

# Lebanon Urban Design Verification (UDV)

- **Project Type:**  
Planning
- **Presenter:**  
James Feldmann  
Sr. Transportation Planner
- **Intent:**  
Planning Project Early Communications



# Lebanon UDV Agenda

## Topics:

- Study Overview
- Design Concepts
- Next Steps

## Objective(s)

- Early Communication Only
- Seeking Interested Parties Feedback



# Lebanon UDV

## Why a UDV Study?

Define specific concepts to improve biking, walking, and driving. Once completed, the study positions ODOT and Lebanon to leverage related projects and apply for grant funding. This work includes:

1. Developing concepts in alignment with current ODOT standards
2. Recommending new and enhanced bicycle facility designs and parallel off-highway routes
3. Recommending new and enhanced pedestrian crossings (including curb extensions and flashing beacons with overhead masts)
4. Seeking feedback on proposed concept designs

# Lebanon UDV Context

## Highways:

- OR 34 (N 12<sup>th</sup> St to US 20)
- US 20 (Reeves Pkwy to Weldwood Dr)

## Roadway Characteristics:

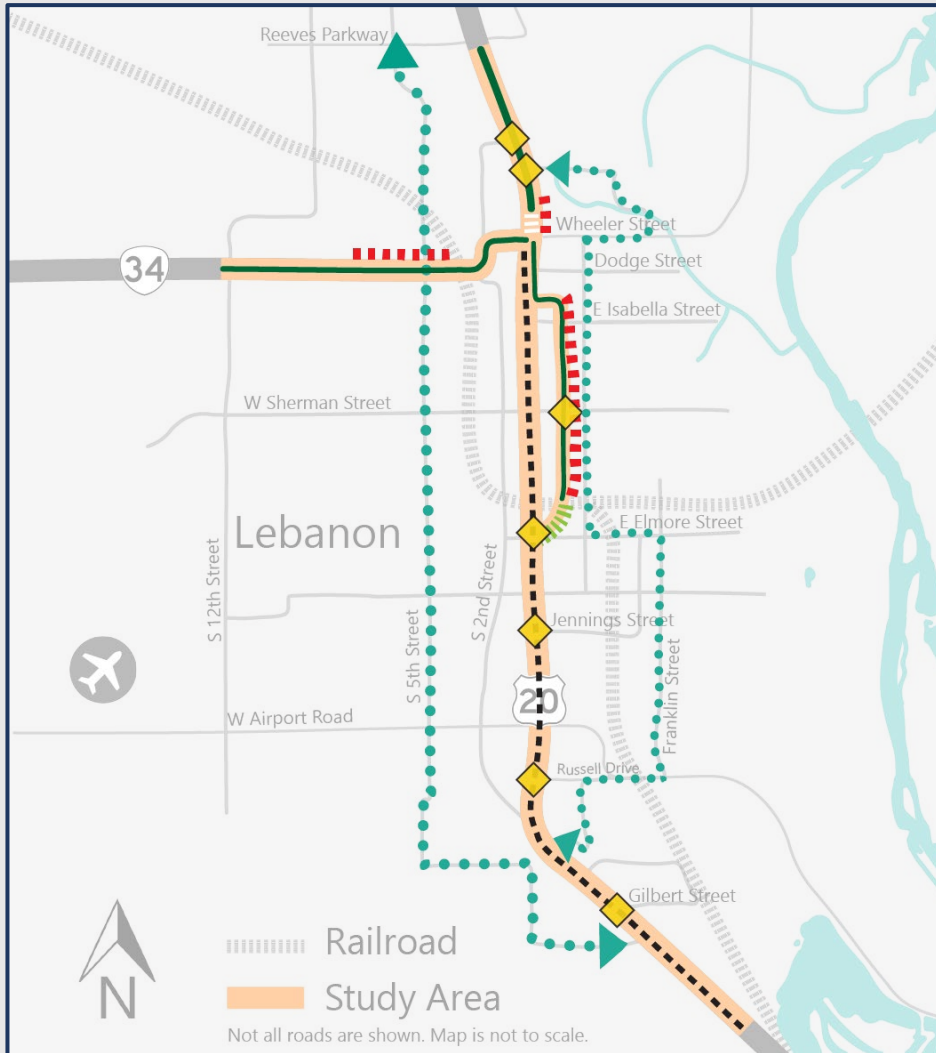
City	AADT	Truck %	Urban Context	366.215
OR 34	7,321	10	Residential & Downtown	Yes
US 20 (Reeves Pkwy - couplet)	6,413 - 10,847	11-13%	Commercial, Urban Mix, & Downtown	Yes
US 20 (south of couplet)	18,798 - 21,041	5-11%	Urban Mix & Commercial	Yes








## Single Trip Permits (6 yrs, over 12’):

- OR 34: 1,280
- US 20: 1,283



# Lebanon UDV Project Elements



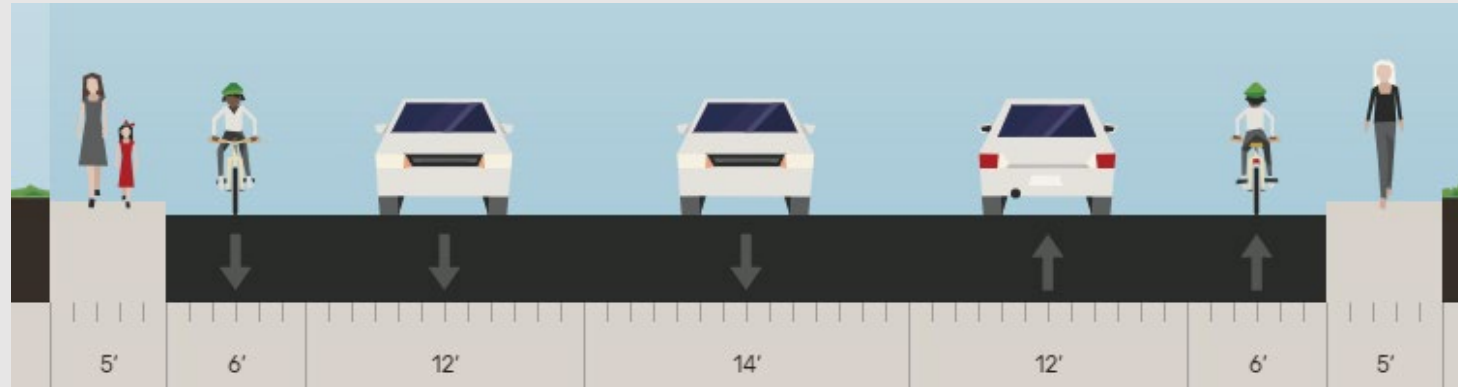
-  New buffered bike lanes
-  Buffered bike lane replaces parking
-  New standard bike lanes
-  Existing bike lanes
-  New enhanced crossings
-  No bike lanes
-  Parallel off-highway bike route

Proposed projects would **not** create new pinch points but a reduction in vehicle carrying capacity would result from

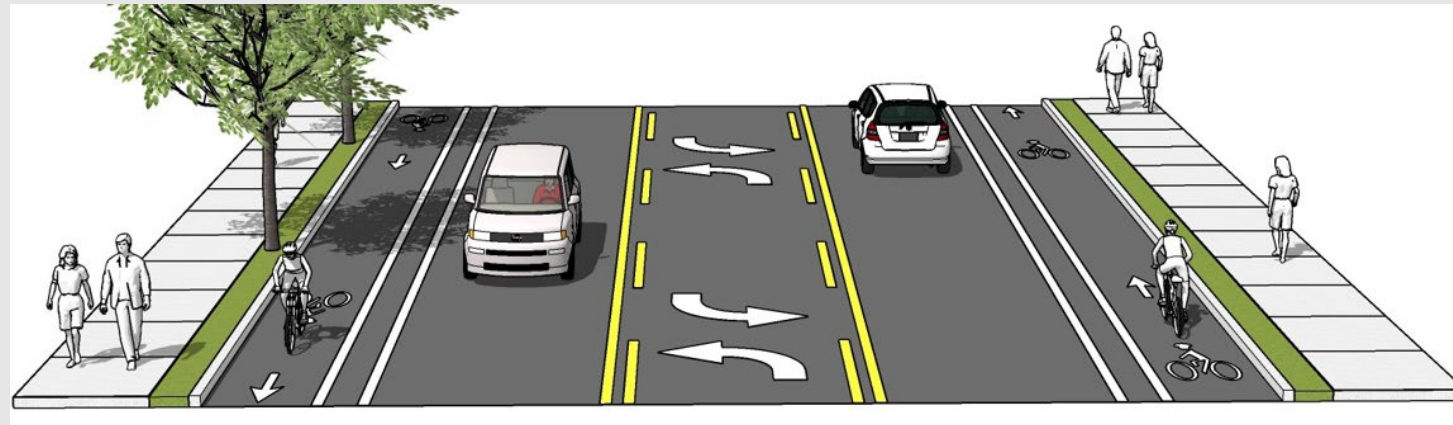
- curb extensions and raised crosswalk medians
- overhead masts associated with 4 new enhanced crossings
- vertical elements if buffered bike facilities became protected bike facilities

# Lebanon UDV

## OR 34: Hansard Ave to 6<sup>th</sup> St (Existing & Proposed)



Existing example, no buffered bike lane (~50' curb-to-curb)



Proposed example (with buffered bike lanes & lane width reallocation)

# Lebanon UDV

## OR 34: Hansard Ave to 6<sup>th</sup> St (Proposed)

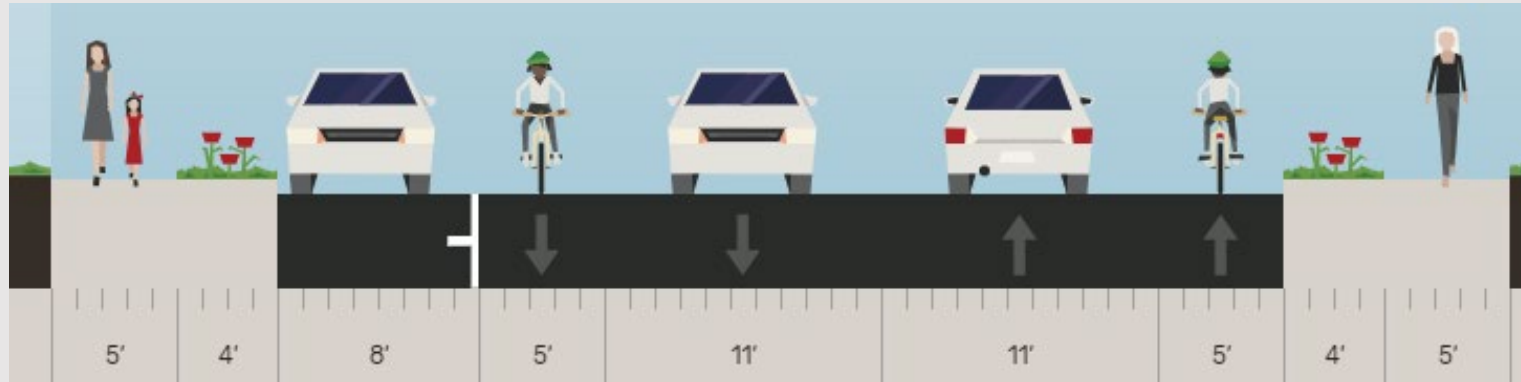


\* Protected bike lanes would reduce vehicle carrying capacity

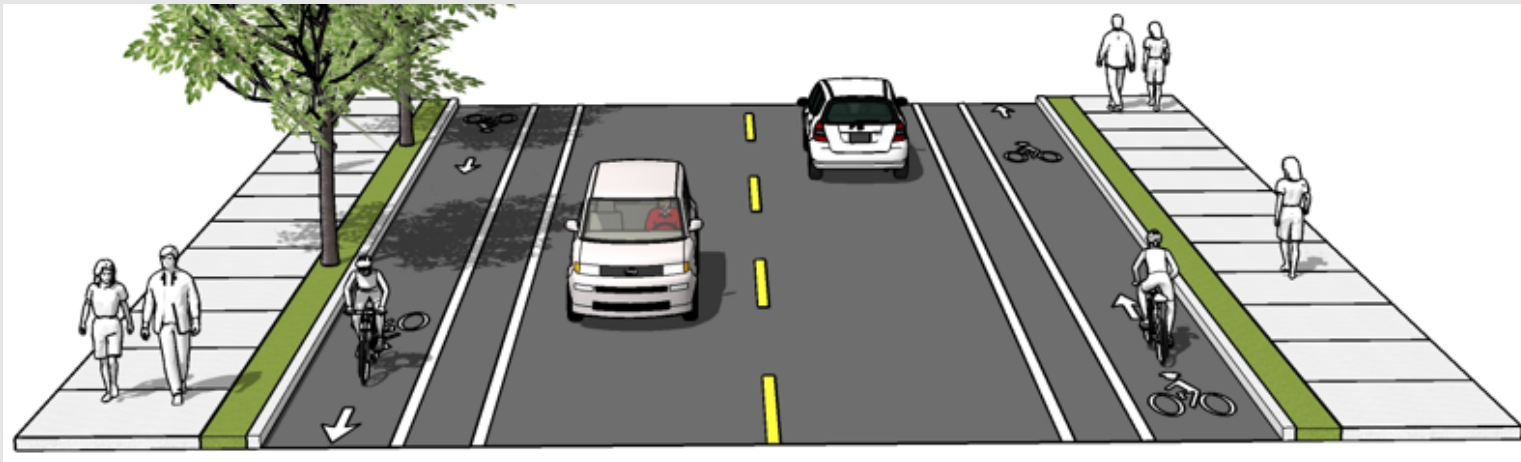


# Lebanon UDV

## OR 34: 6<sup>th</sup> St to US 20 (Existing & Proposed)



Existing example with parking (~40' curb-to-curb)



Proposed example (~40' curb-to-curb with buffered bike lanes and parking removal)

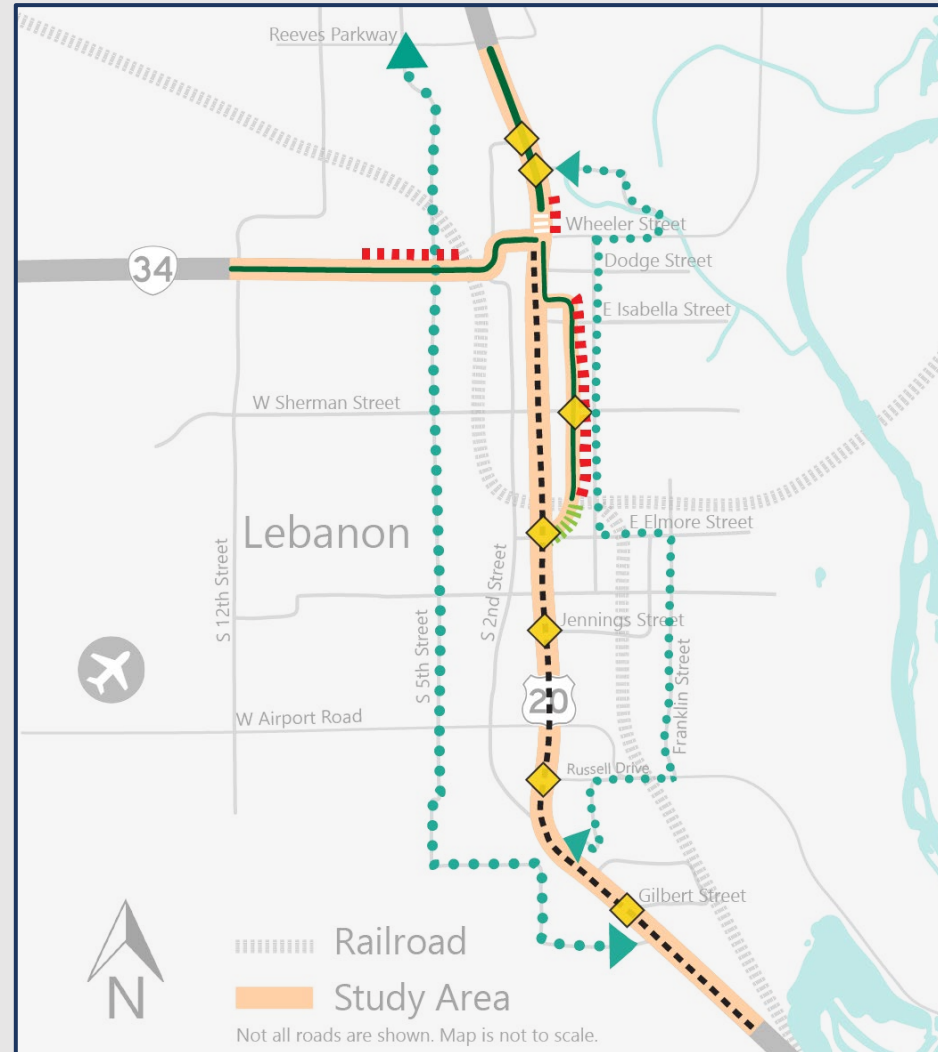









# Lebanon UDV

## OR 34: 6<sup>th</sup> St. to US 20 (Proposed)



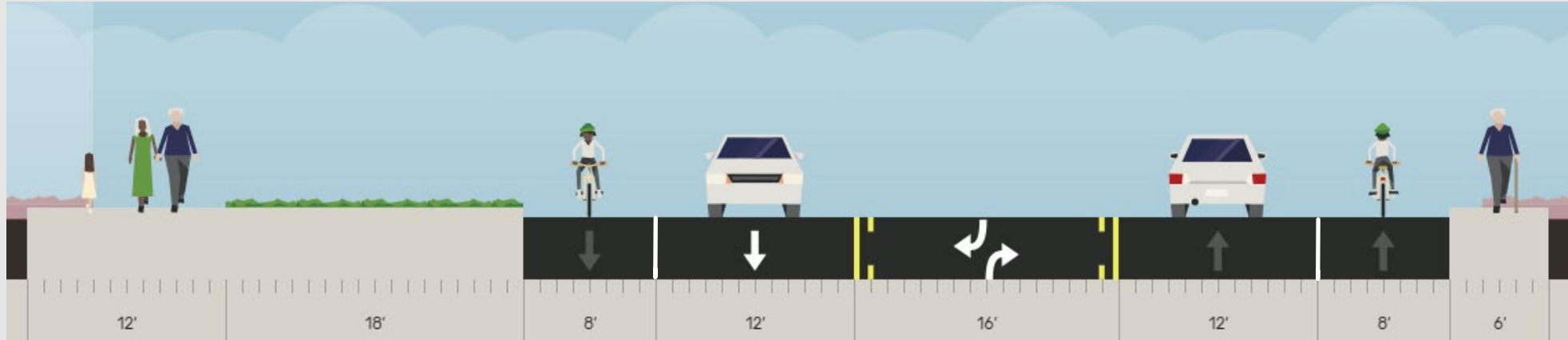
# Lebanon UDV Project Elements



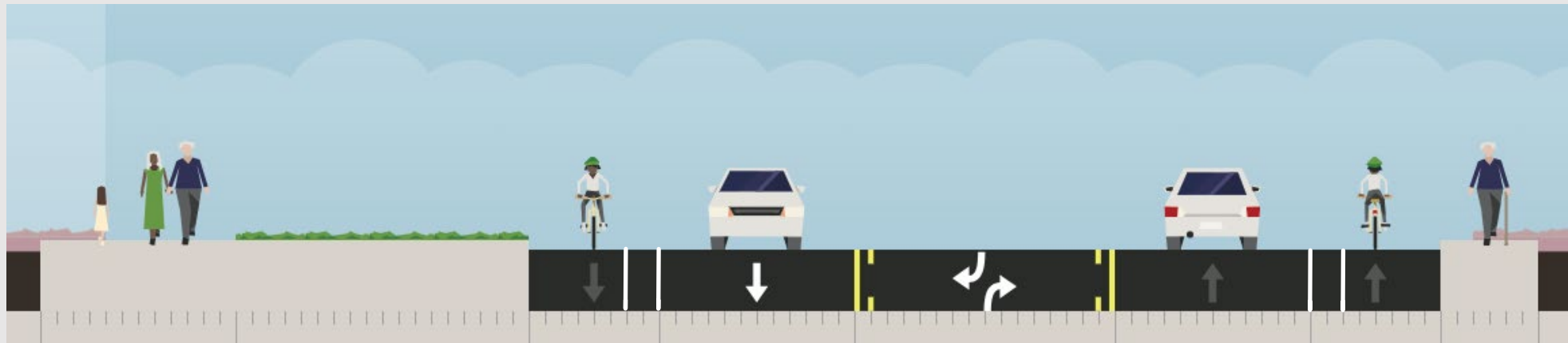
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# Lebanon UDV

## US 20: Reeves Pkwy to Industrial Way (Existing & Proposed)



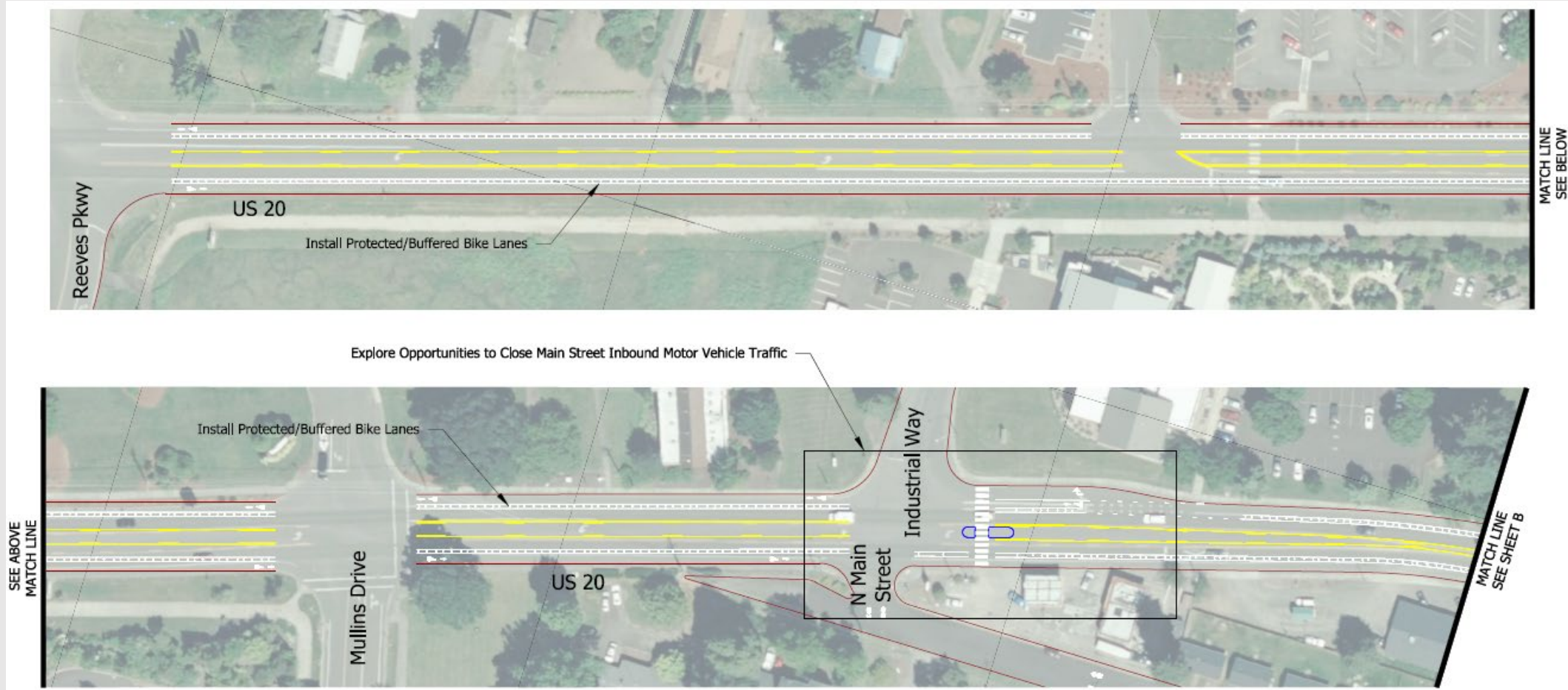
Existing example (~56' curb-to-curb)



Proposed example (~56' curb-to-curb with buffered bike lanes)

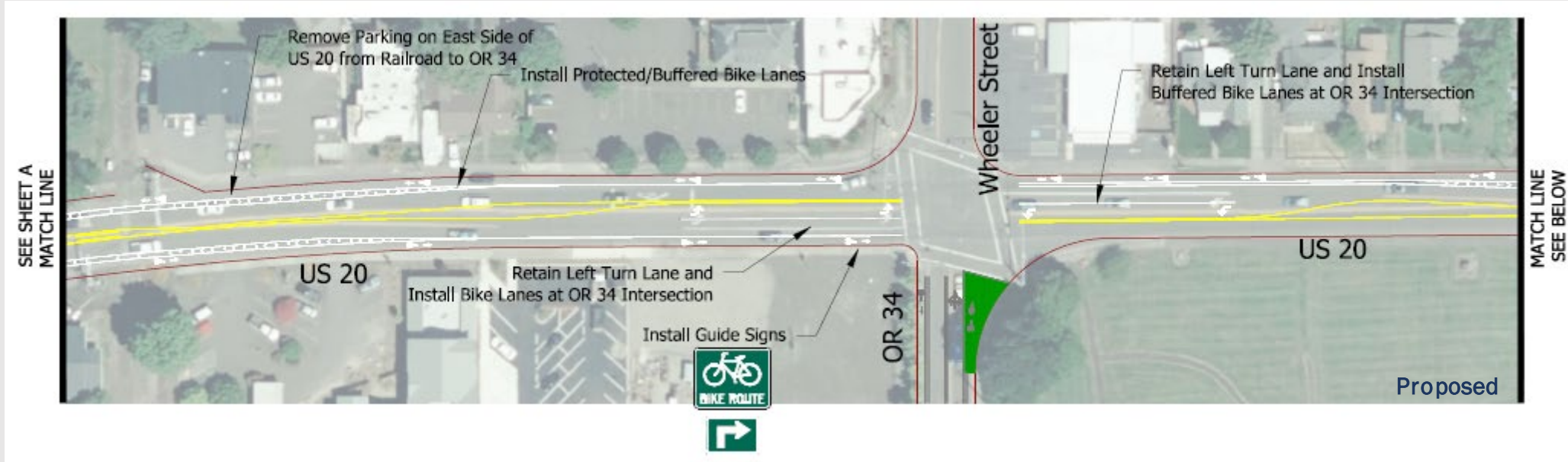
# Lebanon UDV

## US 20: Reeves Pkwy to Industrial Way (Proposed)

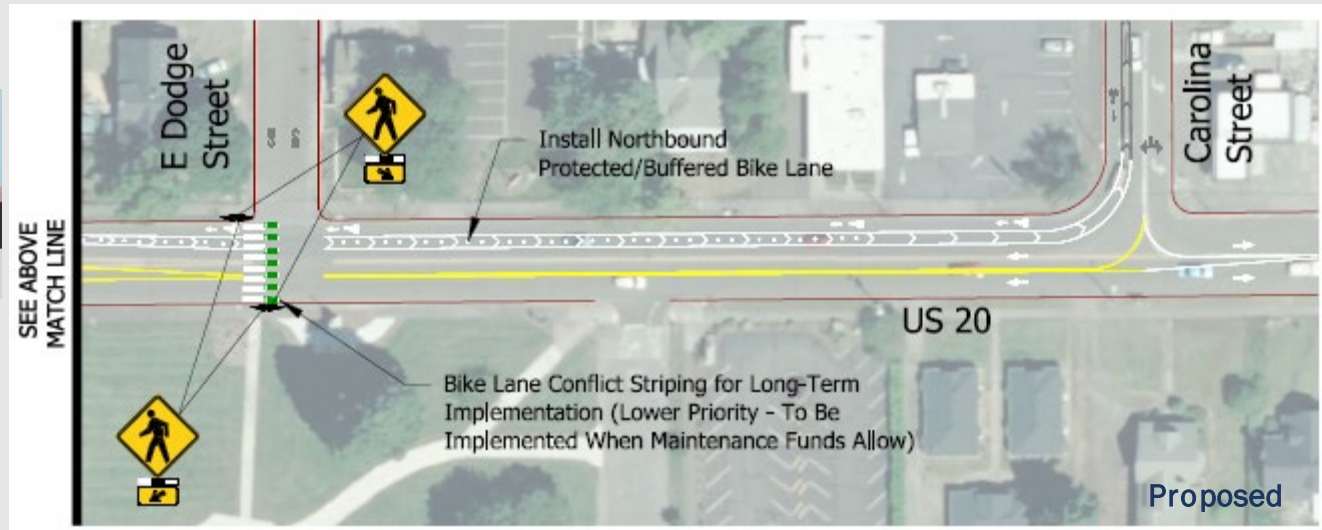
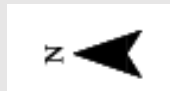
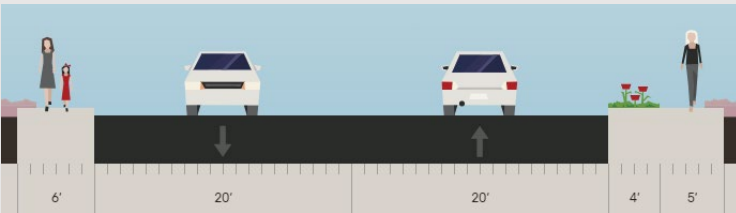


# Lebanon UDV

## US 20: Industrial Way to Carolina St. (Existing & Proposed)

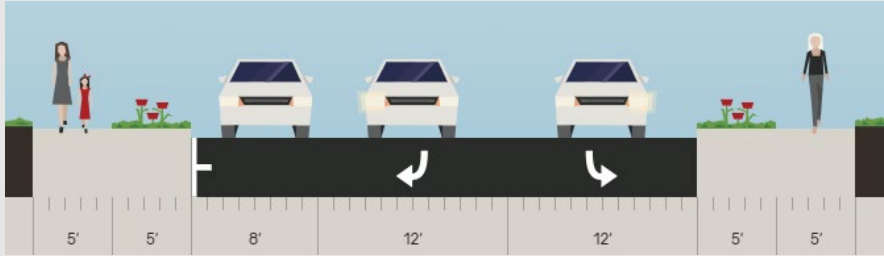


Existing (US 20: betw/ Dodge & Carolina St.)  
~40' curb-to-curb

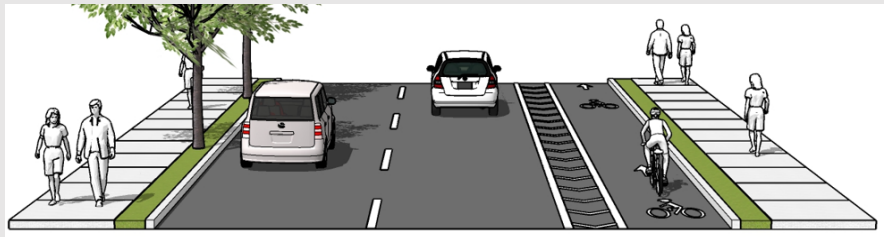


# Lebanon UDV

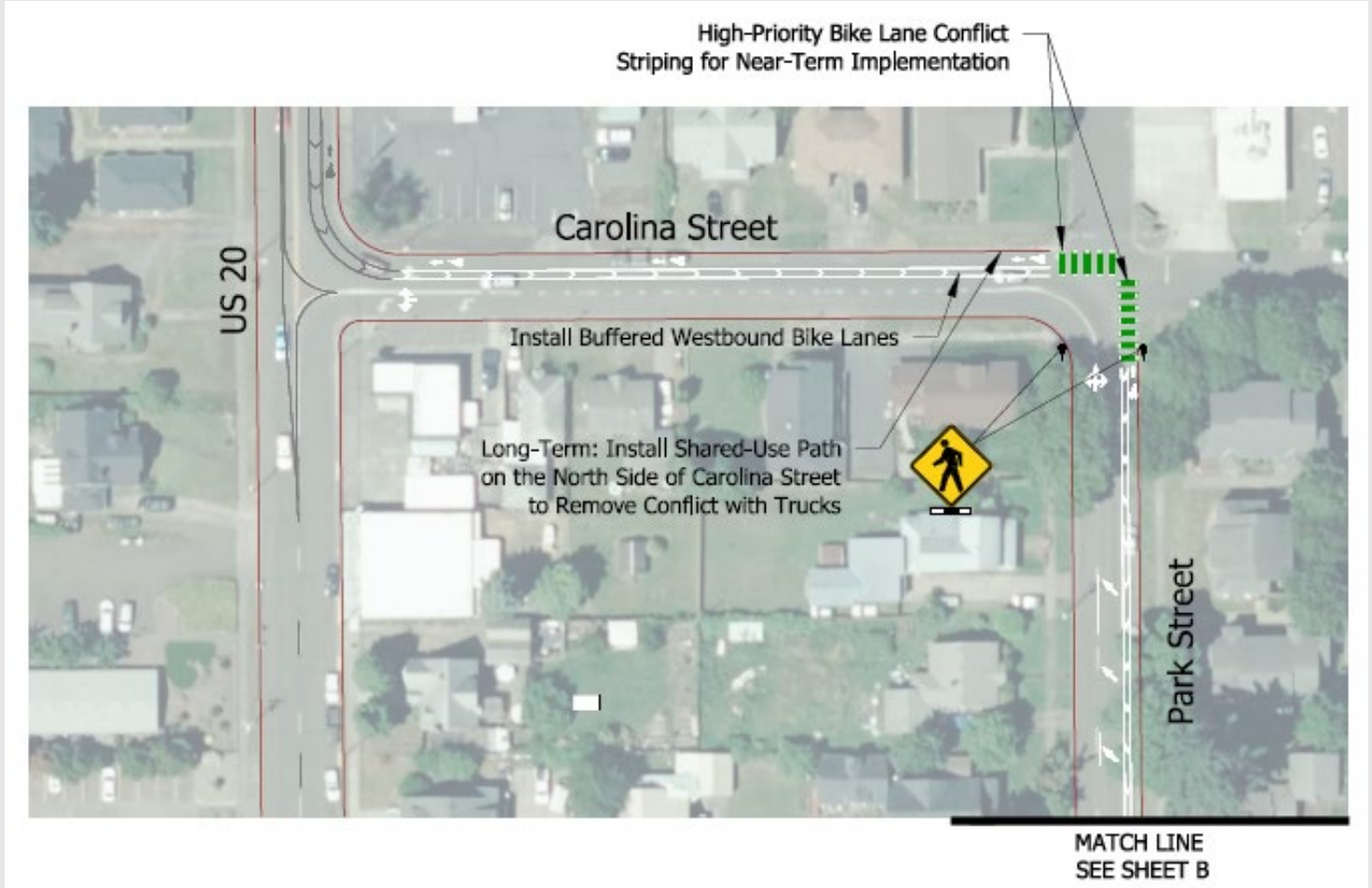
## US 20: Carolina St. to Park St. (Existing & Proposed)



Existing on Park St. and Carolina St  
(with parking and no bike facility)  
~32' curb-to-curb



Proposed on Park St.  
(buffered bike lane and parking removal)



Proposed



# Lebanon UDV

## US 20: Park St. (Existing & Proposed)



### LEGEND

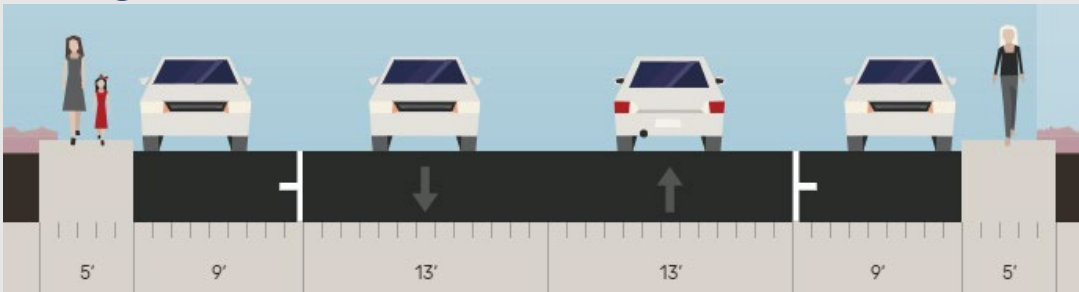
-  Existing Curb
-  Proposed Curb
-  Striping for Connector Streets
-  Bike Lane Conflict Striping
-  Rectangular Rapid Flashing Beacon (RRFB)
-  Tubular Markers (to be added when funding allows for maintenance of protected bicycle facilities)

### NOTES

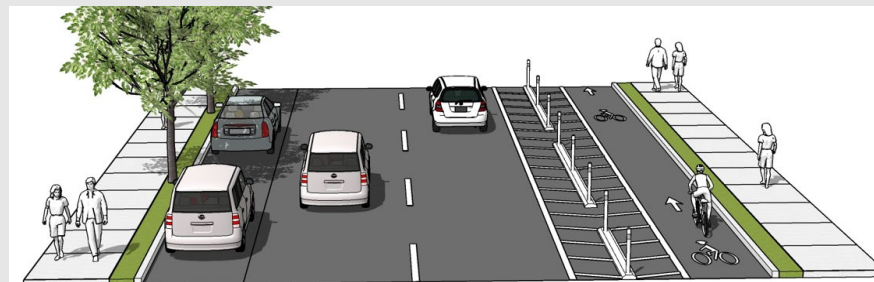
1. Proposed Crosswalk Striping Shown is in Addition to Any Existing Crosswalk Striping
2. Consider Leading Pedestrian Intervals at Signalized Intersections



Existing ~44' curb-to-curb



Proposed (with buffered or protected bicycle facility)



Curb extensions & protected bike facilities reduce vehicle carrying capacity

# Lebanon UDV

## US 20: Park St. (Existing & Proposed)



### LEGEND

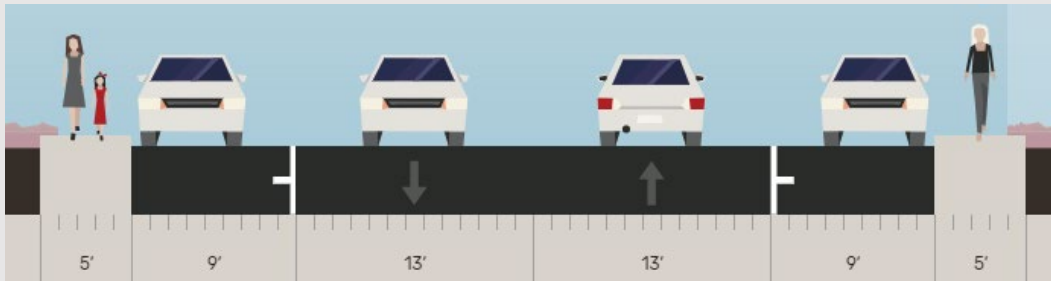
	Existing Curb		Rectangular Rapid Flashing Beacon (RRFB)
	Proposed Curb		Tubular Markers (to be added when funding allows for maintenance of protected bicycle facilities)
	Striping for Connector Streets		
	Bike Lane Conflict Striping		

### NOTES

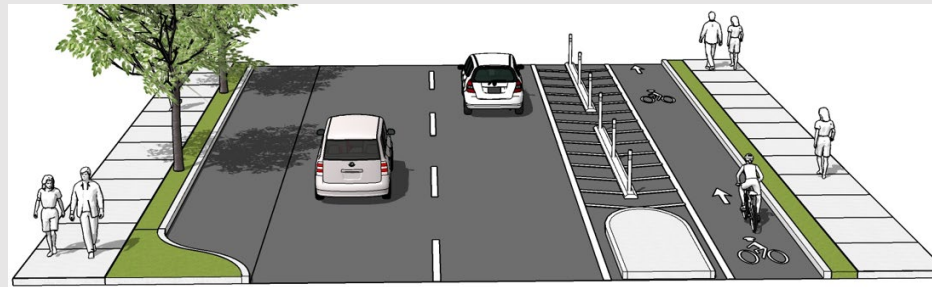
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Existing ~44' curb-to-curb



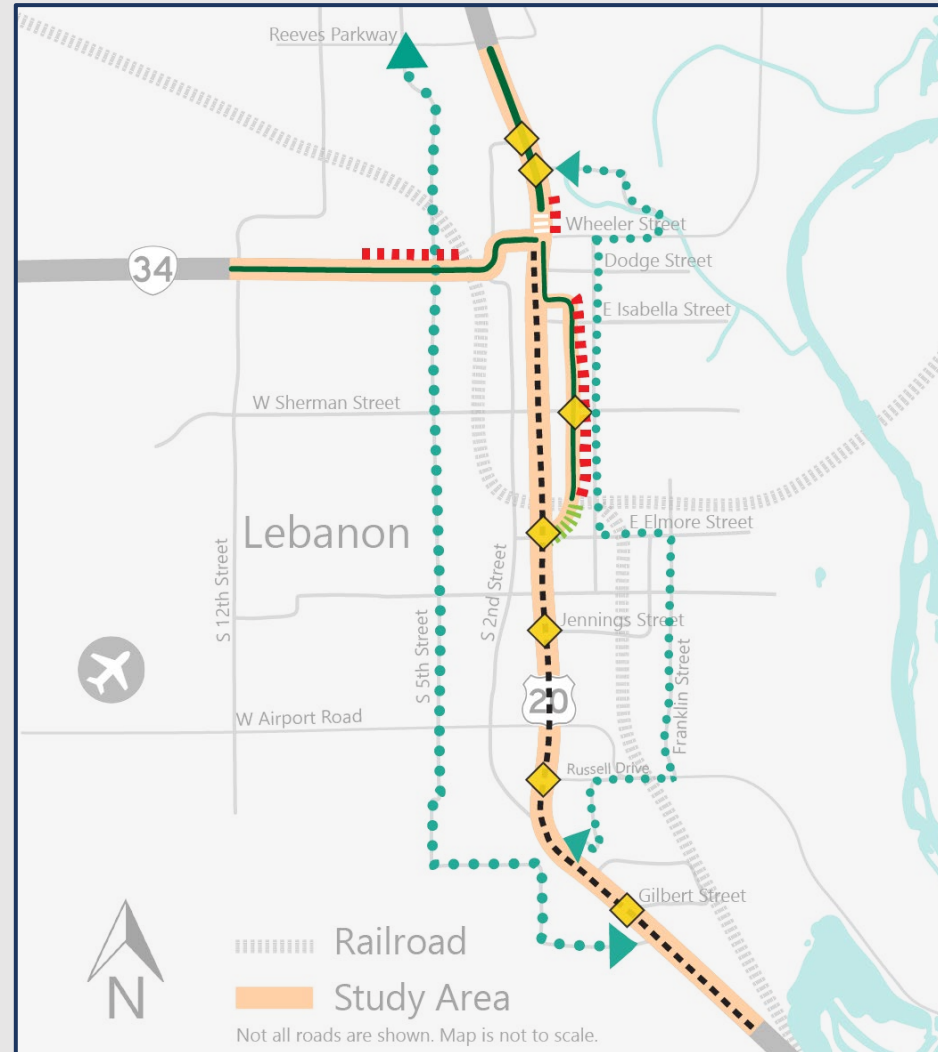
Proposed (w/ curb extensions & buffered or protected bicycle facility)










Curb extensions & protected bike facilities reduce vehicle carrying capacity



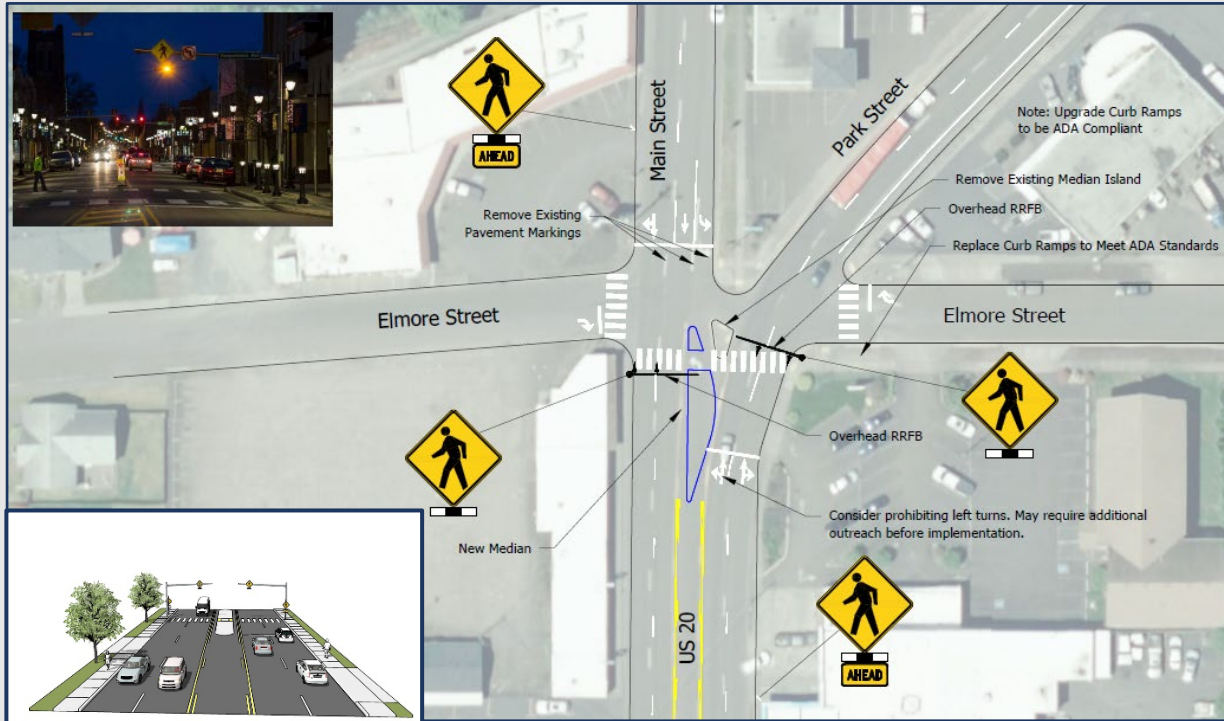
# Lebanon UDV Project Elements



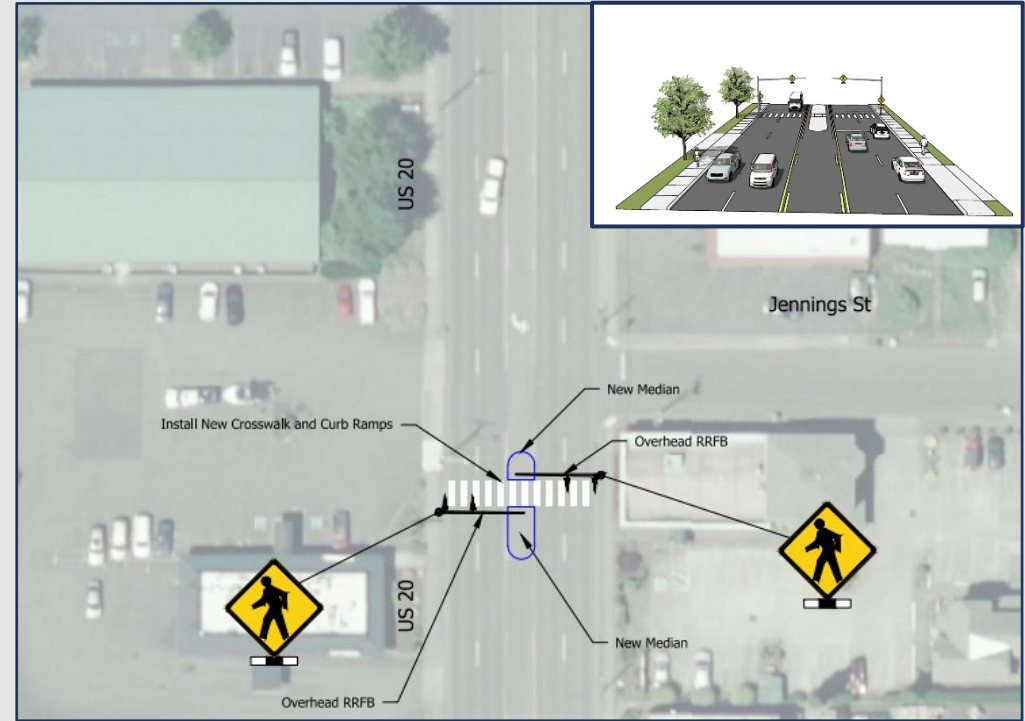
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# Lebanon UDV

## US 20: Elmore & Jennings St Proposed Overhead Enhanced Crossings



Overhead\* RRFB at Elmore Street



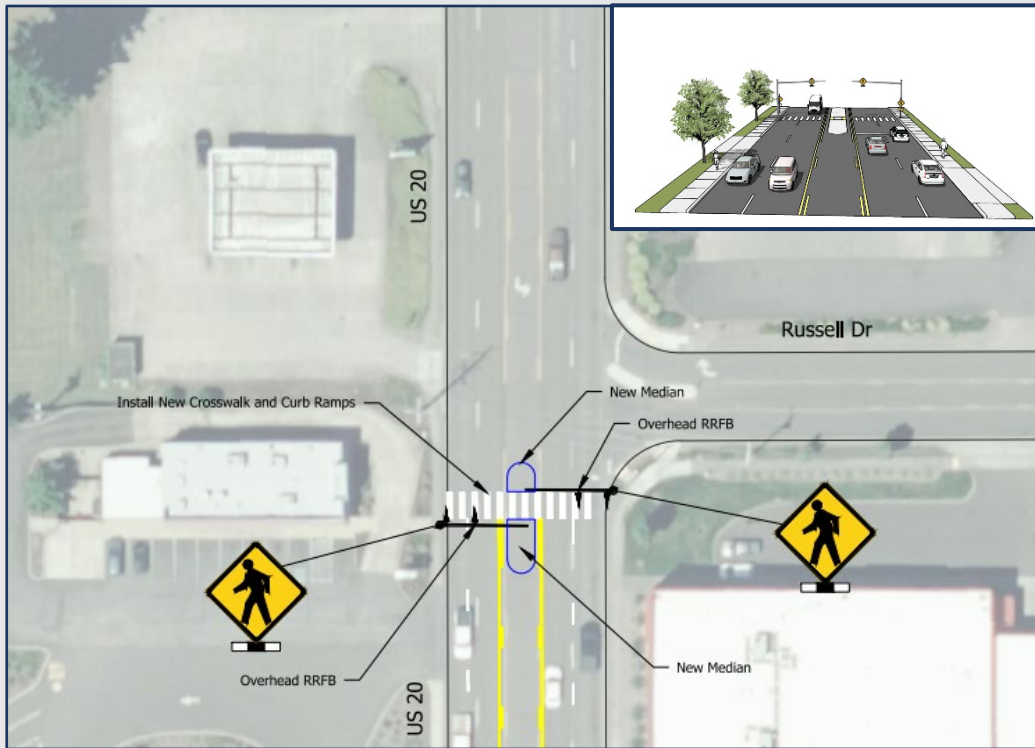
Overhead\* RRFB at Jennings Street



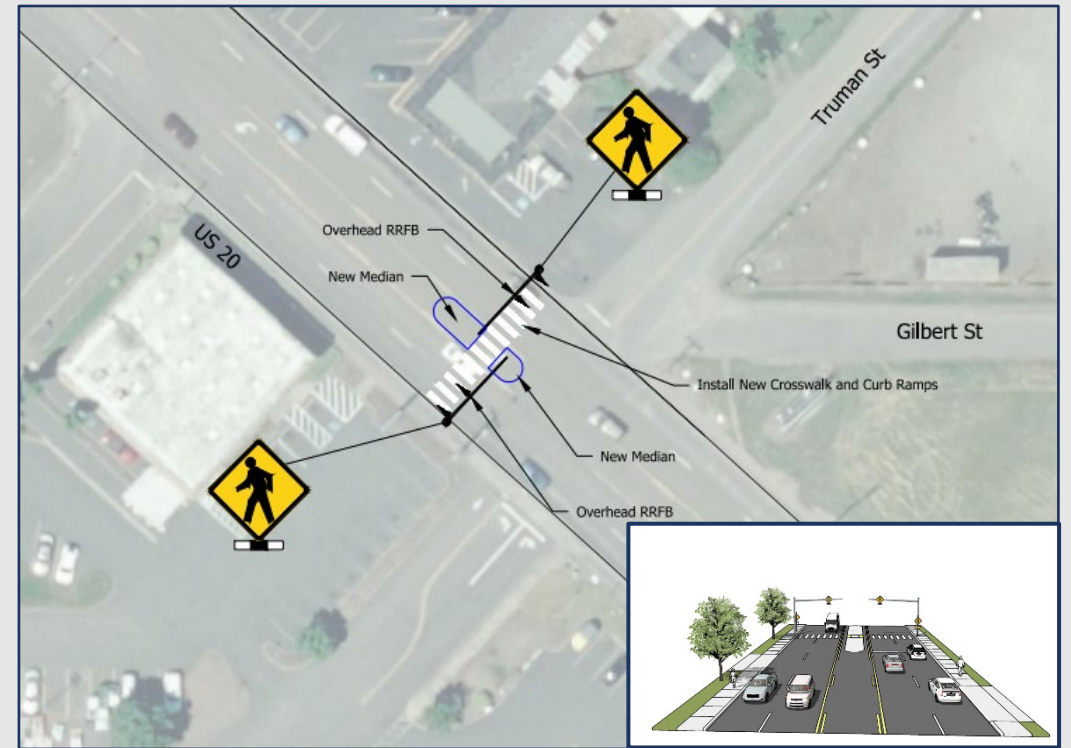
\* Overhead masts create a reduction in vehicle carrying capacity. Vertical clearance would meet or exceed other signal heights in the corridor, typically ~18' in height.

# Lebanon UDV

## US 20: Russell Dr & Truman St Proposed Overhead Enhanced Crossings



Overhead\* RRFB at Russell Drive



Overhead\* RRFB at Truman Street



\* Overhead masts create a reduction in vehicle carrying capacity. Vertical clearance would meet or exceed other signal heights in the corridor, typically ~18' in height.

# Lebanon UDV

## Feedback & Next Steps

Any feedback on potential concepts?

### Next Steps

- Complete preferred concepts
- Seek project funding
- Return to present potential ORS 366.215 impacts

Thank you!

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