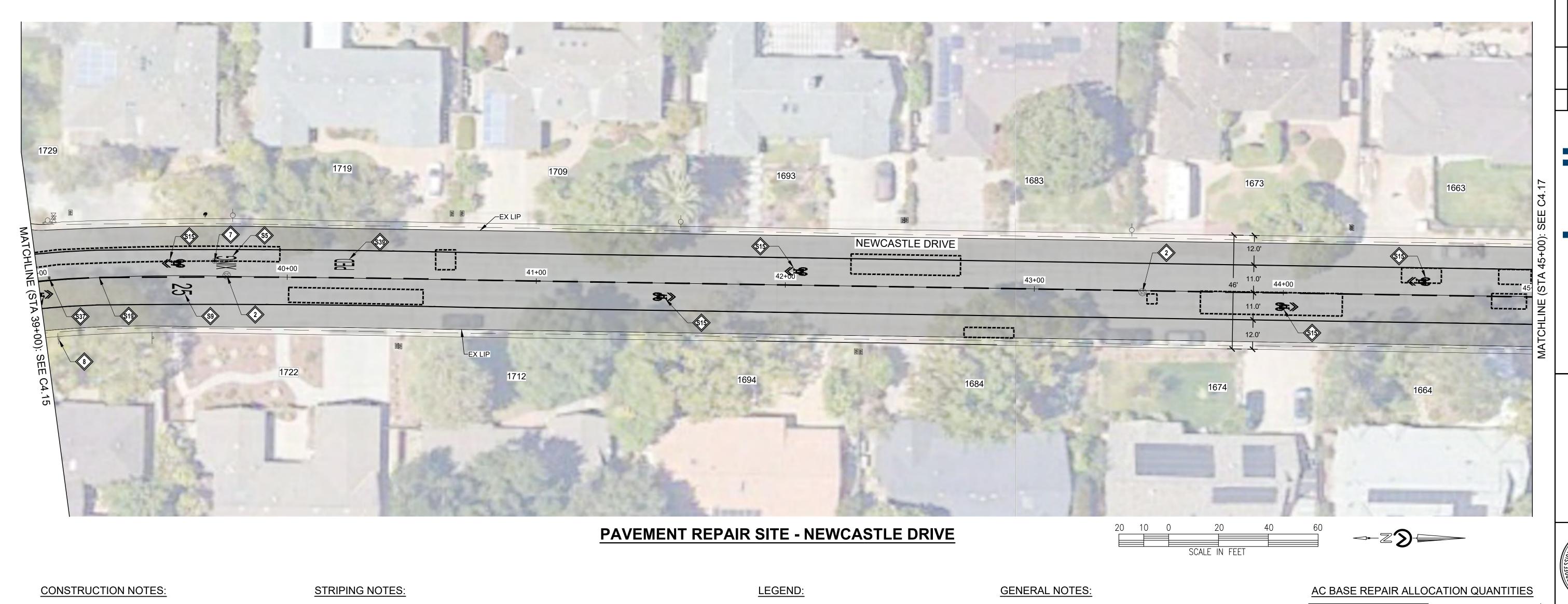


90% SUBMITTAL MAY 16, 2025

C4.15



NEW(39+00



LOWER & RESTORE TO GRADE STORM MANHOLE

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : (C5.1)

INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL : X

LOWER & RESTORE TO GRADE SEWER CLEANOUT

NSTALL R1-1 & RED
• SEE DETAIL : X INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

\$3 MARKING WORD "AHEAD"

S5 MARKING WORD "XING"

S9 MARKING "25"

MARKING SHARROW

DETAIL 22

S19 DETAIL 2

DETAIL 27B

(\$33) WHITE LADDER CROSSWALK

BLUE HYDRANT MARKER

MARKING WORD "PED"

3" AC COLDPLANE 1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

———— APPROXIMATE RIGHT OF WAY

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

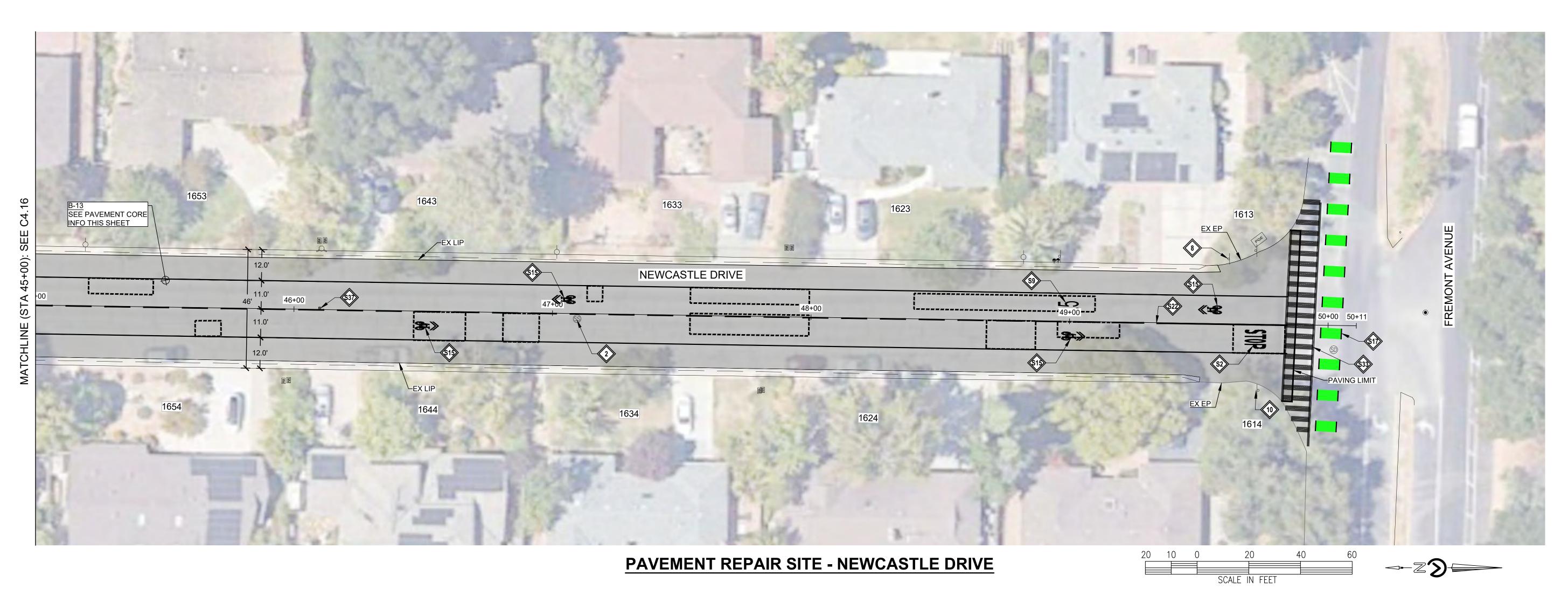
Call Two Working



90% SUBMITTAL MAY 16, 2025

NEW 45+00

C4.16



LOWER & RESTORE TO GRADE STORM MANHOLE

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : (C5.1)

INSTALL W11-1 & W16-1 SIGNS ON NEW POST. • SEE DETAIL : X

LOWER & RESTORE TO GRADE SEWER CLEANOUT

NSTALL R1-1 & KEL ...
• SEE DETAIL : X INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

\$2 MARKING WORD "STOP"

(\$3) MARKING WORD "AHEAD"

S5 MARKING WORD "XING"

(\$9) MARKING "25"

MARKING SHARROW

S19 DETAIL 2

DETAIL 22

DETAIL 27B

WHITE LADDER CROSSWALK

BLUE HYDRANT MARKER

MARKING WORD "PED"

LEGEND:

3" AC COLDPLANE 1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS

SEE DETAIL X

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET.

APPROXIMATE LIMIT OF WORK

— – – APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING LOG

CORE#	SUBGRADE	AC (IN)	AB (IN)	FABRIC ²
B-13	SILTY SAND (SM/MH)	5.0	4.5	N/A

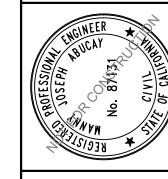
PAVEMENT CORING NOTES

- 1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL
- EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
- 2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

Call Two Working



90% SUBMITTAL MAY 16, 2025



NEW 50+1

PAVEMENT REPAIR SITE - SHELBY LANE

SCALE IN FEET



CONSTRUCTION NOTES:

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE WATER VALVE

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : 4

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. • SEE DETAIL : X

• SEE DETAIL : X REMOVE & REPLACE PCC ROLLED CURB AND GUTTER.

STRIPING NOTES:

321

WHITE LIMIT LINE (STOP LINE)

VALLEY GUTTER.

CX.X

PAVING-SS LIMIT

SEE DETAIL

INSTALL PCC X
CURB RAMP. CX.X
SEE DETAIL

WHITE "STOP" MARKING

BLUE HYDRANT MARKER

LEGEND:

1" LEVELING COURSE + PAVING MAT + 1 1/2" INLAY. SEE TYPICAL SECTIONS

> 4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING LOG

CORE#	SUBGRADE	AC (IN)	AB (IN)	FABRIC ²
B-5	SILTY SAND (SM)	1.0	3.0	N/A

PAVEMENT CORING NOTES

- 1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL
- EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
- 2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

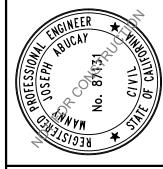
Call Two Working



90% SUBMITTAL MAY 16, 2025

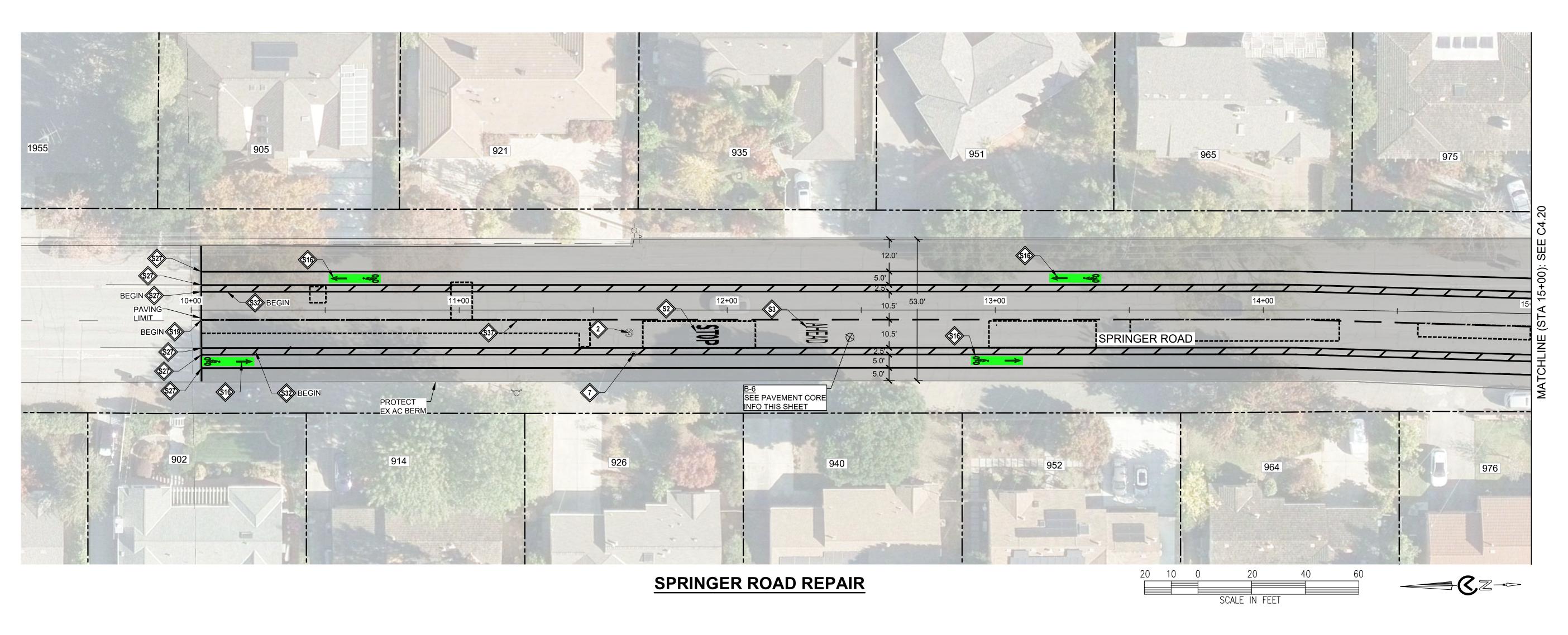
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PLAN to 13+

ENT F 0+00



- LOWER & RESTORE TO GRADE STORM MANHOLE
- LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- LOWER & RESTORE TO GRADE WATER VALVE
- REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. SEE DETAIL :
- INSTALL W11-1 & W16-1 SIGNS ON NEW POST. • SEE DETAIL : X
- INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. • SEE DETAIL : X
- PROTECT IN PLACE EXISTING AC BERM
- INSTALL NEW AC BERM
- INSTALL R1-1 & RED RETROFLECTIVE PANEL ON NEW POST.
- CONSTRUCT NEW CONCRETE ISLAND.

Call Two Working



STRIPING NOTES:

- WHITE LIMIT LINE (STOP LINE)
- (S2) MARKING WORD "STOP"
- **⊗S3** MARKING WORD "AHEAD"
- MARKING WORD "ONLY"
- **≤10** MARKING "30"
- MARKING ARROW TYPE I
- MARKING ARROW TYPE IV (L)
- MARKING ARROW TYPE IV (R)
- MARKING SHARROW
- MARKING BIKE LANE SYMBOL. SEE DETAIL (A)
- GREEN DASHED BIKE LANE. SEE DETAIL $\frac{(B,C)}{(C4.3)}$
- GREEN BACK
- S19 DETAIL 2
- DETAIL 8
- DETAIL 15

STRIPING NOTES:

- DETAIL 22
- DETAIL 25
- DETAIL 27B

S25 DETAIL 37B

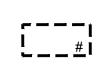
- DETAIL 38
- DETAIL 39
- DETAIL 39A

DETAIL 40

- 6" DIAGONAL YELLOW STRIPE
- 4" DIAGONAL WHITE STRIPE
- WHITE LADDER CROSSWALK
- YELLOW CONTINENTAL CROSSWALK
- **₡3** BLUE HYDRANT MARKER
- 36" HIGH YELLOW FLEXIBLE CHANNELIZER
- \$45 MARKING ARROW TYPE VII

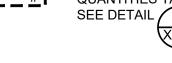
LEGEND:

3" AC COLDPLANE 1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS



CORE#

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X



APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE CITY.

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING NOTES

PAVEMENT CORING LOG

1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL

AC (IN) | AB (IN) | FABRIC 2

4.0

EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.

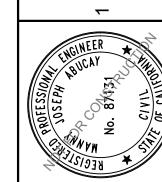
SUBGRADE

CLAYEY SAND / SILTY SAND (SC/SM)

2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

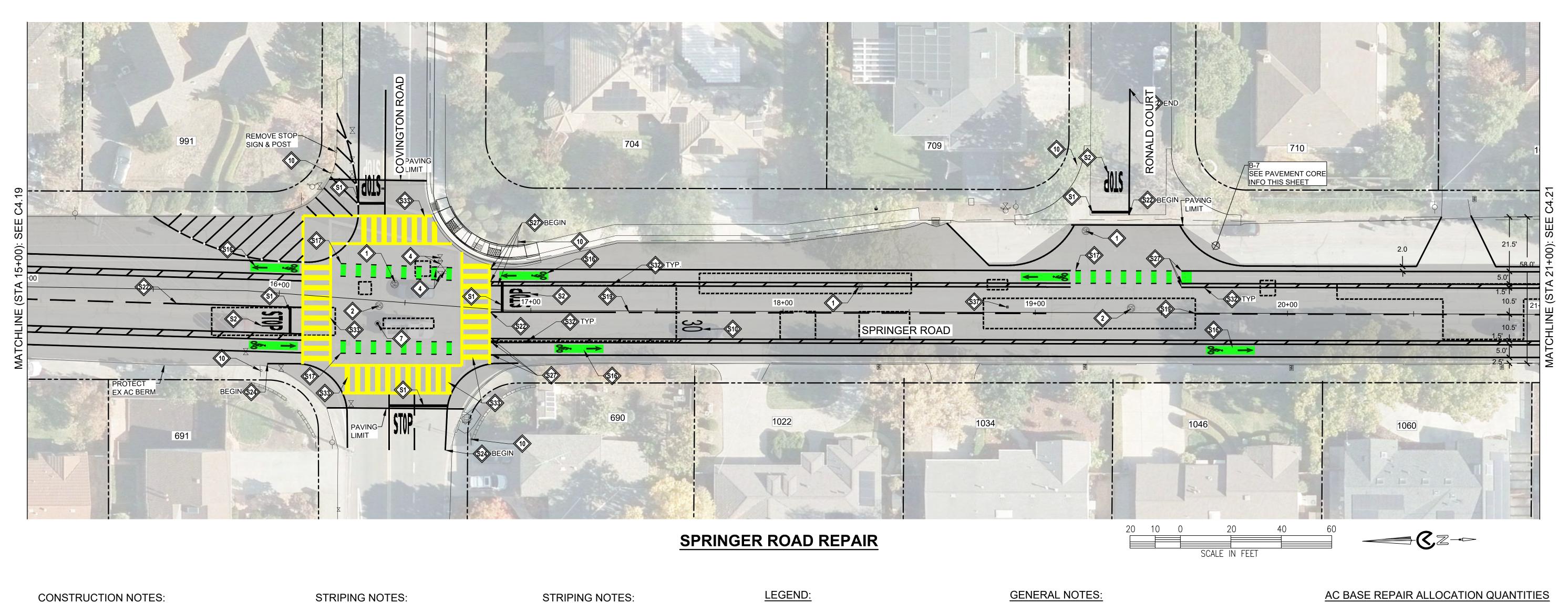
> 90% SUBMITTAL MAY 16, 2025





:MENT 10+00 to Ш —

C4.19



- LOWER & RESTORE TO GRADE STORM MANHOLE
- LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- LOWER & RESTORE TO GRADE WATER VALVE
- REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. SEE DETAIL :
- INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL : X
- INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. • SEE DETAIL : X
- PROTECT IN PLACE EXISTING AC BERM
- INSTALL NEW AC BERM
- INSTALL R1-1 & RED RETROFLECTIVE PANEL ON NEW POST.
- CONSTRUCT NEW CONCRETE ISLAND.

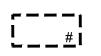
Call Two Working



- WHITE LIMIT LINE (STOP LINE)
- \$2 MARKING WORD "STOP"
- (\$3) MARKING WORD "AHEAD"
- MARKING WORD "ONLY"
- **510** MARKING "30"
- MARKING ARROW TYPE I
- MARKING ARROW TYPE IV (L)
- MARKING ARROW TYPE IV (R)
- MARKING SHARROW
- MARKING BIKE LANE SYMBOL. SEE DETAIL (A)
- GREEN DASHED BIKE LANE. SEE DETAIL $\frac{(B,C)}{(C4.3)}$
- GREEN BACK
- S19 DETAIL 2
- S20 DETAIL 8
- DETAIL 15

- DETAIL 22
- S23 DETAIL 25
- DETAIL 27B
- DETAIL 37B S26 DETAIL 38
- DETAIL 39
- S28 DETAIL 39A
- \$29 DETAIL 40
- (\$3) 6" DIAGONAL YELLOW STRIPE
- 4" DIAGONAL WHITE STRIPE
- WHITE LADDER CROSSWALK
- YELLOW CONTINENTAL CROSSWALK
- \$3 BLUE HYDRANT MARKER
- (\$4) 36" HIGH YELLOW FLEXIBLE CHANNELIZER
- \$45 MARKING ARROW TYPE VII

3" AC COLDPLANE 1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS



CORE#

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X



— – – APPROXIMATE RIGHT OF WAY

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING NOTES

PAVEMENT CORING LOG

1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL

AC (IN) | AB (IN) | FABRIC ²

5.0

EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.

SUBGRADE

SAND WITH GRAVEL / GRAVELLY SAND

(SW/GC)

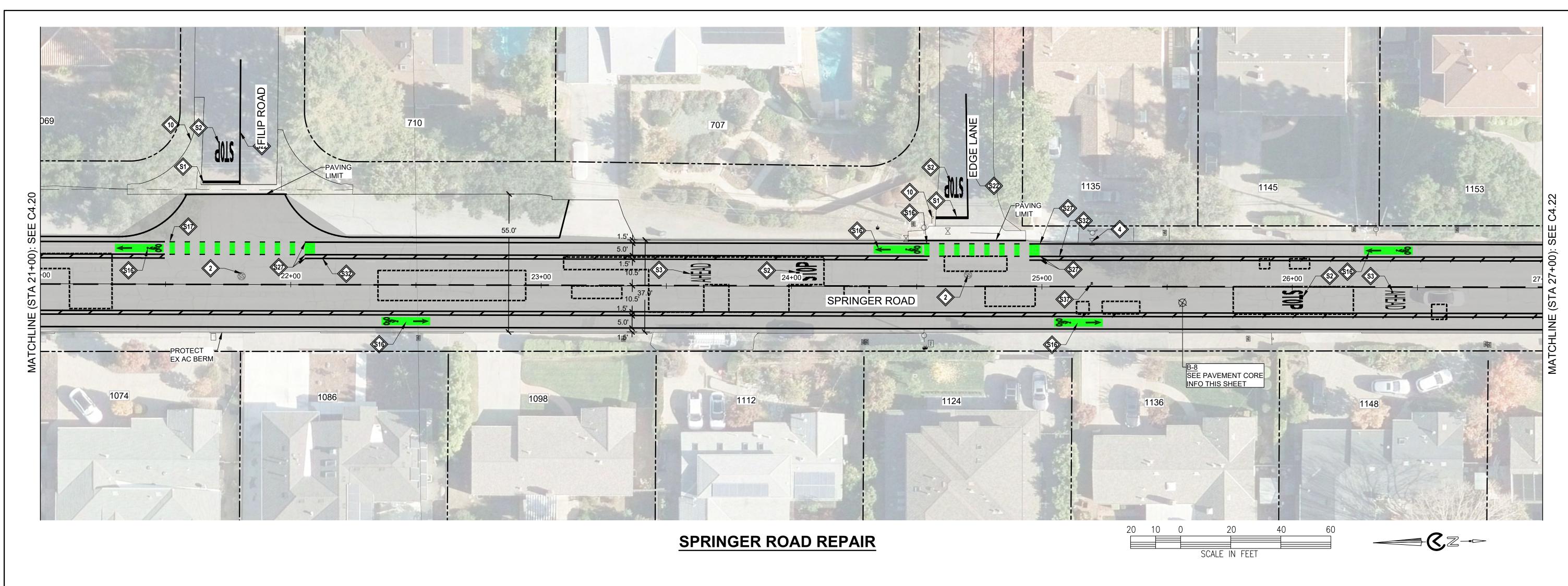
2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

90% SUBMITTAL MAY 16, 2025

C4.20

LAN -

MENT 5+00 to



- LOWER & RESTORE TO GRADE STORM MANHOLE
- LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- LOWER & RESTORE TO GRADE WATER VALVE
- REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : (C5.1
- INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL : X
- INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. • SEE DETAIL : X
- PROTECT IN PLACE EXISTING AC BERM
- INSTALL NEW AC BERM
- INSTALL R1-1 & RED RETROFLECTIVE PANEL ON NEW POST.
- CONSTRUCT NEW CONCRETE ISLAND.

Call Two Working



STRIPING NOTES:

- (\$1) WHITE LIMIT LINE (STOP LINE)
- \$2 MARKING WORD "STOP"
- \$3 MARKING WORD "AHEAD"
- MARKING WORD "ONLY"
- **\$10** MARKING "30"
- MARKING ARROW TYPE I
- MARKING ARROW TYPE IV (L)
- MARKING ARROW TYPE IV (R)
- MARKING SHARROW
- MARKING BIKE LANE SYMBOL. SEE DETAIL (A)
- GREEN DASHED BIKE LANE. SEE DETAIL $\frac{B,C}{C4.3}$
- GREEN BACK
- S19 DETAIL 2
- S20 DETAIL 8
- DETAIL 15

STRIPING NOTES:

- DETAIL 22
- S23 DETAIL 25
- DETAIL 27B
- DETAIL 37B

DETAIL 38

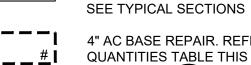
- DETAIL 39
- DETAIL 39A DETAIL 40
- 6" DIAGONAL YELLOW STRIPE
- 4" DIAGONAL WHITE STRIPE
- WHITE LADDER CROSSWALK
- YELLOW CONTINENTAL CROSSWALK

\$3 BLUE HYDRANT MARKER

- 36" HIGH YELLOW FLEXIBLE CHANNELIZER
- \$45 MARKING ARROW TYPE VII

LEGEND:

3" AC COLDPLANE 1" LEVELING COURSE + PAVING MAT + 2" INLAY.



4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

CORE#

—— – — APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING NOTES

PAVEMENT CORING LOG

1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL

AC (IN) | AB (IN) | FABRIC ²

0.0

EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.

SUBGRADE

SAND WITH GRAVEL (SP/SW)

2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

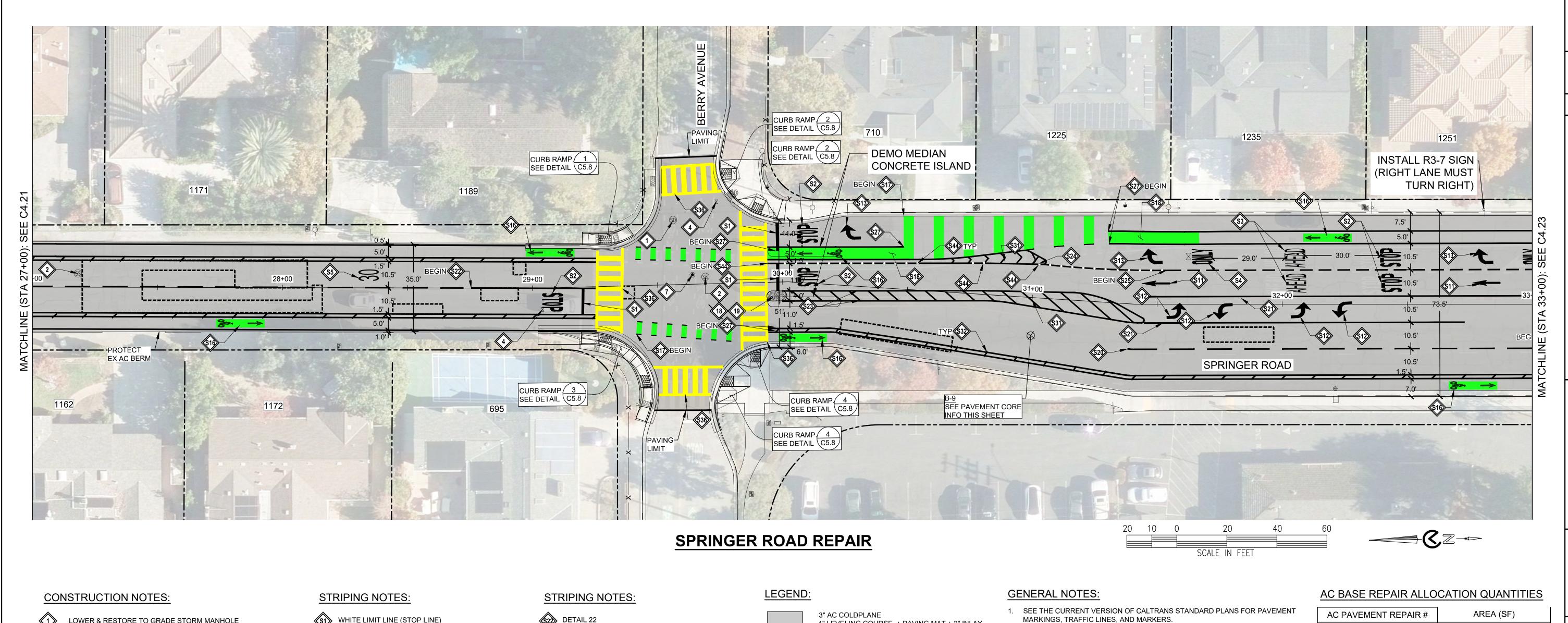
EMENT 21+00 to

90% SUBMITTAL MAY 16, 2025

2 2.







- LOWER & RESTORE TO GRADE STORM MANHOLE
- LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)
- LOWER & RESTORE TO GRADE WATER VALVE
- REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. SEE DETAIL :
- INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL : X
- INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. • SEE DETAIL : X
- PROTECT IN PLACE EXISTING AC BERM
- INSTALL NEW AC BERM
- INSTALL R1-1 & RED RETROFLECTIVE PANEL ON NEW POST.
- CONSTRUCT NEW CONCRETE ISLAND.

Call Two Working



- WHITE LIMIT LINE (STOP LINE)
- (S2) MARKING WORD "STOP"
- \$3 MARKING WORD "AHEAD"
- MARKING WORD "ONLY"
- **510** MARKING "30"
- MARKING ARROW TYPE I
- MARKING ARROW TYPE IV (R)

MARKING ARROW TYPE IV (L)

- MARKING SHARROW
- MARKING BIKE LANE SYMBOL. SEE DETAIL (A)
- GREEN DASHED BIKE LANE. SEE DETAIL $\frac{B,C}{C4.3}$
- GREEN BACK
- S19 DETAIL 2
- S20 DETAIL 8
- DETAIL 15

- DETAIL 22
- S23 DETAIL 25
- DETAIL 27B
- S25 DETAIL 37B
- DETAIL 38
- DETAIL 39 DETAIL 39A
- \$29 DETAIL 40
- 6" DIAGONAL YELLOW STRIPE
- 4" DIAGONAL WHITE STRIPE
- WHITE LADDER CROSSWALK
- YELLOW CONTINENTAL CROSSWALK
- \$3 BLUE HYDRANT MARKER
- (\$4) 36" HIGH YELLOW FLEXIBLE CHANNELIZER
- \$45 MARKING ARROW TYPE VII

1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS



CORE#

B-9

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET.



APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING NOTES

PAVEMENT CORING LOG

1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL

AC (IN) | AB (IN) | FABRIC

0.0

N/A

6.0

EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.

SUBGRADE

SAND WITH GRAVEL (SP/SW)

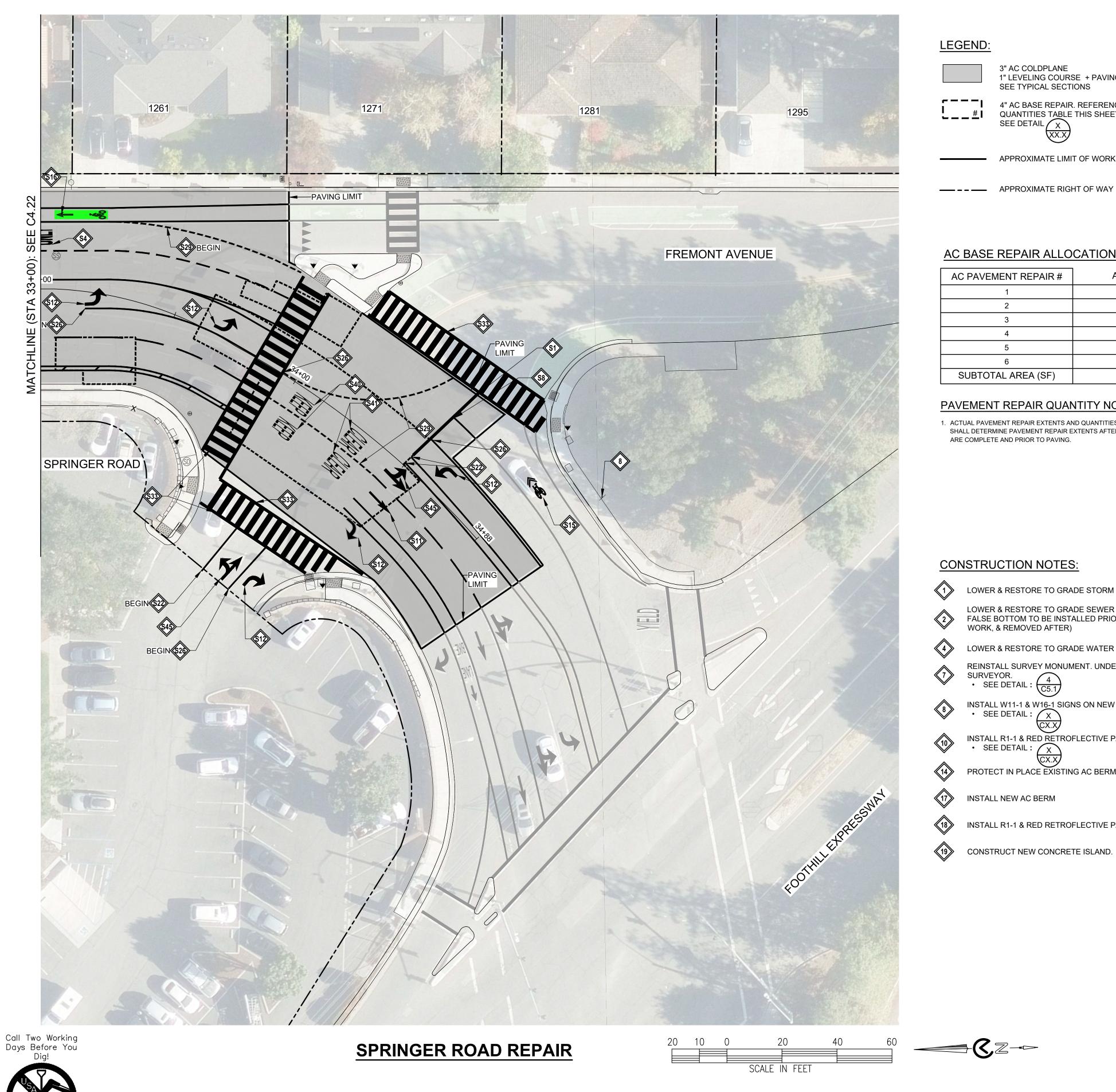
2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

90% SUBMITTAL MAY 16, 2025

C4.22

PRINGER

EMENT 27+00 to



LEGEND:

3" AC COLDPLANE 1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS

APPROXIMATE LIMIT OF WORK

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

PAVEMENT REPAIR QUANTITY NOTES

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

WHITE LIMIT LINE (STOP LINE)

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE

LOWER & RESTORE TO GRADE WATER VALVE

WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE STORM MANHOLE

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR.

• SEE DETAIL:

(C5.1)

INSTALL W11-1 & W16-1 SIGNS ON NEW POST.

• SEE DETAIL: X
CX.X

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST.

• SEE DETAIL: X
CX.X

PROTECT IN PLACE EXISTING AC BERM

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON NEW POST.

STRIPING NOTES:

≪\$2 MARKING WORD "STOP"

(\$3) MARKING WORD "AHEAD"

MARKING WORD "ONLY"

MARKING "30"

MARKING ARROW TYPE I

MARKING ARROW TYPE IV (L)

MARKING ARROW TYPE IV (R)

MARKING SHARROW

STRIPING NOTES:

DETAIL 22

DETAIL 25

DETAIL 27B

DETAIL 37B

DETAIL 38

DETAIL 39

DETAIL 39A

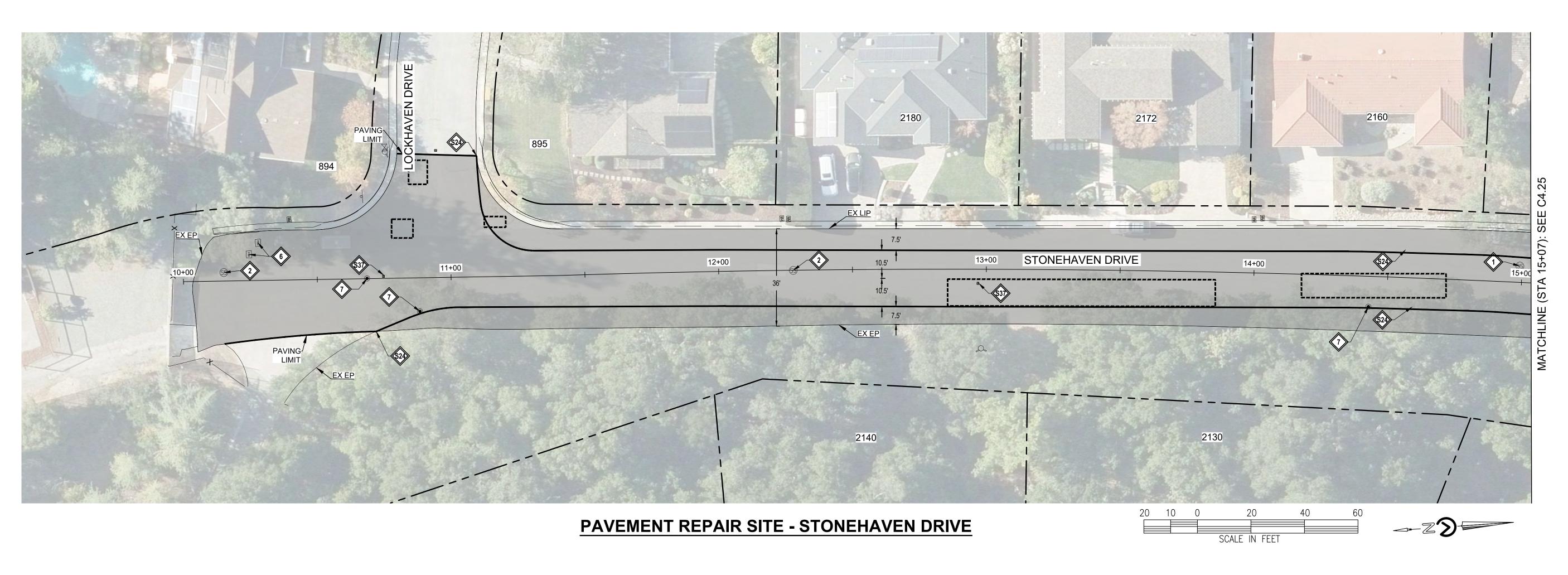
DETAIL 40

6" DIAGONAL YELLOW STRIPE

90% SUBMITTAL MAY 16, 2025

C4.23

SPRINGER r PLAN - 8 to 64+88 MARKING BIKE LANE SYMBOL. SEE DETAIL $\begin{pmatrix} A \\ C4.3 \end{pmatrix}$ CONSTRUCT NEW CONCRETE ISLAND. 4" DIAGONAL WHITE STRIPE GREEN DASHED BIKE LANE. SEE DETAIL B,C C4.3 WHITE LADDER CROSSWALK IMPROVEMENT F 33+00 to YELLOW CONTINENTAL CROSSWALK GREEN BACK DETAIL 2 BLUE HYDRANT MARKER DETAIL 8 36" HIGH YELLOW FLEXIBLE CHANNELIZER DETAIL 15 MARKING ARROW TYPE VII STREET



LOWER & RESTORE TO GRADE STORM MANHOLE

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE WATER VALVE

LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : 4

INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL : X

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. • SEE DETAIL : X

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

S2 MARKING WORD "STOP"

(\$3) MARKING WORD "AHEAD"

S5 MARKING WORD "XING"

\$6 MARKING WORD "YIELD"

S7 MARKING WORD "SLOW"

(\$8) MARKING WORD "SCHOOL"

\$14 SPEED BUMP MARKING

\$15 SHARROW MARKING

GREEN DASHED BIKE LANE. SEE DETAIL $\frac{B,C}{C4.3}$

DETAIL 22

DETAIL 27B

YELLOW LADDER CROSSWALK

BLUE HYDRANT MARKER

LEGEND:

1" LEVELING COURSE + PAVING MAT +1 1/2 " INLAY. SEE TYPICAL SECTIONS



4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT

2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.

3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.

5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS

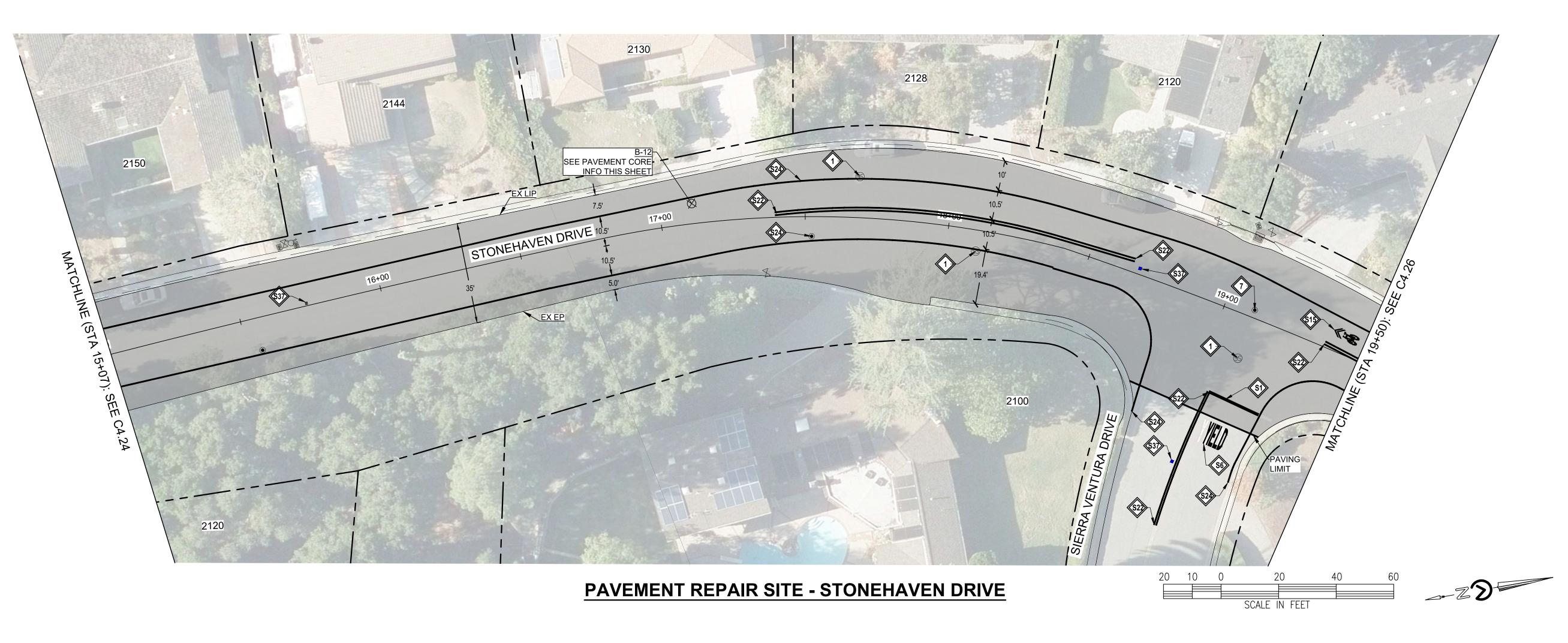
ARE COMPLETE AND PRIOR TO PAVING.

Call Two Working



90% SUBMITTAL MAY 16, 2025

24



LOWER & RESTORE TO GRADE STORM MANHOLE

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE WATER VALVE

LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : (C5.1)

INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL : X

STALL R1-1 & KEL .

SEE DETAIL: X
CX.X INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST.

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

S3 MARKING WORD "AHEAD"

S5 MARKING WORD "XING"

≪S6 MARKING WORD "YIELD"

MARKING WORD "SLOW"

\$8 MARKING WORD "SCHOOL"

\$14 SPEED BUMP MARKING

\$15 SHARROW MARKING

GREEN DASHED BIKE LANE. SEE DETAIL $\frac{B,C}{C4.3}$

DETAIL 22

DETAIL 27B

YELLOW LADDER CROSSWALK

\$3 BLUE HYDRANT MARKER

LEGEND:

2.5" AC COLDPLANE 1" LEVELING COURSE + PAVING MAT +1 1/2" INLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE CITY.

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING LOG

CORE#	SUBGRADE	AC (IN)	AB (IN)	FABRIC ²
B-12	SILTY SAND WITH GRAVEL (SM)	5.5	4.5	N/A

PAVEMENT CORING NOTES

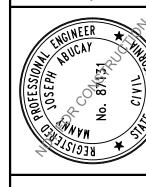
- 1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
- 2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT THAT CONTAINS PAVEMENT FABRIC.

Call Two Working

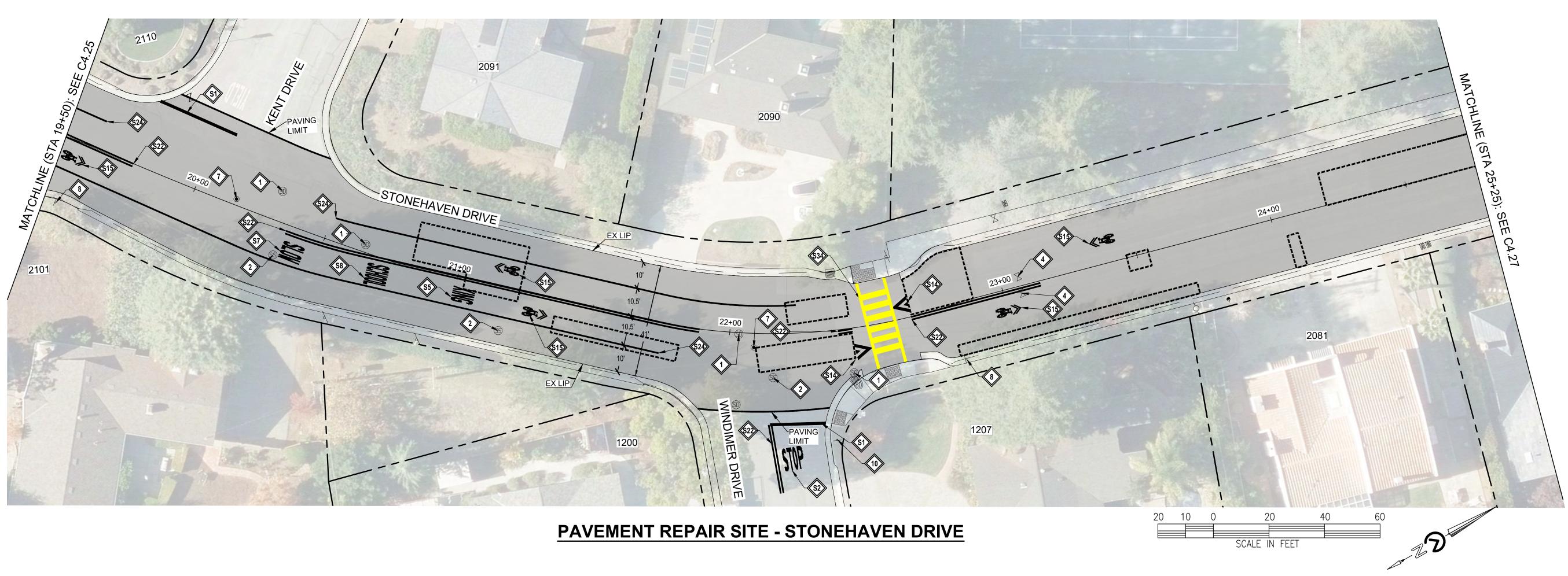


90% SUBMITTAL MAY 16, 2025

2



Z U - STONEH O 19+50



LOWER & RESTORE TO GRADE STORM MANHOLE

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE WATER VALVE

LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : 4 C5.1

INSTALL W11-1 & W16-1 SIGNS ON NEW POST. • SEE DETAIL : X

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. • SEE DETAIL : X

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

\$3 MARKING WORD "AHEAD"

S5 MARKING WORD "XING"

\$6 MARKING WORD "YIELD"

(\$7) MARKING WORD "SLOW"

(\$8) MARKING WORD "SCHOOL"

SPEED BUMP MARKING

\$15 SHARROW MARKING

GREEN DASHED BIKE LANE. SEE DETAIL $\frac{B,C}{C4.3}$

S22 DETAIL 22

DETAIL 27B

YELLOW LADDER CROSSWALK

BLUE HYDRANT MARKER

LEGEND:



1" LEVELING COURSE + PAVING MAT +1 ½" INLAY. SEE TYPICAL SECTIONS



4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X



APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT

2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.

3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY

4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.

5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)			
1	0.0			
2	0.0			
3	0.0			
4	0.0			
5	0.0			
6	0.0			
SUBTOTAL AREA (SF)	0.0			

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS

ARE COMPLETE AND PRIOR TO PAVING.

Call Two Working



90% SUBMITTAL MAY 16, 2025

OF

- STONEH - 25+25

C4.26

LOWER & RESTORE TO GRADE STORM MANHOLE

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE WATER VALVE

LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : 4 C5.1

INSTALL W11-1 & W16-1 SIGNS ON NEW POST. SEE DETAIL : X

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. • SEE DETAIL : X

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

(\$3) MARKING WORD "AHEAD"

S5 MARKING WORD "XING"

\$6 MARKING WORD "YIELD"

87 MARKING WORD "SLOW"

(\$8) MARKING WORD "SCHOOL"

SPEED BUMP MARKING

\$15 SHARROW MARKING

GREEN DASHED BIKE LANE. SEE DETAIL $\frac{B,C}{C4.3}$

DETAIL 22

DETAIL 27B

YELLOW LADDER CROSSWALK

BLUE HYDRANT MARKER

LEGEND:

1" LEVELING COURSE + PAVING MAT + 1 $\frac{1}{2}$ " INLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

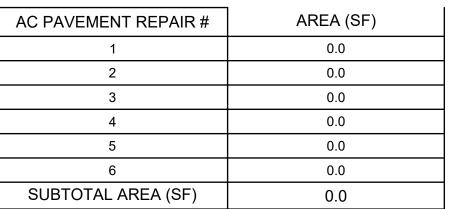
APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES



1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

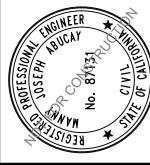
Call Two Working



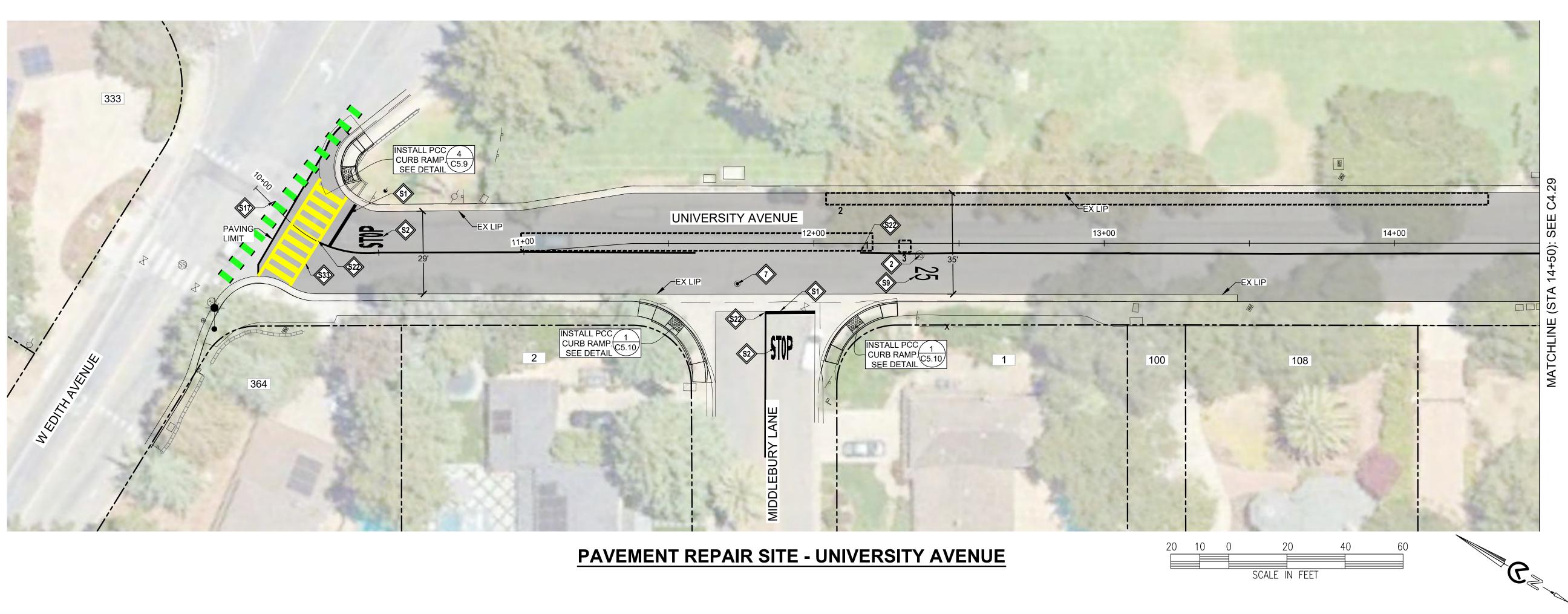
90% SUBMITTAL MAY 16, 2025

.27





- STONEH - 29+79



LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)

LOWER & RESTORE TO GRADE WATER VALVE

LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : 4 C5.1

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. SEE DETAIL : X

COORDINATE WITH CITY ON ARBORIST RECOMMENDATION FOR POSSIBLE TREE ROOT REMOVAL

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

MARKING "25"

GREEN DASHED BIKE LANE. SEE DETAIL (B,C)

DETAIL 22

WHITE LADDER CROSSWALK

BLUE HYDRANT MARKER

REPAINT RED CURB

LEGEND:



4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

—— — APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

AC PAVEMENT REPAIR #	AREA (SF)	
1	0.0	
2	0.0	
3	0.0	
4	0.0	
5	0.0	
6	0.0	
SUBTOTAL AREA (SF)	0.0	

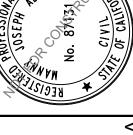
1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

Call Two Working Days Before You



90% SUBMITTAL MAY 16, 2025

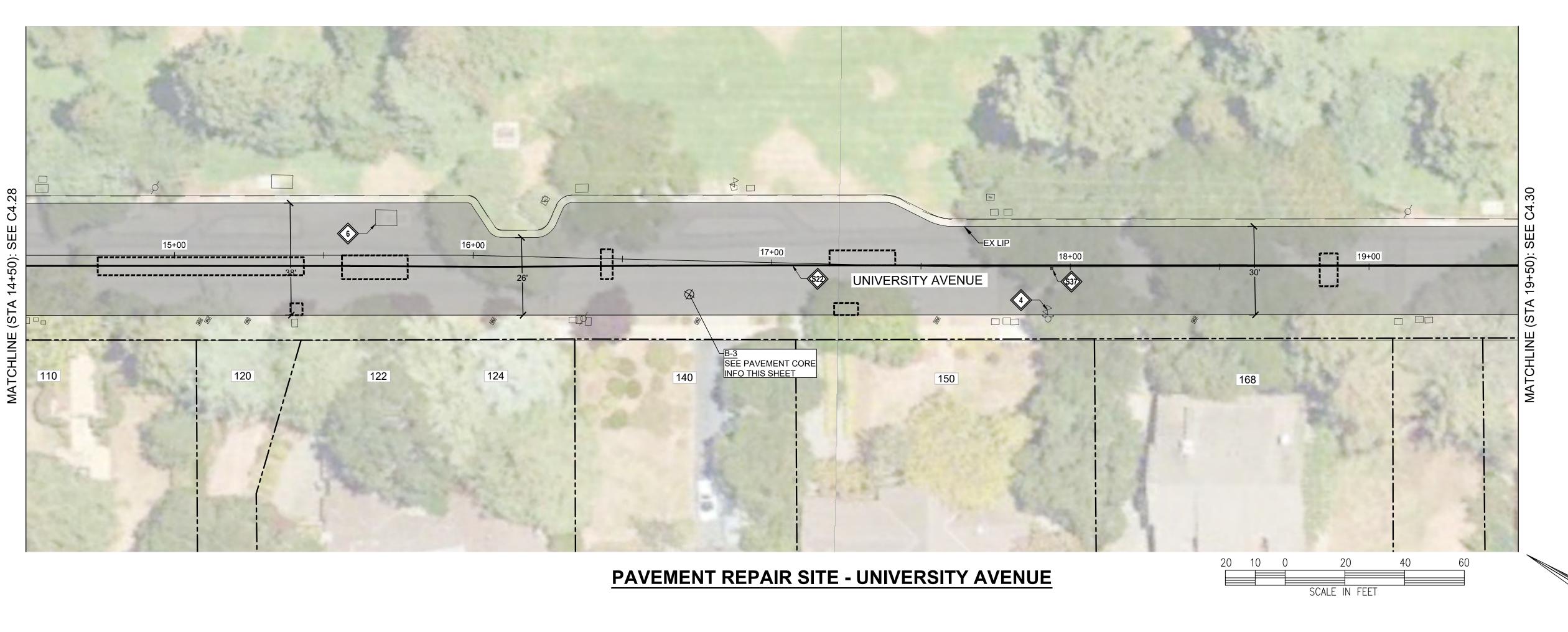




UNIVEF 14+50

PLAN 10+00

28



LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)

LOWER & RESTORE TO GRADE WATER VALVE

LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : 4
C5.1

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. SEE DETAIL : X

COORDINATE WITH CITY ON ARBORIST RECOMMENDATION FOR POSSIBLE TREE ROOT REMOVAL

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING "25"

GREEN DASHED BIKE LANE. SEE DETAIL (B,C)

DETAIL 22

WHITE LADDER CROSSWALK

BLUE HYDRANT MARKER

REPAINT RED CURB

LEGEND:

1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
- 4. CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES AND SURVEY MONUMENTS WITH THE WORK AREAS. THE CONTRACTOR SHALL REVIEW THE WORK SITES PRIOR TO SUBMISSION OF BIDS.
- 5. EXISTING CITY MONUMENTS SHALL NOT BE DISTURBED. PER SECTION 8771 OF THE CALIFORNIA BUSINESS AND PROFESSIONAL CODE, ANY MONUMENTS THAT ARE ACCIDENTALLY DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF CALIFORNIA, AND PAVEMENT REPAIR QUANTITY NOTES A RECORD OF SURVEY OR CORNER RECORD SHALL BE PREPARED AND FILED. THE REPLACEMENT OR RELOCATION OF THESE SURVEY MONUMENTS MUST BE COORDINATED WITH THE COUNTY SURVEY DEPARTMENT. THE REPLACEMENT OR RELOCATION OF THESE MONUMENTS SHALL BE AT NO ADDITIONAL COST TO THE

AC BASE REPAIR ALLOCATION QUANTITIES

	•		
AC PAVEMENT REPAIR #	AREA (SF)		
1	0.0		
2	0.0		
3	0.0		
4	0.0		
5	0.0		
6	0.0		
SUBTOTAL AREA (SF)	0.0		

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

PAVEMENT CORING LOG

THAT CONTAINS PAVEMENT FABRIC.

CORE#	SUBGRADE	AC (IN)	AB (IN)	FABRIC ³
B-3	CLAYEY SAND (SC)	4.0	2.0	N/A
		-		

PAVEMENT CORING NOTES

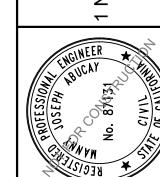
- 1. PAVEMENT CORING WAS PERFORMED IN SELECT LOCATIONS AND IS NOT REPRESENTATIVE OF ALL EXISTING PAVEMENT CONDITIONS WITHIN THE WORK LIMITS.
- 2. PAVEMENT TO BE COLD PLANED OR REMOVED MAY CONTAIN PAVEMENT FABRIC. PAVEMENT BORINGS ARE NOT REPRESENTATIVE OF ENTIRE STREET WORK LIMITS. ANY ASSUMPTION OF THE PRESENCE OF PAVEMENT FABRIC SHALL BE AT THE CONTRACTOR'S SOLE RISK. THERE SHALL BE NO ADJUSTMENT IN COMPENSATION TO THE CONTRACTOR FOR COLD PLANING, REMOVING, AND DISPOSING AC PAVEMENT

Call Two Working Days Before You



90% SUBMITTAL MAY 16, 2025

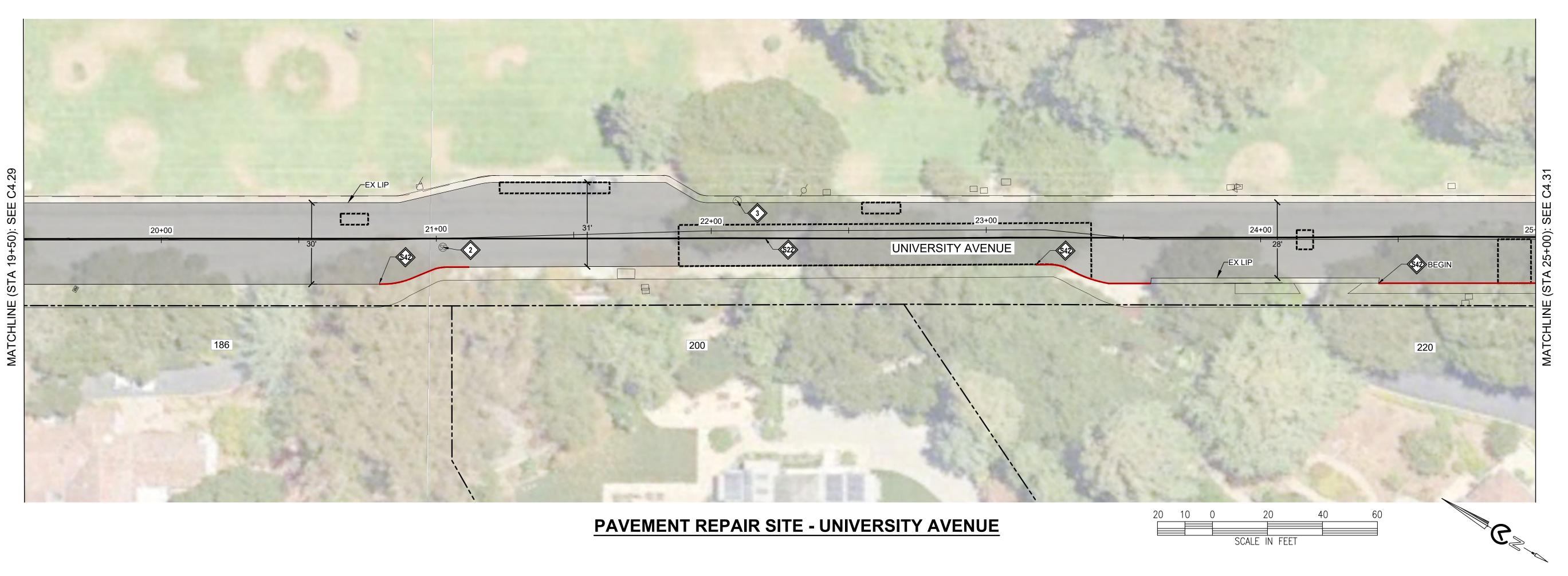




UNIVERSIT 19+50

PLAN 14+50

29



CONSTRUCTION NOTES:

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)

LOWER & RESTORE TO GRADE WATER VALVE

LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : 4 C5.1

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. SEE DETAIL : X

COORDINATE WITH CITY ON ARBORIST RECOMMENDATION FOR POSSIBLE TREE ROOT REMOVAL

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

MARKING "25"

GREEN DASHED BIKE LANE. SEE DETAIL (B,C)

DETAIL 22

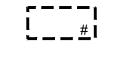
WHITE LADDER CROSSWALK

BLUE HYDRANT MARKER

REPAINT RED CURB

LEGEND:

1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS



4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

— – – APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

- 1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.
- 2. STRIPING AND PAVEMENT MARKINGS SHALL BE HIGH VISIBILITY USING HIGH-PERFORMANCE RETROSPECTIVE GLASS BEADS.
- 3. A TACK COAT EMULSION SHALL BE APPLIED TO CLEAN DRY PAVEMENT BETWEEN (E) AND (N) PAVEMENT SECTIONS PRIOR TO INLAY/OVERLAY
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AC BASE REPAIR ALLOCATION QUANTITIES

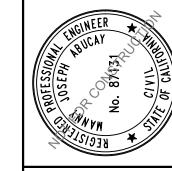
AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0
•	

1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

Call Two Working Days Before You



90% SUBMITTAL MAY 16, 2025

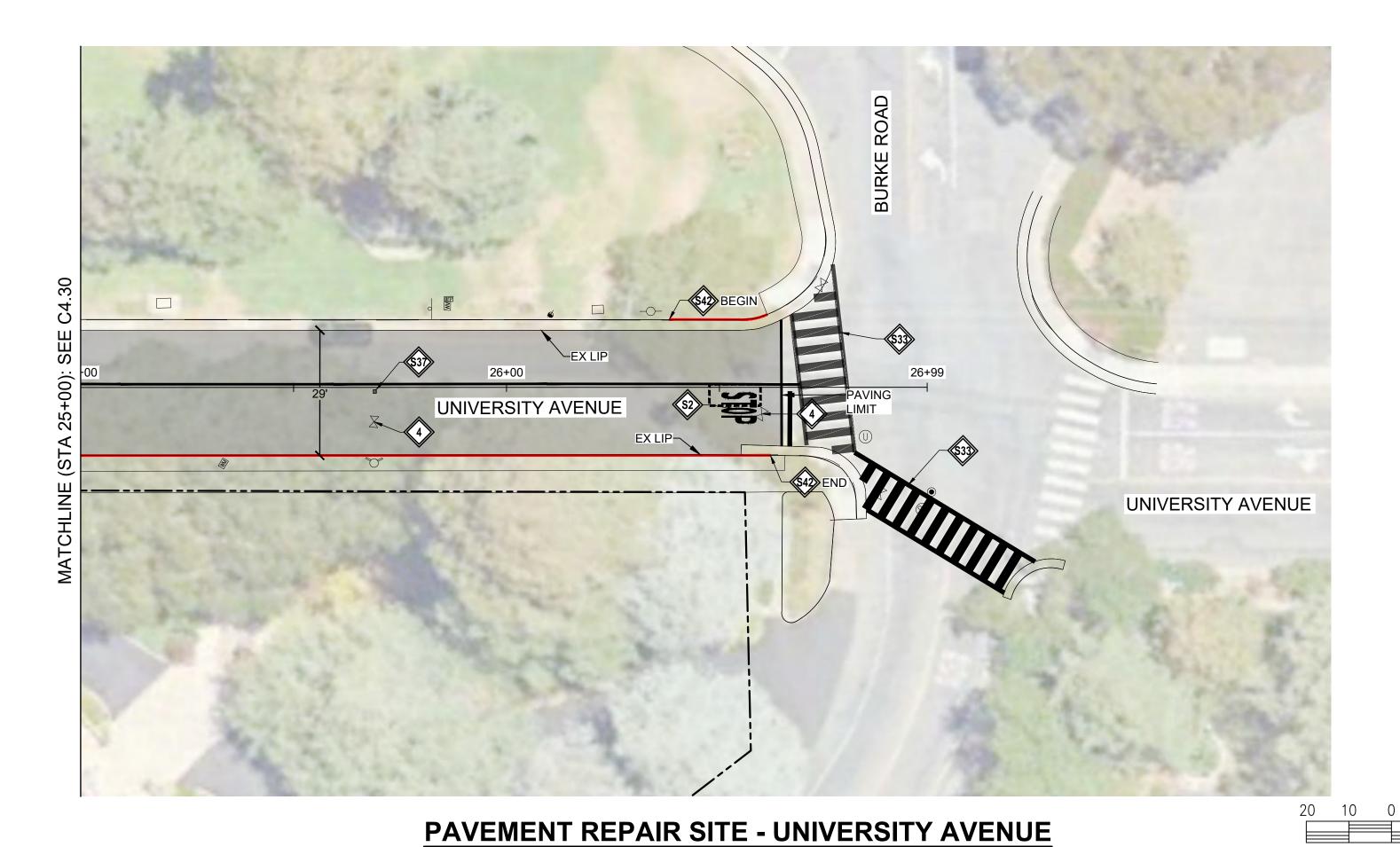


UNIVERSIT 25+00

PLAN 19+50 1

4.30 OF

31



CONSTRUCTION NOTES:

LOWER & RESTORE TO GRADE SEWER MANHOLE (TEMPORARY FALSE BOTTOM TO BE INSTALLED PRIOR TO ANY MANHOLE WORK, & REMOVED AFTER)

LOWER & RESTORE TO GRADE UTILITY MANHOLE (NOTIFY & COORDINATE WITH UTILITY COMPANY PRIOR TO WORK)

LOWER & RESTORE TO GRADE WATER VALVE

LOWER & RESTORE TO GRADE UTILITY BOX

REINSTALL SURVEY MONUMENT. UNDER CALIFORNIA LICENSED SURVEYOR. • SEE DETAIL : 4 C5.1

INSTALL R1-1 & RED RETROFLECTIVE PANEL ON EXISTING POST. • SEE DETAIL : X CX.X

COORDINATE WITH CITY ON ARBORIST RECOMMENDATION FOR POSSIBLE TREE ROOT REMOVAL

STRIPING NOTES:

WHITE LIMIT LINE (STOP LINE)

MARKING WORD "STOP"

MARKING "25"

GREEN DASHED BIKE LANE. SEE DETAIL (B,C)

DETAIL 22

WHITE LADDER CROSSWALK

BLUE HYDRANT MARKER

REPAINT RED CURB

LEGEND:

1" LEVELING COURSE + PAVING MAT + 2" INLAY. SEE TYPICAL SECTIONS

4" AC BASE REPAIR. REFERENCE ALLOCATION QUANTITIES TABLE THIS SHEET. SEE DETAIL X

APPROXIMATE LIMIT OF WORK

APPROXIMATE RIGHT OF WAY

GENERAL NOTES:

1. SEE THE CURRENT VERSION OF CALTRANS STANDARD PLANS FOR PAVEMENT MARKINGS, TRAFFIC LINES, AND MARKERS.

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AC BASE REPAIR ALLOCATION QUANTITIES

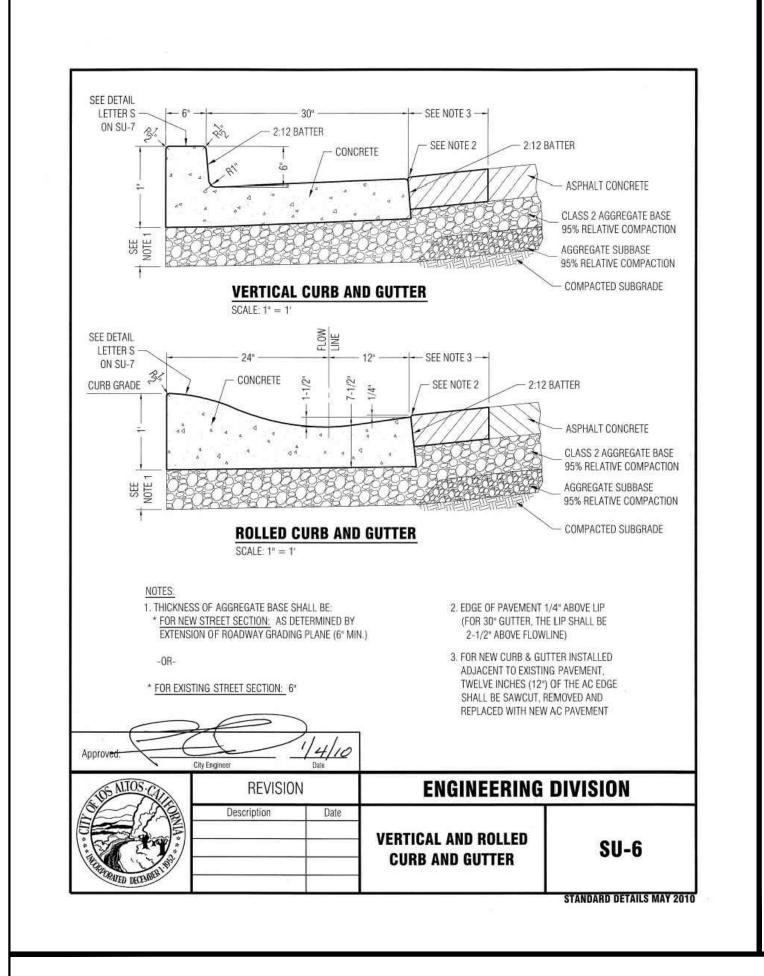
AC PAVEMENT REPAIR #	AREA (SF)
1	0.0
2	0.0
3	0.0
4	0.0
5	0.0
6	0.0
SUBTOTAL AREA (SF)	0.0

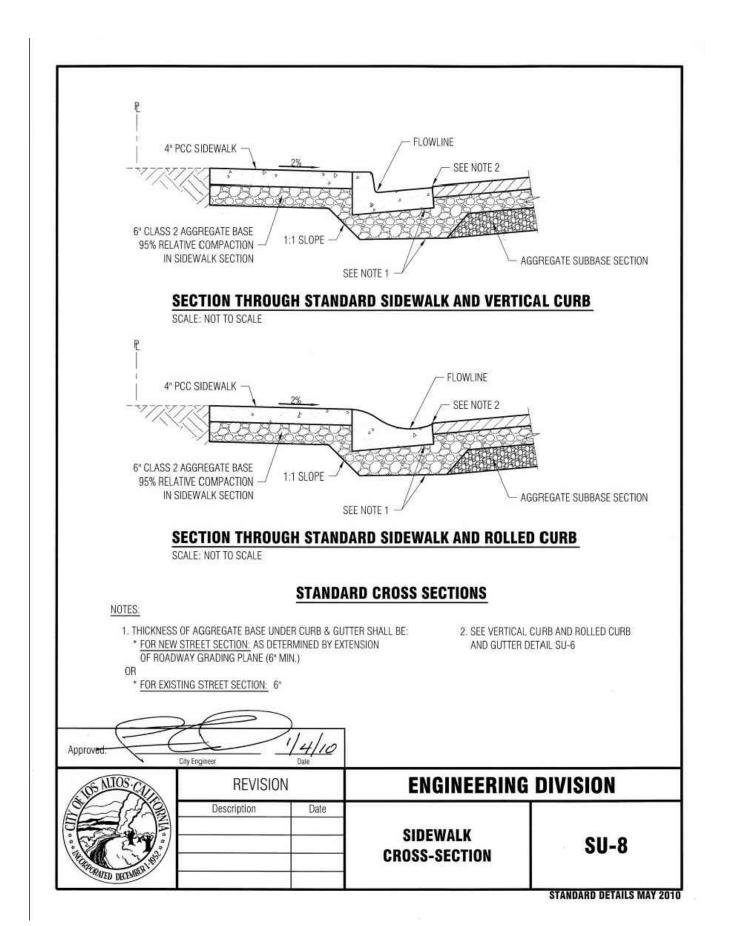
1. ACTUAL PAVEMENT REPAIR EXTENTS AND QUANTITIES MAY VARY. ENGINEER SHALL DETERMINE PAVEMENT REPAIR EXTENTS AFTER COLD PLANE OPERATIONS ARE COMPLETE AND PRIOR TO PAVING.

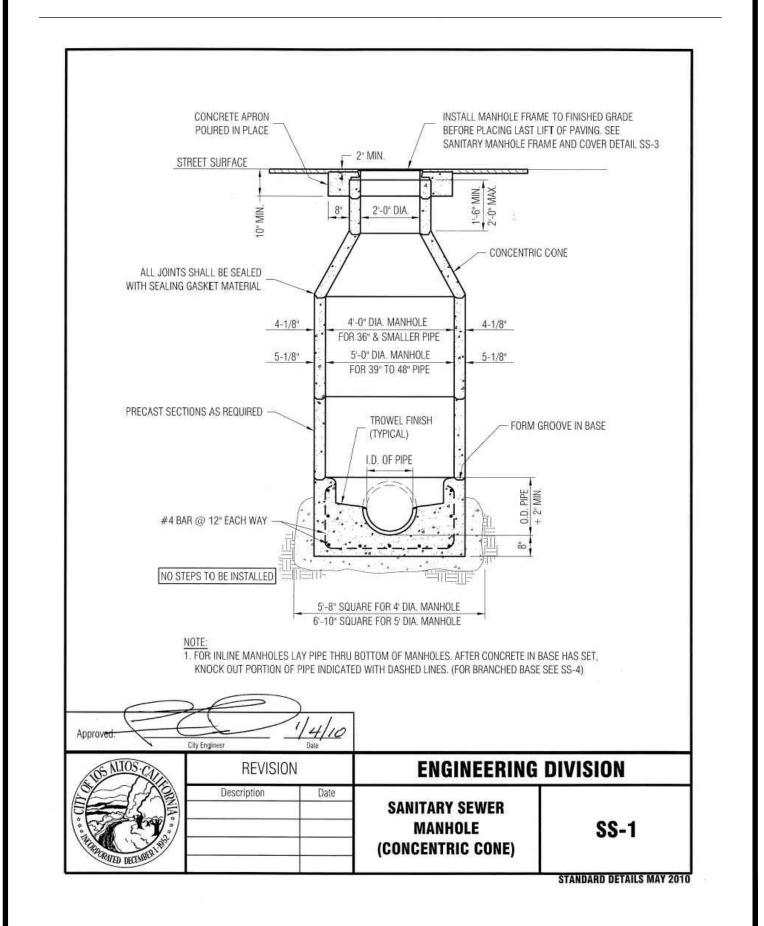
Call Two Working

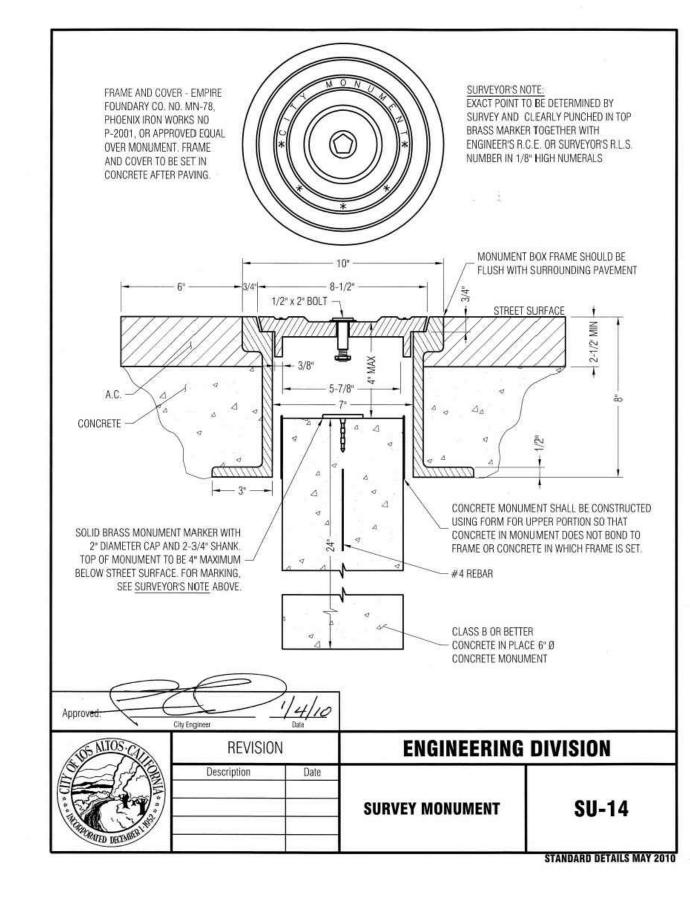


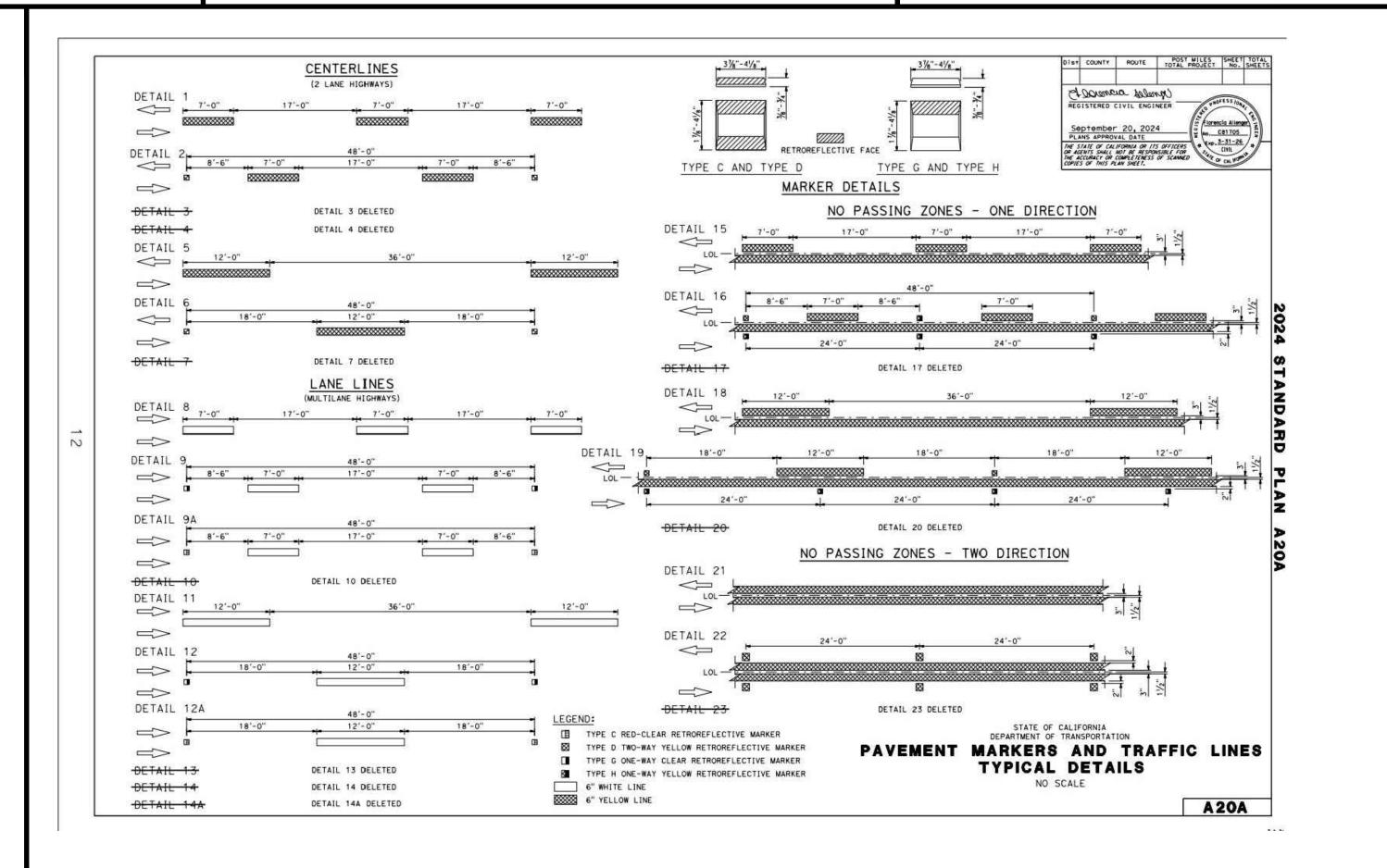
90% SUBMITTAL MAY 16, 2025







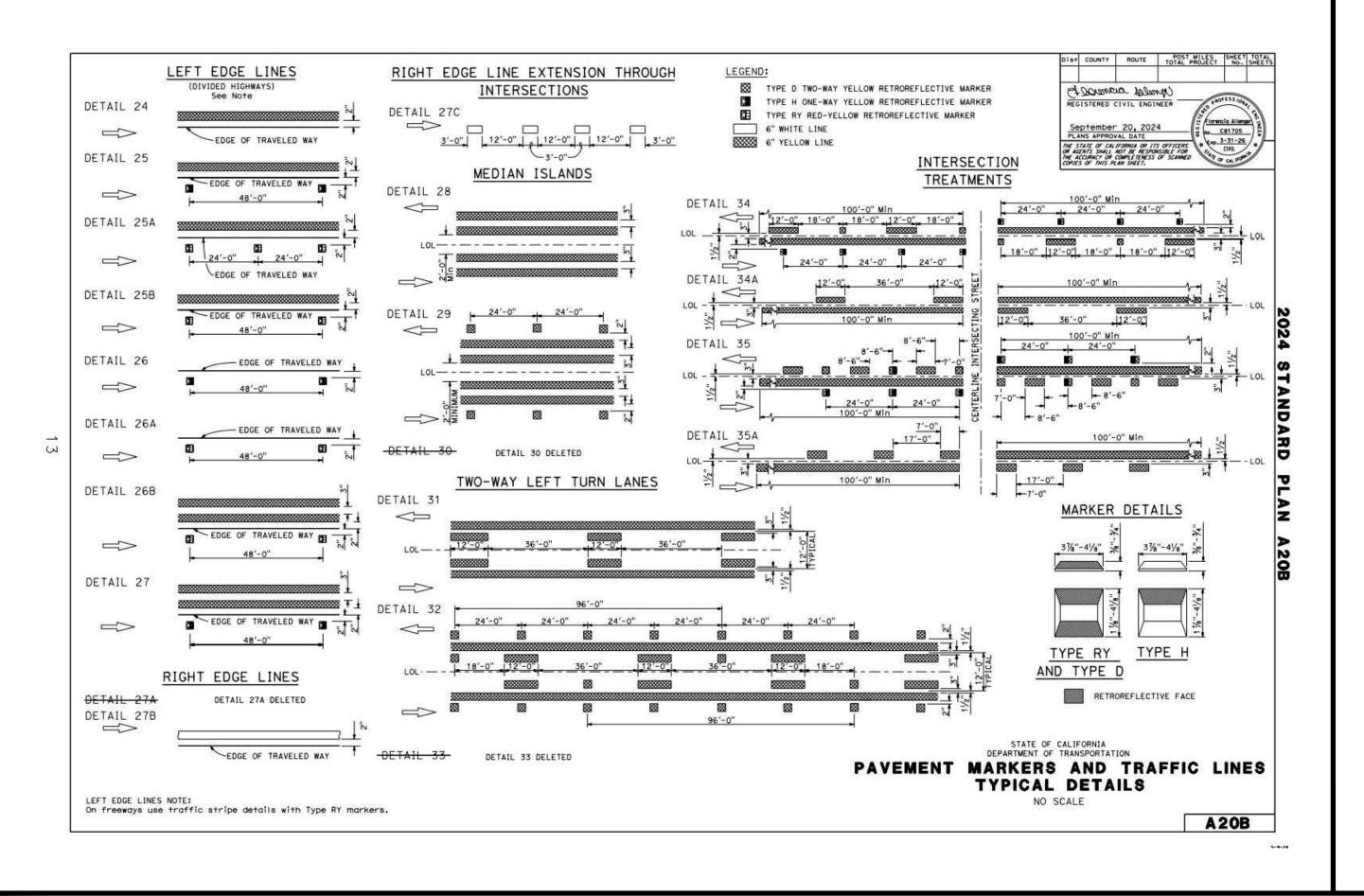


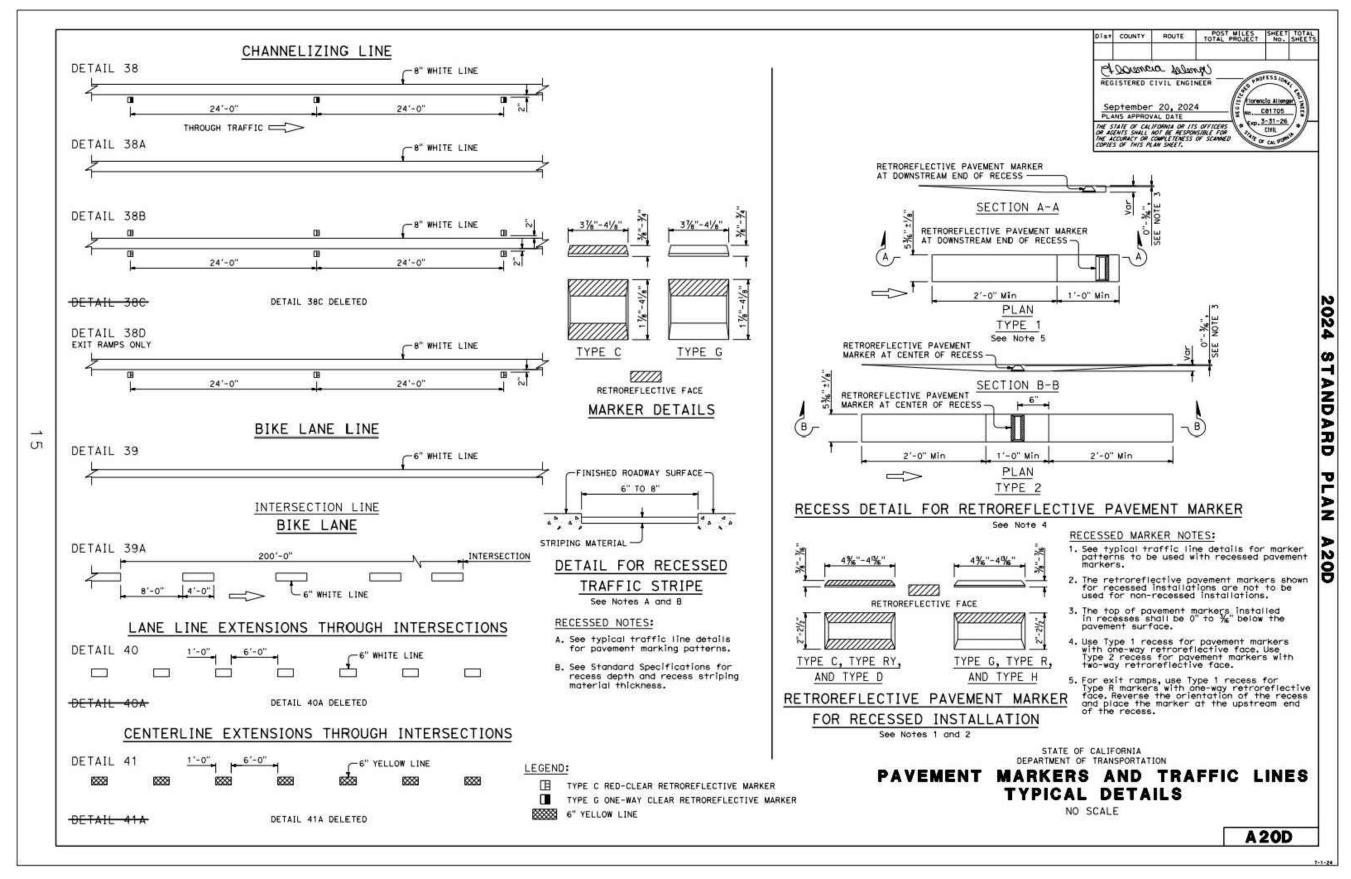


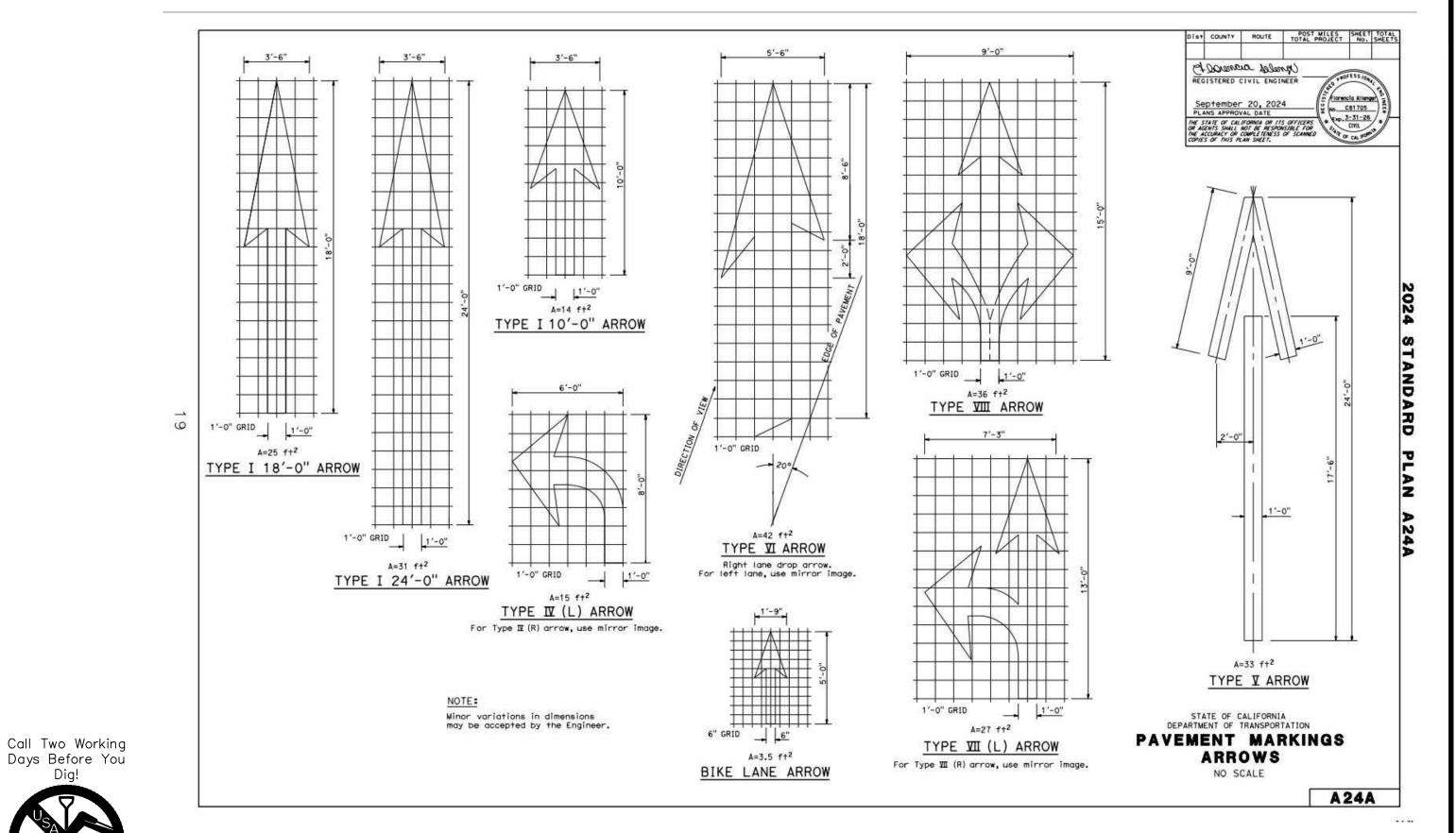


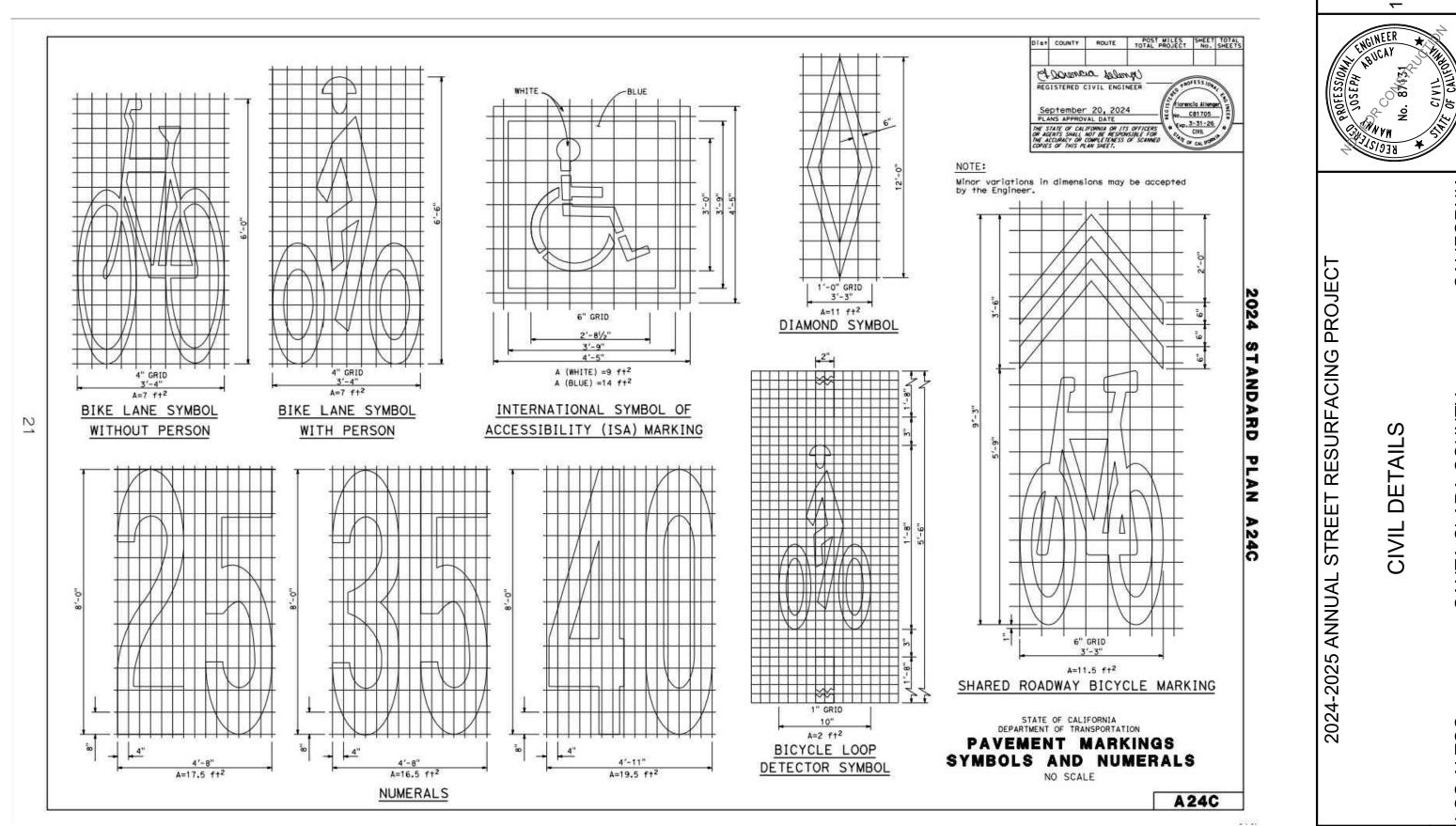
65% SUBMITTAL APRIL 07, 2025

C5.









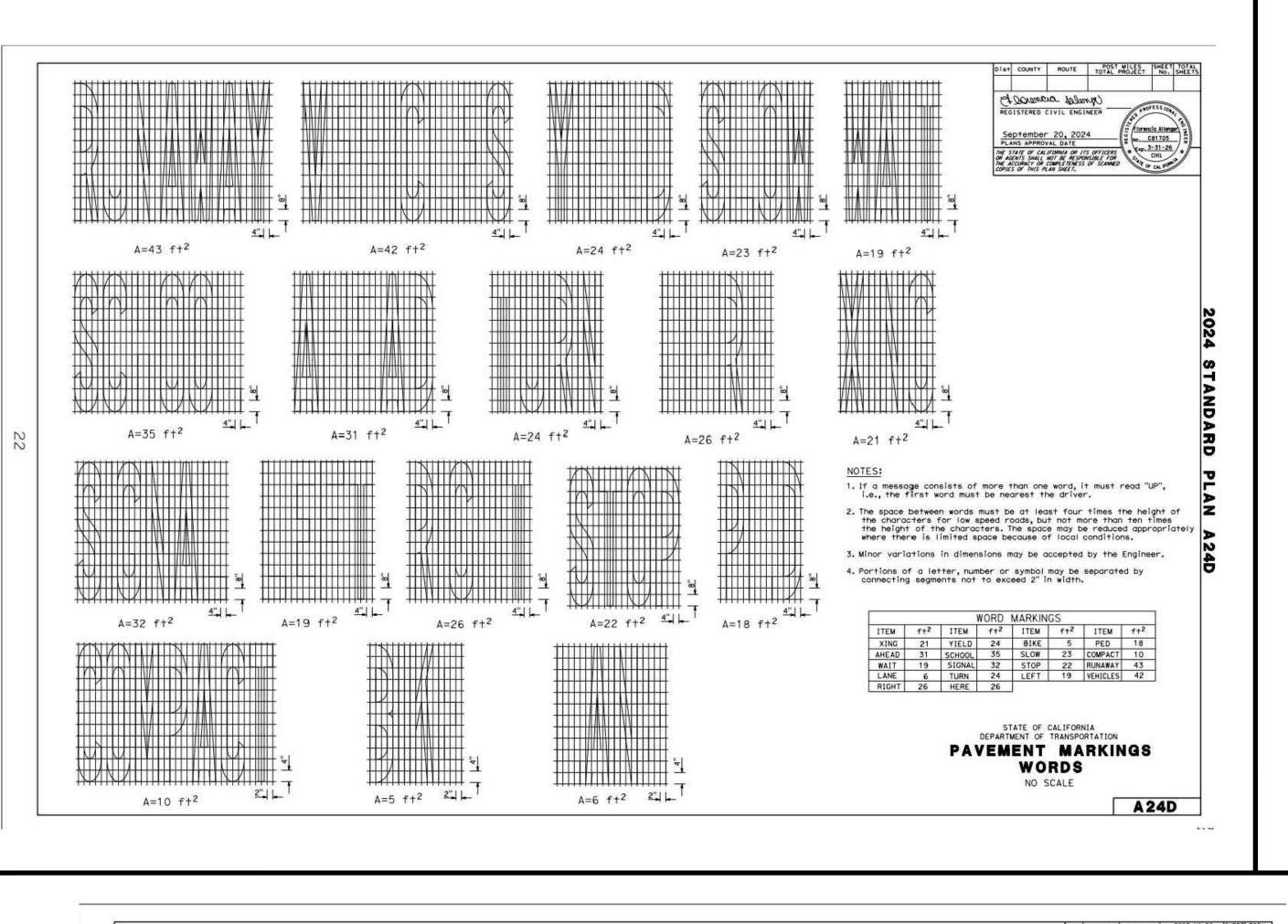
65% SUBMITTAL APRIL 07, 2025

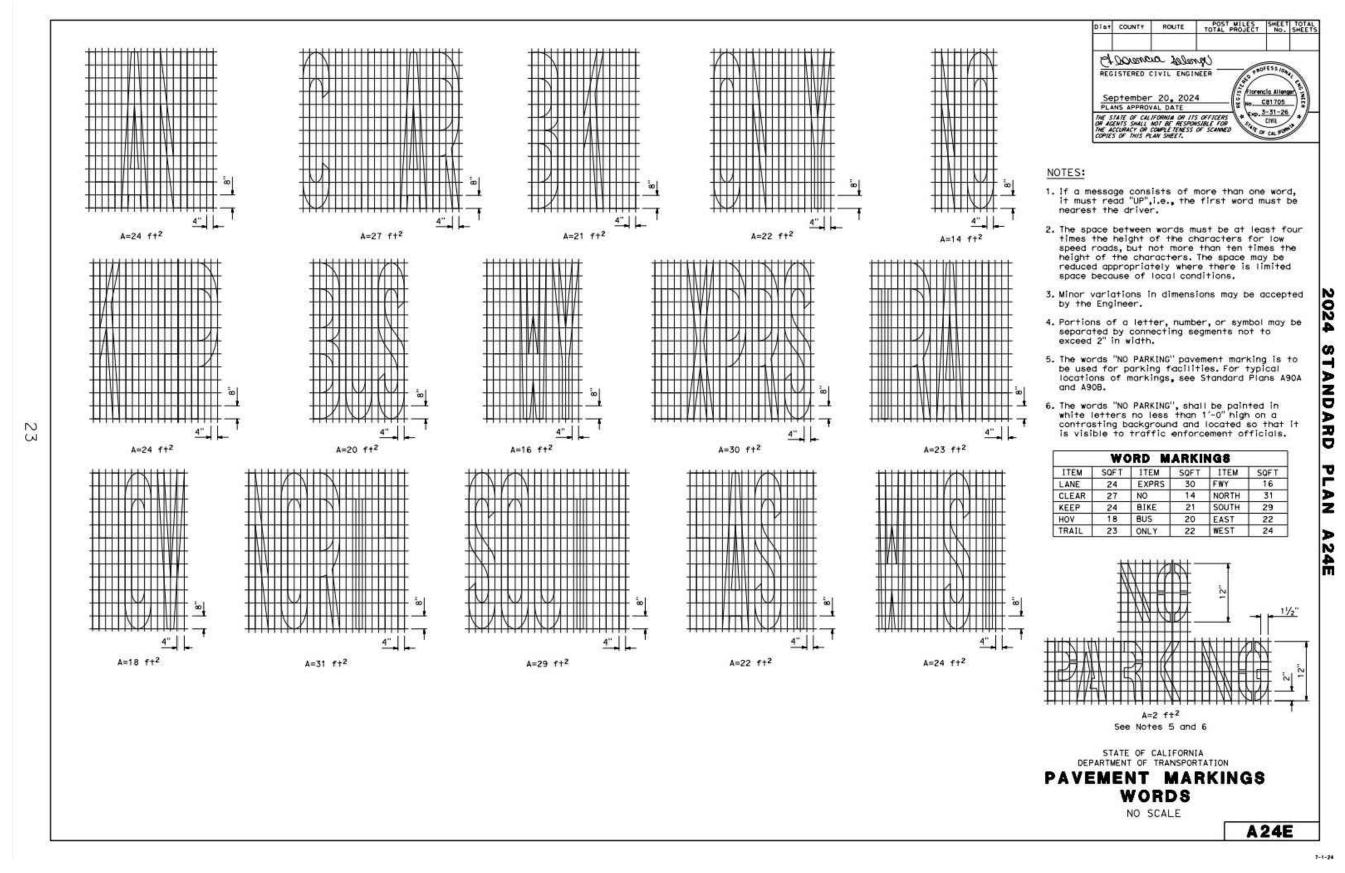
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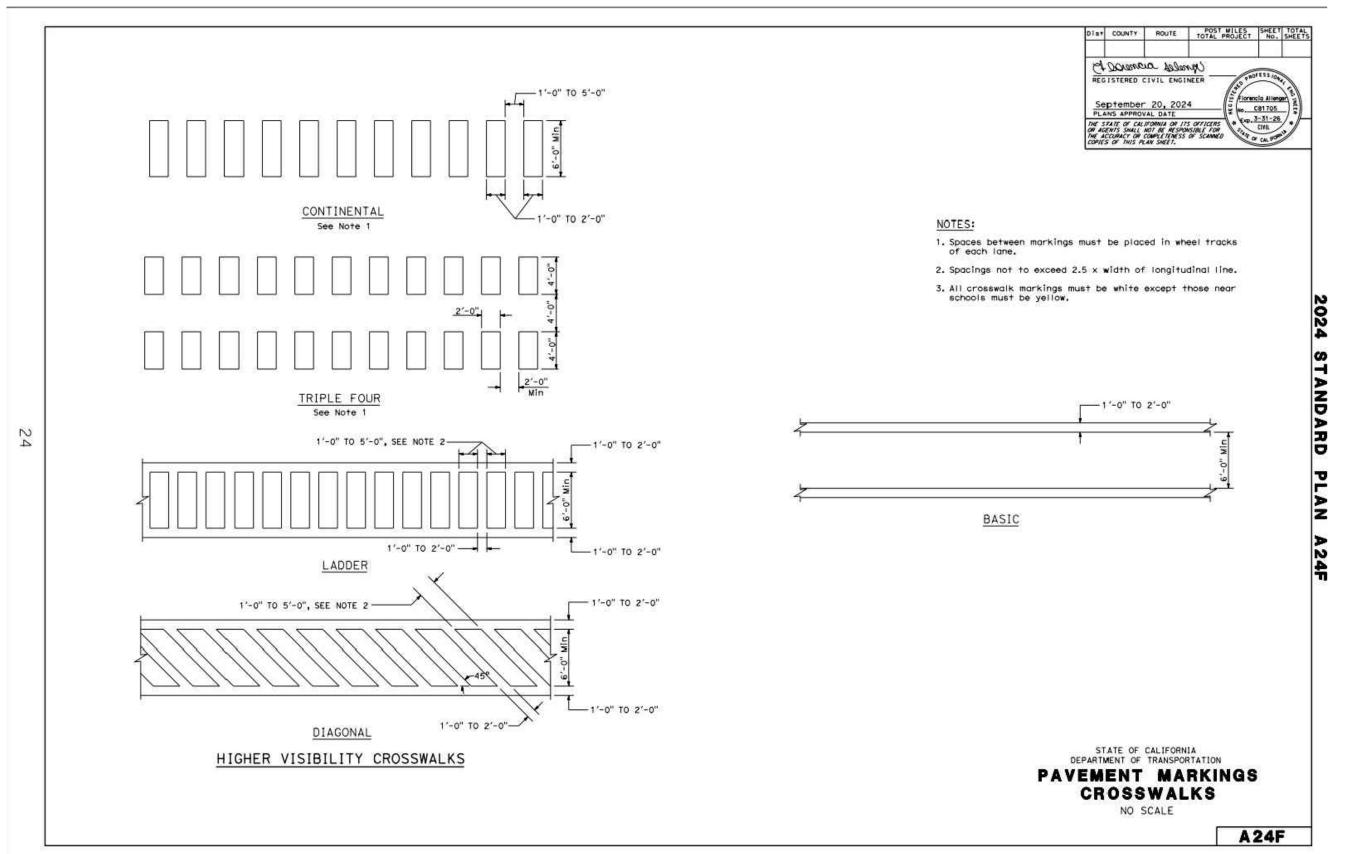
C5.2

40 OF 50
JOB NO.
25007

SAN NORTH

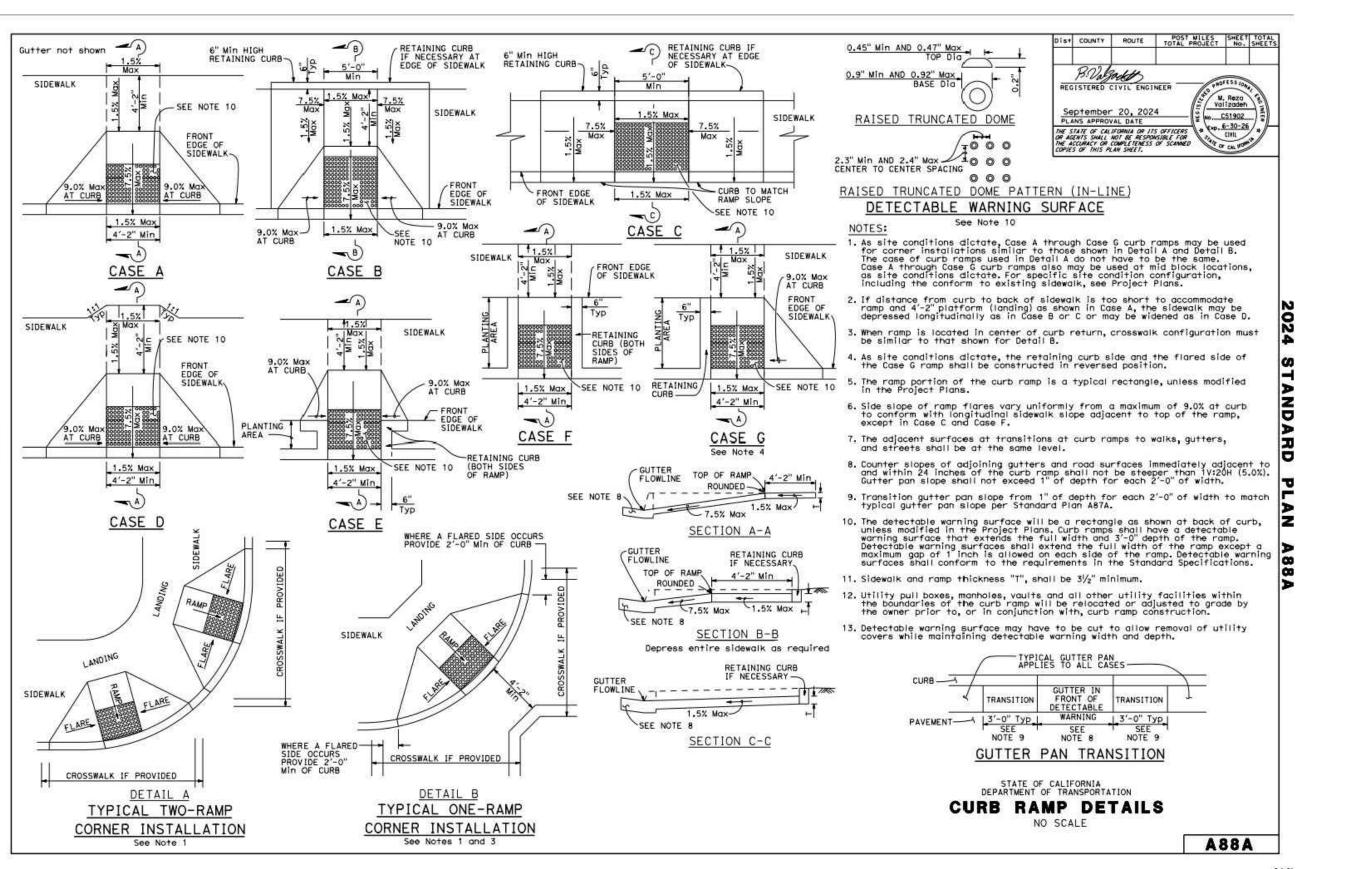






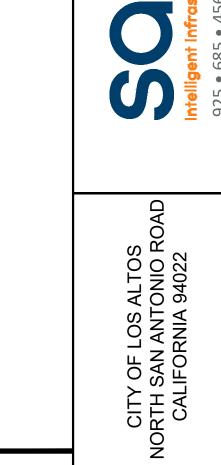
Call Two Working

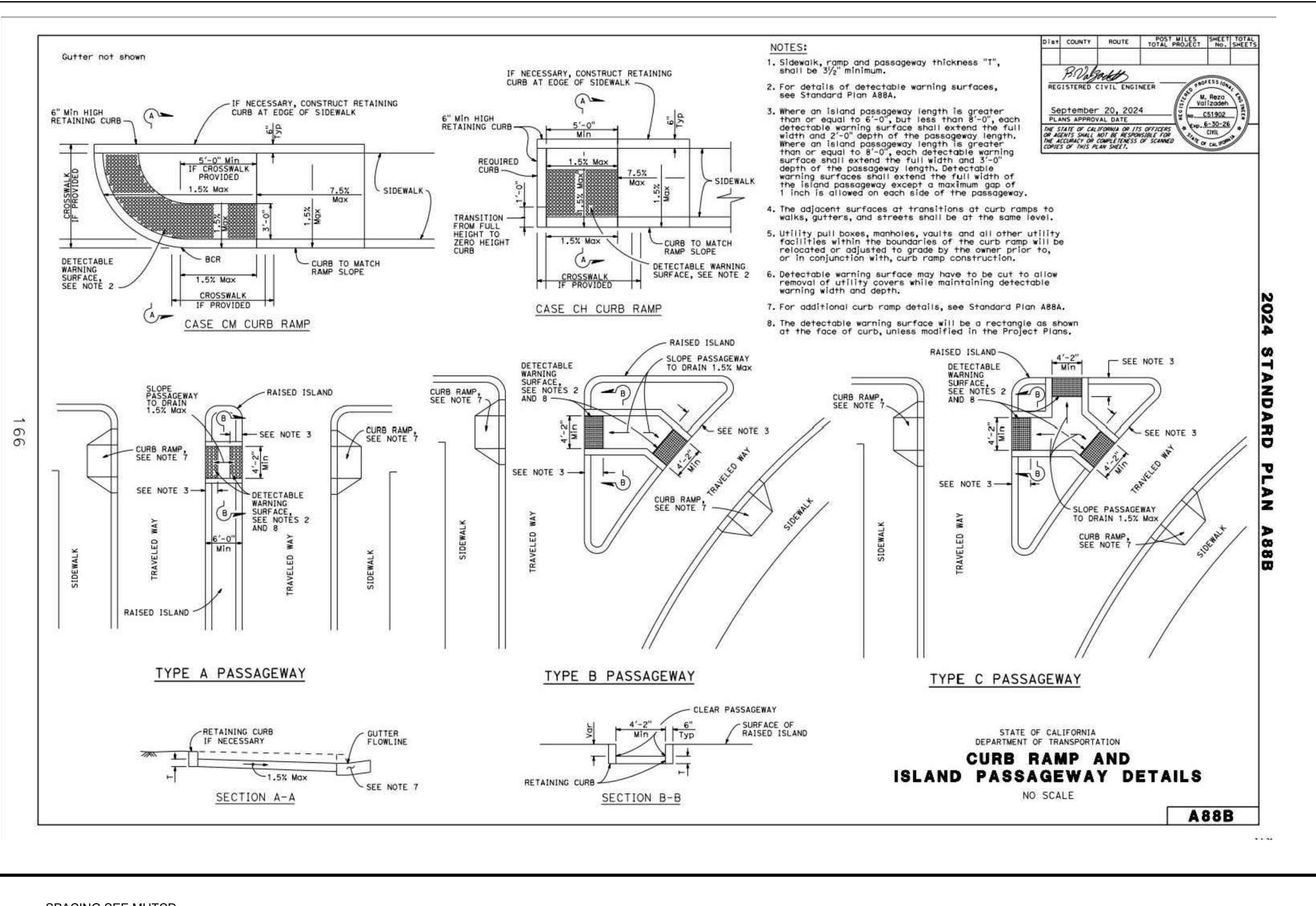
Days Before You

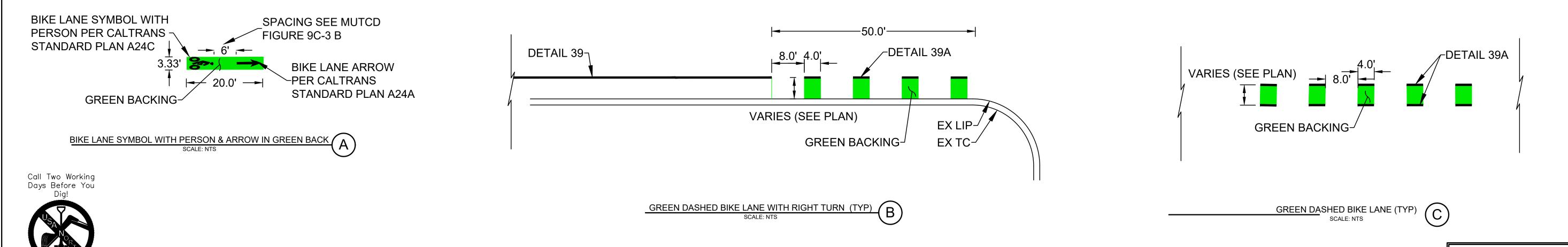


65% SUBMITTAL APRIL 07, 2025

3 Я C5.



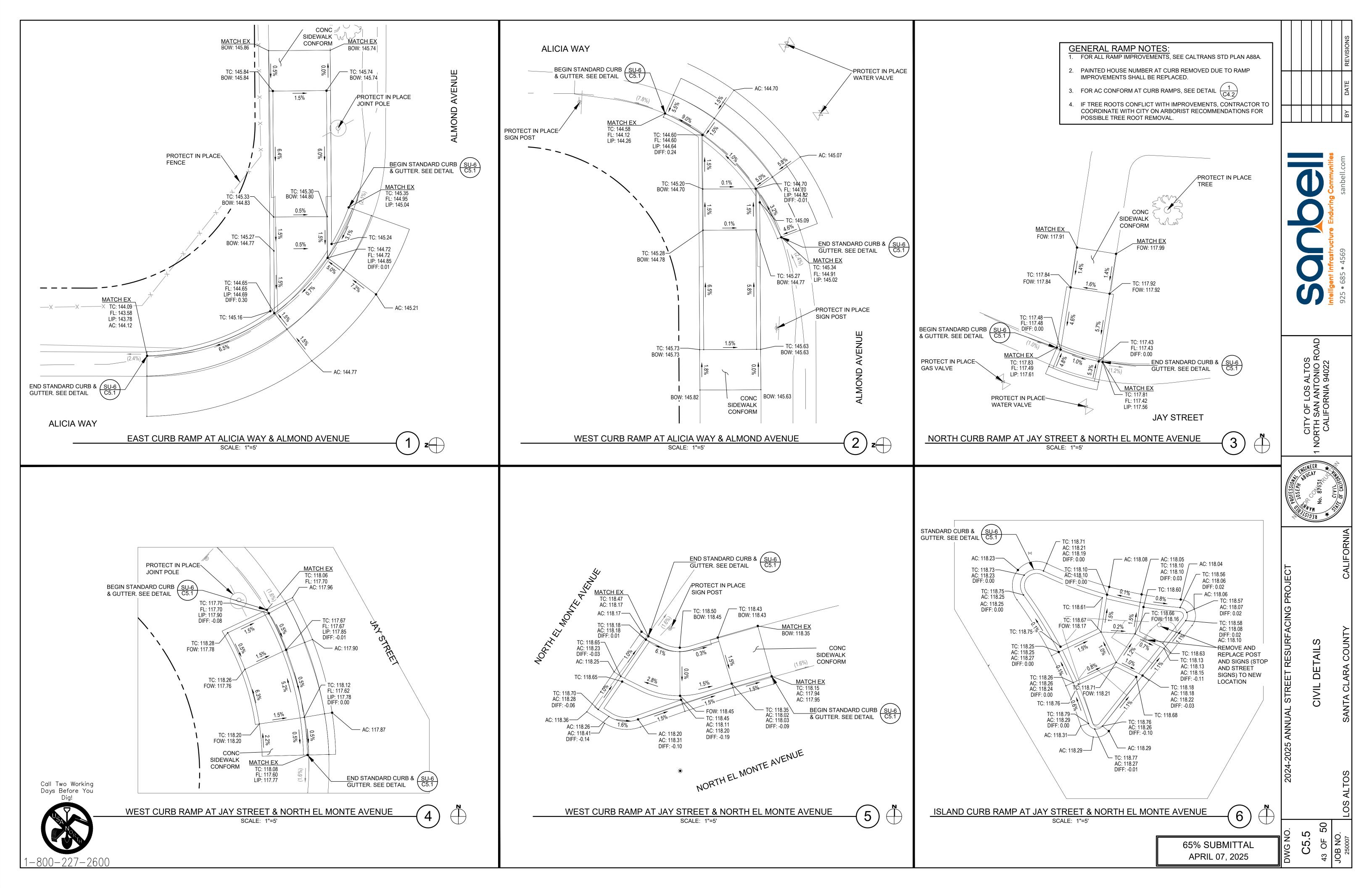


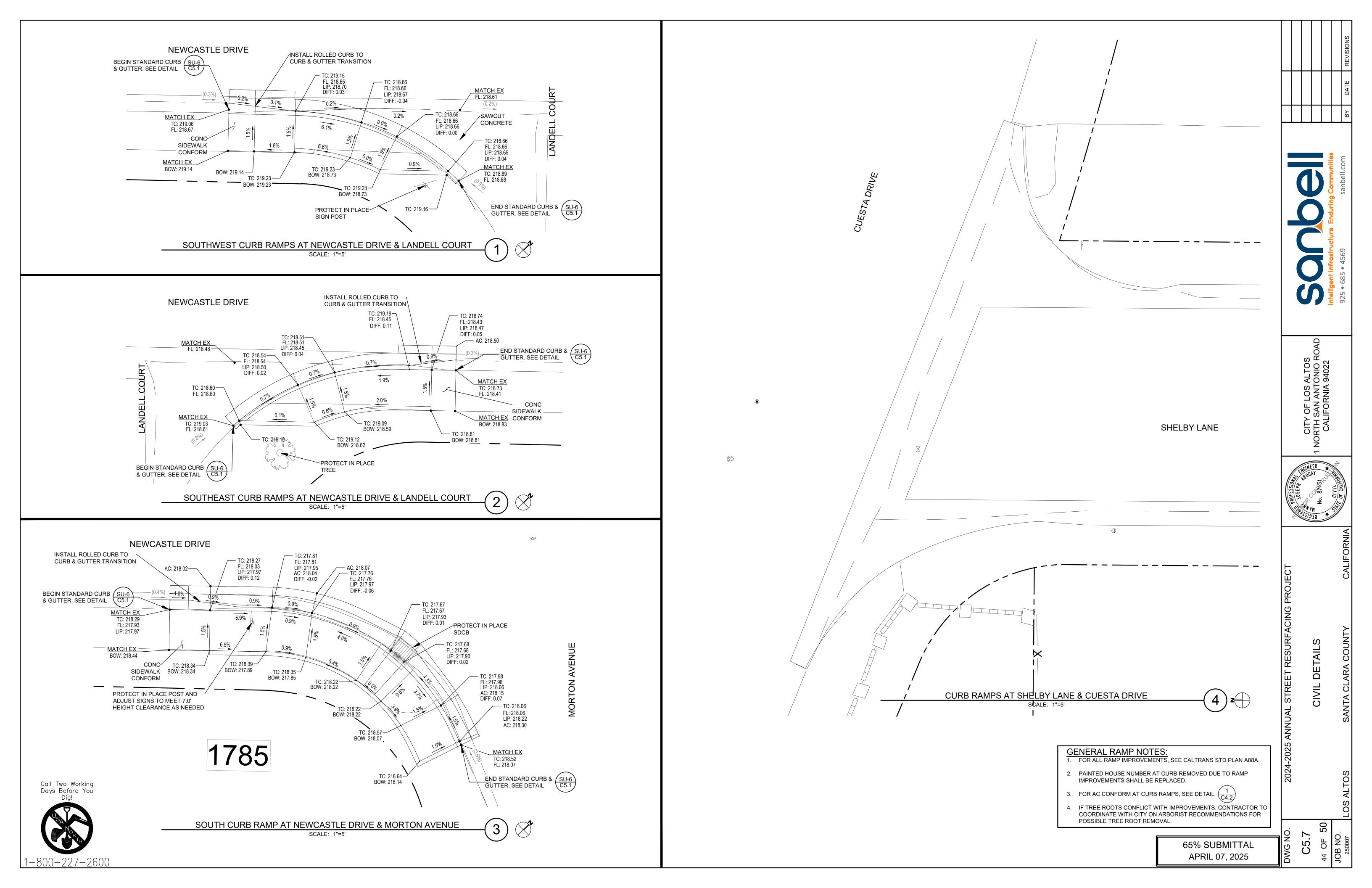


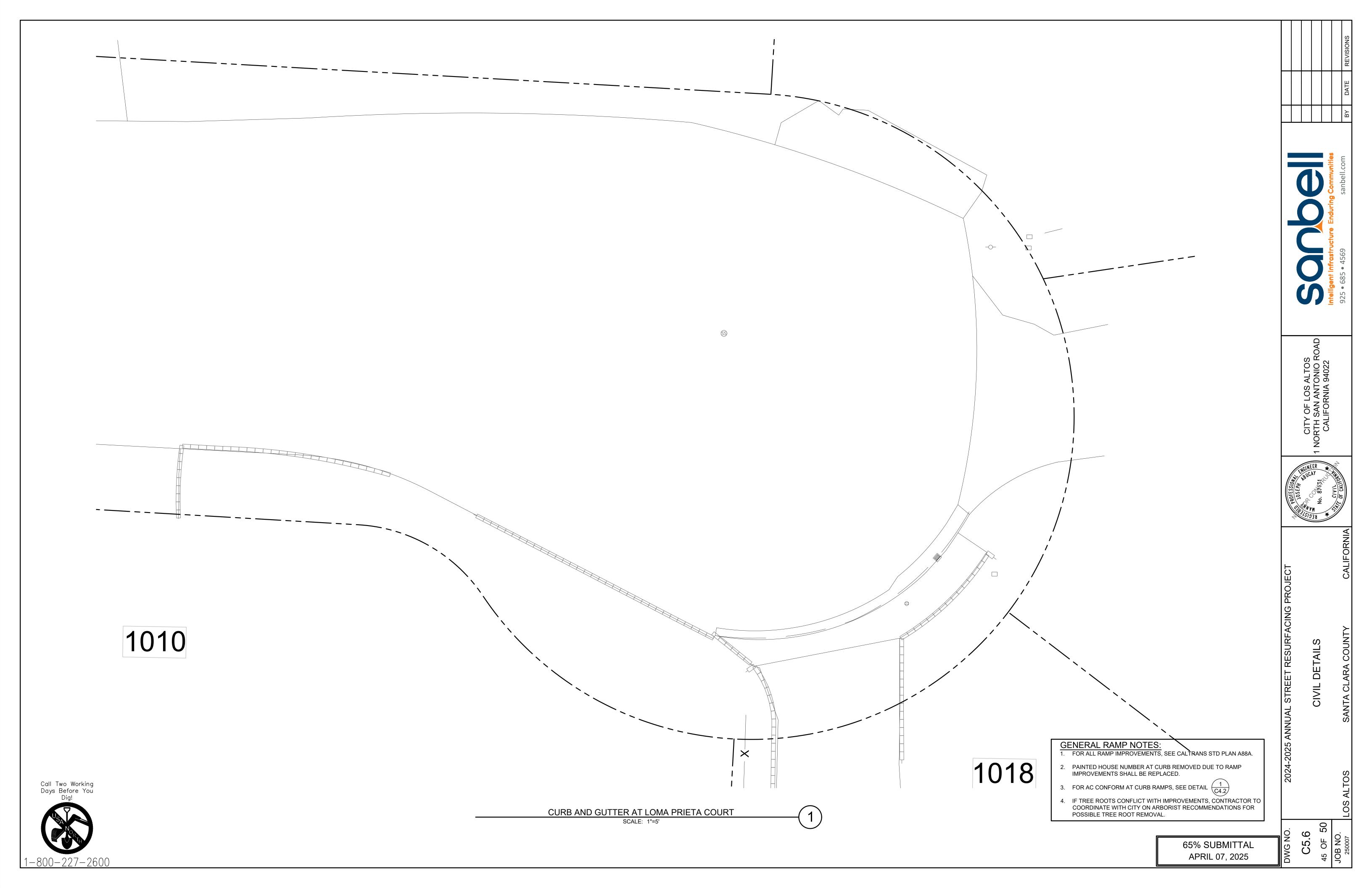
65% SUBMITTAL

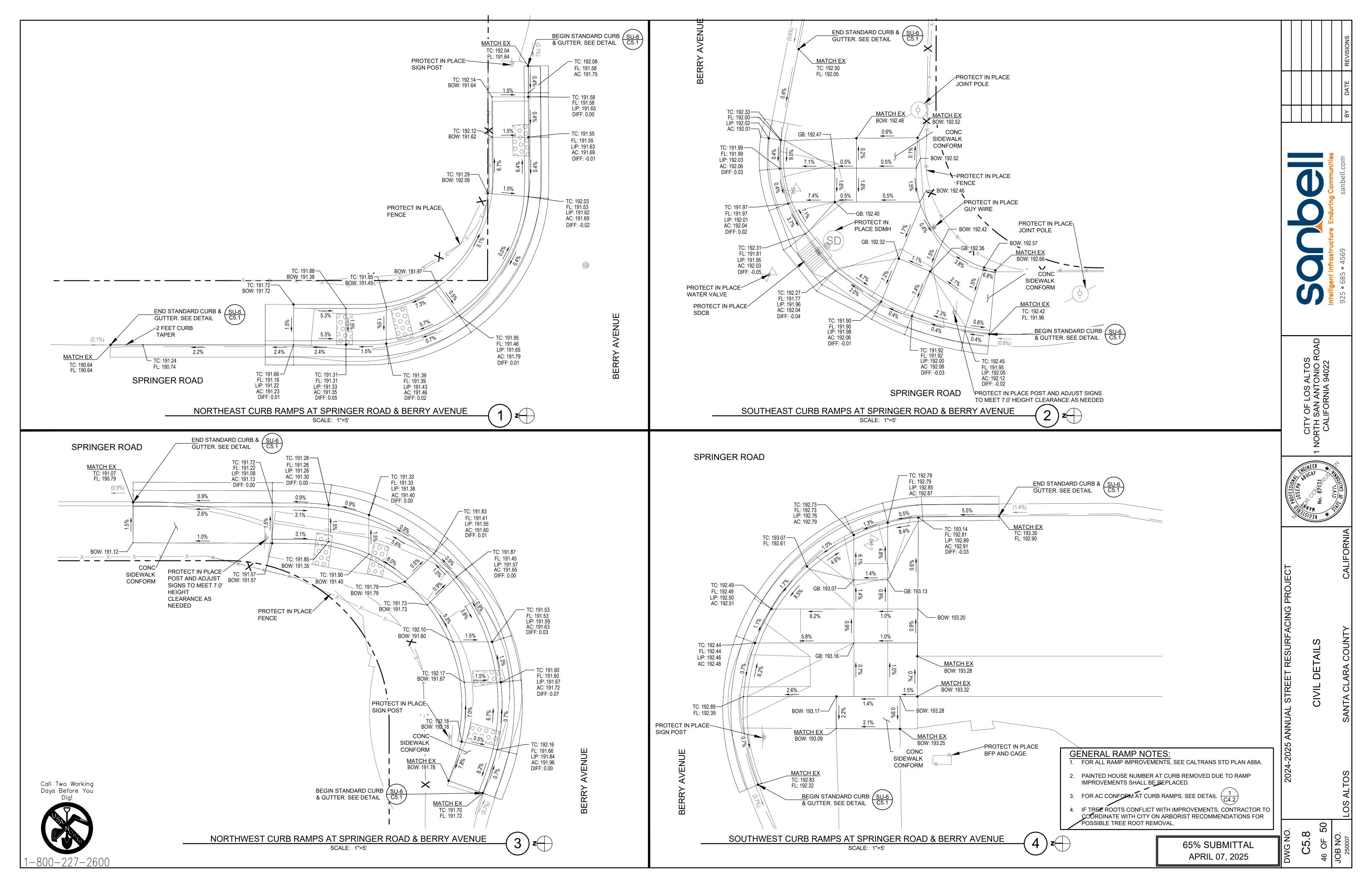
APRIL 07, 2025

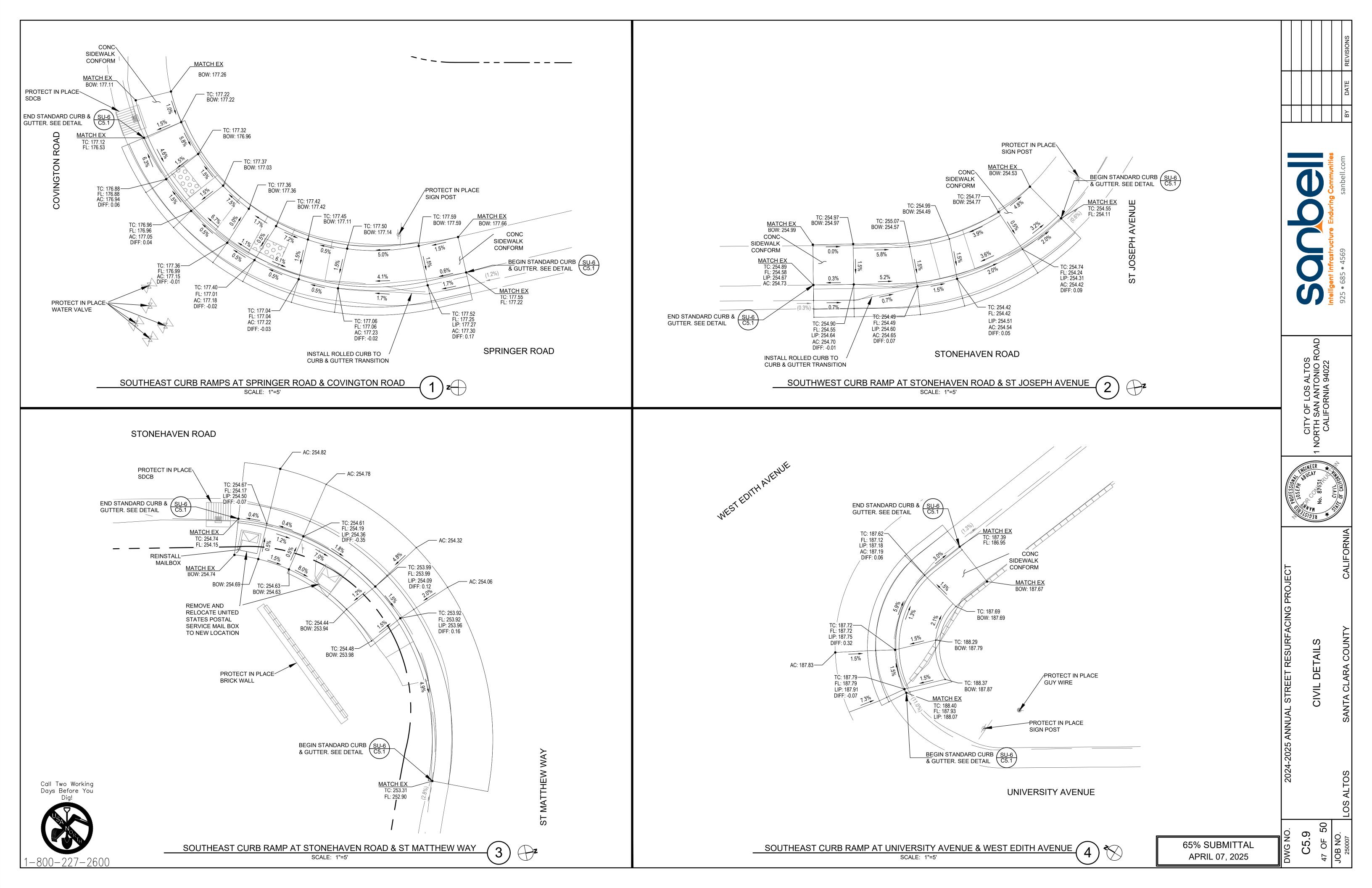
C5.

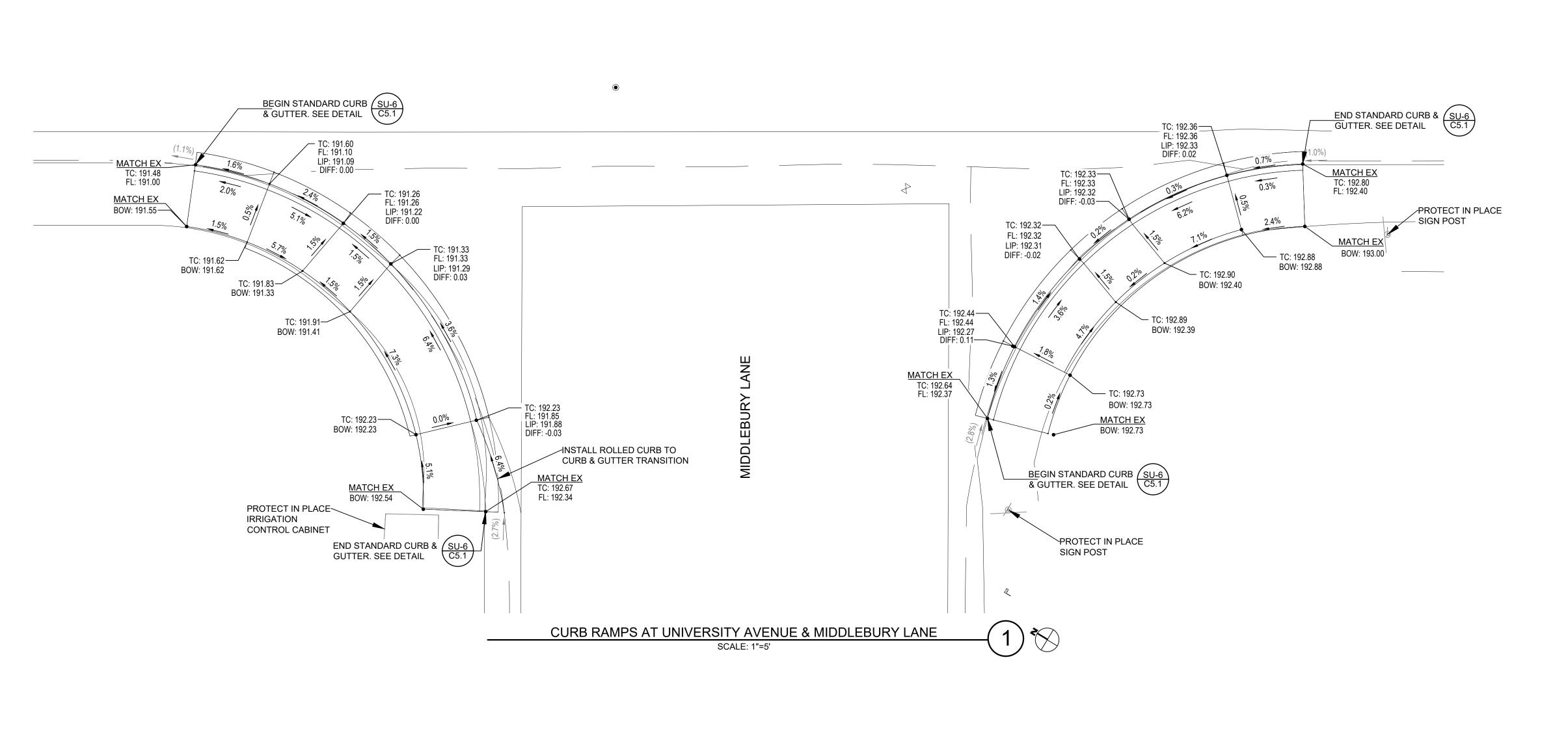












Call Two Working Days Before You Dig!

GENERAL RAMP NOTES:

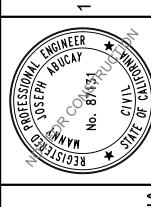
1. FOR ALL RAMP IMPROVEMENTS, SEE CALTRANS STD PLAN A88A.

PAINTED HOUSE NUMBER AT CURB REMOVED DUE TO RAMP IMPROVEMENTS SHALL BE REPLACED.

3. FOR AC CONFORM AT CURB RAMPS, SEE DETAIL $\left(\frac{1}{C4.2}\right)$

4. IF TREE ROOTS CONFLICT WITH IMPROVEMENTS, CONTRACTOR TO COORDINATE WITH CITY ON ARBORIST RECOMMENDATIONS FOR POSSIBLE TREE ROOT REMOVAL.





C5.10
48 OF 50
JOB NO.

DWG NO

65% SUBMITTAL APRIL 07, 2025

