



Pinnacle on Sixth

Justification Statement

First Re-Submission: June 12, 2026

Second Re-Submission: July 17, 2026



Urban Design
Land Planning
Landscape Architecture

REQUEST

Urban Design Studio, (UDS), on behalf of the Applicant, is pleased to present this application to request approval for a planned development, to be known as “Pinnacle on Sixth” for 92 residential affordable/workforce housing units south of 6th Avenue South at South H Street. While the Applicant has the ability to achieve the 92 units by-right through the Live Local Act, the Applicant believes the discussion and relationship with the City of Lake Worth Beach (the “City.”) is critical to a successful development and therefore is requesting approval for a rezoning to Multiple Use Urban Planned Development (MUUPD). The proposed applications include the following:

- Rezoning Mixed Use – Dixie Highway (MU-DH) to Multiple Use Urban Planned Development (MUUPD)
- Major Site Plan
- Conditional Use Permit
- Sustainable Bonus Incentive Program – Tiers 1 & 2
- Affordable/Workforce Housing Program.
- Two Waivers to the City’s Code

APPLICATION SUMMARY

Applicant: Pinnacle on Sixth, LLC

Property Owners: Dixie Capital Partners, LLC

Addresses: 610, 615, 617, 621, 622, 625, 626, 629, 630 and 702 South H Street

PCNs: 38-43-44-21-15-203- 0100/ 0130/ 0140/ 0150/ 0160
38-43-44-21-15-205-0010 / 0020 / 0030/ 0040
38-43-44-21-15-223-0080

Site Location: West and east sides of South H Street between 6th Avenue South and 7th Avenue South, plus a single lot at the southeast corner of South H Street and 7th Avenue South

Size: 2.01 gross acres

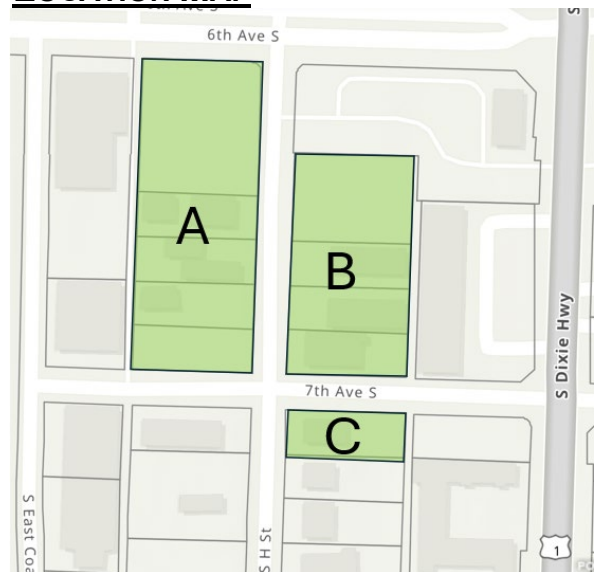
Existing Land Use: Vacant land that was formerly residential and multi-family residential

Future Land Use Designation: Mixed Use – East (MU-E)

Existing Zoning District: Mixed Use – Dixie Highway (MU-DH)

Proposed Zoning District: Multiple Use Urban Planned Development (MUUPD)

LOCATION MAP



SURROUNDING PROPERTY

	FLU	ZONING	EXISTING
NORTH	AMU	AI	Light Ind.
EAST	MU-E	MU-DH	Comm.
SOUTH	P	P	Vacant
WEST	AMU	AI	Light Ind.

SITE BACKGROUND

The project area consists of thirteen (13) platted lots within the boundaries of the Community Redevelopment Area (CRA). Some of the lots have been joined and the ten (10) parcel addresses include 610, 615, 617, 621, 622, 625, 626, 629, 630 and 702 South H Street. Below is a summary of the properties based on Palm Beach Property Appraiser's records:

610 South H Street - this .31 acre site was previously improved with a City electrical transfer station which has been removed, and the site is currently vacant.

615, 617, 625, 629 South H Street - these combined sites consist of .92 acres and are currently vacant, however appear to have been previously developed with residential use and parking.

621, 622, 626, 630, 702 South H Street - these combines sites consist of .78 acres and are improved with multifamily residential housing and related parking.

PROJECT DESCRIPTION

While the Applicant has the ability to achieve the 92 units by-right through the Live Local Act, Pinnacle on Sixth will unify thirteen (13) platted lots/ten (10) parcels with a Multiple Use Urban Planned Development (MUUPD) to provide 100% of the proposed 92 units for affordable/workforce housing. The project is located on the west and east sides of South H Street between 6th Avenue South and 7th Avenue South, plus a single lot at the southeast corner of South H Street and 7th Avenue South (see prior Location Map). For this request, the property on the west side of South H Street is identified as Parcel/Building A. The property on the east side of South H Street is identified as Parcel/Building B and the single lot is identified as Parcel C. The proposed development consists of one building on Parcels A and one building on B and no building on Parcel C. Building A is proposed at 5 stories (49'-8" to roof deck and 59'-8" to the top of rooftop appurtenances) with 57 multifamily residential units and supported with 83 on-site parking spaces with access from 7th Avenue North and widened alley. Building B is proposed at 4 stories (39'-0" to the roof deck and 49'-0" to the top of rooftop appurtenances) with 35 multifamily residential units and supported with 42 on-site parking spaces with access from South H Street and the widened alley. Parcel C is proposed for a small passive park and 3 parking spaces with access from 7th Avenue North. The intent of the Applicant is that each Parcel A and Parcel B achieve more than one parking space per unit in each Parcel independently. In addition to on-site parking, there are 8 bicycle parking spaces located on Parcel A and 12 proposed parallel parking spaces along South H Street, that the Applicant did not count to meet the 109 parking space requirement.

The Pinnacle on Sixth property is currently designated with a future land use of Mixed Use East and zoning of Mixed Use – Dixie Highway. The designations and existing uses of the surrounding properties are provided in the table above. The proposed use is permitted by-right with the density proposed under the Live Local Act due to its existing zoning of Residential, however the Applicant opted for the rezoning to a Multiple Use Urban Planned Development after consultation with the City.

This application proposes to amend the zoning from MU-DH to a Multiple Use Urban Planned Development, consistent with LDR Sec. 23.3-25, as well as incorporate other City bonus programs as outlined below:

- Multiple Use Urban Planned Development (MUUPD) with two buildings - one that is 5 stories and the other 4 stories with a total of 92 dwelling units, all of which will be designated and restricted for Affordable/Workforce Housing.
- Conditional Use Permit and Major Site Plan for a residential development area greater than 7,500 square feet.
- Sustainable Bonus Incentive Program (SBIP) for additional density, intensity (FAR), and height.
- Affordable/Workforce Housing Program (AWFH) for additional density, reduction in the gross living area for dwelling units, and reduction of the required parking.

Two waivers are being requested from the City's code sections. Waiver 1 is to waive the City's Code Section 234.10 (j) Minimum Parking Dimensions which requires a standard parking space to measure nine (9) feet in width and eighteen (18) feet in length, and compact spaces to measure eight (8) feet by sixteen (16) feet. The Applicant is requesting a two (2) foot parking overhang for all standard and compact parking spaces to function as a landscape buffer to achieve the minimum code required depth of the parking stall dimensions. The proposed standard parking space is measured as nine (9) feet in width and sixteen (16) feet in length with a two (2) foot parking overhang and compact spaces to measure eight (8) feet by fourteen (14) feet with a two (2) foot parking overhang. The parking overhang is providing a benefit for additional pervious area for the entire site.

Waiver 2 is being requested to waive the City's Code Section 23.6-1 (c) (2) (b) which requires a five (5) foot landscape strip to be located between the City's alley and off-street parking areas or other vehicular use areas. The waiver request is to allow pavement instead of the required five (5) foot landscape strip located between the off-street parking areas and the three (3) adjacent alleys. There is no public benefit to providing this required landscape strip as the parcels are being improved as one planned development which the development reflects planned development standards that create a more desirable and attractive development in the City.

APPLICATION PREPARATION

The applications were prepared based on review and implementation of the City of Lake Worth Beach Land Development Regulations (LDRs) including the City's Major Thoroughfare Design Guidelines, and for consistency with the Comprehensive Plan and Strategic Plan. This review will confirm that the proposed Multiple Use Urban Planned Development is consistent with the Comprehensive Plan, Strategic Plan, and LDRs.

Consistency with the Comprehensive Plan and Strategic Plan - The subject properties currently have a Future Land Use (FLU) designation of Mixed Use East (MU-E). Per Comprehensive Plan Future Land Use Policy 1.1.1.5, the MU-E FLU is intended to provide for a mixture of residential, office, service and commercial retail uses within specific areas east of I-95, near or adjacent to the central commercial core and major thoroughfares of the City. The maximum density of permitted residential development without further bonus programs is 30 dwelling units per acre. *Zoning regulations implementing the Mixed Use East category shall permit the establishment and expansion of residential (including single family, two-family and multi-family), office, service and commercial retail uses either as uses permitted by right or through conditional use permit provisions. All buildings are required to provide transitional buffering and design features to mitigate impact of the MU-E sites adjacent to residential zoning districts."*

The City's Strategic Plan focuses on fostering safer neighborhoods, encouraging community pride, building a vibrant and diverse economy, planning for the future, and enhancing the natural, historic, and cultural environment of the City.

RESPONSE: The proposed Pinnacle on Sixth Multiple Use Urban Planned Development will allow for the redevelopment of multiple parcels with the construction of 92 affordable/workforce residential units and associated amenities, as well as site improvements including buffers to adjacent sites, consistent with the City's Comprehensive Plan and Strategic Plan, and is the same maximum permitted density (46 units per acre) if the Live Local Act were to be applied.

Consistency with the Land Development Regulations (LDRs) - The proposed application for Pinnacle on Sixth has been prepared with the intent of consistency with all applicable requirements in the City's Land Development Regulations (LDR), including the Mixed Use – Dixie Highway zoning district, Multiple Use Urban Planned Development, Sustainability Bonus Incentive Programs (SBIP - Tiers I & II) and, Affordable and Workforce Housing Program (AWFH).

APPLICATION STANDARDS ANALYSIS

This request proposes the following applications and complies with the standards or criteria of each application as indicated below:

- a Planned Development to unify the property and utilize the flexibility of regulations,
- a Conditional Use Permit due to the property being greater than 7,500 square feet,
- Sustainability Bonus Incentive Programs as they relate to additional density, intensity and building height,
- Affordable Workforce Housing Program

Sec. 23.2-36. Rezoning of Land

A request for rezoning that changes the actual zoning map designation of a parcel, or parcels of land shall be considered pursuant to the following procedure:

- (3) Review criteria. An amendment to the official zoning map processed without the FLUM amendment shall be reviewed based on the following factors:
 - a. Consistency. Whether the proposed rezoning amendment would be consistent with the purpose and intent of the applicable comprehensive plan policies, redevelopment plans, and land development regulations. Approvals of a request to rezone to a planned zoning district may include limitations or requirements imposed on the master plan in order to maintain such consistency.

RESPONSE: *The subject properties currently have a Future Land Use (FLU) designation of Mixed Use East (MU-E). Per Comprehensive Plan Future Land Use Policy 1.1.1.5, the MU-E FLU is intended to provide for a mixture of residential, office, service and commercial retail uses within specific areas east of I-95, near or adjacent to the central commercial core and major thoroughfares of the City. The maximum density of permitted residential development without further bonus programs is 30 dwelling units per acre. Rezoning to a Multiple Use Urban Planned Development (MUUPD) allows for additional density, intensity and flexibility in design.*

The proposed Pinnacle on Sixth Multiple Use Urban Planned Development, along with other bonus programs, will allow for the redevelopment of multiple parcels with the construction of 92 affordable/workforce residential units and associated amenities between commercial uses to the east and industrial uses to the west, south of the 6th Avenue South major thoroughfare, consistent with the City's Comprehensive Plan and Strategic Plan. The proposed density is 45.67 dwelling units per acre within two buildings at 4 and 5 stories, based on the MUUPD bonus program, the AWFH bonus program and SBIP Tiers I and II, all of which are incorporated in the City LDR's and have been found to be consistency with the Comprehensive Plan.

- b. Land use pattern. Whether the proposed rezoning amendment would be contrary to the established land use pattern or would create an isolated zoning district unrelated to adjacent and nearby classifications or would constitute a grant of special privilege to an individual property owner as contrasted with the protection of the public welfare. This factor is not intended to exclude rezoning that would result in more desirable and sustainable growth for the community.

RESPONSE: *The existing Mixed Use- Dixie Highway zoning district allows for a variety of uses including the proposed multi-family residential development. The rezoning to ` allows*

for flexibility and additional density, intensity and building height which is necessary to provide for the more desirable and sustainable growth of affordable/workforce housing product to serve the community and is not contrary to the established land use pattern of the surrounding neighborhood.

- c. Sustainability. Whether the proposed rezoning would support the integration of a mix of land uses consistent with smart growth or sustainability initiatives, with an emphasis on 1) complementary land uses; 2) access to alternative modes of transportation; and 3) interconnectivity within the project and between adjacent properties.

RESPONSE: *Pinnacle on Sixth is located strategically on 6th Avenue South and one block away from Dixie Highway, between commercial and industrial uses that could provide employment opportunities close to Pinnacle on Sixth. Additionally, there are bus routes on both streets less than 0.10 miles from the development which provide alternative modes of transportation for the future residents. The Lake Worth Beach Tri-Rail Station is also less than 1 mile from the subject property, providing commuting opportunities to residents. Interstate 95 is less than .75 miles.*

- d. Availability of public services/infrastructure. Requests for rezoning to planned zoning districts shall be subject to review pursuant to section 23.5-2.

RESPONSE: *All public services/infrastructure, including streets, utilities, police, fire, libraries and schools are evaluated with this application, however anticipated to be available to serve the proposed Pinnacle on Sixth development.*

- e. Compatibility. The application shall consider the following compatibility factors:
1. Whether the proposed rezoning would be compatible with the current and future use of adjacent and nearby properties or would negatively affect the property values of adjacent and nearby properties.
 2. Whether the proposed rezoning is of a scale which is reasonably related to the needs of the neighborhood and the city as a whole.

RESPONSE: *As indicated, the proposed Pinnacle on Sixth is located between commercial uses to the east and industrial uses to the west, however the property has historically been used for lower density multifamily residential housing. The proposed development complies with the intent of the MU-E Future Land Use and MU-Dixie Highway zoning district to provide higher density residential housing. Proposing 100% affordable/workforce housing units contributes to the deficit of attainable housing not only in Lake Worth Beach, but also Palm Beach County and beyond.*

- f. Direct community sustainability and economic development benefits. For rezoning involving rezoning to a planned zoning district, the review shall consider the economic benefits of the proposed amendment, specifically, whether the proposal would:
1. Further implementation of the city's economic development (CED) program.
 2. Contribute to the enhancement and diversification of the city's tax base.
 3. Respond to the current market demand or community needs or provide services or retail choices not locally available.
 4. Create new employment opportunities for the residents, with pay at or above the county average hourly wage.
 5. Represent innovative methods/technologies, especially those promoting sustainability.
 6. Support more efficient and sustainable use of land resources in furtherance of overall community health, safety and general welfare.
 7. Be complementary to existing uses, thus fostering synergy effects; and

8. Alleviate blight/economic obsolescence of the subject area.

RESPONSE: *Pinnacle on Sixth is a 100% affordable/workforce housing development this is committed to providing attainable housing with rent controls for decades. These 92 attainable units will be available to and will benefit the livelihood of eligible residents of the City of Lake Worth Beach, with new accommodations and with direct access to multiple alternative modes of transportation.*

- g. Economic development impact determination for conventional zoning districts. For rezoning involving rezoning to a conventional zoning district, the review shall consider whether the proposal would further the economic development program, and also determine whether the proposal would:
1. Represent a potential decrease in the possible intensity of development, given the uses permitted in the proposed land use category and/or zoning district; and
 2. Represent a potential decrease in the number of uses with high probable economic development benefits.

RESPONSE: *The rezoning is to a planned development and not a conventional zoning district there, this standard does not apply.*

Sec. 23.3-17. - MU-DH—Mixed use-Dixie Highway.

See Attachment B for the responses to this code section.

Sec. 23.3-25. Planned Development District

- a) *Intent.* The intent of this section is to encourage, through height, density and/or intensity incentives, the use of innovative land planning and development techniques to create more desirable and attractive development in the city. Incentives include but are not limited to:
- (1) Relaxing or waiving of setbacks, parking requirements, lot dimensions and lot area requirements.
 - (2) Allowing a decrease in minimum living area per dwelling unit; and
 - (3) Permitting uses or a mixture of uses not normally permitted in the underlying zoning district.

Planned development districts can occur in all mixed use, residential, commercial, and industrial districts. The increase in density contemplated shall not exceed the density provided in the Lake Worth Comprehensive Master Plan.

RESPONSE: A Mixed Use Urban Planned Development (MUUPD), per Section 23.3-25 c), encourages, through height, density and/or intensity incentives, the use of innovative land planning and development techniques to create more desirable and attractive development in the city.

The proposed Pinnacle on Sixth MUUPD is consistent with Section 23.3-25. The primary purpose of the MUUPD is to consolidate the properties into one development plan, utilizing the incentives of the MUUPD, SBIP, AWFH bonus programs. These incentives include additional density, building height, floor area ratio (FAR) and reductions to unit size. There is no proposed reduction to unit sizes, however the AWFH program allows a reduction in the number of parking spaces, which is applied to the project as further described below. The incentives included in the proposal for Pinnacle on Sixth are outlined below with calculations provided in Exhibit A and on the Site Plan.

- Density - the base density of the MU-DH zoning district is 30 dwelling units per gross acre. Then with the cumulation of the 25% planned development bonus, plus the 15% AWFH Program, the allowed maximum density is 42 units per gross acre or 84 units. In addition, when in the CRA

boundary, the project can include 1 additional unit for each platted lot, which then equals a maximum total of 97 units allowed for the project. The number of units for the proposed project is 92, consistent with the by-right density if the Live Local Act is applied to the existing zoning district.

- Building Height - the base building height of the MU-DH zoning district is 30 feet and 2 stories, however under the Sustainability Bonus Incentive Program (SBIP), the height can be increased by 15 feet and 2 stories. Under a Planned Development, the height can be increased an additional 25% to 5 stories and a total of 56.25 feet to the roof deck, plus an additional 10'-0" is permitted by code for rooftop appurtenances. Building A is proposed for 5 stories, 49'-6" to the roof deck and 59'-6" to the top of roof top appurtenances. Building A is proposed for 4 stories, 39'-0" to the roof deck and 49'-0" to the top of roof top appurtenances. Accordingly, the buildings comply with the maximum allowed height.
- Floor Area Ratio (FAR) - the base FAR of the combined property of 87,750 square feet is .90, however under a Planned Development and with the Sustainability Bonus Incentive Program (SBIP), FAR can be increased to a maximum of 1.65. The FAR of the proposed project is 1.21.
- Unit Size - minimum unit sizes within the MU-DH zoning district are 600 square feet for 1 bedroom and 750 square feet for a 2 bedroom unit. Under the AWFH program, the units can be reduced by 15%. The proposed 1 bedroom units are 647 square feet, and the 2 bedroom units are 883 square feet and therefore do not propose reductions.
- Parking - standard parking rates are 1.5 spaces per 1 bedroom unit and 1.75 spaces per 2 bedroom unit. Under the AWFH program, the number of parking spaces can be reduced by 25%. The required parking for the project with the 25% reduction is 109 spaces and there are 109 spaces provided on the proposed site. In addition, there is bike parking on site and the potential for 12 parallel parking spaces along South H Street, however those are not counted in the project calculations.

The MU-DH zoning district is *intended to provide the establishment and expansion of a broad range of office and commercial uses, including higher density residential use*. The proposed Pinnacle on Sixth project fulfills the district's intention for higher density multifamily residential uses that will be designated for Affordable/Workforce housing. The project proposes 92 units on the 2.01+/- acre property, at a density of 45.67 dwelling units per acre and therefore complies with applicable zoning requirements and bonus incentives as shown in Attachment A.

See Attachment C for the remaining responses to Code Section 23.3-25.

Sec. 23.2-29. Conditional use permits.

The proposed Pinnacle on Sixth exceeds 7,500 square feet of use and therefore is subject to the criteria of a Conditional Use permit as outlined below.

d) *General findings relating to harmony with LDRs and protection of public interest.* Prior to approving any conditional use permit, the decision making authority shall find based on competent and substantial evidence that:

1. The conditional use exactly as proposed at the location where proposed will be in harmony with the uses which, under these LDRs and the future land use element, are most likely to occur in the immediate area where located.

RESPONSE: *The proposed Pinnacle on Sixth will provide needed affordable/workforce housing within proximity of employment opportunities of the commercial uses to the east and industrial uses to the west, as well as adjacent to or close proximity to major thoroughfares which offer alternative modes of transportation. The project will improve the area with new development, replacing vacant or underutilized land.*

2. The conditional use exactly as proposed at the location where proposed will be in harmony with existing uses in the immediate area where located.

RESPONSE: *The proposed Pinnacle on Sixth will provide needed affordable/workforce housing within proximity of employment opportunities of the commercial uses to the east and industrial uses to the west.*

3. The conditional use exactly as proposed will not result in substantially less public benefit or greater harm than would result from use of the site for some use permitted by right or some other conditional use permitted on the site.

RESPONSE: *The proposed Pinnacle on Sixth is providing 100% affordable/workforce housing with 92 units that will only benefit the community.*

4. The conditional use exactly as proposed will not result in more intensive development in advance of when such development is approved by the future land use element of the comprehensive plan.

RESPONSE: *The proposed Pinnacle on Sixth utilizes the bonus programs of the LDR's to achieve a development project that can provide attainable as intended by the Comprehensive Plan.*

- e) *Specific findings for all conditional uses.* Prior to approving any conditional use, the decision making authority shall find that:

1. The proposed conditional use will not generate traffic volumes or movements which will result in a significant adverse impact or reduce the level of service provided on any street to a level lower than would result from a development permitted by right.

RESPONSE: *A traffic study has been prepared and submitted with this application as well as directly to Palm Beach County for review and confirmation that the proposed development does not reduce the level of service.*

2. The proposed conditional use will not result in a significantly greater amount of through traffic on local streets than would result from a development permitted by right and is appropriately located with respect to collector and arterial streets.

RESPONSE: *The proposed development is based on bonus programs to increase density and intensity which will result in additional traffic than that permitted by right, however it addresses a greater need in providing 100% affordable/workforce housing. A traffic study has been prepared and submitted with this application as well as directly to Palm Beach County for review and confirmation that the proposed development does not reduce the level of service.*

3. The proposed conditional use will not produce significant air pollution emissions or will appropriately mitigate anticipated emissions to a level compatible with that which would result from a development permitted by right.

RESPONSE: *As a residential project, the proposed Pinnacle on Sixth will not produce any significant air pollution emissions.*

4. The proposed conditional use will be so located in relation to the thoroughfare system that neither extension nor enlargement nor any other alteration of that system in a manner resulting in higher net public cost or earlier incursion of public cost than would result from development permitted by right.

RESPONSE: *The proposed Pinnacle on Sixth fronts on 6th Avenue South and is one block west of Dixie Highway, both of which are considered Major Thoroughfare roadways in the City's LDR's, and no improvement are required from the traffic generated by the project.*

5. The proposed conditional use will be so located in relation to water lines, sanitary sewers, storm sewers, surface drainage systems and other utility systems that neither extension nor enlargement nor any other alteration of such systems in a manner resulting in higher net public cost or earlier incursion of public cost than would result from development permitted by right.

RESPONSE: *Engineering plans have been prepared and submitted with this application for review and confirmation that the existing utilities will adequately serve the proposed development and all costs of connecting to these utilities will be the responsibility of the Applicant.*

6. The proposed conditional use will not place a demand on municipal police or fire protection service beyond the capacity of those services, except that the proposed facility may place a demand on municipal police or fire protection services which does not exceed that likely to result from a development permitted by right.

RESPONSE: *This application will be reviewed by City staff to confirm that there will be no adverse demand on municipal police and fire protection services.*

7. The proposed conditional use will not generate significant noise or will appropriately mitigate anticipated noise to a level compatible with that which would result from a development permitted by right. Any proposed use must meet all the requirements and stipulations set forth in section 15.24, Noise control.

RESPONSE: *As a residential project, the proposed Pinnacle on Sixth will not produce any significant noise.*

8. The proposed conditional use will not generate light or glare which encroaches onto any residential property in excess of that allowed in section 23.4-10, Exterior lighting.

RESPONSE: *A photometric plan has been prepared and submitted for review to confirm compliance with Section 23.4-10.*

Sec. 23.2-39. Affordable/workforce housing program.

- a) *Intent.* The affordable/workforce housing program is intended to implement Objective 3.1.2 of the city comprehensive plan future land use element and provisions therein regarding affordable and workforce housing. The affordable/workforce housing program provides for a density bonus and a reduction in overall housing unit areas for developments that incorporate residential units with restrictive covenants that meet the requirements of the program.

RESPONSE: Pinnacle on Sixth is a development funded for 100% affordable/workforce housing. The proposed development has utilized the incentives in Tier 1 of a 15% increase and a Tier 2 15% increase in overall density and a 25% reduction in required parking to achieve 100% of the 92 total units as affordable/workforce housing and 109 required parking spaces. The calculations are provided in Attachment A as well as on the Site Plan.

- d) *Qualifying income restrictions.* The following provisions outline the required income limits and overall percentage of household income to qualify units as being affordable/workforce under the affordable/workforce housing program. All income values shall be based on the then current area (County) median household income published annually by the U.S. Department of Housing and Urban Development. Whether with a rental unit or for a fee simple, for sale unit, the overall housing expense (rent, mortgage, property taxes, and insurances) for the unit shall not exceed thirty (30) percent of the income limit provided for each unit type, based upon the number of bedrooms.
 1. For a studio unit, the annual gross household income shall not exceed forty-five (45) percent of area median income and minimum household size is one (1) person, not to exceed two (2) people.
 2. For a one-bedroom unit, the annual gross household income shall not exceed sixty-five (65) percent of the area median income and minimum household size of one (1) person, not to exceed two (2) people.

3. For a two-bedroom unit, the annual gross household income shall not exceed eighty-five (85) percent of the area median income and minimum household size of two (2) people, not to exceed two (2) people per bedroom.

RESPONSE: Pinnacle on Sixth offers one-bedroom and two-bedroom units which will comply with the qualifying income restrictions as indicated above.

All proposed units are affordable at or below 70% of the County-wide area median income for a period of no less than fifty (50) years. Further breakdown as follows:

- 14 units at 30% of County AMI (\$35,070 for a family of 4, based a sliding scale according to household size)
- 36 units at 60% of County AMI (\$70,140 for a family of 4)
- 42 units at 70% of County AMI (\$81,830 for a family of 4)

Sec. 23.2-33. Sustainable Bonus Incentive Program.

- a) *Intent.* The City of Lake Worth Beach Sustainable Bonus Incentive Program is intended to implement Objective 1.2.3 of the city comprehensive plan future land use element and provisions therein regarding a Sustainable Bonus Incentive Program. This incentive program offers the opportunity to attain an option for increased height, as provided in the comprehensive plan future land use element, an option for increased intensity (measured by floor area ratio (FAR)), and an option for increased density for a planned development (as specified in the Planned Development District section of the Land Development Regulations), and any combination thereof, within certain zoning districts in exchange for the incorporation of sustainable design features, community based improvements and overall design excellence as part of a development proposal.

RESPONSE: Per the City's Comprehensive Plan, a Multiple Use Urban Planned development may obtain a 25% bonus on density, intensity (FAR), and height over the base line as outlined in Table 1 of the Comprehensive Plan. The proposed project has implemented a bonus height, a maximum density of 46 units per acre, including 1 lot per platted lot in the CRA for a maximum of 97 units, and a floor area ratio (FAR) of 1.65. The proposed project has 92 units between the two buildings, which are 49'-6"/5 stories to the roof deck and 39'-0"/4 stories to the roof deck respectively with a project FAR of 1.21 - all less than the maximum allowance that can be permitted for intensity through a sustainable bonus incentive in a planned development in the MU-DH zoning district.

As indicated in the table below, the total square footage of the additional proposed bonus area above the second floor is ± 59,550 square feet in total (± 46,140 square feet through Tier 1 SBIP for the Buildings A and B third and fourth stories and ± 13,410 square feet through Tier 2 SBIP for Building A fifth story). Therefore, the value of required improvements for the SBIP bonus areas are \$346,050 (46,140 sf X \$7.50 per sf), plus an additional \$201,150 (13,410 sf X \$15 per square foot). The total combined value of both SBIP Tiers is \$547,200.

Sustainability Bonus Calculation						
Above Level 2	Building A - Square Feet			Building B - Square Feet		
	SF	Tier I	Tier II	Total	Tier I	Tier II
Level 3	13,410	26,820	13,410	9,660	19,320	
Level 4	13,410			9,660		
Level 5	13,410					
TOTALS	40,230			19,320		
Rate		\$7.50	\$15.00		\$7.50	
SBIP Fee	\$547,200	\$201,150	\$201,150		\$144,900	
City Fee - 50%	\$273,600					
Site Features	\$273,600					

Fifty percent (50%) of this value must be paid to the City (\$273,600), unless otherwise approved.

The applicant respectfully submits that this project is eligible for 100% award in on-site features or improvements as further justified by the applicable qualifying features or improvements outlined below as responses to the applicable provisions of LDR Sec. 23.2-33.d).

d) *Qualifying sustainability features or improvements.* The following features or improvements may qualify for the incentive award of either bonus height or intensity, or both. In order to qualify for each incentive award under subsections 1.(d) through 1.(h), the total value of the qualifying features or improvements must equal at least the amount of the fee-in-lieu established by the city commission pursuant to subsection e) below.

1. *On-site features or improvements.*

(a) LEED certification or other nationally recognized and accredited sustainable rating program shall entitle the applicant to one hundred (100) percent of the incentive award regardless of the number of additional stories or additional square feet above the initial two stories.

RESPONSE: *Pinnacle Communities as a developer and this project will be designed and constructed in accordance with National Green Building Standards (NGBS) as documented in the attached third party letter from Abney + Abney Green Solutions as well as the NGBS Green Certification issued in March 2026. This feature in itself should entitle 100% of the incentive award as indicated.*

(f) Public amenity such as a law enforcement substation, cultural gallery, public plaza, community meeting space, library, or garden.

RESPONSE: *Pinnacle on Sixth is providing a community park as part of the project, located at the southeast corner of South H Street and SE 7th Avenue. The park will serve as a meeting space and dog walk area that will serve the residents of the project as well as surrounding community residents.*

(h) Other project components open to the public, or offering a direct community benefit meeting the intent of the comprehensive plan, which are similar to those listed as part of the USGBC's LEED for neighborhood development program, or which include elements of sustainable design such as:

vii. Character and aesthetic excellence.

RESPONSE: *The redevelopment of this area will eliminate undeveloped and blighted properties creating heightened character and aesthetic improvements to the area.*

viii. Urban form and density.

RESPONSE: *The architectural design of Pinnacle on Sixth includes distinctive urban form and density with contemporary style and architecture features, including a artistic mural.*

xii. Bicycle mobility systems.

RESPONSE: *Pinnacle on Sixth includes bike storage to encourage residents to use alternative means of mobility, providing a direct community benefit of less vehicular traffic.*

xiii. Pedestrian mobility systems.

RESPONSE: *The design of Pinnacle on Sixth includes a pedestrian sidewalk system that not only connects residents to all parts of the project, but also extended to the public sidewalk system which lead to the public bus routes along 6th Avenue South and Dixie Highway, as well as local employment opportunities, shopping, restaurants and entertainment.*

xvi. Workforce housing.

RESPONSE: *All units in Pinnacle on Sixth will be affordable at an average of sixty percent (60%) of the area-wide median income as required under Section 42 of the IRS code governing the Housing Tax Credit program, under which Pinnacle on Sixth will be financed. Units in the development will be affordable at varying income tiers between 30% and 70% of area-wide median income.*

xviii. Housing diversity and accessibility.

RESPONSE: *Pinnacle on Sixth will provide a minimum of 5% of the units as ADA compliant.*

xxi. Energy efficiency and conservation.

RESPONSE: *For compliance with the NGBS certification requirements, the residential units will include Energy Star and energy efficient appliances and features. The project also incorporates the use of permeable pavers/pavement for parking which provides an aesthetic improvement and a more ecological method for paving.*

Again, as indicated above, the applicant respectfully submits that this project is eligible for 100% award in on-site features or improvements as further justified by the applicable qualifying features or improvements outlined below as responses to the applicable provisions of LDR Sec. 23.2-33.d).

Sec. 23.2-30. Site plan review.

With the project exceeding 7,500 square feet of use, a Site Plan is required and has been prepared and submitted for review for compliance with the City LDR's.

Section 23.2-31 - Site Design Qualitative Standards

Site Design Qualitative Standards are intended to "promote safety and minimize negative impacts of development on its neighbors by establishing qualitative requirements for the arrangements of buildings, structures, parking areas, landscaping and other site improvements. The qualitative standards are designed to ensure that site improvements are arranged in ways which cannot be otherwise accomplished with quantitative standards." These qualitative standards are applicable to site plan applications as well as all conditional uses. The Major Thoroughfare Design Guidelines are an adopted component of these Site Design Qualitative Standards as per Section 23.2-31(j), which are applicable to properties adjacent to the City's major thoroughfares inclusive of the subject site.

RESPONSE: The Major Thoroughfare Design Guidelines apply to the portion of Parcel A which fronts 6th Avenue South. The project design has incorporated the improvements along this frontage to with the applicable standards in Section 23.2-31.

Parcel A is configured with a five (5) story building with “store front” glazing on ground level fronting 6th Avenue South. Parking is proposed on surface parking lots predominately behind the building. Vehicular ingress and egress is proposed from 7th Avenue South and the alley. The pedestrian and vehicular site circulation safely connects to the existing public right-of-way. Further, the site pedestrian circulation system is appropriately insulated from vehicular circulation. Refuse will be stored in interior trash rooms and in a screened refuse area. Landscaping is proposed to screen the perimeter of the site and provide internal vegetation to screen mechanical equipment.

The proposed development consists of two buildings, one five story building and one four story building, designed in a contemporary architectural style with Art Deco and modern influences, which is consistent with the Lake Worth Beach Major Thoroughfare Design Guidelines recognizing contemporary architecture as appropriate along the City’s major corridors and encouraging creativity, durable materials, public art, a defined and active street wall, and a strong pedestrian environment. The exterior composition uses smooth stucco, scored stucco, strong accent bands, flat roof forms, vertical elements, bronze framed glazing, bronze canopies and railings, wood look exterior panels, mosaic tile, and an integrated mural, creating a vibrant tropical façade that reflects both the City’s contemporary corridor vision and Lake Worth Beach’s historic design vocabulary. The project supports the MU-DH district intent for redevelopment by providing active, habitable ground floor frontage, articulated façades, balconies for resident activity and overlook, and visually engaging materials and colors that enhance the streetscape. In addition, Parcel C is proposed as a passive park with an art feature that has yet to be identified; however, the intent is to create visual interest for the residents of the project and the surrounding area, consistent with the City’s guidelines encouraging public art and murals to strengthen identity, sense of place, and the public realm. This approach also aligns with Pinnacle’s own Art in Public Places initiative, through which Pinnacle collaborates with local artists to incorporate murals, sculptures, and other installations into the public spaces of its developments across Florida, reinforcing the project’s commitment to community identity and meaningful placemaking. Overall, the building design is contemporary and Art Deco in character while drawing from Lake Worth Beach’s architectural heritage in a way that is compatible with the City’s design goals and appropriate for this prominent corridor location.

The design of the proposed Pinnacle on Sixth has taken the qualitative development standards into consideration and comply as indicated below:

c) *Qualitative development standards.*

1. *Harmonious and efficient organization.* All elements of the site plan shall be harmoniously and efficiently organized in relation to topography, the size and type of plot, the character of adjoining property and the type and size of buildings. The site shall be developed so as to not impede the normal and orderly development or improvement of surrounding property for uses permitted in these LDRs.

RESPONSE: *The parcels of the subject property are a mix of vacant land or underutilized residential housing. The proposed development will provide an opportunity for new and 92 attainable housing units within an area with nearby employment opportunities and on major thoroughfares that offer alternative modes of transportation. The proposed development unifies these parcels into a refined planned development compliant with the Comprehensive Plan and City LDR’s.*

2. *Preservation of natural conditions.* The natural (refer to landscape code, article 6 of these LDRs) landscape shall be preserved in its natural state, insofar as practical, by minimizing tree and soil removal and by such other site planning approaches as are appropriate. Terrain and

vegetation shall not be disturbed in a manner likely to significantly increase either wind or water erosion within or adjacent to a development site. Natural detention areas and other means of natural vegetative filtration of stormwater runoff shall be used to minimize ground and surface water pollution, particularly adjacent to major waterbodies as specified in chapter 12, health and sanitation, article V, fertilizer friendly use regulations. Fertilizer/pesticide conditions may be attached to development adjacent to waterbodies. Marinas shall be permitted only in water with a mean low tide depth of four (4) feet or more.

RESPONSE: *The majority of the property has been cleared of vegetation, however, where possible, native vegetation and been incorporated into the proposed Landscape Plan for the project.*

3. **Screening and buffering.** Fences, walls or vegetative screening shall be provided where needed and practical to protect residents and users from undesirable views, lighting, noise, odors or other adverse off-site effects, and to protect residents and users of off-site development from on-site adverse effects. This section may be interpreted to require screening and buffering in addition to that specifically required by other sections of these LDRs, but not less.

RESPONSE: *Landscape buffering is provided as required around the perimeter of and within the interior of the property to screen parking and mechanical equipment. Trash receptacles will be stored interior to the building and taken out to the street and returned on trash pickup days by project management.*

4. **Enhancement of residential privacy.** The site plan shall provide reasonable, visual and acoustical privacy for all dwelling units located therein and adjacent thereto. Fences, walks, barriers and vegetation shall be arranged for the protection and enhancement of property and to enhance the privacy of the occupants.

RESPONSE: *The proposed Pinnacle on Sixth development abuts commercial uses to the east and industrial uses to the west. The buildings themselves will be secured with limited entry points for safety purposes and privacy.*

5. **Emergency access.** Structures and other site features shall be so arranged as to permit emergency vehicle access by some practical means to all sides of all buildings.

RESPONSE: *The site and buildings has been designed and will be constructed to provide access to emergency vehicles and services.*

6. **Access to public ways.** All buildings, dwelling units and other facilities shall have safe and convenient access to a public street, walkway or other area dedicated to common use; curb cuts close to railroad crossings shall be avoided.

RESPONSE: *The sites have been designed with vehicular and pedestrian access from the adjacent streets and alleys with portions of the alleys being widened and improved.*

7. **Pedestrian circulation.** There shall be provided a pedestrian circulation system which is insulated as completely as reasonably possible from the vehicular circulation system.

RESPONSE: *The site and buildings has been designed with pedestrian circulation which is insulated from vehicular circulations.*

8. **Design of ingress and egress drives.** The location, size and numbers of ingress and egress drives to the site will be arranged to minimize the negative impacts on public and private ways and on adjacent private property. Merging and turnout lanes traffic dividers shall be provided where they would significantly improve safety for vehicles and pedestrians.

RESPONSE: *The sites have been designed with vehicular and pedestrian access from the adjacent streets and alleys with portions of the alleys being widened and improved.*

9. **Coordination of on-site circulation with off-site circulation.** The arrangement of public or common ways for vehicular and pedestrian circulation shall be coordinated with the pattern of

existing or planned streets and pedestrian or bicycle pathways in the area. Minor streets shall not be connected to major streets in such a way as to facilitate improper utilization.

RESPONSE: *The sites have been designed with vehicular access and circulation to accommodate the future residents, guests as well as larger service and emergency vehicles.*

10. *Design of on-site public right-of-way.* On-site public street and rights-of-way shall be designed for maximum efficiency. They shall occupy no more land than is required to provide access, nor shall they unnecessarily fragment development into small blocks. Large developments containing extensive public rights-of-way shall have said rights-of-way arranged in a hierarchy with local streets providing direct access to parcels and other streets providing no or limited direct access to parcels.

RESPONSE: *There are no -site public streets or rights of way proposed.*

11. *Off-street parking, loading and vehicular circulation areas.* Off-street parking, loading and vehicular circulation areas shall be located, designed and screened to minimize the impact of noise, glare and odor on adjacent property.

RESPONSE: *The sites have been designed with parking that has access from the existing adjacent streets and screened by the building or landscape.*

12. *Refuse and service areas.* Refuse and service areas shall be located, designed and screened to minimize the impact of noise, glare and odor on adjacent property.

RESPONSE: *Trash receptacles will be stored interior to the building and taken out to the street and returned on trash pickup days by project management.*

13. *Protection of property values.* The elements of the site plan shall be arranged so as to have minimum negative impact on the property values of adjoining property.

RESPONSE: *The parcels of the subject site are either vacant or under-utilized. The proposed Pinnacle on Sixth is anticipated to enhance property values and offer 92 attainable housing units.*

14. *Transitional development.* Where the property being developed is located on the edge of the zoning district, the site plan shall be designed to provide for a harmonious and complementary transition between districts. Building exteriors shall complement other buildings in the vicinity in size, scale, mass, bulk, height, rhythm of openings and character. Special consideration shall be given to a harmonious transition in height and design style so that the change in zoning districts is not accentuated. Additional consideration shall be given to complementary setbacks between the existing and proposed development.

RESPONSE: *As indicated, the proposed development will provide affordable/workforce housing in between commercial to the east and industrial uses, plus less dense residential to the south.*

15. *Consideration of future development.* In finding whether or not the above standards are met, the review authority shall consider likely future development as well as existing development.

RESPONSE: *Criteria acknowledged.*

In addition to the above, provided with this resubmittal is a letter from a third party specifically addressing the criteria of Section 23.2-31d)16).

Sec. 23.2-31.d)17)

See Attachment D for the response to this code section.

Section 23.4-10. - Off-street parking: This section provides general provisions for off-street parking. The standards “apply to all parking spaces required for new buildings, new uses, additions, enlargements, or changes.”

RESPONSE: Using the City’s standard parking rates, a total of 145 parking spaces are required for a residential project with the new density of 66 1-bedroom dwelling units and 28 2-bedroom dwelling units, however the AWFH program allows for a 25% reduction of parking. Therefore, 109 spaces are required for the project, and 128 spaces are provided on site as calculated below:

Required Parking

66 1 bedroom units x 1.5 spaces/unit = 99 spaces

26 2 bedroom units x 2.0 spaces/unit = 46 spaces

Total Spaces Required = 145 spaces w/ 25% AWFH Reduction = 109 spaces

As shown on the Site Plan, the project provides a total of 109 parking spaces on site, plus there are additional on-street spaces, however not counted toward the project. The spaces provided are distributed as follows:

Standard Spaces	76 spaces
Compact Spaces	20 spaces
ADA Handicap Spaces	7 spaces

Section 12-7, Dumpster Requirements: The location of all dumpsters shall be approved by the public services director or his designee and/or the building official or his designee. All dumpsters shall meet the requirements set forth in this section and all other ordinances, rules, regulations and policies adopted by the city.

RESPONSE: Trash collection is provided in an enclosed separate room within both proposed buildings and then management will roll trash receptacles to designated pick up locations and back to the trash room on days of trash collection.

Section 23.4-3, Exterior Lighting: All outdoor lighting shall be installed in conformance with the provisions of this chapter, applicable electrical and energy codes, and applicable sections of the building code.

RESPONSE: A photometric plan is provided depicting compliance with the exterior lighting requirements in Section 23.4-3 and does not allow light trespass upon neighboring residential properties or districts in excess of 12.57 lumens.

Section 23.6-1. - Landscape regulations: The objective of this section is to provide minimum standards for the installation and maintenance of landscaping within the City. Per Section 23.6-1(c)(2), “on the site of a building or open- lot use providing an off-street parking, storage or other vehicular use area, where such an area will not be screened visually by an intervening building or structure from an abutting right-of-way or dedicated alley, shall require landscaping.”

RESPONSE: The proposed landscape plan provides perimeter landscaping and shade trees and was prepared to be generally consistent with the City’s landscape regulations and the Major Thoroughfare Design Guidelines.

f

Community Appearance Criteria:

The proposed development and associated site improvements will be a substantial improvement in the general appearance of the property and has been prepared with intent to be consistent with the Comprehensive Plan, Major Thoroughfare Design Guidelines, and the City’s Land Development

Regulations (LDRs). The project is in conformity with the principals of good design and quality and is in harmony with the city and the surrounding area.

Project Phasing and Plat

The project is proposed to be developed in one (1) phase. The planned development will be unified by unity of control, plat and/or restrictive covenant as determined by the City.

CONCLUSION

The MU-E FLU is intended to provide for a mixture of residential, office, service and commercial retail uses within specific areas east of I-95, near or adjacent to the central commercial core and major thoroughfares of the City. The City has also identified a need for new affordable/workforce housing. Although the Applicant has the right to process this application under the Live Local Act, they have chosen to unify the property for Pinnacle on Sixth and the 92 affordable/workforce housing units under a Multiple Use Urban Planned Development, consistent with Comprehensive Plan and City LDR's. The justification and analysis above, along with the set of drawings, propose a development plan intended to implement the goals and objectives of the Comprehensive Plan and comply with the applicable City LDR's. Accordingly, the application respectfully request City staff review and support as the applications are presented to the City Planning & Zoning Board and City Commission for approval.



Project Name

Pinnacle on Sixth

Location

6th Ave. South & H Street

Future Land Use

Mixed Use - East (MU-E)

Zoning

Mixed Use - Dixie Highway (MU-DH)

Overlay

Marjor Thouroughfare Plan

Site Area

2.01 AC

87,750 SF

Platted Lots w/in CRA

13 Lots

PCN(s)

38-43-44-21-15-203-0100

38-43-44-21-15-205-0010

38-43-44-21-15-203-0130

38-43-44-21-15-205-0020

38-43-44-21-15-203-0140

38-43-44-21-15-205-0030

38-43-44-21-15-203-0150

38-43-44-21-15-205-0040

38-43-44-21-15-203-0160

38-43-44-21-15-223-0080

Proposed Use(s)

Multi Family Residential

Proposed No. of Units

1 BR

39

27

66

2 BR

18

8

26

Total

57

35

92

Units

Proposed AWFH Units

92

Proposed Parking

Std.

Compact.

HC

108

Spaces

Parcel A

49

10

3

62

Parcel B

28

10

3

41

Parcel C

4

0

1

5

PROPERTY DEVELOPMENT REGULATIONS	ALLOWED MIN./MAX. with BONUS PROGRAM OPTIONS								PROPOSED PLANNED DEV.			
	Comp Plan Table 1		Zoning (MU-DH)		Planned Development		AWFH Program				Total Bonus'	
	Base	w/ SBIP	Base	w/ SBIP I & II	Bonus	w/ TDR	Bonus	in CRA	Bonus'	in CRA	Total	AWFH
Density (u/a)	30		30		+25%		+15%	+1/lot	42	13	45.67	
Number of Units	60		60		15		9	13	84	97	92	92
FAR - Max.	1.55		1.50		NOT PROPOSED							
< 5,000 sf lot	1.00		1.00									
5,000-7999 sf lot	0.95		0.95									
≥ 7500 sf lot	0.90		0.90									
Building Height (ft)	30	45	+15 adj. to AI		+25%				56.25		48.5	
Building Stories	2	4	+2 adj. to AI		+25%				5		5	

Attachment A



ATTACHMENT B

Sec. 23.3-17. - MU-DH—Mixed use-Dixie Highway.

- a) *Intent.* The MU-DH mixed use-Dixie Highway district is designed for Dixie Highway, Lake Worth's commercial spine. The MU-DH district is intended to provide the establishment and expansion of a broad range of office and commercial uses, including higher density residential use. Certain commercial uses are not permitted in the district because they will be detrimental to the shopping or office functions of the area. The establishment of certain uses is subject to conditional use review to ensure they will not have a negative impact on nearby residential uses or on the commercial viability of their neighbors. The district implements in part the downtown mixed use land use category of the Lake Worth Comprehensive Plan.

RESPONSE: Pinnacle on Sixth is consistent with the intent of the MU-DH district.

- b) *Use restrictions and development regulations for residential uses in the MU-DH district.* Multiple-family residential uses, excluding single-family and two-family uses, may be established and expanded in the MU-DH district subject to the provisions of section 23.3-10 for uses on the east side of Dixie Highway and section 23.3-11 for uses on the west side of Dixie Highway. Provided however that residential uses shall not be permitted at the ground floor of any building fronting on Dixie Highway.

1. Minimum living area shall be as follows:

- A. Efficiency units: Four hundred (400) square feet.
- B. One-bedroom units: Six hundred (600) square feet.
- C. Two-bedroom units: Seven hundred fifty (750) square feet.
- D. Three-bedroom units: Nine hundred (900) square feet.
- E. Four-bedroom units: One thousand three hundred fifty (1,350) square feet.

RESPONSE: The unit sizes proposed for Pinnacle on Sixth comply with the minimum living areas.

- c) *Use restrictions for nonresidential uses in the MU-DH district.* Refer to the permitted use table at section 23.3-6 for a complete list of uses.

1. *Principal nonresidential uses permitted by right.*

- A. Commercial - low intensity.
- B. Office - low intensity.
- C. Low intensity retail uses - low intensity.
- D. Personal services uses - low intensity.
- E. Cultural and artisanal arts uses - low intensity.
- F. Institutional uses - low intensity.
- G. Automotive vehicular - low intensity.

- H. Eating and drinking establishments without drive through facilities.
 - I. Essential services.
 - J. Mixed use developments between major intersections.
 - K. Stand alone commercial uses within one (1) block of major intersections. The major intersections in this district are 10th Avenue North, 6th Avenue south and 12th Avenue south.
2. *Principal uses permitted as either administrative or conditional uses.*
- A. Commercial - medium to high intensity.
 - B. Office - medium to high intensity.
 - C. Low intensity retail uses - medium to high intensity.
 - D. Personal services uses - medium to high intensity.
 - E. Cultural and artisanal arts uses - medium to high intensity.
 - F. Institutional uses - medium to high intensity.
 - G. Automotive vehicular - medium to high intensity.
 - H. Open air retail operations.
 - I. Parking facilities.
 - J. Places of worship (see Article 4, Development Standards).
 - K. Hotels and motels, subject to the following requirement: Each guest room and bath unit shall have a minimum area of two hundred fifty (250) square feet measured from the interior face of the exterior walls and the interior face of the demising walls of the unit.
3. *Accessory uses permitted by right.*
- A. Home occupations.
 - B. Any use accessory to and customarily incidental to a principal use permitted by right.
4. *Accessory uses permitted as either administrative or conditional uses.*
- A. Day care centers and nursery school uses accessory to places of worship.
 - B. Restaurants accessory to motels.
 - C. Establishments selling alcoholic beverages, whether for consumption on or off the premises, accessory to motels having accessory restaurants.
 - D. Residential apartments as secondary uses in structures with office uses as primary uses.
 - E. Any use accessory to and customarily incidental to a principal use permitted as either an administrative or conditional use.

RESPONSE: Pinnacle on Sixth does not propose any non-residential uses, therefore this subsection does not apply.

- d) *Development regulations for uses permitted by right.*

RESPONSE: Pinnacle on Sixth was designed with the intent of complying with the applicable development regulations which will be confirmed through the City review process.

Lot Area	East side of Dixie Highway - 6,500 square feet with max density 1 du per each 2,175 square feet net lot area	
	West side of Dixie Highway - 6,500 square feet with max density 1 du per each 1,450 square feet net lot area	
	Max density 30 dwelling units per gross acre of 43,560 square feet, minimum of 1,450 square feet per unit	
Lot Width	50 ft. on East side of Dixie Highway	
	50 ft. on West side of Dixie Highway	
Height	Primary	East side of Dixie Highway - 30 ft. (not to exceed 2 stories) *Additional 5 ft. of height under Sustainable Bonus Incentive Program (not to exceed 3 stories).
		West side of Dixie Highway - 30 ft. (not to exceed 2 stories) *Additional 5 ft. of height under Sustainable Bonus Incentive Program (not to exceed 3 stories) for blocks adjacent to a single-family residential (SF-R) district. *Additional 15 ft. of height under Sustainable Bonus Incentive Program (not to exceed 4 stories) for blocks fronting both Dixie Highway and G Street or blocks adjacent to the artisanal industrial (AI) district.
	Accessory	24 ft. (not to exceed 2 stories)

Setback	Front	10 ft. minimum not to exceed 22 ft.
	Rear	15 ft. or 10% of lot depth when next to residential zoning district. 10 ft. in general. 5 ft. for accessory structure
	Side	10 ft. on street and 0 ft. on interior lot.
	Bonus Height and Stories	For all stories above the second story, both the front façade and rear façade must be set back an additional distance beyond the minimum.
		A. Front façade for third story and above must have front setback of eight (8) to twelve (12) feet in addition to minimum.
		B. Rear façade for third floor and above must have rear setback of eight (8) to twelve (12) feet in addition to minimum.
		C. Façades facing major thoroughfares must have setbacks of eight (8) to twelve (12) feet in addition to minimum for third story and above.

Living Area	Single-Family	800 square feet first dwelling
		400 square feet second dwelling
	Multiple-Family	400 square feet Eff.
		600 square feet 1 BR
		750 square feet 2 BR
		900 square feet 3 BR
		1,350 square feet 4 BR
Accessory Structure Limitations	The total area for accessory structures is limited to 40% of the principal structure area or 1,000 square feet, whichever is less.	
Impermeable Surface Total	A. Small lot - 65%	
	B. Medium lot - 65%	
	C. Large lot - 65%	
Maximum Lot Coverage for all Buildings	A. Lots up to 4,999 square feet (small lot) - 55%	
	B. Lots 5,000 square feet to 7,499 square feet (medium lot) - 50%	
	C. Lots over 7,500 square feet (large lot) - 45%	
Maximum Wall Heights at Side Setback	30'-0" wall height at setback.	
	35'-0" wall height with sustainable incentive (east side of Dixie)	
	45'-0" for additional stories with sustainable incentive (west side of Dixie).	

Floor Area Ratio (FAR) Limitation	Maximum FAR is 1.5.
	The FAR shall be 1.00 for lots up to 4,999 square feet; 0.95 for lots between 5,000 square feet and 7,499 square feet; and 0.90 for lots 7,500 square feet and greater.
	An additional 0.50 of FAR shall be granted for Sustainable Bonus Incentive Program.

1. *Minimum lot dimension.*

A. Minimum lot area:

- (1) East side of Dixie Highway: Six thousand five hundred (6,500) square feet.
- (2) West side of Dixie Highway: Six thousand five hundred (6,500) square feet.

B. Minimum lot width:

- (1) East side of Dixie Highway: Fifty (50) feet.
- (2) West side of Dixie Highway: Fifty (50) feet.

2. *Maximum height of buildings.*

- A. Principal building: Thirty (30) feet in height and not to exceed two (2) stories.
- B. Additional five (5) feet in height shall be granted under the Sustainable Bonus Incentive Program (not to exceed three (3) stories) for lots on east side of Dixie and blocks on west side of Dixie abutting residential zoning districts.
- C. Additional fifteen (15) feet in height shall be granted under the Sustainable Bonus Incentive Program (not to exceed four (4) stories) for blocks on west side of Dixie and abutting the artisanal industrial (AI) zoning district or fronting both Dixie Highway and the Florida East Coast Railroad (FEC) tracks (G Street).
- D. Garages and other accessory buildings: Twenty-four (24) feet.

3. *Setbacks and build-to lines.*

A. Front build-to line:

- (1) From Dixie Highway (SR 805) right-of-way lines: Ten (10) feet.
- (2) From all other thoroughfare right-of-way lines: Ten (10) feet.
- (3) Front build-to line can be increased by eight (8), ten (10) or twelve (12) feet if the building provides an open arcade or public plaza.

B. Side:

- (1) Street side build-to line: Ten (10) feet.
- (2) Minimum interior side setback: None.
- (3) Street side build-to line can be increased by eight (8), ten (10) or twelve (12) feet if the building is provided an open arcade or public plaza.
- (4) Roof overhangs shall not exceed more than two (2) feet into setback.

C. Minimum rear setback:

- (1) Ten (10) feet in general.
- (2) For lots next to residential zoning districts: Fifteen (15) feet or ten (10) percent of lot depth, whichever is greater.
- (3) For accessory structures: Five (5) feet.

D. Additional height and stories setback for sustainable incentive: buildings in excess of thirty (30) feet in height shall provide an additional front and rear setback of between eight (8) and twelve (12) feet to the minimum required front and rear setbacks.

E. Additional height and stories setback along major thoroughfares: Buildings in excess of thirty (30) feet in height shall provide an additional setback of between eight (8) and twelve (12) feet for façades facing a major thoroughfare.

F. Awnings shall be exempt from the front and side street setback regulations. Awnings may project over public property to a point of not more than one (1) foot back from the face of the curb. Second story awning(s) installations shall project no more than five (5) feet from the building. If awnings overhang Dixie Highway rights-of-way, the owner will need to acquire a State of Florida Department of Transportation permit.

4. Reserved.

5. *Maximum impermeable surface.* The maximum impermeable surface shall be:

- A. Sixty-five (65) percent of the lots up to four thousand nine hundred ninety-nine (4,999) square feet;
- B. Sixty-five (65) percent for lots between five thousand (5,000) square feet and seven thousand four hundred ninety-nine (7,499) square feet; and
- C. Sixty-five (65) percent for lots seven thousand five hundred (7,500) square feet and greater.

6. *Maximum impermeable surface for all structures.* The maximum impermeable surface shall be:

- A. Fifty-five (55) percent for lots up to four thousand nine hundred ninety-nine (4,999) square feet;

- B. Fifty (50) percent for lots between five thousand (5,000) square feet and seven thousand four hundred ninety-nine (7,499) square feet; and
 - C. Forty-five (45) percent for lots seven thousand five hundred (7,500) square feet and greater.
7. *Floor area ratio (FAR) limitations.* Maximum FAR is 1.50. The FAR shall be:
- A. 1.00 for lots up to four thousand nine hundred ninety-nine (4,999) square feet;
 - B. 0.95 for lots between five thousand (5,000) square feet and seven thousand four hundred ninety-nine (7,499) square feet;
 - C. 0.90 for lots seven thousand five hundred (7,500) square feet and greater; and
 - D. An additional 0.50 of FAR shall be granted under the Sustainable Bonus Incentive Program.
8. *Accessory structures.* All accessory structures shall not exceed forty (40) percent of the gross floor area of the principal structure, excluding approved prefabricated metal storage buildings totaling no more than one hundred forty-four (144) square feet.
9. *Location of accessory buildings, pools, etc.* Accessory buildings, pools and similar structures shall be allowed within the rear or side yards of a double front or corner lot between the main structure and a public street, provided that minimum setbacks are maintained.
10. *Required street trees.* Street trees shall be installed at a minimum of every twenty-five (25) feet of frontage. Approved native species shall be used.
11. *Ground floor regulations.* Ground floor uses with frontage on Dixie Highway must be habitable and active, such as office or retail, and shall not include parking or mechanical uses.
12. *Parking.* In addition to the requirements in Article 4, Development Standards, off-street parking may be addressed by shared parking arrangements for mixed-use developments.
13. *Major thoroughfare design guidelines.* Additional developmental regulations are applicable to certain locations in this district pursuant to the major thoroughfare guidelines, as adopted from time to time by resolution of the city commission. See also section 23.2-31.
- e) *Locational and development regulations for uses permitted as either administrative or conditional uses.* Uses permitted as conditional uses shall be regulated pursuant to the requirements of sections 23.2-28 and 23.2-29 Article 4, Development Standards.

(Ord. No. 2016-13, § 6(Exh. E), 5-17-16; Ord. No. 2018-10, § 8(Exh. G), 7-17-18; Ord. No. 2019-13, § 4, 12-3-19; Ord. No. 2024-13, § 13(Exh. L), 11-19-24; Ord. No. 2024-18, § 3(Exh. B), 1-21-25)

ATTACHMENT C

Sec. 23.3-25. - Planned development district.

b) General provisions and requirements.

1. *Compliance with other sections.* All planned developments shall comply with sections 23.2-27, 23.2-28 and 23.2-29.
2. *Increase in development potential.* Planned developments allow for increases in height, intensity and density based on the allowances described in the future land use element of the city's comprehensive plan as follows:
 - A. As part of a planned development, mixed use planned development, residential planned development or urban planned development, a project may maximize the density, intensity and height as outlined in Comprehensive Plan Policy 1.1.1.1, Table 1 (hereinafter referred to as Table 1). For each project requesting the bonus, the base line sustainable bonus value shall apply to each square foot above the maximum allowed in the underlying zoning district.

Response: Pinnacle on Sixth is proposed as an urban planned development and incorporates the allowed bonus' of density, intensity and height. The chart below shows the base line of the MU-DH district, the maximum allowed as an UPD and what is proposed.

Planned Development Bonus Comparison

	MU-DH	UPD	Proposed Building A	Proposed Building B
Max. Density (du/ac)	30	49.5 - SBIP +25%, AWFH +15%, CRA Platted Lots +13	45.67	
Max. Intensity (FAR)	0.9	1.35	1.10	
Max. Building Height/Stories	30 / 2	67.5 / 6	49'-7.5" / 5	39'-0" / 4

- B. As part of a planned development, mixed use planned development, residential planned development or urban planned development, a project may obtain a twenty-five (25) percent bonus on density, intensity and height as outlined in Table 1. For each project requesting the additional bonus, twice the base line sustainable bonus value shall apply to each square foot above the maximum threshold as shown in Table 1.

Response: Pinnacle on Sixth incorporates the 25% bonus. As shown in the Sustainable Bonus Program Worksheet, the Tier 1 bonus includes 46,140 square feet and the Tier 2 bonus includes 13,410 square feet of additional building area.

- C. For mixed use urban planned developments located west of Dixie Highway, which include at least three use categories, one being residential, and a minimum project size of two acres, an additional fifty (50) percent bonus in density, intensity and height as outlined in Table 1 may be obtained. For each project requesting the additional bonus, twice the base line sustainable bonus value shall apply to each square foot above the maximum threshold as shown in Table 1. **Not applicable**
- D. For mixed use planned developments located west of Interstate 95, which include at least three use categories, one being residential, and a minimum project size of five (5) acres, an additional fifty (50) percent bonus in density, intensity and height as outlined in Table 1 may be obtained. For each project requesting the additional bonus, twice the base line sustainable bonus value shall apply to each square foot above the maximum threshold as shown in Table 1. **Not applicable**
- E. For a mixed use planned development with a Tri-Rail Station component located within the Transit Oriented Development West land use area, which includes at least three use categories, one being residential, and a minimum project size of five (5) acres, an additional one hundred (100) percent bonus in density and intensity and fifty (50) percent increase in height as outlined in Table I may be obtained. For each project requesting the additional bonus, twice the base line sustainable bonus value shall apply to each square foot above the maximum threshold as shown in Table 1. **Not applicable**
3. *Conflict with other regulations.* The provisions of this section shall apply generally to the creation and regulation of all planned development districts. Where conflicts exist between these special planned development provisions and regulations relating to the installation operation or service requirements of any utility system or service, the utility regulations shall apply. Where conflicts exist between these special provisions and general zoning, subdivision or other applicable non-utility regulations, these special regulations shall apply. **Not applicable**
4. *Dedication of public facilities.* Dedication, grant, reservation or improvement of property or easements therein for public rights-of-way, streets, schools, parks, utilities or other public facilities may be required as a condition or requirement of approval pursuant to this section. **Acknowledged**
5. *Effect of planned development approval.* When approved pursuant to the provisions of this section, the master development plan and all information and documents formally incorporated with the application shall constitute an amendment the official zoning map. Development within a planned development shall occur in conformity with the approved master development plan and development phasing. **Acknowledged**
6. *Utilities.* All utilities, including telephone, cable television, and electrical service systems, shall be installed underground. However, the following facilities may be exempt from this requirement:

- A. Accessory facilities normally associated with such systems that require above-ground installation, provided such facilities are screened adequately; and **Acknowledged**
 - B. Primary facilities, such as electric substations, providing service to the planned development or to service areas not located within the planned development. Primary facilities shall be screened or landscaped. **Acknowledged**
7. *Visibility triangle*. In all planned development, visibility at all street and alley intersections shall be provided pursuant to section 23.4-4.

Provided

8. *Open space.* In all planned development, sufficient areas of common open space shall be provided at each stage of development and upon completion of development. Such common open space shall include areas not covered by water.
9. *Establishment of planned development districts.* Planned development districts will be established from designated existing zoning districts by amendment to the official zoning map for tracts of land suitable in location, extent, and character for the structures and uses proposed.
10. *Unified control.* All land included for purpose of development within a planned development district shall be owned or under the control of the petitioner for such zoning designation, whether that petitioner be an individual, partnership or a corporation, or a group of individuals, partnerships or corporations. The petitioners shall present firm evidence of the unified control of the entire area within the proposed planned development district and shall agree that when the development proceeds:
 - A. It will be in accordance with the ordinance officially adopted for the district and the regulations in effect when the planned development was approved.
 - B. Agreements, contracts, or deed restrictions and covenants will be provided to the city to insure that the development will occur in accordance with the master development plan; and that the developer, his successors, assignees, or heirs, are responsible for the continued maintenance and operation of common areas and facilities, including sodding, watering down and fencing of undeveloped areas earmarked for future stages of development that are disturbed during development.
11. *Master development plan.* Any petition for planned development district zoning shall be accompanied by a professionally prepared master development plan of the development comprised of at least the following elements:
 - A. A site plan, drawn to acceptable scale, which shall indicate:
 - (1) The title of the project and name of developer.
 - (2) The scale, date, a north arrow, and a general location map.
 - (3) A survey prepared by a registered surveyor, indicating the boundaries of the subject property, all existing streets, all buildings, watercourses, easements, section lines, the existing topography at a contour interval of one (1) foot or less, and other important physical features within and adjoining the proposed project.
 - (4) The proposed use of all land within the project boundaries, including the location and function of all areas proposed to be dedicated or reserved for community or public use.
 - (5) The location and size as appropriate, of all existing and proposed drainage, water, sewer, and other utility service systems.

- (6) The existing vegetative cover and soil conditions in sufficient detail to indicate suitability for proposed structures and uses.
- B. A written legal description of the subject property, together with names and addresses of all owners of record.
- C. Agreements, provisions or covenants which govern the use, maintenance, and continued protection of the planned development and any of its common areas or facilities.
- 12. *Supporting information.* Applications for planned development approval shall include the following documentation:
 - A. A statement indicating the manner in which the proposed project complies with the comprehensive plan;
 - B. A general description of the proposed development, including the total acreage involved in the project; the number and percentage of acres devoted to various categories of land use; the number and type of dwelling units proposed and the overall project density in dwelling units per gross acre; the minimum design standards for such features as lot shape and size, building size and lot coverage, open space, off-street parking and loading, signs, and landscaping;
 - C. The proposed schedule of development which identifies the anticipated project start and completion dates, stages of development (if any), and the area and location of common open space to be provided at each stage;
 - D. A detailed traffic impact analysis study;
 - E. A detailed market analysis study prepared pursuant to commonly accepted professional practices (this study may be required at the discretion of the city commission); and
 - F. Schematic architectural drawings (floor plans, elevations, perspectives, method of attachment, etc.) of all proposed structure types and improvements in the proposed project, except detached single-family residences and related accessory buildings (at the discretion of the city commission, these drawings may be submitted in stages if the project is to be developed in stages).
- 13. *Professional services required.* A master development plan for any proposed planned development district shall be prepared utilizing the professional services of individuals possessing appropriate licensure or registration.
- 14. *Application fees.* Application fees for planned development districts shall be established and amended by resolution of the city commission.
- c) *Residential planned developments (RPDs); special requirements.*
 - 1. *Location.* RPDs may be created in any residential district.

2. *Minimum area required.* The minimum area required for a residential planned development district east of Interstate 95 shall be two (2) acres. The minimum area required for a residential planned development district west of Interstate 95 shall be five (5) acres, unless otherwise provided in these LDRs. However, any area of lesser size may be approved for residential planned development zoning upon findings by the planning board or historic resources preservation board and the city commission that one (1) or more of the following conditions exists:

- A. Particular circumstances justify such reduction.
 - B. Requirements for RPD zoning and the benefit to be derived from such zoning can be derived in such lesser area.
 - C. Permitting such lesser area for RPD zoning is in conformity with the comprehensive plan.
3. *Permitted uses.* Within any residential planned development any use permitted in the underlying zoning district is permitted.
4. *Required setbacks.* Required setbacks shall be as provided in these LDRs for the zoning district in which the planned development is to be located.
5. *Parking and loading space requirements.* Parking and loading spaces for all uses within a residential planned development district shall be provided as required by sections 23.4-18 and 23.4-19. No off-street parking shall be located within a required setback area.
6. *Landscaping.* Landscaping, tree protection, screening and buffering shall be provided as required by section 23.6-1. However, additional landscaping, screening, and buffering may be required to provide additional privacy and protection for residents within a planned development district and adjacent property owners.
7. *Signs.* Signs may be erected pursuant to the provisions of section 23.5-1.

d) *Mixed use planned development district.*

- 1. *Location.* Planned developments may be located in any mixed use district with the exception of the neighborhood commercial district. Industrial planned developments may be located in the industrial districts.
- 2. *Minimum area required.* The minimum area required for a commercial or industrial planned development district east of Interstate 95 shall be two (2) acres. The minimum area required for a commercial or industrial planned development district west of Interstate 95 shall be five (5) acres; however, an area of lesser size may be approved for a mixed use planned development zoning upon findings by the planning and zoning board or historic resources preservation board, as applicable, and the city commission that particular circumstances justify such a reduction, and requirements for planned development district zoning and the benefit to be derived from planned development district zoning can be derived in such lesser area.

3. *Permitted uses.* Permitted uses within a mixed use development are shown in Article 3 of these LDRs.

4. *Required setbacks.* Required setbacks shall be as provided in these LDRs for the zoning district in which the planned development is to be located.
5. *Parking and loading space requirements.* Parking and loading spaces shall be provided pursuant to Article 4 of these LDRs.
6. *Landscaping/buffering.* Landscaping and buffering shall be provided as required by section 23.6-1.
7. *Illumination.* Any source of illumination located within a commercial or industrial planned development district shall not exceed one (1) foot candle at or beyond the boundaries of such development.
8. *Outdoor storage.* All outdoor storage facilities are prohibited in any commercial planned development district. However, this shall not apply to the following:
 - A. The outdoor display of new automotive vehicles for sale or rent.
 - B. The display of goods or chattels for sale or rent in a commercial planned development by an establishment having activities that occur within a building.

e) *Mixed use urban planned development district.*

1. *Location.* Urban planned developments may be located in any mixed use district, such as Mixed Use — East, Mixed Use — West, Mixed Use — Dixie Highway, Mixed Use — Federal Highway, Transit Oriented Development — East, Transit Oriented Development — West and Downtown with the exception of the neighborhood commercial district. Industrial planned developments are not allowed as a mixed use urban planned development.
2. *Minimum area required.* The minimum area required for an urban planned development district shall be one-half (0.5) acres.
3. *Permitted uses.* Permitted uses within a mixed use urban development are shown in article 3 of these LDRs. An urban planned development may be residential alone or may be any mixture of residential, retail, commercial, office, personal services, institutional, and cultural and artisanal arts or other uses specifically listed with the use tables of section 23.3-6 for the districts where the planned development is to be located.
4. *Required setbacks.* Required setbacks shall be as provided in these LDRs for the zoning district in which the planned development is to be located.
5. *Parking and loading space requirements.* Parking and loading spaces shall be provided pursuant to article 4 of these LDRs.
6. *Landscaping/buffering.* Landscaping and buffering shall be provided as required by section 23.6-1.

7. *Illumination.* Any source of illumination located within a commercial or industrial planned development district shall not exceed one (1) foot candle at or beyond the boundaries of such development.

8. *Outdoor storage.* All outdoor storage facilities are prohibited in any mixed use urban planned development district.

9. *Sustainability.* All mixed use urban planned development districts shall include provisions for sustainability features such as those listed in section 23.2-33, City of Lake Worth Sustainable Bonus Incentive Program.

f) *Amendments to approved planned development master plans.* Minor amendments to existing planned development master plan may be approved by the development review official. Minor changes shall not include the following:

1. An increase in the total square footage of any building by more than five (5) percent the number of residential dwelling units, or the densities as specified by the adopted master plan. However, a decrease in the total square footage of any building, or reduction of the number of structures, stories or units as specified by the master plan may be approved as minor amendments.
2. Any boundary change of the planned development.
3. Any change, except as provided herein, in the adopted master plan that increases the use in intensity or density. Rearrangement of uses or locations on a property may be permitted unless they conflict with a specific provision or the adopted planned development ordinance.
4. The relocation of more than five (5) percent of the total square footage indicated as being covered by structures.
5. Any increase in the traffic impact above that established in the master plan approved by the city commission.

g) *City of Lake Worth Beach Transfer Development Rights (TDR) Program.*

1. Incorporation of transferred developed rights from city-owned properties with a future land use designation of Public (P) shall be required in order to obtain the following:
 - A. One additional story of no more than fifteen (15) feet in overall height.
 - B. An increase in overall density of ten (10) units per acre.
 - C. An increase in overall floor area ratio (FAR) of ten (10) percent.
2. The additional story, height, density and/or floor area ratio available under the TDR program will be in addition to any maximums allowed under the city's sustainable bonus incentive program.
3. Projects incorporating transfer development rights may be located anywhere in the city predicated on the following:

A. For projects east of Dixie Highway transfer development rights may increase density by up to ten (10) units per acre.

B. For projects west of Dixie Highway transfer development rights may increase density by up to ten (10) units per acre, provide for one additional story of not more than fifteen (15) feet in height, and/or increase overall FAR by up to ten (10) percent.

4. Projects incorporating transfer development rights must be a mixed use urban planned development, planned development, mixed use planned development or residential planned development.
5. Projects must have incorporated all of the density, height and intensity bonuses available under the sustainable bonus program prior to being eligible for the transfer development rights program.

(Ord. No. 2015-12, § 4(Exh. C), 10-6-15; Ord. No. 2018-10, § 9(Exh. H), 7-17-18; Ord. No. 2019-14, § 4(Exh. C), 12-3-19; Ord. No. 2020-07, § 4(Exh. C), 6-16-20)

ATTACHMENT D

Sec. 23.2-31. - Site qualitative standards.

d) Buildings, generally.

17) In addition to the items enumerated above, all new planned developments shall strive to incorporate design elements, performance standards and/or specifications to enhance the public's awareness and appreciation of the community's commitment to the preservation and enhancement of the following sustainability qualities, values and principles:

- a. Cultural resources,
- b. Historical resources,
- c. Ecological/natural resources,
- d. Diversity and inclusion,
- e. Social justice,
- f. Economic investment,
- g. Neighborhood vitality,
- h. Sense of place,
- i. Education, and
- j. Recreation.

Response: Pinnacle on Sixth has been designed to comply with this subsection and strive toward using best practices to enhance the public's awareness and appreciation to enhancement of the community. The property has been historically low density residential however over years it has become blighted and dilapidated. The economic investment into Pinnacle on Sixth is anticipated to incentivize improvement for the entire area and to upgrade the vitality of the neighborhood and to create a sense of place. The project also includes a park area to offer passive recreation and community interaction.

CITY OF LAKE WORTH BEACH UTILITIES

CONTRIBUTION IN AID OF CONSTRUCTION



DATE: 05/15/26
CASE #: 630 S H St
WORK ORDER #: N/A

JOB DESCRIPTION: This quote includes work for 630 S H St and 615 S H St. At 630 S H St, ounder ground facilities are extended to the south of 7th Ave to eliminates all overhead structures. At 615 S H St. the powerline is moved underground and a padmount transformer is set for existing services.

AT THE CUSTOMER EXPENSE AND REQUEST

MATERIALS	\$91,120.15
MISC. MATERIALS	\$18,224.03
COST OF STORES, FREIGHT, ETC.	\$5,467.21
TOTAL COST OF MATERIALS	\$114,811.39

TRUCK NUMBER	TRUCK TYPE	HOURS	COST
558	2013 GMC SIERRA 2500 HD FOREMAN TRUCK (SJ)	80.0	\$1,600.00
553	2008 INTERNATIONAL 7300 SFA BUCKET	80.0	\$2,320.00
552	2008 INTERNATIONAL 7300 SFA BUCKET	80.0	\$2,320.00
TOTAL TRANSPORTATION			\$6,240.00

EMPLOYEE TITLE	NO. OF EMPLOYEE(S)	HOURS	OT HOURS	COST
T&D ENGINEER	0	0.00	0.00	\$0.00
FIELD PLANNING ENGINEER	0	0.00	0.00	\$0.00
LINE FOREMAN	1	64.00	16.00	\$4,840.00
LINEMAN	2	64.00	16.00	\$9,152.00
TROUBLEMAN	0	0.00	0.00	\$0.00
SUBSTATION FOREMAN	0	0.00	0.00	\$0.00
SUBSTATION ELECTRICIAN/LINEMAN	0	0.00	0.00	\$0.00
APPRENTICE LINEMAN	1	64.00	16.00	\$4,048.00
TOTAL LABOR				\$18,040.00

LABOR BENEFITS	\$8,523.90
TOTAL COST OF WORK	\$147,615.29

RestoreItAll	\$31,535.00
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TOTAL NET EXPENDITURE	\$179,150.29
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INCREASED EARNINGS NONE
SUBMITTED BY JAKUB PAJAK
CHECKED BY Jean St. Simon



transmittal

TO: Alberto Lopez Tagle
Palm Beach County, Traffic Division
2300 N. Jog Road
West Palm Beach, FL 33411

Urban Planning and Design
Landscape Architecture
Communication Graphics

DATE: April 9, 2026

PROJECT NAME: Pinnacle on Sixth

UDS Number: 24-043.000

For Internal Use Only

Copies to: w/ enc.

- ☐ REGULAR MAIL
☐ HOLD FOR PICK-UP
☐ COURIER
☐ OVERNIGHT DELIVERY
☒ HAND DELIVER

_____	_____
_____	_____
_____	_____
_____	_____
_____	_____

NO. OF COPIES	DATE	DRAWING OR ITEM NO.	DESCRIPTION/REMARKS
1			One Hard copy of the traffic study for Pinnacle on the Sixth within the City of Lake Worth Beach. 1 TPS Review Fee check for \$968.60. RECEIVED APR 9 2026 TRAFFIC DIVISION

URBAN DESIGN STUDIO

BY: Shayne Broadnix

June 12, 2026

Re: Pinnacle on Sixth

To whom it may concern,

My name is Kyle Abney, and I certify that I have met all the requirements and am currently in good standing with the Home Innovation Research Labs' National Green Building Standard (NGBS) green building certification program. This credential allows me to perform third-party design review, field verification and certification services for projects that are pursuing a NGBS certification. Attached please find a copy of my NGBS Master Verifier certificate.

Pinnacle on Sixth LLC has retained my firm, Abney + Abney Green Solutions, for the purposes of NGBS Consulting and Verification services for the above referenced project, located in Lake Worth, Florida. Our role is to assist the project team during the design phase and will continue our services with the builder throughout the construction phase to achieve an NGBS certification.

In addition to providing NGBS services for the project, our firm will assess the project's commitment to meet the City of Lake Worth Beach's additional design principles and performance standards under Section 23.2-31(d)(16) of the City's code as it pertains to green building, a comparative reduction in the development's carbon footprint, conservation and other areas where our guidance, the NGBS Certification and Pinnacle's consistent performance standards will achieve these benefits.

We have worked with Pinnacle for close to 15 years and have green-certified 22 of their projects with four more currently in progress. They have always been dedicated to doing whatever it takes to achieve the sustainability goals of each project. Through our experience working with Pinnacle, we know their internal design specifications which emphasize energy and water efficiency and a healthy living environment for their tenants. From this, we are confident they will achieve at least a Silver level NGBS certification for Pinnacle on Sixth. Pinnacle's selection as one of 20 "Partners of Excellence" nationally in 2025 is proof of their commitment in this area.

Please let me know if you have any questions.

Respectfully,



Principal

March 12, 2026

Timothy P. Wheat, Partner
Pinnacle on Sixth, LLC
9100 S. Dadeland Blvd., Suite 700
Miami, FL 33156

Re: Pinnacle on Sixth

Dear Mr. Wheat:

As you know, my name is Kyle Abney, and I certify that I have met all the requirements and am currently in good standing with the Home Innovation Research Labs' National Green Building Standard (NGBS) green building certification program. This credential allows me to perform third-party design review, field verification and certification services for projects that are pursuing a NGBS certification. Attached please find a copy of my NGBS Master Verifier certificate.

Pinnacle on Sixth LLC has retained my firm, Abney + Abney Green Solutions, for the purposes of NGBS Consulting and Verification services for the above referenced project, located in Lake Worth, Florida. Our role is to assist the project team during the design phase and will continue our services with the builder throughout the construction phase to achieve an NGBS certification.

As the NGBS Green Verifier for this project, we will develop the NGBS Certification Plan, which is a comprehensive list of requirements for the NGBS certification. In addition, we are required to perform multiple site visits during construction to verify the project remains on track for a NGBS certification. We will provide the mandatory rough verifications of all claimed NGBS practices in every unit and all common/amenity areas, and they occur post-insulation but pre-drywall. Final verifications of all claimed NGBS practices in every unit and all common/amenity areas are also mandatory and they occur near the end of construction. We will work closely with the builder to coordinate each of our required verifications.

Once all the NGBS requirements during design and construction have been verified and documented, the project will be submitted for the final review by the Home Innovation Research Labs. When the NGBS certificate is issued, it will be provided to our client for their use as needed.

We have worked with Pinnacle for close to 15 years and have green-certified 22 of their projects with four more currently in progress. They have always been dedicated to doing whatever it takes to achieve the sustainability goals of each project. Through our experience working with Pinnacle, we know their internal design specifications which emphasize energy and water efficiency and a healthy living environment for their tenants. From this, we are confident they will achieve at least a Silver level NGBS certification for Pinnacle on Sixth. Attached is the NGBS Scoring Summary for a current project. The same scoring is expected for Pinnacle on Sixth.

Lastly, I have been informed that Pinnacle has just been announced as a 2025 NGBS Partner of Excellence! This prestigious designation is reserved for developers who go above and beyond to promote sustainable, resilient and healthy homes. Congratulations on this tremendous honor!

Please let me know if you have any questions.



Principal



Home Innovation
NGBS GREEN VERIFIER™

M A S T E R

THIS CERTIFICATE SIGNIFIES

Kyle Abney
Abney + Abney Green Solutions
Palm City
FL

IS AN

ACCREDITED MASTER VERIFIER

Michelle Foster, Vice President, Sustainability

05/22/20

Accreditation Date

09/28/26

Expiration Date

This certificate becomes invalid upon suspension, cancellation, revocation, or expiration of certification.



Summary of Results of the Design Phase

Scoring Tool Version: 5.3.03 Revision Date: 10/1/2025

Project Name: Pinnacle at Cypress - Phase I

Location: 6520 N Andrews Ave, Fort Lauderdale, Florida 33309

✓ No Mandatory items missing on the "Overview (Design)" page

	Points Required			Points Claimed	Mandatory Practices	No Errors
	Bronze	Silver	Gold			
Chapter 5: Lot Design, Preparation, and Development	50	64	93	121	✓	✓
Chapter 6: Resource Efficiency	43	59	89	119	✓	✓
Chapter 7: Energy Efficiency	30	45	60	70	✓	✓
Chapter 8: Water Efficiency	25	39	67	92	✓	✓
Chapter 9: Indoor Environmental Quality	25	42	69	97	✓	✓
Chapter 10: Operation, Maintenance, and Building Owner Education	8	10	11	12	✓	✓
Additional Points required	50	75	100	100		
Additional points required due to SF over 4000 (601.1)	0	0	0	0		
Total points required	231	334	489	611		

Additional Points Claimed	234	156	26	(96)
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Overall Level Achieved for Design	Silver
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June 10, 2026

Re: DRAINAGE REPORT for Pinnacle on Sixth, Lake Worth Beach, FL

PROJECT DESCRIPTION

The site comprises ± 2 acres and is generally located south of 6th Avenue South along the east and west sides of South H Street in the City of Lake Worth Beach. The project proposes a multi-family development on the properties.

LEGAL, POSITIVE OUTFALL

Legal, positive outfall is available through overflow to the adjacent street system once onsite drainage requirements have been met.

BASIN & ALLOWABLE DISCHARGE

The property is located within the Intracoastal Basin with an allowable discharge rate not to exceed the existing predevelopment discharge rate for the 25Y-3D storm event.

WATER QUALITY

Water quality will be provided in the proposed exfiltration trench system.

FLOOD DESIGNATION

The site is located in FEMA flood zone X.

SYSTEM DESIGN

The proposed drainage system will include a series of inlets to direct stormwater runoff into the exfiltration trench system for treatment, storage, and attenuation. Once treated and attenuated, stormwater runoff will recover through percolation to the groundwater. The proposed drainage design will comply with applicable standards of Lake Worth Beach, PBC, SFWMD, FEMA and FDEP including volume, rate, pollutant loads and water quality.

LEVELS OF SERVICE

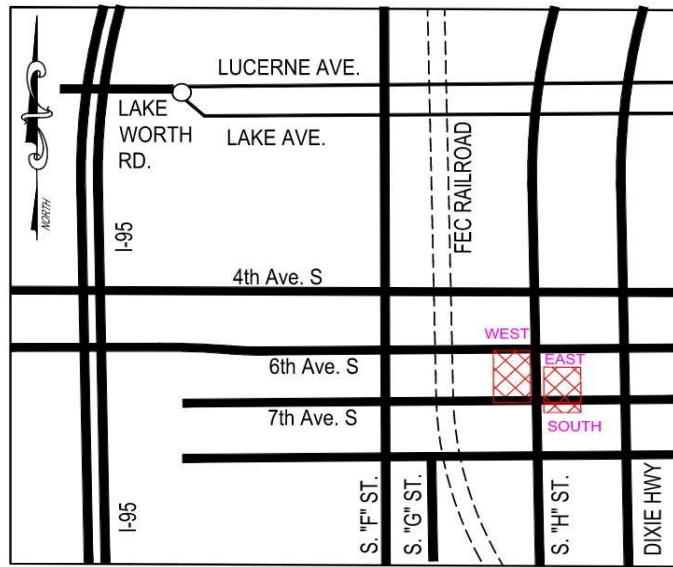
The minimum required level of service elevations will be met. Specifically:

1. The 3-year, 1 hour storm event will be contained onsite per the City of Lake Worth Beach requirements.
2. The perimeter elevations will be set to contain the pre v. post development runoff generated by the calculated 25-year, 3-day storm event (per Intracoastal basin requirements).
3. Finished floors will be designed at or above the calculated 100-year, 3-day storm event.

ANTICIPATED DRAINAGE APPROVALS

Drainage approvals are anticipated to include: City of Lake Worth Beach and FDEP (10-2 permit exemption). No other agencies are expected to review the drainage design for this project.

T. Jeff Trompeter, State of Florida, Professional Engineer,
License No. 51045. This item has been digitally signed and
sealed by T. Jeff Trompeter on the date indicated here.
Printed copies of this document are not considered signed
and sealed and the signature must be verified on any
electronic copies.



SEC. 28 - TWP. 44 S - RGE. 43 E

LOCATION MAP
N.T.S.

STORM WATER MANAGEMENT CALCULATIONS
FOR
Pinnacle on Sixth - WEST BASIN

South H Street Between 6th & 7th Avenues South
Lake Worth Beach, Florida 33460
CDI Project No. 25974-01

June 10, 2026

Datum = NAVD '88

Civil Design, Inc.
1400 Centrepark Blvd, Suite 905
West Palm Beach, FL 33401
Tel: 561.659.5760
Fax: 561.659.5772
www.civil-design.com

Registry No. 9664

CDI CIVIL DESIGN, INC.
Service Driven Design ~ Engineered Results

Date

COVER

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - WEST BASIN

CDI Project #: Lake Worth Beach, Florida 33460

LAND USE BREAKDOWN

EXISTING

Site Area = **1.08 ac**
Non-Contributing Area = **0.00 ac**
Offsite Area = **0.00 ac**
Basin Area = **1.08 ac**

	Acres	%	Grading From	To	Avg. Elev.
Impervious Area					
Building	0.14 ac	(13%)	16.17	16.21	16.19
Pavement/Concrete	0.15 ac	(14%)	13.86	16.21	15.04
Pervious Area					
Green Space	0.79 ac	(73%)	13.40	16.23	14.82
Subtotal Impervious Areas	0.29 ac	(27%)			
Subtotal Pervious Areas	0.79 ac	(73%)			

Find Curve Number:

Overall Weighted Average Elev. = 15.02

Avg. Pervious Ground El. = 14.82
Control Elevation = 3.0 Water Table
Depth to Water Table = 11.82
Soil Type = Coastal

Soil Storage Table

(SFWMD's Vol. IV, Basis of Review, page E-2)

Depth to W.T. (ft)	Coastal Storage (in)	Flatwoods Storage (in)	Depression Storage (in)
1.0	0.6	0.6	0.6
2.0	2.5	2.5	2.1
3.0	6.6	5.4	4.4
4.0	10.9	9.0	6.8

Pervious Area = 0.79 ac
Storage from Table = 8.18 in (w/ 25% compaction)
Available Soil Storage = 0.54 af
Soil Moisture Storage (S) = 5.98 in
Curve Number = **63**

CDI Project #: Lake Worth Beach, Florida 33460

PROPOSED

	Acres	%	Grading		Avg. Elev.
			From	To	
Impervious Area					
Building	0.21 ac	(19%)	15.50		15.50
Pavement/Concrete	0.29 ac	(27%)	13.62	15.50	14.56
Pervious Pvmt (50%)	0.10 ac	(9%)	14.10	14.90	14.50
Pervious Area					
Green Space	0.38 ac	(35%)	13.40	15.50	14.45
Pervious Pvmt (50%)	0.10 ac	(9%)	14.10	14.90	14.50
Subtotal Impervious Areas	0.60 ac	(56%)			
Subtotal Pervious Areas	0.48 ac	(44%)			

Find Curve Number:

Soil Storage Table

Depth to W.T. (ft)	Coastal Storage (in)	Flatwoods Storage (in)	Depression Storage (in)
1.0	0.6	0.6	0.6
2.0	2.5	2.5	2.1
3.0	6.6	5.4	4.4
4.0	10.9	9.0	6.8

Basin

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - WEST BASIN

CDI Project #: Lake Worth Beach, Florida 33460

STAGE -STORAGE CALCULATIONS

EXISTING

Starting Stage **3.00**
Ending Stage **16.00**
Stage Increment **1.00**

Impervious Areas	Pervious Areas
------------------	----------------

Name	Pavement/ Concrete	0	0	Green Space	0
Area	0.15	0.00	0.00	0.79	0.00
Start Elev	13.86	0.00	0.00	13.40	0.00
End Elev	16.21	0.00	0.00	16.23	0.00

Stage Feet	Linear Storage Ac-ft	Vert Storage Ac-ft	Vert Storage Ac-ft	Linear Storage Ac-ft	Vert Storage Ac-ft	Total Storage Ac-ft
NAVD						
3.00	0.00	0.00	0.00	0.00	0.00	0.00
4.00	0.00	0.00	0.00	0.00	0.00	0.00
5.00	0.00	0.00	0.00	0.00	0.00	0.00
6.00	0.00	0.00	0.00	0.00	0.00	0.00
7.00	0.00	0.00	0.00	0.00	0.00	0.00
8.00	0.00	0.00	0.00	0.00	0.00	0.00
9.00	0.00	0.00	0.00	0.00	0.00	0.00
10.00	0.00	0.00	0.00	0.00	0.00	0.00
11.00	0.00	0.00	0.00	0.00	0.00	0.00
12.00	0.00	0.00	0.00	0.00	0.00	0.00
13.00	0.00	0.00	0.00	0.00	0.00	0.00
14.00	0.00	0.00	0.00	0.05	0.00	0.05
15.00	0.04	0.00	0.00	0.36	0.00	0.40
16.00	0.15	0.00	0.00	0.94	0.00	1.09

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - WEST BASIN

CDI Project #: Lake Worth Beach, Florida 33460

STAGE-STORAGE CALCULATIONS PROPOSED

Starting Stage **3.00**
Ending Stage **16.00**
Stage Increment **1.00**

Name	Impervious Areas		Pervious Areas		Trench	Pervious Pavement Storage			
	Pavement /Concrete	Pervious Pvmt (50%)	Green Space	Pervious Pvmt (50%)					
Area	0.29	0.10	0.38	0.10	0.23 (AF)	0.09 (AF)			
Start Elev	13.62	14.10	13.40	14.10	8.00	12.85			
End Elev	15.50	14.90	15.50	14.90	12.00	15.35			
							Post Dev	Pre Dev	
Stage	Linear	Linear	Linear	Linear	Linear	Linear	Total	Total	
Feet	Storage	Storage	Storage	Storage	Storage	Storage	Storage	Storage	Difference
NAVD	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft
3.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
4.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
5.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
6.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
7.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
8.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
9.00	0.00	0.00	0.00	0.00	0.06	0.00	0.06	0.00	0.06
10.00	0.00	0.00	0.00	0.00	0.12	0.00	0.12	0.00	0.12
11.00	0.00	0.00	0.00	0.00	0.18	0.00	0.18	0.00	0.18
12.00	0.00	0.00	0.00	0.00	0.23	0.00	0.23	0.00	0.23
13.00	0.00	0.00	0.00	0.00	0.23	0.01	0.24	0.00	0.24
14.00	0.01	0.00	0.03	0.00	0.23	0.04	0.32	0.05	0.27
15.00	0.15	0.10	0.23	0.10	0.23	0.08	0.89	0.40	0.49
16.00	0.44	0.20	0.61	0.20	0.23	0.09	1.77	1.09	0.68

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - WEST BASIN

CDI Project #: Lake Worth Beach, Florida 33460

RUNOFF CALCULATIONS

Soil Moisture Storage (S_{exist})	5.98 in
Soil Moisture Storage (S_{prop})	3.63 in

3 Year, 1 Hour Rainfall Amount (P):	2.6 in	Figure C-8
25 Year, 3 Day Rainfall Amount (P):	13.0 in	
100 Year, 3 Day Rainfall Amount (P):	16.0 in	Figure C-9

RETENTION: 3-YEAR, 1-HR RUNOFF CALCULATIONS:

$$Q = (P - (0.2XS))^2 / (P + (0.8*S))$$
$$= 0.64 \text{ in}$$

$$\text{Volume} = Q \times \text{Site Area} \times 1'/12''$$
$$= 0.6 \text{ in} \times 1.08 \times 1'/12'' = 0.06 \text{ AF}$$

0.06 AF Provided at Elev. **8.98** Ft

3-Year 1-Hour Storm Contained On-Site - OKAY

25-YEAR, 3-DAY RUNOFF CALCULATIONS (PRE v. PST DEVELOPMENT):

Existing:

$$Q = (P - (0.2XS))^2 / (P + (0.8*S))$$
$$= 7.8 \text{ in}$$

$$\text{Volume} = Q \times \text{Site Area} \times 1'/12''$$
$$= 7.8 \text{ in} \times 1.08 \times 1'/12'' = 0.71 \text{ AF}$$

Proposed:

$$Q = (P - (0.2XS))^2 / (P + (0.8*S))$$
$$= 9.5 \text{ in}$$

$$\text{Volume} = Q \times \text{Site Area} \times 1'/12''$$
$$= 9.5 \text{ in} \times 1.08 \times 1'/12'' = 0.85 \text{ AF}$$

Pre- vs. Post- = 0.15 AF of storage required
0.15 AF Provided at Elev. **10.51** Ft

Pre v. Post Development Runoff Contained On-Site - OKAY

100-YEAR, 3-DAY ZERO DISCHARGE RUNOFF CALCULATIONS:

Proposed:

$$Q = (P - (0.2XS))^2 / (P + (0.8*S))$$
$$= 12.3 \text{ in}$$

$$\text{Volume} = Q \times \text{Site Area} \times 1'/12''$$
$$= 12.3 \text{ in} \times 1.08 \times 1'/12'' = 1.11 \text{ AF}$$

1.11 AF Provided at Elev. **15.39** Ft

Below FFE of 15.50 - OKAY

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - WEST BASIN

CDI Project #: Lake Worth Beach, Florida 33460

WATER QUALITY CALCULATIONS

1-inch Over the Project Area

$$\text{(Treated Volume)} \quad 1\text{-inch} \quad * \quad 1\text{-ft}/12\text{-in} \quad * \quad \frac{1.08}{\text{PROJECT AREA (AC)}} = \frac{0.09}{\text{TREATED VOLUME}} \text{ ac-ft}$$

2.5-inches Times the Percent Impervious

$$\text{(Site Area)} \quad \frac{1.08}{\text{PROJECT AREA (AC)}} - \left(\frac{0.00}{\text{LAKES (AC)}} + \frac{0.21}{\text{ROOFS (AC)}} \right) = \frac{0.87}{\text{SITE AREA}} \text{ ac}$$

$$\text{(Impervious Area)} \quad \frac{0.87}{\text{SITE AREA (AC)}} - \frac{0.48}{\text{PERVIOUS AREA (AC)}} = \frac{0.39}{\text{IMPERVIOUS AREA}} \text{ ac}$$

$$\text{(\% Impervious)} \quad \frac{\text{IMPERVIOUS AREA} * 100\%}{\text{SITE AREA (AC)}} = 44.83\%$$

$$\text{(2.5-in * \% Imp.)} \quad 2.5\text{-inches} * \frac{44.83\%}{\text{PERCENT IMPERVIOUS}} = \frac{1.12}{\text{INCHES TO BE TREATED}} \text{ in}$$

$$\text{(Treated Volume)} \quad \frac{1.12}{\text{TREATED (IN)}} * 1\text{-ft}/12\text{-in} * \frac{1.08}{\text{PROJECT AREA - LAKES (AC)}} = \frac{0.11}{\text{TREATED VOLUME}} \text{ ac-ft}$$

THEREFORE 2.5-INCHES X %IMP GOVERNS

$$\text{Required WQ Treatment} = \frac{0.11}{\text{ac-ft}}$$

$$\text{Provided WQ Treatment (via lowest basin elevation = 13.90)} = \frac{0.27}{\text{ac-ft}}$$

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - WEST BASIN

CDI Project #: Lake Worth Beach, Florida 33460

EXFILTRATION TRENCH DESIGN

Minimum Ground Elevation = 12.90 (Lowest Grate Elevation)

Weir Elevation = 12.90 (Lowest Grate Elevation)

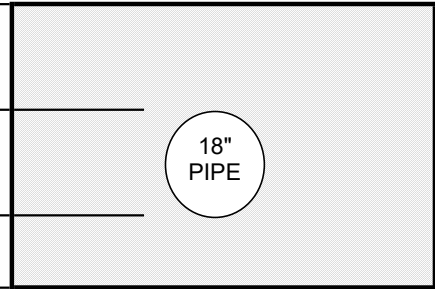
Trench Top Elevation = 11.00

Pipe Overt Elevation = 10.00

Pipe Invert Elevation = 8.50

Trench Bottom Elevation = 7.00

Highwater Elevation = 3.00 



STANDARD FORMULA:

$$L = V / (K(H_2W + 2H_2Du - Du^2 + 2H_2Ds) + (1.39 \times 10^{-4})W Du)$$

 <== INPUT ONLY IN GRAY CELLS

V	Volume Treated (acre-in)	<u>2.81</u>
W	Trench Width (feet)	<u>8</u>
K	Hydraulic Conductivity (cfs/ft ² -ft.head)	<u>3.85E-04</u>
H ₂	Depth to Water Table (feet)	<u>4.00</u>
Du	Non Saturated Trench Depth (feet)	<u>4.00</u>
Ds	Saturated Trench Depth (feet)	<u>0.00</u>

Retain 3Y-1H Storm Event on Site

1.08 ac x 2.6" = 2.81 Ac-In

Average per NV5 Geotech Report

<== H₂ can extend no lower than the trench bottom

L Trench Volume for 2.6" of storage = 123

TRENCH VOLUME PROVIDED:

$$V = L * (K(H_2W + 2H_2Du - Du^2 + 2H_2Ds) + (1.39 \times 10^{-4})W Du)$$

 <== INPUT ONLY IN GRAY CELLS

L	Length of Trench Provided	<u>123</u>
W	Trench Width (feet)	<u>8</u>
K	Hydraulic Conductivity (cfs/ft ² -ft.head)	<u>3.85E-04</u>
H ₂	Depth to Water Table (feet)	<u>4</u>
Du	Non Saturated Trench Depth (feet)	<u>4</u>
Ds	Saturated Trench Depth (feet)	<u>0</u>

<== H₂ can extend no lower than the trench bottom

V Volume Treated (acre-in) 2.82

Volume Treated (acre-ft) 0.23

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - WEST BASIN

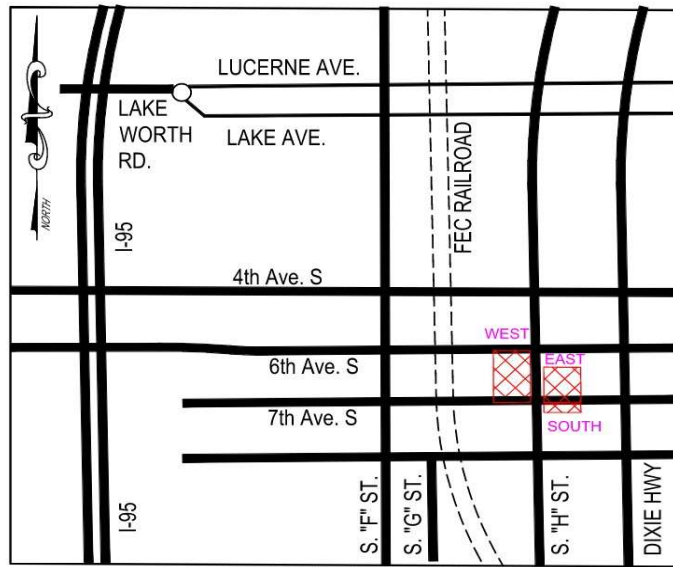
CDI Project #: Lake Worth Beach, Florida 33460

PERVIOUS PAVEMENT VOLUME CALCULATIONS

Pervious pavement Volume Calculation:

Area of Pervious Pavement	0.20 Acres
Depth of Pervious Pavement	6 Inches
Depth of Reservoir Layer	36 Inches
Void Ratio for #4 rock reservoir	30 Percentage
Storage Volume Treated (AF)	0.09 AF

NOTE: DOES NOT ACCOUNT FOR PERCOLATION VOLUME



SEC. 28 - TWP. 44 S - RGE. 43 E

LOCATION MAP
N.T.S.

STORM WATER MANAGEMENT CALCULATIONS
FOR
Pinnacle on Sixth - EAST BASIN

South H Street Between 6th & 7th Avenues South
Lake Worth Beach, Florida 33460
CDI Project No. 25974-01

June 10, 2026

Datum = NAVD '88

Civil Design, Inc.
1400 Centrepark Blvd, Suite 905
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Date

COVER

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - EAST BASIN

CDI Project #: Lake Worth Beach, Florida 33460

LAND USE BREAKDOWN

EXISTING

Site Area = **0.77 ac**
Non-Contributing Area = **0.00 ac**
Offsite Area = **0.00 ac**
Basin Area = **0.77 ac**

	Acres	%	Grading		Avg. Elev.
			From	To	
Impervious Area					
Building	0.16 ac	(21%)	14.76	15.45	15.11
Pavement/Concrete	0.14 ac	(18%)	12.90	15.45	14.18
Shellrock Lot (50%)	0.10 ac	(13%)	13.00	14.20	13.60
Pervious Area					
Green Space	0.27 ac	(35%)	12.90	14.50	13.70
Shellrock Lot (50%)	0.10 ac	(13%)	13.00	14.20	13.60
Subtotal Impervious Areas	0.40 ac	(52%)			
Subtotal Pervious Areas	0.37 ac	(48%)			

Find Curve Number:

Overall Weighted Average Elev. = 12.29

Avg. Pervious Ground El. = 13.67
Control Elevation = **3.0** Water Table
Depth to Water Table = **10.67**
Soil Type = **Coastal**

Soil Storage Table

(SFWMMD's Vol. IV, Basis of Review, page E-2)

Depth to W.T. (ft)	Coastal Storage (in)	Flatwoods Storage (in)	Depression Storage (in)
1.0	0.6	0.6	0.6
2.0	2.5	2.5	2.1
3.0	6.6	5.4	4.4
4.0	10.9	9.0	6.8

Pervious Area = 0.37 ac
Storage from Table = **8.18 in** (w/ 25% compaction)
Available Soil Storage = 0.25 af
Soil Moisture Storage (S) = **3.93 in**
Curve Number = **72**

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - EAST BASIN

CDI Project #: Lake Worth Beach, Florida 33460

LAND USE BREAKDOWN

PROPOSED

Site Area = **0.77 ac**
 Non-Contributing Area = **0.00 ac** less dedications
 Offsite Area = **0.00 ac**
 Basin Area = **0.77 ac**

	Acres	%	Grading From	To	Avg. Elev.
Impervious Area					
Building	0.21 ac	(27%)	14.50		14.50
Pavement/Concrete	0.20 ac	(26%)	12.70	14.50	13.60
Pervious Pvmt (50%)	0.07 ac	(9%)	12.90	13.95	13.43
Pervious Area					
Green Space	0.22 ac	(29%)	12.70	14.50	13.60
Pervious Pvmt (50%)	0.07 ac	(9%)	12.90	13.95	13.43
Subtotal Impervious Areas	0.48 ac	(62%)			
Subtotal Pervious Areas	0.29 ac	(38%)			

Therefore, **0.08 ac** New Impervious Area **Overall Weighted Average Elev. = 13.81**

Find Curve Number:

Avg. Pervious Ground El. = 13.56
 Control Elevation = **3.00** Water Table / High Water (Geotech Report)
 Depth to Water Table = **10.56**
 Soil Type = Coastal

Soil Storage Table

(SFWMMD's Vol. IV, Basis of Review, page E-2)

Depth to W.T. (ft)	Coastal Storage (in)	Flatwoods Storage (in)	Depression Storage (in)
1.0	0.6	0.6	0.6
2.0	2.5	2.5	2.1
3.0	6.6	5.4	4.4
4.0	10.9	9.0	6.8

Pervious Area = 0.29
 Storage from Table = 8.18 in (w/ 25% compaction)
 Avail Soil Storage = 0.20 af
 Soil Moisture Storage (S) = 3.08 in
 Curve Number = **76**

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - EAST BASIN

CDI Project #: Lake Worth Beach, Florida 33460

STAGE -STORAGE CALCULATIONS

EXISTING

Starting Stage **3.00**
Ending Stage **15.00**
Stage Increment **1.00**

Impervious Areas	Pervious Areas
------------------	----------------

Name	Pavement/ Concrete	Shellrock Lot (50%)	0	Green Space	Shellrock Lot (50%)
Area	0.14	0.10	0.00	0.27	0.10
Start Elev	12.90	13.00	0.00	12.90	13.00
End Elev	15.45	14.20	0.00	14.50	14.20

Stage Feet	Linear Storage Ac-ft	Linear Storage Ac-ft	Vert Storage Ac-ft	Linear Storage Ac-ft	Linear Storage Ac-ft	Total Storage Ac-ft
NAVD						
3.00	0.00	0.00	0.00	0.00	0.00	0.00
4.00	0.00	0.00	0.00	0.00	0.00	0.00
5.00	0.00	0.00	0.00	0.00	0.00	0.00
6.00	0.00	0.00	0.00	0.00	0.00	0.00
7.00	0.00	0.00	0.00	0.00	0.00	0.00
8.00	0.00	0.00	0.00	0.00	0.00	0.00
9.00	0.00	0.00	0.00	0.00	0.00	0.00
10.00	0.00	0.00	0.00	0.00	0.00	0.00
11.00	0.00	0.00	0.00	0.00	0.00	0.00
12.00	0.00	0.00	0.00	0.00	0.00	0.00
13.00	0.00	0.00	0.00	0.00	0.00	0.00
14.00	0.03	0.04	0.00	0.10	0.04	0.22
15.00	0.12	0.14	0.00	0.37	0.14	0.78

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - EAST BASIN

CDI Project #: Lake Worth Beach, Florida 33460

STAGE-STORAGE CALCULATIONS PROPOSED

Starting Stage **3.00**
Ending Stage **15.00**
Stage Increment **1.00**

Name	Impervious Areas		Pervious Areas		Trench	Pervious Pavement Storage			
	Pavement /Concrete	Pervious Pvmt (50%)	Green Space	Pervious Pvmt (50%)					
Area	0.20	0.07	0.22	0.07	0.17 (AF)	0.06 (AF)			
Start Elev	12.70	12.90	12.70	12.90	8.00	12.85			
End Elev	14.50	13.95	14.50	13.95	12.00	15.35			
							Post Dev	Pre Dev	
Stage	Linear	Linear	Linear	Linear	Linear	Linear	Total	Total	
Feet	Storage	Storage	Storage	Storage	Storage	Storage	Storage	Storage	Difference
NAVD	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft
3.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
4.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
5.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
6.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
7.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
8.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
9.00	0.00	0.00	0.00	0.00	0.04	0.00	0.04	0.00	0.04
10.00	0.00	0.00	0.00	0.00	0.08	0.00	0.08	0.00	0.08
11.00	0.00	0.00	0.00	0.00	0.12	0.00	0.12	0.00	0.12
12.00	0.00	0.00	0.00	0.00	0.17	0.00	0.17	0.00	0.17
13.00	0.01	0.00	0.01	0.00	0.17	0.00	0.18	0.00	0.18
14.00	0.09	0.07	0.10	0.07	0.17	0.03	0.53	0.22	0.31
15.00	0.29	0.14	0.32	0.14	0.17	0.05	1.12	0.78	0.34

STORM WATER MANAGEMENT CALCULATIONS

Project Name: **Pinnacle on Sixth - EAST BASIN**

CDI Project #: **Lake Worth Beach, Florida 33460**

RUNOFF CALCULATIONS

Soil Moisture Storage (S_{exist})	3.93 in
Soil Moisture Storage (S_{prop})	3.08 in

3 Year, 1 Hour Rainfall Amount (P):	2.6 in	Figure C-8
25 Year, 3 Day Rainfall Amount (P):	13.0 in	
100 Year, 3 Day Rainfall Amount (P):	16.0 in	Figure C-9

RETENTION: 3-YEAR, 1-HR RUNOFF CALCULATIONS:

$$Q = (P - (0.2XS))^2 / (P + (0.8*S))$$
$$= 0.78 \text{ in}$$

$$\text{Volume} = Q \times \text{Site Area} \times 1' / 12''$$
$$= 0.8 \text{ in} \times 0.77 \times 1' / 12'' = 0.05 \text{ AF}$$

0.05 AF Provided at Elev. **9.20** Ft

3-Year 1-Hour Storm Contained On-Site - OKAY

25-YEAR, 3-DAY RUNOFF CALCULATIONS (PRE v. PST DEVELOPMENT):

Existing:

$$Q = (P - (0.2XS))^2 / (P + (0.8*S))$$
$$= 9.2 \text{ in}$$

$$\text{Volume} = Q \times \text{Site Area} \times 1' / 12''$$
$$= 9.2 \text{ in} \times 0.77 \times 1' / 12'' = 0.593$$

Proposed:

$$Q = (P - (0.2XS))^2 / (P + (0.8*S))$$
$$= 9.9 \text{ in}$$

$$\text{Volume} = Q \times \text{Site Area} \times 1' / 12''$$
$$= 9.9 \text{ in} \times 0.77 \times 1' / 12'' = 0.636$$

Pre- vs. Post- = 0.043 of storage required
0.043 Provided at Elev. **9.05** Ft

Pre v. Post Development Runoff Contained On-Site - OKAY

100-YEAR, 3-DAY ZERO DISCHARGE RUNOFF CALCULATIONS:

Proposed:

$$Q = (P - (0.2XS))^2 / (P + (0.8*S))$$
$$= 12.8 \text{ in}$$

$$\text{Volume} = Q \times \text{Site Area} \times 1' / 12''$$
$$= 12.8 \text{ in} \times 0.77 \times 1' / 12'' = 0.82 \text{ AF}$$

0.82 AF Provided at Elev. **14.49** Ft

Below FFE of 14.50 - OKAY

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - EAST BASIN

CDI Project #: Lake Worth Beach, Florida 33460

WATER QUALITY CALCULATIONS

1-inch Over the Project Area

$$\begin{array}{l} \text{(Treated Volume)} \quad 1\text{-inch} \quad * \quad 1\text{-ft}/12\text{-in} \quad * \quad \frac{0.77}{\text{PROJECT AREA (AC)}} \quad = \quad \boxed{0.06} \text{ ac-ft} \\ \text{TREATED VOLUME} \end{array}$$

2.5-inches Times the Percent Impervious

$$\begin{array}{l} \text{(Site Area)} \quad \frac{0.77}{\text{PROJECT AREA (AC)}} - \left(\frac{0.00}{\text{LAKES (AC)}} + \frac{0.21}{\text{ROOFS (AC)}} \right) = \frac{0.56}{\text{SITE AREA}} \text{ ac} \end{array}$$

$$\begin{array}{l} \text{(Impervious Area)} \quad \frac{0.56}{\text{SITE AREA (AC)}} - \frac{0.29}{\text{PERVIOUS AREA (AC)}} = \frac{0.27}{\text{IMPERVIOUS AREA}} \text{ ac} \end{array}$$

$$\begin{array}{l} \text{(\% Impervious)} \quad \frac{\text{IMPERVIOUS AREA} * 100\%}{\text{SITE AREA (AC)}} = 48.21\% \end{array}$$

$$\begin{array}{l} \text{(2.5-in * \% Imp.)} \quad 2.5\text{-inches} * \frac{48.21\%}{\text{PERCENT IMPERVIOUS}} = \frac{1.21}{\text{INCHES TO BE TREATED}} \text{ in} \end{array}$$

$$\begin{array}{l} \text{(Treated Volume)} \quad \frac{1.21}{\text{TREATED (IN)}} * 1\text{-ft}/12\text{-in} * \frac{0.77}{\text{PROJECT AREA - LAKES (AC)}} = \boxed{0.08} \text{ ac-ft} \\ \text{TREATED VOLUME} \end{array}$$

THEREFORE 2.5-INCHES X %IMP GOVERNS

$$\text{Required WQ Treatment} = \boxed{0.08} \text{ ac-ft}$$

$$\text{Provided WQ Treatment (via lowest basin elevation = 12.90)} = \boxed{0.17} \text{ ac-ft}$$

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - EAST BASIN

CDI Project #: Lake Worth Beach, Florida 33460

EXFILTRATION TRENCH DESIGN

Minimum Ground Elevation = 12.90 (Lowest Grate Elevation)

Weir Elevation = 12.90 (Lowest Grate Elevation)

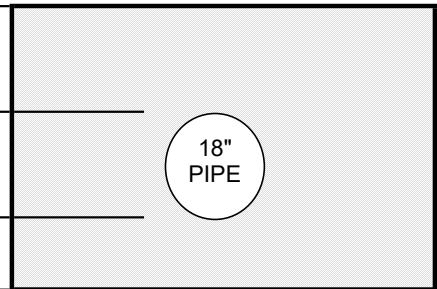
Trench Top Elevation = 11.00

Pipe Overt Elevation = 10.00

Pipe Invert Elevation = 8.50

Trench Bottom Elevation = 7.00

Highwater Elevation = 3.00 



STANDARD FORMULA:

$$L = V / (K(H_2W + 2H_2Du - Du^2 + 2H_2Ds) + (1.39 \times 10^{-4}) * W * Du)$$

 <== INPUT ONLY IN GRAY CELLS

V	Volume Treated (acre-in)	<u>2.00</u>
W	Trench Width (feet)	<u>8</u>
K	Hydraulic Conductivity (cfs/ft ² -ft.head)	<u>3.85E-04</u>
H ₂	Depth to Water Table (feet)	<u>4.00</u>
Du	Non Saturated Trench Depth (feet)	<u>4.00</u>
Ds	Saturated Trench Depth (feet)	<u>0.00</u>

Retain 3Y-1H Storm Event on Site

0.77 ac x 2.6" = 2.00 Ac-In

Average per NV5 Geotech Report

<== H₂ can extend no lower than the trench bottom

L Trench Volume for 2.6" of storage = 87

TRENCH VOLUME PROVIDED:

$$V = L * (K(H_2W + 2H_2Du - Du^2 + 2H_2Ds) + (1.39 \times 10^{-4}) * W * Du)$$

 <== INPUT ONLY IN GRAY CELLS

L	Length of Trench Provided	<u>87</u>
W	Trench Width (feet)	<u>8</u>
K	Hydraulic Conductivity (cfs/ft ² -ft.head)	<u>3.85E-04</u>
H ₂	Depth to Water Table (feet)	<u>4</u>
Du	Non Saturated Trench Depth (feet)	<u>4</u>
Ds	Saturated Trench Depth (feet)	<u>0</u>

<== H₂ can extend no lower than the trench bottom

V Volume Treated (acre-in) 1.99

Volume Treated (acre-ft) 0.17

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - EAST BASIN

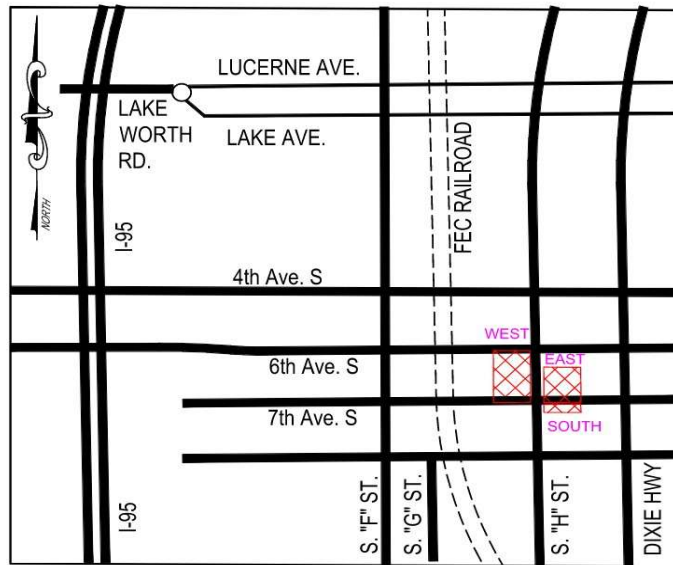
CDI Project #: Lake Worth Beach, Florida 33460

PERVIOUS PAVEMENT VOLUME CALCULATIONS

Pervious pavement Volume Calculation:

Area of Pervious Pavement	0.14 Acres
Depth of Pervious Pavement	6 Inches
Depth of Reservoir Layer	36 Inches
Void Ratio for #4 rock reservoir	30 Percentage
Storage Volume Treated (AF)	<u>0.06</u> AF

NOTE: DOES NOT ACCOUNT FOR PERCOLATION VOLUME



SEC. 28 - TWP. 44 S - RGE. 43 E

LOCATION MAP
N.T.S.

STORM WATER MANAGEMENT CALCULATIONS
FOR
Pinnacle on Sixth - SOUTH BASIN

South H Street Between 6th & 7th Avenues South
Lake Worth Beach, Florida 33460
CDI Project No. 25974-01

June 10, 2026

Datum = NAVD '88

Civil Design, Inc.
1400 Centrepark Blvd, Suite 905
West Palm Beach, FL 33401
Tel: 561.659.5760
Fax: 561.659.5772
www.civil-design.com

Registry No. 9664

CDI CIVIL DESIGN, INC.
Service Driven Design ~ Engineered Results

Date

COVER

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - SOUTH BASIN

CDI Project #: Lake Worth Beach, Florida 33460

LAND USE BREAKDOWN

EXISTING

Site Area = **0.15 ac**
 Non-Contributing Area = **0.00 ac**
 Offsite Area = **0.00 ac**
 Basin Area = **0.15 ac**

	Acres	%	Grading From	To	Avg. Elev.
Impervious Area					
Building	0.02 ac	(13%)	15.00		15.00
Pavement/Concrete	0.06 ac	(40%)	13.10	15.00	14.05
Pervious Area					
Green Space	0.07 ac	(47%)	12.20	14.50	13.35
Subtotal Impervious Areas	0.08 ac	(53%)			
Subtotal Pervious Areas	0.07 ac	(47%)			

Find Curve Number:

Overall Weighted Average Elev. = 13.85

Avg. Pervious Ground El. = 13.35
 Control Elevation = **3.0** Water Table
 Depth to Water Table = **10.35**
 Soil Type = Coastal

Soil Storage Table

(SFWMD's Vol. IV, Basis of Review, page E-2)

Depth to W.T. (ft)	Coastal Storage (in)	Flatwoods Storage (in)	Depression Storage (in)
1.0	0.6	0.6	0.6
2.0	2.5	2.5	2.1
3.0	6.6	5.4	4.4
4.0	10.9	9.0	6.8

Pervious Area = 0.07 ac
 Storage from Table = 8.18 in (w/ 25% compaction)
 Available Soil Storage = 0.05 af
 Soil Moisture Storage (S) = 3.82 in
 Curve Number = **72**

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - SOUTH BASIN

CDI Project #: Lake Worth Beach, Florida 33460

LAND USE BREAKDOWN

PROPOSED

Site Area = **0.15 ac**
 Non-Contributing Area = **0.00 ac** less dedications
 Offsite Area = **0.00 ac**
 Basin Area = **0.15 ac**

			Grading		Avg.
	Acres	%	From	To	Elev.
Impervious Area					
Pavement/Concrete	0.05 ac	(33%)	12.00	13.30	12.65
Pervious Pvmt (50%)	0.01 ac	(4%)	12.25	12.80	12.53
Pervious Area					
Green Space	0.09 ac	(59%)	12.07	14.00	13.04
Pervious Pvmt (50%)	0.01 ac	(4%)	12.25	12.80	12.53
Subtotal Impervious Areas	0.06 ac	(37%)			
Subtotal Pervious Areas	0.09 ac	(63%)			

Therefore, **-0.02 ac New Impervious Area** **Overall Weighted Average Elev. = 12.87**

Find Curve Number:

Avg. Pervious Ground El. = 13.00
 Control Elevation = 3.00 Water Table / High Water (Geotech Report)
 Depth to Water Table = 10.00
 Soil Type = Coastal

Soil Storage Table

(SFWM's Vol. IV, Basis of Review, page E-2)

Depth to W.T. (ft)	Coastal Storage (in)	Flatwoods Storage (in)	Depression Storage (in)
1.0	0.6	0.6	0.6
2.0	2.5	2.5	2.1
3.0	6.6	5.4	4.4
4.0	10.9	9.0	6.8

Pervious Area = 0.09
 Storage from Table = 8.18 in (w/ 25% compaction)
 Avail Soil Storage = 0.06 af
 Soil Moisture Storage (S) = 5.12 in
 Curve Number = 66

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - SOUTH BASIN

CDI Project #: Lake Worth Beach, Florida 33460

STAGE -STORAGE CALCULATIONS

EXISTING

Starting Stage **3.00**
Ending Stage **15.00**
Stage Increment **1.00**

Impervious Areas			Pervious Areas		
------------------	--	--	----------------	--	--

Name	Pavement/ Concrete	0	0	Green Space	0
Area	0.06	0.00	0.00	0.07	0.00
Start Elev	13.10	0.00	0.00	12.20	0.00
End Elev	15.00	0.00	0.00	14.50	0.00

Stage	Linear	Vert	Vert	Linear	Vert	Total
Feet	Storage	Storage	Storage	Storage	Storage	Storage
NAVD	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft
3.00	0.00	0.00	0.00	0.00	0.00	0.00
4.00	0.00	0.00	0.00	0.00	0.00	0.00
5.00	0.00	0.00	0.00	0.00	0.00	0.00
6.00	0.00	0.00	0.00	0.00	0.00	0.00
7.00	0.00	0.00	0.00	0.00	0.00	0.00
8.00	0.00	0.00	0.00	0.00	0.00	0.00
9.00	0.00	0.00	0.00	0.00	0.00	0.00
10.00	0.00	0.00	0.00	0.00	0.00	0.00
11.00	0.00	0.00	0.00	0.00	0.00	0.00
12.00	0.00	0.00	0.00	0.00	0.00	0.00
13.00	0.00	0.00	0.00	0.01	0.00	0.01
14.00	0.01	0.00	0.00	0.05	0.00	0.06
15.00	0.06	0.00	0.00	0.12	0.00	0.18

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - SOUTH BASIN

CDI Project #: Lake Worth Beach, Florida 33460

STAGE-STORAGE CALCULATIONS PROPOSED

Starting Stage **3.00**
Ending Stage **15.00**
Stage Increment **1.00**

Name	Impervious Areas		Pervious Areas		Trench	Pervious Pavement Storage			
	Pavement /Concrete	Pervious Pvmt (50%)	Green Space	Pervious Pvmt (50%)					
Area	0.05	0.01	0.09	0.01	0.03 (AF)	0.01 (AF)			
Start Elev	12.00	12.25	12.07	12.25	8.00	12.85			
End Elev	13.30	12.80	14.00	12.80	12.00	15.35			
							Post Dev	Pre Dev	
Stage	Linear	Linear	Linear	Linear	Linear	Linear	Total	Total	
Feet	Storage	Storage	Storage	Storage	Storage	Storage	Storage	Storage	Difference
NAVD	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft	Ac-ft
3.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
4.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
5.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
6.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
7.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
8.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
9.00	0.00	0.00	0.00	0.00	0.01	0.00	0.01	0.00	0.01
10.00	0.00	0.00	0.00	0.00	0.02	0.00	0.02	0.00	0.02
11.00	0.00	0.00	0.00	0.00	0.02	0.00	0.02	0.00	0.02
12.00	0.00	0.00	0.00	0.00	0.03	0.00	0.03	0.00	0.03
13.00	0.02	0.01	0.02	0.01	0.03	0.00	0.08	0.01	0.07
14.00	0.07	0.01	0.08	0.01	0.03	0.00	0.21	0.06	0.15
15.00	0.12	0.02	0.17	0.02	0.03	0.00	0.37	0.18	0.19

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - SOUTH BASIN

CDI Project #: Lake Worth Beach, Florida 33460

RUNOFF CALCULATIONS

Soil Moisture Storage (S_{exist})	3.82 in
Soil Moisture Storage (S_{prop})	5.12 in

3 Year, 1 Hour Rainfall Amount (P):	2.6 in	Figure C-8
25 Year, 3 Day Rainfall Amount (P):	13.0 in	
100 Year, 3 Day Rainfall Amount (P):	16.0 in	Figure C-9

RETENTION: 3-YEAR, 1-HR RUNOFF CALCULATIONS:

$$Q = (P - (0.2XS))^2 / (P + (0.8*S))$$
$$= 0.37 \text{ in}$$

$$\text{Volume} = Q \times \text{Site Area} \times 1' / 12''$$
$$= 0.4 \text{ in X } 0.15 \text{ X } 1' / 12'' = 0.005$$

0.005 Provided at Elev. **8.57** Ft

3-Year 1-Hour Storm Contained On-Site - OKAY

25-YEAR, 3-DAY RUNOFF CALCULATIONS (PRE v. PST DEVELOPMENT):

Existing:

$$Q = (P - (0.2XS))^2 / (P + (0.8*S))$$
$$= 9.3 \text{ in}$$

$$\text{Volume} = Q \times \text{Site Area} \times 1' / 12''$$
$$= 9.3 \text{ in X } 0.15 \text{ X } 1' / 12'' = 0.117$$

Proposed:

$$Q = (P - (0.2XS))^2 / (P + (0.8*S))$$
$$= 8.4 \text{ in}$$

$$\text{Volume} = Q \times \text{Site Area} \times 1' / 12''$$
$$= 8.4 \text{ in X } 0.15 \text{ X } 1' / 12'' = 0.105$$

Pre- vs. Post- = -0.012 of storage required (net decrease in runoff)

Pre v. Post Development Runoff Contained On-Site - OKAY

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - SOUTH BASIN

CDI Project #: Lake Worth Beach, Florida 33460

WATER QUALITY CALCULATIONS

1-inch Over the Project Area

$$\begin{array}{l} \text{(Treated Volume)} \quad 1\text{-inch} \quad * \quad 1\text{-ft}/12\text{-in} \quad * \quad \frac{0.15}{\text{PROJECT AREA (AC)}} = \frac{0.01}{\text{TREATED VOLUME}} \text{ ac-ft} \end{array}$$

2.5-inches Times the Percent Impervious

$$\begin{array}{l} \text{(Site Area)} \quad \frac{0.15}{\text{PROJECT AREA (AC)}} - \left(\frac{0.00}{\text{LAKES (AC)}} + \frac{0.00}{\text{ROOFS (AC)}} \right) = \frac{0.15}{\text{SITE AREA}} \text{ ac} \end{array}$$

$$\begin{array}{l} \text{(Impervious Area)} \quad \frac{0.15}{\text{SITE AREA (AC)}} - \frac{0.09}{\text{PERVIOUS AREA (AC)}} = \frac{0.06}{\text{IMPERVIOUS AREA}} \text{ ac} \end{array}$$

$$\begin{array}{l} \text{(\% Impervious)} \quad \frac{\text{IMPERVIOUS AREA} * 100\%}{\text{SITE AREA (AC)}} = 37.33\% \end{array}$$

$$\begin{array}{l} \text{(2.5-in * \% Imp.)} \quad 2.5\text{-inches} * \frac{37.33\%}{\text{PERCENT IMPERVIOUS}} = \frac{0.93}{\text{INCHES TO BE TREATED}} \text{ in} \end{array}$$

$$\begin{array}{l} \text{(Treated Volume)} \quad \frac{0.93}{\text{TREATED (IN)}} * 1\text{-ft}/12\text{-in} * \frac{0.15}{\text{PROJECT AREA - LAKES (AC)}} = \frac{0.02}{\text{TREATED VOLUME}} \text{ ac-ft} \end{array}$$

THEREFORE 2.5-INCHES X %IMP GOVERNS

$$\text{Required WQ Treatment} = \frac{0.02}{\text{ac-ft}}$$

$$\text{Provided WQ Treatment (via lowest basin elevation = 12.00)} = \frac{0.03}{\text{ac-ft}}$$

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - SOUTH BASIN

CDI Project #: Lake Worth Beach, Florida 33460

EXFILTRATION TRENCH DESIGN

Minimum Ground Elevation = 12.90 (Lowest Grate Elevation)

Weir Elevation = 12.90 (Lowest Grate Elevation)

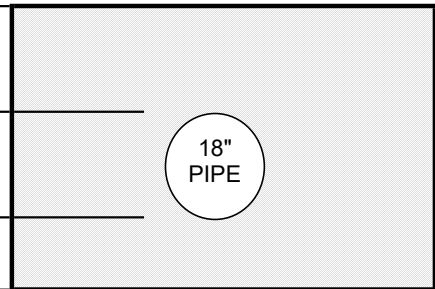
Trench Top Elevation = 11.00

Pipe Overt Elevation = 10.00

Pipe Invert Elevation = 8.50

Trench Bottom Elevation = 7.00

Highwater Elevation = 3.00 



STANDARD FORMULA:

$$L = V / (K(H_2W + 2H_2Du - Du^2 + 2H_2Ds) + (1.39 \times 10^{-4})W Du)$$

 <== INPUT ONLY IN GRAY CELLS

V	Volume Treated (acre-in)	<u>0.39</u>
W	Trench Width (feet)	<u>8</u>
K	Hydraulic Conductivity (cfs/ft ² -ft.head)	<u>3.85E-04</u>
H ₂	Depth to Water Table (feet)	<u>4.00</u>
Du	Non Saturated Trench Depth (feet)	<u>4.00</u>
Ds	Saturated Trench Depth (feet)	<u>0.00</u>

Retain 3Y-1H Storm Event on Site

0.15 ac x 2.6" = 0.39 Ac-In

Average per NV5 Geotech Report

<== H₂ can extend no lower than the trench bottom

L Trench Volume for 2.6" of storage = 17

TRENCH VOLUME PROVIDED:

$$V = L * (K(H_2W + 2H_2Du - Du^2 + 2H_2Ds) + (1.39 \times 10^{-4})W Du)$$

 <== INPUT ONLY IN GRAY CELLS

L	Length of Trench Provided	<u>17</u>
W	Trench Width (feet)	<u>8</u>
K	Hydraulic Conductivity (cfs/ft ² -ft.head)	<u>3.85E-04</u>
H ₂	Depth to Water Table (feet)	<u>4</u>
Du	Non Saturated Trench Depth (feet)	<u>4</u>
Ds	Saturated Trench Depth (feet)	<u>0</u>

<== H₂ can extend no lower than the trench bottom

V Volume Treated (acre-in) 0.39

Volume Treated (acre-ft) 0.03

STORM WATER MANAGEMENT CALCULATIONS

Project Name: Pinnacle on Sixth - SOUTH BASIN

CDI Project #: Lake Worth Beach, Florida 33460

PERVIOUS PAVEMENT VOLUME CALCULATIONS

Pervious pavement Volume Calculation:

Area of Pervious Pavement	0.01 Acres
Depth of Pervious Pavement	6 Inches
Depth of Reservoir Layer	36 Inches
Void Ratio for #4 rock reservoir	30 Percentage
Storage Volume Treated (AF)	0.01 AF

NOTE: DOES NOT ACCOUNT FOR PERCOLATION VOLUME



April 30, 2026

Juan F. Ortega, Ph.D., P.E.
JFO Group, Inc.,
6671 W Indiantown Rd, Suite 50-324
Jupiter, FL 33458

**Engineering and
Public Works Department**

P.O. Box 21229
West Palm Beach, FL 33416-1229
(561) 684-4000
FAX: (561) 684-4050
www.pbc.gov



**Palm Beach County
Board of County
Commissioners**

Sara Baxter, Mayor
Marci Woodward, Vice Mayor

Maria G. Marino
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Joel G. Flores
Maria Sachs
Bobby Powell Jr.

County Administrator

Joseph Abruzzo

**RE: Pinnacle on Sixth
Project #: 260403
Traffic Performance Standards (TPS) Review**

Dear Dr. Ortega:

The Palm Beach County Traffic Division has reviewed the above referenced project Traffic Impact Study, dated March 30, 2026, pursuant to the Traffic Performance Standards in Article 12 of the Palm Beach County (PBC) Unified Land Development Code (ULDC). The project is summarized as follows:

Municipality:	Lake Worth Beach
Location:	South side of 6 th Ave S, just west of Dixie Highway
PCN:	38-43-44-21-15-203-0100/-0130/-0140/-0150/-0160/- 205-0010/-205-0020/-205-0030/-205-0040/-223-0080
Access:	One full access driveway connection onto S H Street, and another full access driveway connection onto 7 th Ave S. <u>(As used in the study and is NOT necessarily an approval by the County through this TPS letter)</u>
Existing Uses:	Single Family Detached = 3 DUs, Multifamily Low-Rise Housing = 8 DUs
Proposed Uses:	Replacing the existing development with two new buildings, resulting in the following Land Use on the site: Multifamily Mid-Rise Housing = 92 DUs
Net Daily Trips:	334 (Proposed-Existing)
Net Peak Hour Trips:	29 (6/23) AM; 29 (17/12) PM (Proposed-Existing)
Project Daily Trips:	418
Project Peak Hr Trips:	34 (8/26) AM; 36 (22/14) PM
Build-out:	December 31, 2030

Based on our review, the Traffic Division has determined the proposed development is within the Coastal Residential Traffic Concurrency Exception Area (TCEA), and therefore, the residential portion of the development is exempt from meeting the TPS. The project is not expected to have any significant impact on non-municipal roadways or intersections and meets the Traffic Performance Standards in Article 12 of the Palm Beach County (PBC) Unified Land Development Code (ULDC).

Please note the receipt of a TPS approval letter does not constitute the review and issuance of a Palm Beach County Right-of-Way (R/W) Construction Permit nor does it eliminate any requirements that may be deemed as site related. For work within Palm Beach County R/W, a detailed review of the project will be provided

"An Equal Opportunity Employer"



Juan F. Ortega, Ph.D., P.E.
April 30, 2026
Page 2

upon submittal for a R/W permit application. The project is required to comply with all Palm Beach County standards and may include R/W dedication.

No building permits are to be issued by the City after the build-out date specified above or as approved by the City. The County traffic concurrency approval is subject to the Project Aggregation Rules set forth in the Traffic Performance Standards Ordinance.

The approval letter shall be valid no longer than one year from date of issuance, unless an application for a Site Specific Development Order has been approved, an application for a Site Specific Development Order has been submitted, or the approval letter has been superseded by another approval letter for the same property.

If you have any questions regarding this determination, please contact me at 561-478-5755 or email MRahman@pbc.gov.

Sincerely,

A handwritten signature in blue ink, appearing to read "Moshir Rahman".

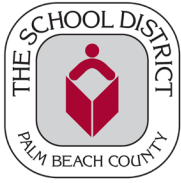
Moshir Rahman, Ph.D., P.E.
Professional Engineer
Engineering and Public Works Dept.
Traffic Division

MR:QB:ep

cc: Addressee

Scott Rodriguez, AICP, Assistant Director-Planning, Zoning, & Preservation Community
Sustainability Department, City of Lake Worth Beach
Quazi Bari, P.E., PTOE, Manager - Growth Management, Traffic Division
Alberto Lopez Tagle, Technical Assistant III, Traffic Division

File: General - TPS - Mun - Traffic Study Review
F:\TRAFFIC\MR\MUN\APPROVED\2026\260403 PINNACLE ON SIXTH.DOCX



THE SCHOOL DISTRICT OF
PALM BEACH COUNTY, FL

PLANNING & INTERGOVERNMENTAL RELATIONS
3661 INTERSTATE PARK ROAD NORTH, 200
RIVIERA BEACH, FL 33404

PHONE: 561-434-8020 / FAX: 561- 434-8942
WWW.PALMBEACHSCHOOLS.ORG

JOYELL SHAW
DIRECTOR

MICHAEL J. BURKE
SUPERINTENDENT

JOSEPH M. SANCHES, MBA
CHIEF OPERATING OFFICER

SCHOOL CAPACITY AVAILABILITY DETERMINATION (SCAD)

Application	Submittal Date	07/07/2026		
	SCAD Case No.	26070701Z/Rezoning and 26070701D/ D.O.		
	FLU /Rezoning/D.O. No.	PZB26-01000001 – City of Lake Worth Beach		
	PCN No. / Address	38434421152030100 /615 S H ST and more		
	Development Name	Pinnacle on Sixth		
	Owner / Agent Name	Pinnacle on Sixth, LLC / Urban Design Studio		
	SAC No.	405		
	Proposed Rezoning Proposed D. O.	Maximum 92 Residential Units 92 Mid-Rise Apt/Condo Units		
Impact Review		South Grade Elementary School	Lake Worth Middle School	Lake Worth High School
	New Students Generated	3	2	2
	Capacity Available	140	312	99
	Projected Utilization	83%	78%	97%
School District Staff’s Recommendation	Based on the findings and evaluation of the proposed development, there will be no negative impact on the School District of Palm Beach County public school system. Therefore, the School District has no comment on this application.			
Validation Period	1) This determination is valid from 07/21/2026 to 07/20/2027 or the expiration date of the site-specific development order approved during the validation period. 2) A copy of the approved D.O. must be submitted to the School District Planning Department prior to 07/20/2027 or this determination will expire automatically on 07/20/2027.			
Notice	School age children may not necessarily be assigned to the public school closest to their residences. Students in Palm Beach County are assigned annually to schools under the authority of the School Board and by direction of the Superintendent, public school attendance zones are subject to change.			

Joyce Cai

School District Representative Signature

Joyce C. Cai, Senior Planner

Print Name & Title of School District Representative

July 21, 2026

Date

joyce.cai@palmbeachschools.org

Email Address

CC: Lauren Pruss, Principal Planner, City of Lake Worth Beach
Joyell Shaw, PIR Director, School District of Palm Beach County