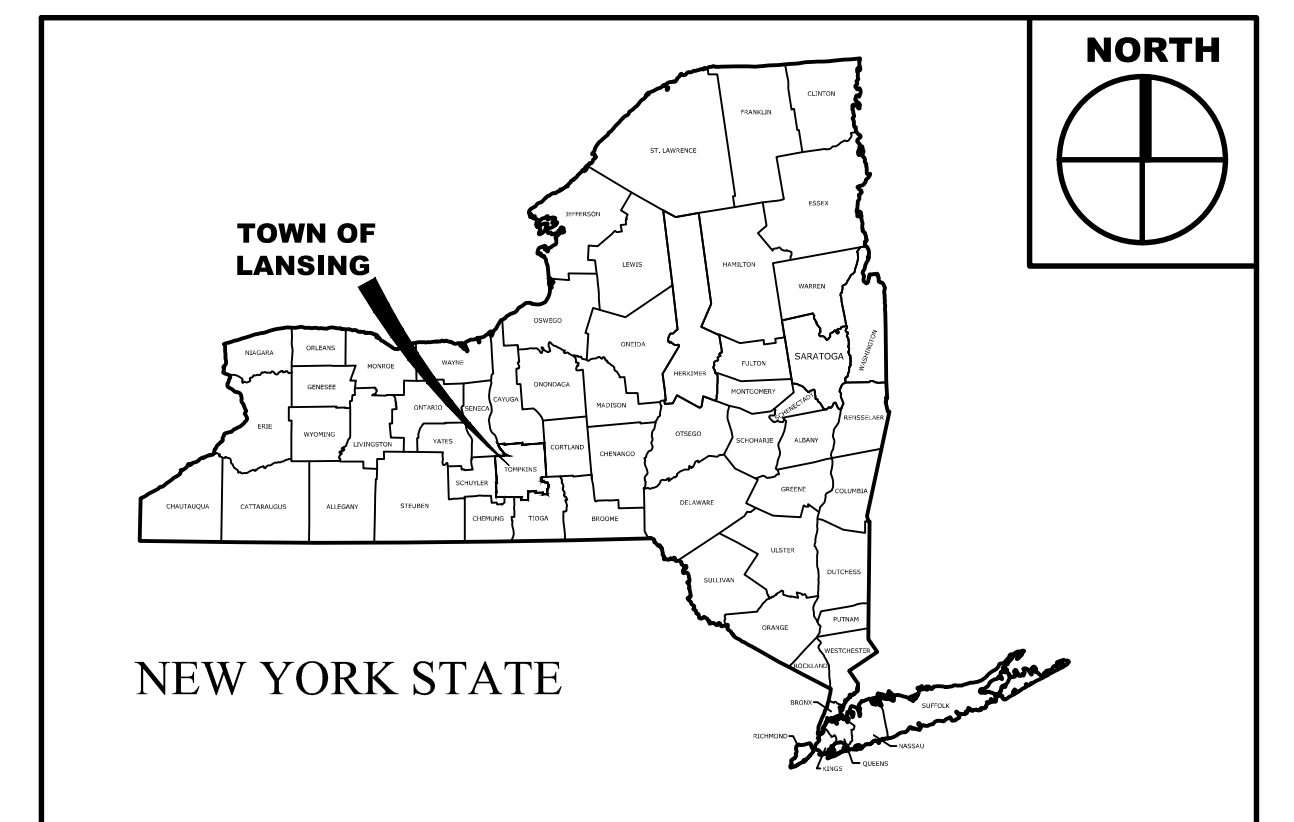


LANSING COMMUNITY SOLAR, LLC GENIE SOLAR ENERGY

LANSINGVILLE ROAD

MARCH 24, 2023



SITE LOCATION MAP

PROJECT SUMMARY

NAME PLATE RATING	6.252 MW DC / 5.0 MW AC
UTILITY TERRITORY	NYSEG
UTILITY ZONE	C
SUBSTATION	N. LANSING
CIRCUIT	4303101
DC/AC RATIO	1.25
ANNUAL PRODUCTION	8,420,030 MWh
PV MODULES	14,050 PRISM SOLAR PST-445W-M72H
INVERTERS	40 CHINT CPS SCH125KTL-DO/US-600
STRINGS PER INVERTER	25
MODULES PER STRING	14-15

DRAWING LIST

Sheet Description	Sheet Title	Sheet Number
G-001	COVER SHEET	01
C-101	EXISTING CONDITIONS NORTH	02
C-102	EXISTING CONDITIONS SOUTH	03
C-103	OVERALL SITE AND ESC PLAN	04
C-104	LANDSCAPING PLAN	05
C-105	SOLAR ARRAY & ESC PLAN	06
C-106	ENTRANCE AND UTILITY POLE PLAN	07
C-501	SITE DETAILS	08
C-502	SITE DETAILS	09
C-503	SITE & EROSION AND SEDIMENT CONTROL DETAILS	10
C-504	EROSION AND SEDIMENT CONTROL DETAILS	11
C-701	TRAFFIC AND MAINTENANCE CONTROL DETAILS	12
C-702	TRAFFIC AND MAINTENANCE CONTROL DETAILS	13
C-703	TRAFFIC AND MAINTENANCE CONTROL DETAILS	14

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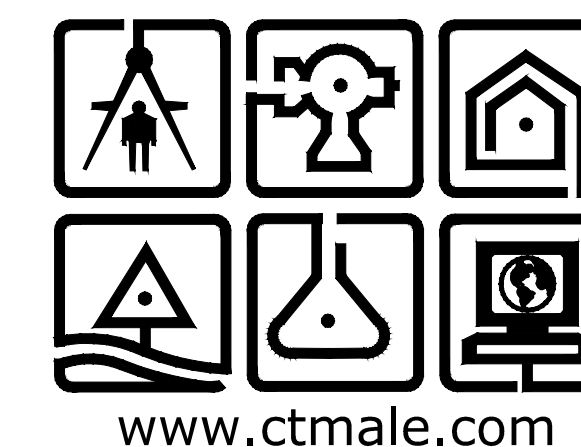
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WARNING: IT IS A VIOLATION OF THIS LAW FOR ANY PERSON, UNLESS HE IS ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE SEAL OF AN ENGINEER OR LAND SURVEYOR IS ALTERED, THE ALTERING ENGINEER OR LAND SURVEYOR SHALL AFFIX TO THE ITEM HIS SEAL AND THE NOTATION "ALTERED BY" FOLLOWED BY HIS SIGNATURE AND THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. PROFESSIONAL ENGINEERING AND LAND SURVEYING - ART. 145, SECTION 7209

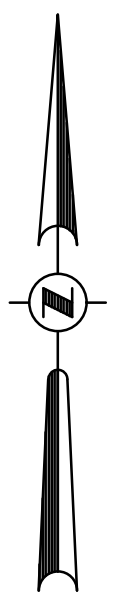
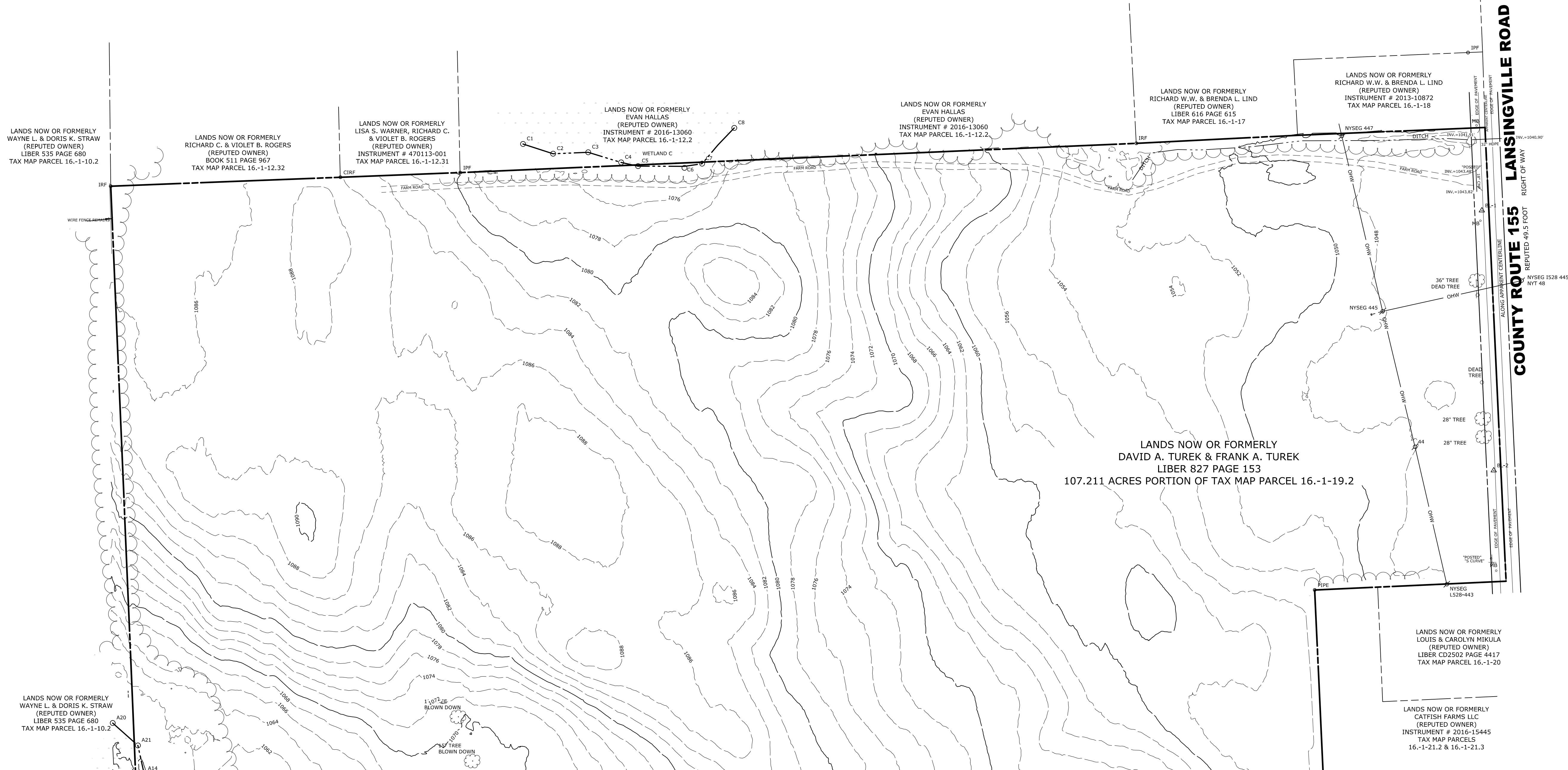
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PROJECT NO. 22.2303
DRAWING NO. 23-0157

G-001
SHEET 01 OF 14



SEE SHEET C-102

SEE SHEET C-102

LEGEND

- IRF IRON ROD FOUND
- IPP IRON PIPE FOUND
- CIRF CAPPED IRON ROD FOUND
- ⋈ UTILITY POLE
- WF WETLAND FLAG
- CMF CORRUGATED METAL PIPE
- HDPE HIGH DENSITY POLYETHYLENE PIPE
- WETLAND AREA

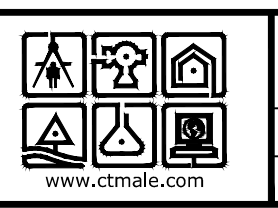
PRELIMINARY

EXISTING CONDITIONS NORTH

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C-101
SHEET 02 OF 14
DWG. NO: 23-0157

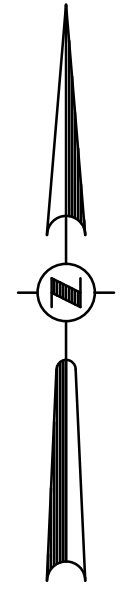
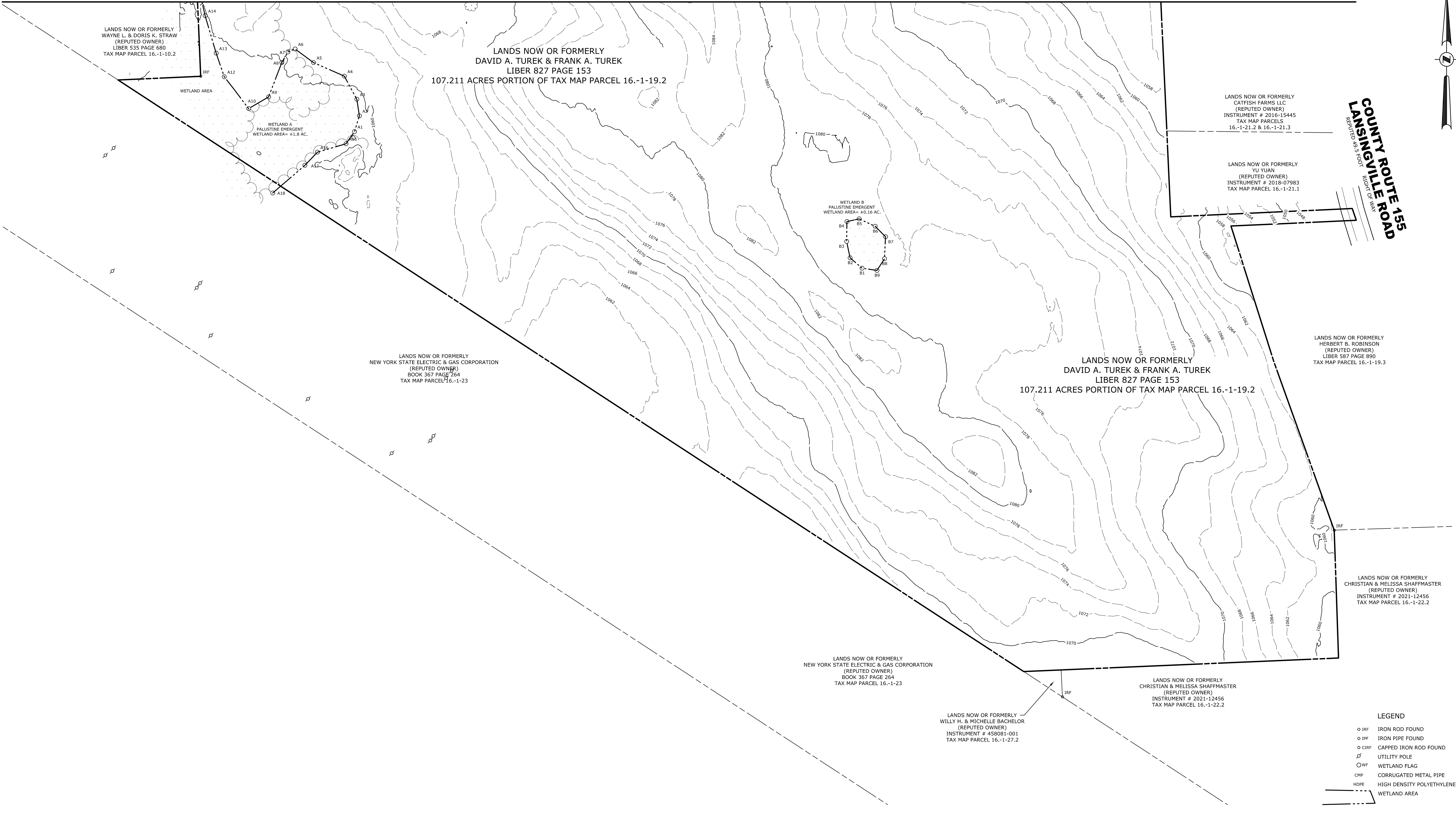
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SEE SHEET C-101

SEE SHEET C-101



COUNTY ROUTE 155
 49.3 FOOT RIGHT OF WAY

- LEGEND**
- IRF IRON ROD FOUND
 - IFF IRON PIPE FOUND
 - CIRF CAPPED IRON ROD FOUND
 - ⚡ UTILITY POLE
 - WF WETLAND FLAG
 - CMF CORRUGATED METAL PIPE
 - HDPE HIGH DENSITY POLYETHYLENE PIPE
 - WETLAND AREA

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EXISTING CONDITIONS SOUTH

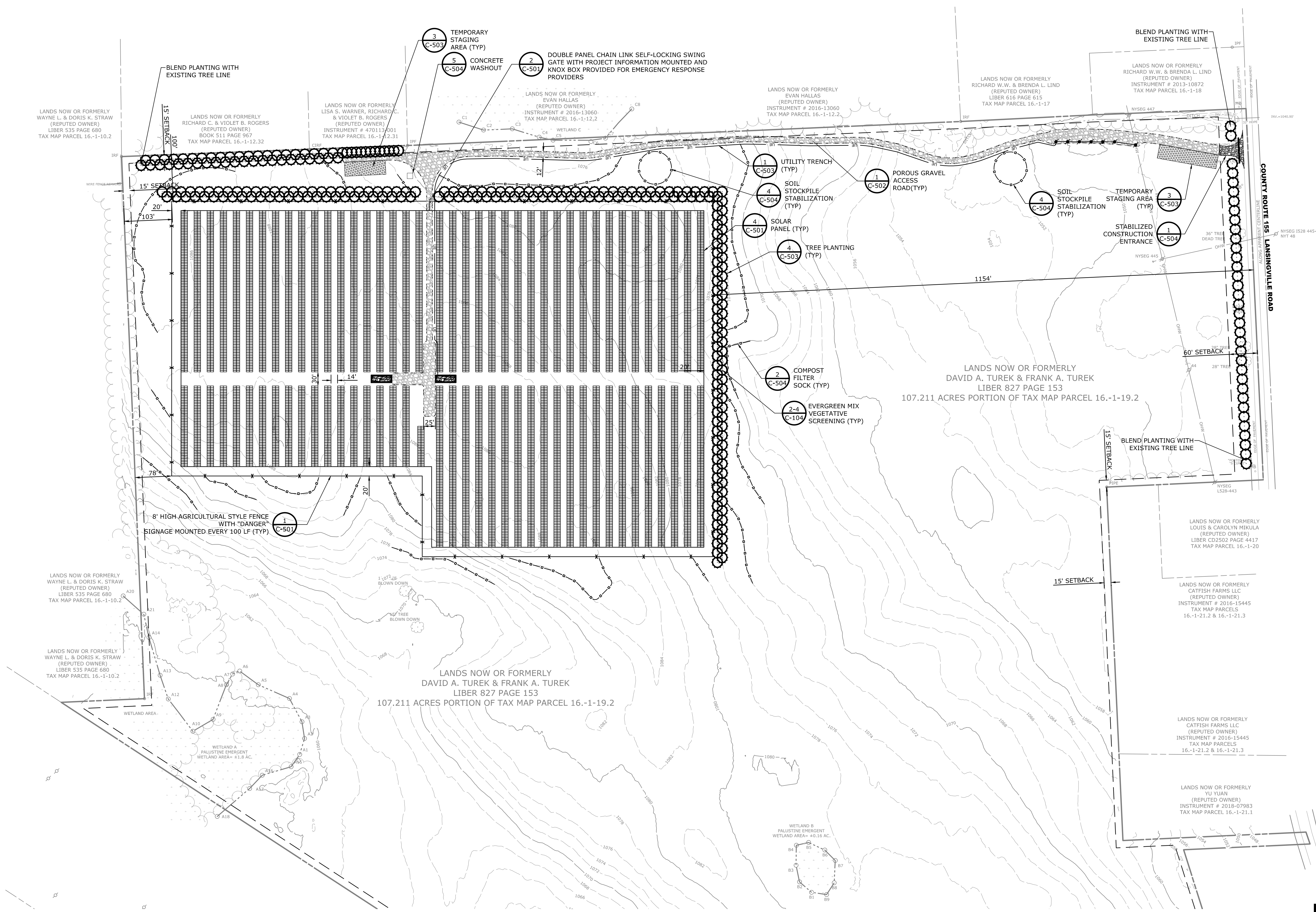
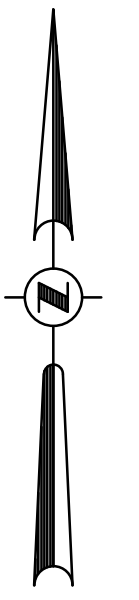
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GENIE SOLAR ENERGY

TOWN OF LANSING TOMPKINS COUNTY, NEW YORK

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 SHEET 03 OF 14
 DWG. NO: 23-0157

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SITE PLAN NOTES:

ZONING DISTRICT: (RA) RESIDENTIAL AGRICULTURE
 TAX MAP I.D.: 16-1-19.2
 TOTAL PARCEL SIZE: 107.2± AC
 PROPOSED FENCE AREA: 18.0± AC (16.8% OF PARCEL)
 PROPOSED ARRAY ENVELOPE: 16.3± AC (15.2% OF PARCEL)
 PROPOSED LOT COVERAGE BY SOLAR PANELS: 7.7± AC (7.2% OF PARCEL)
 PROPOSED AREA OF DISTURBANCE: 22.67± AC
 NEW GRAVEL ACCESS ROAD: 2,575± LF, 40,771± SQ. FT.
 TOTAL TREE CLEARING: 0.0± AC

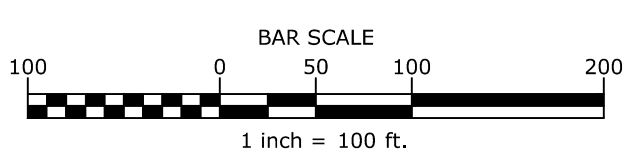
ZONING ANALYSIS:

METRIC	RA ZONING DISTRICT	PROPOSED
MAX PANEL HEIGHT	18 FT.	18 MAX. FT.
MIN. FRONT YARD SETBACK	60 FT.	1,154± FT.
MIN. SIDE YARD SETBACK	15 FT.	100± FT.
MIN. REAR YARD SETBACK	15 FT.	78± FT.
MAX LOT COVERAGE	NONE	7.2± %

LEGEND:

	SOLAR PANEL
	UTILITY POLE
	PERIMETER FENCE
	PROPOSED TREE LINE
	UNDERGROUND ELECTRIC LINE
	DELINEATED WETLAND
	PERVIOUS GRAVEL ROAD
	COMPOST FILTER SOCK
	PROPOSED TREE
	TEMPORARY STAGING AREA
	TOPSOIL STOCKPILE AREA
	CONCRETE EQUIPMENT PAD

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OVERALL SITE AND ESC PLAN

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2 NORWAY SPRUCE
C-104 SCALE: NONE
CROSS REFERENCE: NONE



3 WHITE SPRUCE
C-104 SCALE: NONE
CROSS REFERENCE: NONE



4 EASTERN RED CEDAR
C-104 SCALE: NONE
CROSS REFERENCE: NONE

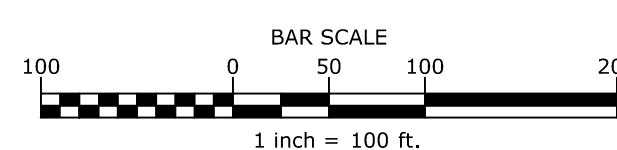
LEGEND:

- SOLAR PANEL
- UTILITY POLE
- PERIMETER FENCE
- PROPOSED TREE LINE
- UNDERGROUND ELECTRIC LINE
- DELINEATED WETLAND
- PERVIOUS GRAVEL ROAD
- PROPOSED TREE
- CONCRETE EQUIPMENT PAD

PLANTING SCHEDULE:

TREE TYPE/MIX	ESTIMATED QTY.	TREE TYPE	SPACING	PLANTING HEIGHT
EVERGREEN MIX	290±	WHITE SPRUCE NORWAY SPRUCE EASTERN RED CEDAR	10' O.C.	5 FT. TO 8 FT.

NOTES:
AN EVERGREEN MIX CONSISTING OF NORWAY SPRUCE, WHITE SPRUCE, AND EASTERN RED CEDAR WILL BE INSTALLED, AS TREES ARE AVAILABLE PRIOR TO CONSTRUCTION.



1 LANDSCAPING PLAN
C-104 SCALE: 1" = 100'
CROSS REFERENCE: NONE

PRELIMINARY

LANDSCAPING PLAN

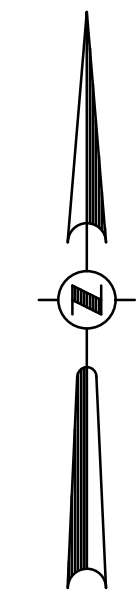
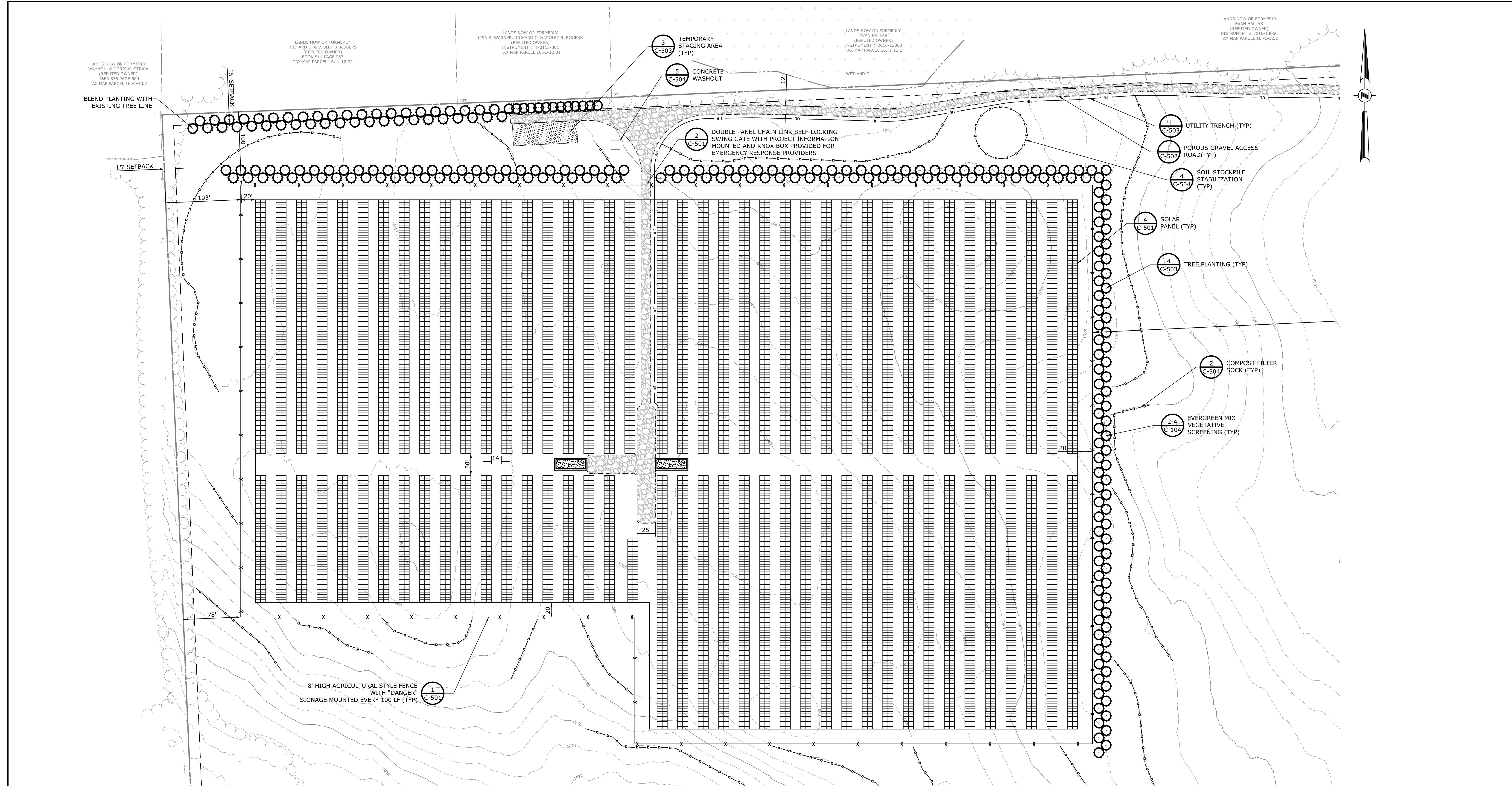
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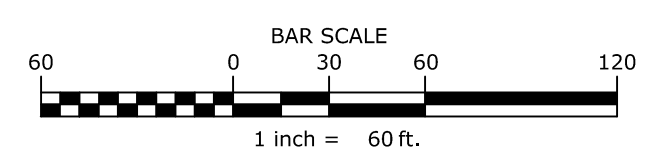
- SOLAR PANEL
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- PERIMETER FENCE
- PROPOSED TREE LINE
- UNDERGROUND ELECTRIC LINE
- DELINEATED WETLAND
- PERVIOUS GRAVEL ROAD
- COMPOST FILTER SOCK
- PROPOSED TREE
- TEMPORARY STAGING AREA
- TOPSOIL STOCKPILE AREA
- CONCRETE EQUIPMENT PAD

SITE PLAN NOTES:

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ZONING ANALYSIS:

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MIN. SIDE YARD SETBACK	15 FT.	100± FT.
MIN. REAR YARD SETBACK	15 FT.	78± FT.
MAX LOT COVERAGE	NONE	7.2± %



PRELIMINARY

SOLAR ARRAY & ESC PLAN

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GENIE SOLAR ENERGY

TOWN OF LANSING TOMPKINS COUNTY, NEW YORK

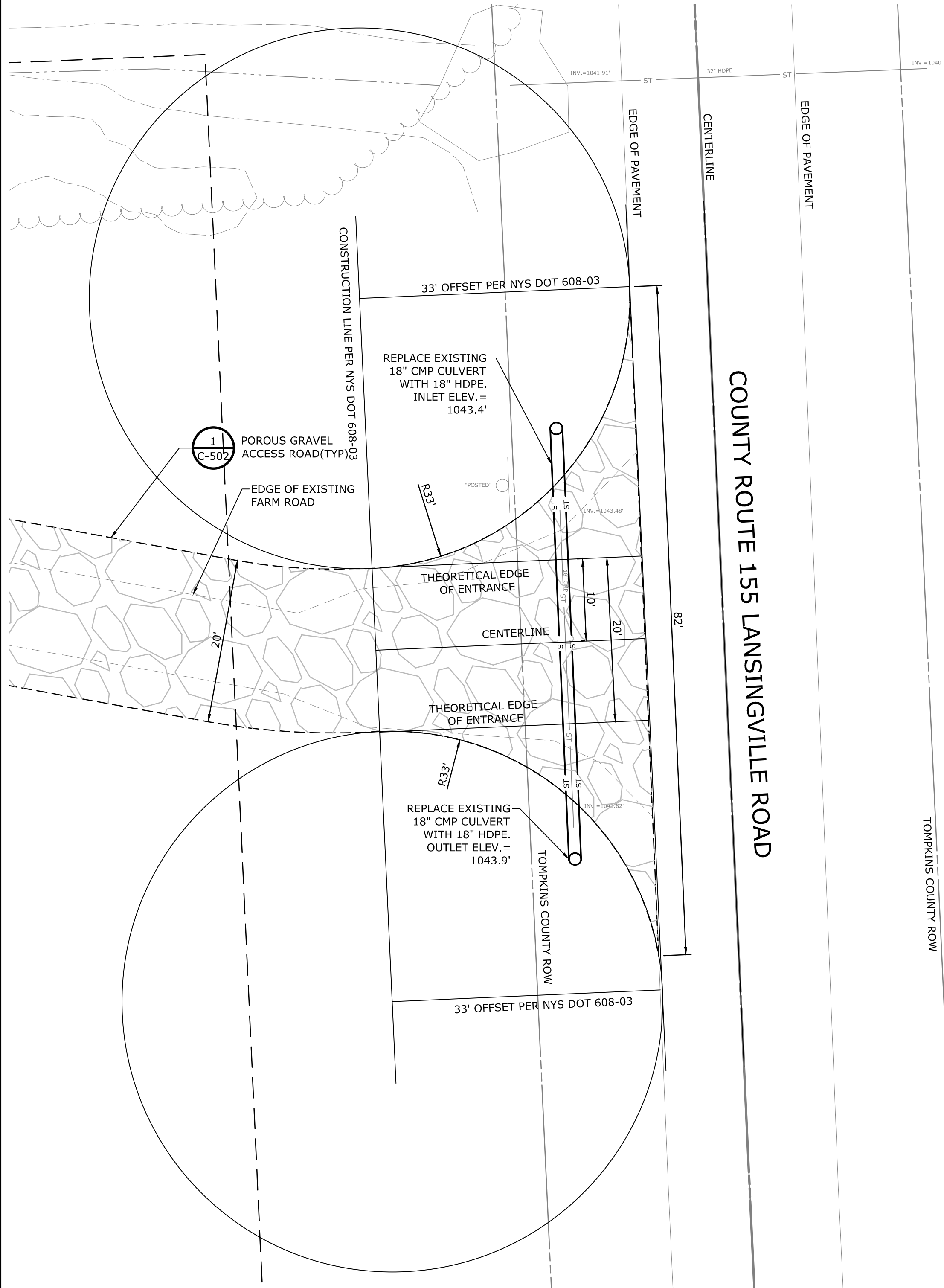
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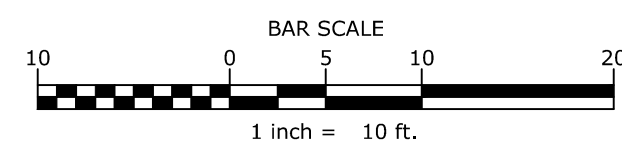
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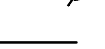






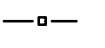


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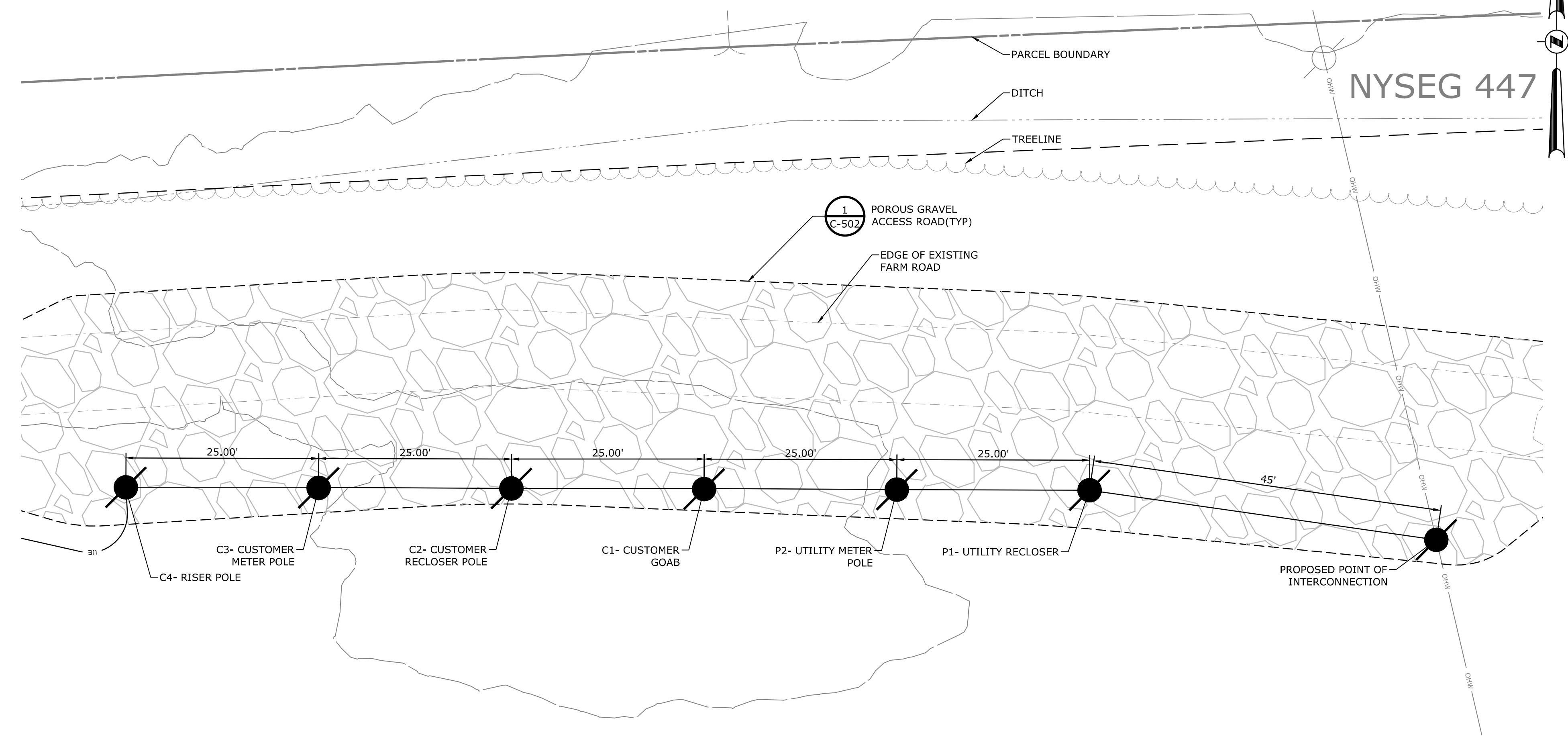
1 ENTRANCE PLAN
C-106
SCALE: 1" = 10'
CROSS REFERENCE: NONE



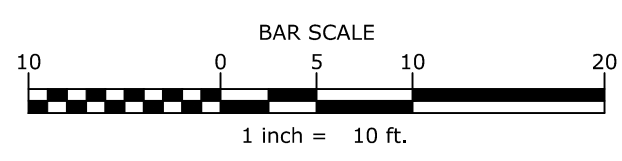
LEGEND:

-  SOLAR PANEL
-  UTILITY POLE
-  PERIMETER FENCE
-  PROPOSED TREE LINE
-  UNDERGROUND ELECTRIC LINE
-  DELINEATED WETLAND
-  PERVIOUS GRAVEL ROAD
-  COMPOST FILTER SOCK
-  PROPOSED TREE
-  TEMPORARY STAGING AREA
-  TOPSOIL STOCKPILE AREA
-  CONCRETE EQUIPMENT PAD

SITE DISTANCE LEFT @42"= ±830'
 SITE DISTANCE RIGHT @42"= ±1,285'
 SITE DISTANCE LEFT @24"= ±830'
 SITE DISTANCE RIGHT @24"= ±1,285'
 JERRY SMITH ROAD TO PROPOSED ENTRANCE= ±660'
 EXISTING DRIVEWAY TO PROPOSED ENTRANCE WEST SIDE OF LANSINGVILLE ROAD= ±185'
 EXISTING DRIVEWAY TO PROPOSED ENTRANCE EAST SIDE OF LANSINGVILLE ROAD= ±60'



2 UTILITY POLE PLAN
C-106
SCALE: 1" = 10'
CROSS REFERENCE: NONE



PRELIMINARY

ENTRANCE AND UTILITY POLE PLAN

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GENIE SOLAR ENERGY

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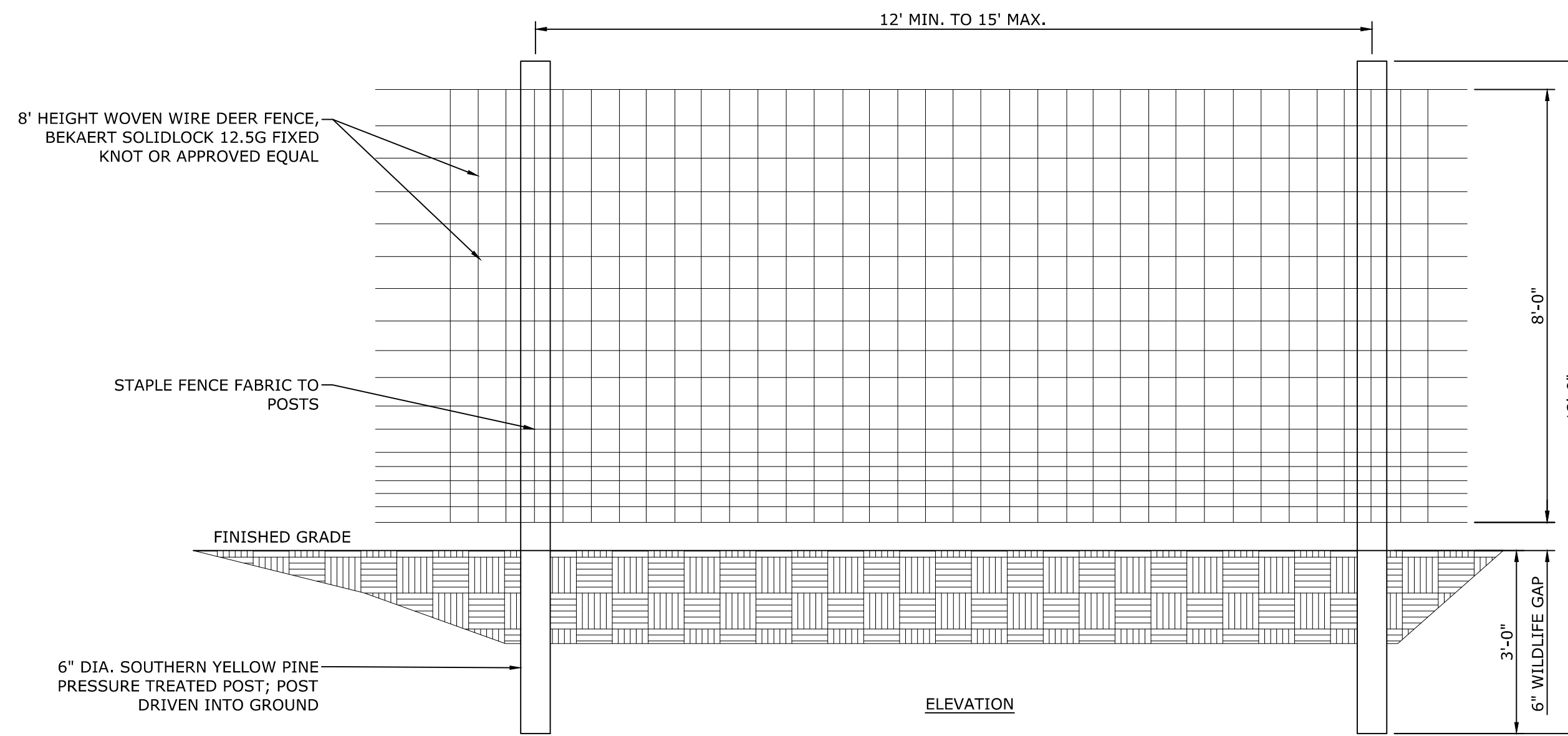
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GENERAL NOTES:

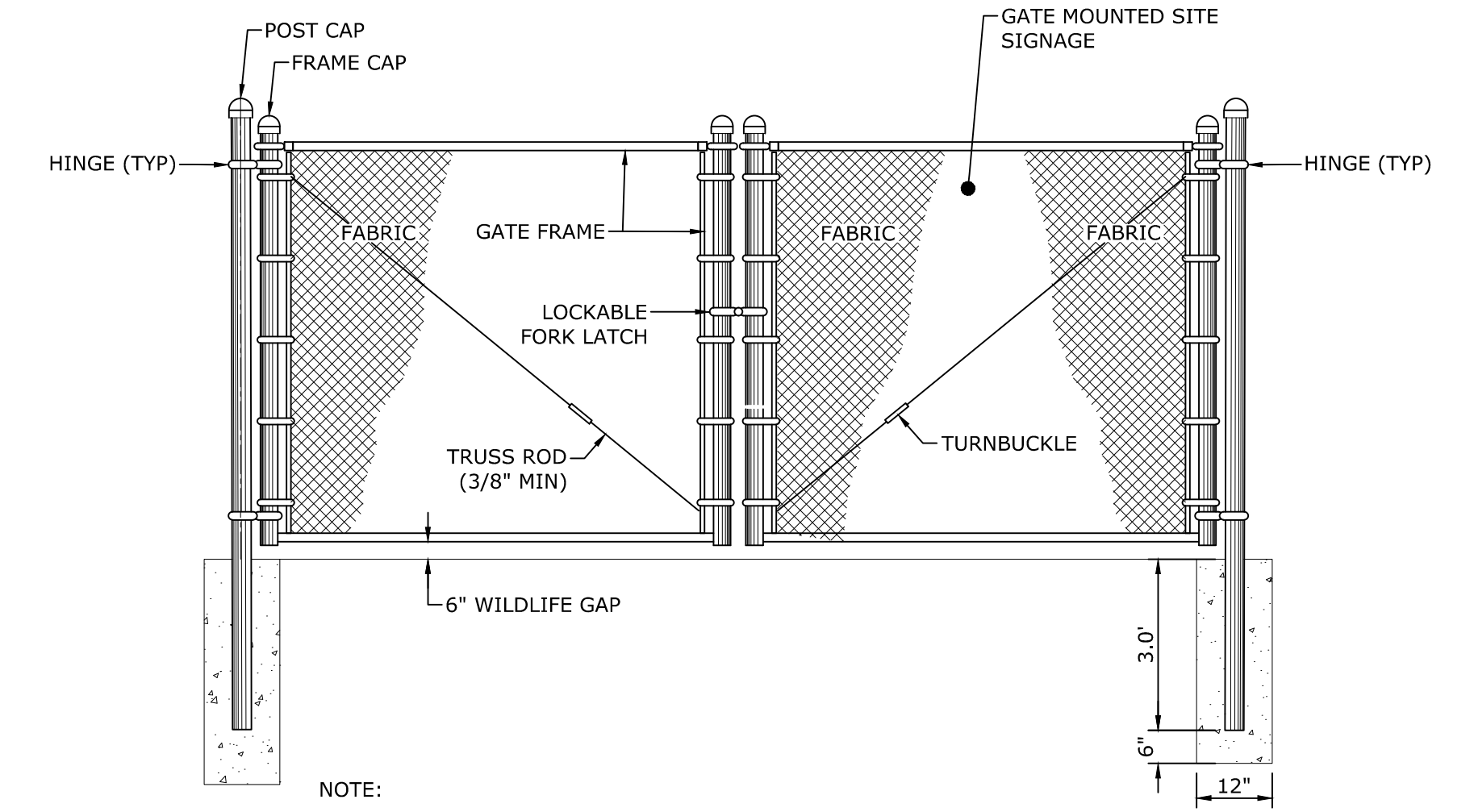
- BEFORE UNDERTAKING ANY CONSTRUCTION ACTIVITY, ALL CONTRACTORS AND SUBCONTRACTORS INVOLVED WITH SITE WORK THAT INVOLVES PHYSICAL GROUND DISTURBANCE ON THE PROJECT SITE SHALL SIGN AND DATE A COPY OF THE CERTIFICATION STATEMENT, WHICH IS LOCATED IN THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP), PREPARED FOR THIS PROJECT.
- ALL EROSION AND SEDIMENT CONTROL PRACTICES SHALL CONFORM TO THE "NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL" (2016 ISSUE) AND ANY ADDENDA THERETO.
- THE SEDIMENT CONTROL MEASURES DETAILED IN THESE PLANS SHALL BE IN PLACE PRIOR TO THE START OF EACH CONSTRUCTION PHASE. ONCE CONSTRUCTED, ALL MEASURES SHALL BE PROPERLY MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD, AND THEN REMOVED FROM THE SITE ONCE THE SITE IS STABILIZED.
- AFTER THE START OF CONSTRUCTION, SITE SWPPP INSPECTIONS SHALL BE CONDUCTED AT LEAST ONCE EVERY (7) CALENDAR DAYS.
- BASED ON THE WEEKLY SITE SWPPP INSPECTIONS, THE EROSION AND SEDIMENT CONTROL MEASURES IDENTIFIED IN THE SWPPP MAY BE REVISED AS SITE CONDITIONS WARRANT. THE CONTRACTOR SHALL IMPLEMENT THESE CHANGES AS SOON AS PRACTICABLE. AND KEEP AND UPDATED SWPPP ONSITE.
- THE EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED AND MAINTAINED BY THE CONTRACTOR UNTIL THE FINAL SURFACE TREATMENT HAS BEEN INSTALLED AND VEGETATED AREAS HAVE ESTABLISHED 80% COVERAGE. AFTER THE VEGETATED AREAS HAVE BEEN STABILIZED WITH AT LEAST 80% VEGETATIVE COVER, AS DETERMINED BY THE ENGINEER, THE PROJECT SPONSOR SHALL ASSUME RESPONSIBILITY FOR MAINTAINING THE EROSION AND SEDIMENT CONTROL SYSTEM(S).
- THE EROSION AND SEDIMENT CONTROL MEASURES SHOWN ON THE CONTRACT DOCUMENTS WILL NEED TO BE SUPPLEMENTED WITH INTERIM MEASURES PRIOR TO ACHIEVING FINAL GRADES. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN INTERIM EROSION AND SEDIMENT CONTROL MEASURES AS NEEDED TO CONTROL EROSION AND SEDIMENTATION THROUGHOUT THE DURATION OF CONSTRUCTION. THE DETAILS AND EXTENT OF THESE MEASURES ARE HIGHLY DEPENDENT ON THE CONTRACTORS MEANS AND METHODS AND THEREFORE NOT DETAILED ON THESE PLANS. THE COSTS ASSOCIATED WITH INSTALLING AND MAINTAINING THESE INTERIM MEASURES SHALL BE INCLUDED IN THE CONTRACTORS BID.
- CONSTRUCTION ACTIVITIES SHALL PROCEED IN ACCORDANCE WITH THE CONSTRUCTION SEQUENCING NOTES.
- OUTSIDE THE GROWING SEASON, OTHER METHODS OF SOIL STABILIZATION (SUCH AS THE USE OF JUTE MESH, EXCELSIOR MATTING, OR TACKIFIER) SHALL BE USED UNTIL SUCH TIME AS VEGETATIVE COVER CAN BE ESTABLISHED.
- EXISTING VEGETATION SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE. SITE WORK ACTIVITIES SHALL BE PLANNED TO MINIMIZE THE AREA AND DURATION OF SOIL DISTURBANCE. REMOVAL OF WOODY VEGETATION SHALL BE KEPT TO THE MINIMUM EXTENT PRACTICABLE.

GRADING PLAN NOTES:

- THE CONTRACTOR SHALL NOTIFY ALL UTILITY OWNERS HAVING UNDERGROUND UTILITIES ON-SITE OR IN THE RIGHT-OF-WAY THAT MAY BE AFFECTED BY THE WORK, PRIOR TO EXCAVATING AND SITE GRADING.
- SITE GRADING SHALL NOT PROCEED UNTIL APPROPRIATE EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DEWATERING AND THE MAINTENANCE OF SURFACE DRAINAGE PATTERNS DURING THE COURSE OF THE WORK.
- EARTHWORK SHALL BE SMOOTHLY AND EVENLY BLENDED INTO EXISTING CONDITIONS. IN THE EVENT THAT WORK OUTSIDE OF DESIGNATED LIMITS IS NECESSARY, THE PERMISSION OF THE PROPERTY OWNER MUST FIRST BE OBTAINED BEFORE COMMENCING SUCH WORK.
- BOX TREES, SHRUBS, AND HEDGES TO REMAIN BEFORE PLACING EARTH AGAINST OR NEAR THEM. SHRUBS AND HEDGES, WHICH MUST BE REMOVED TO PERFORM THE WORK, SHALL BE HEALED IN AND REPLANTED IN AS GOOD A CONDITION AS THEY WERE BEFORE THEIR REMOVAL. ANY DAMAGED TREES, SHRUBS AND/OR HEDGES INTENDED TO REMAIN SHALL BE REPLACED IN SIMILAR SIZE AND SPECIES AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL PROVIDE TEMPORARY FENCING, BARRICADES, OR OTHER SUITABLE PROTECTION LOCATED OUTSIDE THE DRIP LINE (OUTER PERIMETER OF BRANCHES) TO PROTECT TREES AND OTHER PLANTS INTENDED TO REMAIN FROM DAMAGE.

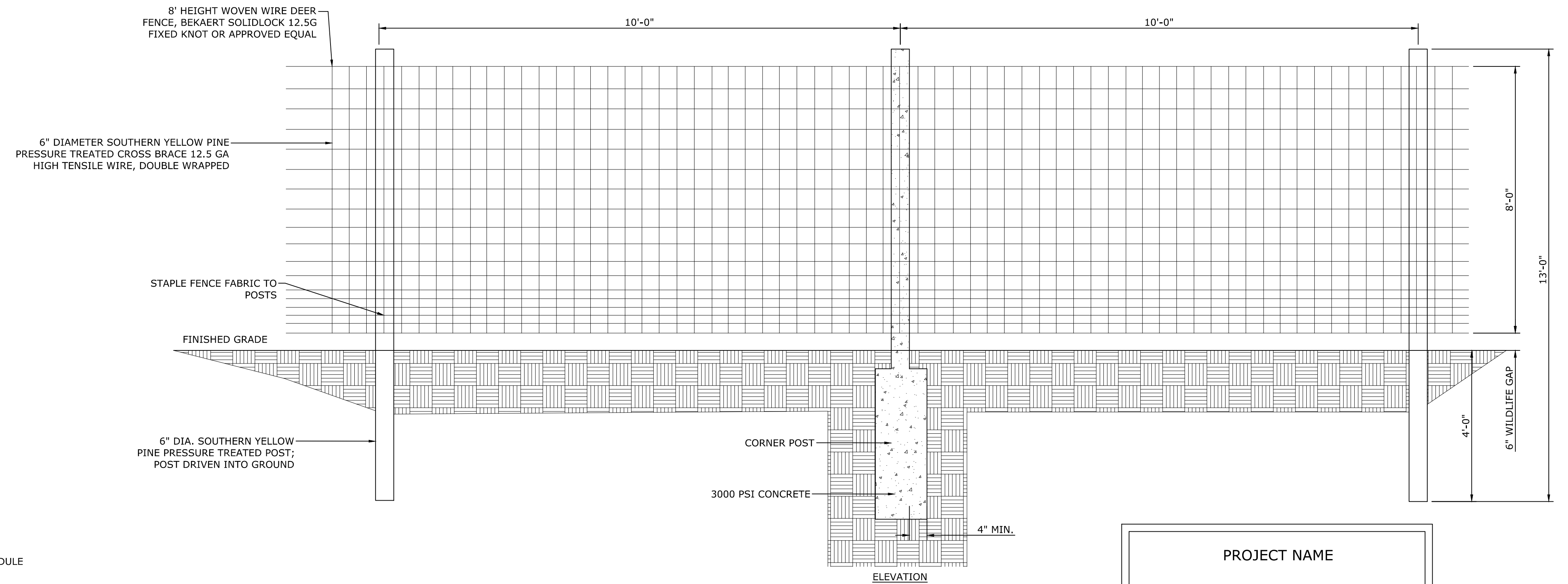


1 PERIMETER FENCE
SCALE: NONE
CROSS REFERENCE: NONE



2 DOUBLE PANEL SWING GATE
SCALE: NONE
CROSS REFERENCE: NONE

NOTE:
1. PERIMETER GATE SECURED FOR RESTRICTED ACCESS MUST BE EQUIPPED WITH AN EMERGENCY SERVICES RAPID ENTRY SYSTEM. PROVIDE KNOX EXTERIOR PADLOCK. KNOX PADLOCK SHALL BE DAISY CHAINED WITH OWNER PROVIDED PADLOCK(S)



3 FENCE CORNER POST
SCALE: NONE
CROSS REFERENCE: NONE

PROJECT NAME

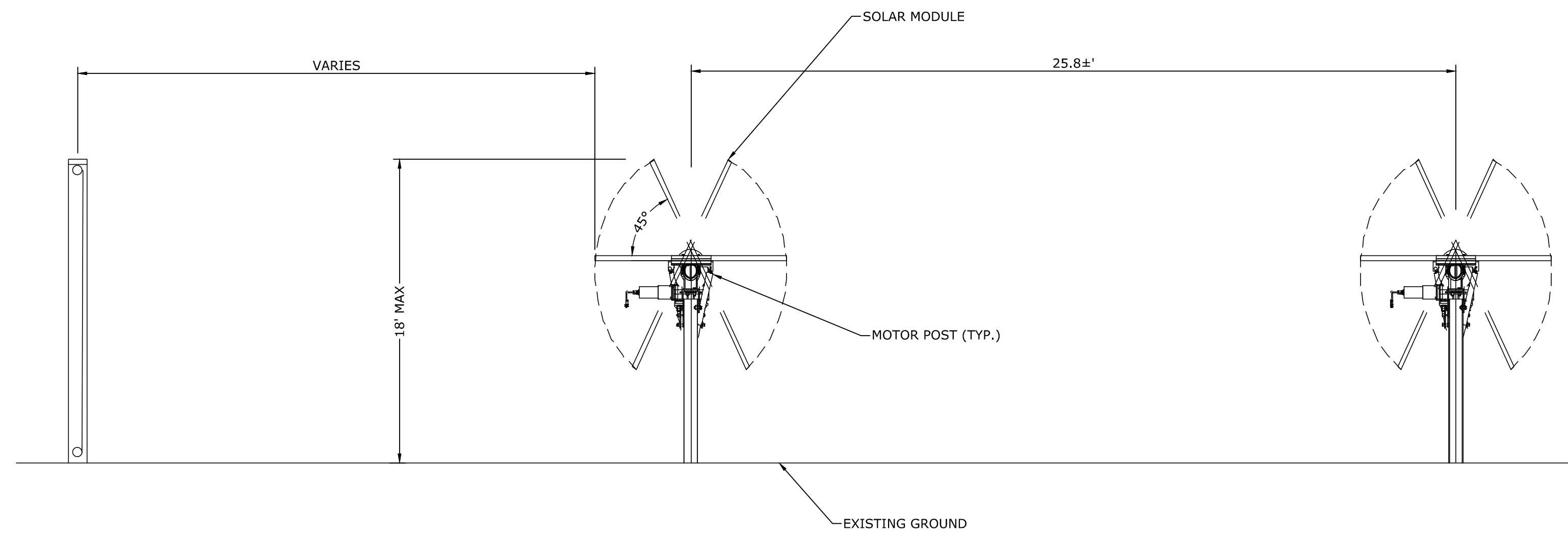
PROJECT OWNER
OWNER PHONE NUMBER

UTILITY NAME
UTILITY PHONE NUMBER

PROJECT VOLTAGE
NO TRESPASSING

- NOTES:
- SIGN TO BE STAINLESS STEEL OR APPROVED EQUIVALENT AND INSTALLED ON APPROPRIATE METAL POSTS.
 - SIGN SHALL NOT EXCEED 8 SQUARE FEET.
 - SIGN SHALL BE OF WHITE BACKGROUND WITH BLACK OR DARK LETTERING WITH A NEAT LINE BORDER.

6 PROJECT SIGNAGE
SCALE: NONE
CROSS REFERENCE: NONE



4 TYPICAL ARRAY SECTION
SCALE: NONE
CROSS REFERENCE: NONE



5 SIGNAGE DETAIL
SCALE: NONE
CROSS REFERENCE: NONE

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					DESIGNER: MLS
					DRAFTED : MLS
					CHECKED : OKS
					PROJ. NO : 22.2303
					SCALE : AS NOTED
					DATE : MARCH 24, 2023

SITE DETAILS

LANSING COMMUNITY SOLAR, LLC
GENIE SOLAR ENERGY

TOWN OF LANSING TOMPKINS COUNTY, NEW YORK

C.T. MALE ASSOCIATES
Engineering, Surveying, Architecture, Landscape Architecture & Geology, D.P.C.
50 CENTURY HILL DRIVE, LATHAM, NY 518.786.7400
COBLESKILL, NY • GLENS FALLS, NY • POUGHKEEPSIE, NY
JOHNSTOWN, NY • RED HOOK, NY • SYRACUSE, NY

C-501
SHEET 08 OF 14
DWG. NO: 23-0157

CAD DWG. FILE NAME: K:\Projects\222303\Civil\00_Drawings and Maps\C-501.dwg

NOTES FOR PERVIOUS ACCESS ROADS:

- USE OF THIS DETAIL/CRITERION IS LIMITED TO ACCESS ROADS USED ON AN OCCASIONAL BASIS ONLY (I.E. PROVIDE ACCESS FOR MOWING, EQUIPMENT REPAIR OR MAINTENANCE, ETC.).
- LIMITED USE PERVIOUS ACCESS ROAD IS LIMITED TO LOW IMPACT IRREGULAR MAINTENANCE ACCESS ASSOCIATED WITH RENEWABLE ENERGY PROJECTS IN NEW YORK STATE.
- REMOVE STUMPS, ROCKS AND DEBRIS AS NECESSARY. FILL VOIDS TO MATCH EXISTING NATIVE SOILS AND COMPACTION LEVEL.
- REMOVED TOPSOIL MAY BE SPREAD IN ADJACENT AREAS AS DIRECTED BY THE PROJECT ENGINEER. COMPACT TO THE DEGREE OF THE NATIVE INSITU SOIL. DO NOT PLACE IN AN AREA THAT IMPEDES STORMWATER DRAINAGE.
- GRADE ROADWAY, WHERE NECESSARY, TO NATIVE SOIL AND DESIRED ELEVATION. MINOR GRADING FOR CROSS SLOPE CUT AND FILL MAY BE REQUIRED.
- REMOVE REFUSE SOILS AS DIRECTED BY THE PROJECT ENGINEER. DO NOT PLACE IN AN AREA THAT IMPEDES STORMWATER DRAINAGE.
- ROADWAY WIDTH TO BE DETERMINED BY CLIENT.
- THE LIMITED USE PERVIOUS ACCESS ROAD CROSS SLOPE SHALL BE 2% IN MOST CASES AND SHOULD NOT EXCEED 6%. THE LONGITUDINAL SLOPE OF THE ACCESS DRIVE SHOULD NOT EXCEED 15%.
- LIMITED USE PERVIOUS ACCESS ROAD IS NOT INTENDED TO BE UTILIZED FOR CONSTRUCTION WHICH MAY SUBJECT THE ACCESS TO SEDIMENT TRACKING. THE PERVIOUS ACCESS ROAD SHALL BE PROTECTED DURING CONSTRUCTION BY PLACING A GEOTEXTILE (MIRAFI 140N OR APPROVED EQUIVALENT) ON THE SURFACE OF THE PERVIOUS ROAD FOLLOWED BY 3'-4" OF GRAVEL OR NYSDOT CRUSHER RUN.
- TO ENSURE THAT SOIL IS NOT TRACKED ONTO THE LIMITED USE PERVIOUS ACCESS ROAD, IT SHALL NOT BE USED BY CONSTRUCTION VEHICLES TRANSPORTING SOIL, FILL MATERIAL, ETC. IF THE LIMITED USE PERVIOUS ACCESS IS COMPLETED DURING THE INITIAL PHASES OF CONSTRUCTION, A STANDARD NEW YORK STATE STABILIZED CONSTRUCTION ACCESS SHALL BE CONSTRUCTED AND UTILIZED TO REMOVE SEDIMENT FROM CONSTRUCTION VEHICLES AND EQUIPMENT PRIOR TO ENTERING THE LIMITED USE PERVIOUS ACCESS ROAD FROM ANY LOCATION ON, OR OFF SITE. MAINTENANCE OF THE PERVIOUS ACCESS ROAD WILL BE REQUIRED IF SEDIMENT IS OBSERVED WITHIN THE CLEAN STONE.
- THE LIMITED USE PERVIOUS ACCESS ROAD SHALL NOT BE CONSTRUCTED OR USED UNTIL ALL AREAS SUBJECT TO RUNOFF ONTO THE PERVIOUS ACCESS HAVE ACHIEVED FINAL STABILIZATION.
- PROJECTS SHOULD AVOID INSTALLATION OF THE LIMITED USE PERVIOUS ACCESS ROAD IN POORLY DRAINED AREAS, HOWEVER IF NO ALTERNATIVE LOCATION IS AVAILABLE, THE PROJECT SHALL UTILIZE WOVEN GEOTEXTILE MATERIAL AS DETAILED IN FOLLOWING NOTES.
- THE DRAINAGE DITCH IS OFFERED IN THE DETAIL FOR CIRCUMSTANCES WHEN CONCENTRATED FLOW COULD NOT BE AVOIDED. THE INTENTION OF THIS DESIGN IS TO MINIMIZE ALTERATIONS TO HYDROLOGY, HOWEVER WHEN DEALING WITH 5%-15% GRADES NOT PARALLEL TO THE CONTOUR, A ROADSIDE DITCH MAY BE REQUIRED. THE NYS STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROLS FOR GRASSED WATERWAYS AND VEGETATED WATERWAYS ARE APPLICABLE FOR SIZING AND STABILIZATION. DIMENSIONS FOR THE GRASSED WATERWAY SPECIFICATION WOULD BE DESIGNED FOR PROJECT SPECIFIC HYDROLOGIC RUNOFF CALCULATIONS, AND A SEPARATE DETAIL FOR THE SPECIFIC GRASSED WATERWAY WOULD BE INCLUDED IN THIS PRACTICE. RUNOFF DISCHARGES WILL BE SUBJECT TO THE OUTLET REQUIREMENTS OF THE REFERENCED STANDARD. INCREASED POST-DEVELOPMENT RUNOFF FROM THE ASSOCIATED ROADSIDE DITCH MAY REQUIRE ADDITIONAL PRACTICES TO ATTENUATE RUNOFF TO PRE-DEVELOPMENT CONDITIONS.
- A HYDROLOGIC ANALYSIS OF THE ARRAY SITE HAS BEEN PERFORMED AND THE RESULTS SHOW THAT THE HYDROLOGY HAS NOT BEEN ALTERED FROM THE PRE- TO POST-DEVELOPMENT CONDITIONS AS DEFINED IN APPENDIX A OF GP-0-20-001. THE SUPPORTING CALCULATIONS FOR THIS ANALYSIS ARE INCLUDED IN THE SEPARATELY BOUND SWPPP PREPARED FOR THIS PROJECT.
- AT THE COMPLETION OF ARRAY INSTALLATION THE UPPER 3"-4" OF GRAVEL WEARING SURFACE SHALL BE REMOVED ALONG WITH THE MIRAFI 140N EXPOSING THE PROTECTED PERVIOUS ROAD SECTION

GEGRID MATERIAL NOTES:

- THE GEGRID, OR COMPARABLE PRODUCT, IS INTENDED FOR USE FOR ALL CONDITIONS, IN ORDER TO ASSIST IN MATERIAL SEPARATION FROM NATIVE SOILS AND PRESERVE ACCESS LOADS.
- GRAVEL FILL MATERIAL SHALL CONSIST OF 1-4" CLEAN, DURABLE, SHARP-ANGLED CRUSHED STONE OF UNIFORM QUATY, MEETING THE SPECIFICATIONS OF NYSDOT ITEM 703-02, SIZE DESIGNATION 3-5 OF TABLE 703-4. STONE MAY BE PLACED IN FRONT OF, AND SPREAD WITH, A TRACKED VEHICLE. GRAVEL SHALL NOT BE COMPACTED.
- GEGRID SHALL BE MIRAFI BXG110 OR APPROVED EQUAL. GEGRID SHALL BE DESIGNED BASED ON EXISTING SOIL CONDITIONS AND PROPOSED HAUL ROAD SLOPES.
- IF MORE THAN ONE ROLL WIDTH IS REQUIRED, ROLLS SHOULD OVERLAP A MINIMUM OF SIX INCHES.
- REFER TO MANUFACTURER'S SPECIFICATION FOR PROPER TYING AND CONNECTIONS.
- LIMITED USE PERVIOUS ACCESS ROAD SHALL BE TOP DRESSED AS REQUIRED WITH ONLY 1-4" CRUSHED STONE MEETING NYSDOT ITEM 703-02 SPECIFICATIONS.

BASIS OF DESIGN: TENCATE MIRAFI BXG110 GEGRIDS; 365 SOUTH HOLLAND DRIVE, PENDERGRASS, GA; 800-685-9990 OR 706-693-2226; WWW.MIRAFI.COM

GEOWEB MATERIAL NOTES:

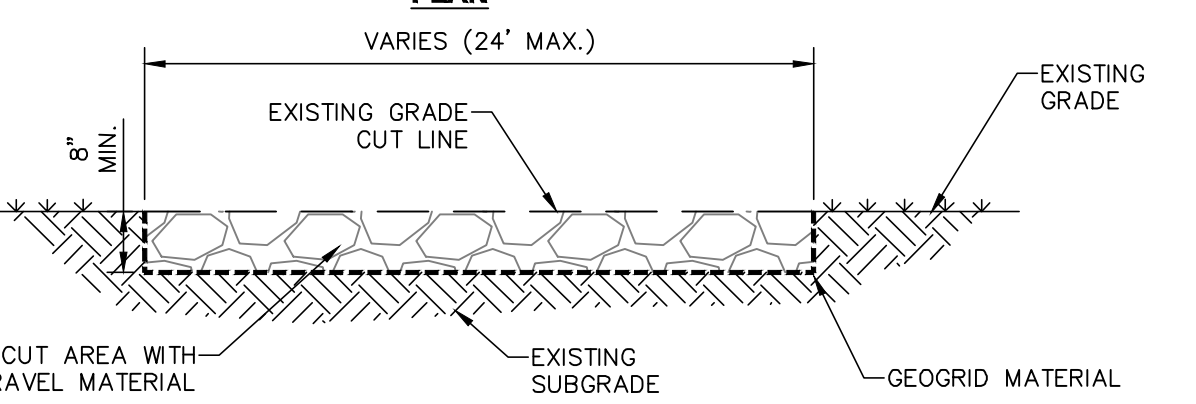
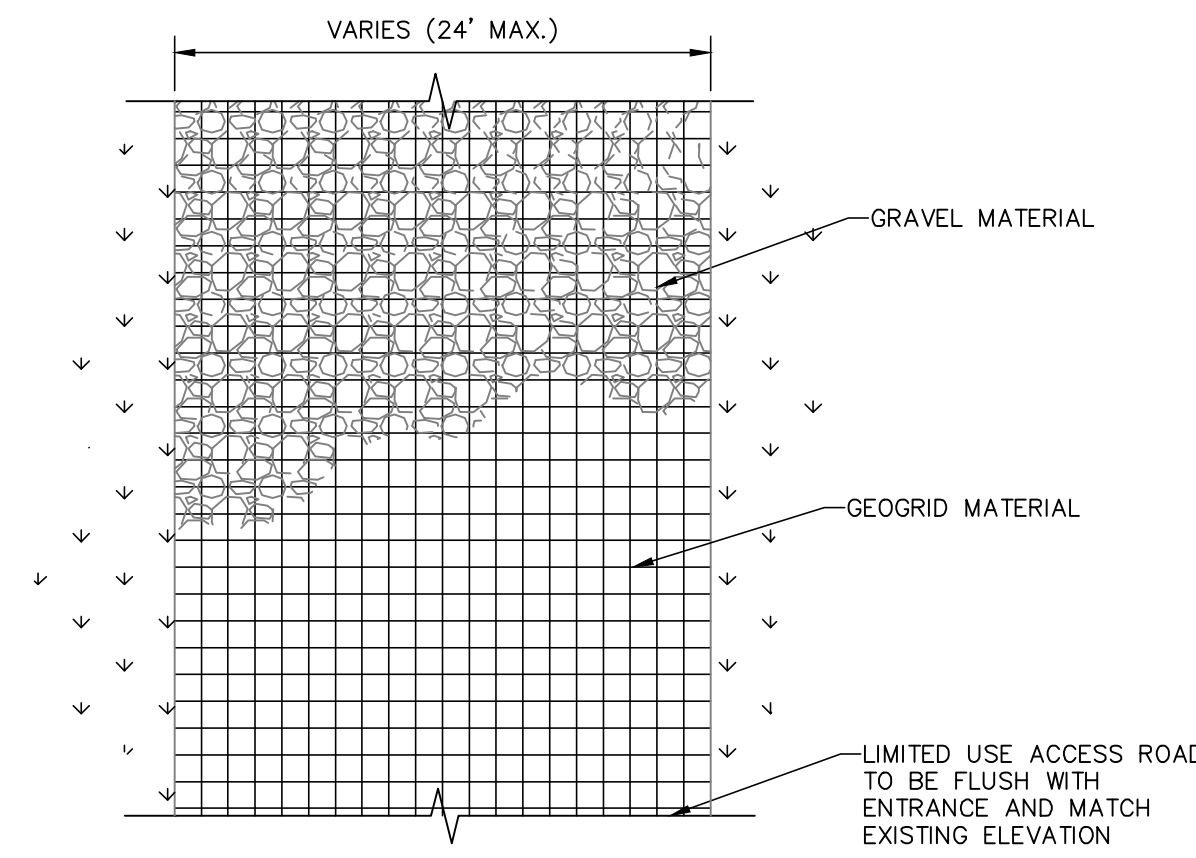
- THE GEOWEB, OR COMPARABLE PRODUCT, IS SUGGESTED FOR USE ON ROAD PROFILES EXCEEDING 10%. THE GEOWEB PRODUCT IS INTENDED TO LIMIT SHIFTING STONE MATERIAL DURING USE.
- INSTALLATION TO BE COMPLETED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
- WHERE REQUIRED, A NATIVE SOIL WEDGE SHALL BE PLACED TO ACCOMMODATE ROAD CROSS SLOPE OF 2%. NATIVE SOIL SHALL BE COMPACTED TO MATCH EXISTING SOIL CONDITIONS.
- GRAVEL FILL MATERIAL SHALL CONSIST OF 1-4" CLEAN, DURABLE, SHARP-ANGLED CRUSHED STONE OF UNIFORM QUATY, MEETING THE SPECIFICATIONS OF NYSDOT ITEM 703-02, SIZE DESIGNATION 3-5 OF TABLE 703-4. STONE MAY BE PLACED IN FRONT OF, AND SPREAD WITH, A TRACKED VEHICLE. GRAVEL SHALL NOT BE COMPACTED.
- GEOWEB SYSTEM SHALL BE PRESTO GEOSYSTEM GEOWEB OR APPROVED EQUAL. GEOWEB SHALL BE DESIGNED BASED ON EXISTING SOIL CONDITIONS AND PROPOSED HAUL ROAD SLOPES.
- LIMITED USE PERVIOUS ACCESS ROAD SHALL BE TOP DRESSED AS REQUIRED WITH ONLY 1-4" CRUSHED STONE, SIZE 3A, MEETING NYSDOT ITEM 703-02 SPECIFICATIONS.
- THE TOP EDGES OF ADJACENT CELL WALLS SHALL BE FLUSH WHEN CONNECTING. ALIGN THE I-SLOTS FOR INTERLEAF AND END TO END CONNECTIONS. THE GEOWEB PANELS SHALL BE CONNECTED WITH ATRA KEYS AT EACH INTERLEAF AND END TO END CONNECTIONS. REFER TO MANUFACTURER'S SPECIFICATION FOR PROPER INSTALLATION, TYING AND CONNECTIONS.

BASIS OF DESIGN: PRESTO GEOSYSTEMS GEOWEB; 670 NORTH PERKINS STREET, APPLETON, WI; 800-548-3424 OR 920-738-1222; INFO@PRESTOGEO.COM; WWW.PRESTOGEO.COM

WOVEN GEOTEXTILE MATERIAL NOTES:

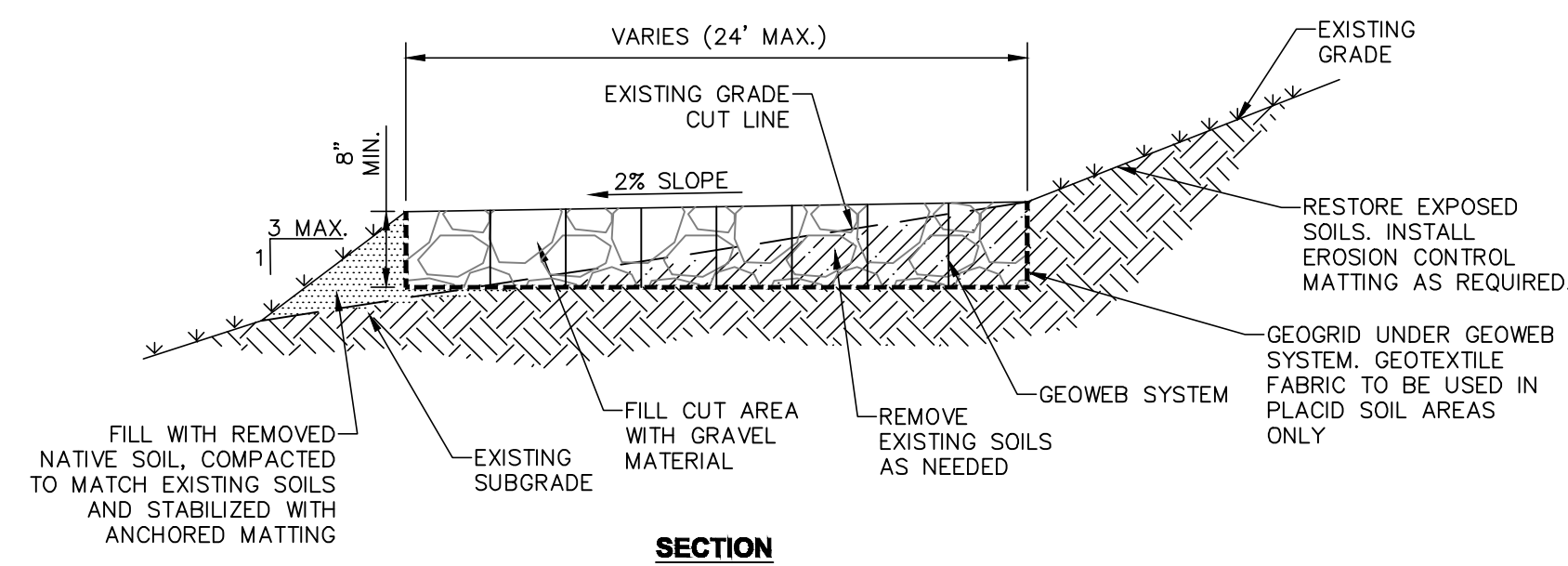
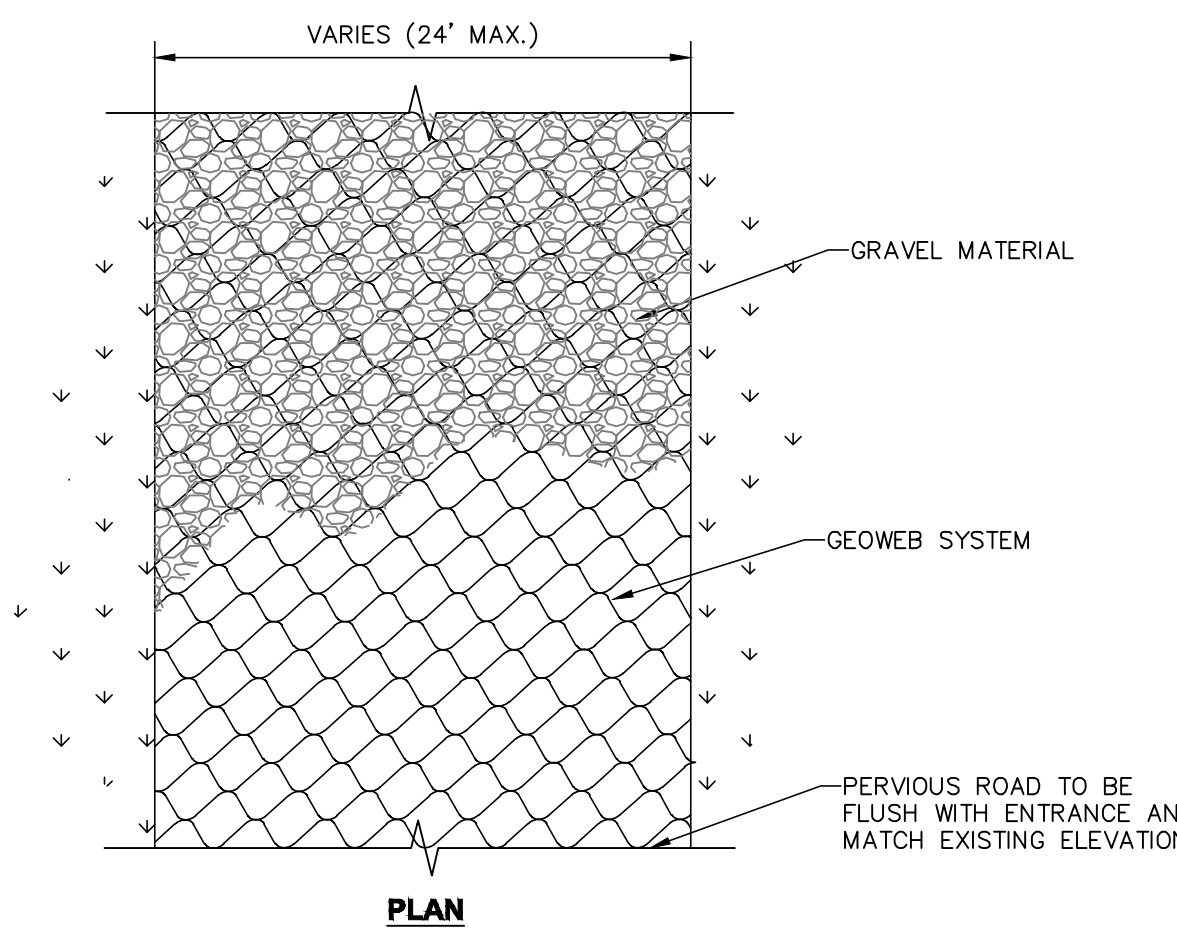
- SPECIFIED GEOTEXTILE WILL ONLY BE UTILIZED IN PLACID SOILS. PLACID SOILS CONSIST OF POORLY DRAINED SOILS COMPOSED OF FINELY TEXTURED PARTICLES AND ARE PRONE TO RUTTING. PLACID SOILS ARE TYPICALLY PRESENT IN LOW-LYING AREAS WITH HYDROLOGIC SOILS GROUP (HSG) OF C OR D, OR AS SPECIFIED FROM AN ENVIRONMENTAL SCIENTIST, SOIL SCIENTIST, OR GEOTECHNICAL DATA.
- THE CONCERN FOR POTENTIAL REDUCTION OF NATIVE INFILTRATION RATES DUE TO THE GEOTEXTILE MATERIAL WOULD NOT BE A SIGNIFICANT CONCERN IN POORLY DRAINED SOILS WHERE SEGREGATION OF PERVIOUS STONE AND NATIVE MATERIALS IS CRUCIAL FOR LONG TERM OPERATION AND MAINTENANCE.

BASIS OF DESIGN: TENCATE MIRAFI 160N; 365 SOUTH HOLLAND DRIVE, PENDERGRASS, GA; 800-685-9990 OR 706-693-2226; WWW.MIRAFI.COM



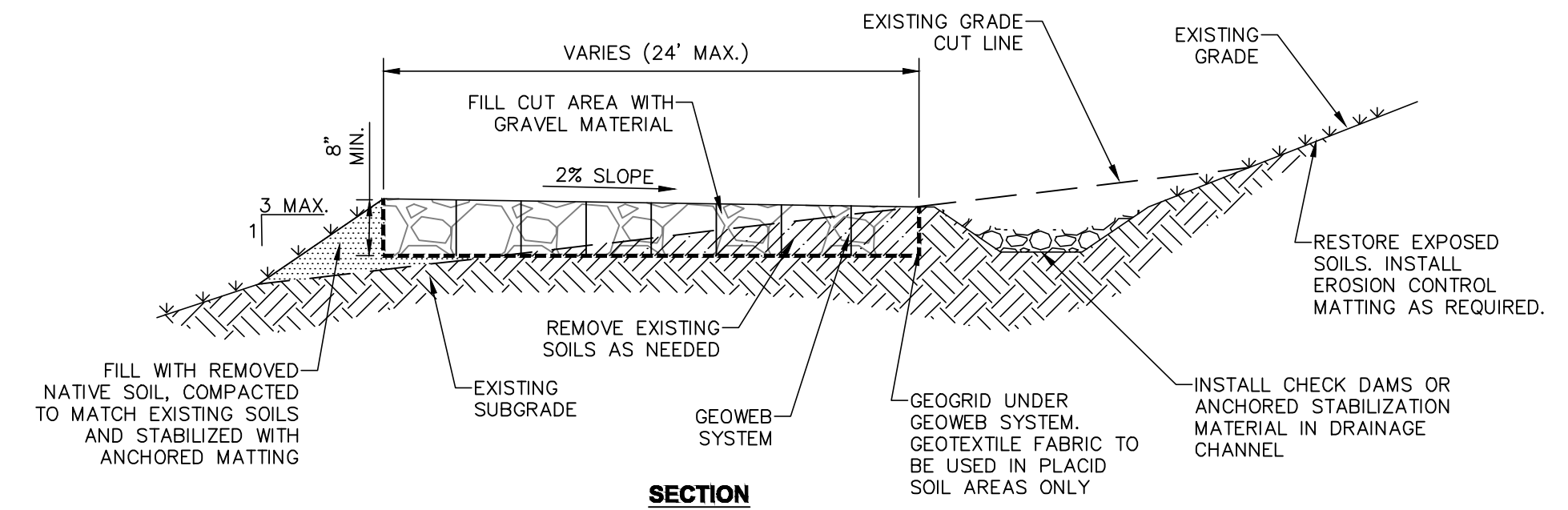
A LIMITED USE PERVIOUS ACCESS ROAD - 0% TO 10% SLOPES

SCALE: N.T.S.



C LIMITED USE PERVIOUS ACCESS ROAD - 10% AND GREATER SLOPES

SCALE: N.T.S.

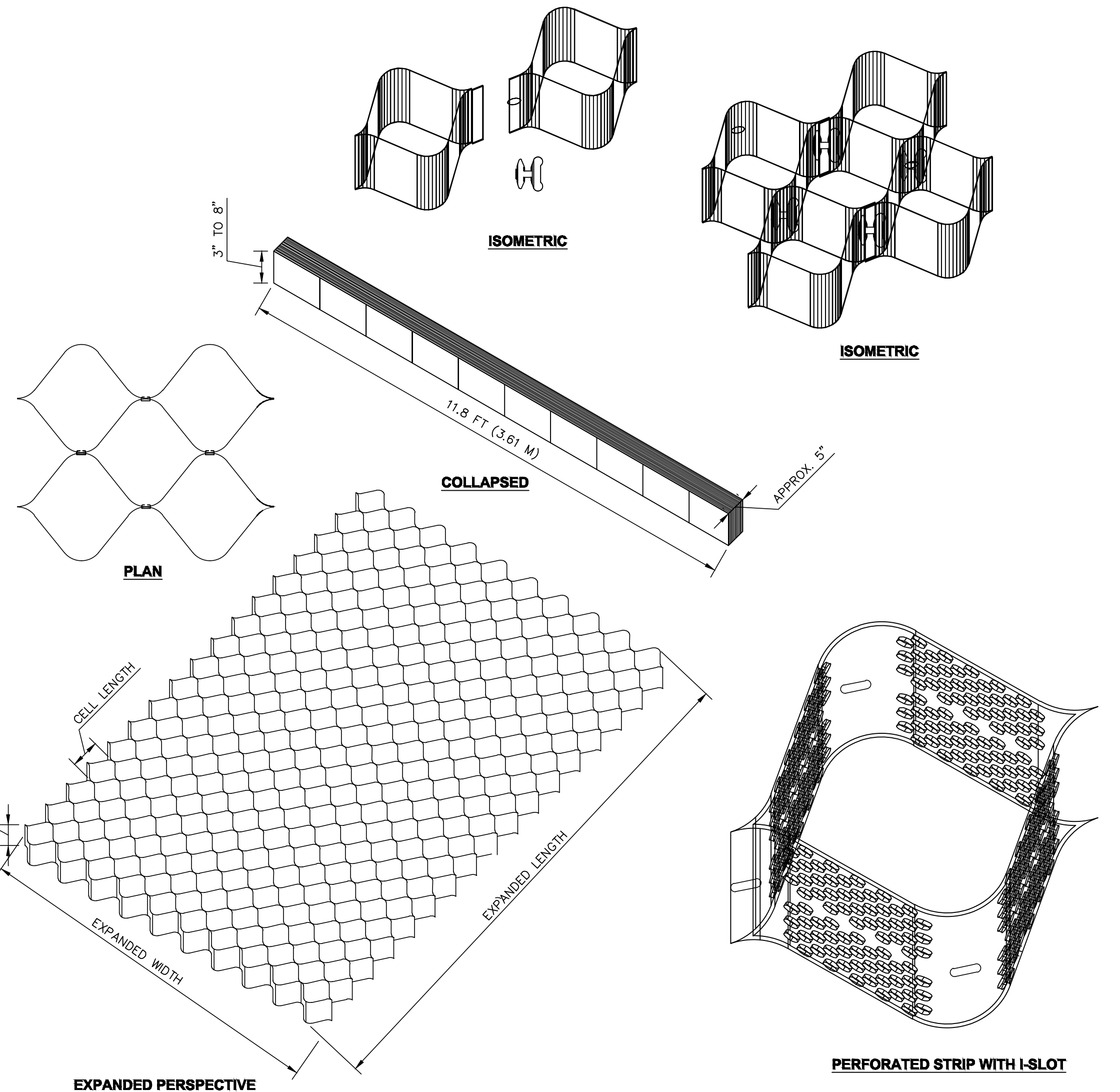


NOTE:

- THE ROADSIDE DITCH SHALL BE DESIGNED IN ACCORDANCE WITH THE NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROLS FOR GRASSED AND VEGETATED WATERWAYS. ADDITIONAL DETAILS WILL BE PROVIDED SPECIFIC TO THE SITE DESIGN.

B LIMITED USE PERVIOUS ACCESS ROAD - 10% AND GREATER SLOPES WITH DITCH

SCALE: N.T.S.



D GEOWEB SYSTEM

SCALE: N.T.S.

1 ACCESS ROAD SECTION - TYP.

SCALE: NONE
CROSS REFERENCE: NONE

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SITE DETAILS

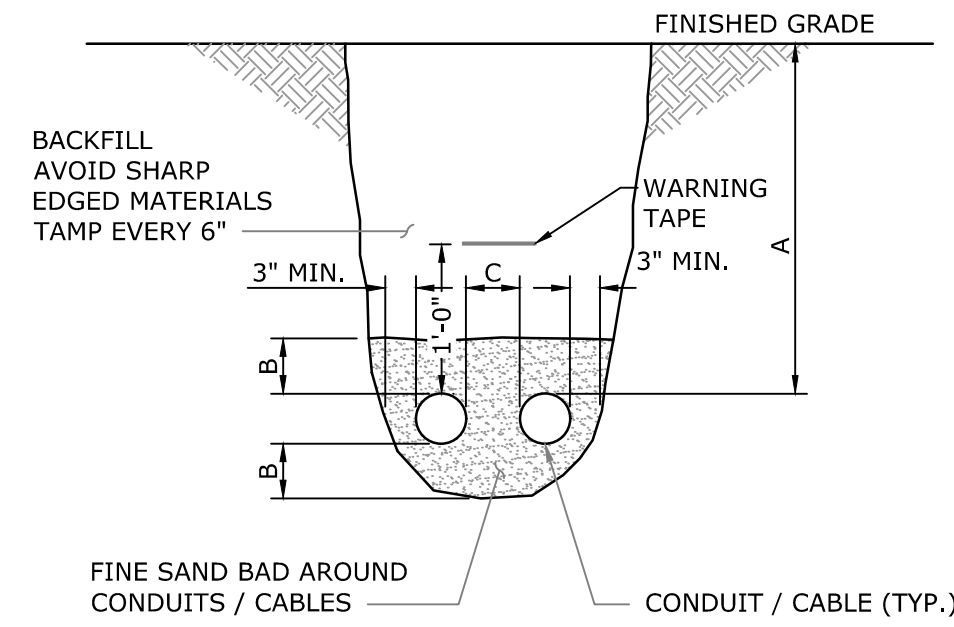
LANSING COMMUNITY SOLAR, LLC
GENIE SOLAR ENERGY

TOWN OF LANSING TOMPKINS COUNTY, NEW YORK

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www.ctmale.com

C-502
SHEET 09 OF 14
DWG. NO: 23-0157

CAD DWG. FILE NAME: K:\Projects\222303\Civil\00_Drawings and Maps\C-502.dwg



MINIMUM DIMENSIONS			
SERVICE TYPE	A	B	C
≤ 1,000 VOLTS	18"	3"	6"
> 1,000 VOLTS	30"	6"	6"
≤ 1,000 VOLTS DIRECT BURIAL	36"	6"	6"

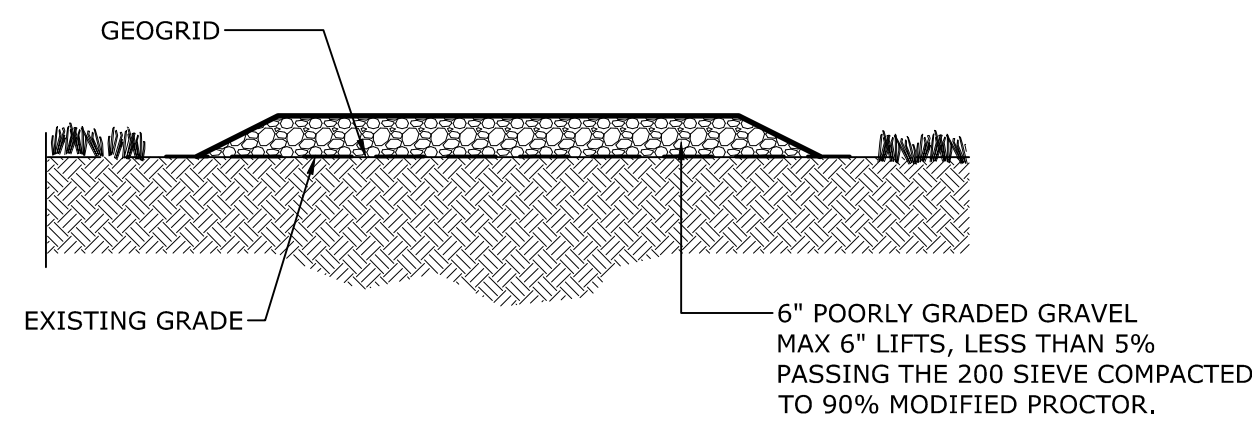
NOTES:

- ALL UNDERGROUND CONDUIT SHALL BE PVC. CONDUIT SHALL TRANSITION TO RGS FOR ELBOW AND STUB-UPS AND STAY AS RGS UP INTO CABINET OR ENCLOSURE.
- UNDER ROADS AND PARKING AREAS CONDUIT SHALL BE SCHEDULE 80 PVC, UNDER GRASSY AREAS CONDUIT SHALL BE SCHEDULE 40 PVC.
- CALL BEFORE YOU DIG, DIAL 811 TO BE CONNECTED TO THE LOCAL ON-CALL CENTER. YOU MUST CALL AT LEAST 48 HOURS BEFORE EXCAVATING.
- REFER TO ELECTRICAL DESIGN PLANS FOR DETAILS OF THIS INSTALLATION.
- MAINTAIN 3' SEPARATION DISTANCE FROM OTHER UTILITIES.

1 TYPICAL CONDUIT TRENCH SECTION
C-503 SCALE: NONE CROSS REFERENCE: NONE

UPLAND SEED MIX		
LOW-GROWING WILDFLOWER & GRASS MIX - ERNM#156 (OR APPROVED EQUAL)		
SEEDING RATE: 20 LB PER ACRE WITH A COVER CROP OF GRAIN RYE AT 30 LB PER ACRE		
	COMMON NAME	% OF MIX
	SHEEP FESCUE, VARIETY NOT STATED	63.60%
LOLIUM MULTIFLORUM (L. PERENNE VAR. ITALICUM)	ANNUAL RYEGRASS	17%
	PERENNIAL BLUE FLAX	8%
	BLACKEYED SUSAN, COASTAL PLAIN NC ECOTYPE	2%
	LANCELEAF COREOPSIS, COASTAL PLAIN NC ECOTYPE	2%
	OXEYE DAISY	2%
	SHASTA DAISY	1%
	PARTRIDGE PEA, PA ECOTYPE	1%
	CORN POPPY/SHIRLEY MIX	1%
	COMMON YARROW	0.5%
ASTER OBLONGIFOLIUS (SYMPHYOTRICHUM OBLONGIFOLIUM)	AROMATIC ASTER, PA ECOTYPE	0.5%
	MISTFLOWER, VA ECOTYPE	0.5%
MONARDA PUNCTATA, COASTAL PLAIN SC ECOTYPE	SPOTTED BEEBALM, COASTAL PLAIN SC ECOTYPE	0.5%
	BUTTERFLY MILKWEED	0.3%
	SLENDER MOUNTAINMINT	0.1%
COMPANY INFORMATION		
ERNST CONSERVATION SEEDS, INC.		
ADDRESS: 8884 MERCER PIKE, MEADVILLE, PA 16335		
PHONE: (800) 873-3321		
WEB: HTTP://WWW.ERNSTSEED.COM		

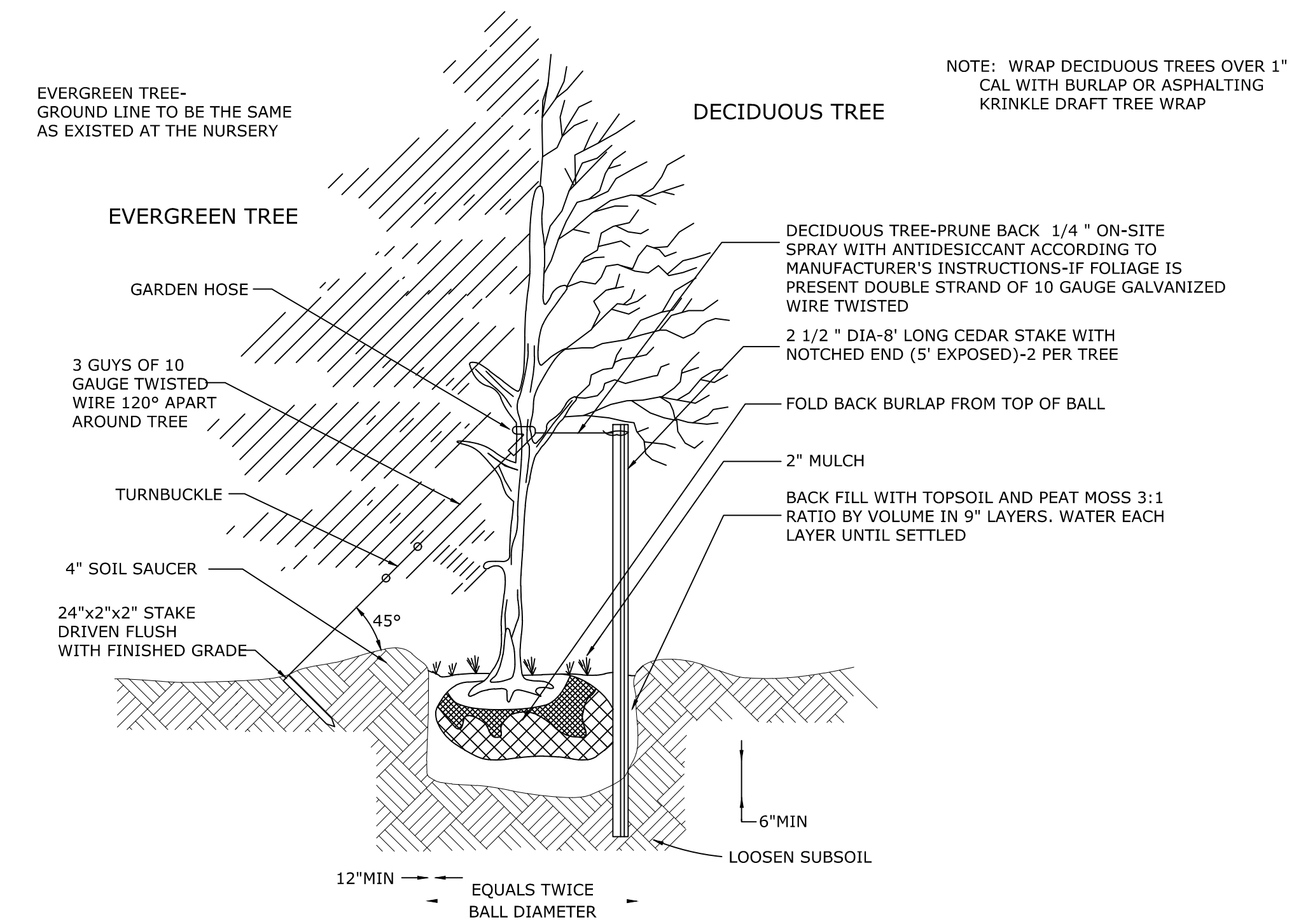
2 SEED MIXTURES DETAIL
C-503 SCALE: NTS CROSS REFERENCE: NONE



NOTES:

- PLACED ON EXISTING UNDISTURBED GRADE. SOIL DISTURBANCE SHALL BE LIMITED TO THE AREAS INDICATED ON THE SITE PLAN.
- GRASS AND VEGETATION SHALL BE MOWED TO MAXIMUM HEIGHT OF 1" PRIOR TO PLACING GEOGRID.
- UPON COMPLETION OF CONSTRUCTION ACTIVITY APPLY 3" THICKNESS OF TOPSOIL ON POORLY GRADED GRAVEL WHERE SHOWN AND APPLY GRASS SEED

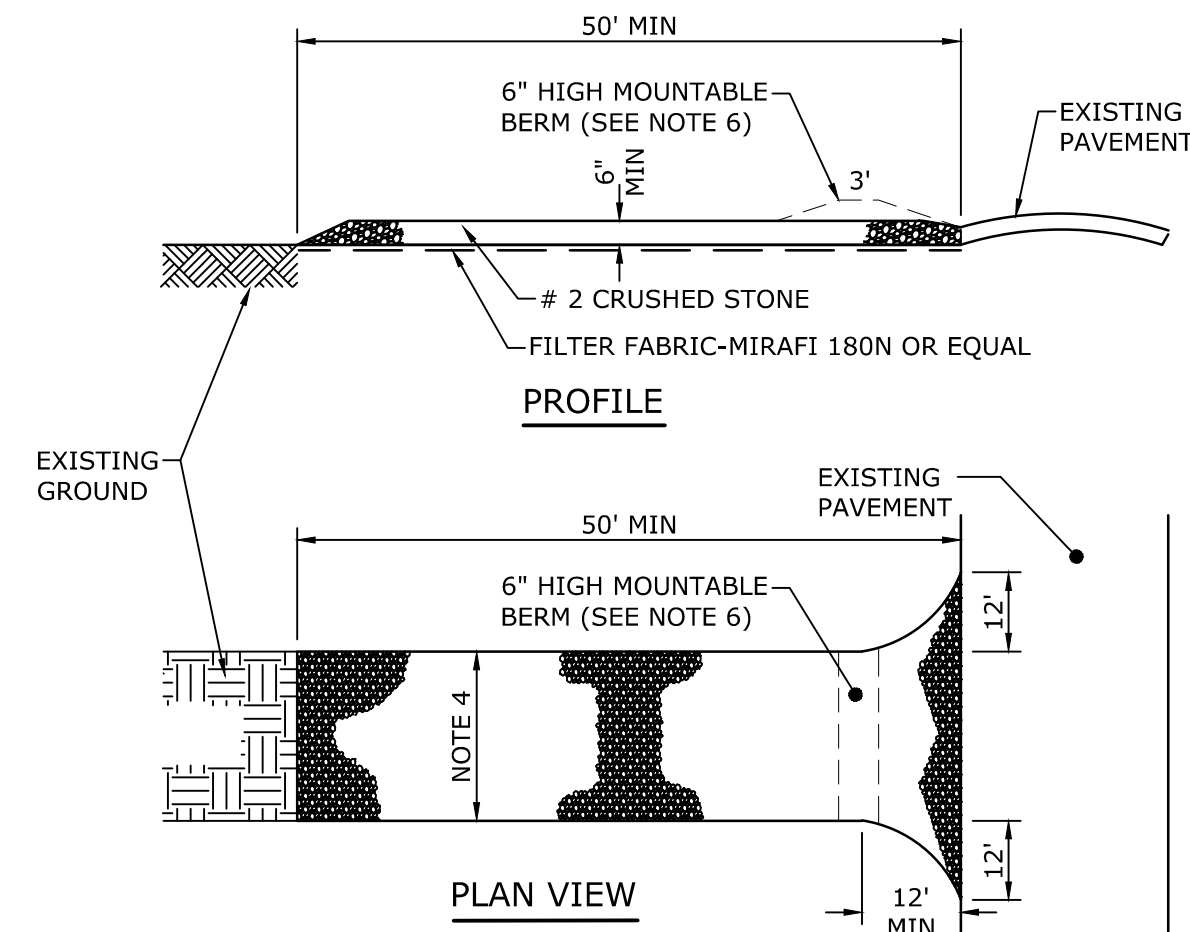
3 TEMPORARY STAGING AREA
C-503 SCALE: NONE CROSS REFERENCE: NONE



4 TYPICAL PLANTING DETAIL
C-503 SCALE: NONE CROSS REFERENCE: NONE

PRELIMINARY

OWEN K. SPEULSTRA P.E. NO. 104125	DATE	REVISIONS RECORD/DESCRIPTION	DRAFTER	CHECK	APPR.	UNAUTHORIZED ALTERATION OR ADDITION TO THIS DOCUMENT IS A VIOLATION OF THE NEW YORK STATE EDUCATION LAW. © 2023 C.T. MALE ASSOCIATES DESIGNER: MLS DRAFTED : MLS CHECKED : OKS PROJ. NO : 22.2303 SCALE : AS NOTED DATE : MARCH 24, 2023	SITE & ERSOSION AND SEDIMENT CONTROL DETAILS
						TOWN OF LANSING	TOMPKINS COUNTY, NEW YORK
						C.T. MALE ASSOCIATES Engineering, Surveying, Architecture, Landscape Architecture & Geology, D.P.C. 50 CENTURY HILL DRIVE, LATHAM, NY 518.786.7400 COBLESKILL, NY • GLENS FALLS, NY • POUGHKEEPSIE, NY JOHNSTOWN, NY • RED HOOK, NY • SYRACUSE, NY www.ctmale.com	C-503 SHEET 10 OF 14 DWG. NO: 23-0157



- NOTES:**
- USE NYS DOT #2 STONE, RECLAIMED, OR RECYCLED CONCRETE OR APPROVED EQUAL.
 - THE LENGTH SHALL NOT BE LESS THAN 50 FEET (EXCEPT ON A SINGLE RESIDENCE LOT WHERE A 30 FOOT MINIMUM LENGTH WOULD APPLY).
 - CRUSHED STONE SHALL BE MAINTAINED AT A MINIMUM OF 6" IN DEPTH.
 - ENTRANCE SHALL HAVE A 12 FOOT MINIMUM WIDTH, BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS. ENTRANCE SHALL BE AT LEAST 24 FEET WIDE IF SINGLE ENTRANCE TO SITE.
 - GEOTEXTILE SHALL BE PLACED OVER THE ENTIRE AREA PRIOR TO THE PLACING OF STONE.
 - ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED BENEATH THE ENTRANCE. IF PIPING IS NOT PRACTICAL, A MOUNTABLE BERM WITH 1:5 SLOPES WILL BE PERMITTED.
 - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY SHALL BE REMOVED IMMEDIATELY.
 - WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.
 - PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.

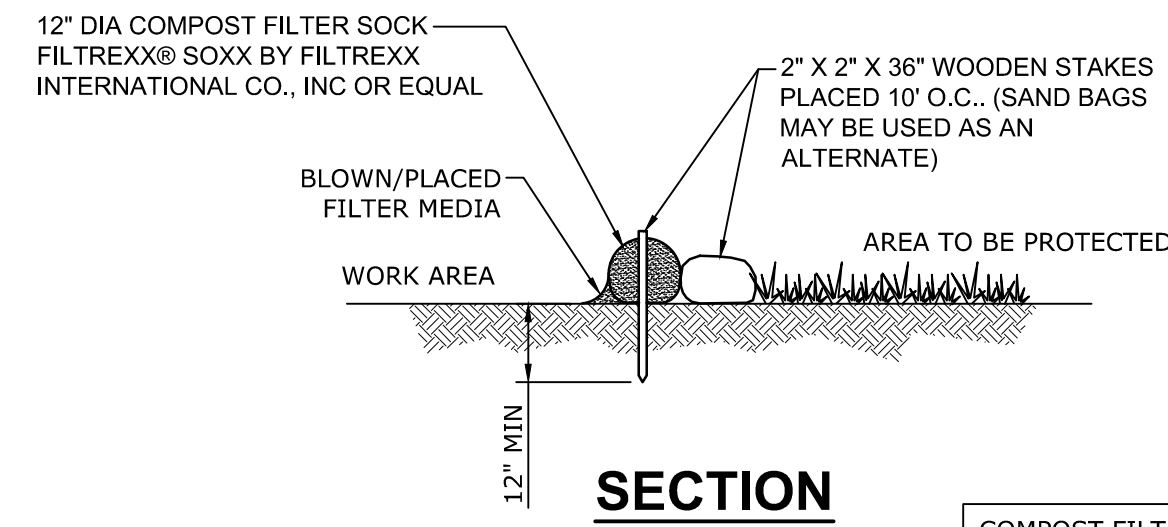
1
C-504
STABILIZED CONSTRUCTION ENTRANCE DETAIL
SCALE: NONE
CROSS REFERENCE: NONE

SEEDING AND MULCHING NOTES:

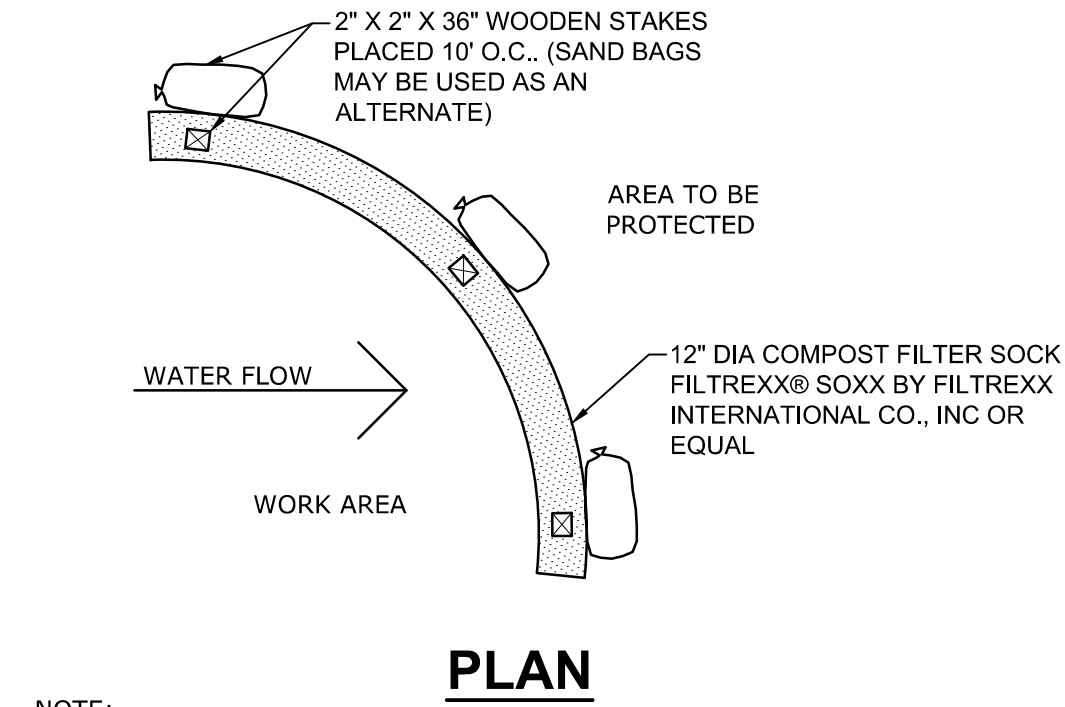
- TEMPORARY STABILIZATION MEASURES SHALL START AS SOON AS PRACTICAL ON PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT NOT MORE THAN (7) DAYS AFTER WORK HAS CEASED. ACCEPTABLE TEMPORARY STABILIZATION MEASURES INCLUDE, BUT MAY NOT BE LIMITED TO SEEDING, MULCH, STRAW, EROSION CONTROL BLANKETS, SOIL STABILIZING EMULSION PRODUCTS, OR SOME FUNCTIONALLY EQUIVALENT MEASURE. TEMPORARY SEEDING SHALL BE ANNUAL RYE GRASS, APPLIED AT A RATE OF 30 LBS./ACRE.
- TEMPORARY EROSION CONTROL PROTECTION BY MULCHING SHALL BE CARRIED OUT WITHIN (7) DAYS OF THE FINAL GRADE BEING FINALIZED TO AVOID POSSIBLE CONTAMINATION OF PONDS, STREAMS, OR OTHER WATERCOURSES. PLACEMENT OF JUTE MESH OR EROSION CONTROL BLANKETS OVER THE MULCH IS RECOMMENDED TO PROVIDE POSITIVE "TACKING" OF THE MULCH AND INCREASED PROTECTION AGAINST EROSION.
- PERMANENT SEEDING AND MULCH SHALL BE APPLIED AS SOON AS THE DISTURBED AREAS HAVE ACHIEVED FINAL GRADE. IF THE SPECIFIED SEEDING DATES ARE MISSED, MULCH SHALL BE APPLIED TO THE SLOPE AND SEED SHALL BE APPLIED TO THE TOP OF THE MULCH IN THE NEXT SEEDING SEASON AFTER RECONDITIONING THE TOPSOIL. WHEN THE FINAL GRADE CANNOT BE OBTAINED IN (7) DAYS, MULCH SHALL BE APPLIED FOR PURPOSES OF TEMPORARY EROSION CONTROL.
- EROSION CONTROL BLANKETS OR SOIL STABILIZING EMULSION PRODUCTS SERVE AS A TEMPORARY EROSION CONTROL MEASURE ON ALL SLOPES STEEPER THAN OR EQUAL 1V:3H AND AS INDICATED ON THE PLANS.
- THE UNDERLYING SOIL IN AREAS THAT WILL BE PERMANENTLY PERVIOUS (LAWN, GRASS AND LANDSCAPED AREAS) SHALL BE RESTORED IN ACCORDANCE WITH THE MEASURES IDENTIFIED IN THE JANUARY 2015, NYSDEC STORM WATER MANAGEMENT DESIGN MANUAL, SECTION 5.1.6 "SOIL RESTORATION".
- SEEDBED SHALL BE PREPARED BY LOOSENING THE TOPSOIL TO A DEPTH OF 4 TO 6 INCHES, AND LIMING TO A PH OF 6.5. FERTILIZER SHALL BE APPLIED IF NECESSARY.
- MULCH OVER PREEMINENT SEED AREAS SHALL CONSIST OF SMALL GRAIN STRAW APPLIED AT A RATE OF 2 TONS PER ACRE AND ANCHORED WITH WOOD FIBER HYDROMULCH APPLIED AT A RATE OF 500 TO 750 POUNDS PER ACRE. THE WOOD FIBER MULCH SHALL BE APPLIED THROUGH A HYDROSEEDER IMMEDIATELY AFTER SEEDING
- SEED MIXTURE:
 - LOW GROWING WILDFLOWER AND GRASS MIX - ERNMX#156 OR APPROVED EQUAL.
 - AS PER SEED MIX DETAIL,

TEMPORARY SEEDING AND MULCHING NOTES:

- TEMPORARY STABILIZATION MEASURES SHALL START AS SOON AS PRACTICAL ON PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT NOT MORE THAN (7) DAYS AFTER WORK HAS CEASED. ACCEPTABLE TEMPORARY STABILIZATION MEASURES INCLUDE, BUT MAY NOT BE LIMITED TO SEEDING MULCH, STRAW, EROSION CONTROL BLANKETS, SOIL STABILIZING EMULSION PRODUCTS, OR SOME FUNCTIONALLY EQUIVALENT MEASURE. TEMPORARY SEEDING SHALL BE ANNUAL RYE GRASS, APPLIED AT A RATE OF 30 LBS./ACRE.
- AREAS TO RECEIVE TEMPORARY SEEDING AND MULCHING SHALL RECEIVE BOTH GRASS SEED AND MULCH, AS DESCRIBED BELOW.
- SEED MIX TO BE LOW GROWING WILDFLOWER AND GRASS MIX - ERNMX#156 OR APPROVED EQUAL.
- MULCH SHALL CONSIST OF STRAW APPLIED AT A RATE OF 2 TONS PER ACRE OR WOOD CHIPS (MIN. 3" DEEP). (A WOOD FIBER HYDROMULCH OR OTHER APPROVED SPRAYABLE PRODUCT MAY BE SUBSTITUTED, IF APPLIED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS.)
- A JUTE MESH SHALL BE PLACED OVER THE MULCH IN AREAS WHERE WIND OR WATER EROSION PREVENTS ESTABLISHMENT OF GRASS COVER.

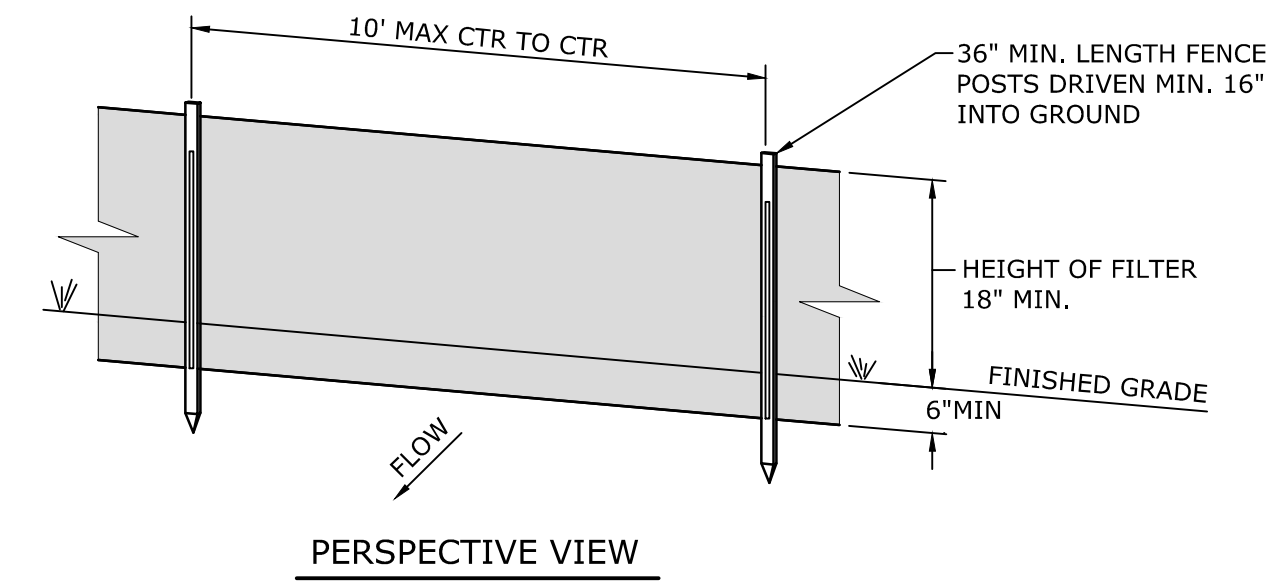


COMPOST FILTER SOCK MAY BE USED IN AREAS OF ROCK WHERE INSTALLATION OF SILT FENCE IS IMPRACTICAL



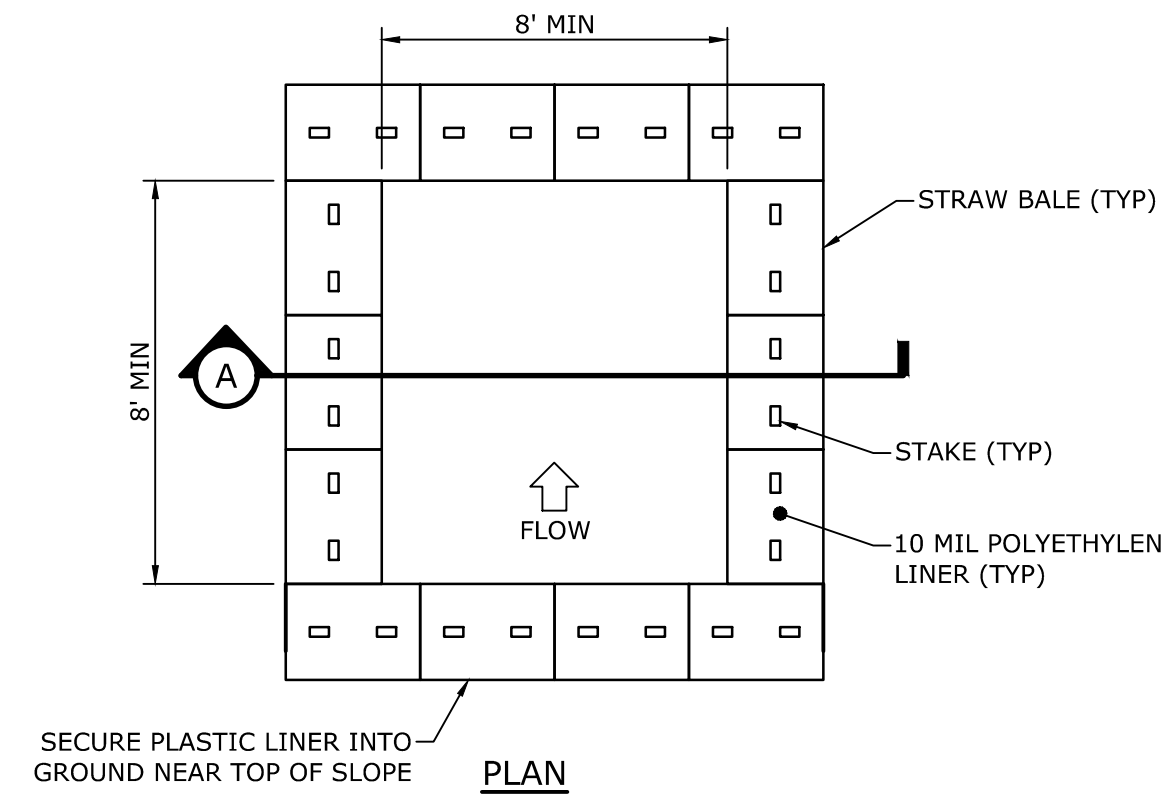
- NOTE:**
- FILL COMPOST FILTER SOCK WITH FILTER MEDIA APPROVED BY NYSDEC FOR THIS APPLICATION.
 - WHEN USING COMPOST FILTER SOCKS ADJACENT TO SURFACE WATER, THE COMPOST SHOULD HAVE A LOW NUTRIENT VALUE

2
C-504
COMPOST FILTER SOCK
SCALE: NTS
CROSS REFERENCE: NONE



- NOTES:**
- POSTS SHALL BE STEEL EITHER "T" OR "U" TYPE OR HARDWOOD.
 - FILTER CLOTH SHALL BE FASTENED SECURELY TO POSTS.
 - WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6", FOLDED AND STAPLED.
 - FILTER CLOTH SHALL BE MIRAFI 100X OR APPROVED EQUAL.
 - MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL SHALL BE REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. WHEN THE ACCUMULATED SEDIMENT REACHES 30% OF THE SILT FENCE HEIGHT, THE SEDIMENT SHALL BE REMOVED AND DISPOSED OF IN AN APPROPRIATE UPLAND AREA.
 - PREFABRICATED UNITS SHALL BE MIRAFI SILT FENCE, MIRAFI ENVIROFENCE OR APPROVED EQUIVALENT.

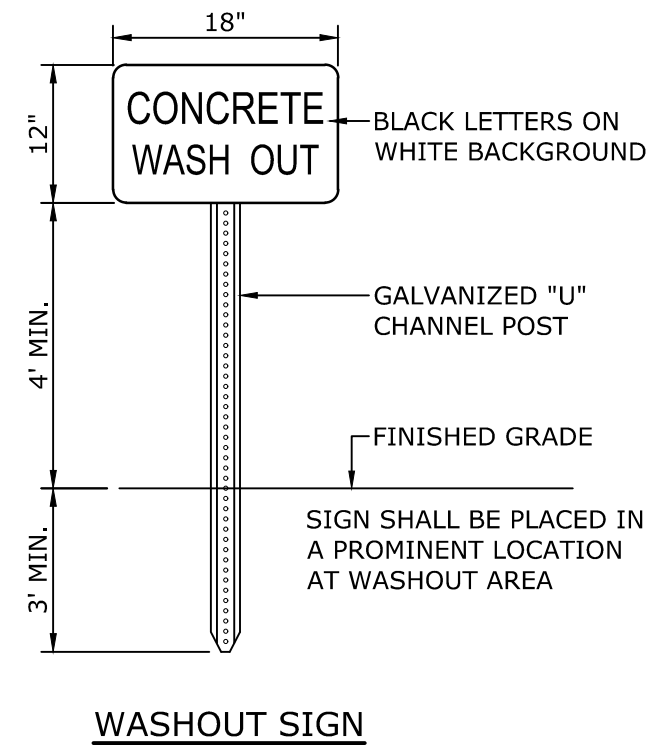
3
C-504
STANDARD SILT FENCE
SCALE: NONE
CROSS REFERENCE: NONE



NOTES

- CONTAINMENT MUST BE STRUCTURALLY SOUND AND LEAK FREE AND CONTAIN ALL LIQUID WASTES.
- CONTAINMENT DEVICES MUST BE OF SUFFICIENT QUANTITY OR VOLUME TO COMPLETELY CONTAIN THE LIQUID WASTES GENERATED.
- WASHOUT MUST BE CLEANED OR NEW FACILITIES CONSTRUCTED AND READY TO USE ONCE WASHOUT IS 75% FULL.
- WASHOUT AREA(S) SHALL BE INSTALLED IN A LOCATION EASILY ACCESSIBLE BY CONCRETE TRUCKS.
- ONE OR MORE AREAS MAY BE INSTALLED ON THE CONSTRUCTION SITE AND MAY BE RELOCATED AS CONSTRUCTION PROGRESSES.
- AT LEAST WEEKLY REMOVE ACCUMULATION OF SAND AND AGGREGATE AND DISPOSE OF PROPERLY.
- SIGN SHALL BE PLACED IN A PROMINENT LOCATION AT WASHOUT AREA

5
C-504
CONCRETE WASHOUT DETAIL
SCALE: NONE
CROSS REFERENCE: NONE



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					DATE : MARCH 24, 2023	
						C-504 SHEET 11 OF 14 DWG. NO: 23-0157

NYS DOT STANDARD WZTC NOTES

WORK ZONE TRAFFIC CONTROL REVISIONS:

PROPOSED REVISIONS TO THE WORK ZONE TRAFFIC CONTROL (WZTC) PLAN OR MODIFICATIONS TO THE 619 STANDARD SHEETS SHALL BE SUBMITTED TO THE ENGINEER FOR THE REVIEW AND APPROVAL BY THE REGIONAL TRAFFIC ENGINEER PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH REVISIONS OR MODIFICATIONS. THE CONTRACTOR SHALL NOT IMPLEMENT THE PROPOSED REVISIONS WITHOUT APPROVAL FROM THE REGIONAL TRAFFIC ENGINEER. WHEN APPLICABLE, NYS DOT WORK ZONE TRAFFIC CONTROL (WZTC) TYPICAL APPLICATIONS SHALL BE USED. TYPICALS CAN BE FOUND AT [HTTPS://WEBAPPS.DOT.NY.GOV/WORK-ZONE-TRAFFIC-CONTROL](https://webapps.dot.ny.gov/work-zone-traffic-control)

TRAVEL LANE WIDTHS IN WORK ZONES:

WHERE NOT SHOWN IN THE WZTC PLANS OR OTHERWISE AUTHORIZED BY NYS DOT (OR THE ENGINEER), TRAVEL LANE WIDTHS IN WORK ZONES SHALL BE A MINIMUM OF 11 FT ON FREEWAYS, RAMPS, EXPRESSWAYS AND MULTI-LANE CONVENTIONAL ROADWAYS AND 10 FT ON ALL OTHER CONVENTIONAL ROADWAYS.
*(MULTI-LANE ROADWAYS ARE THOSE WITH TWO OR MORE THRU LANES IN ONE OR BOTH DIRECTIONS.)

WORK ZONES SHALL BE RESTRICTED TO ONE SIDE OF THE ROADWAY AT A TIME IN EACH DIRECTION ON DIVIDED ROADWAYS, UNLESS APPROVED BY THE ENGINEER.

THE CONTRACTOR SHALL SCHEDULE WORK SO THAT ALL TRAVEL LANES AND RAMPS IN EACH DIRECTION ARE OPEN WHEN THE CONTRACTOR'S OPERATIONS ARE CLOSED OR SUBSTANTIALLY CLOSED DOWN.

DAILY CLOSURES MAY OCCUR OFF OF LONG-TERM CLOSURES AND SHALL BE SUBJECT TO DAILY CLOSURE RESTRICTIONS. WORK ZONES SHALL BE RESTRICTED TO ONE SIDE OF THE ROADWAY AT A TIME ON UNDIVIDED HIGHWAYS.

FLAGGING OPERATIONS:

WHEN A PEDESTRIAN APPROACHES A FLAGGER STATION, THE FLAGGER SHALL STOP TRAFFIC AND DIRECT THE PEDESTRIAN TO A SAFE ROUTE THROUGH THE WORK AREA. FLAGGERS SHALL COORDINATE THE FLAGGING OF THE WORK ZONE TO ENSURE PEDESTRIANS CAN SAFELY PROCEED THROUGH THE AREA. IF THERE IS MORE THAN THE OCCASIONAL PEDESTRIAN WITHIN THE PROJECT LIMITS, REFER TO THE SITE SPECIFIC PEDESTRIAN WZTC PLAN.

NOTIFICATION REQUIREMENTS:

REGION 3 HAS A WORK ZONE TRAFFIC CONTROL (WZTC) NOTIFICATION POLICY WHICH REQUIRES ENGINEERS/CONTRACTOR TO NOTIFY THE REGIONAL TRANSPORTATION MANAGEMENT CENTER (RTMC) PRIOR TO ALLOWING A CONTRACTOR TO IMPLEMENT WORK ZONE TRAFFIC CONTROL ACTIVITIES WITHIN THE HIGHWAY RIGHT OF WAY. WORK ZONE NOTIFICATION IS REQUIRED FOR THE FOLLOWING:

FREEWAYS AND EXPRESSWAYS: ALL LANE, SHOULDER, ROAD, RAMP OR BRIDGE CLOSURES. (THIS INCLUDES MOBILE OPERATIONS WHICH OCCUPY THE LANE OR SHOULDERS.)

ALL OTHER STATE HIGHWAYS: ALL LANE CLOSURES WHOSE DURATION WILL BE GREATER THAN 2 HOURS AND ALL ROAD/BRIDGE CLOSURES.

THE CONTRACTOR SHALL REPORT PROPOSED WZTC ACTIVITIES NOTED ABOVE TO THE TMC BY NOON OF THE BUSINESS DAY (I.E. MONDAY THROUGH FRIDAY EXCLUDING HOLIDAYS) PRECEDING THE PROPOSED WZTC ACTIVITY. FAILURE TO DO SO WILL RESULT IN DISAPPROVAL TO PERFORM THE UNREPORTED WZTC ACTIVITY UNTIL THE ABOVE NOTIFICATIONS REQUIREMENTS ARE SATISFIED.

NO PLANNED WZTC ACTIVITY SHALL BE IMPLEMENTED WITHOUT FIRST RECEIVING CLEARANCE FROM THE RTMC.

ACCESS:

THE CONTRACTOR SHALL ENSURE THAT ACTIVE LANES OF TRAFFIC ON FREEWAYS ARE NOT CROSSED BY PEDESTRIAN WORKERS. FOR ALL OTHER HIGHWAYS, THE CONTRACTOR SHALL ENSURE THAT PEDESTRIAN WORKERS CROSS ACTIVE LANES OF TRAFFIC ONLY AT PROPERLY MARKED OR UNMARKED CROSSWALKS AND/OR DEDICATED PEDESTRIAN WALKWAYS. IT IS REQUIRED THAT THE PROJECT SAFETY AND HEALTH PLAN ADDRESS ACCESS TO EACH WORK AND STAGING AREA.

WHERE IT IS FEASIBLE, VEHICLES AND EQUIPMENT USED FOR THE WORK AND TRANSPORTING OF WORKERS TO/FROM THE WORK SITE SHALL ENTER AND LEAVE THE AREA CLOSED BY CHANNELIZING DEVICES WITHIN THE TERMINATION AREA OF THE TEMPORARY TRAFFIC CONTROL ZONE. WHERE SUCH ACCESS WITHIN THE TERMINATION AREA IS NOT FEASIBLE, OTHER AREAS FOR ENTRY AND EXIT SHALL BE DETERMINED AND INCLUDED IN THE PROJECT SAFETY & HEALTH PLAN, INCLUDING ILLUSTRATED EXAMPLES (TYPICALS) TO CLEARLY SHOW THE TEMPORARY TRAFFIC CONTROL ELEMENTS THAT WILL BE PROVIDED.

CHANNELIZING DEVICES:

ALL CHANNELIZING DEVICES SHALL BE PLACED SO AS TO PROVIDE A 2-FOOT LATERAL CLEARANCE TO THE TRAVELED WAY UNLESS OTHERWISE SHOWN ON THE PLANS. WHERE POSSIBLE A LATERAL BUFFER SPACE OF 2 FOOT MINIMUM SHALL BE PROVIDED BETWEEN THE WORKSPACE AND THE CHANNELIZING DEVICES.

CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL BE 40' MAXIMUM FOR POSTED SPEED LIMITS 40 MPH OR GREATER AND 20' MAXIMUM FOR POSTED SPEED LIMITS 35 MPH OR LESS.

STANDARD CONES AND TUBULAR MARKERS SHALL NOT BE USED FOR CHANNELIZATION AND DELINEATION DURING THE HOURS OF DARKNESS, WHICH IS DEFINED AS THE PERIOD BETWEEN SUNSET AND SUNRISE.

SIGNS
ALL CONSTRUCTION SIGNS SHALL BE MOUNTED AT A HEIGHT OF 7 FEET ABOVE THE EDGE OF TRAVEL LANE.

SIGNS SHALL NOT ENCRoACH MORE THAN 4" INTO SHOULDERS USED BY PEDESTRIANS OR BICYCLES.

WHERE SHOULDER WIDTHS ARE LIMITED AND SIGNS CANNOT BE ERECTED BEYOND THE SHOULDER, CONSTRUCTION SIGNS MAY NEED TO BE MOUNTED ON CONCRETE MEDIAN BARRIERS, BRIDGE PARAPETS, ETC.

MISCELLANEOUS (LOCAL OR PERMIT PROJECTS):

THE CONTRACTOR SHALL BE AWARE THAT THE WORK ZONE TRAFFIC CONTROL IS A VERY CRITICAL ITEM OF THE PERMIT AND SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 619 "WORK ZONE TRAFFIC CONTROL" OF THE STANDARD SPECIFICATIONS, THE 2009 EDITION OF THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS AND THE NEW YORK STATE SUPPLEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR WORK ZONE TRAFFIC CONTROL AT ALL TIMES FOR THE DURATION OF THE PERMITTED WORK.

ACTUAL FIELD CONDITIONS MAY REQUIRE OTHER SIGNS AND OTHER ARRANGEMENTS OF SIGNS. DISTANCES SHALL BE ADAPTED TO PREVAILING CONDITIONS. SIGNS SHALL BE LOCATED TO PROVIDE OPTIMUM VISIBILITY. SIGNS THAT ARE NOT APPLICABLE SHALL BE COVERED OR OBSCURED FROM SIGHT. ALL SIGN NUMBERS REFER TO THE 2009 EDITION OF THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS AND THE NEW YORK STATE SUPPLEMENT.

PEDESTRIAN ACCOMMODATIONS SHALL BE MAINTAINED FOR THE DURATION OF THE PROPOSED WORK. ANY DISTURBED AREAS WITHIN THE STATE RIGHT-OF-WAY SHALL BE ADEQUATELY FENCED TO PREVENT PEDESTRIAN ACCESS WHEN THE CONTRACTORS' OPERATIONS ARE SHUT DOWN.

MATERIALS, EQUIPMENT, AND VEHICLES SHALL NOT BE STORED OR PARKED WITHIN THE STATE RIGHT-OF-WAY BEFORE WORK BEGINS OR AFTER CONTRACTOR'S OPERATIONS ARE SHUT DOWN. STAGING AREAS OUTSIDE THE RIGHT-OF-WAY SHALL BE USED TO STOCKPILE ALL CONSTRUCTION MATERIALS.

DURING WORKING HOURS, NO CONSTRUCTION MATERIAL MAY BE STORED OR PLACED ON THE ROADWAY OR ROADBED EXCEPT WITHIN A PROTECTED WORK AREA.

VEHICLES BELONGING TO THE CONTRACTOR OR WORKERS SHALL NOT BE PARKED WITHIN 30 FEET OF THE EDGE OF PAVEMENT ALONG A ROADWAY BEING USED BY THE PUBLIC, UNLESS THEY ARE PARKED WITHIN A PROTECTED WORK AREA.

DURING NON-WORKING HOURS, CONSTRUCTION EQUIPMENT AND MATERIALS SHALL NOT BE STORED WITHIN 30 FEET OF THE EDGE OF PAVEMENT.

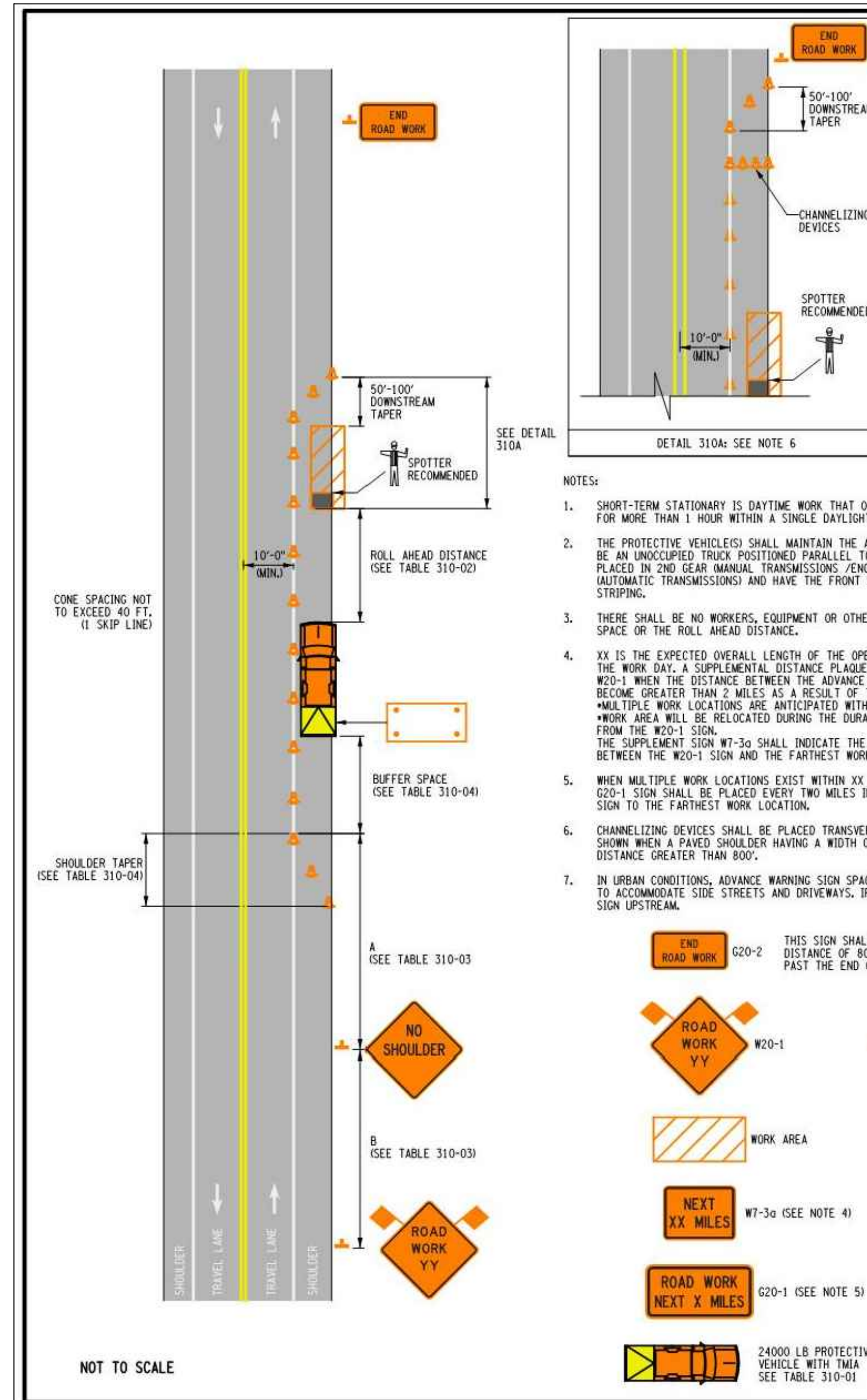
W20-7A "FLAGGER" SIGNS SHALL BE USED WHENEVER FLAGGING OCCURS FOR MORE THAN A BRIEF PERIOD OF TIME. THE SIGNS SHALL BE PROMPTLY REMOVED, COVERED, OR FACED AWAY FROM TRAFFIC WHEN THE FLAGGING OPERATION CEASES.

ALL FLAGGING STATIONS AND LANE CLOSURES SHOULD BE LOCATED TO ENSURE MAXIMUM VISIBILITY.

NO DROP-OFF GREATER THAN SIX INCHES SHALL BE LEFT OVERNIGHT WITHIN 30 FEET OF THE EDGE OF PAVEMENT. DROP-OFFS LESS THAN SIX INCHES WILL BE PERMITTED IF PROPER DELINEATION AND SIGNING IS PROVIDED, AND PRIOR PERMISSION IS GRANTED IN WRITING BY A REPRESENTATIVE OF THE DEPARTMENT. A DROP-OFF IS CONSIDERED ELIMINATED IF TAPERED AWAY BY A 1 ON 6 SLOPE OR FLATTER.

CARE SHALL BE TAKEN TO ENSURE THAT NO DAMAGE OCCURS TO THE EXISTING PAVEMENT/SHOULDER/CURB AREAS AS A RESULT OF CONSTRUCTION EQUIPMENT MOVEMENT.

THE CONTRACTOR MAY SUBMIT REVISIONS TO THIS PLAN FOR APPROVAL, BUT ANY CHANGE THAT ALTERS THE BASIC CONCEPTS OF THE PLAN MUST BE APPROVED BY THE NYS DOT REGIONAL DIRECTOR OR HIS DESIGNEE.



- NOTES:
1. SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.
 2. THE PROTECTIVE VEHICLE(S) SHALL MAINTAIN THE APPROPRIATE ROLL AHEAD DISTANCE. BE AN UNOCCUPIED TRUCK POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN ONE GEAR (MANUAL TRANSMISSIONS: ENGINE OFF OR PARK / NEUTRAL (AUTOMATIC TRANSMISSIONS)) AND HAVE THE FRONT WHEELS ALIGNED WITH THE LANE STRIPING.
 3. THERE SHALL BE NO WORKERS, EQUIPMENT OR OTHER VEHICLES IN THE BUFFER SPACE OR THE ROLL AHEAD DISTANCE.
 4. XX IS THE EXPECTED OVERALL LENGTH OF THE OPERATION TO BE COMPLETED WITHIN THE WORK DAY. A SUPPLEMENTAL DISTANCE PLAQUE W7-3a SHALL BE USED WITH SIGN W20-1 WHEN THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND WORK MAY BECOME GREATER THAN 2 MILES AS A RESULT OF THE FOLLOWING SITUATIONS:
* MULTIPLE WORK LOCATIONS ARE ANTICIPATED WITHIN XX MILES FROM THE W20-1 SIGN * WORK AREA WILL BE RELOCATED DURING THE DURATION OF THE WORK WITHIN XX MILES FROM THE W20-1 SIGN. THE SUPPLEMENT SIGN W7-3a SHALL INDICATE THE MAXIMUM ANTICIPATED DISTANCE BETWEEN THE W20-1 SIGN AND THE FARTHEST WORK LOCATION.
 5. WHEN MULTIPLE WORK LOCATIONS EXIST WITHIN XX MILES FROM THE W20-1 SIGN, A G20-1 SIGN SHALL BE PLACED EVERY TWO MILES INDICATING THE DISTANCE FROM THE SIGN TO THE FARTHEST WORK LOCATION.
 6. CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY A MINIMUM OF EVERY 800' AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 800'.
 7. IN URBAN CONDITIONS, ADVANCE WARNING SIGN SPACINGS MAY BE ADJUSTED IN ORDER TO ACCOMMODATE SIDE STREETS AND DRIVEWAYS. IF THERE IS A CONFLICT, MOVE THE SIGN UPSTREAM.

TABLE 310-01: PROTECTIVE VEHICLE REQUIREMENTS

CLOSURE TYPE	ROAD TYPE & SPEED	NON-FREEWAY		
		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
LANE CLOSURE OR ENCROACHMENT	EXPOSURE CONDITIONS ¹			
	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMA	P, TMA	P
SHOULDER CLOSURE OR ENCROACHMENT	OTHER HAZARDS NO WORKERS EXPOSED	P, TMA	P	SEE NOTE 2
	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMA	P	P
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMA	P	SEE NOTE 2

LEGEND
P: PROTECTIVE VEHICLE REQUIRED FOR EACH CLOSED LANE & EACH CLOSED PAVED SHOULDER 8' OR WIDER. IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE PROTECTIVE VEHICLE SHALL BE REPOSITIONED ACCORDINGLY
TMA: TMA REQUIRED
NOTES:
1. THE EXPOSURE CONDITIONS ASSUMES THERE IS NO POSITIVE PROTECTION PRESENT
2. EITHER A PROTECTIVE VEHICLE OR THE STANDARD BUFFER SPACE SHALL BE PROVIDED

TABLE 310-02: ROLL AHEAD DISTANCE

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	STATIONARY OPERATION	
	MIN	MAX
≥ 55	120/3	200/5
45 - 50	80/2	160/4
≤ 40	40/1	120/3

TABLE 310-03: ADVANCE WARNING SIGN SPACING

ROAD TYPE	DISTANCE BETWEEN SIGNS		SIGN LEGEND	
	A (FT.)	B (FT.)	XX	YY
URBAN (≤ 30 MPH)	100	100	AHEAD	AHEAD
URBAN (35-40 MPH)	200	200	AHEAD	AHEAD
URBAN (45 MPH)	350	350	1000 FT.	AHEAD
RURAL	500	500	1500 FT.	1000 FT.

* PRECONSTRUCTION POSTED SPEED LIMIT

TABLE 310-04: LONGITUDINAL BUFFER SPACE AND TAPER LENGTHS

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	LONGITUDINAL BUFFER SPACE DISTANCE (FT.) ¹ * OF SKIP LINES	TAPER LENGTH L (FT.) ¹ * OF SKIP LINES/ ¹ * OF CHANNELIZING DEVICES				SHOULDER TAPER LENGTH L ¹ (FT.) ¹ * OF SKIP LINES/ ¹ * OF CHANNELIZING DEVICES	
		FOR LANE WIDTH IN FT. (LATERAL SHIFT OF TRAFFIC FLOW PATH)				FOR SHOULDER WIDTH	
		10	11	12	≤ 4 FT.	5 - 7 FT.	≥ 8 FT.
25	155/4	120/3/4	120/3/4	120/3/4	40/1/2	40/1/2	40/1/2
30	200/5	160/4/5	160/4/5	200/5/6	40/1/2	40/1/2	40/1/2
35	250/6	200/5/6	240/6/7	240/6/7	40/1/2	40/1/2	80/2/3
40	305/8	280/7/8	320/8/9	320/8/9	40/1/2	80/2/3	80/2/3
45	360/9	440/11/12	520/13/14	560/14/15	80/2/3	80/2/3	120/3/4
50	425/11	520/13/14	560/14/15	600/15/16	80/2/3	120/3/4	160/4/5
55	495/13	560/14/15	600/15/16	680/17/18	80/2/3	120/3/4	160/4/5

TABLE 310-05: REQUIRED SIGN SIZES*

SIGN	NON-FREEWAY	FREEWAY
G20-1	36x18	48x24
G20-2	36x18	48x24
W7-3a	24x18	36x30
W8-23	36x36	48x48
W20-1	36x36	48x48
WARNING FLAG	18x18	18x18

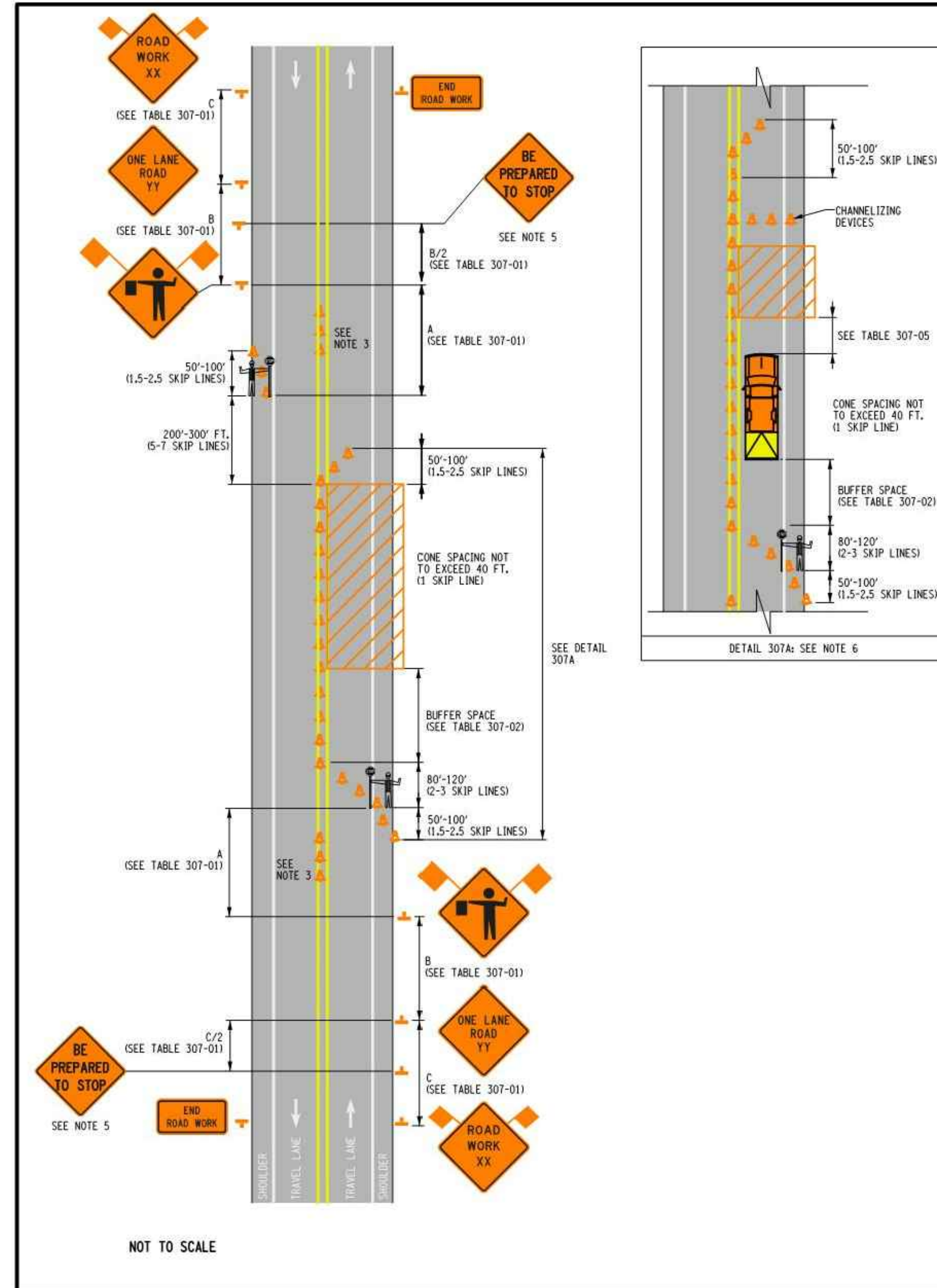
*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.

NEW YORK STATE OF OPPORTUNITY
Department of Transportation
U.S. CUSTOMARY STANDARD SHEET
WORK ZONE TRAFFIC CONTROL NON-FREEWAY SHOULDER CLOSURE SHORT TERM OPERATION
APPROVED DECEMBER 2, 2021
ISSUED UNDER E1 21-028
Robert Limoges, P.E., DIRECTOR, OTSM
619-310

1 SHORT DURATION SHOULDER CLOSURE
SCALE: NONE
CROSS REFERENCE: NONE

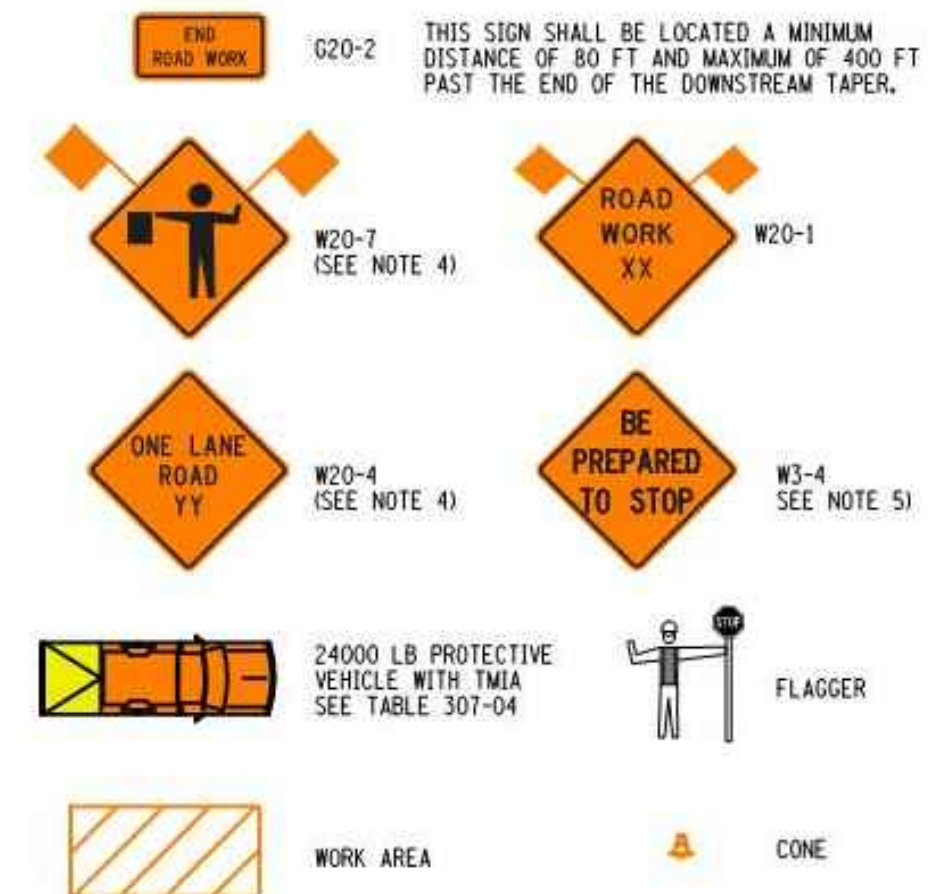
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SHEET 12 OF 14						
DWG. NO: 23-0157						



NOTES:

1. SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAY PERIOD.
2. IN URBAN CONDITIONS, ADVANCE WARNING SIGN SPACINGS MAY BE ADJUSTED IN ORDER TO ACCOMMODATE SIDE STREETS AND DRIVEWAYS. IF THERE IS A CONFLICT, MOVE THE SIGN UPSTREAM.
3. CENTERLINE CONES MAY BE ADDED TO ENHANCE THE VISIBILITY OF THE FLAGGER STATION. IF CONES ARE USED, PLACE THEM 100 FT. MINIMUM FROM FLAGGER.
4. FLAGGER SYMBOL SIGN (W20-7) AND "ONE LANE ROAD AHEAD" SIGN (W20-4) SHALL BE REMOVED, COVERED OR TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
5. IF THE TRAFFIC IS EXPECTED TO QUEUE PAST THE W20-4 SIGN, A W3-4 SIGN SHOULD BE ADDED.
6. IF CONDITIONS WARRANT, PROTECTIVE VEHICLE WITH APPROPRIATE ROLL AHEAD DISTANCE MAY BE USED IN ADVANCE OF THE WORK AREA. TO USE PROTECTIVE VEHICLE, BUFFER SPACE SHALL BE PROVIDED ACCORDINGLY.
7. WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE ROADWAY WITHIN A WORK ZONE TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES AND/OR FLAGGERS SHALL BE PLACED AS NEEDED. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK SPACE.
8. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 40' IN THE ACTIVE WORK SPACE.
9. CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY A MINIMUM OF EVERY 800' AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 800'.
10. ALL FLAGGERS SHALL USE 24" (MIN) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6" STAFF. THE PADDLE IS THE PREFERRED DEVICE, BUT THE FLAG MAY BE USED AT INTERSECTIONS WHERE THE STOP/SLOW PADDLE WOULD OFFER CONTRADICTING INFORMATION TO DRIVERS TRAVELING IN OPPOSITE DIRECTIONS/LEGS OF THE INTERSECTION OR DURING INCIDENT MANAGEMENT SITUATIONS.



REFER TO SHEET 2 OF 2 FOR ALL TABLES

NEW YORK
STATE OF OPPORTUNITY.

Department of Transportation

U.S. CUSTOMARY STANDARD SHEET

**WORK ZONE TRAFFIC CONTROL
TWO-LANE TWO-WAY ROADWAY
LANE CLOSURE WITH FLAGGERS
SHORT TERM OPERATION
(SHEET 1 OF 2)**

APPROVED DECEMBER 2, 2021 ISSUED UNDER ET 21-028

Robert Limoges
ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

619-307

FILE NAME = 619-307-1.dwg
DATE/TIME = 06-DEC-2021 15:59
USER = oboataright

NOT TO SCALE

1 SHORT DURATION LANE CLOSURE
SCALE: NONE
CROSS REFERENCE: NONE

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					DATE : MARCH 24, 2023

TRAFFIC AND MAINTENANCE CONTROL DETAILS

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GENIE SOLAR ENERGY**

TOWN OF LANSING TOMPKINS COUNTY, NEW YORK

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C-702
SHEET 13 OF 14
DWG. NO: 23-0157

TABLE 307-01: ADVANCE WARNING SIGN SPACING

ROAD TYPE	DISTANCE BETWEEN SIGNS			SIGN LEGEND	
	A (FT.)	B (FT.)	C (FT.)	XX	YY
URBAN (< 30 MPH)	100	100	100	AHEAD	AHEAD
URBAN (35-40 MPH)	200	200	200	AHEAD	AHEAD
URBAN (> 45 MPH)	350	350	350	1000 FT.	AHEAD
RURAL	500	500	500	1500 FT.	1000 FT.

* PRECONSTRUCTION POSTED SPEED LIMIT

TABLE 307-02: LONGITUDINAL BUFFER SPACE

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	LONGITUDINAL BUFFER SPACE DISTANCE (FT.) / # OF SKIP LINES
25	155/4
30	200/5
35	250/6
40	305/8
45	360/9
50	425/11
55	495/13

TABLE 307-03: REQUIRED SIGN SIZES*

SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
W3-4	36x36	48x48
W20-1	36x36	48x48
W20-4	36x36	48x48
W20-7	36x36	48x48
WARNING FLAG	18x18	18x18

*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.

TABLE 307-04: PROTECTIVE VEHICLE REQUIREMENTS

CLOSURE TYPE	ROAD TYPE & SPEED	NON-FREEWAY		
		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P, TMIA	P
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	P	SEE NOTE 2
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P	P
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	P	SEE NOTE 2

LEGEND

P: PROTECTIVE VEHICLE REQUIRED FOR EACH CLOSED LANE & EACH CLOSED PAVED SHOULDER 8' OR WIDER, IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE PROTECTIVE VEHICLE SHALL BE REPOSITIONED ACCORDINGLY

TMIA: TMIA REQUIRED

NOTES:

- THE EXPOSURE CONDITIONS ASSUMES THERE IS NO POSITIVE PROTECTION PRESENT
- EITHER A PROTECTIVE VEHICLE OR THE STANDARD BUFFER SPACE SHALL BE PROVIDED

TABLE 307-05: ROLL AHEAD DISTANCE

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	STATIONARY OPERATION	
	MIN	MAX
≥ 55	120/3	200/5
45 - 50	80/2	160/4
≤ 40	40/1	120/3

NEW YORK STATE OF OPPORTUNITY Department of Transportation

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY LANE CLOSURE WITH FLAGGERS SHORT TERM OPERATION (SHEET 2 OF 2)

APPROVED DECEMBER 2, 2021 ISSUED UNDER ET 21-028

Robert Limoges
ROBERT LIMOGES, P.E., DIRECTOR, OTSM

619-307

GENERAL NOTES:

- GENERAL:** ALL SIGN, CONES, BARRELS, BARRICADES AND CONC BARRIERS SHALL BE FABRICATED AND ERRECTED IN ACCORDANCE WITH THE CONTRACT PLANS AND SPECIFICATIONS OR AS DIRECTED BY THE NEW YORK STATE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES CONTRACTOR REQUIRED TO PROVIDE SIGNAGE PLAN TO ENGINEER AND NYS DOT PRIOR TO CONSTRUCTION.
- SIGN SUPPORT:** THE CONTRACTOR SHALL SUPPLY ADEQUATE SUPPORTS SO THAT THE SIGNS ARE IN PROPER POSITION AND ALIGNMENT AS SHOWN IN THE NEW YORK STATE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, ALL SUPPORTS SHALL BE PAINTED WITH TWO(2) COATS OF WHITE PAINT.
- SIGN PANELS:** THE SIGN PANELS MAY BE MADE OF ALUMINUM, STEEL, OR PLYWOOD THE BACKS OF ALL WOOD SIGN PANELS SHALL RECEIVE TWO(2) COATS OF WHITE PAINT.
- SIGN FACE:** COLOR - THE COLOR OF THE BACKGROUND AND THE LEGEND OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE NEW YORK STATE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THE MARGIN SHALL BE OF THE SAME COLOR AS SPECIFIED FOR THE BACKGROUND. THE BORDER SHALL BE OF THE SAME COLOR AS THE LEGEND. SHAPE - THE SHAPE OF ALL SIGNS SHALL BE AS SHOWN ON THIS SHEET. CORNERS OF SIGNS SHALL HAVE ROUNDED CORNERS. REFLECTORIZATION - ALL SIGNS SHALL BE REFLECTORIZED USING HIGH INTENSITY REFLECTORIZED TAPE OR PAINT. ALL SIGNS SHALL BE CLEANED AND MAINTAINED REGULARLY. ALL SIGNS NOT MEETING PROPER REFLECTION REQUIREMENTS WILL BE REPLACED. LETTERING AND BORDERS - SIGN LETTERING, BORDERS AND MARGINS SHALL BE IN ACCORDANCE WITH THE NEW YORK STATE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
- SIGN LOCATION:** SIGNS SHALL BE GENERALLY LOCATED ON THE RIGHT SIDE OF THE HIGHWAY FACING APPROACHING TRAFFIC. THE NEAR EDGE OF THE SIGN SHALL BE BETWEEN 6 AND 12 FEET FROM NEAREST EDGE OF THE TRAVELED ROADWAY OR BETWEEN 2 AND 12 FEET FROM THE FACE OF THE VERTICAL CURB. SIGNS SHOULD GENERALLY BE PLACED AT RIGHT ANGLES TO TRAFFIC. ROADSIDE SIGNS SHOULD BE MOUNTED SO THAT THE BOTTOM OF THE SIGN IS APPROXIMATELY 5 FEET ABOVE THE EDGE OF PAVEMENT. SIGNS MOUNTED ON BARRICADES OR TEMPORARY SIGNS IN THE ROADWAY MAY BE AT LOWER HEIGHTS. ALL SIGNS SHALL BE LOCATED SO AS TO BE PLAINLY VISIBLE TO TRAFFIC.
- MAINTENANCE:** THE CONTRACTOR SHALL KEEP SIGNS CLEANED AND CLEARED AT ALL TIMES. ALL SIGNS SHALL BE THE PROPERTY OF THE CONTRACTOR AND SHALL BE MAINTAINED IN GOOD CONDITION FOR THE DURATION OF THE CONTRACT. ALL SIGNS SHALL BE REMOVED FROM THE WORK SITE WHEN THE CONTRACT WORK IS ACCEPTED.

WORK DURATION DEFINITIONS

LONG-TERM STATIONARY IS WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS.

INTERMEDIATE-TERM STATIONARY IS WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.

SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.

SHORT DURATION IS WORK THAT OCCUPIES A LOCATION UP TO 1 HOUR.

MOBILE IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY.

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USER = abbas@cmale.com

1 SHORT DURATION LANE CLOSURE CHARTS
C-703 SCALE: NONE CROSS REFERENCE: NONE

NYS DOT STANDARD GENERAL PLAN NOTES:

- THE ROADWAY SHALL BE KEPT CLEAN OF MUD AND DEBRIS AT ALL TIMES.
- ROADSIDE DRAINAGE SHALL BE MAINTAINED AT ALL TIMES.
- MATERIALS, EQUIPMENT AND VEHICLES SHALL NOT BE STORED OR PARKED WITHIN THE NEW YORK STATE RIGHT-OF-WAY.
- WORK ZONE TRAFFIC CONTROL SHALL COMPLY WITH THE 2009 EDITION OF THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS AND THE NEW YORK STATE SUPPLEMENT, AND SHALL BE IN ACCORDANCE WITH THE NYS DOT CONTRACT OR HIGHWAY WORK PERMIT DOCUMENTS AND AS DEEMED NECESSARY BY THE NYS ENGINEER IN CHARGE.
- NOTIFY NEW YORK STATE DEPARTMENT OF TRANSPORTATION RESIDENT ENGINEER AT THE APPLICABLE RESIDENCY, THREE WORKING DAYS PRIOR TO WORKING IN THE STATE RIGHT-OF-WAY.
- NOTIFY DIG SAFELY NEW YORK THREE WORKING DAYS PRIOR TO DIGGING, DRILLING OR BLASTING AT 1-800-962-7962, FOR A UTILITY STAKE-OUT.
- ALL WORK CONTEMPLATED AND MATERIALS USED WITHIN THE NYS RIGHT-OF-WAY SHALL BE COVERED BY AND IN CONFORMITY WITH THE NYS DEPARTMENT OF TRANSPORTATION MAY 1, 2008 SPECIFICATIONS BOOK AND ANY SUBSEQUENT ADDENDA ALONG WITH ANY APPROPRIATE CURRENT NYS DEPARTMENT OF TRANSPORTATION STANDARD SHEETS, EXCEPT AS MODIFIED IN THESE PLANS AND IN THE ITEMIZED PROPOSAL. METRIC UNITS MAY BE CONVERTED TO ENGLISH.
- QUALITY CONTROL OF ASPHALT CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 402 OF THE STANDARD SPECIFICATIONS. ASPHALT COURSE DEPTHS SHOWN ON THE PLANS ARE COMPACTED DEPTHS.
- NO NIGHT WORK WILL BE ALLOWED UNLESS PRIOR APPROVAL IS GIVEN BY THE DEPARTMENT. ADDITIONAL MAINTENANCE AND PROTECTION OF TRAFFIC WILL BE REQUIRED INCLUDING THE ADDITION OF REFLECTIVE MATERIALS AND LIGHTING.
- HAZARDOUS WASTE NOTIFICATION - THE PERMITTEE ACCEPTS THE RIGHT-OF-WAY OF THE STATE HIGHWAY IN ITS' AS IS CONDITION. THE DEPARTMENT OF TRANSPORTATION MAKES NO REPRESENTATION AS TO THE ABSENCE OF UNDERGROUND TANKS, STRUCTURES, FEATURES OR SIMILAR IMPEDIMENTS TO THE COMPLETION OF THE WORK PERMITTED HEREUNDER. SHOULD PERMITTEE FIND SOME PREVIOUSLY UNKNOWN UNDERGROUND IMPEDIMENTS TO ITS WORK, THE DEPARTMENT OF TRANSPORTATION SHALL HAVE NO OBLIGATION TO CURE, REMOVE, REMEDY OR OTHERWISE DEAL WITH SUCH PREVIOUSLY UNKNOWN UNDERGROUND IMPEDIMENTS. THE DEPARTMENT WILL PERMIT THE PERMITTEE TO REMOVE, MODIFY OR OTHERWISE DEAL WITH SUCH UNDERGROUND TANKS, STRUCTURE FEATURE OR IMPEDIMENT IF SUCH IS DONE IN A MANNER WHICH MEETS ACCEPTABLE ENGINEERING PRACTICE AND IS PRE-APPROVED BY THE DEPARTMENT OF TRANSPORTATION. SHOULD PERMITTEE DETERMINE THAT SUCH UNFORESEEN UNDERGROUND IMPEDIMENT RENDERS PERMITTEES WORK AS AUTHORIZED BY THIS PERMIT UNFEASIBLE, PERMITTEE SHALL HAVE THE OPTION OF RESTORING THE HIGHWAY TO ITS ORIGINAL CONDITION AND NOT PERFORMING SUCH WORK.
- OPEN CUTTING OF THE ROADWAY SHALL NOT BE ALLOWED UNLESS PERMISSION IS GRANTED IN WRITING, BY THE REGIONAL TRAFFIC ENGINEER.

TEMPORARY LANE/SOULDER CLOSURE RESTRICTIONS FOR HOLIDAYS 2023

HOLIDAY	FALLS ON	TEMPORARY LAND CLOSURES ARE NOT ALLOWED FROM
NEW YEAR'S DAY	SUNDAY JANUARY 1	BEGINNING 6 AM FRIDAY DECEMBER 30, 2022, AND ENDING 6 AM MONDAY JANUARY 2, 2023
MEMORIAL DAY	MONDAY MAY 29	BEGINNING 6 AM FRIDAY MAY 27, 2023, AND ENDING 6 AM TUESDAY MAY 30, 2023
INDEPENDENCE DAY	TUESDAY JULY 4	BEGINNING 6 AM FRIDAY JUNE 30, 2023, AND ENDING 6 AM WEDNESDAY JULY 5, 2023
LABOR DAY	MONDAY SEPTEMBER 4	BEGINNING 6 AM FRIDAY SEPTEMBER 1, 2023, AND ENDING 6 AM TUESDAY SEPTEMBER 5, 2023
THANKSGIVING DAY	THURSDAY NOVEMBER 23	BEGINNING 6 AM WEDNESDAY NOVEMBER 23, 2023, AND ENDING 6 AM MONDAY NOVEMBER 27, 2023
CHRISTMAS DAY	MONDAY DECEMBER 25	BEGINNING 6 AM FRIDAY DECEMBER 22, 2023, AND ENDING 6 AM TUESDAY DECEMBER 26, 2023

PRELIMINARY

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