



City of Ketchum

October 7, 2019

Mayor Bradshaw and City Councilors
City of Ketchum
Ketchum, Idaho

Mayor Bradshaw and City Councilors:

Recommendation to accept the Planning and Zoning Commission's recommendations on the PEG Ketchum Hotel Planned Unit Development CUP and related applications (Project), hear from the applicant and staff, hold a public hearing and receive public testimony, and begin deliberations on Project

Background

Staff analysis of the Project and its inter-related applications is provided in **Attachment A**.

PEG Companies has submitted drawings, narratives, and studies for the development of a 100-room hotel at the southwest corner of River Street and State Highway 75/Main Street ("Gateway Site"), including updates received by the City on October 2, 2019. See Attachment B for details.

City department and other expert reviews of the applicant team submittal have been provided as set forth in **Attachment C**.

Public comments that have been received are provided in **Attachment D**.

The Ketchum Planning and Zoning Commission (PZ or Commission) reviewed Attachments A thru D, held individual public hearings for each of the six (6) inter-related applications, and approved/recommended for approval to the Ketchum City Council (Council or KCC) the following:

- (1) The Commission approved the Hotel Floodplain Development/Waterways Design Review Permit – a copy of the signed findings is provided in **Attachment E.1**.

(No further KCC action on this matter is required)

- (2) The Commission recommended approval to KCC the proposed Preliminary Plat – a copy of the signed PZ findings is provided in **Attachment E.2**.

- (3) The Commission moved to advance the pre-Application Design Review to full Design Review (DR) consistent with the reasoned statement provided in **Attachment E.3**.

(Note 1: once overall direction from the Council on hotel waivers are received, it is anticipated that the Applicant will submit for full DR and a publicly noticed hearing before the PZ will be scheduled.

Note 2: Once full DR is approved by the Commission, subject drawings and approval documents are proposed as an Exhibit to the Development Agreement – see item 6, below).

- (4) & (5) The Commission recommended approval to the Council of both the PUD Conditional Use Permit and CUP – a copy of the signed PZ findings is provided in **Attachment E.4**.
- (6) The Commission recommended approval to the Ketchum City Council the proposed Development Agreement consistent with the draft terms and conditions as set forth in **Attachment E.5**.

Analysis

The Commission reviewed and either approved or recommended for approval each of the six (6) inter-related applications. As noted in Attachments E.1 thru E.5, the PZ recommend each of the applications be approved, advanced, or recommended to the Ketchum City Council for approval. Sequentially, these recommendations are summarized in general terms as follows.

Floodplain Development/Waterways Design Review Permit: Floodplain Development and Waterway Design Review Permits are typically reviewed administratively; however, this permit application was forwarded to the Commission in order to provide a comprehensive understanding of each component of the development. Six (6) conditions of approval are provided in the Commission’s approval as noted in the signed findings set forth in **Attachment E.1**. These conditions pertain to the Applicant’s landscape plan, arborist care and on-going maintenance provisions, limiting access, and protecting the twenty-five foot (25’) riparian buffer adjacent Trail Creek before, during and after construction.

Plat: The Commission recommended conditional approval of the plat amendment, which combines the existing three lots into one larger 1.1 acre parcel. Final plat processes, as recommended by the PZ, require the completion of infrastructure improvements, which in this case need to be coordinated with final construction approvals related to the overall Planned Unit Development Conditional Use Permit (PUD) and Design Review. See **Attachment E.2**.

Pre-application Design Review: In its reasoned statement to move the Project toward a full DR submittal, the Commission noted how the mass of the building steps up the sloping grade from the south corner of parcel adjacent to Trail Creek the front property line at River Street in a manner that was “congruous with the natural topography of the development parcel”. The Commission also looked at the Project design against the backdrop of the 2007 city-sponsored Mass and Scale Study for Hotels at the City’s Gateway. See Attachment A. Direction to the Applicant with their full DR submittal includes, in part, meaningfully addressing Commission critiques related to the bulk and mass of the building, softening the hotel’s impact to the adjacent property at 220 E. River Street, reducing the impact of the northeast architectural feature of the hotel, and developing a comprehensive landscaping plan. See **Attachment E.3** for additional details.

Planned Unit Development CUP / Conditional Use Permit: The Commission recommended conditional approval of the joint PUD and CUP applications. Notably, the Commission deliberated the specifics of the Project from both a detailed code standpoint as well as the broader neighborhood and community perspective. Analysis of traffic circulation, highway improvements, and pedestrian safety occurred, and conditions of approval were developed to address concerns. Site constraints were identified and evaluated against the Ketchum Municipal Code (KMC), Ketchum’s Comprehensive Plan, and related neighborhood issues with nearby hotels and sub-areas (along River Street, around Forest Service Park, and towards the condos across Trail Creek). Subject to a number of conditions, assurances, and drawing clarifications, the Commission recommended approval of the Planned Unit Development CUP to the Ketchum City Council as set forth in **Attachment E.4**.

Development Agreement: To help memorialize key provisions of its deliberations, the Commission recommended approval to the Council of a Development Agreement (DA) to be recorded against the Project.

This is a best practice and the DA forwarded to Council reflects all the key provisions contained in the Commission’s Findings of Fact and Conclusions of Law for each of the Project applications as recommended, to date, by the PZ. See **Attachment E.5**.

Financial Impact

Approximately \$107,691 in Planning Permit fees have been paid to date and an additional DR application fee of approximately \$48,000 is anticipated in next fiscal year. The projected FY20 Building Permit fee is estimated to be approximately \$581,405. Future local option tax and tax increment increases are also anticipated.

Attachments

A – Staff Analysis

1. Comparative Hotel PUD Summary Chart
2. Illustration A / Height Diagram from KMC
3. 2007 Gateway Study
4. Vitruvian Walkability Assessment for Ketchum Excerpt and Copy of Ketchum Traffic Authority Meeting Minutes of August 8, 2019 re: Idaho Transportation Department Presentation of HAWK (High Intensity Activated Crosswalk) System at 3-Intersections on Main Street (4th, River & 6th Streets)
5. Staff Parking Demand Analysis
6. State Highway Record of Decision Road Section
7. 2003 Functional Classification Map (River Street, not a Collector)
8. Project Comprehensive Plan Analysis
9. Hotels in Ketchum: Historic and Present Importance as Recognized in City PUD and Other Regulations

B – PEG Ketchum Hotel, LLC Gateway Site Submittal Drawings, Narratives & Studies

1. Narratives (as Presented to PZ thru 8/12/19)

- Written Description of Development
- Development Plan
- Project Analysis | Social Impact Study
- Application of Standards 16.08.080
- Development Schedule
- Parking Analysis | Traffic Study
- Employee Housing Plan
- Sub-Area Analysis
- Contextual Hotel Components Analysis
- Draft Development Agreement
- Standards of Evaluation | Exhibit Key

2. Exhibit Maps (as Presented to PZ thru 8/12/19)

- Plans | Elevations | Sections
- Sun Studies
- Height Analysis
- Civil Drawings
- Landscape Drawings
- Exterior Color Palette
- Dark Sky Compliant Fixtures
- Traffic Study Diagrams
- Plat Map | Lot Line Adjustment
- Public Way Improvements

- Soils Report | Perc Test
- Flood Plain | Riparian Corridor
- List of Adjacent Property Owners
- Waiver List
- Galena Staking Exhibit
- AJC Architecture Virtual Reality Images
- AJC Architecture July 29th and 30th PowerPoint Presentations
- Hales Engineering July 29th PowerPoint Presentation

3. New Maps and Information (since PZ approval on 8/12/19)

- Hales Engineering Access Memorandum, Dated 8/13/19
- In-Group Hospitality Parking Plan, Dated 8/26/19

Following Ketchum Tribute Design Update Drawings, Dated 10/1/19

- Tribute Portfolio
- Council Update
- Architectural Site Plan
- Landscape Plan
- Floor Plans (Each Level)
- Contextual Elevations (5 Drawings)
- Project Perspectives from NE, NW, SE, NE, N & Rooftop Bar
- Waiver Supporting Exhibits
 - Minimum Lot Size for PUD
 - Floor Area Ratio
 - Average Setbacks
 - Height / Contextual Elevations
- Planning & Zoning Comments
- PZ Response

C - City department, agency & peer review letters/memos

D - Public comment – please use following link:

<https://ketchumidaho.org/citycouncil/page/ketchum-boutique-hotel>

E – Signed Planning & Zoning Commission Findings

1. Floodplain Development Permit
2. Plat / Readjustment of Lot Lines
3. Pre-Application Design Review reasoned statement [Placeholder - Full Design Review Findings]
4. PUD Conditional Use Permit and CUP
5. Development Agreement

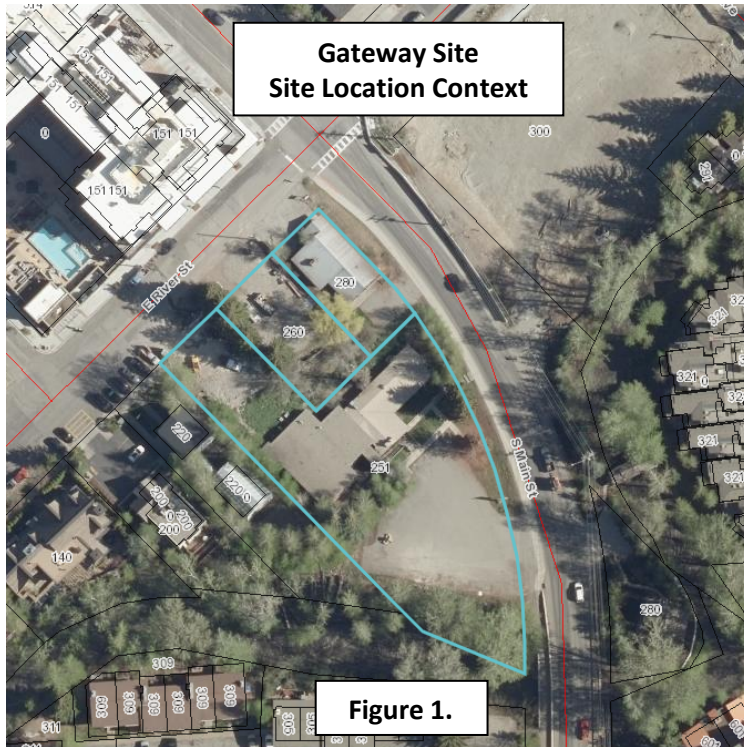
Attachment A - E

Attachment A

Staff Analysis, including following attachments:

1. Comparative Hotel PUD Summary Chart
2. Illustration A / Height Diagram from KMC
3. 2007 Gateway Study Excerpts – Note: a full copy of the Gateway Study is available at the following link:
https://ketchumidaho.org/sites/default/files/fileattachments/planning_and_building/page/2201/gateway_mass_and_scale_study_final_201409151111022457.pdf
4. Vitruvian Walkability Assessment for Ketchum Excerpt **and** Copy of Ketchum Traffic Authority Meeting Minutes of August 8, 2019 re: Idaho Transportation Department Presentation of HAWK (High Intensity Activated Crosswalk) System at 3-Intersections on Main Street (4th, River & 6th Streets)
5. Staff Parking Demand Analysis
6. State Highway Record of Decision Road Section
7. 2003 Functional Classification Map (River Street, not a Collector)
8. Project Comprehensive Plan Analysis
9. Hotels in Ketchum: Historic and Present Importance as Recognized in City PUD and Other Regulations

PEG Companies has submitted drawings, narratives, and studies for the development of a 100-room hotel at the southwest corner of River Street and State Highway 75/Main Street ("Gateway Site", Figure 1, Site Location Context).



The subject development site currently consists of three parcels (parcel boundaries outlined in blue, Figure 1), which are proposed to be combined into a single parcel. The two existing, vacant structures on the site will be removed, as will the existing asphalt surface parking lot and its approach directly onto Main Street/State Highway 75 (SH75).

The site is characterized by an approximately 37' (5796' to 5833') change in grade from the southern property line, which is the mean high-water mark of the northern bank of Trail Creek, to the northern property line, adjacent to River Street. The property is bounded to

the east by State Highway 75 (named Main Street as it passes through Ketchum city limits) and to the west by commercial mixed-use office buildings.

The property is zoned Tourist with Floodplain overlay. The Comprehensive Plan's land use designation for the subject parcel, as well as the parcels that comprise all four corners of the Main Street/River Street intersection, is classified as Commercial Employment. The parcel is also located within the boundaries of the Ketchum Urban Renewal Agency Revenue Allocation Area. The subject parcel has long been considered for development of a hotel, and was included in the 2007 Mass and Scale Study for the Gateway Area, commissioned by the City of Ketchum, to analyze and set forth recommendations for new hotels to be constructed on the northwest corner (Limelight hotel site), southeast corner (Bariteau hotel site), and southwest corner (subject development parcel) of Main Street and River Street.

Six (6) development applications have been submitted concurrently and each application is evaluated in depth in subsequent attachments to this staff report. Two applications which would typically be reviewed administratively (Floodplain/Waterways and Plat/Readjustment of Lot Lines) have been forwarded to the Commission for review in order to provide a more comprehensive understanding of the proposed development; the Commission is the approving body for the Floodplain/Waterways application and is the recommending body for the Lot Line Shift Plat. Of the remaining four applications, the Commission is the decision-making body for the Pre-Application Design Review (which is the first application in the two-stage Pre-

Application Design Review and Design Review process) and recommending body for the Planned Unit Development CUP and Development Agreement.

Staff's analysis finds the Floodplain/Waterways, Conditional Use, and Lot Line Shift Preliminary Plat applications to be in full compliance with the standards of the zoning and subdivision codes and that any discretionary standards of evaluation, including standards related to traffic circulation, have been met or will be met as conditioned. The Development Agreement (or Development Conditions Acceptance Agreement) does not have standards of evaluation per se, but rather is a document intended to memorialize all conditions of approval for all applications related to the proposed development.

Pre-Application Design Review (and later, full Design Review) and the Planned Unit Development application are interrelated in that the zoning and subdivision code provide for hotels are to be evaluated as Planned Unit Developments and Planned Unit Developments are eligible from waivers from the typical zoning standards in a given zoning district, such as height, floor area ratio (FAR), and setbacks. All new hotels approved and constructed or approved but yet-to-be constructed in the past fifteen years have been evaluated on a project-by-project basis through the Planned Unit Development process and standards and PEG's proposal, the Ketchum Boutique Hotel, has applied for certain waivers to typical zoning standards consistent with the Planned Unit Development process.

While detailed analysis and supporting exhibits pertaining to compliance with, or waivers from, Ketchum Municipal Code are contained in attachments A, B, C, and E, the following topics are called out for the Commission's attention:

Architectural Design

- **Topography considerations:** The form of the building follows the topography of the site, stepping mass of the building concurrent as it rises from the south near Trail Creek to the north, adjacent to River Street.
- **Undulation and relief:** The mass of the building steps up the sloping grade from the south corner of the parcel to the front property line at River Street. As the building form terraces down towards Trail Creek, following the contours of the development parcel, the building façade is distinguished by a combination of horizontal and vertical elements, such as the patio railing and window openings, providing additional relief to the staggered facade. The rooftop bar overhang element echoes the porte-cochere and overhangs proposed at the corner of Main & River Streets linking the public amenities proposed with the project including the restaurant, dining area, and lounge.
- **Open space-setbacks-view corridor:** The eastern edge of the proposed hotel includes a section of building that intrudes into the required 32' to 25' setbacks required from the 80' and 66' wide sections of the SH75; a waiver from the SH75 side yard setback is requested, however, the average setback of the building from SH75 exceeds 32'. By maintaining an ample setback from the eastern property line, views to north as one enters Ketchum from the south are maintained. Relative to the existing development on site, the proposed development is further setback from SH 75 than the existing building at the north end of the site.

Traffic Impacts

- **Pedestrian safety:** As a condition of approval, the developer shall install pedestrian-activated Rapid Flashing Beacons on the east and west sides of Main Street at the crosswalk on the north side of River Street are required.
- **North-bound left-turn lane from SH75:** In order to mitigate the potential traffic backup (queuing) that could occur as the result of north-bound vehicles making left-hand turn lanes from SH75 onto River Street, staff recommends as a condition of approval that the Applicant install a left-hand turn lane. The left-hand turn lane as set forth in the traffic impact study (TIS) conducted with this Project should be approximately 100' in length, however, final design is subject to Idaho Transportation Department (ITD) approval.
- **Right turn only from River Street:** As a condition of approval, staff recommends that the developer install signs and striping that indicates "right turn only" permitted for the approaches from River Street onto SH75. This is consistent with the recommendations of Vitruvian, which consulted in 2018 with the City of Ketchum to improve walkability throughout downtown.

Parking

- **Displaced Public Parking Along River Street:** Six (6) parking spaces along River Street will be displaced by the Project, consistent with PEG's Master Plan. The Applicant has offered to make subject displaced spaces available to the general public in its parking garage. Staff recommends that subject spaces be available to the public at no charge while using the conference center, spa, restaurant, and/or bar at the Project.
- **Employee, Food Service, and Hotel Parking:** The Applicant has provided a parking study in Attachment B, which Staff has validated by separate analysis in Attachment B. Both methodologies indicate that the parking garage will have ample parking spaces at the proposed ninety (90) spaces proposed within the Project parking garage.

Public Benefits

- **Rooftop amenities:** The upper floor of the proposed hotel includes a 1,336 enclosed rooftop bar with a 1,642 square foot patio that steps down to a 3,008 square foot terrace; the bar will be open to the public.
- **Ground floor amenities:** The ground floor (accessed from River Street) includes a dining room, lounge, terraces, and two meeting rooms that may be rented for conferences/meetings.
- **Employee housing:** The proposed thirty (30) beds of employee housing will be the largest employee housing offering in the City with approximately 13,756 net square footage of space proposed. This square footage and number of beds exceeds the 25 employee beds required based on the ratio of 1 employee bed per 4 hotel rooms; the applicant has proposed 30 employee beds to be located within 14 employee housing units (12 suites and two bunk rooms).

Waivers

- **Planned Unit Development site size criteria:** The zoning code allows hotels, meeting the definition of "hotel", to be evaluated as Planned Unit Developments. For other types of development, a Planned Unit Development typically must be developed on a site 3.0 acres in size or greater. The subdivision code explicitly allows hotels to apply for a waiver from the 3.0-acre site size standard. This proposed hotel, like the Limelight and Bariteau's hotel project, has requested a waiver from the 3.0-acre site size in order to be evaluated as a Planned Unit Development.
- **Floor Area Ratio:** The total developed gross floor area of the Project, as defined in KMC §17.08.020, is proposed to not exceed a FAR of 1.9 or approximately 88,917 gross floor area exclusive of basement

areas and underground parking as shown in Exhibit B. Staff has reviewed the Subarea Analysis provided by the Applicant. Further, Staff has provided a Comparative Hotel PUD Summary Chart and reviewed the city-sponsored 2007 Gateway Study (see Attachment A). Each of these documents indicated that the proposed Hotel is both by design and use consistent with envisioned plans for the corner of SH75 and River Street. Further, the FAR of the Project is significantly less than the CC-Limelight Hotel and Tourist Zone Bariteau Project.

- **Height/Story:** Planned Unit Developments may request a waiver from the typical maximum height permitted in a zoning district. Ketchum's zoning code measures height as the highest point of the roof of a building to the lowest point of existing grade, natural grade, or finished grade (whichever is lower). In the Tourist Zoning District the typical maximum height permitted is 35'. The façade height of each façade of the proposed hotel is +/- 58' and the technical height, based on the zoning code definition, is approximately 72'. Other approved hotels in the Tourist zone have received height waivers (Bariteau project – 103', Warm Springs Ranch Resort – 93', River Run Hotel Core – 110'); the Limelight hotel is 68', is located in the Community Core, and did not need a height waiver as 68' hotels are permitted in the Community Core.

In addition to the height waiver, the proposed hotel is requesting a waiver and/or interpretation of height relative to KMC and the City's definition of story or the allowance of four stories for a hotel. KMC. As noted, the existing grade from River Street to Trail Creek drops thirty-seven feet (37'), causing in the center of the site a portion of the building to a maximum height of seventy-two feet (72') as measured from existing grade. At this location, the structure consists of four stories of hotel use sandwiched between two stories of public amenity use (employee housing and a roof top bar for the public).

The hotel has four floors above grade on River Street, but due to the topography of the site and significantly lower grade adjacent to Trail Creek, a maximum of six floors/stories are visible in a portion of the hotel (approximately the center of the hotel, where the rooftop bar and lower floor employee housing suites are located). By comparison, Limelight has five floors above grade and two below (parking), because it was constructed on a relatively flat site. Similar to the proposed hotel, the Bariteau hotel is four floors on River Street (plus a partial fifth floor observatory) but seven exposed floors are depicted in the elevation drawings of the south façade because the topography of the site drops in grade as it approaches Trail Creek.

- **Side and front setbacks:** While the proposed hotel respects the 32' and 25' setbacks from the 80' and 60' wide portions of the SH 75 right-of-way and the 25' riparian setback from Trail Creek by providing a 35' setback, waivers from the front and side setbacks are requested.

Additional staff analysis on issues affecting the Project are set forth in **Attachment E**. In particular, staff has developed with each of the six permits, draft findings and a series of suggested conditions of approval for the Commission to consider. If acceptable, these conditions would then be memorialized in a development agreement to assure the Project is built as planned and consistent with city requirements for platting, right of way improvements, floodplain development, waiver allowances, and design issues.

Comparative Hotel PUD Summary Chart

Project Name	Zoning District	Project Address or Location	Maximum number of stories visible above grade (no parking garages located completely below grade on all sides are included)	Maximum height	FAR of all approved buildings on subject development parcel	Development parcel size	Gross square footage of hotel / hotel core	Project Status
Ketchum Boutique Hotel	Tourist	251 S. Main Street (southwest corner of Main and River)	6 from lowest grade on site (includes partial-story rooftop bar) 4 from River Street (partial-story rooftop bar does not have River Street frontage)	72' from lowest grade on site 58' or less from River Street	1.9	1.08 acres	135,915 square feet (includes below grade/partially below grade parking)	Proposed
Limelight	Community Core	151 S. Main Street (northwest corner of Main and River)	5	68'	2.7	1.08 acres	231,400 square feet (includes below grade parking)	Complete
Bariteau hotel (formerly "Auberge")	Tourist	200 S. Main Street (southeast corner of Main and River)	7 from lowest grade on site (includes partial-story rooftop observatory) 4 from River Street (partial-story rooftop observatory also has River Street frontage)	103' from lowest grade on site 58' from River Street, not including observatory 75' from River Street, including observatory	2.4	0.93 acres	149,325 square feet (includes below grade/partially below grade parking)	Under construction
River Run Master Plan (Sun Valley Resort)	Tourist	Existing skier pick up/drop off and parking lot on the east side of the Big Wood River	Undefined ("tent" diagram representing schematic building mass approved)	110'	1.4	21.76 acres*	250,000-300,000 square feet range for hotel core (excludes below grade parking)	Entitled, not yet started
Warm Springs Ranch Resort	Tourist	South of Warm Springs Road between Bald Mountain Road and Geezer Alley	Undefined ("tent" diagram representing schematic building mass approved)	93'	1.43	13.72 acres**	538, 151 for hotel core (excludes below grade parking)	Entitled, not yet started

* Represents the Hotel Core "block" of the River Run Master Plan

** Represents Block 1 of the Warm Springs Ranch Resort master development

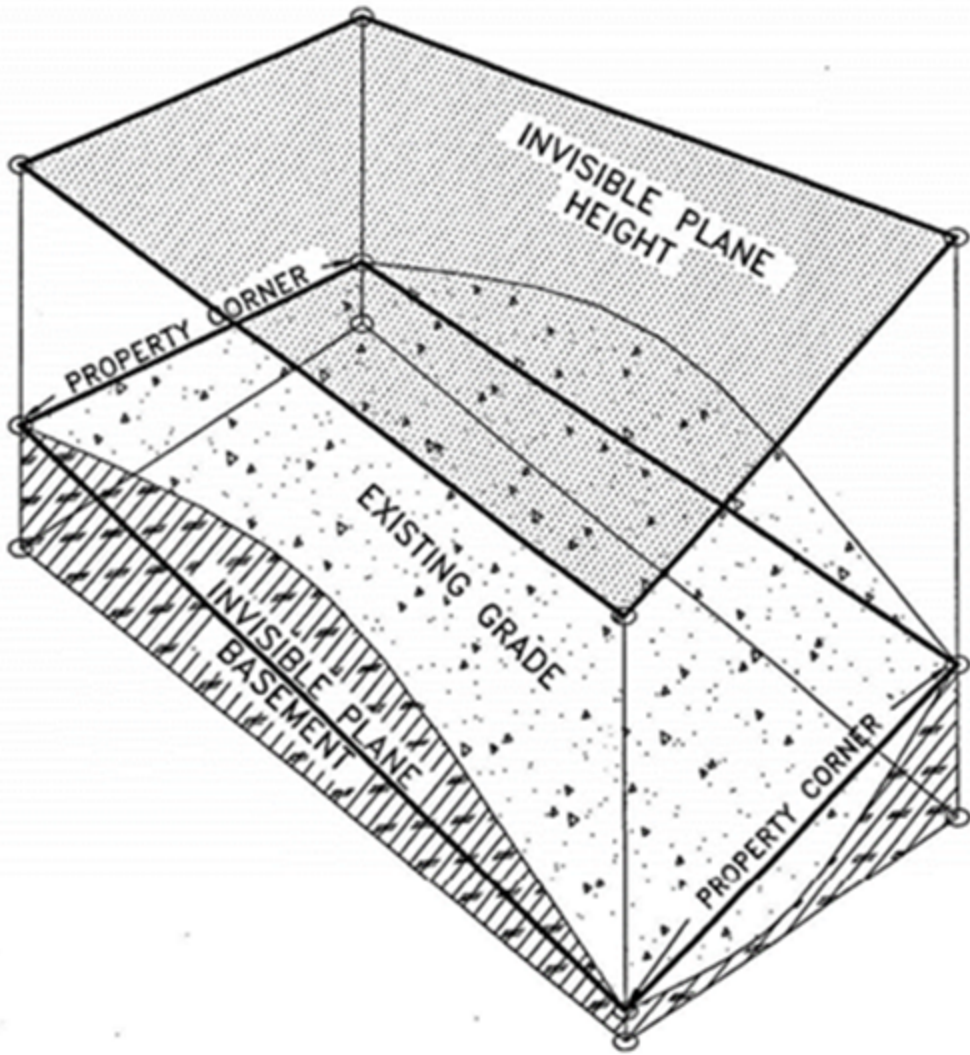


ILLUSTRATION A

2007 GATEWAY STUDY

The design team has been referencing the massing recommendations of the 2007 Ketchum Gateway Study as precedent for overall design principles, building stepping / terracing, and treatment of facades with regards to the context. The current proposed design mimics these recommendations provided by the City of Ketchum in their preferred treatment of this corner

Site 2 - Design Principles

These principles apply to Site 2. A key objective is to provide a transition from a low scale, mountain town character of the neighborhoods to the south of the site to the more urban downtown.

Large lots should be developed to appear as several buildings, rather than a single large structure.

- Several buildings rather than a single large structure will reduce the perceived mass of the development.
- A small element connecting the individual buildings is appropriate.

Step down in height toward a pedestrian way

Step down in height toward Trail Creek

Locate outdoor amenity space adjacent to Trail Creek

Exposed foundation should not exceed 4 feet.

Provide a landscaped area adjacent to Highway to convey a "forested" character

Maintain a high percentage of open space

Set back any potential 4th floor from the sidewalk and locate it adjacent to River Street

Building facade along River Street may reflect a more urban character

Maintain open space at corner as a gateway feature.

Frame gateway plaza with a building element and step down in height adjacent to Highway



View looking southwest



View looking northeast

HEIGHT ANALYSIS - GATEWAY STUDY RECOMMENDATIONS

12

City of Ketchum, Idaho

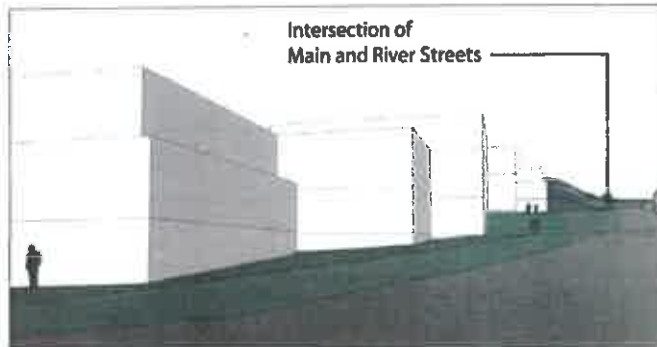
June 12, 2007

Site 2: Alternative 1 - 2 and 3 Stories

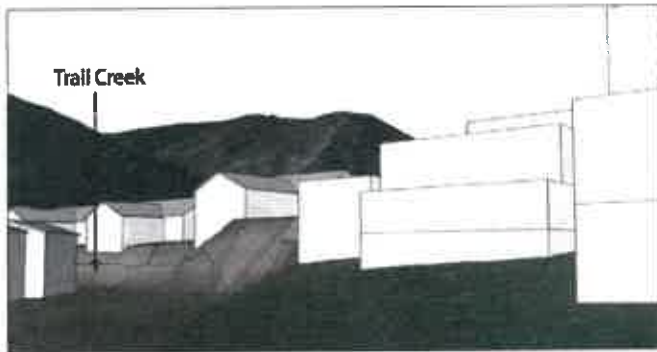
This scenario illustrates the "base" mass of no more than three stories. The third level steps back from the River Street, to minimize its visual impacts and reduce shading of the street.



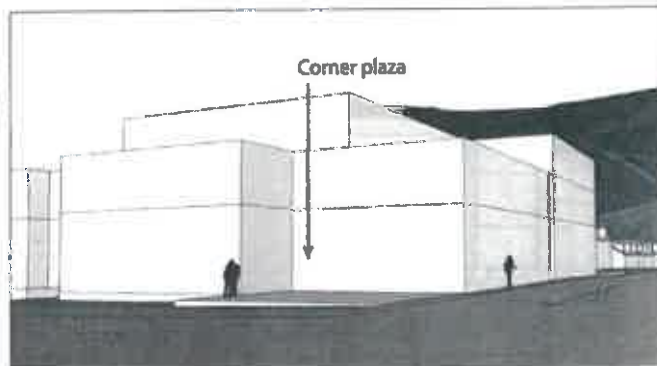
View looking northwest



View looking north along Highway 75 toward River Street from the bridge over Trail Creek. The mass steps up toward River Street with the topography.



View looking west from the southern point of Site 2 toward Bald Mountain,



View looking east toward the northwest corner of Site 1, along River Street. A gateway plaza is located in the foreground, in keeping with the gateway design

HEIGHT ANALYSIS - GATEWAY STUDY PREVIOUS SUBMITTAL

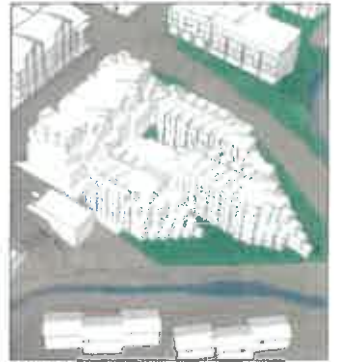
City of Ketchum, Idaho

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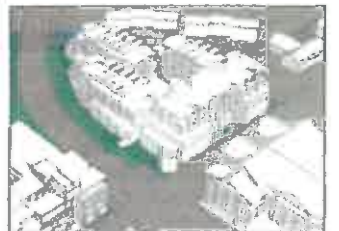
Site 2 Preliminary Applicant Submittal



View looking northwest



View looking northeast



View looking southwest

2007 GATEWAY STUDY

A previous project submittal was included in the Gateway Study as a sample use for this lot. This design maximized buildable area on the site, and was not sensitive to the scale of the surrounding context or topography of the site. This example helps contrast our proposed Boutique hotel design as we are addressing the context in a much more responsible manner than previous designs.

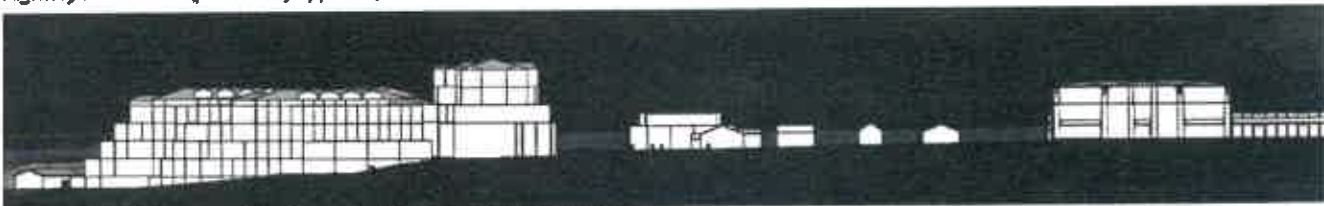
Site 2 Preliminary Applicant Submittal



River Street Elevation (provided by applicant)



Highway 75 Elevation (provided by applicant)



An elevation of the proposed development adjacent to existing residential development to the south (left) and existing, larger commercial development to the north (right)



Ketchum: Through the Looking Glass

A Walkability Assessment | Ketchum, Idaho

DRAFT for City Review



October 2018

Introduction

The City of Ketchum, Idaho hosted a two-day Looking Glass Academy in September 2018 following increased interest by the City's leaders in improving conditions for walking. The 25 workshop attendees included representatives from the City of Ketchum's Administration, the Mayor, City Council Members, MountainRides, and Idaho Transportation Department. Staff from the City of Hailey, as well as several members of the public were also on-hand for the first day workshop.

The purpose of the two-day course was to immerse participants in understanding and assessing the building blocks of walkability through what is known as the 4-C's approach:

- **Comfort:** What human beings need to be comfortable while walking—their walk speeds, their operational needs, and how we experience walking through our senses.
- **Context:** Where we are drawn when walking, what we try to avoid—how land use, land forms, traffic volumes, traffic speeds, road width and lighting impact the desirability of a place.
- **Connectivity:** How linkages in sidewalks and pathways along streets, through parks, and within varying types of land development pose opportunities and barriers for people of all ages and abilities.
- **Crossings:** How access to crosswalks, crosswalk types, pedestrian signals, and signal timing influence how safe a crossing can be in terms of access, driver compliance, and suitability for walking.

By examining and understanding walkability through the lens of these 4-C's, participants are able to make a more cohesive case to planners, engineers and public officials for improved conditions for people who walk. Locations identified by workshop participants as in need of examination were evaluated through the lens of the 4Cs. Recommendations stemming from that review are included in this report.

The five locations are illustrated on the following pages, with recommendations for the City of Ketchum to pursue through city departments, grant applications, and working with ITD to improve conditions for pedestrians along and crossing Main Street (State Highway 75).

The report also includes a set of general recommendations for Ketchum to continue improving walkability in and around town. Many of these items will also improve conditions for bicycling as they relate the Wood River Trail and other bikeways throughout the city.



Time to Hula!

One of the first lessons in walkability is the concept of human comfort. A simple hula hoop, shown here around Ketchum officials, represents a person's personal space, or bubble. The functional width of that bubble is approximately 3 feet, with another foot of clear space needed for someone to walk comfortably down a city street. Two people walking side by side or passing on a sidewalk necessitates at least 6 feet of space to comfortably walk two abreast.

Ketchum: Through the Looking Glass

Ketchum is a city of nearly 3,000 full-time residents with a population and more than 400,000 visitors a year to the area's outdoor recreation and culture sites. Influenced by the adjacent ski slopes, trails, and outdoor features, Ketchum is a mixed-use hub of the north Wood River Valley and draws service sector workers from nearby cities and points as far south as Twin Falls.

From a walkability perspective, Ketchum has a well-connected street system in the downtown core with sidewalks on both sides of many streets. Some streets have sidewalks on only one side and fringe areas are lacking in connected sidewalks. The Wood River Trail bisects the city and provides connections to recreational areas to the north and south, as well as the cities of Hailey and Bellevue to the south. Ketchum also has multi-use sidepaths along several roadways linking to the Wood River Trail.

From an accessibility perspective, many street corners lack modern curb ramps and some streets are still lacking curb cuts. The City of Ketchum is working to retrofit many of these street corners with new curb ramps and recently completed a sidewalk project along the west side of Warm Springs Road to 9th Street. This addition to the walking network provides a connection to residential, commercial and industrial areas in the northwest sector of the City and provides for safer routes for school children to the Hemingway School, Atkinson Park, and YMCA. Main Street (State Highway 75) remains a physical and mental barrier to cross-city walking and bicycling, even with full traffic signals at Sun Valley Road, 5th Street and 1st Street, as well as a Rectangular Rapid Flashing Beacon crossing at 4th Street.

The group of attendees identified the following locations as focus areas for continued investment in walkability within Ketchum:

- **Main Street Crossings at River Street, 2nd St, 4th Street and 6th Street;**
- **East Street at 5th Street and Sun Valley Road;**
- **Warm Springs Road between 10th Street and Saddle Road;**
- **Main Street (SH 75) at Warm Springs; and**
- **The "Gem Streets" south of downtown along Main Street.**

The following pages contain profiles of these locations and recommended improvements at either an intersection or corridor level. The recommended treatments, while location specific, may be used as test cases for implementation in other areas of the city. The *Resources* section contains links to salient FHWA and other design guidelines to support these recommendations.



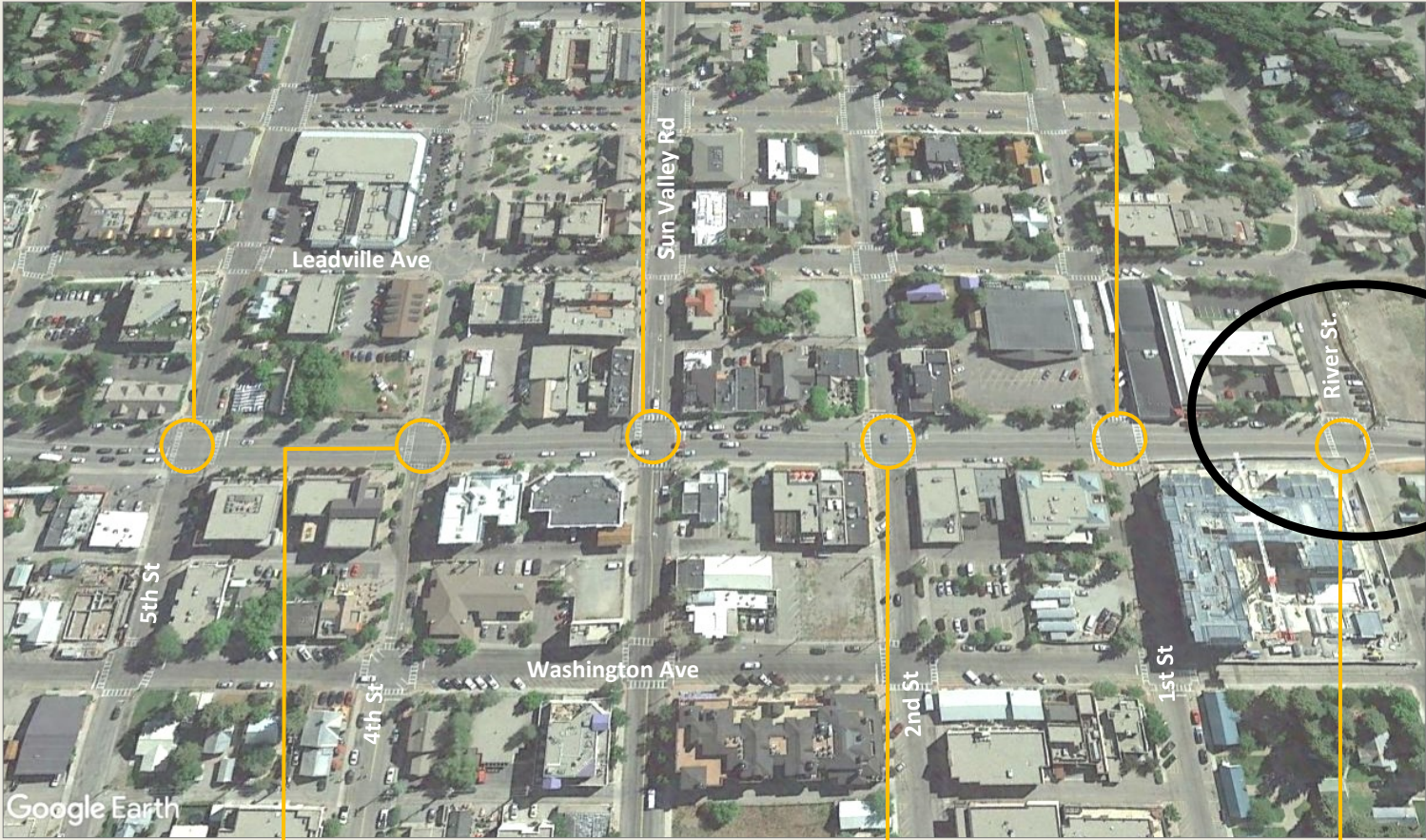
“Well-connected neighborhoods and places encourage social interaction and gatherings, allow outdoor experiences to be more spontaneous and accessible, decrease pollution, and promote healthier lifestyles through walking and bicycling.”

- Ketchum Comprehensive Plan

Main Street (SH 75) Crossings



Request ITD alter pedestrian crossing signals to automatically provide a WALK signal rather than requiring use of the push button for people crossing side streets in crosswalks parallel to SH75.



See page 4 for detailed recommendations on 4th Street intersection



Upgrade unsignalized crossings to include RRFBs, similar to 4th Street. Track use trends to determine if both crossings need RRFBs or if one side is preferred. Patterns may shift over time.

Resources

Manual on Uniform Traffic Control Devices (MUTCD)

- See Chapters 3B, 4E, 4F, 6D, 7 for pedestrian-focused elements. Chapter 9 is bicycling-focused.
- http://mutcd.fhwa.dot.gov/htm/2009r1r2/html_index.htm

Public Right of Way Accessibility Guidelines (ADA PROWAG)

- Chapter R3 contains Technical Requirements discussed in the workshop
- <https://www.access-board.gov/guidelines-and-standards/streets-sidewalks/public-rights-of-way>

Designing Sidewalks and Trails for Access

- Contains illustrations and simpler language on ADA requirements
- https://www.fhwa.dot.gov/environment/bicycle_pedestrian/publications/sidewalks/

Walkable America Walkability Checklist

- <http://www.walkableamerica.org/checklist-walkability.pdf>

Pedestrian Road Safety Audit Prompt Lists

- http://safety.fhwa.dot.gov/ped_bike/tools_solve/ped_rsa/

AARP Walk Audit Tool Kit (and Leader Guide)

- <http://www.aarp.org/livable-communities/getting-around/info-2014/aarp-walk-audit-tool-kit.html>

FHWA PEDSAFE Countermeasures (How to address problems)

- <http://www.pedbikesafe.org/PEDSAFE/index.cfm>

Pedestrian & Bicycle Information Center – Data and Resources

- <http://www.pedbikeinfo.org/data/>

Walk Friendly Communities

- <http://www.walkfriendly.org/>

FHWA Small Town and Rural Multimodal Networks Guide

- <http://ruraldesignguide.com/>

FHWA Guidance on Rectangular Rapid Flashing Beacons

- https://mutcd.fhwa.dot.gov/resources/interim_approval/ia21/index.htm

FHWA Guidance on Mini-Roundabouts

- <https://safety.fhwa.dot.gov/intersection/innovative/roundabouts/fhwasa10007/>

Streetmix: Website to design your own streets

- <https://streetmix.net/>

Project Consultants



Chris Danley &
Don Kostelec, AICP
Vitruvian Planning
Boise, Idaho



City of Ketchum

CITY OF KETCHUM, IDAHO TRAFFIC AUTHORITY MEETING MINUTES

Thursday, August 8, 2019 9:00AM

Ketchum City Hall Council Chambers

480 East Avenue North

Ketchum, Idaho

Present: Suzanne Frick, City Administrator
David Kassner, Ketchum Chief of Police
Brian Christiansen, Director of Streets & Facilities
John Gaeddert, Director of Planning & Building
Michael David, Councilmember
Joseph Meek, ITD Traffic Engineer
Steve Cook, AIA, Ketchum resident
Steve Morcone, Ketchum resident
Jeff Loomis, Galena Engineering
Kelli Trapp, Recording Secretary

1. Opening of Meeting

Meeting called to order at 9:05 AM by City Administrator Suzanne Frick

New Business

2. Adoption of Minutes

July 11, 2019

MOTION:

Motion to approve the minutes of June 13, 2019

MOVER: John Gaeddert

SECONDER: Dave Kassner

All in favor

Action Items

3. Discussion with Joe Meek, ITD Traffic Engineer about a Ped Scramble at SV Road and Main St (photo attached)

Joe Meek explained the functions of a Ped Scramble and stated that he's seeing a large amount of pedestrian traffic and tourist activity on Main Street. He would like to get everyone around town safely and get freight across and through town as well. He is having trouble lining up the time west bound on Sun Valley Rd as many drivers and pedestrians are confused. For that reason, he is looking at a Ped Scramble at that intersection. The PED Scramble could be linked to the HAWK system on River St. Meek explained that he cannot coordinate those signals with RFB's and with RFB's there is no control over them. Meek said that a Ped Scramble is a good solution that would work with the HAWK. Meek added that he would like to see how it works on Sun Valley Rd and if it attracts more pedestrians and then see how it could work at other locations.

4. Discussion with Joe Meek on a possible HAWK system at River St (for the Ketchum Boutique Hotel) that could be coordinated with the signal (photo attached)

Meek explained a HAWK system. HAWK stands for High Intensity Activated Crosswalk and is pedestrian activated. Meek said he believes that a HAWK system will help keep pedestrians safer in getting them around town. When a pedestrian is at River St, they can activate the HAWK which will stop traffic and show a hand symbol and then begin a countdown and talk to the signal at Sun Valley Rd. Frick asked if there is something that we can have that would sense when cyclists approach the HAWK, so they don't have to get off their bike and press the button in order to cross. Meek said yes, he can set something up so it can sense

when someone is there. Meek said the hardware would go up and over in the driver's lane and we would need the hardware at River and 6th.

5. Adding a "Not A Through Street" sign at 1st St and East Avenue near the alley at Cristina's Restaurant for safety issues brought by Steve Cook, AIA (photo attached)

Steve Cook, AIA spoke about the speeding vehicles down the alley by Cristina's Restaurant progressively getting worse. Cook said that it has reached a level of urgency. Cook stated he sees a lot of people using the alley to avoid the stop sign at 2nd and East Avenue. Vehicles come speeding down the narrow alley and it has turned into a dangerous situation. Cook suggested placing a large "Not A Through Street" sign near the alley at Cristina's Restaurant by East Ave. The Group discussed various options along with the pros and cons of each. Jeff Loomis spoke about the traffic study they did when removing the stop sign at 1st and East Ave. Since then there has been more development and traffic, Loomis stated. *The Group decided on putting up three stop signs; one at the south end of the alley by Cristina's Restaurant, one at 2nd Ave northbound and one coming down the alley at 1st Ave.*

MOTION:

Motion to install three stop signs; one at the south end of the alley by Cristina's Restaurant, one at 2nd Ave northbound and one coming down the alley at 1st Ave

MOVER: John Gaeddert

SECONDER: Michael David

All in favor

6. Speed concerns on Bird Dr brought by Ketchum City Council President Michael David

Bird Drive resident Steve Marcone said that Bird Drive is the longest stretch of road with no controls and a lot of speeding is going on there. The Group discussed various options to control speeding on Bird Drive with Marcone. Christiansen said they could measure the roadway and see if there's enough width to paint a bike path on one side of the road. Suzanne Frick said that they would try to get something in that would not impede snow removal in the winter. Frick said we should measure the street and see how we can use striping. Michael David suggested painting center lines and fog lines. *Frick said that they would come up with a new striping plan. Christiansen added that it could be done after chip seal. David Kassner said that he would put the speed trailer on Bird Drive Friday, August 9th.*

7. Bike and pedestrian safety at the bike path at the Bigwood/Zenergy driveway

Michael David that there were a lot of near misses at the bike path of Saddle Rd and Thunder Trail. You have to pull up so far out in order to see incoming traffic. Frick said that we can do enforcement on the private landscaping there that is blocking the view of oncoming traffic. *Frick said after the streets have been resurfaced and we're going to do the striping, the Group will discuss what can be put there.*

8. Request for an RRFB at Main and 6th brought by Shannon Allen, general manager of Hotel Ketchum

Frick said that since many people are staying at the hotel the bus stop there has become popular. Christiansen agreed and said that one needs attention. Frick said that we would work with ITD on this for options and solutions as to how do we get pedestrians across this area and what would be the most effective. Meek said that he would like to take a look at that and see what we can do to make it safer. *Meek will meet with the Group after he takes a look at the location as we need ITD's approval.*

9. River access on Parkway Drive brought by Ketchum Police Chief Dave Kassner

Police Chief Dave Kassner said that a resident who was trying to fish was turned away by a homeowner that lives nearby. Kassner found railroad ties preventing anyone from accessing the area. The homeowner believes that this is private property and is blocking access from the road. It is close to the end of Parkway Drive. Frick said that vacant lot is private property, but the access way is public property. The Group agreed that we should put a sign there as signage would help. David added that we want people to be able to access the river. We will need to order one sign that says "Parking For Public Access" there. Christiansen suggested first getting the area surveyed and staked. It was clear where the access point was from the road a few years ago and now it's very vague Kassner added. *Frick said that we will recommit to this and Christiansen will add this to his list.*

10. Update on 1st Ave parking brought by Todd Mandeville, Ketchum Computers, 540 1st Ave N (photo attached)

Todd Mandeville was unable to make the meeting but sent an update to Frick and she read it at the meeting: "The 15 minute zone that was installed on 1st Ave continues to be of benefit. The parking during construction continues to be a challenge. Now that there is middle of the street parking on the north end of 1st Ave, it could be worth adding 2 hour parking to the sides of 1st Ave

between 5th and 6th St.” The Group discussed Mandeville’s update and decided to hold off on making any changes at this time. They want to wait until the two construction projects are completed and then evaluate the conditions at that time. The Group will be discussing contractor parking with the two contractors to deal with construction issues.

Motion:

Motion to adjourn at 11:15AM

MOVER: John Gaeddert

SECONDER: Michael David

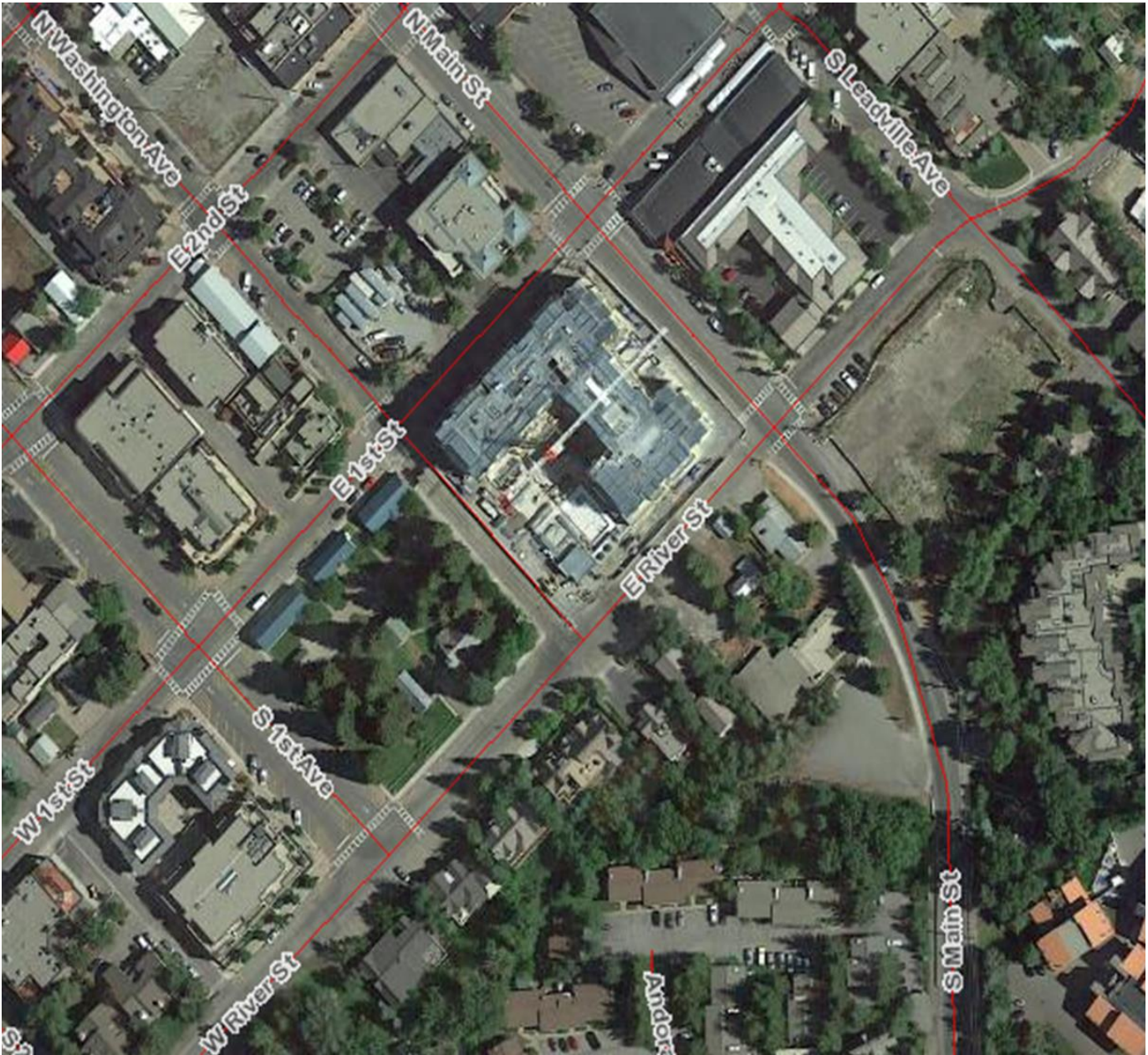
All in favor

Photos for Action & Discussion Items:

Photo for Action Item 3 Discussion with Joe Meek, ITD Traffic Engineer, about a PED Scramble at SV Rd & Main St



Photo for Action Item Number 4 Discussion with Joe Meek on a possible HAWK system at River St (for the Ketchum Boutique Hotel) that could be coordinated with the signal



Ketchum Boutique Hotel Staff Parking Demand Analysis

In the Parking Study as prepared by Hales Engineering and included as Exhibit B, the applicant has calculated that the hotel project will generate a total parking demand of 87 off street parking spaces. The applicant has proposed providing 90 total parking spaces.

Staff Parking Demand Analysis Assumptions Pursuant to Chapter 17.125, Off Street Parking and Loading

Pursuant to KMC §17.12.020, hotels are a commercial use permitted in the Tourist (T) Zoning District subject to the hotel development standards specified in KMC §17.125.050. Pursuant to Ketchum Municipal Code (KMC) §17.125.040.B, non-residential uses in the Tourist (T) Zoning District require 1 parking space per 1,000 gross square feet. Area measurements for non-residential uses are to be computed based on the basis of gross floor area as defined by KMC §17.08.020 (KMC §17.125.040.A.3b) with the additional exclusion of common and public areas (Off Street Parking Matrix Footnote 1). While basements are not included in the calculation for gross floor area, Staff has included all employee housing units and back of house operation areas into the gross floor area computation for the parking demand analysis. Common areas as defined by KMC §17.08.020 include area of a building used for circulation. Ketchum Municipal Code does not specify whether on-site employee housing units are to be considered a residential or commercial use. For the purposes of the Staff Parking Demand Analysis, Staff has calculated the required parking spaces for the employee housing units as a component of the commercial hotel operations utilizing 1 parking spaces required per 1,000 gross square feet. Calculating the employee housing units under the multiple-family residential category yields the same parking demand. Pursuant to KMC §17.125.040.C.1.b, food service establishments are exempt from providing off street parking in the T Zone. In addition to the total parking demand generated by the hotel project, the applicant will incorporate six parking spaces within the garage to offset the on-street parking spaces that will be eliminated within the River Street right-of-way.

Hotel Floor Level	Gross Floor Area (gross sq ft)	1 parking space per 1,000 gross sq ft (KMC §17.125.040.B)	Fractions Rounded (KMC §17.125.040.A2)	Floor Areas Included	Floor Areas Excluded
Basement Level 1	7,628	7.628	8	Employee Rooms & BOH/Storage	Parking Garage & Circulation
Basement Level 2	7,134	7.134	7	Employee Rooms & BOH/Storage	Parking Garage & Circulation
Basement Level 3	1,150	1.15	1	BOH/Storage	Parking Garage & Circulation

Main Level 1	19,378	19.378	19	BOH, Guestrooms, Lounge, Lobby, Meeting Rooms A & B	Circulation, Kitchen & Dining Room, Terraces
Level 2	14,685	14.685	15	BOH, Guestrooms, Fitness Area, Warming Kitchen	Circulation, Terrace, Patio
Level 3	12,104	12.104	12	BOH & Guestrooms	Circulation & Terrace
Level 4	8,684	8.684	9	BOH & Guestrooms	Rooftop Bar & Patio
On-Street Parking Offset			6		
Total Parking Spaces Required					77

Relevant Definitions (KMC §17.08.020)

HOTEL: A building designed and used for overnight occupancy by the general public on a short term basis for a fee. Hotels shall include adequate on site food and beverage service with kitchen facilities, common reservation and cleaning services, meeting room space, combined utilities, on site management and reception services, access to all sleeping rooms through an inside lobby supervised by a person in charge no less than eighteen (18) hours per day, and adequate on site recreational facilities. Unless otherwise approved by the city council, occupancy periods of a hotel, or unit thereof, by any one person or entity with an ownership interest in the hotel, or unit thereof, shall not exceed thirty (30) consecutive days or exceed ninety (90) days within any calendar year, regardless of the form of ownership. A hotel room which includes cooking facilities shall not be considered a dwelling for the purposes of density, area and bulk regulations of this title and other land use regulations. For the purposes of granting height and density bonuses, a hotel building may contain other residential uses not used in connection with the hotel operation, so long as the total gross square footage of the hotel rooms, associated common areas, and other hotel uses outlined above comprises seventy five percent (75%) or more of the entire project's gross square footage. Parking which meets the definition of "gross floor area" shall not be counted toward the seventy five percent (75%) calculation.

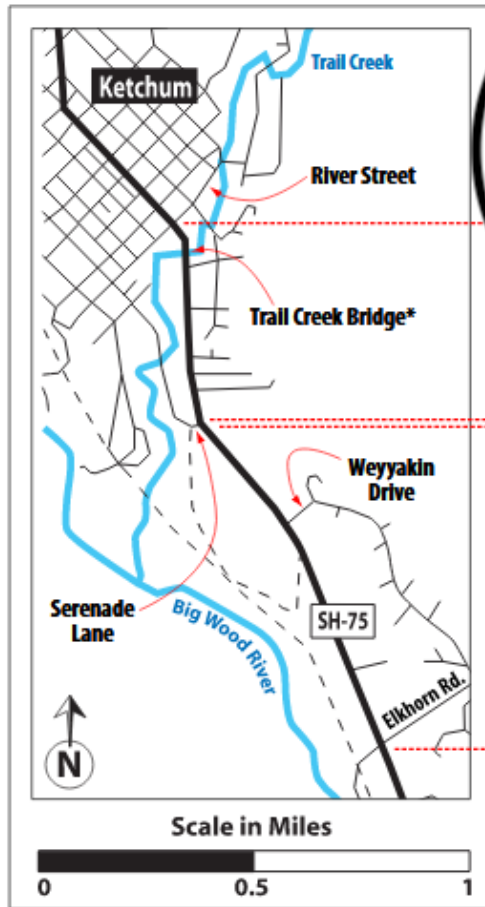
FLOOR AREA, GROSS: The sum of the horizontal area of the building measured along the outside walls of each floor of a building or portion of a building, including stair towers and elevators on the ground floor only, and fifty percent (50%) of atriums over eighteen feet (18') plate height, but not including basements, underground parking areas or open unenclosed decks. Parking areas covered by a roof or portion of the building and enclosed on three (3) or more sides by building walls are included. Four (4) parking stalls for developments on single Ketchum town site lots of five thousand six hundred (5,600) square feet in size or less are not included in the gross floor area calculation.

COMMON AREA: The area of a building used by tenants and their guests for circulation, gathering, seating and other subordinate uses which are incidental and accessory to the primary uses and functions of the building.

UNDERGROUND PARKING: An enclosed off street parking area within the lowest floor of a building; provided, that a minimum of seventy five percent (75%) of the ceiling surface area of such floor is not more than four feet (4') above the basement invisible plane (see illustration A on file in the office of the city clerk).

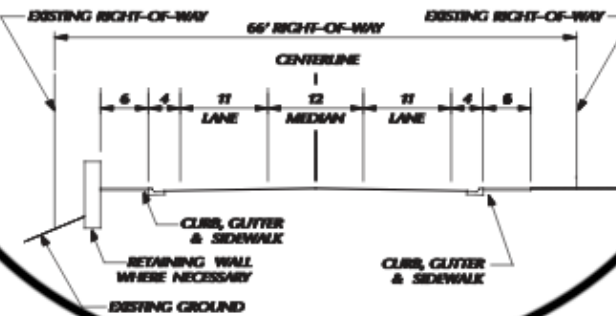
Preferred Alternative Typical Sections: Elkhorn Road to River Street

Key Map:



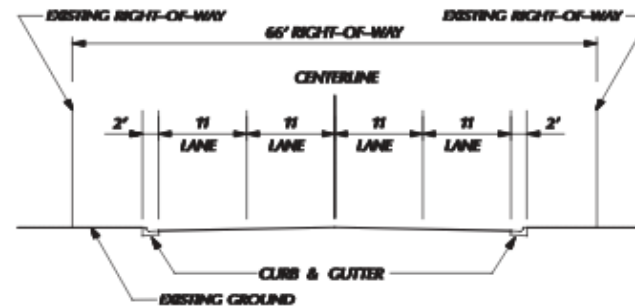
* 58-foot long Trail Creek Bridge reconstructed to accommodate 4 lanes but striped to 3 lanes.

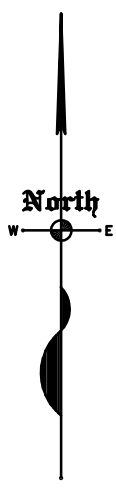
Cross Section 2 Serenade Lane to River Street



NOTE:
Number of through lanes transitions at intersection of Serenade Lane and SH-75.

Cross Section 1 Elkhorn to Serenade Lane

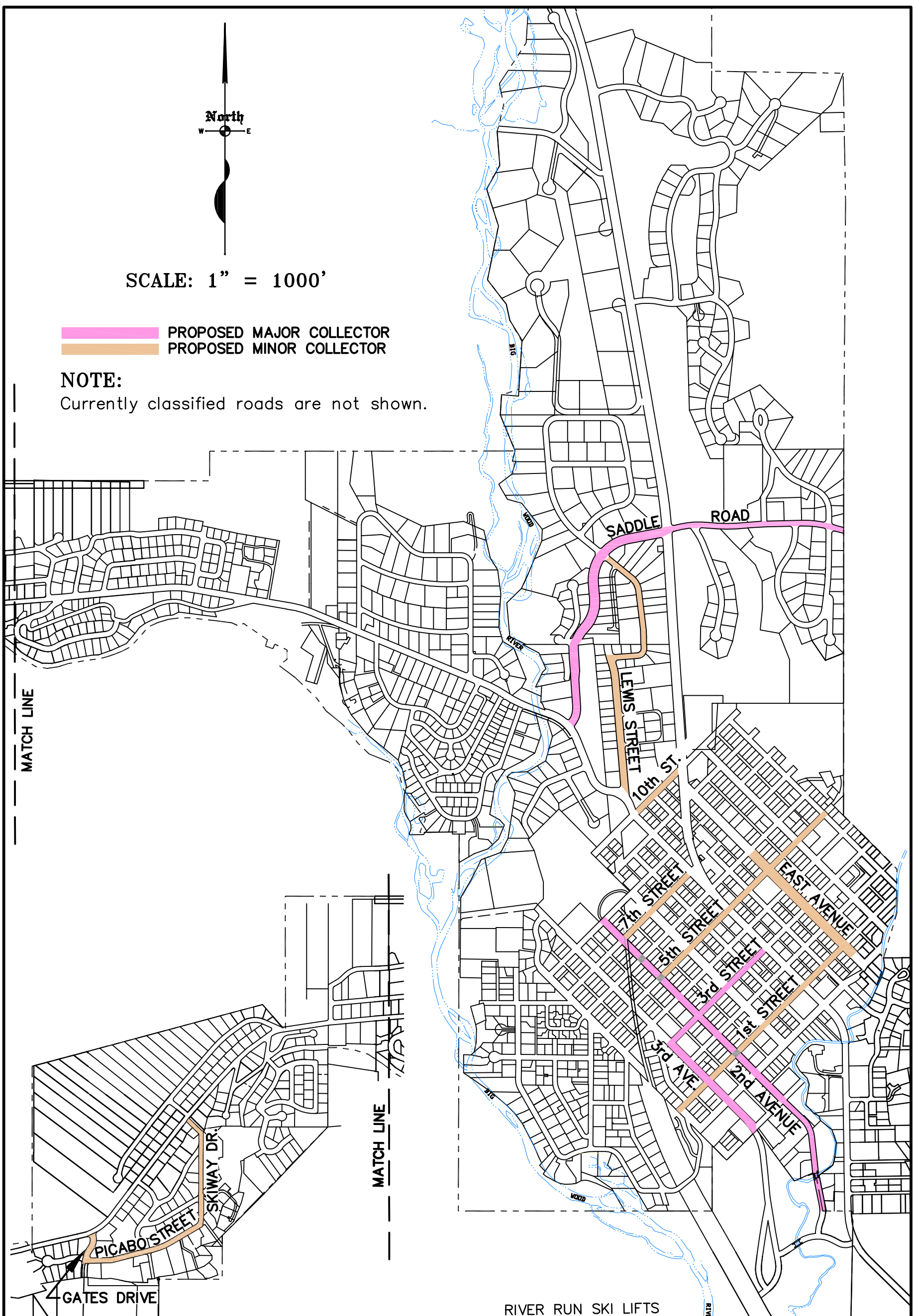




SCALE: 1" = 1000'

PROPOSED MAJOR COLLECTOR
PROPOSED MINOR COLLECTOR

NOTE:
Currently classified roads are not shown.



MATCH LINE

MATCH LINE

RIVER RUN SKI LIFTS

2003 KETCHUM ROAD FUNCTIONAL CLASSIFICATION UPDATE MAP

CITY OF KETCHUM, BLAINE COUNTY, IDAHO

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Engineering
Inc.**

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P.O. Box 425
Ketchum, Idaho 83340
(208) 726-4729
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email galena@galena-engineering.com

Project Comprehensive Plan Analysis

The 2014 Comprehensive Plan describes the long-term values, policies, and goals for Ketchum’s growth and development. Among many themes, goals and policies contained in the plan are three inter-related themes embodied by the proposed hotel – the importance of livable housing to grow the population of year-round residents and the resident workforce, sustainability (from an environmental standpoint as well as a community stability standpoint), and vibrancy (downtown and economic). Alignment with the Comprehensive Plan and references to specific goals and policies are detailed in the following sections.

Site location and context:

The subject development parcel is known locally as the “Gateway” parcel and has been vacant for a number of years. Although the parcel is zoned Tourist (as opposed to Community Core), the property is considered by the Comprehensive Plan to be part of “downtown” and the Comprehensive Plan classifies its Future Land Use designation (as well as the other three corners of the Main Street/River Street intersection) “Commercial-Employment”. Hotel, residential use, and hospitality uses are identified as appropriate primary uses, especially within vertically integrated mixed-use buildings. The Comprehensive Plan notes that mixed-use development is appropriate in this location because it fosters an attractive pedestrian environment, stimulates further reinvestment, and increases transit use over time.

The site is proximate to existing transit (Mountain Rides bus stops are located in the city block adjacent to the north), is walkable to other downtown services and amenities, and redevelopment of the site as proposed (urban-feeling massing concentrated on River Street and the Main/River intersection with the scale of the building tapering south as the grade drops toward Trail Creek, preservation of the Trail Creek riparian corridor) will enhance the experience of “having arrived” in downtown Ketchum. The hotel’s program, with its 100-rooms, on-site housing for employees, food and beverage services, and reservable meeting rooms will build on the momentum that the Limelight Hotel and the Argyros Performing Arts Center have brought to the vicinity, enhancing destination programs and amenities for hotel guests, hotel employees, and the public at large. Further, as a custom-branded boutique hotel, but within the Marriott portfolio, the hotel is uniquely positioned to increase year-round visitation due to its revered and extensive guest-loyalty programs.

Lastly, the Comprehensive Plan promotes infill development and redevelopment in this area and this site, currently vacant and verging on blighted, is ideal for redevelopment. The Comprehensive Plan promotes infill and redevelopment within existing developed parts of the city because it aids in keeping the city dynamic, competitive, and economically viable, and it takes advantage of existing utility capacity. The proposed redevelopment, with its design respective of site topography and Trail Creek, is contextually appropriate for the neighborhood as it exists today and as it has been envisioned for the future. The proposed redevelopment of this parcel also furthers the Comprehensive Plan’s goal of adding residential density (on-site employee housing) in downtown and major transportation corridors.

Inclusion of employee housing:

The Comprehensive Plan speaks to the importance of providing a variety of housing options and cites attracting year-round population (including younger residents) and the creation of diverse, affordable housing types among the most challenging issues the city faces. The plan notes that “in order to maintain a strong economy with a base of jobs and diverse demographic of residents it is important for the community to provide a varied supply of housing choices” (Comprehensive Plan, pg. 9). The plan also boldly states, “The Ketchum Community wants the majority of people who work in Ketchum to have an opportunity to reside here,” and that “a diversity of housing is critically linked to a strong economy and year-round population.” Ketchum Municipal Code requires that hotels provide employee housing at a ratio of one employee bed per 4 hotel rooms (the Limelight was able to receive a waiver from this requirement, the

Auberge was not). At 100 rooms, the developer would be required to provide 25 “beds” for employees. However, the developer has proposed 30 “beds” spread out across 12 employee suites and 2 bunk units. Further, the developer has proposed to integrate the employee housing into the hotel building so that employees are able to live on-site. In doing so, the hotel will provide the largest amount of on-site employee housing of any development in Ketchum city limits. As such, the proposed hotel raises the bar and sets a precedent for the inclusion of housing that will be affordable to the workforce (as stated in the developer’s narrative, rental rates will be set at rates affordable to wages) while increasing the variety of housing types available in the city and increasing the year-round population at the same time. Additionally, the Comprehensive Plan supports the integration of housing within mixed-use developments, and the proposed hotel exemplifies this by providing workforce housing within the hotel in conjunction with the restaurant, bar, and hotel guest rooms.

Vibrancy:

As touched on in previous sections, downtown vibrancy is identified as important in the Comprehensive Plan. The Comprehensive Plan states, “Ketchum celebrates a vibrant arts and cultural life as a core value and identifier of the community,” (Comprehensive Plan, pg. 45) a vibrant, thriving downtown is seen as a pillar of a sustainable community (Comprehensive Plan, pe. iii), and the downtown core is viewed as “critical to the economic health and well-being of Ketchum” (Comprehensive Plan, pg. 8). The proposed hotel will offer more than just hotel rooms and amenities for guests (spa/hot tub and fitness center) – it will offer amenities that will draw in the public at large, beginning with the interface with pedestrians at street level and providing access to reservable meeting/conference rooms, ground-floor and rooftop bar/food service. With the recent development of the state-of-the-art Argyros Performing Arts Center and the Limelight hotel, this sector of downtown has been infused with new energy sparked by year-round performances, live music, conferences, and other public-facing events. The addition of the proposed new hotel will contribute to this node of activity, enabling larger professional conferences and events to take place (through the use of reservable conference rooms at the Limelight, Argyros, and new hotel). Additionally, the international draw of Marriott-loyal guests is anticipated by the developer to create an influx of new visitors, repeat visitors, and visitors during non-peak seasons.

SUPPORTING SECTION	SUMMARY OF COMPLIANCE WITH THE 2014 COMPREHENSIVE PLAN
Future Land Use Category	
Commercial Employment	The Commercial/Employment category predominantly provides a variety of business, service, arts/culture, public, hotel , motel and other types of visitor lodging, residential, office, and hospitality service uses. Visitor and neighborhood limited retail. Examples include convenience or boutique retail. Multi-family housing are also considered secondary uses. The Commercial/Employment district includes Warm Springs base and is between the downtown and River Run Plaza. The area has good access and business exposure from Main Street and other arterial and collector streets. The intent is to allow for vertical or horizontal mix of uses on sites, including some high-density residential.
A Strong and Diverse Economy	
Policy E-1(b) Downtown as a Major Community Asset and Tourism Attraction	The community will strive to maintain a single concentrated commercial and retail core. The City will reinforce the downtown core’s role as a major asset and visitor attraction by encouraging businesses that fit the downtown character and by developing policies, programs, investment strategies, and organizations that help retain downtown businesses.
Goal E-3	Ketchum will continue to support our tourism and economic base.
Policy E-3(b) Tourism-Related Land Uses,	Continue to support tourism-related land uses and businesses including lodging development and venues. Support national sporting and cultural events, and strong marketing.

Businesses, Events and Marketing	
Community Design and Neighborhoods	
Policy CD-1.1 Unique Design Elements for Identifiable Neighborhoods	Each neighborhood or district should include a mix of design elements that will reinforce its unique design quality.
Policy CD-1.3 Compatible Infill and Redevelopment Projects	Infill and redevelopment projects should be contextually appropriate to the neighborhood and development in which they will occur. Context refers to the natural and manmade features adjoining a development site; it does not imply a certain style.
Policy CD-1.4 High-Quality Site Planning and Building and Landscape Design	It is important to maintain and reinforce development quality, particularly in the built-up community core area. New development should convey a positive image for the community—particularly through high quality design of residential and commercial buildings and resort accommodations. Each new project should be well-designed and attractive, and should complement surrounding land uses and existing neighborhood character. The City will adopt and implement more specific standards for high-quality site planning and building design of residential, commercial, and activity areas. Standards should address transitions or setbacks between different land uses, landscaping, quality of architecture and building materials, and pedestrian amenities.
Policy CD-1.6 High-Quality Public Spaces in New Commercial Development	Future buildings should be designed to include public amenities, particularly public gathering spaces.
Housing	
Goal H-1	Ketchum will increase its supply of homes, including rental and special-needs housing for low-, moderate-, and median income households.
Policy H-1.2 Local Solutions to Attainable Housing	The City of Ketchum will place greater emphasis on locally-developed solutions to meet the housing needs of low-, moderate-, and median-income households. The City further recognizes that such needs likely will not be met solely through private development. To facilitate affordable housing opportunities, the City will look to new funding mechanisms, and encourage a broad range of regulatory incentives and options for community housing. These may include unit buy-downs, unit reuse, density increases, and height bonuses.
Policy H-1.4 Integrated Housing in Business and Mixed-Use Areas	Housing should be integrated into the downtown core and light industrial areas, and close to the ski base. The resulting mix of land use will help promote a greater diversity of housing opportunities as well as social interactions.
Goal H-3	Ketchum will have a mix of housing types and styles.
Policy H-3.1 Mixture of Housing Types in New Development	The City should encourage the private sector, through land-use regulations and incentive programs, to provide a mixture of housing types with varied price ranges and densities that meet a variety of needs. The City will evaluate the use of incentives, such as flexibility in height, density and parking requirements to achieve greater housing diversity. Additionally, the City will promote the siting of higher density housing near public transportation, the ski base areas, shopping, and designated neighborhoods and districts.
Natural Resources and Stewardship	
Policy NR4.3 Riparian Resource Protection	Riparian areas are valuable for their ecological functions, including their ability to enhance water quality. The City will continue to implement and enforce programs that protect and enhance riparian resources in Ketchum.
Parks, Recreation, and Open Space	

Policy OS-1.4 Public Plaza Space	Support improvements to public streets, parking lots, and plazas that provide spaces for street fairs, festivals, and other gatherings.
Policy OS-1.5 Public Gathering Spaces	Promote the development of public gathering spaces throughout the City as part of public and private development.
Mobility	
Policy M-1.2 Transportation Planning and Access Management on Highway 75 and Arterials	Develop and implement a City-wide Transportation Plan. Control new curb cuts on Highway 75, Warm Springs Road and Saddle Road and reduce access points where possible (especially in the Mortgage Row area) to promote safe and efficient circulation.
Policy M-1.3 Compact Development and Housing Downtown and in Activity Centers	Encourage compact development, mixed uses, and additional housing density in the downtown and in high-activity areas. This will increase opportunities for walking, bicycling and transit ridership and reduce vehicle traps.
Future Land Use	
Goal LU-2	Support infill and redevelopment in the downtown, major activity areas and specific areas that can take advantage of proximity to services and transportation.
Policy LU-2.1 Infill and Redevelopment	Support intensification of land uses on appropriate infill and redevelopment sites in the following areas: downtown, industrial areas, St Luke's Hospital/McHanville/Cold Springs Canyon, Warm Springs area, and existing neighborhoods with significant vacant parcels.
LU-2.3 Land Use Patterns for a Dynamic and Thriving Downtown	Commercial strip development along arterial streets and high intensity retail and office uses outside the downtown core will be discouraged, except to provide neighborhood-scale retail and service uses.

Hotels in Ketchum: Historic and Present Importance as Acknowledge by City Regulations

Introduction

Planned Unit Developments are used throughout the U.S. as tool for reviewing and approving dynamic developments, that typically combine a mix of uses within a development site, and that by their nature do not fit within the confines of standard zoning and subdivision regulations. Because of the dynamism of such proposals, and the economic, social, and other public benefits the mix of uses creates, communities allow such projects to deviate from the strict standards of subdivision and zoning codes.

Planned Unit Developments allow for ‘waivers’ from code requirements through the Planned Unit Development (PUD) review process. Waivers granted through a PUD review process are different than variances from code requirements that all other types of development are entitled to apply for.

Variances are narrowly allowed when a site-specific factor, beyond the property owner’s control and not caused by the property owner, prohibit a parcel of land from enjoying the same rights that other parcels of land in the zoning district are entitled. PUD waivers, in contrast, are special exceptions from zoning and subdivision requirements that projects eligible to be reviewed as PUDs are able to request in order to facilitate the type of multi-faceted, dynamic project that brings benefits to the community that the community wants.

PUDs in Ketchum

The City of Ketchum first enacted an ordinance allowing Planned Unit Developments in 1977 (Ord. 252) and established standards, purpose, and intent for PUDs in 1979 (Ord. 316). The original intent was to “encourage flexibility in the development of land to improve the design, character, and quality of new development, and to preserve open space and natural features of land” (Section 8.1, Ord. 316) for development primarily residential in nature and on sites 3-acres or larger.

Over the years as Ketchum developed the ordinance evolved, allowing developments on parcels of land 1.5 acres in size and with 30% of housing units dedicated to community housing (1998, Ord. 723), and allowing hotels to be eligible for review as PUDs on sites less than 3 acres in size (2002, Ord. 892).

Tourism and hotels in Ketchum

Ketchum has planned for and regulated land areas designated for tourism-related functions, and lodging for visitors and tourists, since the city’s first zoning regulations (1961, Ord. 62). Ordinance 62 created a Tourist (T) zoning district separate and distinct from the Business (B) and Limited Business (LB) districts. The Tourist district is where boarding and rooming houses, hotels, motels, resort cabins and lodges were allowed; these uses were not permitted in the Business or Limited Business zones.

Notably, the parcel of land proposed by PEG to become the Ketchum Tribute Hotel was originally zoned Tourist by Ketchum’s first zoning ordinance and zoning map, and the former Holiday Inn (subsequently Alpen Rose, demolished, site of new Thunder Springs residences), River Street Inn (now a private single family residence) and the Tyrolean (still in operation) all had Tourist zoning designations.

While Ketchum has formally recognized the need to have land available for tourist accommodations for decades, in the mid-to-late 2000s the city recognized it needed to take action to curb negative trends impacting the availability of local lodging options. The city’s 2006 Downtown Master Plan, a guiding

design document recognized "...a special consideration of hotel development, a high priority for sustaining tourism," and called for "prominent hotels and visitor-serving uses" at both the north and south entrances to downtown (Ketchum Downtown Master Plan, p. 10, 80). This was followed by the 2007 Gateway Massing Study, which designated the four corners of the Main Street and River Street intersection as appropriate for hotel development.

The extent of the loss of hotel rooms in Ketchum was captured in a June 2008 city presentation that documented a net reduction of 228 hotel rooms between 2003 and 2007 (924 to 696 rooms), and a 5.1% decline in Local Option Tax lodging collections from 2001 to 2005. The presentation cited three hotel proposals in the pipeline at the time, Warm Springs Ranch Resort, the Bariteau project, and Simplot hotel project, with a total of 287 hotel rooms proposed. To date, the three referenced projects have not been constructed but the Limelight has come online, with 99 hotel rooms.

In 2008 the city took action to incentivize the development of new hotels and adopted an ordinance (Ord. 1033) exempting any hotel that met the definition of "hotel" (hotel must include guest room, food and beverage, and other amenities) from community housing and employee housing obligations if a building permit was issued for the project by June 1, 2010. No hotel received a building permit by 2010 and so the city extended the exemption (Ord. 1068) to June 1, 2012. However, the Limelight Hotel's development agreement was first approved during this time period and waiving of the Community Housing obligation was granted consistent with the zoning code exemption. The city further incentivized construction of the Limelight Hotel by approving a waiver for on-site employee housing as well.

Attachment B

PEG Ketchum Hotel, LLC Gateway Site Submittal Drawings, Narratives & Studies

1. **Narratives** (as Presented to PZ thru 8/12/19)
2. **Exhibit Maps** (as Presented to PZ thru 8/12/19)
3. **New Maps and Information** (since PZ approval on 8/12/19)

Attachment B

PEG Ketchum Hotel, LLC Gateway Site Submittal Drawings, Narratives & Studies

1. Narratives (as Presented to PZ thru 8/12/19)

- Written Description of Development
- Development Plan
- Project Analysis | Social Impact Study
- Application of Standards 16.08.080
- Development Schedule
- Parking Analysis | Traffic Study
- Employee Housing Plan
- Sub-Area Analysis
- Contextual Hotel Components Analysis
- Draft Development Agreement
- Standards of Evaluation | Exhibit Key

PRE-APPLICATION DESIGN SUBMITTAL



KETCHUM BOUTIQUE HOTEL

JULY 29, 2019 PLANNING AND ZONING COMMISSION MEETING SUBMITTAL

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JULY 29, 2019 PLANNING AND ZONING COMMISSION MEETING SUBMITTAL FOR
KETCHUM BOUTIQUE HOTEL

- Pre-Application Design Review
- Development Agreement
- Hotel Planned Unit Development | Conditional Use Permit
- Lot Line Shift
- Flood-plain | Waterways Design Review



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01

NARRATIVES

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NARRATIVE | 01.01

WRITTEN DESCRIPTION OF DEVELOPMENT

WRITTEN DESCRIPTION OF DEVELOPMENT

BUILDING PROGRAM SUMMARY

Primary components of the new building include:

Public Amenities - Indoor

Lobby	1,681 NSF
Restaurant / Lounge	4,047 NSF
Meeting Space - Level 01	2,522 NSF
Roof-top Bar	1,336 NSF

Public Amenities - Outdoor

Restaurant Patio	1,185 NSF
Lounge Terrace	978 NSF
Roof-top Bar Patio | Level 04	1,672 NSF
Roof-top Bar Terrace | Level 03	3,008 NSF

Guest Amenities

100 Guest Rooms	25,603 NSF
Fitness Center	1,039 NSF
Outdoor Terrace | Hot-tubs	3,277 NSF

Employee Housing | 30 Beds

12 Employee Suites	10,346 NSF
2 Employee Bunk Rooms	3,410 NSF

Support Spaces

Offices | BOH | Storage	9,262 NSF
Parking Garage | 102 Stalls	41,184 NSF
Circulation	17,789 NSF

Total NSF	127,569 NSF
Total GSF	139,295 GSF

SCHEMATIC DESIGN NARRATIVE

The proposed project is a 100 guest room boutique hotel on a 1.08 acre site on the south-west corner of River and Main Street. Referred to as the 'Gateway Hotel' site in the Ketchum Master Plan, currently the site houses multiple vacant commercial office buildings that are in dis-repair. This new hotel development will act as a gateway to downtown Ketchum with thoughtful attention to the approach and scale of the existing context, and a strong connection to neighboring Trail Creek. With steep slopes entering town along Main Street, the building will terrace along with the change in grade to soften the mass, and create seamless transition into downtown. Strong indoor/outdoor connections from both the guest rooms and public amenities will link the city fabric to the main lobby, and take advantage of the panoramic views from roof-top terraces and upper level patios above.

PUBLIC AMENITIES

The goal of this hotel is to fit into the character and charm of Ketchum, while offering world-class amenities for local residents and visitors from abroad. Public amenities will include a main level restaurant and bar / lounge space with a direct connection to both Main and River Streets. Public meeting space on this level will be situated with a view down Trail Creek, and a flexible plan to allow various sizes of groups to make use the facility. A lower level meeting space is schematically planned directly adjacent to Trail Creek with an improved outdoor space next to the natural riparian habitat. On the fourth floor a small roof-top bar is planned with 270 degree panoramic views from Baldy to Sun Valley. This bar will have access to roof terraces with additional seating surrounded by rooftop gardens. These final design of these amenities are still being designed, but would all be available to the general public as well as hotel guests.

SITE AERIAL



GUEST AMENITIES

This hotel will offer 100 guest rooms with a mix of standard and suite room-types at a boutique hotel price point. Each room will have a prime view of the surrounding landscape, with operable windows and various patios / Juliet balconies for patrons to enjoy the views. The second level of the hotel will include a large outdoor terrace oriented towards Baldy with appetizer and bar service for guests, as well as two large hot-tubs viewing over Trail Creek. A gracious fitness center will be directly adjacent to the patio with fully operable sliding glass doors that open onto the terrace during nice weather

EMPLOYEE HOUSING

On-site housing will options will be provided to a percentage of employees to help offset the demand and cost of rental housing in the Ketchum area. The goal with this housing is to provide a quality product to draw high caliber employees wanting to live in downtown Ketchum.

SUPPORT SPACE

Offices, service spaces, storage and a below-grade parking garage will be designed to have minimal impact on public and guest experiences in the hotel. These necessary program elements will be designed to adequately support the hotel function and create a healthy work environment for the employees.

ARCHITECTURAL CHARACTER

The hotel site has an approximately 30 foot elevation change from Trail Creek to River Street, with a healthy, tall tree canopy to the South and the recently developed Limelight Hotel to the North. In response to the steep conditions of the site, the building mass is being broken into smaller volumes that terrace and step with the topography to break down the scale as it moves towards Trail Creek. These steps in mass create a series of opportunities for green-roofs and outdoor terraces to continue to reinforce the connection of the building to the surrounding landscape.

A base of employee housing program will be on the hotel lower level running along Main Street down to Trail Creek. Again, strong indoor / outdoor connections from these spaces will be provided.

On the street-level of River and Main, the elevation will be transparent and inviting to the public, with guest rooms stacked above viewing down-town Ketchum, Sun Valley, and Baldy depending on their orientation.

One capstone of this project will be the panoramic rooftop bar on the south side of the fourth level. Architecturally minimal in its appearance, this bar will have views in all directions and is oriented to provide year-round service.

Materiality of the building be still in development, but will be selected to fit the diverse context of rural Idaho and Ketchum architecture. A mix of stone, wood, steel, and glass will be balanced to accent the architecture and compliment the diversity of downtown.

CONCEPT SKETCH



NARRATIVE | 01.02

DEVELOPMENT PLAN

DEVELOPMENT PLAN

The development of the Ketchum Boutique Hotel is an influential project in the further growth and economics of the City of Ketchum. We plan to make this a successful project through transparent communication with the community and the city. Creating a sustainable landmark in the community that everyone is proud of and wants to enjoy is our goal. We intend to be creative and bring solutions to a complicated site that will be thoughtful and well developed

There are many steps to a well executed, collaborative development process. Our plan for this is as follows:

- Working proactively with city officials and planning department to make sure all issues are vetted and resolved.
- Maintaining an open dialogue with the community to increase awareness and local involvement
- Offering public amenities and employee housing stock to add to the already vibrant community feel in Ketchum.
- Focusing on sustainable design, construction, and execution with programs to recycle materials from the demolition of existing structures as well as recycling materials throughout the construction process.
- Working with a contractor that is familiar with Ketchum City standards and works well with the community.

NARRATIVE | 01.03

PROJECT ANALYSIS | SOCIAL IMPACT STUDY

PROJECT ANALYSIS | SOCIAL IMPACT STUDY

MARKET STUDY

Part of creating a successful product involves performing a specific market study on what is and is not working in the community, and adjusting the development plans accordingly. The following is part of the recently completed professional market study:

- Recent \$35 million airport renovation has increased traffic by 26.9% in 2014, and 16.5% in 2016. Project property is 11 miles north of the airport, and one of the most prominent possible lodging destinations when entering the town.
- Newly renovated Performing Arts Center is expected to host more than 260 events per year, including conferences, receptions, and weddings. It is also expected to partner with local hotels for conference space and lodging packages
- The town is growing and an increase in off-season activity is helping boost year-round demand. Oct. 25th 2018, Hotels.com reported 91% room occupancy during Jazz Festival
- Boutique Service Hotel model provides flexibility to offer amenities to drive RevPar when demand is there, but dial back service levels in the off-season to reduce operating costs.
- A strong international hotel brand affiliation will create a strong destination driver that does not exist in the Sun Valley area yet. This will generate increased business and demand that is difficult to quantify.
- The hotel will be the first '4-star' branded property in the Sun Valley Resort Area.
- The hotel will offer a restaurant with a lounge that will open for breakfast and dinner with the flexibility to be open for lunch during peak seasons or if demand exists. The restaurant & lounge should be a vibrant atmosphere and offer a variety of light appetizers, as well as a full menu featuring unique and creative dishes. The hotel will have flexibility to accommodate groups and events.

- The hotel will offer flexible meeting and event space to accomplish both group meetings and social events. Based on our research and interviews, we believe that the property would have an excellent opportunity to accommodate wedding groups as well.

BRANDING CONSULTANT

A branding consultant has been engaged on this project to study the diverse experiences and various environments that exist in Ketchum to help inform the owner and design team on what makes Ketchum unique, and how to harness that to create a project that fits into the cultural fabric of town. This team has been spending time in the greater Sun Valley area to determine what elements of this project can compliment the beautiful landscape and comfortable atmosphere.

As part of the next submittals to the Planning Department and Commission, a full branding package will be submitted with an explanation of the feel, materiality, and story that this Boutique Hotel will bring to downtown Ketchum.

SOCIAL EFFECT

The primary goal of the Ketchum Boutique Hotel is to have a positive, sustainable, and lasting impact on the city, and to fit in seamlessly with the culture of the town. Projects that have a positive social impact and create an iconic destination within the city, will have the most positive reaction from the community. We intend for this project to appeal to locals, those visiting for the first time, or those who have been coming to Ketchum since the 1970's. This hotel will offer several restaurant, bar, and meeting spaces, which will all offer different perspectives of the surrounding area. From a rooftop bar with panoramic views of the surrounding landscape, to meeting rooms just outside the canopy of Trail Creek, this Boutique Hotel will offer something for everyone in the community to enjoy.

PROJECT ANALYSIS | SOCIAL IMPACT STUDY



ECONOMIC | FISCAL EFFECT

Based on our recent market study, there is an increasing trend in year-round travellers visiting the area, drawn to seasonal recreation opportunities, performing arts events / festivals, and various conferences, meetings, and corporate retreats. This hotel will be a boutique brand, but tied to companies with global reach, that can potentially expand the exposure and visitor draw to the area. These new visitors will add tax revenue to the local businesses from restaurants, bars, shops, and markets. This larger network can also draw corporate groups during 'slack' seasons and help equalize the shifts in the small town economy. The development of the hotel will result in the direct investment of over \$500,000 in state and city fees and #30 million in construction activity. We project the operating hotel will generate approximately \$1 million in combined annual property and sales taxes. The indirect economic impacts are more difficult to quantify, but they will also be significant

ENVIRONMENTAL EFFECT

Responsible development requires a thoughtful, sustainable approach to design, construction, and operations. This project design will integrate a high performance building envelope to ensure it operates efficiently and effectively to minimize its environmental foot-print and utility demand during day to day operations in the mountain climate of Ketchum. In addition to building design, the hotel operations team plans on integrating sustainable practices in their maintenance and house-keeping programs, and including a thorough recycling program for all employees and guests. The overall intent of the project is to be as sustainable as economically possible to minimize environmental impacts and effects.

PROJECT ANALYSIS | KETCHUM COMPREHENSIVE PLAN

OVERVIEW

We have actively been working with the Ketchum Planning Staff to create a project that is aligned with the priorities of the community of Ketchum and that is compliant with the Ketchum 2014 Comprehensive Plan. From the opening statement “A New Way Forward” (p. iii) priorities are set for a sustainable future and the Ketchum Boutique Hotel is aligned with, and assists in meeting the goal of almost every bullet point including:

- **Becoming a more year-round community** – the Ketchum Boutique Hotel will increase tourism visits during the peak and shoulder seasons through the marketing engine of a strong international hotel brand with a loyal customer base, including those using points to visit unique destinations such as Ketchum, ID.
- **Increasing employment opportunities** – the Ketchum Boutique Hotel will create over 100 new full and part time staff positions and will house 30% of those new employees in the on-site employee apartments.
- **Creating a greater variety of housing options** – The boutique hotel will provide on site employee apartments, we also will have 2 employee dorm suites that will provide 30 beds.
- **Expanding tourism opportunities** – the Ketchum Boutique Hotel will be a destination property, with amenities and a level of service that will create a very loyal customer base of returning guests and become a highly sought after destination in Marriott’s collection of Western Mountain Resort properties.
- **Providing transportation options that are an alternative to use of the automobile** - the Ketchum Boutique Hotel guests will have the opportunity to take full advantage of the walkable nature of downtown and not add to the vehicle traffic load on Ketchum’s streets. Guests of the hotel can anticipate a seamless airport, to hotel, to recreation destination, experience without the need of a personal vehicle while visiting downtown Ketchum. An airport courtesy shuttle will be provided and hotel guest will also have a courtesy shuttle available to provide access to the Sun Valley Resort mountain bases and other visitor destinations during their stay.
- **Encouraging local buying and locally-grown food** – the architectural massing of the Ketchum Boutique Hotel has created multiple outdoor roof terraces, we will work with the future chef of the restaurant to explore rooftop box gardens that would produce on site some of the herbs and vegetables used in the restaurant. Locally grown seasonal flowers will be provided in the main lobby, restaurant and other public spaces and locally-sourced ingredients will be used in the restaurant when possible.
- **Establishing new public amenities** – the flagship amenities of the Ketchum Boutique Hotel will include: a unique restaurant, a roof top bar with views of Bald Mountain and Dollar Mountain, and meeting and banquet spaces available to host community events.



PROJECT ANALYSIS | KETCHUM COMPREHENSIVE PLAN



The Ketchum Boutique Hotel contributes to all 10 of the 10 Community Vision and Core Value primary objectives:

1. **Strong and Diverse Economy** – New internationally branded flagship hotel - ✓
2. **Vibrant Downtown** - Downtown core infill with destination public amenities - ✓
3. **Community Character** – by Enhancing the Southern Gateway with a new architectural landmark - ✓
4. **A variety of Housing Options** – 12 on site rental apartments, 2 dorm suite apartments with 30 beds - ✓
5. **Environmental Quality and Scenic Beauty** – public amenity spaces take advantage of scenic vistas with indoor and outdoor spaces, the hotel's massing has been coordinated to preserve vistas of Bald Mountain from Main Street for both vehicular and pedestrian north bound traffic entering downtown Ketchum; as a new architectural landmark that thoughtfully integrates with its neighboring building forms and uses - ✓
6. **Exceptional Recreation Opportunities** – the hotel will support recreation opportunities through its marketing efforts that will attract guests interested in those opportunities and then through on site gear storage, on site gear rentals, and an adventure concierge coordinating tours and excursions for guests -
7. **Well Connected Community** – the downtown location will leverage Ketchum's walkable downtown, guests will have walking paths through the site landscaping to viewpoints of Trail Creek (located outside of the riparian zone) - ✓
8. **Enliven the Arts and Culture** – the hotel will provide public banquet and meeting spaces to host cultural events, live music from local musicians may be provided in the restaurant and rooftop bar, the restaurant will be a unique culinary destination. We would also like to showcase local artists work throughout the hotel - ✓
9. **Working as a Region** – the hotel will be a high quality thoughtfully planned regional landmark- ✓
10. **A "Greener" Community** – the hotel will recycle it's waste during construction and will have an ongoing waste recycling program; the hotel will have green roof areas that reduce storm water runoff lessening peak flows to the water treatment plant, native drought tolerant landscape material selection and we will have better than code: water conservation features, energy conservation features, exterior envelope wall assemblies, and thermal windows and doors - ✓

PROJECT ANALYSIS | KETCHUM COMPREHENSIVE PLAN

The Ketchum Boutique Hotel also aligns with the following Goals:

ECONOMY

E-1(b) – Downtown as a Major Community Asset and Tourism Attraction

Compliance: The Ketchum Boutique Hotel leverages Downtown Ketchum as a destination with a location that enhances walkability and (along with the adjacent entitled but unbuilt hotel) completes the buildout of the 2007 Gateway Scale and Massing Study. We are creating new flagship destination with amenities that will draw tourists and locals to downtown including: a unique restaurant with a locally curated atmosphere and menu, meeting rooms and the only public 4th level panoramic rooftop bar in Ketchum.

E-3(b) Tourism-Related Land Uses, Businesses, Events, and Marketing

Compliance: the Ketchum Boutique Hotel completes the 2007 Gateway Scale and Massing Study, it will complete the intersection of Main Street and River Road as the hospitality gateway to downtown Ketchum. As a Marriott branded property the hotel will become an international flag for Ketchum on the Marriott Envoy website and marketing engine, which will enhance and compliment any local tourism marketing campaigns.

E-4(a) Balance of Business Types

Compliance: the Ketchum Boutique Hotel will add an additional tourism related business to downtown Ketchum. The hotel's restaurant, meeting / banquet rooms and rooftop bar will provide recreational and social spaces for tourists and locals alike.

HOUSING

H-1.4 Integrated Housing in Business and Mixed-Use Areas

Compliance: the Ketchum Boutique Hotel includes 12 on site employee rental apartments (which include 2 dorm multi-unit suites) for a net increase of 30 total beds located in downtown. The residents of those apartments and dorms will live in Ketchum full time, and by having their full time employment on site, will reduce vehicular traffic on Ketchum's streets. With their downtown location on-site residents will also have grocery, banking, entertainment, and healthcare services, all within walking distance of their residences.

H-3.1 Mixture of Housing Types in New Development

Compliance: the Ketchum Boutique Hotel's on site market rate apartments will provide rental housing opportunities for employees of the hotel in the downtown neighborhood, a location that, due to high land costs, would not likely support the development of new market rate rental apartments. This will add a newly developed affordable housing component to a Ketchum neighborhood that would otherwise not support the development of new affordable housing and will increase the income diversity in this neighborhood. The 12 on site employee rental apartments (which include 2 dorm multi-unit suites) for a net increase of 30 total beds will also provide rental price point diversity - with some units being at a price point catered to management positions and the dorms being set at a price point that a starting wage could reasonably afford.

PROJECT ANALYSIS | KETCHUM COMPREHENSIVE PLAN

COMMUNITY

HD-1.1 Unique Design Elements for Identifiable Neighborhoods

Compliance: the Ketchum Boutique Hotel will be an architectural flagship landmark for downtown, its massing has been crafted to pull back from Main Street so the hotel gently terraces up the bench towards River Road with artfully designed landscaping between the hotel and Main Street that reinforces the site as an entry portal to a signature mountain community.

CD-1.3 Compatible Infill and Redevelopment Projects

Compliance: the Ketchum Boutique Hotel is an infill project that takes full advantage of existing urban infrastructure, after analyzing the site no significant utility upgrades will be necessary to support the new building. It also takes full advantage of existing transportation infrastructure and isolates southern (northern bound) destination traffic to the gateway intersection of downtown. The hotel is compatible with both long term master planning for its location and with the current developments (and entitled developments) for all four corners of the Main Street and River Road intersection. (also applies to M-1.3 and LU1.1, LU-2.1, LU-2.3)

CD-1.4 High-Quality Site Planning and Building and Landscape Design

Compliance: the Ketchum Boutique Hotel will go through a PUD process to ensure that there is ample opportunity for community input in its design solution. The design guidelines of Ketchum will ensure a final design that will be appropriate to its location and compatible with its neighbors. PEG Development, Marriott, and the Design Team, are fully invested in creating a community landmark that will be celebrated for generations to come as a signature building in Ketchum. The landscape design will be crafted with a sensitivity to local foliage and fauna, and will provide a natural environment that both elegantly frames the new hotel and effectively ties it to its Idaho mountain environment.

CD-1.6 High-Quality Public Spaces in New Commercial Development

Compliance: PEG Development and the design team have been working closely with the Ketchum Planning staff to identify public amenity spaces that would be highly beneficial to the community. We have also worked to place those public amenity spaces in the premier locations in the building. These amenity spaces include: a unique restaurant with a locally curated atmosphere and menu, meeting rooms and the only 4th level panoramic rooftop bar in Ketchum. We also plan to provide public access to several of the rooftop terraces of the hotel.

CD-2.3 Night Sky Conservation

Compliance: The Ketchum Boutique Hotel will have night sky compliant exterior light fixtures – see exhibit #7 for fixtures that will be similar to those in the final lighting selection of the hotel. As part of the PUD process at the later stages of design we will also provide an exterior lighting analysis to verify the compliance with Dark Sky lighting design standards.



CD-2.4 Development Designed for Natural Feature Preservation

Compliance: The Ketchum Boutique Hotel is honoring its riparian setback from Trail Creek but celebrates it through providing pedestrian viewpoints along landscape pathways (located above the riparian setback); the landscape design will celebrate and accentuate the bench side location of the site – see exhibit #5. (also applies to NR-1.1 & OS-2.1)

CD-2.5 Energy and Water Efficiency in New Development

Compliance: The Ketchum Boutique Hotel will be designed to meet Marriott corporate standard requirements for sustainable design – which exceed minimum industry standards for energy and water efficient design. ajc architects has a firm ethos of bringing innovative sustainable design solutions to all of our projects which will continue with the design solution for the hotel.

CD-3.1 Scenic Corridors and the Community's Key Gateways

Compliance: the Ketchum Boutique Hotel will complete the 2007 Gateway Scale and Massing Study which intended to produce a southern gateway with a hospitality focused portal to enter downtown Ketchum. The hotel's massing has been coordinated to preserve vistas of Bald Mountain from Main Street for both vehicular and pedestrian north bound traffic entering downtown Ketchum. As a new architectural landmark that thoughtfully integrates with its neighboring building forms and uses, the Ketchum Boutique Hotel will be the signature entry element that has been envisioned in previous master plans and in the Ketchum 2014 Comprehensive Plan. (also applies to OS-3.6)

MOBILITY

M-6.3 Bicycle Parking Facilities

Compliance: the Ketchum Boutique Hotel will feature gear storage facilities for guests that will provide bicycle storage. Bicycle parking will also be accommodated in the site plan design.

NARRATIVE | 01.04

APPLICATION OF STANDARDS 16.08.080

APPLICATION OF STANDARDS 16.08.080

Planned Unit Development (PUD) Standards of Evaluation	
EVALUATION STANDARDS: 16.08.080	
<p>The standards set forth in this section shall apply to review of all PUD conditional use permit applications. The standards shall be used to review and evaluate the proposal in comparison to the manner of development and effects of permitted uses and standard development allowed on the property in question. Modification or waiver from certain standard zoning and subdivision requirements may be permitted subject to such conditions, limitations and/or additional development standards, pursuant to section 16.08.130 of this chapter, as the city council may prescribe to mitigate adverse impact at the proposed planned unit development, or to further the land use policies of the city, or to ensure that the benefits derived from the development justify a departure from such regulations. Where the city council determines that conditions cannot be devised to achieve the objectives, and/or the standards contained in this chapter are not met, applications for conditional use permits shall be denied. The city council shall make findings that each of the following evaluation standards have been met. The evaluation standards are as follows:</p>	
Code Section	City Standards and Applicant Comments
16.08.080.A	<p>Minimum lot size of three (3) acres. All land within the development shall be contiguous except for intervening waterways. Parcels that are not contiguous due to intervening streets are discouraged. However, the commission and the council may consider lands that include intervening streets on a case by case basis. The commission may recommend waiver or deferral of the minimum lot size, and the council may grant such waiver or deferral only for projects which:</p> <ol style="list-style-type: none"> 1. Include a minimum of thirty percent (30%) of community or employee housing, as defined in section 16.08.030 of this chapter; 2. Guarantee the use, rental prices or maximum resale prices based upon a method proposed by the applicant and approved by the Blaine County housing authority and/or the Ketchum city council; and 3. Are on parcels that are no less than one and one-half (1 1/2) acres (65,340 square feet). Application for waiver or deferral of this criteria shall include a description of the proposed community or employee housing and the proposed guarantee for the use, rental cost or resale cost. 4. For a hotel which meets the definition of "hotel" in section 17.08.020, "Terms Defined", of this code, and conforms to all other requirements of section 17.18.130, "Community Core District (CC)", or section 17.18.100, "Tourist District (T)", of this code. Waivers from the provisions of section 17.18.130 of this code may be granted for hotel uses only as outlined in section 17.124.040 of this code. Waivers from the provisions of section 17.18.100 of this code may be granted for hotel uses only as outlined in section 17.124.040 of this code.
<i>Comment</i>	<p><i>Our current site is comprised of the following lots: Lot 1 - 280 E River St - Block 82 = 0.136 Acres, Lot 2 260 E River St - Block 82 = 0.138 Acres, Lot 3 - 251 S Main St - Block 82 = 0.810 Acres; <u>Total = 1.048 Acres</u>. The (3) sites will be combined into one in a lot consolidation that is being submitted concurrently with this PUD submittal. We do not meet the minimum standard of (3) acres but as a "hotel" we do meet exception 16.08.080.A.4 and conform to all other requirements of 17.18.100. The site is also similar in size to the 1.1 acre site of the currently entitled hotel development across Main Street at 300 E River Street.</i></p>
16.08.080.B	<p>The proposed project will not be detrimental to the present and permitted uses of surrounding areas.</p>
<i>Comment</i>	<p><i>The proposed hotel project is consistent with the current (T) Tourist District zoning and is a</i></p>

APPLICATION OF STANDARDS 16.08.080

	<p><i>was identified as a priority urban infill site for potential hotel development. The site will be surrounded by urban infill hospitality focused uses. The Limelight Hotel is located directly across River Street to the North West. The Best Western Hotel is located directly North across the intersection of Main Street and River Street. An Entitled Future Hotel is located directly across Main Street to the North East. The Sun Valley Board of Realtors is located in a commercial building directly West of the site at 200 River Street. The site is bordered by commercial uses to its North, East, and West and is separated from the multifamily residential uses to the South by Trail Creek.</i></p>
<p>16.08.080.C</p>	<p>The proposed project will have a beneficial effect not normally achieved by standard subdivision development.</p>
<p><i>Comment</i></p>	<p><i>Also please see Narrative #2.</i></p> <p><i>The proposed hotel will bring significant economic and public amenity benefits to Ketchum and would not be achievable on this site without the PUD process. Economic benefits include 100 new hotel rooms that will be booked through the international reach of the internationally recognized hotel brand’s reservation network. The hotel will feature many amenities to the public such as a street front restaurant and lounge, banquet/meeting rooms, and a roof top bar with panoramic views of Bald Mountain and Dollar Mountain. The hotel will also provide 25 on site housing beds for hotel employees in a mix of traditional apartments and dorm style apartment units.</i></p>
<p>16.08.080.D</p>	<p>The development shall be in harmony with the surrounding area.</p>
<p><i>Comment</i></p>	<p><i>The proposed hotel is congruous with its surrounding commercial neighbors. Great attention has been paid during the massing study and early design effort to respect the priorities of the 2014 Ketchum Comprehensive plan – this is reflected in the project’s footprint pulling back from Main Street and in breaking down the scale of the structure with multiple architectural mass forms. The hotel features many outdoor living opportunities including guest terraces and balconies, public terraces, the South West facing amenity courtyard on level 2, and in the rooftop terrace that surrounds the rooftop bar. These outdoor spaces are designed to blur the line between interior and outdoor spaces for the hotel guests and visiting public patrons. Outdoor views have also been a high priority in the design of the hotel, positioning program elements where best able to take advantage of key vistas. The hotel honors the riparian setback of Trail Creek but also will provide pedestrian pathways across the site landscaping to viewpoint areas overlooking Trail Creek (set outside of the riparian zone setback). The close proximity to the downtown core will enhance walkability throughout the neighborhood meeting the economic and sustainable community goals of The Ketchum 2014 Comprehensive Plan and the 2007 Gateway Scale and Massing Study.</i></p>
<p>16.08.080.E</p>	<p>1. Densities and uses may be transferred between zoning districts within a PUD as permitted under this chapter, provided, the aggregate overall allowable density of units and uses shall be no greater than that allowed in the zoning district or districts in which the development is located. Notwithstanding the above, the commission may recommend waiver or deferral of the maximum density and the council may grant additional density above the aggregate overall allowable density only for projects which construct community or employee housing and which:</p> <ul style="list-style-type: none"> a. Include a minimum of thirty percent (30%) of community or employee housing, as defined in section 16.08.030 of this chapter; and b. Guarantee the use, rental prices or maximum resale prices thereof based upon a method proposed by the applicant and approved by the Blaine County housing

APPLICATION OF STANDARDS 16.08.080

	<p>2. Application for waiver or deferral of this criteria shall include a description of the proposed community or employee housing and the proposed guarantee for the use, rental cost or resale cost.</p>
<i>Comment</i>	<p><i>N/A. We are not requesting any density transfers.</i></p>
<p>16.08.080.F</p>	<p>The proposed vehicular and nonmotorized transportation system:</p> <ol style="list-style-type: none"> 1. Is adequate to carry anticipated traffic consistent with existing and future development of surrounding properties. 2. Will not generate vehicular traffic to cause undue congestion of the public street network within or outside the PUD. 3. Is designed to provide automotive and pedestrian safety and convenience. 4. Is designed to provide adequate removal, storage and deposition of snow. 5. Is designed so that traffic ingress and egress will have the least impact possible on adjacent residential uses. This includes design of roadways and access to connect to arterial streets wherever possible, and design of ingress, egress and parking areas to have the least impact on surrounding uses. 6. Includes the use of buffers or other physical separations to buffer vehicular movement from adjacent uses. 7. Is designed so that roads are placed so that disturbance of natural features and existing vegetation is minimized. 8. Includes trails and sidewalks that create an internal circulation system and connect to surrounding trails and walkways.
<i>Comment</i>	<p><i>Also Please see Narrative #5: Traffic Study & Parking Analysis, and Exhibit #8.</i></p> <p><i>We will be utilizing the following strategies to reduce the vehicular impact on Ketchum’s streets from this project: Guest Shuttle (airport and to local destinations), Employee Shuttle, Car Share Program, Employee Transit Passes, Employee, Carpool Program, Employee Alternative Transportation Support Facilities.</i></p> <p><i>Traffic - the following is a summary of key findings and recommendations:</i></p> <ul style="list-style-type: none"> • <i>All study intersections are currently operating at acceptable levels of service during the evening peak hour in existing (2019) background conditions.</i> • <i>The development will consist of a 100-room hotel which will have two accesses onto River Street.</i> <ul style="list-style-type: none"> o <i>One access will be an exit from the hotel porte cochere.</i> o <i>The second access will accommodate ingress and egress movements to the hotel parking garage, as well as be an entrance for the porte cochere.</i> • <i>All study intersections are anticipated to operate at acceptable levels of service during the evening peak hour with project traffic added for the 2019 conditions.</i> • <i>Future (2040) background traffic volumes were calculated based on an annual growth rate calculated for Ketchum City as part of a previously completed study.</i> • <i>With future (2040) background conditions, the River Street / Main Street (SH-75) intersection is anticipated to operate at LOS F during the evening peak hour.</i> <ul style="list-style-type: none"> o <i>The City may consider restricting left-turn movements from the minor approaches onto Main Street (SH-75) at this intersection.</i> o <i>Converting Main Street (SH-75) to a three-lane cross section with left-turn lanes at intersections is anticipated to result in the intersection operating at</i>

APPLICATION OF STANDARDS 16.08.080

	<ul style="list-style-type: none"> • <i>With project traffic added to the future (2040) background traffic, the River Street / Main Street (SH-75) intersection is anticipated to deteriorate to LOS E.</i> <ul style="list-style-type: none"> o <i>The City may consider restricting left-turn movements from the minor approaches onto Main Street (SH-75). With left-turn movements rerouted to First Street, all study intersections are anticipated to operate at acceptable levels of service.</i> • <i>Queues on the westbound approach to the River Street / Main Street intersection are anticipated to extend past the Dropoff Access at times but are not anticipated to interfere with traffic circulation in the study area.</i> <p><u>Parking -</u></p> <ul style="list-style-type: none"> • <i>Current plans propose 90 subterranean off-street parking spaces at the hotel.</i> • <i>Hales Engineering recommends, that 90 off-street parking spaces are adequate to accommodate the anticipated parking demand.</i>
16.08.080.G	<p>The plan is in conformance with and promotes the purposes and goals of the comprehensive plan, zoning ordinance, and other applicable ordinances of the city, and not in conflict with the public interest:</p> <ol style="list-style-type: none"> 1. Pursuant to subsection 16.08.070D of this chapter, all of the design review standards in chapter 17.96 of this code shall be carefully analyzed and considered. This includes detailed analysis of building bulk, undulation and other design elements. The site plan should be sensitive to the architecture and scale of the surrounding neighborhood. 2. The influence of the site design on the surrounding neighborhood, including relationship of the site plan with existing structures, streets, traffic flow and adjacent open spaces, shall be considered. 3. The site design should cluster units on the most developable and least visually sensitive portion of the site.
Comment	<p><i>Also please see Exhibit #2: Analysis / Study which contains a thorough assessment of the Ketchum Boutique Hotel’s compliance with the Ketchum 2014 Comprehensive Plan.</i></p> <p><i>PEG Development and the Design Team for the Ketchum Boutique Hotel have worked closely with the Ketchum City Officials and Planning Staff as we have been developing the design to ensure that the project conforms with the goals of the community. The hotel is aligned with long term planning objectives for the site and is in harmony with the Ketchum 2014 Comprehensive plan.</i></p> <p><i>The massing has been carefully coordinated to honor its bench location by terracing down to follow the topography drop from River Street, those terraces then become gathering spots for guests and the public to enjoy the outdoor climate and take in the scenic vistas that surround the hotel. The massing also provides for a façade that steps in and out of plane, which is enhanced by a layer balconies and articulation of those forms. The building pulls back over 35’ from Trail Creek and has minimal visual impact on Forest Service Park. The building footprint near the property line is confined to a 5’ setback off of the River Street frontage where it has an appropriate relationship to the sidewalk and street scape. The footprint is then pulled back to respect the riparian setback along Trail Creek to minimize the impact of the new building adjacent to a natural feature.</i></p> <p><i>Site landscape design has been designed to complement the bench topography and creek bank features of the site.</i></p>
16.08.080.H	<p>The development plan incorporates the site’s significant natural features.</p>

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	<p><i>scenic views from the site including Bald Mountain and Dollar Mountain. The hotel has been designed to step down, following the bench topography, creating rooftop terraces that take advantage of the vistas available. The landscape plan includes pedestrian pathways for hotel guest and the public to access Trail Creek viewpoint areas set outside of the riparian zone setback.</i></p>
16.08.080.I	<p>Substantial buffer planting strips or other barriers are provided where no natural buffers exist.</p>
	<p><i>Also Please see Exhibit #5 Landscape Drawings.</i></p> <p><i>A buffer landscape island is being provided to separate the hotel’s Porte Cochere drive being requested in an Encroachment Permit request that is being submitted concurrently with the PUD. We have a 25’ setback from Main Street that will be landscaped. We intend to have an outdoor dining patio at the intersection of Main Street and River Street and will have landscape and architectural barriers such as raised planters, raised water features, and architecturally integrated railings separating the dining patio from the street. We have a 25’ Riparian Easement along with a 10’ Utility Easement that combine to create a 35’ setback from the property line that borders Trail Creek.</i></p>
16.08.080.J	<p>Each phase of such development shall contain all the necessary elements and improvements to exist independently from proposed future phases in a stable manner.</p>
<i>Comment</i>	<p><i>N/A. This project is being developed in a single development phase.</i></p>
16.08.080.K	<p>Adequate and usable open space shall be provided. The applicant shall dedicate to the common use of the homeowners or to the public adequate open space in a configuration usable and convenient to the residents of the project. The amount of usable open space provided shall be greater than that which would be provided under the applicable aggregate lot coverage requirements for the zoning district or districts within the proposed project. Provision shall be made for adequate and continuing management of all open spaces and common facilities to ensure proper maintenance.</p>
<i>Comment</i>	<p><i>Per table 17.12.030 we are required to have 30% open space, we currently have 44%. The open space provided exceeds the requirement by 14%. The open space is primarily usable, the building’s footprint was intentionally pulled back from Main Street to create a larger site area against that entrance corridor that will be fully landscaped and have pedestrian pathways that will be accessible to the hotel guests and the public. The landscaping will all be managed by the hotel operator.</i></p> <p><i>Total Site – 47,249 sq. ft. Total Bldg Footprint – 26,555 sq. ft. Total Unbuilt Area – 20,699 sq. ft. Total Unbuilt ‘Open Space’ – 44%</i></p>
16.08.080.L	<p>Location of buildings, parking areas and common areas shall maximize privacy within the project and in relationship to adjacent properties and protect solar access to adjacent properties.</p>
<i>Comment</i>	<p><i>Also please see Exhibit #1 Architectural Drawings, Exhibit #2 Sun Studies, Exhibit #3 Height Analysis & Compatibility Views.</i></p> <p><i>The Ketchum Boutique Hotel is configured along a northwest spine that has allowed for the building’s mass to be pulled back from the roadway view corridor leading to Main Street. All onsite parking is contained below grade and will have no visual impact on the site. The hotel features an interior courtyard located on level 2 that faces south, the courtyard will be hotel’s</i></p>

APPLICATION OF STANDARDS 16.08.080

	<p><i>properties. The hotel features many architectural balcony elements that serve to create another layer of structure between the guests and the exterior, enhancing a sense of a perimeter of privacy in those guest rooms.</i></p> <p><i>The Sun Studies provided in Exhibit #2 demonstrate that the massing of the hotel will have very minimal shade impact on adjacent buildings, only during the December studies do any shadows from the hotel intrude appreciably on any adjacent properties, and in those cases the shadow impacts from our hotel are not any more intrusive than the affected buildings have on their adjacent neighbors.</i></p>
16.08.080.M	<p>Adequate recreational facilities and/or daycare shall be provided. Provision of adequate on site recreational facilities may not be required if it is found that the project is of insufficient size or density to warrant same and the occupant's needs for recreational facilities will be adequately provided by payment of a recreation fee in lieu of such facilities to the city for development of additional active park facilities. On site daycare may be considered to satisfy the adequate recreational facility requirement or may be required in addition to the recreational facilities requirement.</p>
Comment	<p><i>Also please see Exhibit #1 Architectural Drawings.</i></p> <p><i>The Ketchum Gateway Hotel will feature a 2nd floor rooftop courtyard with 2 hot tubs that feature views of Bald Mountain. The hotel will also have a fitness room that is located off of the rooftop courtyard. There will also be additional rooftop terraces available to the hotel guests and the public. We feel those amenities will adequately meet the recreational needs appropriate to the scale of this hospitality property. On site daycare is not being provided.</i></p>
16.08.080.N	<p>There shall be special development objectives and special characteristics of the site or physical conditions that justify the granting of the PUD conditional use permit.</p>
Comment	<p><i>The site is on a bench with approximately 30 feet of grad change and without the PUD process would not be developable as a hotel as it would have to have one building along River Street, and a second building at the bottom of the hotel accessible via Main Street (a curb cut that Ketchum City and the Idaho Transportation Department both would like to see eliminated).</i></p>
16.08.080.O	<p>The development will be completed within a reasonable time.</p>
Comment	<p><i>PEG has every intent of expeditiously moving the project through a thorough public entitlement process and then proceeding with the construction of the project, they are targeting a spring 2020 construction start and a fall 2021 opening. Please see Narrative #4 Development Schedule.</i></p>
16.08.080.P	<p>Public services, facilities and utilities are adequate to serve the proposed project and anticipated development within the appropriate service areas.</p>
Comment	<p><i>The design team met with the Ketchum City department heads on 4/9/2019 and has since continued to work with the applicable sewer, water, power, gas, and electric utility entities regarding the utility needs of the hotel project. There have been no significant hurdles raised to providing the necessary utilities to this project.</i></p>
16.08.080.Q	<p>The project complies with all applicable ordinances, rules and regulations of the city of Ketchum, Idaho, except as modified or waived pursuant to this section</p>
Comment	<p><i>A concurrent Design Review Application is being submitted for Commission review, PEG Development and the design team feel that with the allowances granted that are being requested as part of this submittal process, the project meets all of the requirements of the municipal development requirements of Ketchum City.</i></p>

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NARRATIVE | 01.05

DEVELOPMENT SCHEDULE

DEVELOPMENT SCHEDULE

We understand that there are a lot of factors that could, and will affect the development and construction schedules. Below are target project milestones that we would like to achieve in order to have a successful, and on-time project. Current assumption is that this will not be a phased development, and that all construction will occur in one continuous block of time. Local contractors are currently being engaged for pre-construction services, and will provide more detailed construction schedules at a later date, including demolition of existing structures, excavation, public utilities connections, permanent shoring, vertical construction, and finished site work. Anticipated construction duration is approximately 18 months.

TARGET SCHEDULE

Entitlements | Ketchum Approval Process

Pre-Application Planning Commission Meeting	July 29th, 2019
Planning Commission | Full Application	August 15th, 2019
Ketchum City Council | Full Application	October 1st, 2019
Construction Documents | Planning Submittal	February 15th, 2020
Final Permit Approvals	April 2020

Construction

Construction Start - Demo | Shoring | Utilities	April 2020
Construction Duration	16 - 18 Months
Construction Completion	Fall 2021

NARRATIVE | 01.06

PARKING ANALYSIS | TRAFFIC STUDY

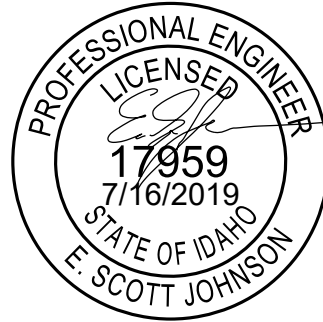
PARKING ANALYSIS



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MEMORANDUM

Date: July 16, 2019
To: Nick Blayden, PEG Companies
From: Hales Engineering



Subject: Ketchum Boutique Hotel Parking Study

UT19-1441

This memorandum discusses the parking study completed for the proposed Ketchum Boutique Hotel located in Ketchum, Idaho. The study will identify the City of Ketchum parking requirements, the parking demand rates identified by the Institute of Transportation Engineers (ITE) and propose methods for reducing parking demand at the proposed hotel.

Project Description

The proposed Ketchum Boutique Hotel is located on the southwest quadrant of the Main Street (SH-75) / River Street intersection in Ketchum, Idaho. A vicinity map of the project site is shown in Figure 1. The proposed hotel will have 100 guestrooms, with an onsite restaurant, bar, meeting space, and other typical hotel amenities. The hotel will also have on-site employee housing to accommodate up to 30 employees. The site plan calls for 90 off-street parking spaces in an underground parking garage.

A market study for the proposed hotel projects that weekend occupancy at the hotel will range from 42% to 93% throughout the year, with an average occupancy of 72%.

City of Ketchum Parking Requirements

The City of Ketchum parking requirements set forth for this land use in the Ketchum Municipal Code (KMC), Title 17, Chapter 17.125 are as follows:

Restaurant/Bar - First 5,500 gross square feet are exempt from parking requirements. Parking required for additional square footage at a ratio of 1 space per 1,000 gross square feet.

Hotel (Excluding Restaurant/Bar) - 1 space per 1,000 gross square feet.

Employee Housing - 0 parking space required for units under 750 square feet, 1 parking space required for units 751-2,000 square feet, 2 parking spaces required for units 2,001 square feet or more.

The City also requires that the six on-street parking spaces that will be eliminated with the construction of this project be offset in the off-street parking garage.



Figure 1: Site vicinity map of the project in Ketchum, Idaho

Parking Demand Rate Calculations

Hales Engineering calculated the required parking according to the rates discussed in the previous section. These calculations are summarized in Table 1. As shown in Table 1, the planned uses for this site would require 87 parking spaces. The site plan for the project identifies a supply of 90 parking spaces on-site; therefore, the project is ***overparked by 3 spaces***.

PARKING ANALYSIS



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Table 1 Ketchum Gateway Hotel Parking Study Required Parking Calculation			
Use	Quantity	Unit	Total Required Spaces
Restaurant / Bar	6,435	Square Feet	1
Hotel	66,000	Square Feet	66
Employee Housing (751-2,000 Square Feet)	14	Units	14
On-Street Parking Offset	6	Spaces	6
Total			87

SOURCE: Hales Engineering, July 2019

Parking Demand Management

It may be desirable to reduce the parking demand on site further. This can be accomplished by providing amenities and services that eliminate the need for a hotel employee or guest to use a personal vehicle. Hales Engineering identifies the following methods to reduce parking demand on site:

Guest Shuttle – Providing a shuttle between the airport and the hotel allows guests to stay at the hotel without parking a personal vehicle on site. The shuttle can also be used to transport guests to / from various locations in the area, including dining, shopping, and recreation.

Employee Shuttle – An employee shuttle can be used to transport employees between the hotel and an offsite location, eliminating the need for employees to drive a personal vehicle to work.

Car Share Program – A car share program consists of having a vehicle on site that can be checked out by employees and guests as needed. This would provide employees and guests the mobility of a personal vehicle without bringing personal vehicles to the hotel site.

Subsidized Transit Passes – Providing a subsidized transit pass to employees living off site will allow them to travel to / from work without using a personal vehicle.

Carpool Program – Coordinating and/or incentivizing employee carpooling will reduce the number of employee vehicles needed to park on site.

Alternative Transportation – Providing facilities such as bicycle storage on site will allow employees to use alternative modes of transportation.



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Conclusions and Recommendations

Hales Engineering makes the following conclusions regarding parking at the proposed Ketchum Boutique Hotel:

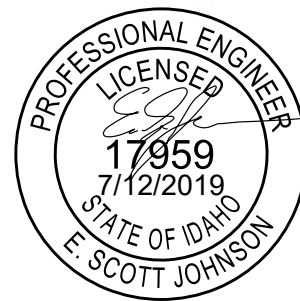
- Current plans call for 90 off-street parking spaces at the hotel (supply).
- KMC requires that 87 off-street parking spaces for the proposed land use (KMC requirement).
- It may be desirable to further reduce parking demand on site. Hales Engineering recommends the following measures to reduce on-site parking demand:
 - Guest Shuttle
 - Employee Shuttle
 - Car Share Program
 - Subsidized transit pass
 - Carpool Program
 - Alternative Transportation
- As shown, the 90 off-street parking spaces planned for this site will meet the KMC parking requirements.

If you have any questions regarding this memorandum, please feel free to contact us.



Ketchum Boutique Hotel

Traffic Impact Study



Ketchum, Idaho

July 12, 2019

UT19-1441

EXECUTIVE SUMMARY

This study addresses the traffic impacts associated with the proposed Ketchum Boutique Hotel located in Ketchum, Idaho. The proposed project is located south of River Street, between Main Street (SH-75) and Washington Avenue.

Included within the analyses for this study are the traffic operations and recommended mitigation measures for existing conditions and plus project conditions (conditions after development of the proposed project) at key intersections and roadways near the site. Future 2025 and 2040 conditions were also analyzed.

The evening peak hour level of service (LOS) was computed for each study intersection. The results of this analysis are shown in Table ES-1.

TABLE ES-1 LOS Analysis - Evening Peak Hour Ketchum - Marriott Autograph Hotel TIS									
Intersection	Level of Service (Sec/Veh) ¹								
	Existing (2019) Background	Existing (2019) Background Mitigated	Existing (2019) Plus Project	Existing (2019) Plus Project Mitigated	Future (2025) Background	Future (2025) Background Mitigated	Future (2025) Plus Project	Future (2040) Background	Future (2040) Plus Project
First Street / Main Street (SH-75)	B (14.3)	B (14.4)	B (19.0)	B (14.5)	B (17.1)	C (25.5)	C (26.4)	D (48.1)	D (45.5)
River Street / Main Street (SH-75)	D (33.9) / WB	D (34.9) / WB	F (>50.0) / WB	C (18.8) / EB	F (>50.0) / WB	C (24.3) / EB	C (24.1) / EB	E (35.5) / WB	F (>50.0) / WB
First Street / Washington Avenue	A (5.3) / WB	A (5.4) / WB	C (17.3) / EB	A (5.5) / WB	A (5.8) / WB	A (5.7) / WB	A (5.8) / WB	A (5.9) / WB	A (5.8) / WB
River Street / Washington Avenue	A (3.5) / SB	A (3.5) / SB	A (3.6) / SB	A (3.6) / SB	A (3.6) / SB	A (3.7) / SB	A (3.8) / SB	A (3.7) / SB	A (3.9) / SB
Serenade Lane / Main Street (SH-75)	E (39.0) / EB	A (4.0) / EB	A (7.3) / NB	A (3.5) / EB	A (4.6) / EB	A (3.6) / EB	A (4.6) / EB	B (10.2) / EB	B (11.8) / EB
Dropoff Access / River Street ²	-	-	E (39.8) / NB	A (7.0) / NB	-	-	B (10.6) / NB	-	C (16.7) / NB
Garage Access / River Street ²	-	-	A (4.2) / NB	A (2.4) / NB	-	-	A (2.8) / NB	-	A (2.8) / NB

1. Intersection LOS and delay (seconds/vehicle) values represent the overall intersection average for roundabout, signalized, all-way stop controlled intersections and the worst approach for all other unsignalized intersections.
2. This intersection is a project access and was only analyzed in "plus project" scenarios.
Source: Hales Engineering, July 2019.

SUMMARY OF KEY FINDINGS

The following is a summary of key findings and recommendations:

- The proposed development will consist of a 100-room hotel and related amenities such as a restaurant/lounge, meeting space, a fitness center, and a bar. There will also be employee housing onsite to accommodate up to 30 employees.
- All development access will be to River Street.
 - One access will be an exit from the hotel drop off area.

TRAFFIC STUDY



- The second access will accommodate ingress and egress movements to the hotel parking garage, as well as be an entrance for the hotel drop off area.
- Peak period turning movement count data was collected on Tuesday, April 2, 2019. Data from nearby ATR #68 was used to adjust the observed volumes to represent a typical weekday in the month of July.

Level of Service (LOS) Analysis

- First Street / Main Street (SH-75)
 - Currently operating at LOS B during the evening peak hour.
 - Anticipated to deteriorate to LOS D with future 2040 background traffic conditions.
 - The addition of project generated traffic to this intersection is not anticipated to have significant impact on levels of service in any analysis scenario.
- River Street / Main Street (SH-75)
 - Currently operating at LOS D during the evening peak hour.
 - Anticipated to deteriorate to LOS F with future 2025 background traffic conditions.
 - It is anticipated that by restricting east- and westbound left-turn movements the intersection will improve to LOS C.
 - Redirecting displaced movements to the First Street / Main Street (SH-75) intersection is not anticipated to significantly affect operations at that intersection.
 - Converting Main Street (SH-75) to a three-lane cross section with left-turn lanes at intersections will increase the number of acceptable gaps for vehicles turning on to Main Street (SH-75) from River Street.
 - Anticipated to operate at LOS E with future 2040 background traffic conditions.
 - The City will need to determine if mitigating this poor level of service will be worth the required tradeoffs.
 - Anticipated to deteriorate to LOS F in current conditions with project generated traffic added.
 - Restricting left-turn movements on the east- and westbound approaches is anticipated to mitigate this poor level of service.
 - A northbound left-turn lane would allow for turning vehicles to queue without blocking through traffic. However, it is not necessary to maintain an acceptable level of service at this intersection.
- First Street / Washington Avenue
 - Currently operating at LOS A.
 - Anticipated to continue to operate at an acceptable LOS in future conditions as well as with project traffic added.
- River Street / Washington Avenue



- Currently operating at LOS A.
- Anticipated to continue to operate at an acceptable LOS in future conditions as well as with project traffic added.
- Serenade Lane / Main Street (SH-75)
 - Currently operating at LOS E.
 - A right-turn acceleration lane for vehicles turning right from Serenade Lane onto Main Street (SH-75)
 - This acceleration lane could be easily accommodated with the planned ITD improvements to SH-75 that will result a three-lane cross section north of Serenade Lane and a four-lane cross section south of Serenade Lane.

Queueing Analysis

- The 95th percentile queues on the south- and westbound approaches to the First Street / Main Street (SH-75) intersection are anticipated to extend for several hundred feet during the evening peak hour with future 2025 and future 2040 background traffic conditions.
 - It is recommended that the City conduct a corridor study to determine the best course of action to accommodate traffic on Main Street (SH-75). It is possible that a coordinated signal system and/or a road diet could improve traffic flow through the downtown area and reduce queuing throughout the corridor.
- The 95th percentile queues on the eastbound approach to the River Street / Main Street (SH-75) intersection are anticipated to extend for up to approximately 80 feet with future traffic conditions. These queues will extend past the hotel drop off access at times but is not anticipated to have a significant impact on traffic flows in the area.
- No additional significant queuing is anticipated.
- The addition of a northbound left-turn lane at the River Street / Main Street (SH-75) intersection is anticipated to result in reduced queuing on the northbound approach. This improvement would mitigate an existing condition and the need for it is not triggered by the addition of hotel generated traffic.
- Based on the anticipated 95th percentile queues, it is recommended that the left-turn lane on the northbound approach to the River Street / Main Street (SH-75) intersection be constructed with 100 feet of storage length.

Pedestrian Analysis

- It is recommended that the gaps in the sidewalk along River Street be eliminated and that appropriate pedestrian ramps be constructed on the southeast and southwest corners of the River Street / Main Street (SH-75) intersection.
- It is recommended that the pedestrian crossings on the uncontrolled north- and southbound approaches to the River Street / Main Street (SH-75) intersection be

TRAFFIC STUDY



eliminated and that all pedestrians be directed to cross Main Street (SH-75) at First Street, which is controlled by a traffic signal. If the City determines that these crosswalks are necessary, it is recommended that only one crosswalk be located on the southbound approach and that crosswalk enhancements, such as RRFBs, be added to the crossing to increase pedestrian visibility and driver awareness of the crossing.

SUMMARY OF RECOMMENDATIONS

The following is a summary of recommendations from each analysis scenario. Recommendations from a “background” scenario are to mitigate existing problems, or problems that are anticipated in the future without the addition of traffic generated by the proposed hotel. Recommendations from a “plus project” scenario are to mitigate problems anticipated by the addition of hotel generated traffic to the roadway network.

Existing (2019) Background

- Restrict east- and westbound left-turn movements at the River Street / Main Street (SH-75) intersection.
- Add right-turn acceleration lane for eastbound right-turning vehicles at the Serenade Lane / Main Street (SH-75) intersection.

Existing (2019) Plus Project

- No additional recommendations.

Future (2025) Background

- A corridor study is recommended to analyze Main Street (SH-75) and determine if a road diet (reduction from a four-lane cross section to a three-lane cross section) and or a coordinated signal system would be beneficial.

Future (2025) Plus Project

- No additional recommendations.

Future (2040) Background

- No additional recommendations.



Future (2040) Plus Project

- No additional recommendations.

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I. INTRODUCTION

A. Purpose

This study addresses the traffic impacts associated with the proposed Ketchum Boutique Hotel located in Ketchum, Idaho. The proposed project is located south of River Street, between Main Street (SH-75) and Washington Avenue. Figure 1 shows a vicinity map of the proposed development.

Included within the analyses for this study are the traffic operations and recommended mitigation measures for existing conditions and plus project conditions (conditions after development of the proposed project) at key intersections and roadways near the site. Future 2025 and 2040 conditions were also analyzed.



Figure 1: Vicinity map showing the project location in Ketchum, Idaho

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B. Scope

The study area was defined based on conversations with City staff and the development team. This study was scoped to evaluate the traffic operational performance impacts of the project on the following intersections:

- First Street / Main Street (SH-75)
- River Street / Main Street (SH-75)
- First Street / Washington Avenue
- River Street / Washington Avenue
- Serenade Lane / Main Street (SH-75)

C. Analysis Methodology

Level of service (LOS) is a term that describes the operating performance of an intersection or roadway. LOS is measured quantitatively and reported on a scale from A to F, with A representing the best performance and F the worst. Table 1 provides a brief description of each LOS letter designation and an accompanying average delay per vehicle for both signalized and unsignalized intersections. Figure 2 provides a visual representation of each LOS letter designation.

The *Highway Capacity Manual* (HCM), 6th Edition, 2016 methodology was used in this study to remain consistent with “state-of-the-practice” professional standards. This methodology has different quantitative evaluations for signalized and unsignalized intersections. For signalized and all-way stop intersections, the LOS is provided for the overall intersection (weighted average of all approach delays). For all other unsignalized intersections, LOS is reported based on the worst approach.

Using Synchro/SimTraffic software, which follow the HCM methodology, the peak hour LOS was computed for each study intersection. Multiple runs of SimTraffic were used to provide a statistical evaluation of the interaction between the intersections. The detailed LOS reports are provided in Appendix B. Hales Engineering also calculated the 95th percentile queue lengths for each of the study intersections using SimTraffic. The detailed queue length reports are provided in Appendix D.

D. Level of Service Standards

For the purposes of this study, a minimum acceptable intersection performance for each of the study intersections was set at LOS D. If levels of service E or F conditions exist, an explanation and/or mitigation measures will be presented. An LOS D threshold is consistent with “state-of-the-practice” traffic engineering principles for urbanized areas.



Table 1: Level of Service Description

Level of Service	Description of Traffic Conditions	Average Delay (seconds/vehicle)
Signalized Intersections		Overall Intersection
A	Extremely favorable progression and a very low level of control delay. Individual users are virtually unaffected by others in the traffic stream.	$0 \leq 10.0$
B	Good progression and a low level of control delay. The presence of other users in the traffic stream becomes noticeable.	> 10.0 and ≤ 20.0
C	Fair progression and a moderate level of control delay. The operation of individual users becomes somewhat affected by interactions with others in the traffic stream.	>20.0 and ≤ 35.0
D	Marginal progression with relatively elevated levels of control delay. Operating conditions are noticeably more constrained.	> 35.0 and ≤ 55.0
E	Poor progression with unacceptably elevated levels of control delay. Operating conditions are at or near capacity.	> 55.0 and ≤ 80.0
F	Unacceptable progression with forced or breakdown operating conditions.	> 80.0
Unsignalized Intersections		Worst Approach
A	Free Flow / Insignificant Delay	$0 \leq 10.0$
B	Stable Operations / Minimum Delays	>10.0 and ≤ 15.0
C	Stable Operations / Acceptable Delays	>15.0 and ≤ 25.0
D	Approaching Unstable Flows / Tolerable Delays	>25.0 and ≤ 35.0
E	Unstable Operations / Significant Delays Can Occur	>35.0 and ≤ 50.0
F	Forced Flows / Unpredictable Flows / Excessive Delays Occur	> 50.0

Source: Hales Engineering Descriptions, based on the *Highway Capacity Manual (HCM)*, 6th Edition, 2016 Methodology (Transportation Research Board)

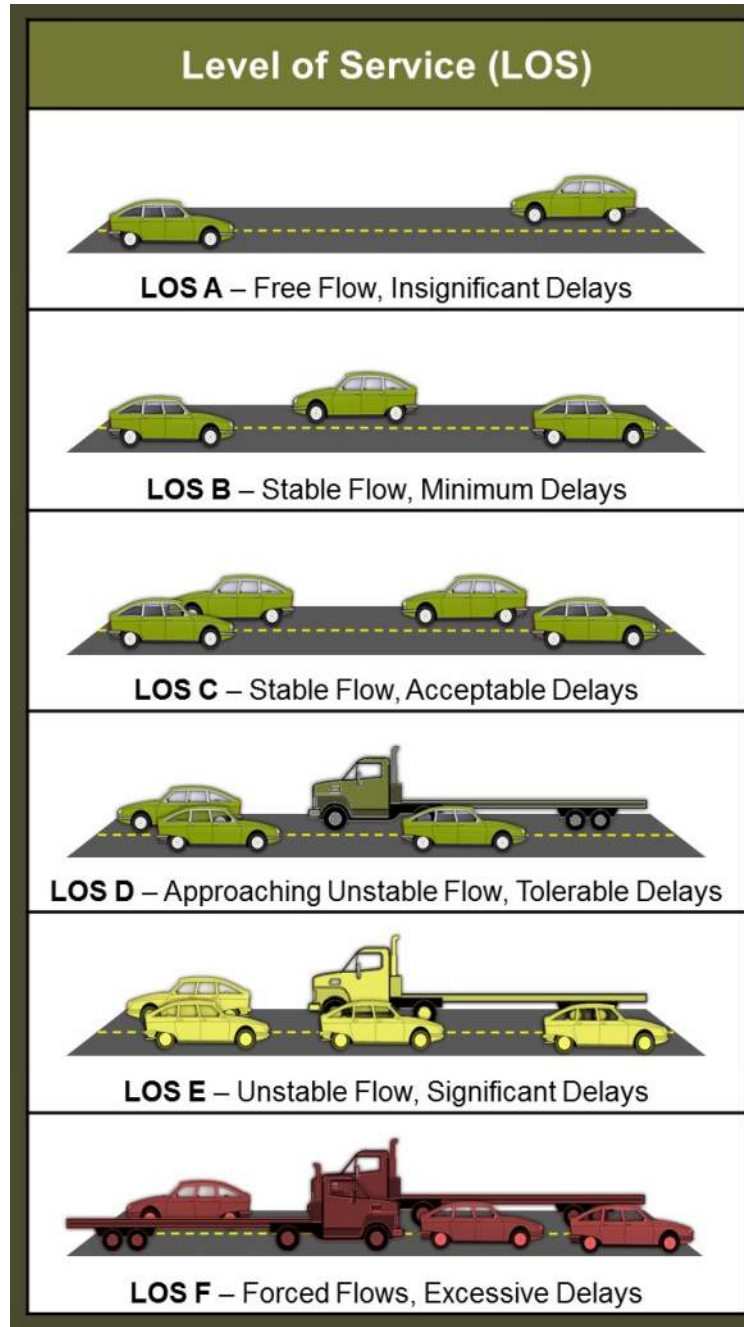


Figure 2: Visual representation of the LOS letter designations



II. EXISTING (2019) BACKGROUND CONDITIONS

A. Purpose

The purpose of the background analysis is to study the intersections and roadways during the peak travel periods of the day with background traffic and geometric conditions. Through this analysis, background traffic operational deficiencies can be identified, and potential mitigation measures recommended. This analysis provides a baseline condition that may be compared to the build conditions to identify the impacts of the development.

B. Roadway System

The primary roadways that will provide access to the project site are described below:

Main Street (SH-75) – is a state-maintained roadway classified by the Idaho Transportation Department (ITD) as a “minor arterial.” Main Street (SH-75) consists of two travel lanes in each direction, which taper down to one travel lane in each direction south of First Street. The posted speed limit is 25 mph through the study area.

River Street – is a City-maintained roadway which will provide direct access to the project site. This roadway has one travel lane in each direction and the posted speed limit was assumed to be 25 mph in the study area.

C. Traffic Volumes

Daily traffic volume data were obtained from a nearby ITD automatic traffic recorder (ATR) on SH-75 between Ketchum and Hailey (ATR #68). The 2018 average daily traffic (ADT) for each day of the week is summarized in Figure 3. As shown in Figure 3, the ADT on SH-75 is significantly higher on weekdays than on weekends. A report from the same ATR summarizing the 500 highest volume hours for 2018 showed that all of the 500 highest hours occurred on a weekday, and all but one occurred between 4:00 p.m. and 6:00 p.m. Therefore, a weekday peak hour was selected for analysis.

Weekday morning (7:00 to 9:00 a.m.) and evening (4:00 to 6:00 p.m.) peak period traffic counts were performed at the following intersections:

- First Street / Main Street (SH-75)
- River Street / Main Street (SH-75)
- First Street / Washington Avenue
- River Street / Washington Avenue

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- Serenade Lane / Main Street (SH-75)

The counts were performed on Tuesday, April 2, 2019. The morning peak hour was determined to be between 7:30 and 8:30 a.m., and the evening peak hour was determined to be between 4:00 and 5:00 p.m. The evening peak hour volumes were between 15% and 25% higher than the morning peak hour volumes. Therefore, the evening peak hour volumes were used in the analysis to represent the worst-case conditions. Detailed count data are included in Appendix A.

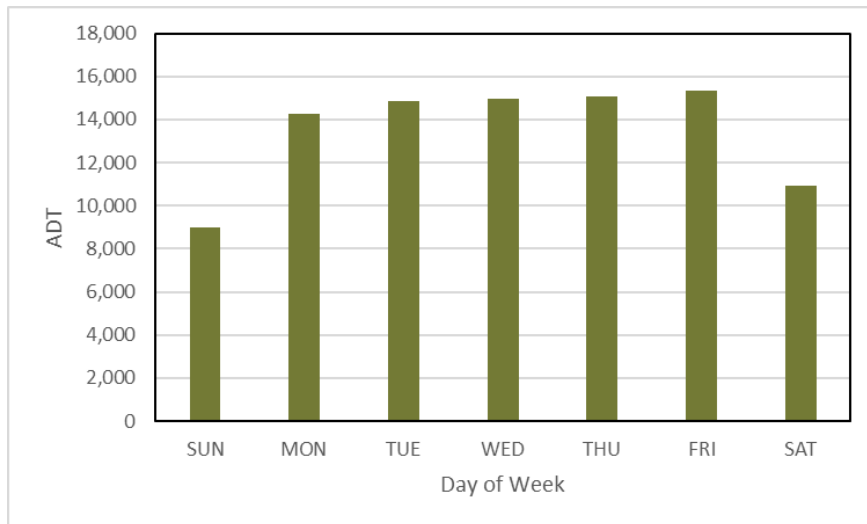


Figure 3: 2018 day of the week ADT for SH-75

Hales Engineering made seasonal adjustments to the observed traffic volumes. Monthly traffic volume data from the past several years were obtained from the previously discussed ATR on SH-75 (ATR #68) and are presented in Figure 4. As shown in Figure 4, the ADT on SH-75 is been highest during the month of July. Traffic volumes in April have been equal to approximately 72% of July traffic volumes. The observed traffic volumes were adjusted accordingly to reflect turning movement volumes at the study intersections on an average July weekday.

The adjusted volumes were compared to turning movement count data collected at two of the study intersections by HDR in July 2018. This is documented in a memorandum to Ketchum City dated August 24, 2018. The adjusted intersections volumes at the First Street / Main Street (SH-75) and River Street / Main Street (SH-75) intersections are between 5% and 7% higher than those presented in the HDR memorandum.



To provide additional context, the daily volumes for July 2018 were plotted and presented in Figure 5. As shown in Figure 5, the highest daily volume of the month occurred on Friday, July 20st. Monday, July 2nd, and Tuesday July 3rd also experience relatively high daily volumes.

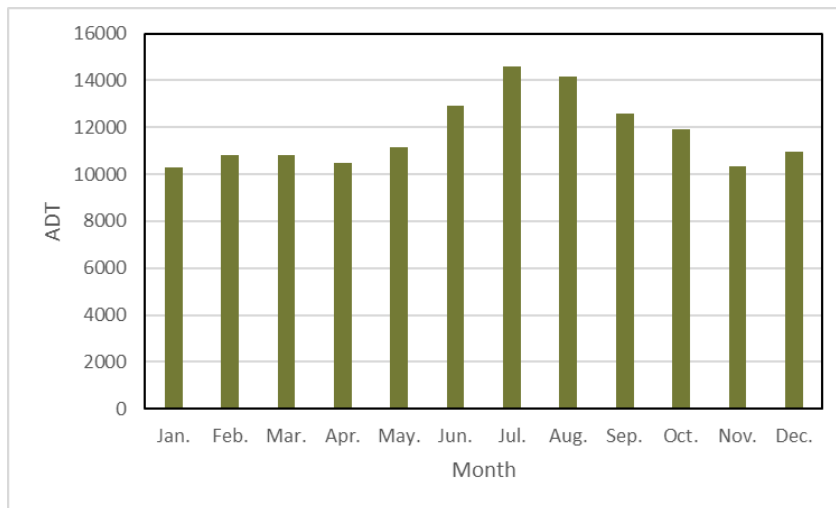


Figure 4: Monthly ADT for SH-75

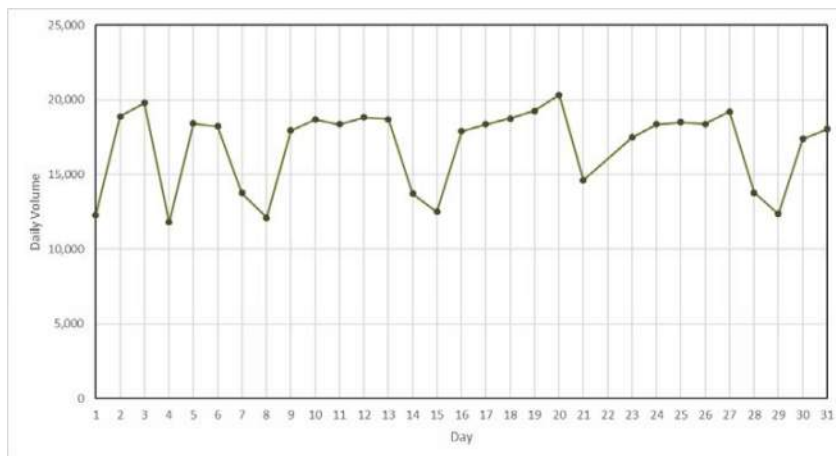


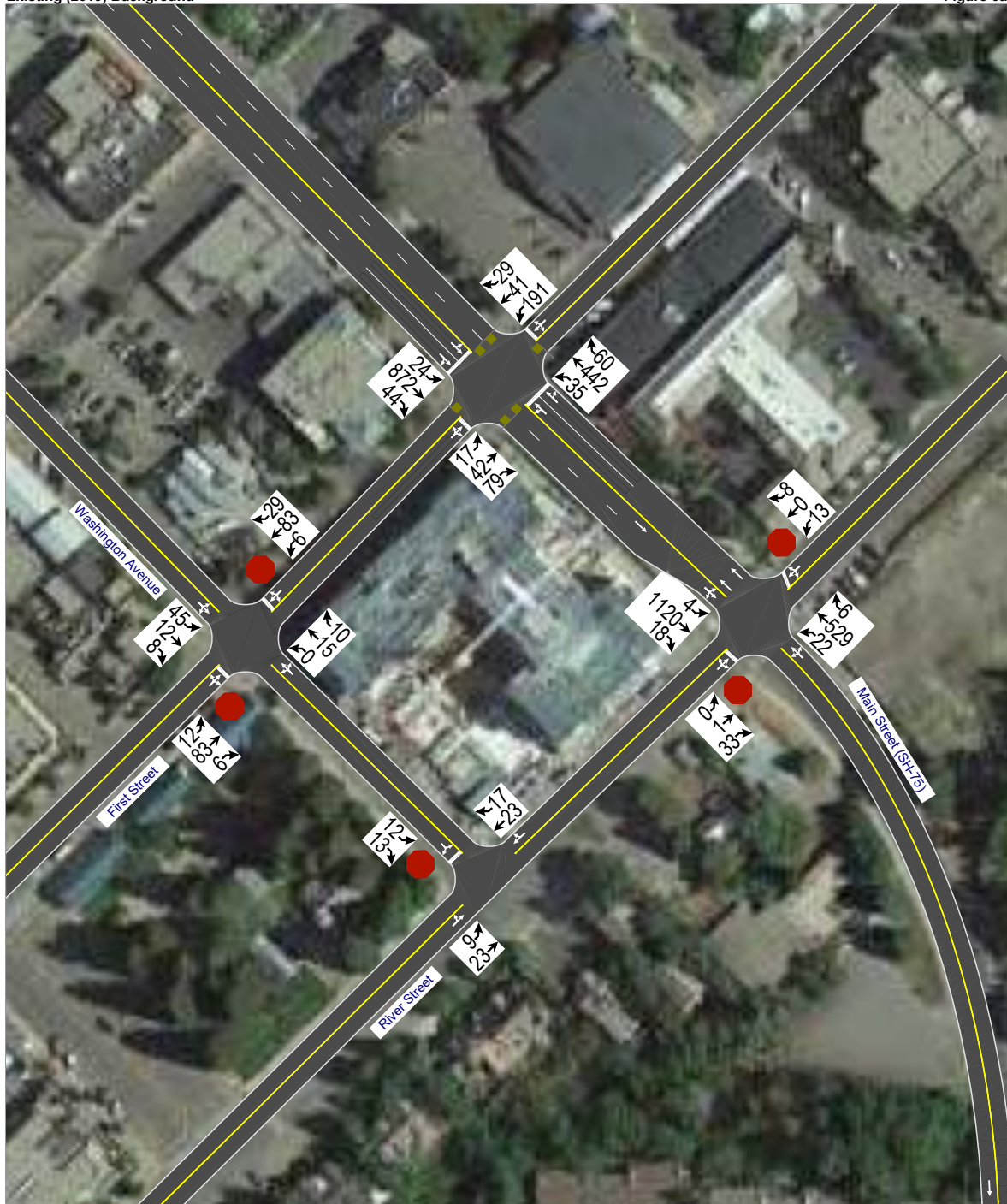
Figure 5: July 2018 daily volumes for SH-75

Figure 6 shows the existing evening peak hour volumes as well as intersection geometry at the study intersections.

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Ketchum Botique Hotel TIS
Existing (2019) Background

Evening Peak Hour
Figure 6a



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Ketchum Botique Hotel TIS
Existing (2019) Background

Evening Peak Hour
Figure 6b



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D. Level of Service Analysis

Hales Engineering determined that the River Street / Main Street (SH-75) intersection is currently operating at LOS D and the Serenade Lane / Main Street (SH-75) intersection is operating at LOS E during the evening peak hour, as shown in Table 2. The remaining study intersections are operating at LOS B or better. These results serve as a baseline condition for the impact analysis of the proposed development during existing (2019) conditions.

Table 2: Existing (2019) Background Evening Peak Hour Level of Service

Intersection Description	Control	Worst Approach			Overall Intersection	
		Approach ^{1,3}	Aver. Delay (Sec/Veh) ¹	LOS ¹	Aver. Delay (Sec/Veh) ²	LOS ²
First Street / Main Street (SH-75)	Signal	-	-	-	14.3	B
River Street / Main Street (SH-75)	EB/WB Stop	WB	33.9	D	-	-
First Street / Washington Avenue	EB/WB Stop	WB	5.3	A	-	-
River Street / Washington Avenue	SB Stop	SB	3.5	A	-	-
Serenade Lane / Main Street (SH-75)	EB Stop	EB	39.0	E	-	-

1. This represents the worst approach LOS and delay (seconds / vehicle) and is only reported for non-all-way stop unsignalized intersections.
 2. This represents the overall intersection LOS and delay (seconds / vehicle) and is reported for all-way stop and signal-controlled intersections.
 3. SB = Southbound approach, etc.

Source: Hales Engineering, July 2019

E. Queuing Analysis

Hales Engineering calculated the 95th percentile queue lengths for each of the study intersections. The 95th percentile queue on the southbound approach to the First Street / Main Street (SH-75) intersection was extends for approximately 260 feet during the evening peak hour. The 95th percentile queue on the westbound approach to the River Street / Main Street (SH-75) intersection extends for approximately 230 feet. No additional significant queuing was observed.



F. Mitigation Measures

The delays at the River Street / Main Street (SH-75) and Serenade Lane / Main Street (SH-75) intersection can be attributed to the difficulty executing a turning movement, especially a left-turn movement, from a stop-controlled approach onto a busy roadway. It is likely that these delays are only prevalent during peak periods.

The City may consider restricting east- and westbound left-turn and through movements and/or north- and southbound left-turn movements at the River Street / Main Street (SH-75) intersection when delay and/or queuing reach undesirable levels. Vehicles that had previously executed these movements could be rerouted to the First Street / Main Street (SH-75) intersection without significant impacts to operations at that intersection.

There is a project planned for SH-75 that would result in a three-lane cross section between River Street and Serenade Lane and a four-lane cross section south of Serenade Lane. With the transition from one southbound lane to two southbound lanes at the Serenade Lane / Main Street (SH-75) intersection, it is recommended that the second southbound lane be constructed as a right-turn acceleration lane to facilitate eastbound right-turning vehicles.

A mitigated scenario was analyzed assuming that the planned SH-75 improvement project had been completed. With the right-turn acceleration lane at the Serenade Lane / Main Street (SH-75) the intersection is anticipated to improve to LOS A.

This mitigated scenario also allowed for a comparison of operations at the River Street / Main Street (SH-75) intersection with and without a northbound left-turn lane. The addition of the northbound left-turn lane did not result in any significant reduction in delay on any approach. There was, however, a significant reduction in the 95th percentile queue on the northbound approach. Without the left-turn lane, the 95th percentile queue extended for approximately 230 feet during the evening peak hour. The addition of a left-turn lane is anticipated to reduce the 95th percentile queue to approximately 45 feet.

No additional mitigation measures are recommended at this time.



III. PROJECT CONDITIONS

A. Purpose

The project conditions discussion explains the type and intensity of development. This provides the basis for trip generation, distribution, and assignment of project trips to the surrounding study intersections defined in Chapter I.

B. Project Description

The proposed Ketchum Boutique Hotel in Ketchum, Idaho is located on the south side of River Street between Main Street (SH-75) and Washington Avenue. The development will consist of a 100-room hotel and related amenities such as a restaurant/lounge, meeting space, a fitness center, and a bar. There will also be employee housing onsite to accommodate up to 30 employees. A concept plan for the proposed developments is provided in Appendix C.

The proposed land use for the development has been identified as follows:

- Hotel 100 Rooms

C. Trip Generation

Trip generation for the development was calculated using trip generation rates published in the Institute of Transportation Engineers (ITE), *Trip Generation*, 10th Edition, 2017. Trip Generation for the proposed project is included in Table 3.

The total trip generation for the development is as follows:

- Daily Trips: 704
- Morning Peak Hour Trips: 46
- Evening Peak Hour Trips: 51

D. Trip Generation Comparison

For comparison purposes, trip generation was calculated assuming that condominiums were constructed on this parcel as opposed to a hotel. Trip generation for this ultimate land use is shown in Table 4. As shown in Table 4, a development consisting of 80 condominiums would only generate 564 daily trips, but would produce the same number of evening peak hour trips as the proposed hotel.



**Table 3
Ketchum - Marriott Autograph Hotel TIS
Trip Generation**

Weekday Daily Land Use ¹	# of Units	Unit Type	Trip Generation	% Entering	% Exiting	Trips Entering	Trips Exiting	Total Daily Trips
Hotel (310)	100	Rooms	704	50%	50%	352	352	704
Project Total Daily Trips						352	352	704
Morning Peak Hour Land Use ¹	# of Units	Unit Type	Trip Generation	% Entering	% Exiting	Trips Entering	Trips Exiting	Total a.m. Trips
Hotel (310)	100	Rooms	46	59%	41%	27	19	46
Project Total a.m. Peak Hour Trips						27	19	46
Evening Peak Hour Land Use ¹	# of Units	Unit Type	Trip Generation	% Entering	% Exiting	Trips Entering	Trips Exiting	Total p.m. Trips
Hotel (310)	100	Rooms	50	51%	49%	26	25	51
Project Total p.m. Peak Hour Trips						26	25	51

1. Land Use Code from the Institute of Transportation Engineers (ITE) *Trip Generation*, 10th Edition, 2017.
SOURCE: Hales Engineering, May 2019

**Table 4
Ketchum - Marriott Autograph Hotel TIS
Trip Generation Comparison**

Weekday Daily Land Use ¹	# of Units	Unit Type	Trip Generation	% Entering	% Exiting	Trips Entering	Trips Exiting	Total Daily Trips
Multifamily Housing (Low-Rise) (220)	80	Dwelling Units	564	50%	50%	282	282	564
Project Total Daily Trips						282	282	564
Morning Peak Hour Land Use ¹	# of Units	Unit Type	Trip Generation	% Entering	% Exiting	Trips Entering	Trips Exiting	Total a.m. Trips
Multifamily Housing (Low-Rise) (220)	80	Dwelling Units	40	23%	77%	9	31	40
Project Total a.m. Peak Hour Trips						9	31	40
Evening Peak Hour Land Use ¹	# of Units	Unit Type	Trip Generation	% Entering	% Exiting	Trips Entering	Trips Exiting	Total p.m. Trips
Multifamily Housing (Low-Rise) (220)	80	Dwelling Units	50	63%	37%	32	19	51
Project Total p.m. Peak Hour Trips						32	19	51

1. Land Use Code from the Institute of Transportation Engineers (ITE) *Trip Generation*, 10th Edition, 2017.
SOURCE: Hales Engineering, July 2019

E. Trip Reduction Strategies

There are methods that can be used to reduce the number of vehicle trips generated by the proposed land use. These methods encourage using alternate modes of transportation or

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increasing the occupancy of vehicles entering and exiting the site. The following trip reduction strategies are proposed:

Guest Shuttle – Providing a shuttle between the airport and the hotel allows guests to stay at the hotel without driving a personal vehicle to the site. The shuttle can also be used to transport guests to and from various locations in the area, including dining, shopping, and recreation.

Employee Shuttle – An employee shuttle can be used to transport employees between the hotel and an offsite location, eliminating the need for employees to drive a personal vehicle to work.

Car Share Program – A car share program consists of having a vehicle on-site that can be checked out by employees and guests as needed. This would provide employees and guests the mobility of a personal vehicle without driving personal vehicles to the hotel site.

Transit Passes – Providing transit passes to employees living off site will allow them to travel to and from work without using a personal vehicle. A bus stop for the Mountain Rides Valley Route is located less than a block north of the project site allowing for convenient use of transit to travel to or from the site.

Carpool Program – Coordinating and/or incentivizing employee carpooling will reduce the number of employee vehicles driven to the site.

Alternative Transportation – Providing facilities such as bicycle storage on site will allow employees to use alternative modes of transportation.

F. Trip Distribution and Assignment

Project traffic is assigned to the roadway network based on the type of trip and the proximity of project access points to major streets, high population densities, and regional trip attractions. Existing travel patterns observed during data collection also provide helpful guidance to establishing these distribution percentages, especially near the site. The resulting distribution of project generated trips during the evening peak hour is as follows:

To/From Project:

- 35% North via Main Street (SH-75)
- 55% South via Main Street (SH-75)
- 10% West via River Street

These trip distribution assumptions were used to assign the evening peak hour generated traffic at the study intersections to create trip assignment for the proposed development. Trip assignment for the development is shown in Figure 7.



G. Access

The proposed access for the site will be gained at the following locations (see also concept plan in Appendix C):

River Street:

- The Dropoff Access will be located approximately 50 feet west of the River Street / Main Street (SH-75) intersection. As the drop-off area is planned to accommodate only one direction of travel, it is anticipated that this access will only serve egress movements.
- The Garage Access will be located approximately 160 feet west of the River Street / Main Street (SH-75) intersection. It is anticipated that this will be a full-movement access.

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Ketchum Boutique Hotel TIS
Trip Assignment

Evening Peak Hour
Figure 7a



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Ketchum Boutique Hotel TIS
Trip Assignment

Evening Peak Hour
Figure 7b



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IV. EXISTING (2019) PLUS PROJECT CONDITIONS

A. Purpose

The purpose of the existing (2019) plus project analysis is to study the intersections and roadways during the peak travel periods of the day for existing background traffic and geometric conditions plus the net trips generated by the proposed development. This scenario provides valuable insight into the potential impacts of the proposed project on background traffic conditions.

B. Traffic Volumes

Hales Engineering added the project trips discussed in Chapter III to the existing (2019) background traffic volumes to predict turning movement volumes for existing (2019) plus project conditions. Existing (2019) plus project evening peak hour turning movement volumes are shown in Figure 8.

C. Level of Service Analysis

Hales Engineering determined that the River Street / Main Street (SH-75) intersection is anticipated to operate at LOS F and the Dropoff Access on River Street is anticipated to operate at LOS E during the evening peak hour with project traffic added, as shown in Table 5. All other study intersections are anticipated to operate at LOS C or better.

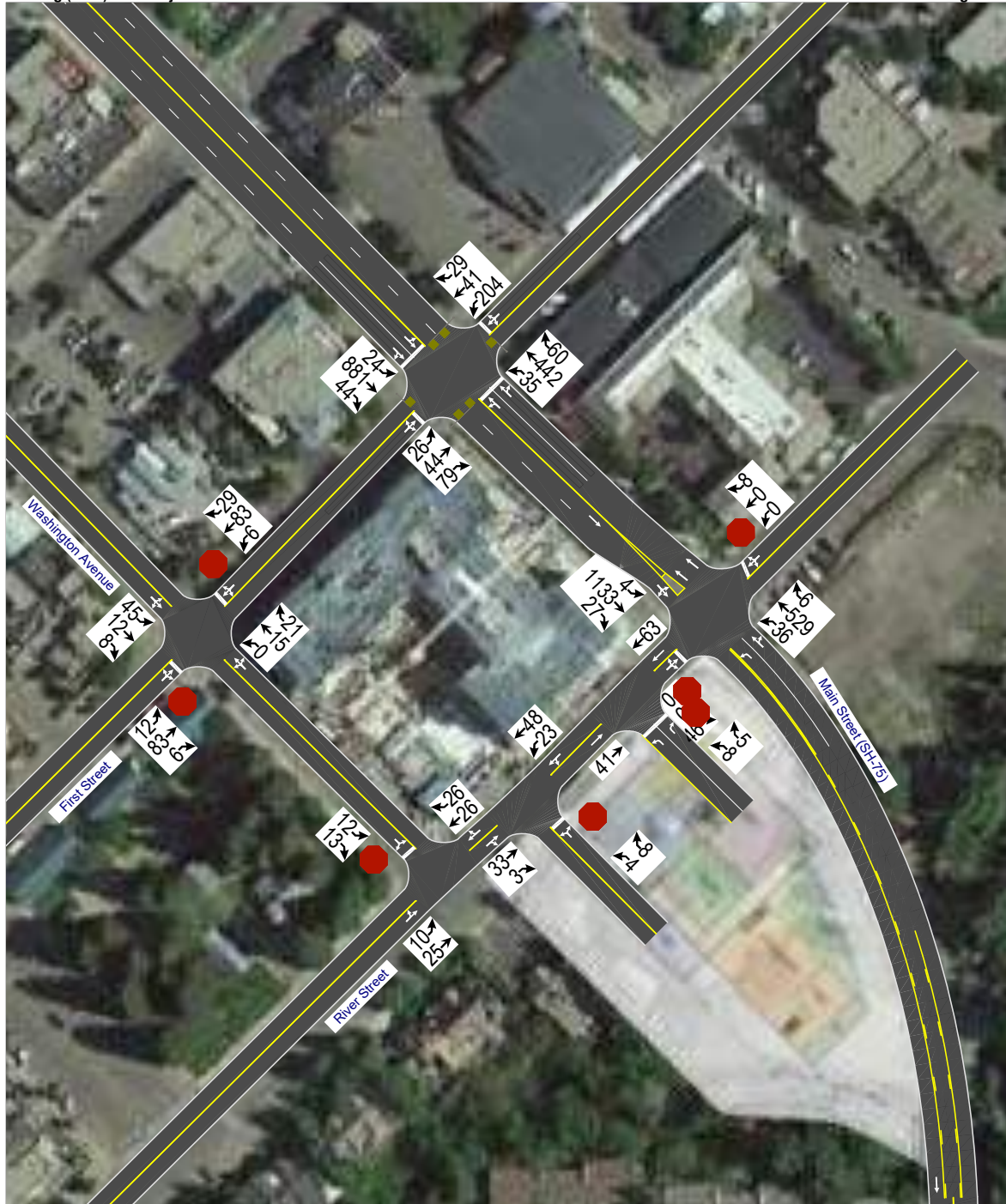
D. Queuing Analysis

Hales Engineering calculated the 95th percentile queue lengths for each of the study intersections. The 95th percentile queue on the southbound approach to the First Street / Main Street (SH-75) intersection is anticipated to extend for approximately 320 feet during the evening peak hour with project traffic added. The 95th percentile queue on the eastbound approach to the River Street / Main Street (SH-75) intersection is anticipated to extend for approximately 75 feet. This will extend past the Dropoff Access at times but is not anticipated to have a significant impact on traffic flows in the area. No additional significant queuing is anticipated during the evening peak hour. An illustration of these queue lengths is shown in Figure 9.

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Ketchum Boutique Hotel TIS
Existing (2019) Plus Project

Evening Peak Hour
Figure 8a



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Ketchum Boutique Hotel TIS
Existing (2019) Plus Project

Evening Peak Hour
Figure 8a



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Figure 9: Existing (2019) conditions queuing figure

E. Mitigation Measures

As discussed in Chapter II, the poor level of service at the River Street / Main Street (SH-75) intersection can be attributed to the difficulty executing a turning movement, especially a left-turn movement, from a stop-controlled approach onto a busy roadway. Also discussed in Chapter II, the City may consider restricting some or all left-turn movements at this intersection, redirecting left-turning vehicles to the nearby First Street / Main Street (SH-75) intersection which is signalized.

The poor level of service at the Dropoff Access can be attributed to vehicles on the eastbound approach to the River Street / Main Street (SH-75) intersection queuing past the access, blocking egress movements. It is anticipated that mitigating the delay and queuing at the River Street / Main Street (SH-75) intersection will reduce delay at this access.

A mitigated scenario was analyzed assuming that east- and westbound left-turn and through movements had been restricted at the River Street / Main Street (SH-75) intersection. With these movements restricted, it is anticipated that the River Street / Main Street (SH-75) intersection will improve to LOS C and the Dropoff Access will improve to LOS A.

TRAFFIC STUDY



No additional mitigation measures are recommended.

Table 5: Existing (2019) Plus Project Evening Peak Hour Level of Service

Intersection		Worst Approach			Overall Intersection	
Description	Control	Approach ^{1,3}	Aver. Delay (Sec/Veh) ¹	LOS ¹	Aver. Delay (Sec/Veh) ²	LOS ²
First Street / Main Street (SH-75)	Signal	-	-	-	19.0	B
River Street / Main Street (SH-75)	EB/WB Stop	WB	>50.0	F	-	-
First Street / Washington Avenue	EB/WB Stop	EB	17.3	C	-	-
River Street / Washington Avenue	SB Stop	SB	3.6	A	-	-
Serenade Lane / Main Street (SH-75)	EB Stop	NB	7.3	A	-	-
Dropoff Access / River Street	NB Stop	NB	39.8	E	-	-
Garage Access / River Street	NB Stop	NB	4.2	A	-	-

1. This represents the worst approach LOS and delay (seconds / vehicle) and is only reported for non-all-way stop unsignalized intersections.
2. This represents the overall intersection LOS and delay (seconds / vehicle) and is reported for all-way stop and signal-controlled intersections.
3. SB = Southbound approach, etc.

Source: Hales Engineering, July 2019



V. FUTURE (2025) BACKGROUND CONDITIONS

A. Purpose

The purpose of the future (2025) background analysis is to study the intersections and roadways during the peak travel periods of the day for future background traffic and geometric conditions. Through this analysis, future background traffic operational deficiencies can be identified, and potential mitigation measures recommended.

B. Roadway Network

As discussed briefly in Chapter II. , information provided by ITD indicates that improvements are planned to SH-75 from Elkhorn Road to River Street. SH-75 is planned to be widened to a three-lane cross section (one travel lane in each direction separated by a center median) between Serenade Lane and River Street and widened to a four-lane cross section (two travel lanes in each direction) south of Serenade Lane. With these changes it was assumed that the northbound approach to the River Street / Main Street (SH-75) intersection would consist of a left-turn lane and a shared through/right-turn lane. It was assumed that these improvements had been implemented prior to 2025. No additional roadway improvements were assumed for the future (2025) analyses. It was assumed that the signal timing plan at the First Street / Main Street (SH-75) intersection had been updated.

C. Traffic Volumes

Hales Engineering used an annual growth rate of 1.40% to project future (2025) peak hour turning movement volumes. This annual growth rate was calculated by HDR and is based on an analysis of data collected by ATRs on SH-75 since 1990. (This is documented in a memorandum to Ketchum City dated August 24, 2018.) Future (2025) evening peak hour turning movement volumes are shown in Figure 10.

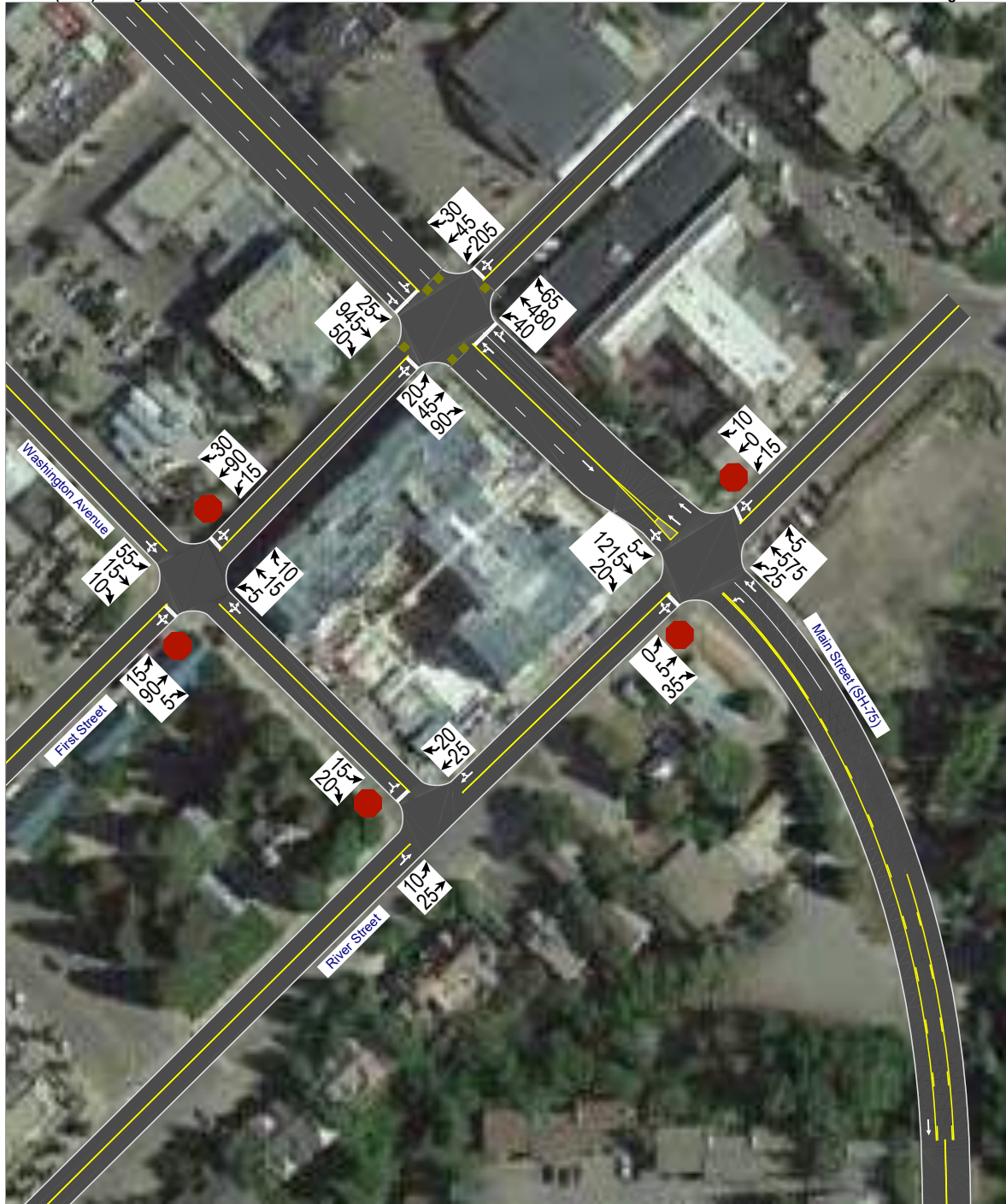
D. Level of Service Analysis

Hales Engineering determined that the River Street / Main Street (SH-75) intersection is anticipated to operate at LOS F during the evening peak hour in future (2025) background conditions, as shown in Table 6. All other study intersections are anticipated to operate at LOS B or better. These results serve as a baseline condition for the impact analysis of the proposed development for future (2025) conditions.

TRAFFIC STUDY

Ketchum Botique Hotel TIS
Future (2025) Background

Evening Peak Hour
Figure 10a



Hales Engineering
1220 North 500 West, Ste. 202 Lehi, Utah 84043

801.766.4343
07/10/2019

TRAFFIC STUDY

Ketchum Botique Hotel TIS
Future (2025) Background

Evening Peak Hour
Figure 10b



Hales Engineering
1220 North 500 West, Ste. 202 Lehi, Utah 84043

801.766.4343
07/10/2019

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NARRATIVE | 01.07

EMPLOYEE HOUSING PLAN

EMPLOYEE HOUSING PLAN

GENERAL OVERVIEW

Having on-site employee housing that is well designed and comfortable to live in is important to a happy and uplifting culture at the hotel. These on-site amenities will draw a higher quality of employee to the project and ultimately increase the overall success of the hotel.

Employee housing units will be located on the bottom two levels of the hotel, and include a mix of one- and two-bedroom apartment units as well as two levels of dorm-style units. Each unit will have its own amenities with private bedrooms, bathrooms, and living / kitchen spaces. Dorms will have shared bunk style rooms with shared bathrooms and living / kitchen spaces. Sliding glass doors to Juliet balconies will be provided on all upper level units, with out-door patios planned for the lower level of each unit.

Single-room apartments are currently planned as two-story units with an upper level entry into a living room / kitchen space. Double story windows and a single staircase will connect to the lower level bedroom with a walk-in closet, private bathroom, and walk-out patio access.

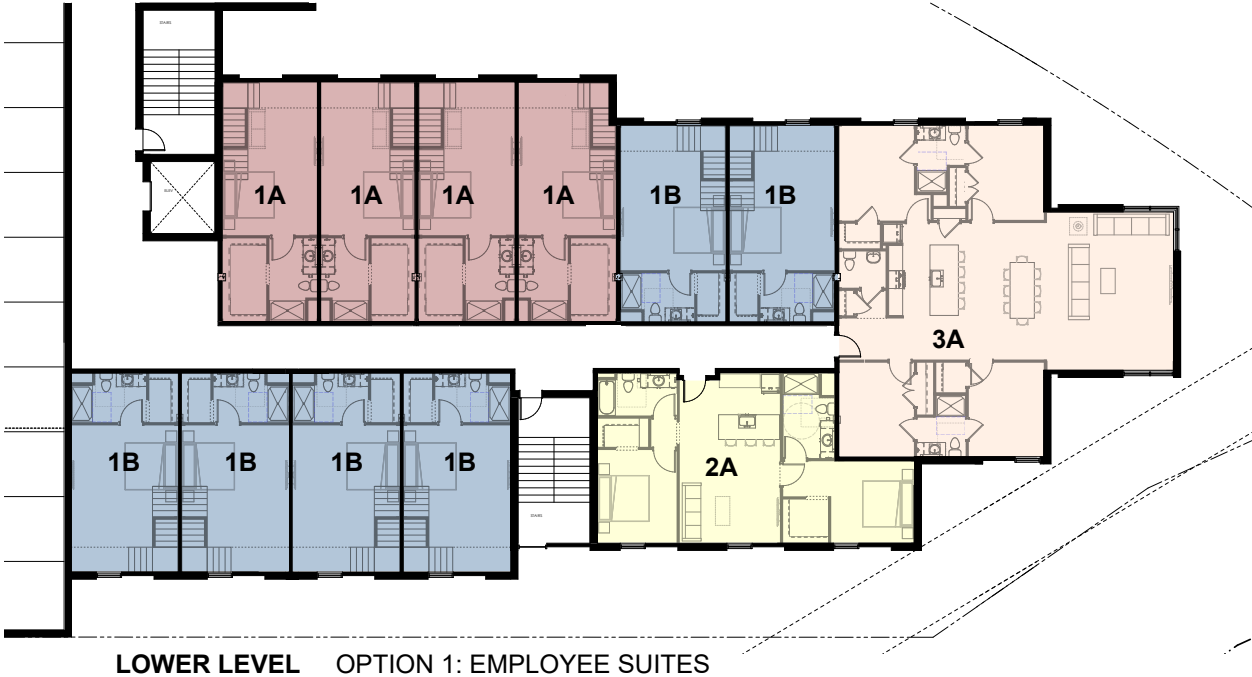
ROOM COUNT

- Current plan for 30 beds
- 12 Apartments - mix of 1 & 2 bedrooms
- 2 Dorms - 8 beds per dorm w/ shared restroom & living facilities
- Schematic Unit Plans are shown on the following sheets.

MANAGEMENT PLAN

On-site employee housing will be managed by the hotel operating company. Each employee will be offered a subsidized rental rate below Ketchum market value. The general manager and assistant general manager of the hotel will have the option to live on-site with rent-free housing as a benefit to entice higher quality management. Security deposits will be taken from each employee equal to one month of rent. Each employee living on-site will also have the option for one parking stall in the garage. Additional rules will be set in place including no overnight guests, no pets, etc.

EMPLOYEE HOUSING PLAN

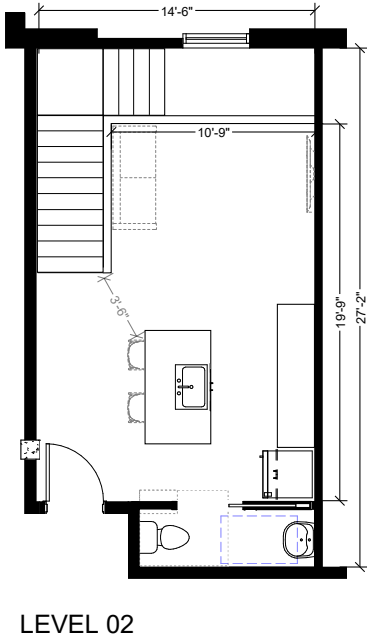
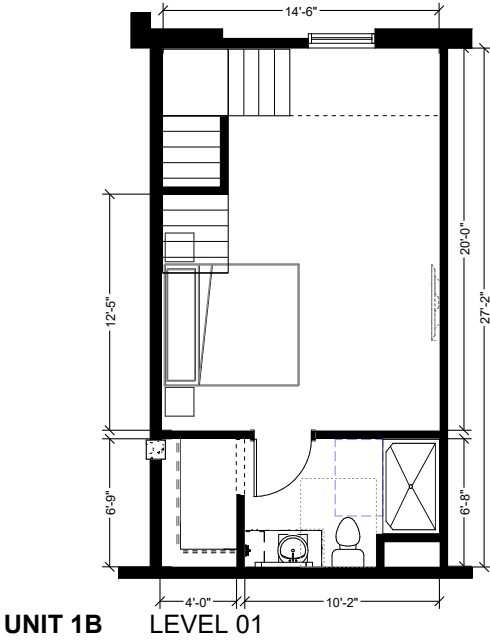
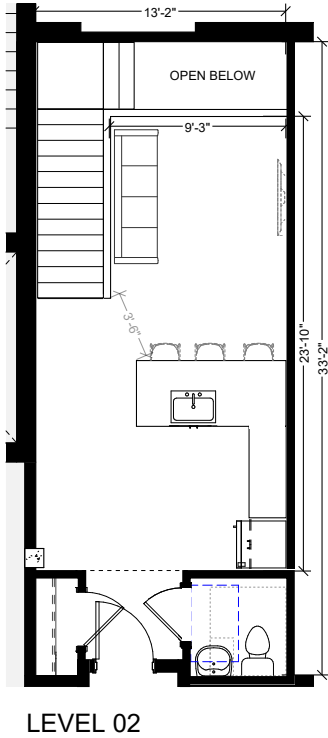
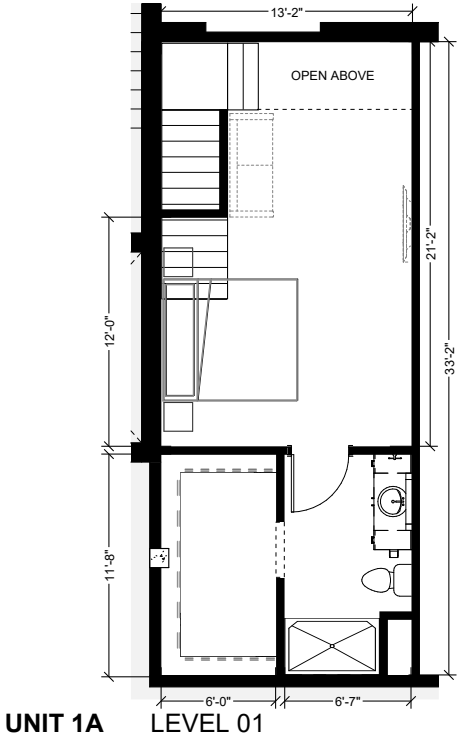


LOWER LEVEL OPTION 1: EMPLOYEE SUITES

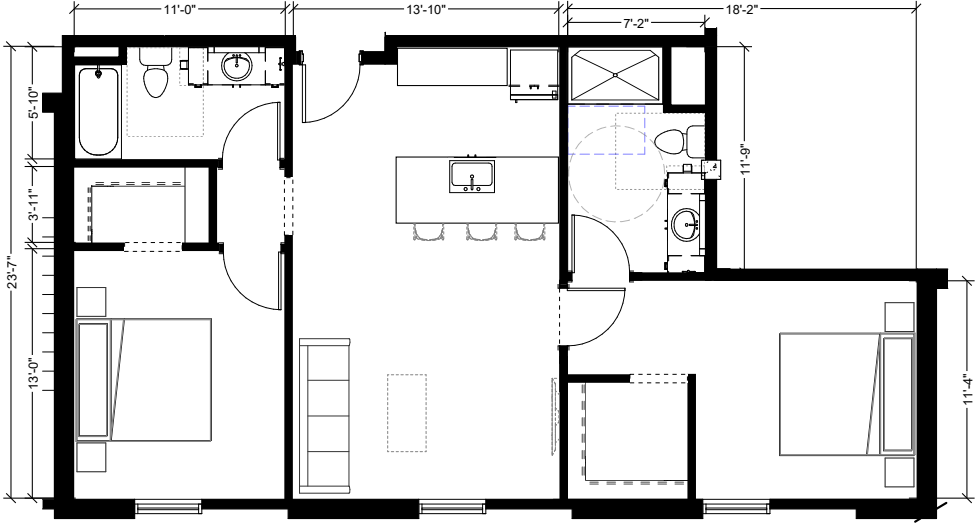


UPPER LEVEL

EMPLOYEE HOUSING PLAN



EMPLOYEE HOUSING PLAN



UNIT 2A LEVEL 02 - ANSI TYPE A



UNIT 3A OPTION 1

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NARRATIVE | 01.08

SUB-AREA ANALYSIS

SUB-AREA ANALYSIS

FITTING INTO THE KETCHUM CONTEXT

HISTORY AND DIVERSITY

The City of Ketchum has a very diverse architectural history, style, and scale. Buildings and street-scapes vary widely in size and shape depending on the period in which they were built, and function of their program. From large heavy timber ski lodges, historic brick mercantile buildings, and ornate mining / ski structures, to modern, clean-line commercial buildings and residences, there is no one specific style that defines the area.

ADJACENT SUB-AREAS

Given the diversity of Ketchum neighborhoods, we have identified three primary sub-areas to address in our analysis. These include the Gateway Corner of River & Main Street, the directly adjacent neighborhood along River Street, and the multi-family residential developments along the Trail Creek corridor. Scale and style varies greatly in each location, and though none of these will be closely mimicked in the final hotel design, they will all influence the final architecture of our project.



GATEWAY CORNER

FOUR HOTELS ON RIVER & MAIN

The River Street intersection is the primary threshold or 'gateway' into town approaching from the south along Highway 75. Master-planned in a previous 'Gateway Study' are four larger scale commercial hotels at each corner of the intersection. The Best Western / Kentwood lodge is a gable roofed structure on the north corner held back and separated from Main Street with a parking lot and evergreen trees. It's street presence is subtle, and scale fairly humble. The recently built Limelight Hotel on the west corner is a large scale five-story building on a platform / elevated podium. This building holds the public right-of-way on all sides of the block and will compliment the hotel site nicely. A development for a high end boutique hotel has been approved on the east corner of the intersection, and is currently excavated for construction to begin. The proposed scale of this building will be closer in size to the Limelight than the Best Western, and will have similar traffic patterns for drop-off and circulation as our proposed hotel.



SUB-AREA ANALYSIS

NEIGHBORHOOD

RIVER STREET COMMERCIAL | RESIDENTIAL

River Street is a fairly quiet, low-traffic street as you enter town. A balanced mix of commercial and residential projects line the south side of the street and scale and style varies from building to building. Houses appear to be a mix of permanent residences and vacation homes without full-time residents. The scale on the street is two - three story gabled roofs, but as the terrain drops into Trail Creek on the back side, these residences are four stories or greater in height. The commercial structures are all actively leased, though minimal traffic was observed going in and out of each business.

FOREST SERVICE PARK

Forest Service Park is a historical element approximately 1/2 city block from the project site. Primarily public open space with small scale historic wood framed structures and large established evergreen trees. Close walking distance from the project with visual connections across the River Street.



TRAIL CREEK

CREEK OPEN SPACE

Trail Creek is a densely vegetated riparian habitat that runs along the south property line of the project site. This corridor appears to be under-utilized and poorly maintained as it contains dead trees, firewood stacks, and misc. green waste piles through-out. Open space of the river corridor is valuable asset to the experience and success of the hotel, but will most likely require some thinning and trimming for aesthetics. No built structures or site improvements will occur in this area.

CONDO COMPLEXES

Across Trail Creek directly to the south and south-east are a series of condominium complexes that appear to be built in the 1980's - 1990's. Scale varies from two - four stories and they are in relatively good physical condition on the exterior. View corridors to the hotel site are primarily blocked by the Trail Creek tree canopy and over-growth during summer months, but have potential visual connection during winter months. As the hotel steps with the terrain, the scale adjacent to the condo's will be comparable to existing structures.



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NARRATIVE | 01.09

CONTEXTUAL HOTEL COMPONENTS ANALYSIS

CONTEXTUAL HOTEL COMPONENTS ANALYSIS

GENERAL OVERVIEW

Many components to a hotel are important in relating to the community and the surrounding conditions. The hotel needs to fit the fabric of the city, but the amenities offered need to provide something unique to locals and guests each time they walk in the door. Ketchum has a large array of lodging and dining options and experiences from historic lodges, steak-houses, and bars, to modern Italian eateries and high-design boutique hotels.

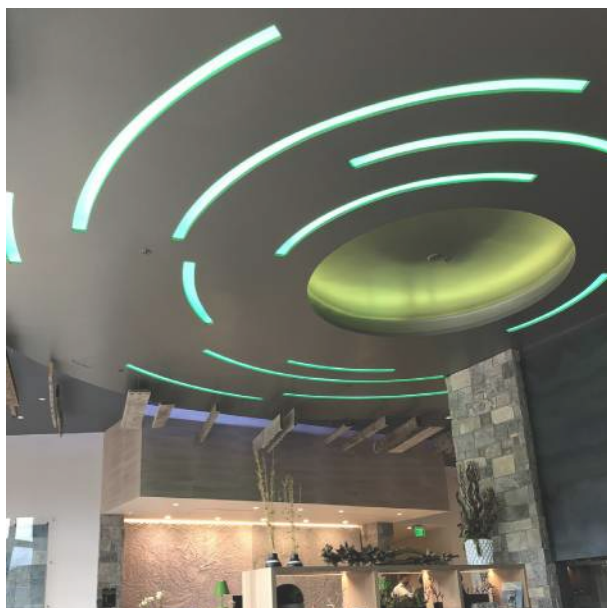
The Ketchum Boutique Hotel is still in its early stages of planning and design, but we have a strong team of architects, interior designers, branding consultants, restaurateurs, and operators aligned to bring the vision to reality. Our intent is to add a well-designed rustic compliment to the entry / gateway of town with an honest exterior material palate, cozy interior public spaces, and a world-class farm-to-table restaurant and bar program that is accessible and affordable to a wide range of guests.

HOTEL LOBBY

The lobby of this hotel will be humble in scale with a warm welcoming layout and direct connection to both the exterior drop-off on River Street, and the restaurant dining room that spills onto the corner of River & Main. In comparison to other recent hotel developments this lobby will be smaller in scale, but still offer all the typical amenities of a four-star hotel property with a check-in desk, on-staff concierge, lounge seating, small business area, and beverages / snack offerings for guests.

RESTAURANT / LOUNGE

The current plan for the main restaurant is to have a cozy, active space with large operable doors to the exterior so that it can open to the patio during comfortable weather months and be as inviting as possible for passers-by. The intent for this location is to help activate the corner of River and Main for pedestrians, and compliment the adjacent hotel properties that share this intersection. While the menu and food plans are still being developed, regional farm-to-table cuisine will be the primary focus with a menu that changes with the seasons and local availability. Simple Western fare to compliment, not compete with the established community restaurants that exist through-out the Sun Valley area.



CONTEXTUAL HOTEL COMPONENTS ANALYSIS

ROOF TOP BAR & GARDENS

One of the very unique public amenities that is being proposed is a roof-top bar with panoramic views from Sun Valley to Baldy. The terracing of the building allows for multiple levels of roof-top experience with mixed seating opportunities and a potential roof-top herb garden to be used in the restaurant and bar during summer months. Exposure to the southern sun will make for a comfortable year-round experience, and be a positive revenue generator and unique location for the City of Ketchum

MEETING SPACE

A flexible meeting space that can be adjusted to accommodate small to medium size groups will be available for bot public and guest use. The current location is on the main level with a small exterior terrace that opens onto the Trail Creek riparian corridor at tree canopy height. A second larger meeting space is proposed as a plan alternate on the lower level, and could potentially offer a unique environment for Ketchum as it would spill onto Trail Creek at ground level, and offer a flexible indoor - outdoor meeting space. See proposed plan alternate in the employee housing exhibits.

FITNESS CENTER

A moderately sized fitness center is currently proposed for the second level. This space would have wall-to-wall, floor-to-ceiling glass, with operable sliding doors that open onto the roof terrace during nice weather. The view corridor from this fitness room is directly towards Baldy, and when the doors are open, could offer various classes (i.e. yoga, pilates, etc.) that spill onto the level 02 guest terrace. These classes would be available for guests and public attendance, and offer an alternate location for locals looking for fitness opportunities with a view

GUEST TERRACE & HOT-TUBS

The second level Guest Terrace will have a south-western exposure, offering a comfortable area for relaxation and isolated views of Baldy and Trail Creek. Two hot-tubs and a warming kitchen will offer guests drinks and appetizers. Currently this area is planned to only be available for hotel guests as essentially all the other hotel amenities are available to the general public.



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NARRATIVE | 01.10

DRAFT DEVELOPMENT AGREEMENT

DRAFT DEVELOPMENT AGREEMENT

RECORDING REQUESTED BY AND WHEN RECORDED RETURN TO:

PEG Ketchum Hotel, LLC
c/o Nick Blayden
180 North University Ave., Suite 200
Provo, Utah 84601

(Above space for recorder's use only)

DEVELOPMENT AGREEMENT

This Development Agreement (“**Agreement**”) is made and entered into as of the ___ day of _____, 20___ (“**Effective Date**”) by and between the CITY OF KETCHUM, IDAHO, a municipal corporation (“**Ketchum**” or “**City**”) and PEG Ketchum Hotel, LLC, a Delaware limited liability company (“**Owner**”), both of whom are collectively referred to herein as the “**Parties**”, and individually as a “**Party**.”

RECITALS

A. Ketchum is a municipal corporation possessing all powers granted to municipalities under the applicable provisions of the Idaho Code, including the power to vacate rights-of-way, to grant variances to building height restrictions, to remove lot lines, grant rights to exceed building floor area ratio limitations, to grant licenses to encroach into the public right-of-way and the power to contract. A development agreement between the Parties is a collaboration that will provide mutual benefit for the Parties, businesses in the Commercial Core District and residents of the City.

B. The Owner has purchased certain real property consisting of approximately 1.1 acres located at approximately 251 South Main Street, City of Ketchum, Blaine County, Idaho, as more particularly described on Exhibit A, attached hereto and incorporated herein (the “**Property**”).

C. Owner seeks to develop and has submitted or will submit for design review approval the construction of improvements on the Property (“**Project**”) consisting of an approximately 135,895 gross square foot wood-framed hotel to be constructed on and over a 64-stall concrete parking structure which will provide substantial public benefits.

D. The Parties agree that the Property shall be developed in accordance with this Agreement; all applicable City ordinances; and any additional conditions and requirements imposed upon the Property by the Ketchum Planning and Zoning Commission (“**Commission**”) and/or City Council (“**Council**”) during the design review, vacation, development agreement, plat amendment, and 4th Street / 1st Avenue encroachment approval processes.

DRAFT DEVELOPMENT AGREEMENT

AGREEMENT

NOW, THEREFORE, in consideration of the mutual covenants, promises, agreements, terms and conditions set forth herein, the Parties agree as hereinafter provided.

1. **Incorporation of Recitals.** The Recitals set forth above are hereby incorporated into and made an integral part of this Agreement.

2. **Incorporation of Related Findings, Agreements, Approvals, Permits and Plans.** The following findings of fact, approvals, permits, plans, and documents are hereby incorporated into and made an integral part of this Agreement by reference as if restated herein in full:

- a. Decision and orders related to the Building Design Review;
- b. Decision and orders related to Owner's application for encroachment;
- c. Design review drawings; and
- d. FAR Exceedance Agreement.

Any material failure to comply with the terms and conditions of any of the above-referenced documents shall constitute a breach of this Agreement.

In the event of any inconsistency between the terms and conditions of this Agreement and the documents listed above, the more restrictive terms and conditions shall govern. Development of the Project shall be vested and governed by policies, procedures, guidelines, ordinances, codes and regulations of the City governing land use in effect as of the date this Agreement is recorded and continue consistent with §17.96.090 of the Ketchum Municipal Code.

Owner may request to be bound by future amendments to the Ketchum Municipal Code (“KMC”), or other regulations, policies or guidelines affecting development, and such request may be approved administratively, by the Commission, and/or by the Ketchum City Council consistent with the KMC.

3. **Right to Develop.** Subject to the requirements of this Agreement and the KMC, the Owner and all future owners of some or all of the Property shall have the right to demolish all or any portion of the existing structures and redevelop, construct, improve and use the Property as a mixed use residential and commercial building as depicted and described in the approved plans incorporated into the Agreement as fully set forth in the recitals, including the Planning and Zoning Commission approved plans dated April __ and May __, 2019 (“Plans”). The improvements on the Property shall be built exclusively as permitted under §17.96 of the KMC relating to design review approval. Any development of any portion of the Property substantially inconsistent with this Agreement or the design review approval for the Project shall constitute a breach of this Agreement by Owner.

Pursuant to KMC §17.96.090, the design review approval on this Project shall be valid

DRAFT DEVELOPMENT AGREEMENT

done within this time as specified in KMC §17.96.090(A)(2). Any extension shall only be as allowed and specified in KMC §17.96.090.

Any application for a building permit submitted by Owner shall substantially comply with the requirements of applicable codes, agreements, approvals, plans, permits and other documents as such requirements exist on the day the building permit application is submitted. A building permit shall be issued within fourteen (14) months from the date of the final decision on the associated Findings of Fact, Conclusions of Law and Decision, and a certificate of occupancy shall be issued for the Project no later than 18 months after the building permit is issued unless the time for completion of the Project is extended by the City Council.

A building permit application that does not substantially comply with the requirements contained in applicable codes, agreements, approvals, plans, permits and other project documents may be rejected by the City within a reasonable time after completing review of the application by providing written notice to Owner describing the non-compliance in detail unless the non-compliance is cured by Owner within thirty (30) days. If a building permit application contains material changes to the above-referenced applicable codes, agreements, approvals, plans, permits and other documents an amendment to this Agreement must be applied for by Owner and considered by the City Council. If such amendment is approved by City Council, all inconsistent terms and conditions of the approvals referenced in Section 2 shall be deemed to have been amended to conform the amendment to this Agreement.

4. **FAR Exceedance Agreement.** The Parties agree Owner may exceed the gross floor area ratio limitations under KMC §17.124.040 and construct improvements on the Property having a floor area ratio up to and including _____, pursuant to that separate FAR Exceedance Agreement, and all conditions thereon, entered into by the Parties, dated _____, 2019, and hereby incorporated by reference. *{City comment: May need revising based on timing of FAR Exceedance Agreement approval.}*

5. **Conditions to Owner's Obligations.** Owner's obligations hereunder are conditioned upon (1) receiving all the referenced approvals from the City and (2) securing financing. If either of those conditions (or part of one) is not satisfied, then this Agreement shall no longer be valid.

6. **Term.** The term of this Agreement shall be _____ from the date of the final decision on the associated Findings of Fact, Conclusions of Law and Decision except such term will not be considered to supersede or amend the standard validity periods as specified in KMC, which may be shorter as to specific approvals and necessary steps (e.g. KMC §17.96.090 specifying that design review approval is only valid for twelve months without additional steps for extension).

7. **Financing.** Owner shall provide and show sufficient evidence to the City Council of full financing and funding for completion of the Project to the satisfaction of the City Council prior to issuance of a building permit for the Project. Owner shall evidence such financing by recording on the Property a deed of trust to secure a construction loan prior to issuance of a building permit and by such other proof of financing reasonably necessary for the satisfaction of

DRAFT DEVELOPMENT AGREEMENT

construction work on the Property until acceptable and approval of such financing evidence by the City.

8. Miscellaneous Provisions.

a) Police Powers. Except as otherwise expressly provided herein, nothing contained herein is intended to limit the police powers of Ketchum or its discretion in review of subsequent applications regarding development of the Property. This Agreement shall not be construed to modify or waive any law, ordinance, rule, or regulation not expressly provided for herein, including, without limitation, applicable building codes, fire codes, Ketchum's Zoning Ordinance, Ketchum's Subdivision Ordinance, and Planned Unit Development requirements for the Property.

b) Amendment. This Agreement may be revised, amended, or canceled in whole or in part, only by means of a written instrument executed by both Parties and as evidenced by amended plats and development plans.

c) Specific Performance. In the event of a breach of this Agreement, in addition to all other remedies at law or in equity, this Agreement shall be enforceable by specific performance by either party hereto. All remedies shall be cumulative.

d) Attorney's Fees. In the event either party hereto is required to retain counsel to enforce a provision of this Agreement, or to recover damages resulting from a breach hereof, the prevailing party shall be entitled to recover from the other party all reasonable attorney's fees incurred, whether or not litigation is actually instituted or concluded.

e) Notices. All notices required or provided for under this Agreement shall be in writing and deemed delivered upon delivery in person or upon mailing by certified mail, return receipt requested, postage prepaid. However, the time period in which a response to such notice must be given shall commence to run from the date of receipt on the return receipt of the notice. Rejection or refusal to accept, or the inability to deliver because of a change of address of which no notice was given shall be deemed to be receipt of the notice.

Notices to City shall be addressed as follows:

City of Ketchum
Post Office Box 2315
Ketchum, ID 83340
Attn: John Gaeddert, Planning Director
Telephone: 208.726-7801
Email: JGaeddert@ketchumidaho.org

DRAFT DEVELOPMENT AGREEMENT

Notices given to Owner shall be addressed as follows:

PEG Ketchum Hotel, LLC
c/o Nick Blayden
180 North University Ave., Ste. 200
Provo, Utah 84601
Telephone: 801-655-1998
Email: nblayden@pegcompanies.com

with a copy to:

Property Enhancement Group, Inc.
c/o Robert Schmidt
180 North University Ave., Ste. 200
Provo, Utah 84601
Telephone: 801-655-1998
Email: rschmidt@pegcompanies.com

A party may change the address to which further notices are to be sent by notice in writing to the other party, and thereafter notices shall be addressed and transmitted to the new address.

f) Reliance by the Parties. This Agreement is intended by Owner to be considered by Ketchum as part of Owner's application for design review approval for the Project and the ancillary applications referenced. Owner acknowledges and intends the City to consider and rely upon this Agreement in its review and consideration of said applications.

g) Relationship of Parties. It is understood that the contractual relationship between City and Owner is such that neither party is the agent, partner, or joint venturer of the other party.

h) Successors and Assigns; Covenant Running With the Land. This Agreement shall inure to the benefit of City and Owner and their respective heirs, successors and assigns. This Agreement, including all covenants, terms, and conditions set forth herein, shall be and is hereby declared a covenant running with the land with regard to the Property or any portion thereof, and is binding on both parties to this Agreement as well as their respective heirs, successors and assigns.

i) Recordation and Release. This Agreement shall be recorded with the Blaine County Recorder. The Parties agree to execute all appropriate documentation to cause the encumbrance of this Agreement to be terminated in the event of termination.

j) No Waiver. In the event that City or Owner, or its successors and assigns, do not strictly comply with any of the obligations and duties set forth herein, thereby causing a default under this Agreement, any forbearance of any kind that may be granted or allowed by Owner,

DRAFT DEVELOPMENT AGREEMENT

manner be deemed or construed as waiving or surrendering any of the conditions or covenants of this Agreement with regard to any subsequent default or breach.

k) Partial Invalidity. In the event any portion of this Agreement, or part hereof, shall be determined by any court of competent jurisdiction to be invalid, void, or otherwise unenforceable, the remaining provisions of this Agreement, or parts hereof, shall remain in full force and effect and shall in no way be affected, impaired or invalidated, it being understood that such remaining provisions shall be construed in a manner most closely approximating the intention of the parties with respect to the invalid, void, or unenforceable provision or part hereof.

l) Entire Agreement. This Agreement constitutes the full and complete agreement and understanding between the parties hereto. Excluding formal conditions placed upon the design review approval, subsequent plat approvals or other matters related to the public process, no representations or warranties made by either party shall be binding unless contained in this Agreement or subsequent written amendments hereto.

m) Exhibits. All exhibits referred to herein are incorporated in this Agreement by reference, whether or not actually attached.

n) Authority. Each of the persons executing this Agreement represents and warrants that he or she has the lawful authority and authorization to execute this Agreement, as well as all deeds, easements, liens and other documents required hereunder, for and on behalf of the entity executing this Agreement.

o) Recitals. The Recitals are incorporated herein and made a part of this Agreement by this reference.

p) Choice of Law. This Agreement shall be governed by and construed in accordance with the laws of the state of Idaho, which shall be the sole jurisdiction and venue for any action which may be brought by either party with respect to this Agreement or the subject matter hereof. Venue shall be in Blaine County, Idaho.

[Remainder of this page intentionally left blank, signature page to follow.]

DRAFT DEVELOPMENT AGREEMENT

EXHIBIT A

Description of the Property

DRAFT DEVELOPMENT AGREEMENT

EXHIBIT B

Description of Developer Improvements

DRAFT DEVELOPMENT AGREEMENT

EXHIBIT C

Site Plan

DRAFT DEVELOPMENT AGREEMENT

EXHIBIT D

Schedule of Development

<u>Milestone</u>	<u>Section Reference</u>	<u>Outside Date</u>
Commence Abatement/Demolition/Construction of the Developer Improvements		
Substantial Completion of Developer Improvements		
Completion of Punch List		

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NARRATIVE | 01.11

STANDARDS OF EVALUATION | EXHIBIT KEY

STANDARDS OF EVALUATION | DESIGN REVIEW

Design Review Standards of Evaluation in All Zoning Districts	
City Code	City Standards and <i>Staff Comments</i>
17.96.060(A)(1) Streets	The applicant shall be responsible for all costs associated with providing a connection from an existing city street to their development.
<i>Comments</i>	<i>The Owner understands that these costs are their responsibility.</i>
17.96.060(A)(2) Streets	All street designs shall be approved by the City Engineer.
<i>Comments</i>	<i>N/A: We do not have any new streets as part of this project.</i>
17.96.060(B)(1)	All projects under 17.96.010(A) that qualify as a “Substantial Improvement” shall install sidewalks as required by the Public Works Department.
<i>Comments</i>	<i>Also please see Exhibit #4 Civil Drawings and Exhibit #5 Landscape Drawings.</i> <i>PEG Development understands that these costs are their responsibility and the improvements are represented in our site plan and landscape plan.</i>
17.96.060 (B)(2)c	Sidewalk width shall conform to the City’s right-of-way standards, however the City Engineer may reduce or increase the sidewalk width and design standard requirements at their discretion.
<i>Comments</i>	<i>Also please see Exhibit #4 Civil Drawings, Exhibit #5 Landscape Drawings, and Exhibit #10 Proposed Improvements in the Public Way.</i> <i>At the River street façade of the hotel we will have a 5’ sidewalk in the right-of-way and then an additional 5’ of sidewalk on our site, which will combine for 10’ of sidewalk width. All other sidewalks will conform with the City’s right-of-way standards.</i>
17.96.060 (B)(3)	Sidewalks may be waived if one of the following criteria is met: <ul style="list-style-type: none"> a. The project comprises an addition of less than 250 square feet of conditioned space. b. The City Engineer finds that sidewalks are not necessary because of existing geographic limitations, pedestrian traffic on the street does not warrant a sidewalk, or if a sidewalk would not be beneficial to the general welfare and safety of the public.
<i>Comments</i>	<i>N/A: We intend to provide sidewalks.</i>
17.96.060 (B)(4)	The length of sidewalk improvements constructed shall be equal to the length of the subject property line(s) adjacent to any public street or private street.
<i>Comments</i>	<i>Also please see Exhibit #4 Civil Drawings and Exhibit #5 Landscape Drawings.</i> <i>We intend to provide sidewalks.</i>
17.96.060 (B)(5)	New sidewalks shall be planned to provide pedestrian connections to any existing or future sidewalks adjacent to the site. In addition, sidewalks shall be constructed to provide safe pedestrian access to and around a building.
<i>Comments</i>	<i>Also please see Exhibit #4 Civil Drawings and Exhibit #5 Landscape Drawings.</i> <i>We have connected the new construction sidewalks into the existing sidewalks. We are working with the Idaho Transportation Department to try and determine what the alignment of the re-worked intersection of Main Street and River Road and we have requested additional meetings with them and Ketchum City to coordinate on that alignment.</i>
17.96.060 (B)(6)	The City may approve and accept voluntary cash contributions in-lieu of the above described improvements, which contributions must be segregated by the City and not used for any purpose other than the provision of these improvements. The contribution amount shall be one hundred ten percent (110%) of the estimated costs of concrete sidewalk and drainage improvements provided by a qualified contractor, plus associated engineering costs, as approved by the City Engineer. Any approved in-lieu contribution shall be paid before the City issues a certificate of occupancy.

STANDARDS OF EVALUATION | DESIGN REVIEW

<i>Comments</i>	N/A.
17.96.060(C)(1)	All storm water shall be retained on site.
	<i>Also please see Exhibit #4 Civil Drawings.</i> <i>Our storm water will be retained on site.</i>
17.96.060(C)(2)	Drainage improvements constructed shall be equal to the length of the subject property lines adjacent to any public street or private street.
<i>Comments</i>	<i>Also please see Exhibit #4 Civil Drawings and Exhibit #5 Landscape Drawings.</i> <i>We will comply with this requirement for drainage improvements.</i>
17.96.060(C)(3)	The City Engineer may require additional drainage improvements as necessary, depending on the unique characteristics of a site.
<i>Comments</i>	<i>We will work with the City Engineer as part of our Design Review Application process.</i>
17.96.060(C)(4)	Drainage facilities shall be constructed per City standards.
<i>Comments</i>	<i>Also please see Exhibit #4 Civil Drawings.</i> <i>We will meet the City standards in the design of Drainage Facilities.</i>
17.96.060(D)(1)	All utilities necessary for the development shall be improved and installed at the sole expense of the applicant.
<i>Comments</i>	<i>PEG Development understands that these costs are their responsibility.</i>
17.96.060(D)(2)	Utilities shall be located underground and utility, power, and communication lines within the development site shall be concealed from public view.
<i>Comments</i>	<i>Also please see Exhibit #4 Civil Drawings.</i> <i>New utilities will be concealed from public view.</i>
17.96.060(D)(3)	When extension of utilities is necessary all developers will be required to pay for and install two (2") inch SDR11 fiber optical conduit. The placement and construction of the fiber optical conduit shall be done in accordance with city of Ketchum standards and at the discretion of the City Engineer.
<i>Comments</i>	<i>We will work with the City Engineer as part of our Design Review Application process to determine if this is necessary for our site.</i>
17.96.060(E)(1)	The project's materials, colors and signing shall be complementary with the townscape, surrounding neighborhoods and adjoining structures.
<i>Comments</i>	<i>Also please see Exhibit #1 Architectural Drawings and Exhibit #4 Exterior Color Palette.</i> <i>Our materials and color palette will be complimentary of and compatible with the townscape and our neighbors.</i>
17.96.060(E)(2)	Preservation of significant landmarks shall be encouraged and protected, where applicable. A significant landmark is one which gives historical and/or cultural importance to the neighborhood and/or community.
<i>Comments</i>	<i>N/A: We do not have significant landmarks on our site.</i>
17.96.060(E)(3)	Additions to existing buildings, built prior to 1940, shall be complementary in design and use similar material and finishes of the building being added to.
<i>Comments</i>	<i>N/A: This project does not feature additions to existing buildings.</i>
17.96.060(F)(1)	Building(s) shall provide unobstructed pedestrian access to the nearest sidewalk and the entryway shall be clearly defined.
<i>Comments</i>	<i>Also please see Exhibit #1 Architectural Drawings and Exhibit #4 Civil Drawings.</i>

STANDARDS OF EVALUATION | DESIGN REVIEW

	<i>The project provides unobstructed pedestrian access to the nearest sidewalk. The public entryway to the hotel is clearly defined.</i>
17.96.060(F)(2)	The building character shall be clearly defined by use of architectural features.
<i>Comments</i>	<i>Also please see Exhibit #1 Architectural Drawings.</i> <i>We have been actively working with the Ketchum planning staff to refine the massing and character of the hotel. The proposed hotel is congruous with its surrounding commercial neighbors. The hotel features many outdoor living opportunities including many guest terraces and balconies, public terraces, the South West facing amenity courtyard on level 2, and in the rooftop terrace that surrounds the rooftop bar. These outdoor spaces are designed to blend the line between interior and outdoor spaces for the hotel guests and visiting public patrons.</i>
17.96.060(F)(3)	There shall be continuity of materials, colors and signing within the project.
<i>Comments</i>	<i>Also please see Exhibit #1 Architectural Drawings and Exhibit #4 Exterior Color Palette.</i> <i>Our materials and color palette will be complimentary of and compatible with the townscape and our neighbors. The exterior signage will be consistent throughout the project with the branding of the hotel.</i>
17.96.060(F)(4)	Accessory structures, fences, walls and landscape features within the project shall match or complement the principal building.
<i>Comments</i>	<i>Currently no accessory structures are planned. As the design is further developed if accessory structures develop they will be compliment the architecture of the hotel. Site fencing will be provided around the outdoor dining elements that will be detailed consistently with the hotel.</i>
17.96.060(F)(5)	Building walls shall provide undulation/relief, thus reducing the appearance of bulk and flatness.
<i>Comments</i>	<i>Also please see Exhibit #1 Architectural Drawings.</i> <i>The massing has been carefully coordinated to honor its hillside location by terracing down the hillside from River Street, those terraces then become gathering spots for guests and the public to enjoy the outdoor climate and take in the amazing vistas that surround the hotel. The massing also provides for a façade that steps in and out of plane, which is further enhanced by a layer balconies and further articulation of those forms. The building pulls back over 35' from Trail Creek and has minimal visual impact on Forest Service Park. Massing near lot line is contained along River Street where it has an appropriate relationship to the sidewalk at that downtown street and then pulled back from lot line against Trail Creek where we have minimized the impact of the new building against a natural feature.</i>
17.96.060(F)(6)	Building(s) shall orient towards their primary street frontage.
<i>Comments</i>	<i>Also please see Exhibit #1 Architectural Drawings.</i> <i>The main entry of the hotel is on River Street which is our primary street frontage.</i>
17.96.060(F)(7)	Garbage storage areas and satellite receivers shall be screened from public view and located off alleys.
<i>Comments</i>	<i>Also please see Exhibit #1 Architectural Drawings.</i> <i>The garbage storage is contained in an interior trash room that will only be visible during pick-up.</i>
17.96.060(F)(8)	Building design shall include weather protection which prevents water to drip or snow to slide on areas where pedestrians gather and circulate or onto adjacent properties.
<i>Comments</i>	<i>Also please see Exhibit #1 Architectural Drawings.</i> <i>The hotel generally has flat roofs which will contain any significant snow, we do not have sloped architectural elements that shed snow onto neighboring properties. If the City has any concerns</i>

STANDARDS OF EVALUATION | DESIGN REVIEW

	<i>over any specific architectural feature being a snow nuisance we will work through a solution to mitigate that with the City.</i>
17.96.060(G)(1)	Pedestrian, equestrian and bicycle access shall be located to connect with existing and anticipated easements and pathways.
<i>Comments</i>	<i>Also please see Exhibit #1 Architectural Drawings, Exhibit #4 Civil Plans, and Exhibit #5 Landscape Plans. The sidewalks around the site connect to the adjacent sidewalks.</i>
17.96.060(G)(2)	Awnings extending over public sidewalks shall extend five (5') feet or more across the public sidewalk but shall not extend within two (2') feet of parking or travel lanes within the right of way.
<i>Comments</i>	<i>Also please see Exhibit #1 Architectural Drawings. The sidewalk at the main entry on River Street will be protected by a roof awning element. This architectural feature will be lifted to allow for highway compliant clearance beneath it but will partially extend over the travel lanes and right of way.</i>
17.96.060(G)(3)	Traffic shall flow safely within the project and onto adjacent streets. Traffic includes vehicle, bicycle, pedestrian and equestrian use. Consideration shall be given to adequate sight distances and proper signage.
<i>Comments</i>	<i>Please see Narrative #5: Traffic Study & Parking Analysis, and Exhibit #8.</i>
17.96.060(G)(4)	Curb cuts and driveway entrances shall be no closer than twenty (20') feet to the nearest intersection of two or more streets, as measured along the property line adjacent to the right of way. Due to site conditions or current/projected traffic levels or speed, the City Engineer may increase the minimum distance requirements.
<i>Comments</i>	<i>Please see Exhibit #4 Civil Drawings.</i>
17.96.060(G)(5)	Unobstructed access shall be provided for emergency vehicles, snowplows, garbage trucks and similar service vehicles to all necessary locations within the proposed project.
<i>Comments</i>	<i>Please see Exhibit #4 Civil Drawings.</i>
17.96.060(H)(1)	Snow storage areas shall not be less than thirty percent (30%) of the improved parking and pedestrian circulation areas.
<i>Comments</i>	<i>Also please see Exhibit #4 Civil Drawings. We do not have any site parking lots, the porte cochere and front entry sidewalk will feature snow melt, the snow storage from the remaining surfaces is intended to be accommodated in the site landscaping.</i>
17.96.060(H)(2)	Snow storage areas shall be provided on-site.
<i>Comments</i>	<i>We will comply with this requirement.</i>
17.96.060(H)(3)	A designated snow storage area shall not have any dimension less than five (5') feet and shall be a minimum of twenty-five (25) square feet.
<i>Comments</i>	<i>Also please see Exhibit #4 Civil Drawings. We do not have any site parking lots, the porte cochere and front entry sidewalk will feature snow melt, the snow storage from the remaining surfaces is intended to be accommodated in the site landscaping and we do not plan to require a designated snow area.</i>
17.96.060(H)(4)	In lieu of providing snow storage areas, snow melt and hauling of snow may be allowed.
<i>Comments</i>	<i>This will be our path to compliance.</i>
17.96.060(I)(1)	Landscaping is required for all projects.
<i>Comments</i>	<i>See Exhibit #5 Landscape Drawings.</i>

STANDARDS OF EVALUATION | DESIGN REVIEW

17.96.060(I)(2)	Landscape materials and vegetation types specified shall be readily adaptable to a site's microclimate, soil conditions, orientation and aspect, and shall serve to enhance and complement the neighborhood and townscape.
<i>Comments</i>	<i>See Exhibit #5 Landscape Drawings.</i>
17.96.060(I)(3)	All trees, shrubs, grasses and perennials shall be drought tolerant. Native species are recommended but not required.
<i>Comments</i>	<i>See Exhibit #5 Landscape Drawings.</i>
17.96.060(I)(4)	Landscaping shall provide a substantial buffer between land uses, including, but not limited to, structures, streets and parking lots. The development of landscaped public courtyards, including trees and shrubs where appropriate, shall be encouraged.
<i>Comments</i>	<i>See Exhibit #5 Landscape Drawings.</i>
17.96.060(J)(1)	Where sidewalks are required, pedestrian amenities shall be installed. Amenities may include, but are not limited to, benches and other seating, kiosks, bus shelters, trash receptacles, restrooms, fountains, art, etc. All public amenities shall receive approval from the Public Works Department prior to design review approval from the Commission.
<i>Comments</i>	<i>Also please see Exhibit #4 Civil Drawings and Exhibit #5 Landscape Drawings.</i> <i>We will accommodate these amenities as the design develops further.</i>

STANDARDS OF EVALUATION | CONDITIONAL USE PERMIT

Conditional Use Permit Standards of Evaluation	
EVALUATION STANDARDS: 17.116.030 and § 67-6512 of Idaho Code A conditional use permit shall be granted by the commission only if the applicant demonstrates the following:	
Code Section	City Standards and Applicant Comments
17.116.030(A)	The characteristics of the conditional use will not be unreasonably incompatible with the types of uses permitted in the applicable zoning district.
<i>Comment</i>	<i>Also please see Narratives #2 Analysis / Study, #7 Sub Area Analysis, #8 Key Components of Hotel in Context</i> <i>The hotel use is allowed in the (T) Tourist District zoning and is compatible with the surrounding commercial and multifamily residential uses.</i>
17.116.030(B)	The conditional use will not materially endanger the health, safety and welfare of the community.
<i>Comment</i>	<i>N/A.</i>
17.116.030(C)	The conditional use is such that pedestrian and vehicular traffic associated with the use will not be hazardous or conflict with existing and anticipated traffic in the neighborhood.
<i>Comment</i>	<i>Please see Narrative #5: Traffic Study & Parking Analysis, and Exhibit #8.</i>
17.116.030(D)	The conditional use will be supported by adequate public facilities or services and will not adversely affect public services to the surrounding area or conditions can be established to mitigate adverse impacts.
<i>Comment</i>	<i>We have been coordinating with officials with Ketchum City and this does not appear to be a concern with this project.</i>
17.116.030(E)	The conditional use is not in conflict with the policies of the Comprehensive Plan or the basic purposes of this Section.
<i>Comment</i>	<i>Please see Narrative #2: Analysis / Study which contains a thorough assessment of the Ketchum Boutique Hotel's compliance with the Ketchum 2014 Comprehensive Plan.</i>

STANDARDS OF EVALUATION | DEVELOPMENT AGREEMENT

Development Agreement Zoning Standards of Evaluation	
<p>17.124.050.B.5: Zoning Development Agreement: Hotels shall enter into a zoning development agreement with the city as part of the approval process. Said zoning development agreement may address the following subjects: community housing, hotel room uses and restrictions, public access on the property, alternatives and remedies if the hotel use ceases, and any other issues the commission or city council deems appropriate. Said zoning development agreement shall follow the public hearing process as outlined in title 16, chapter 16.08, "Planned Unit Developments", of this code. Said zoning development agreement shall be subject to sections 17.154.050, "Enforcement", and 17.154.060, "Modification And Termination", of this title.</p> <p>17.154.010.A Traditional zoning regulates the use of land and structures by assigning each use to one or more districts. However, the functions and characteristics of an increasing number of new kinds of land uses, combined with conclusive experience regarding some of the older, familiar kinds of uses, call for a more flexible and equitable procedure for properly accommodating these uses within the community, while protecting the health, safety and general welfare of the city of Ketchum's residents. Rather than assigning all uses to special individual and limited zoning districts, the city's goals can be met by providing controllable and reasonable flexibility in requirements for certain kinds of uses. This allows practicable latitude for the owner and maintains adequate provisions for the security of the health, safety and general welfare of the community's inhabitants.</p>	
Code Section	City Standards and Applicant Comments
17.154.030.A	<p>Request For A Zoning Development Agreement:</p> <ol style="list-style-type: none"> 1. A request to enter into a zoning development agreement for the subject property may be submitted by the applicant or required by the commission, the administrator, or the council at any point during the processing of the rezoning request. 2. In the event of a determination by the council that a zoning development agreement should be entered into, the council shall remand the matter back to the commission for a public hearing as specified in subsection E of this section. The council shall, upon remand, direct the commission on the specific issues to be addressed by the zoning development agreement.
<i>Comment</i>	<i>Please see Narrative #9.</i>
17.154.030.B	<p>Filing Requirements: A request for a zoning development agreement must be determined to be complete by the city prior to processing. The city shall determine a zoning development agreement request complete when the information described below is included with a complete zone change application:</p> <ol style="list-style-type: none"> 1. The application form shall include at a minimum: <ol style="list-style-type: none"> a. Name, address and telephone number of the property owner, any coapplicants as well as any representatives for the property owner and/or coapplicants. b. Original signatures for the property owner and all coapplicants. If the property owner or coapplicant will be represented by another, the original signature authorizing the representative to represent the property owner and/or coapplicant shall also be submitted. c. A complete legal description of the subject property. d. Title report. e. Application fee. 2. Four (4) copies plus one reduced to at least eleven inches by seventeen inches (11" x 17") of the architectural plan of the proposed construction in sufficient detail to show the following: <ol style="list-style-type: none"> a. Floor plan (not less than 1/8 inch scale). b. All exterior elevations.

STANDARDS OF EVALUATION | DEVELOPMENT AGREEMENT

	<ul style="list-style-type: none"> c. Section through the highest point of the building indicating existing, natural and proposed grade, with dimensions. If the subject property is located in the CC community core zone, an analysis of the height invisible plane shall also be submitted. d. Type and color of exterior materials and roofing. e. Location and type of exterior lighting. f. Existing structures and land uses on and adjacent to the subject property. g. Adjacent roadways, proposed roadways, ingress and egress from said roadways, parking and pedestrian circulation and access. h. Property lines with dimensions, adjacent land uses, structures and zoning. i. Topography at one foot (1') intervals or spot elevations. j. Scale, north arrow, and legend. k. Existing watercourses, utility lines, easements, deed restrictions and other built or natural features restricting the use of the subject property. l. Existing vegetation, labeled as to remain or be removed. m. Conceptual landscape plan that includes plant location, general species type and quantity. <p>3. A draft zoning development agreement as specified in subsection D of this section.</p> <p>4. A written description of the proposed development, including the uses, and how it integrates and complements adjacent land uses.</p> <p>5. A written narrative demonstrating compliance with the goals and policies of the Ketchum comprehensive plan.</p> <p>6. A traffic analysis that includes adjacent roadways, proposed roadways, ingress and egress from said roadways, parking, pedestrian circulation and impacts to nonmotorized and transit facilities.</p> <p>7. Signed and notarized statement by the applicant indicating that failure to comply with all commitments in the approved zoning development agreement shall be deemed consent to revert the zoning of the property to the preexisting zone, or in the case of an initial zone at annexation, a zone deemed appropriate by the council.</p> <p>8. Phasing plan and proposed phasing schedule.</p> <p>9. Additional information as reasonably required at the discretion of the administrator, commission or council prior to or during the review process.</p> <p>10. The materials required in this subsection may be waived by the administrator after administrative review of the application should no need be found therefor.</p>
<p>Comment</p>	<p><i>All required elements are contained in the Narratives and Exhibits of the combined Ketchum Boutique Hotel submittal.</i></p>
<p>17.154.030.D</p>	<p>Attorney Review: The draft zoning development agreement shall be reviewed by the city attorney as to form prior to commission review and shall contain, at a minimum, the following:</p> <ul style="list-style-type: none"> 1. List of use(s) to be allowed. 2. Permitted square footage and building locations. 3. Identification of development standards that shall be required under the agreement. 4. Identification of locations for permitted uses on approved conceptual plan. 5. Planned implementation of improvements with a construction and completion schedule. 6. A provision that the standards and processes of the design review overlay district shall apply to the development of the property. 7. A provision for the use of a security agreement for project completion if the proposal is developed in phases. 8. A provision that the property owner/developer acknowledges and agrees that failure to comply with the terms of the agreement shall result in a reversion of the zoning of the real

STANDARDS OF EVALUATION | DEVELOPMENT AGREEMENT

	<p>property to the zoning existing immediately prior to the agreement, pursuant to the procedure set forth in Idaho Code section 67-6511A.</p> <p>9. A provision specifying that unless modified or terminated by the governing board, pursuant to section 17.154.060 of this chapter, the commitment and all conditions, terms, duties and obligations included in said commitment are binding on the owner of the property, each subsequent property owner and every person(s) acquiring interest in said property.</p> <p>10. Other conditions attached to the project through the public hearing process.</p> <p>11. The administrator may waive subsections D2, D4 and D5 of this section after administrative review of the application should no need be found therefor.</p>
<i>Comment</i>	<i>Please see Narrative #9 which is available for review.</i>
17.154.040.B	<p>Comprehensive Plan Compliance: The proposal is in conformance with and promotes the purposes and goals of the comprehensive plan, this title and other applicable ordinances of the city, and not in conflict with the public interest.</p> <p>1. There will be no significant adverse effect(s) resulting from the proposed zone change and use(s) authorized upon the public health, safety and general welfare of the neighborhood or the community as a whole.</p> <p>2. Includes community or employee housing, as defined in section 16.08.030 of this code, for rezones requesting a higher density zone. Payment in lieu may be accepted for fractions of units as determined by the council.</p>
<i>Comment</i>	<i>Please see Narrative #2: Analysis / Study which contains a thorough assessment of the Ketchum Boutique Hotel's compliance with the Ketchum 2014 Comprehensive Plan. Employee housing is included (30 beds which is over 25% of the keys in the hotel), see Exhibit #6 Employee Housing Plan.</i>
17.154.040.C	<p>Compliance With City Codes:</p> <p>1. All design review standards in chapter 17.96 of this code shall be carefully analyzed and considered for all proposals. This includes detailed analysis of building bulk, undulation and other design elements. The site plan should be sensitive to the architecture and scale of the surrounding neighborhood.</p> <p>2. A project encompassing three (3) or more acres or otherwise meeting the requirements under subsection 16.08.080A of this code is required to submit a planned unit development conditional use permit pursuant to chapter 16.08 of this code.</p> <p>3. The project shall be in conformance with this title; title 16, "Subdivision Regulations", of this code; title 12, "Streets, Sidewalks, Public Utility Easements And Public Places", of this code, and all other applicable city ordinances and regulations.</p>
<i>Comment</i>	<p><i>Also please see Narrative #2: Analysis / Study which contains a thorough assessment of the Ketchum Boutique Hotel's compliance with the Ketchum 2014 Comprehensive Plan, Exhibit #1 Architectural Drawings, and Exhibit #5 Landscape Plans.</i></p> <p><i>The massing has been carefully coordinated to honor its hillside location by terracing down the hillside from River Street, those terraces then become gathering spots for guests and the public to enjoy the outdoor climate and take in the amazing vistas that surround the hotel. The massing also provides for a façade that steps in and out of plane, which is further enhanced by a layer balconies and further articulation of those forms. The building pulls back over 35' from Trail Creek and has minimal visual impact on Forest Service Park. Massing near lot line is contained along River Street where it has an appropriate relationship to the sidewalk at that</i></p>

STANDARDS OF EVALUATION | DEVELOPMENT AGREEMENT

	<p><i>downtown street and then pulled back from lot line against Trail Creek where we have minimized the impact of the new building against a natural feature.</i></p> <p><i>Site landscape design has been designed to complement the hillside and creek bank features of the site.</i></p> <p><i>As a hotel we are processing a PUD submittal concurrently with this submittal. We will be compliant with the "Subdivision Regulations" of the code.</i></p>
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STANDARDS OF EVALUATION | HOTELS

Hotels Standards of Evaluation	
EVALUATION STANDARDS: 17.124.050	
Code Section	City Standards and Applicant Comments
17.124.050.A	Hotels may exceed the maximum floor area, height or minimum open site area requirements of this title subject to the following review process: <ol style="list-style-type: none"> 1. Planned Unit Development: A planned unit development shall be prepared for the proposal and approved by the city which specifically outlines the waivers to bulk regulations requested. 2. Subarea Analysis: A subarea analysis shall be prepared which addresses the comprehensive plan designation for the subarea; impacts of the proposed hotel on the character and scale of the surrounding neighborhood; impacts on proposed height and mass relative to the city's design review standards and the PUD standards; and the appropriateness of the subarea for a hotel which exceeds the dimensional standards requirements of sections 17.12.030 and 17.12.040 of this title.
<i>Comment</i>	<i>Also please see Exhibit #1 Architectural Drawings. See Narrative #7 Sub Area Analysis. We are submitting a Planned Unit Development submittal to Ketchum City.</i>
17.124.050.B.1	In addition to all other hotel requirements of this title, the following standards apply to hotels in the T and CC districts: <ol style="list-style-type: none"> 1. Hotel Developments: For hotel developments, community housing calculations apply to all residential units. However, one hundred percent (100%) of the community housing requirement will be waived only for the residential portion of hotel projects that meet the hotel definition adopted by the Ketchum city council.
<i>Comment</i>	<i>The Ketchum Gateway Hotel is located in the (T) Tourist District and is compatible with this exception.</i>
17.124.050.B.2	Employee Housing: Hotel developments are required to mitigate employee housing impacts at a ratio of twenty five percent (25%) of the total number of employees calculated by the following formula: one employee per hotel room or bedroom.
<i>Comment</i>	<i>We currently have (30) employee housing beds, in a mix of apartment and dorm style units, at 100 hotel keys we are required to provide (25) beds.</i>
17.124.050.B.3	Employee Housing Plan: The applicant shall provide an employee housing plan which outlines the number of employees, income categories and other pertinent data. The employee housing plan shall be the basis of the applicant's proposal for a mix of employee housing which addresses the range of employees needed to serve the hotel.
<i>Comment</i>	<i>Please see Narrative #6.</i>
17.124.050.B.4	Alternate Means For Housing: The city council may consider a request by the hotel developer to satisfy any required employee or community housing square footage by alternate means. Off site mitigation, payment of in lieu fees, land in lieu of units, voluntary real estate transfer fees or other considerations may be proposed by the hotel developer. Larger sites are encouraged to include workforce housing on site. The city council has full discretionary power to deny said request.
<i>Comment</i>	<i>N/A: We will comply with the Employee Hosing Requirements on site.</i>

STANDARDS OF EVALUATION | HOTELS

17.124.050.B.5	<p>Zoning Development Agreement: Hotels shall enter into a zoning development agreement with the city as part of the approval process. Said zoning development agreement may address the following subjects: community housing, hotel room uses and restrictions, public access on the property, alternatives and remedies if the hotel use ceases, and any other issues the commission or city council deems appropriate. Said zoning development agreement shall follow the public hearing process as outlined in title 16, chapter 16.08, "Planned Unit Developments", of this code. Said zoning development agreement shall be subject to sections 17.154.050, "Enforcement", and 17.154.060, "Modification And Termination", of this title.</p>
<i>Comment</i>	<i>Please see Narrative #9 Development Agreement.</i>
17.124.050.B.6	<p>Number Of Floors: Hotels may build a fourth floor. If a site meets the criteria for five-story hotel site designation, a fifth floor may be built. Five-story hotels may only be approved via a planned unit development (PUD) as outlined in title 16, chapter 16.08 of this code.</p> <p style="padding-left: 40px;">a. Five-Story Hotel Site Designation Criteria: A property shall meet all of the following criteria to be designated as a five-story hotel site:</p> <p style="padding-left: 80px;">(1) Is located in subdistrict A, retail core of the community core district.</p> <p style="padding-left: 80px;">(2) Has a minimum lot area of thirty three thousand (33,000) square feet.</p>
<i>Comment</i>	<p><i>Also please see Exhibit #1 Architectural Drawings.</i></p> <p><i>The Ketchum Boutique Hotel has (4) stories from River Street and (4) stories from the south end of the site near Trail Creek, as the massing terraces down the hillside those stories intersect with one another following the slope of the terrain.</i></p>

Attachment B (continued)

PEG Ketchum Hotel, LLC Gateway Site Submittal Drawings, Narratives & Studies

2. Exhibit Maps (as Presented to PZ thru 8/12/19)

- Plans | Elevations | Sections
- Sun Studies
- Height Analysis
- Civil Drawings
- Landscape Drawings
- Exterior Color Palette
- Dark Sky Compliant Fixtures
- Traffic Study Diagrams
- Plat Map | Lot Line Adjustment
- Public Way Improvements
- Soils Report | Perc Test
- Flood Plain | Riparian Corridor
- List of Adjacent Property Owners
- Waiver List
- Galena Staking Exhibit
- AJC Architecture Virtual Reality Images
- AJC Architecture July 29th and 30th PowerPoint Presentations
- Hales Engineering July 29th PowerPoint Presentation

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02

EXHIBITS

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EXHIBIT | 02.01

PLANS | ELEVATIONS | SECTIONS

ajc architects
 703 east 1700 south
 salt lake city, ut 84105
 ajcarchitects.com

ARCHITECT / CONSULTANT

NOT FOR
 CONSTRUCTION

AUTHORITY HAVING JURISDICTION

PROJECT DESCRIPTION
 KETCHUM BOUTIQUE
 HOTEL

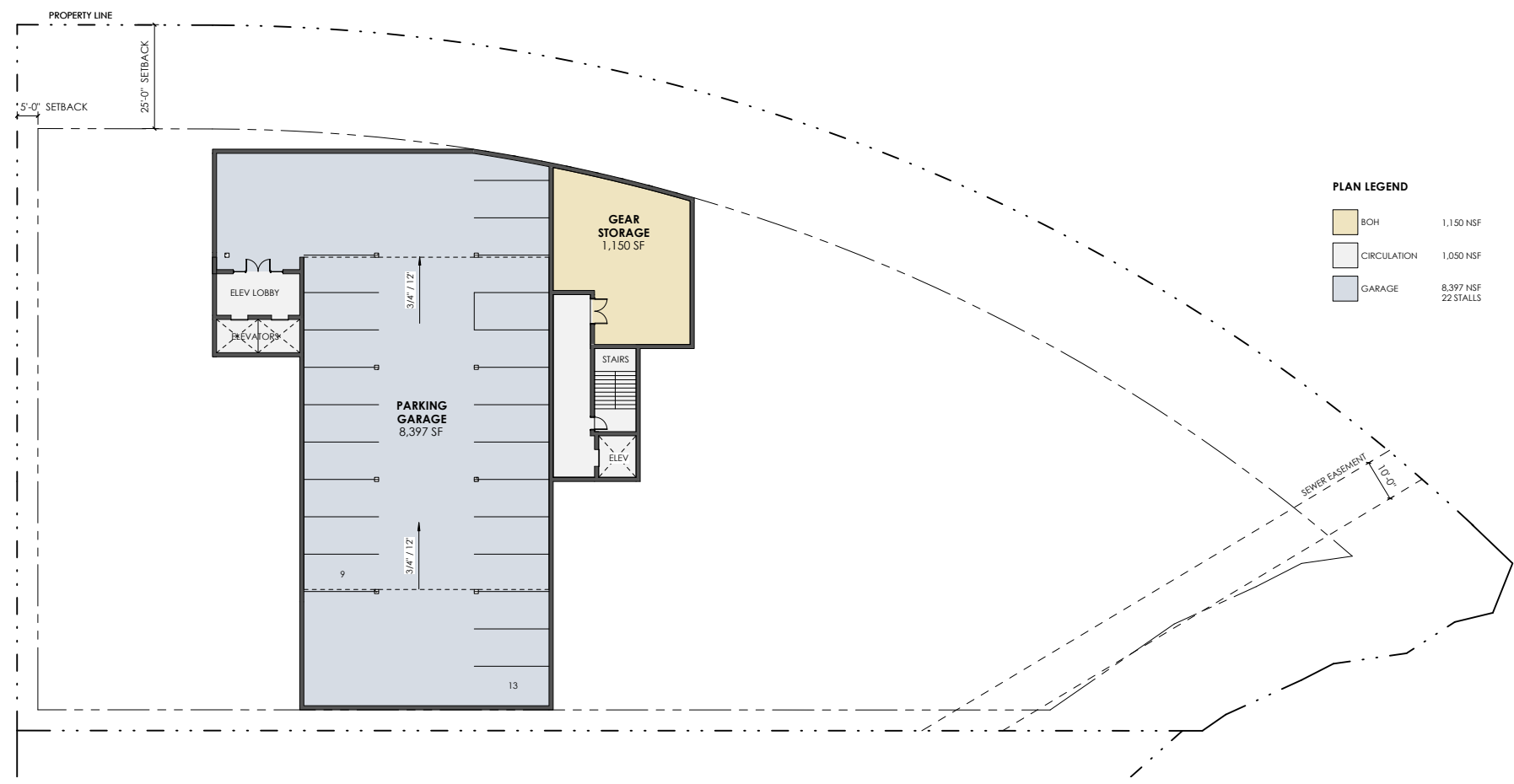
River & Main
 Ketchum, ID 83353

SHEET NAME:
 PRESENTATION PLAN -
 LEVEL -03

REVISIONS
 MARK DATE DESCRIPTION

ISSUE DATE: 06/18/19
 ISSUE TYPE: CONCEPT
 DRAWN BY: JH | RM | DA
 CHECKED BY: K. RIGBY
 PROJECT#: 1912.01

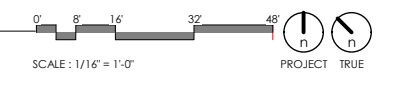
SHEET NUMBER:
PR 000



PLAN LEGEND

BOH	1,150 NSF
CIRCULATION	1,050 NSF
GARAGE	8,397 NSF 22 STALLS

A2 PRESENTATION PLAN - LEVEL -03
 1/16" = 1'-0"



6/18/2019 3:09:55 PM C:\Users\mcmullen\Documents\1912 - Ketchum Hotel\KetchumHotel\ajcarchitects.com.rvt
 THE USER HAS RECORDED THIS SHEET AS A CONCEPTUAL DESIGN. THIS SHEET IS NOT TO BE USED FOR CONSTRUCTION. ANY CONSTRUCTION SHALL BE BASED ON THE FINAL CONSTRUCTION DOCUMENTS. THE USER HAS RECORDED THIS SHEET AS A CONCEPTUAL DESIGN. THIS SHEET IS NOT TO BE USED FOR CONSTRUCTION. ANY CONSTRUCTION SHALL BE BASED ON THE FINAL CONSTRUCTION DOCUMENTS. THE USER HAS RECORDED THIS SHEET AS A CONCEPTUAL DESIGN. THIS SHEET IS NOT TO BE USED FOR CONSTRUCTION. ANY CONSTRUCTION SHALL BE BASED ON THE FINAL CONSTRUCTION DOCUMENTS.

**NOT FOR
 CONSTRUCTION**

AUTHORITY HAVING JURISDICTION

PROJECT DESCRIPTION
**KETCHUM BOUTIQUE
 HOTEL**

River & Main
 Ketchum, ID 83353

SHEET NAME:
**PRESENTATION PLAN -
 LEVEL -02**

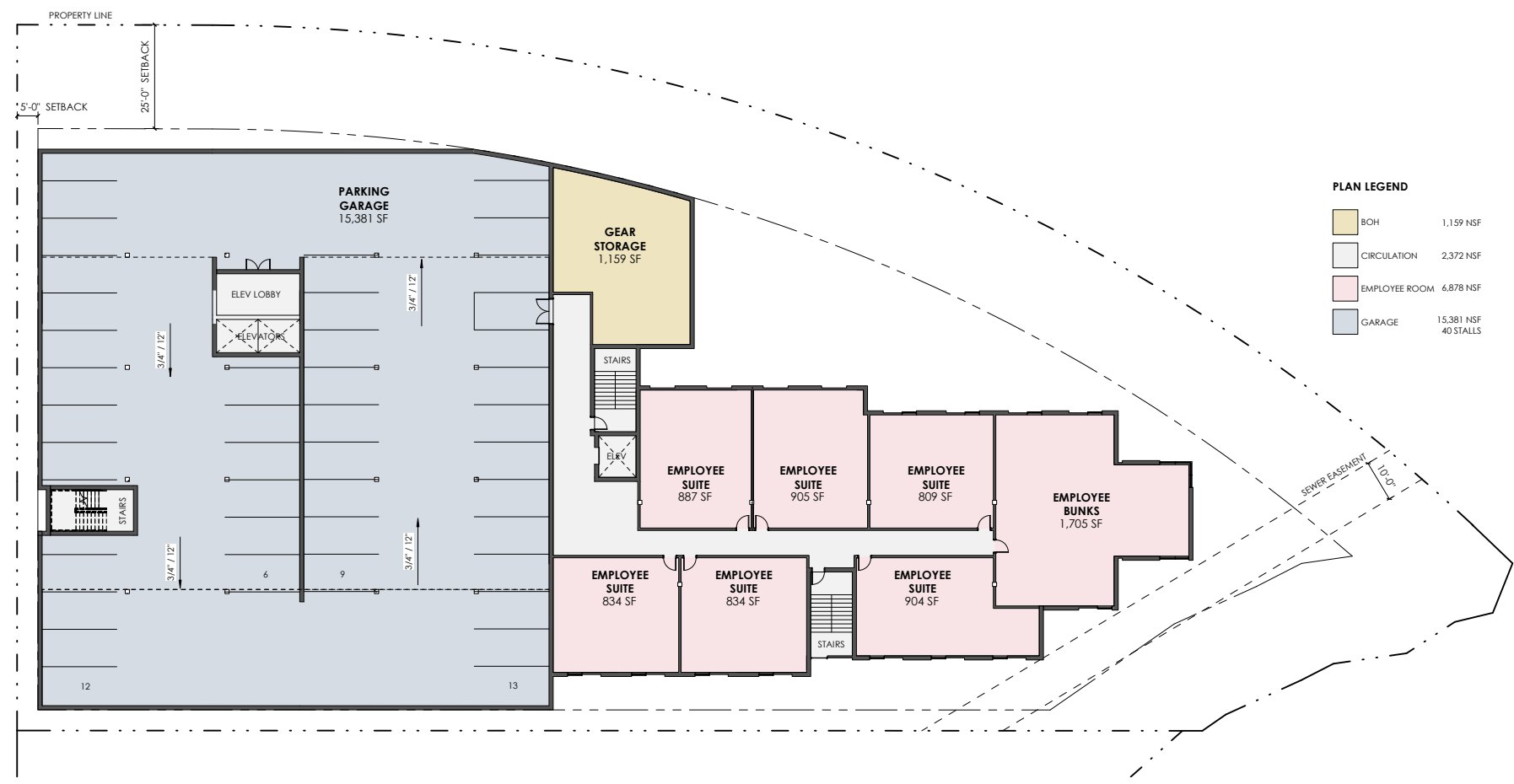
REVISIONS

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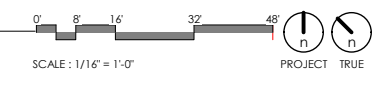
ISSUE DATE: 06/18/19
 ISSUE TYPE: CONCEPT
 DRAWN BY: JH | RM | DA
 CHECKED BY: K. RIGBY
 PROJECT#: 1912.01

SHEET NUMBER:

PR 00



A2 PRESENTATION PLAN - LEVEL -02
 1/16" = 1'-0"



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PROJECT DESCRIPTION
**KETCHUM BOUTIQUE
 HOTEL**

River & Main
 Ketchum, ID 83353

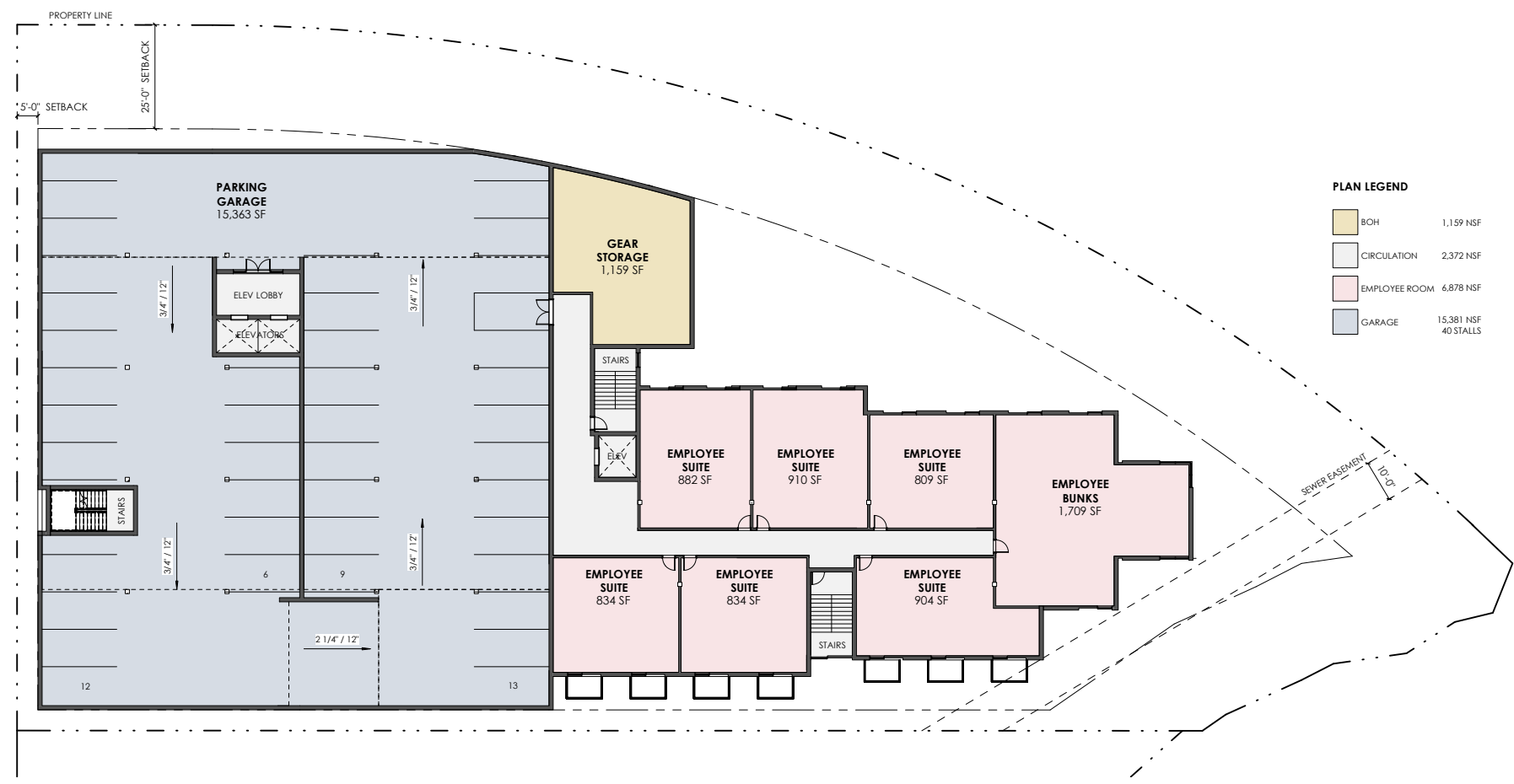
SHEET NAME:
**PRESENTATION PLAN -
 LEVEL -01**

REVISIONS

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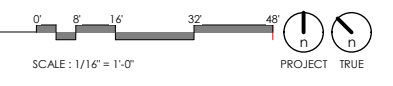
SHEET NUMBER:
PR 0



PLAN LEGEND

BOH	1,159 NSF
CIRCULATION	2,372 NSF
EMPLOYEE ROOM	6,878 NSF
GARAGE	15,381 NSF 40 STALLS

A2 PRESENTATION PLAN - LEVEL -01
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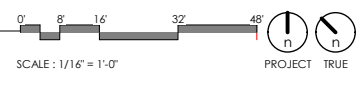
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PLAN LEGEND

BOH	4,609 NSF
CIRCULATION	2,274 NSF
GARAGE	2,025 NSF
GUESTROOM	5,686 NSF
OUTDOOR	1,282 NSF
PUBLIC	8,460 NSF

A2 PRESENTATION PLAN - LEVEL 01
1/16" = 1'-0"



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KETCHUM BOUTIQUE HOTEL

River & Main
 Ketchum, ID 83353

SHEET NAME:
PRESENTATION PLAN - LEVEL 01

REVISIONS

MARK	DATE	DESCRIPTION
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ISSUE DATE: 06/18/19
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PR 01

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PROJECT DESCRIPTION
 KETCHUM BOUTIQUE
 HOTEL

River & Main
 Ketchum, ID 83353

SHEET NAME:
 PRESENTATION PLAN -
 LEVEL 02

MARK	DATE	DESCRIPTION

ISSUE DATE: 06/18/19
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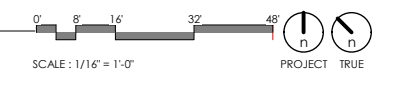
SHEET NUMBER:
PR 02



PLAN LEGEND

BOH	825 NSF
CIRCULATION	3,720 NSF
GUESTROOM	10,776 NSF
OUTDOOR	3,739 NSF
PUBLIC	1,039 NSF

A2 PRESENTATION PLAN - LEVEL 02
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PROJECT DESCRIPTION
**KETCHUM BOUTIQUE
 HOTEL**

River & Main
 Ketchum, ID 83353

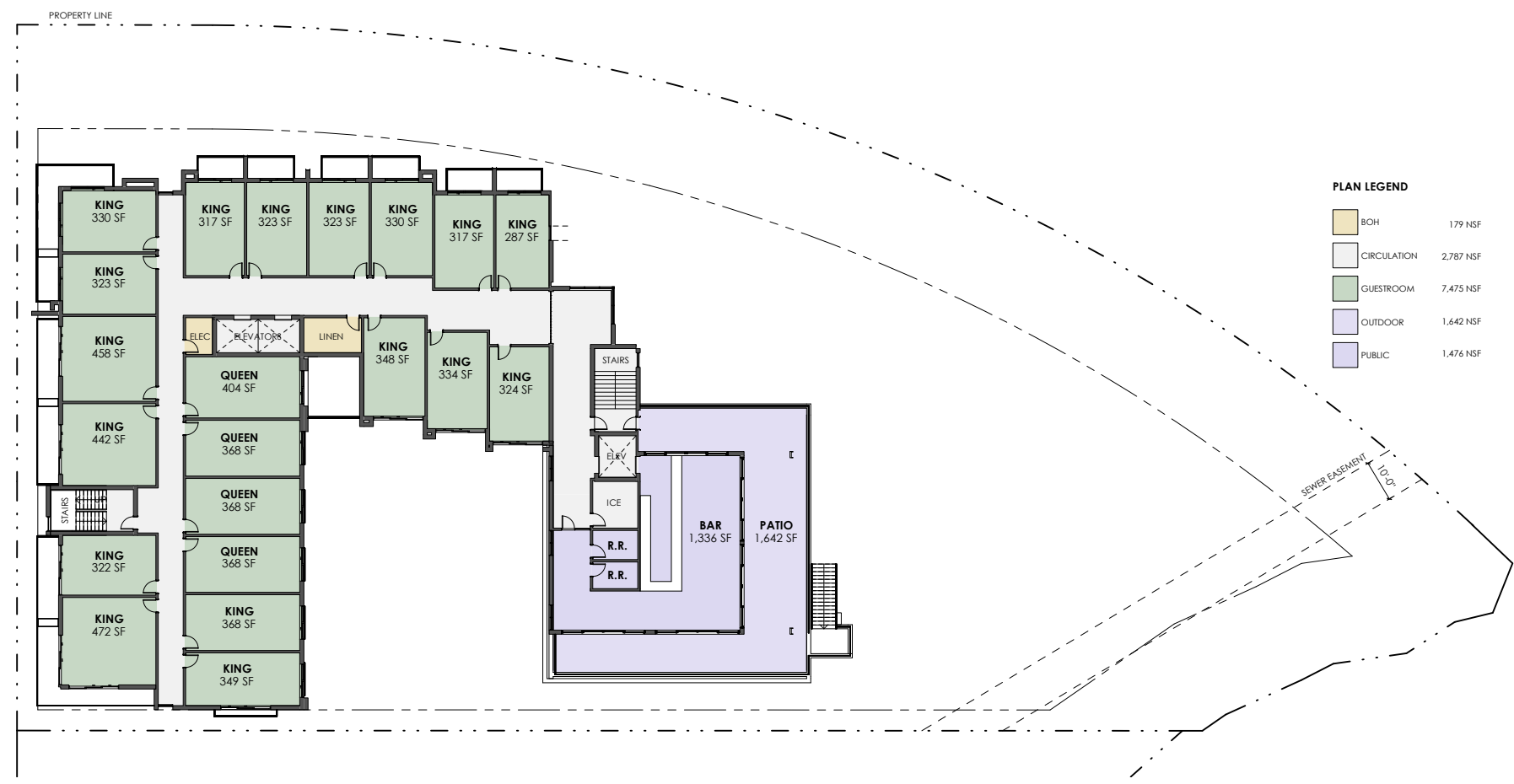
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 LEVEL 04**

REVISIONS
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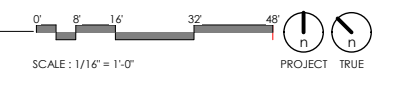
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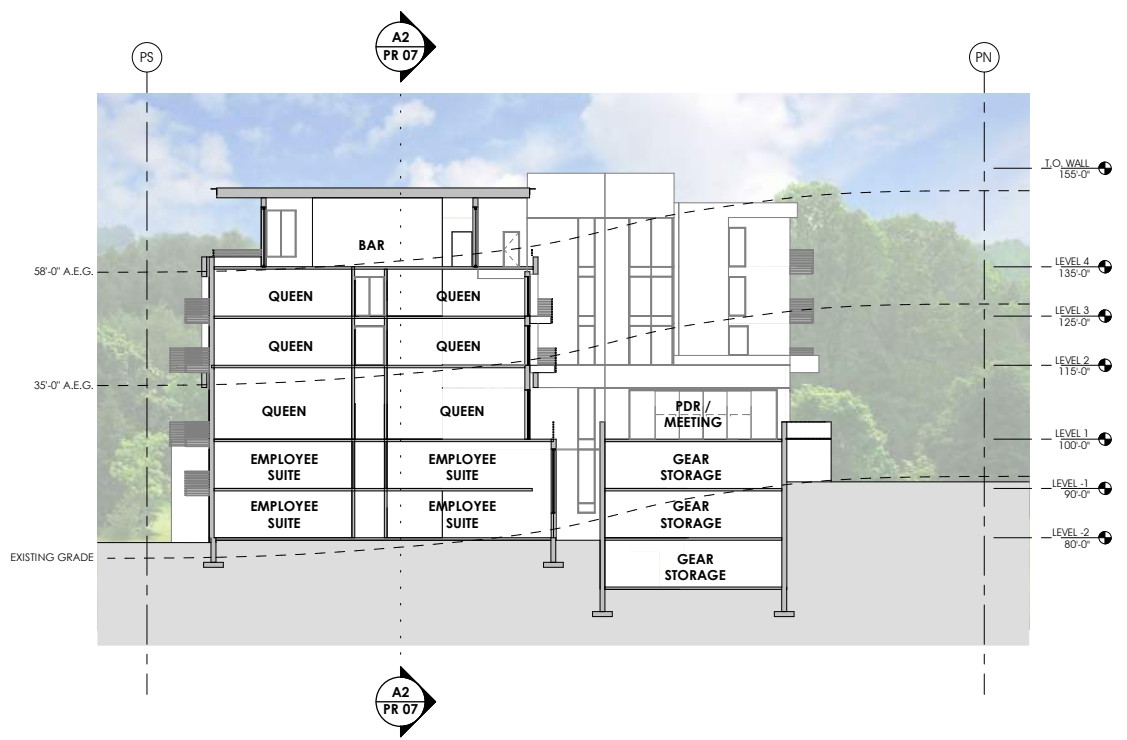


A2 PRESENTATION PLAN - LEVEL 04
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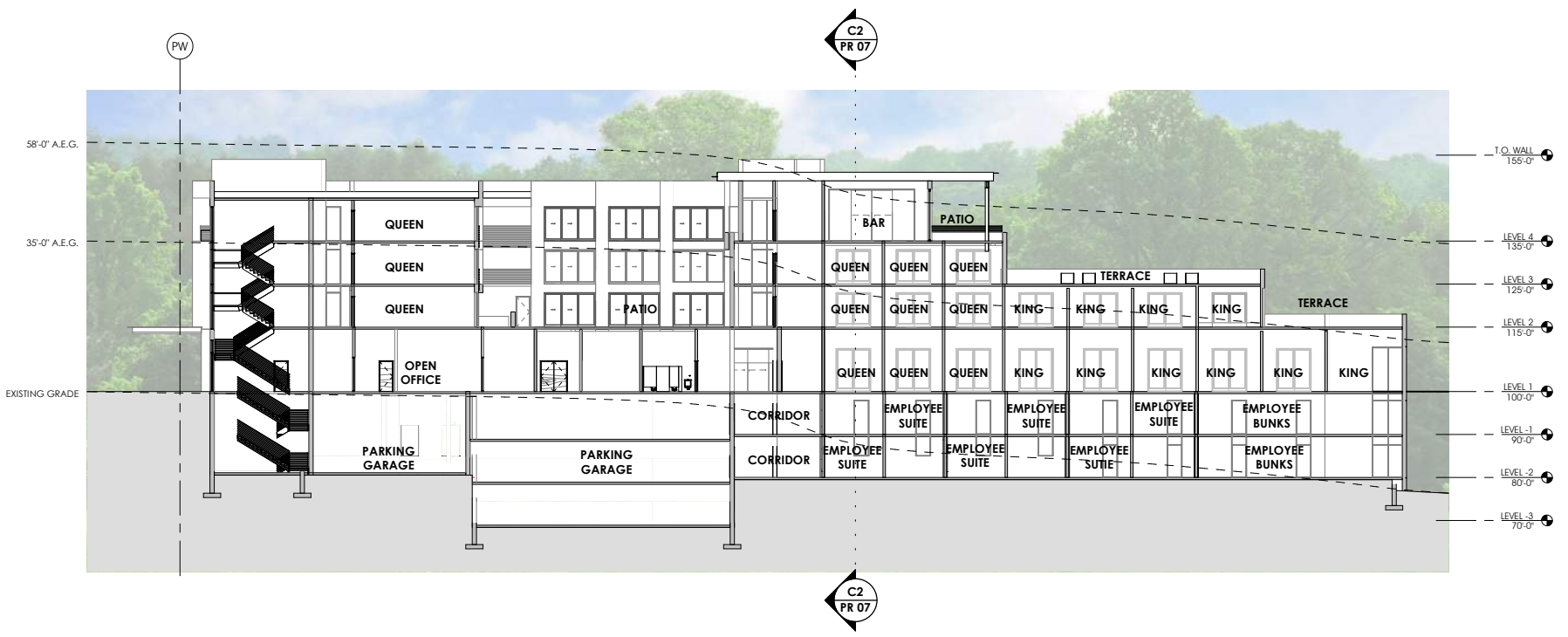


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C2 BUILDING SECTION 2
1/16" = 1'-0"



A2 BUILDING SECTION 1
1/16" = 1'-0"

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PROJECT DESCRIPTION
KETCHUM BOUTIQUE HOTEL

River & Main
 Ketchum, ID 83353

SHEET NAME:
BUILDING SECTION

REVISIONS
 MARK DATE DESCRIPTION

ISSUE DATE: 07/02/19
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PR 07

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EXHIBIT | 02.02

FLOOR AREA RATIO CALCULATIONS

FLOOR AREA RATIO (FAR) CALCULATION

BUILDING AREA DEFINITION

The sum of the horizontal area of the building measured along the outside walls of each floor, including stair towers and elevators on the ground floor only. This calculation factors all enclosed areas including the parking garage

BUILDING AREA

LEVEL 01	25,423	SF
LEVEL 02	18,461	SF
LEVEL 03	15,394	SF
LEVEL 04	13,019	SF
LEVEL -01 (BASEMENT)	26,373	SF
LEVEL -02 (BASEMENT)	26,373	SF
LEVEL -03 (BASEMENT)	10,872	SF
Total Building Area	135,915	SF

OPEN SPACE CALCULATION

Ketchum City requires a 35% minimum open space in the Tourist Zone. Private decks, patios, and walkways can use 5% of this open space area.

LEVEL 01 - FOOTPRINT	25,423	SF
LEVEL 01 - OPEN	21,825	SF
LEVEL 01 - PATIOS	4,485	SF
SITE AREA	47,248	SF
LEVEL 01 - PATIOS	17,340	SF
SITE AREA	47,248	SF

Open Space = Level 01 Open / Site Area

$$\text{OS} = 21,825 \text{ SF} / 47,248 \text{ SF}$$

$$\text{OS} = 46\%$$

FLOOR AREA | NET DEFINITION

The sum of the horizontal areas of all floors in a building including basements but not including open unenclosed decks, interior or exterior circulation, mechanical equipment rooms, parking areas, common areas, public bathrooms, or storage areas in basements.

BUILDING NET FLOOR AREA (NFA)

LEVEL 01	18,289	NFA
LEVEL 02	14,400	NFA
LEVEL 03	11,718	NFA
LEVEL 04	9,907	NFA
LEVEL -01 (BASEMENT)	8,146	NFA
LEVEL -02 (BASEMENT)	6,458	NFA
LEVEL -03 (BASEMENT)	0	NFA
Total Building NFA	68,918	NFA
Site Square Feet	47,248	SF

Net FAR = Net Floor Area / Site Square Feet

$$\text{FAR} = 68,918 \text{ NFA} / 47,248 \text{ SF}$$

$$\text{FAR} = 1.4$$

FLOOR AREA | GROSS DEFINITION

The sum of the horizontal area of the building measured along the outside walls of each floor of a building or portion of a building, including stair towers and elevators on the ground floor only, and fifty percent (50%) of atriums over eighteen feet (18') plate height, but not including basements, underground parking areas, or open unenvlosed decks. Parking areas covered by a roof or portion of the building and enclosed on three (3) or more sides by building walls are included. Four (4) parking stalls for developments on single Ketchum town site lots of five thousand six hundred (5,600) square feet in size or less are not included in the gross floor area calculation.

BUILDING GROSS FLOOR AREA (GFA)

LEVEL 01	25,355	GFA
LEVEL 02	18,405	GFA
LEVEL 03	15,318	GFA
LEVEL 04	12,947	GFA
LEVEL -01 (BASEMENT)	13,601	GFA
LEVEL -02 (BASEMENT)	3,291	GFA
LEVEL -03 (BASEMENT)	0	GFA
Total Building GSF	88,917	GFA
Site Square Feet	47,248	SF

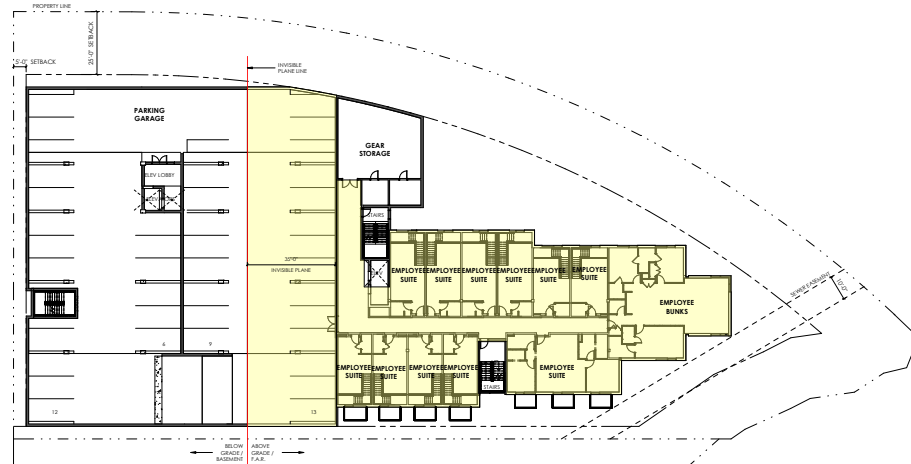
FAR = Gross Floor Area / Site Square Feet

$$\text{FAR} = 88,917 \text{ GFA} / 47,248 \text{ SF}$$

$$\text{FAR} = 1.9$$

LEVEL -1 (BASEMENT)

13,601 GSF

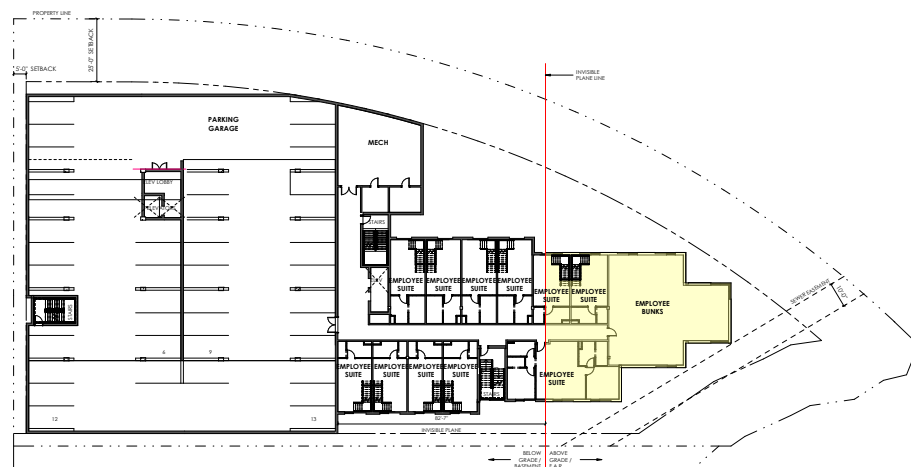


- AREA INCLUDED IN GSF
 - EMPLOYEE BUNKS
 - CORRIDOR
 - EMPLOYEE SUITES
 - EMPLOYEE SUITES STAIRS
 - PARKING AREAS ABOVE INVISIBLE PLANE

- AREA NOT INCLUDED IN GSF
 - PARKING GARAGE
 - ELEVATOR LOBBY
 - ELEVATOR
 - GEAR STORAGE (BASEMENT)
 - STAIRS

LEVEL -2 (BASEMENT)

3,291 GSF

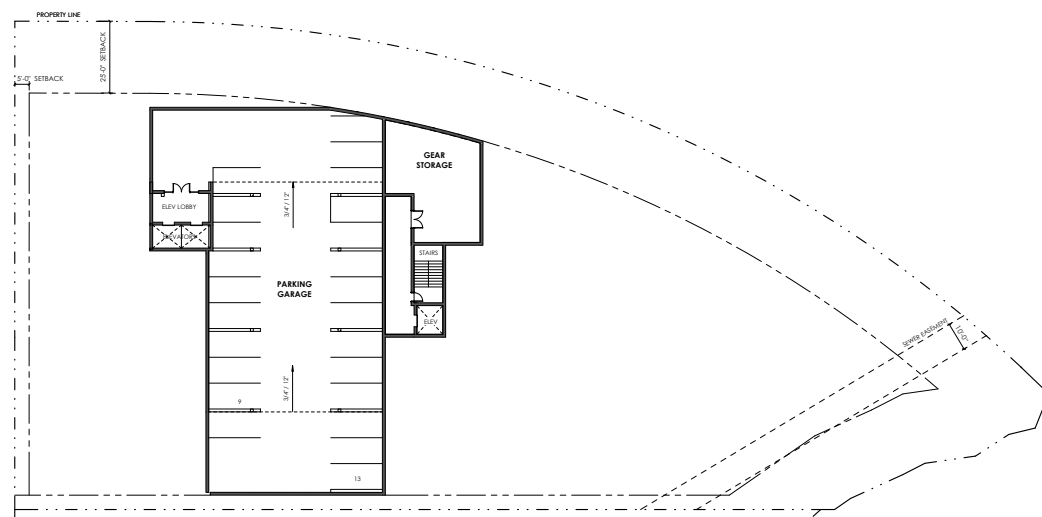


- AREA INCLUDED IN GSF
 - EMPLOYEE BUNKS
 - CORRIDOR
 - EMPLOYEE SUITES

- AREA NOT INCLUDED IN GSF
 - PARKING GARAGE
 - ELEVATOR LOBBY
 - ELEVATOR
 - GEAR STORAGE (BASEMENT)
 - STAIRS
 - BASEMENT AREAS BELOW INVISIBLE PLANE

LEVEL -3 (BASEMENT)

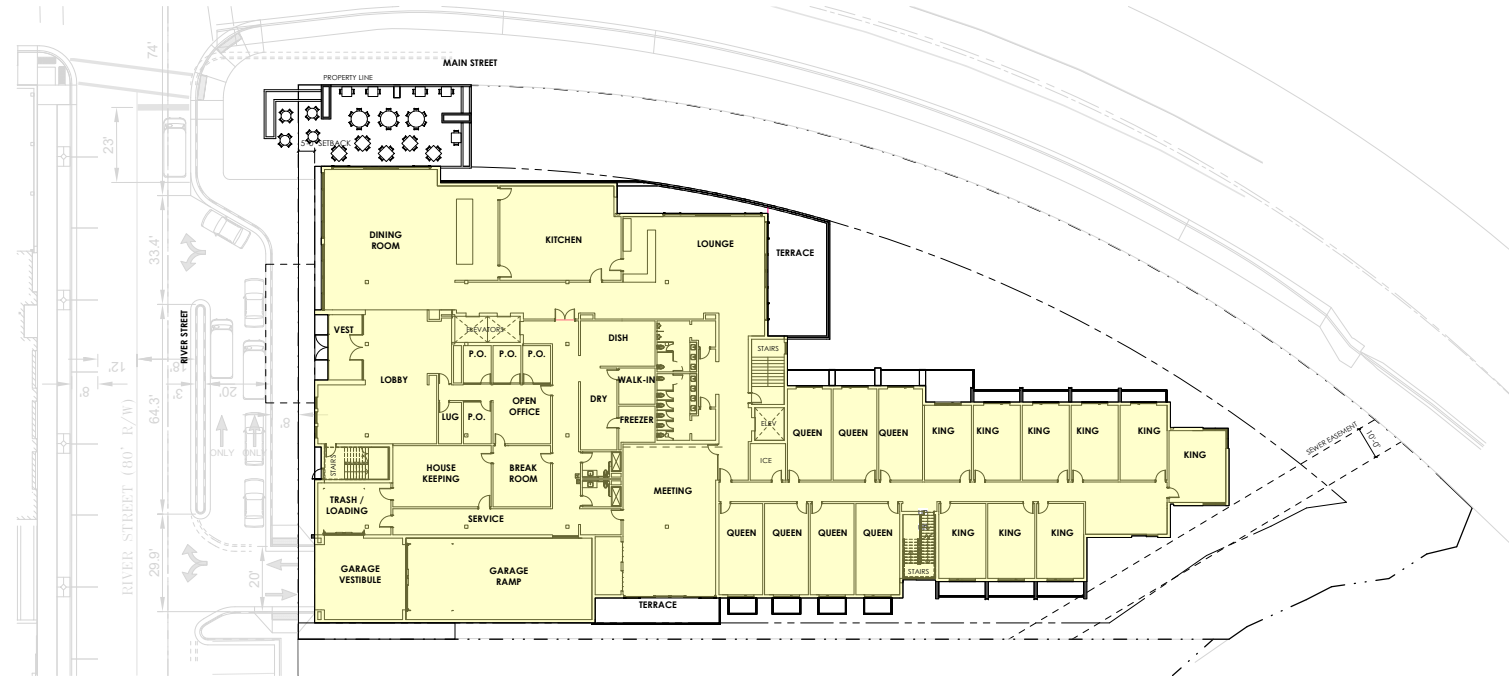
0 GSF



- AREA NOT INCLUDED IN GSF
 - PARKING GARAGE
 - ELEVATOR LOBBY
 - ELEVATOR
 - GEAR STORAGE (BASEMENT)
 - STAIRS
 - CORRIDOR

LEVEL 1 (MAIN LEVEL)

25,335 GSF



- AREA INCLUDED IN GSF
- VESTIBULE
- ELEVATORS
- LOBBY
- STAIRS
- CORRIDOR
- RESTAURANT
- BACK OF THE HOUSE

- AREA NOT INCLUDED IN GSF
- TERRACE
- BALCONIES

LEVEL 2

18,405 GSF



- AREA INCLUDED IN GSF
- KING & QUEEN ROOMS
- FITNESS
- WARMING KITCHEN
- LINEN
- ELECTRICAL
- ICE
- CORRIDOR

- AREA NOT INCLUDED IN GSF
- ELEVATOR
- STAIRS
- BALCONY
- TERRACE
- PATIO

LEVEL 3

15,318 GSF



- AREA INCLUDED IN GSF
 - KING & QUEEN ROOMS
 - LINEN
 - ELECTRICAL
 - ICE
 - CORRIDOR

- AREA NOT INCLUDED IN GSF
 - ELEVATORS
 - STAIRS
 - BALCONIES
 - TERRACE

LEVEL 4

12,947 GSF



- AREA INCLUDED IN GSF
 - KING & QUEEN ROOMS
 - LINEN
 - ELECTRICAL
 - ICE
 - CORRIDOR
 - RESTROOMS
 - BAR

- AREA NOT INCLUDED IN GSF
 - ELEVATORS
 - STAIRS
 - BALCONIES
 - PATIO

FLOOR AREA RATIO (FAR) SECTION

FLOOR AREA | INVISIBLE PLANE
 Section below shows the invisible plane as it relates to floor area. By Ketchum definition, areas below the invisible plane are classified as basement or below grade parking and do not count in the floor area ratio.



EXHIBIT | 02.03
HEIGHT ANALYSIS

HEIGHT ANALYSIS

The Boutique Hotel site has a large slope from Trail Creek at the south end of the lot to the north end along River Street. As currently designed, the boutique hotel is proposed to be a four-story structure on River Street, and step / terrace down to three floors along Trail Creek. Current maximum height is assumed to be no taller than 58'-0" along River Street, and closer to 35'-0" on the south end of the property. A rooftop bar feature in the center of the structure will be 72'-0" above existing grade as the existing grade drops quickly in the center of the site. In comparison to both the Limelight and approved Auberge hotels on opposing corners, the height of the proposed Boutique Hotel will be lower and more closely align to the fourth floor of each building.

KETCHUM BOUTIQUE HOTEL

6 Total Levels

- 4 Above Grade @ River Street
- 2 Below Grade @ River Street

58'-0" to Roof Edge
+ Mechanical Screens

LIMELIGHT COMPARISON

7 Total Levels

- 5 Above Grade @ River Street
- 2 Below Grade @ River Street

68'-0" to Roof Edge
+ Mechanical Screens

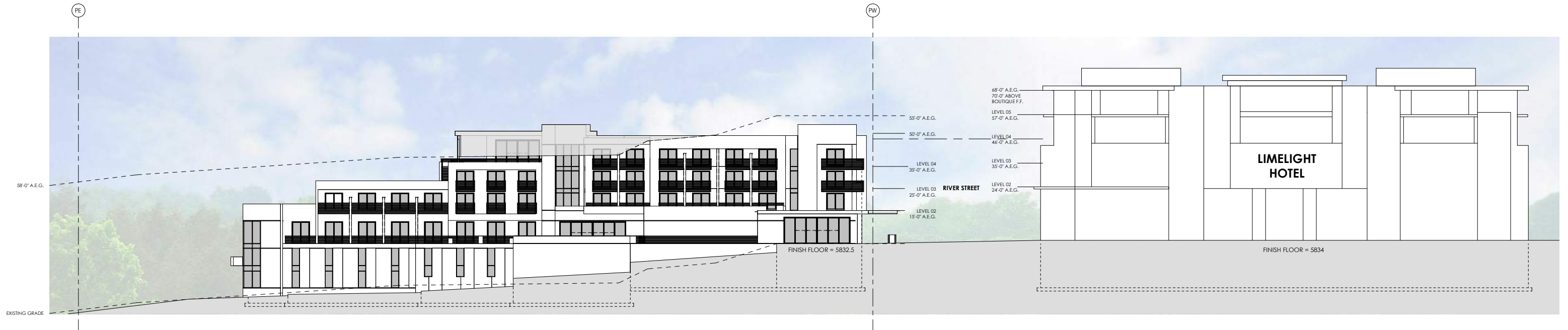
AUBERGE COMPARISON

7 Total Levels

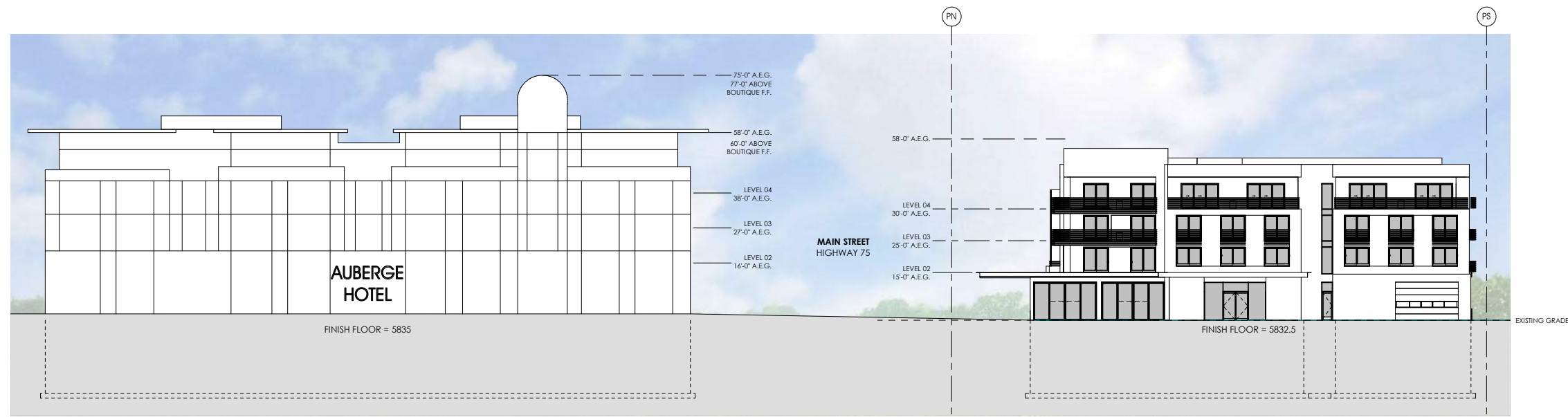
- 4 Above Grade @ River Street
- 3 Below Grade @ River Street

58'-0" to Roof Edge
+ Mechanical Screens
75'-0" to Observatory

HEIGHT ANALYSIS - CONTEXTUAL ELEVATIONS

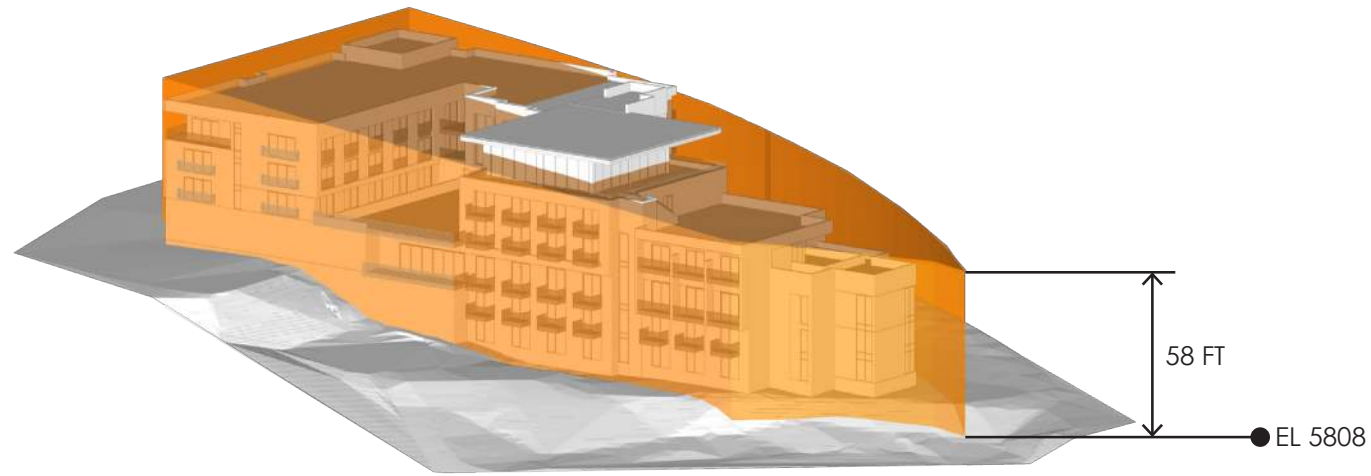


MAIN STREET | EAST SCHEMATIC ELEVATION
ADJACENT TO LIMELIGHT HOTEL

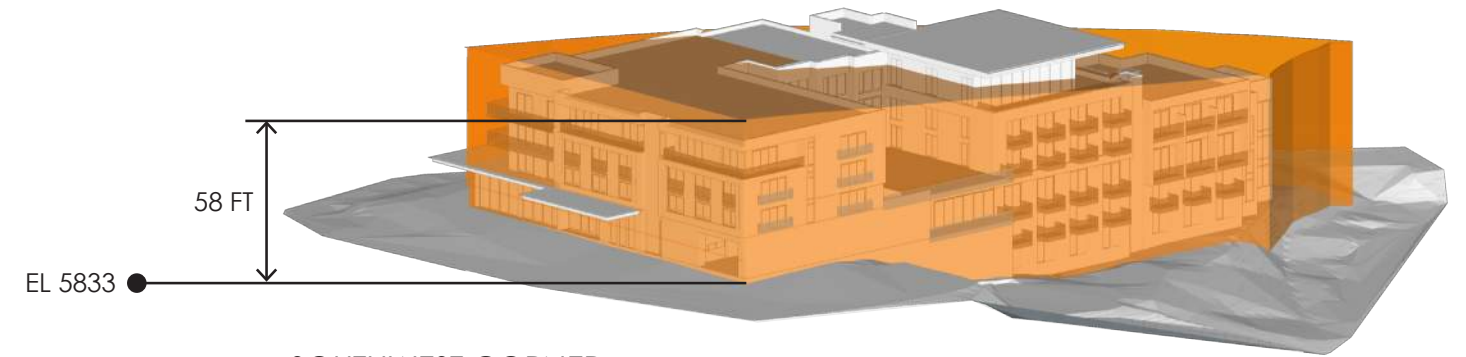


RIVER STREET | NORTH SCHEMATIC ELEVATION
ADJACENT TO APPROVED AUBERGE HOTEL

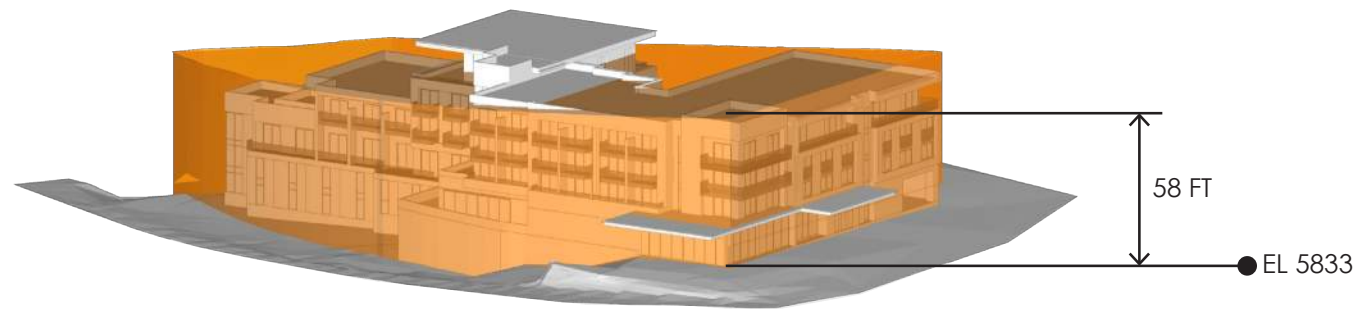
HEIGHT ANALYSIS - FOG PLANE STUDIES - INVISIBLE PLANE DIAGRAM



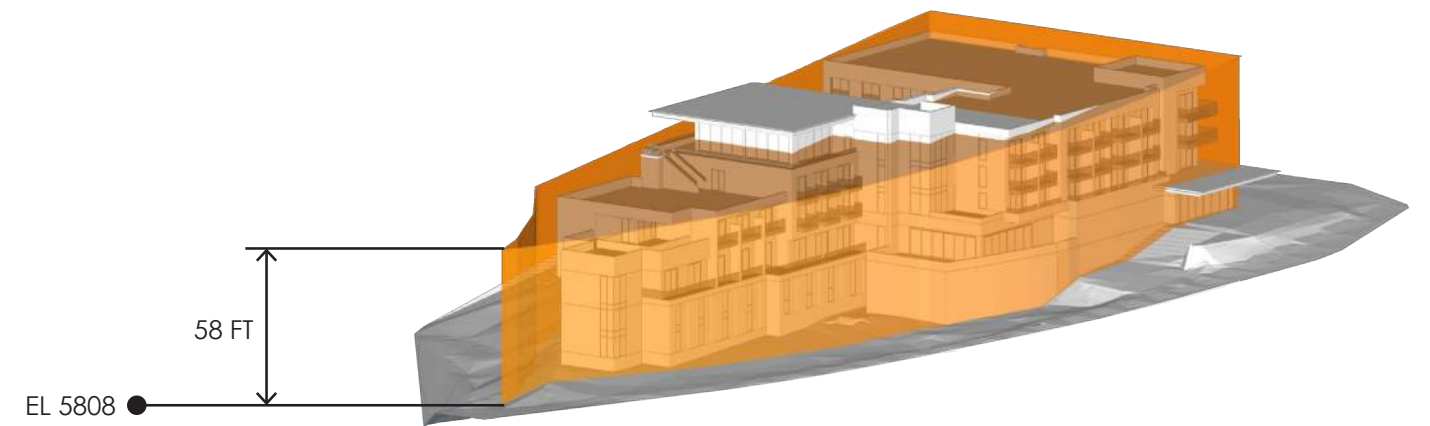
SOUTHEAST CORNER



SOUTHWEST CORNER

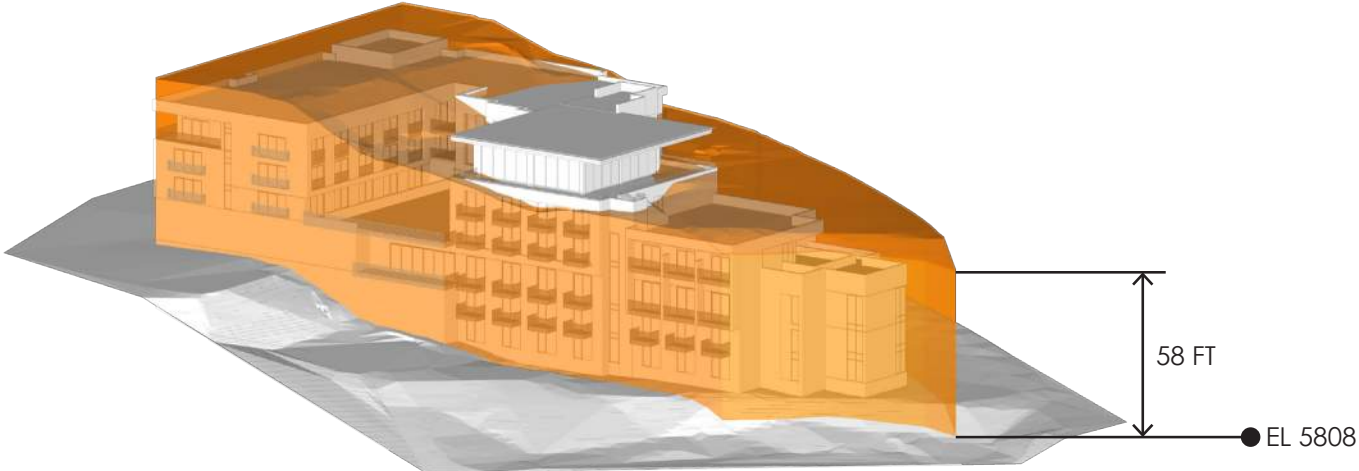


NORTHWEST CORNER



NORTHEAST CORNER

HEIGHT ANALYSIS - FOG PLANE STUDIES - EXISTING TOPOGRAPHY DIAGRAM



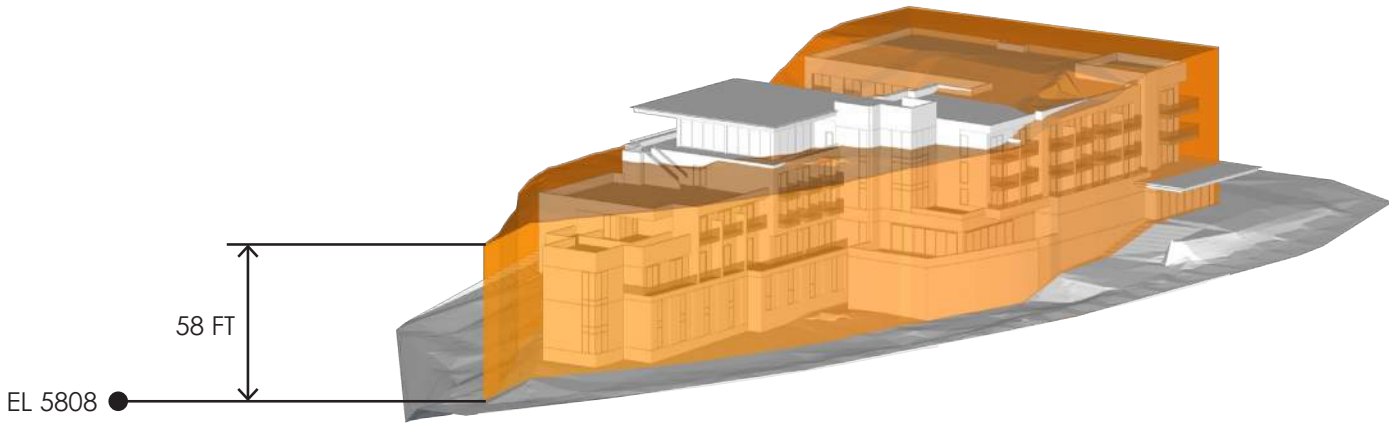
SOUTHEAST CORNER



SOUTHWEST CORNER



NORTHWEST CORNER



NORTHEAST CORNER

HEIGHT ANALYSIS - COMPATIBILITY VIEWS - NORTH APPROACH



HEIGHT ANALYSIS - COMPATIBILITY VIEWS - SOUTH APPROACH



HEIGHT ANALYSIS - GATEWAY STUDY RECOMMENDATIONS

B. Site 2: South Corner of River Street and Main Street

Existing Conditions

- This site is located in the Tourist Zone District.
- No alley exists here.
- Site 2 spans from River Street to Trail Creek and width of a city block from Main Street toward Washington Avenue. The site is currently terraced to accommodate existing development.
- Existing development within the multiple parcels that make Site 2 is compiled of five individual buildings. It is also relatively low in scale ranging from one story near the Highway and three stories on the western side of the site.
- The site currently rises approximately 30 feet from the creek bed to River Street.
- Development along River Street is typically one to one and a half stories facing River Street and drops to three plus stories along the creek side.
- On the west side of the Highway, a more natural riparian edge exists.
- An attached paved sidewalk exists adjacent to the Highway and continues across Trail Creek.
- Views towards Bald Mountain exist from the site looking down River Street as well as across Main Street to the northwest.
- Located at the southwest corner of Main and River Streets, this site receives ample sunlight throughout the day. As redevelopment occurs, it is important to acknowledge the mixed-use development to the west.



The northeast corner of Site 2 is at the intersection of Main Street and River Street.



Trail Creek retains its natural riparian character along the edge of Site 2.



Existing development is one to two stories in height and small in scale.

Key Site Considerations

- The landscaped edge along Highway 75 should be enhanced.
- The scale of development at the southern end of site is lower.
- Sensitive site edge adjacent to Trail Creek and existing development. Development should be set back from this edge.
- The site should establish a transition into more urban downtown. The River Street edge may be more "urban." The Main Street edge should maintain a "green" character.
- Pedestrian access to River Street should be provided.
- The overlook opportunity along Trail Creek toward Bald Mountain should be enhanced.
- No alley access exists on this site, therefore service and vehicular access should be visually minimized.



Existing development steps up with the topography. New development should reflect the topography as well.



See page 13 for an explanation of notes on the diagram.



A steep grade exists adjacent to Trail Creek. This provides a natural buffer area between the creek and potential development.

2007 GATEWAY STUDY

The design team has been referencing the massing recommendations of the 2007 Ketchum Gateway Study as precedent for overall design principles, building stepping / terracing, and treatment of facades with regards to the context. The current proposed design mimics these recommendations provided by the City of Ketchum in their preferred treatment of this corner

Site 2 - Design Principles

These principles apply to Site 2. A key objective is to provide a transition from a low scale, mountain town character of the neighborhoods to the more urban downtown.

- Large lots should be developed to appear as several buildings, rather than a single large structure.
- Several buildings rather than a single large structure will reduce the perceived mass of the development.
- A small element connecting the individual buildings is appropriate.

Step down in height toward a pedestrian way

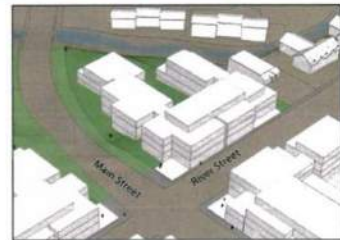
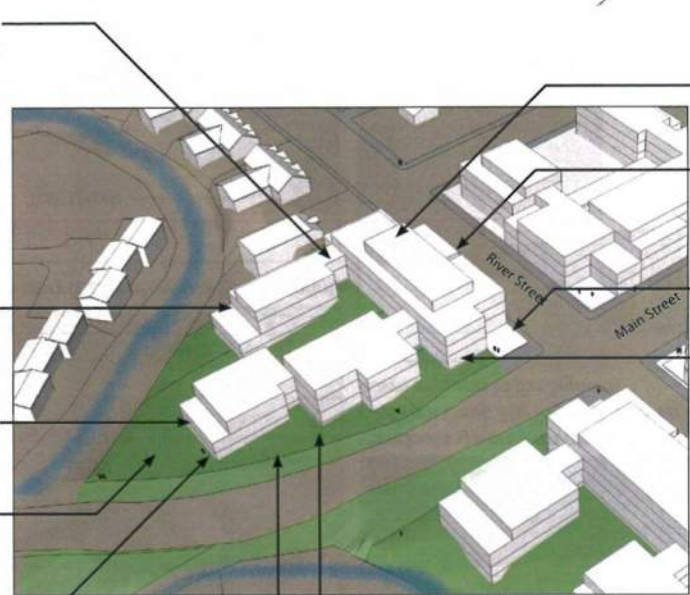
Step down in height toward Trail Creek

Locate outdoor amenity space adjacent to Trail Creek

Exposed foundation should not exceed 4 feet.

Provide a landscaped area adjacent to Highway to convey a "forested" character

Maintain a high percentage of open space



View looking southwest

Set back any potential 4th floor from the sidewalk and locate it adjacent to River Street

Building facade along River Street may reflect a more urban character

Maintain open space at corner as a gateway feature.

Frame gateway plaza with a building element and step down in height adjacent to Highway



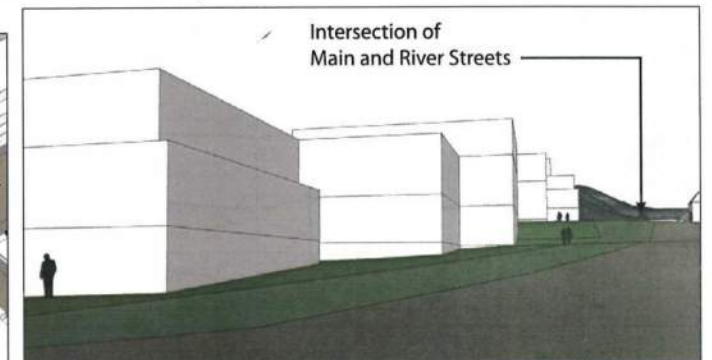
View looking northeast

Site 2: Alternative 1 - 2 and 3 Stories

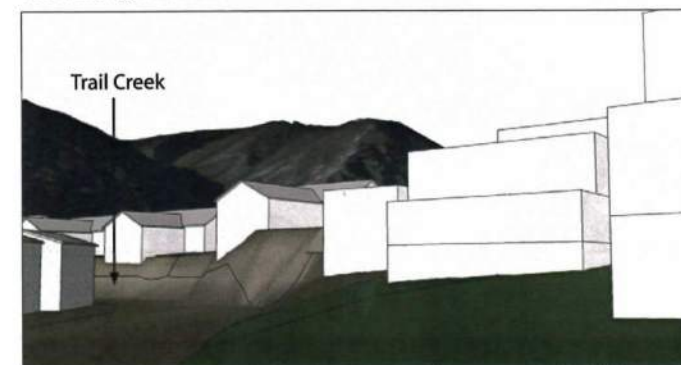
This scenario illustrates the "base" mass of no more than three stories. The third level steps back from the River Street, to minimize its visual impacts and reduce shading of the street.



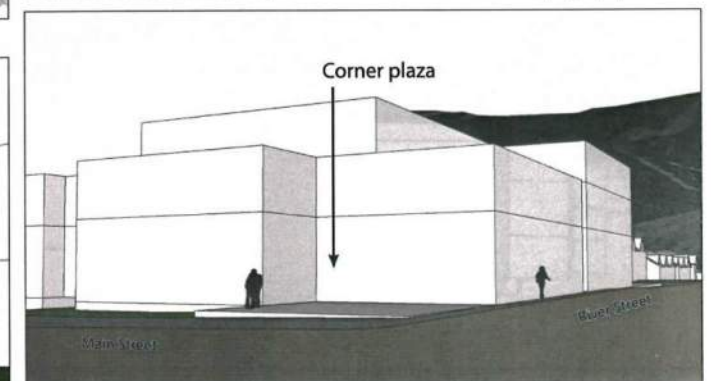
View looking northwest



View looking north along Highway 75 toward River Street from the bridge over Trail Creek. The mass steps up toward River Street with the topography.



View looking west from the southern point of Site 2 toward Bald Mountain,



View looking east toward the northwest corner of Site 1, along River Street. A gateway plaza is located in the foreground, in keeping with the gateway design

HEIGHT ANALYSIS - GATEWAY STUDY PREVIOUS SUBMITTAL

2007 GATEWAY STUDY

A previous project submittal was included in the Gateway Study as a sample use for this lot. This design maximized buildable area on the site, and was not sensitive to the scale of the surrounding context or topography of the site. This example helps contrast our proposed Boutique hotel design as we are addressing the context in a much more responsible manner than previous designs.

City of Ketchum, Idaho

June 12, 2007

Site 2 Preliminary Applicant Submittal



View looking northwest



View looking northeast



View looking southwest

City of Ketchum, Idaho

June 12, 2007

Site 2 Preliminary Applicant Submittal



River Street Elevation (provided by applicant)



Highway 75 Elevation (provided by applicant)



An elevation of the proposed development adjacent to existing residential development to the south (left) and existing, larger commercial development to the north (right)

EXHIBIT | 02.04

SUN STUDIES

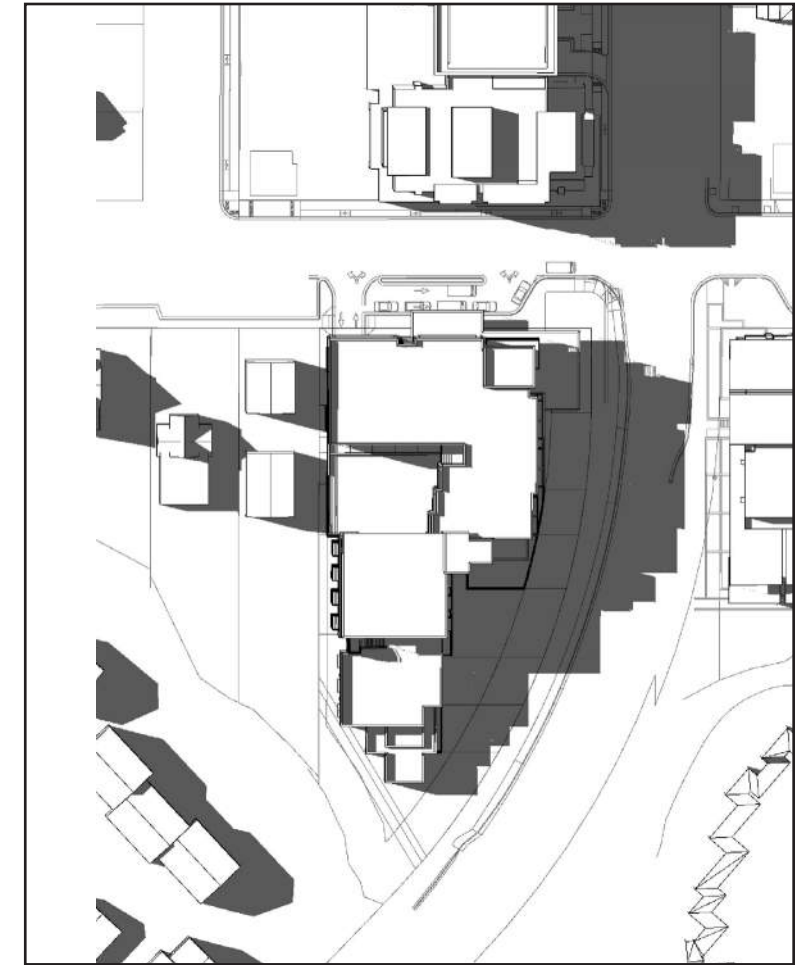
SUN / SHADOW STUDIES - SPRING EQUINOX



MARCH 21 - 9:00 AM



MARCH 21 - 12:00 PM



MARCH 21 - 3:00 PM

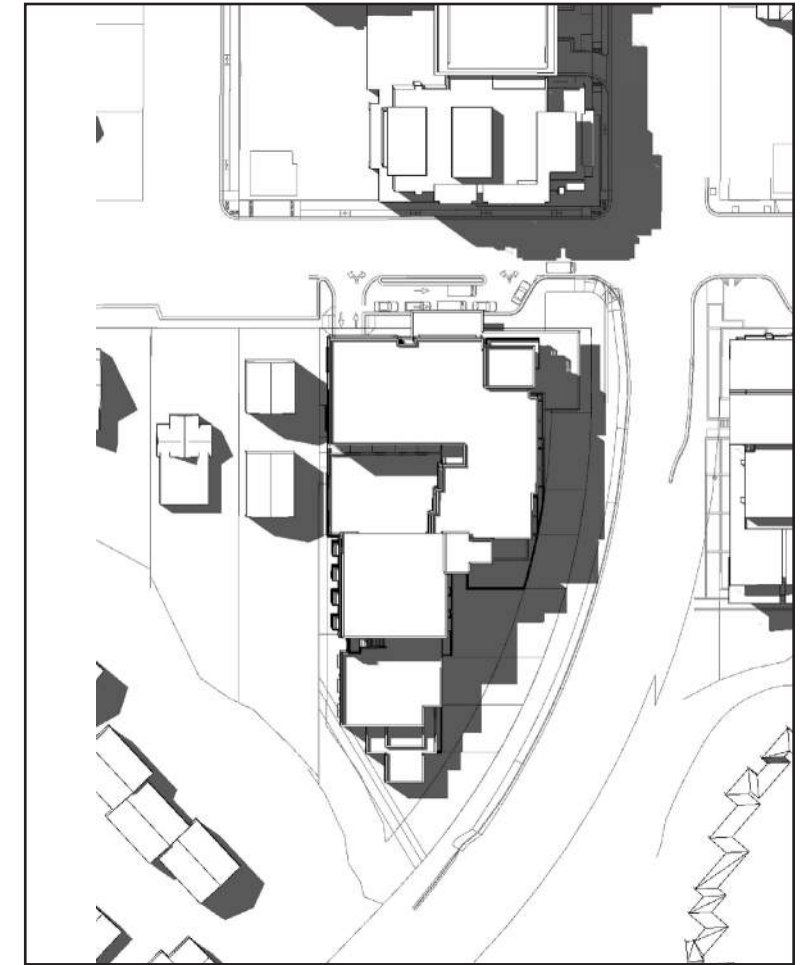
SUN / SHADOW STUDIES - SUMMER SOLSTICE



JUNE 21 - 9:00 AM



JUNE 21 - 12:00 PM



JUNE 21 - 3:00 PM

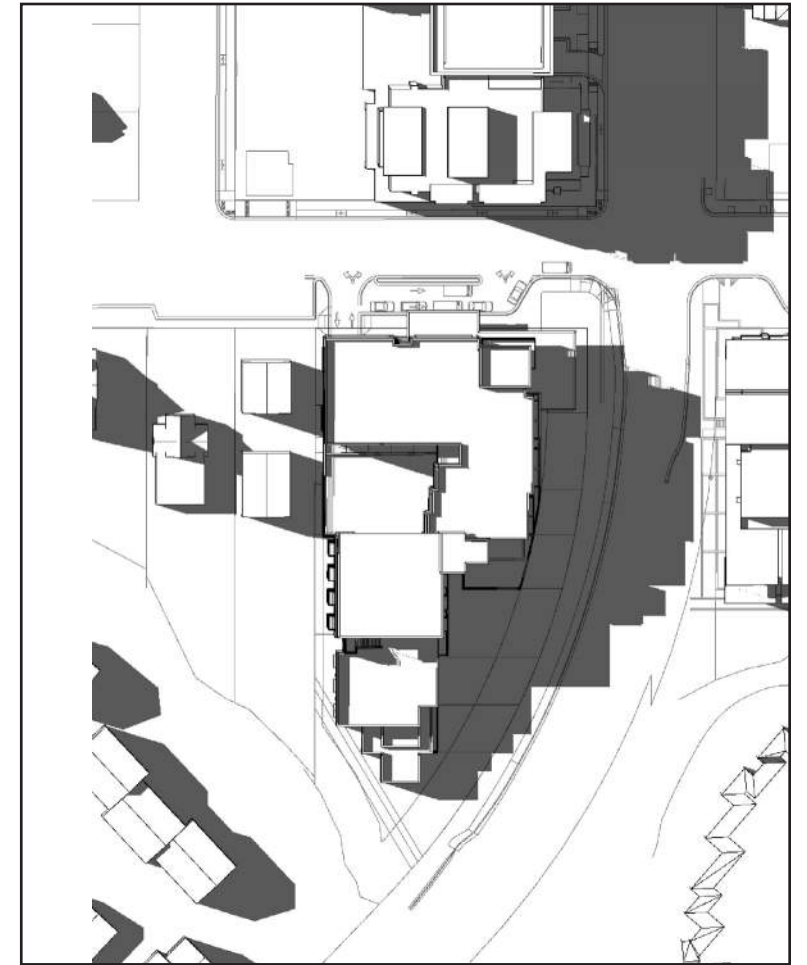
SUN / SHADOW STUDIES - FALL EQUINOX



SEPT 21 - 9:00 AM



SEPT 21 - 12:00 PM



SEPT 21 - 3:00 PM

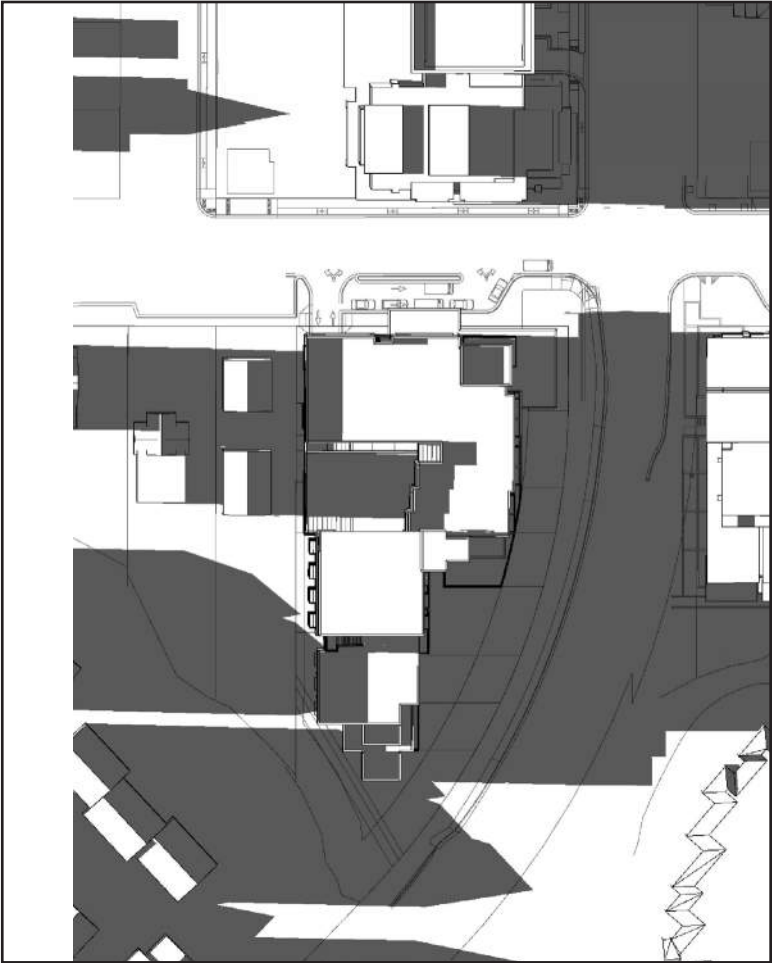
SUN / SHADOW STUDIES - WINTER SOLSTICE



DEC 21 - 9:00 AM



DEC 21 - 12:00 PM

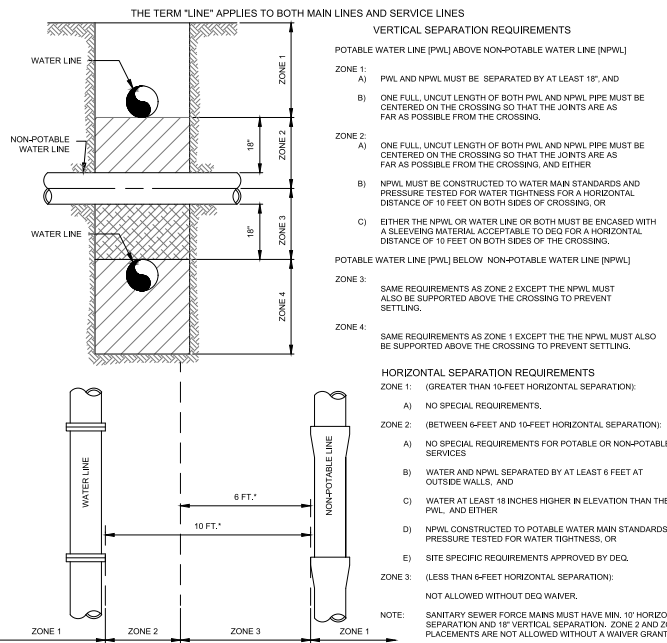


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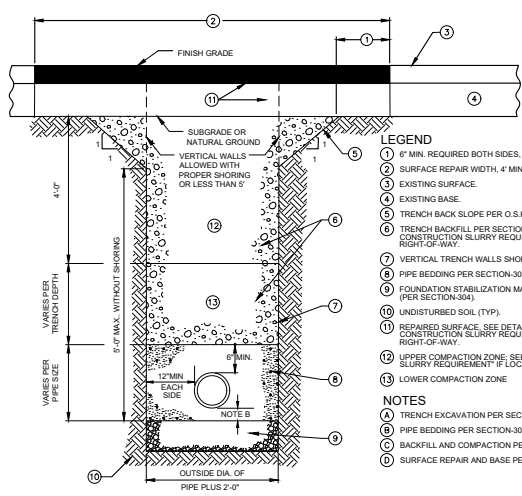
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EXHIBIT | 02.05

CIVIL DRAWINGS

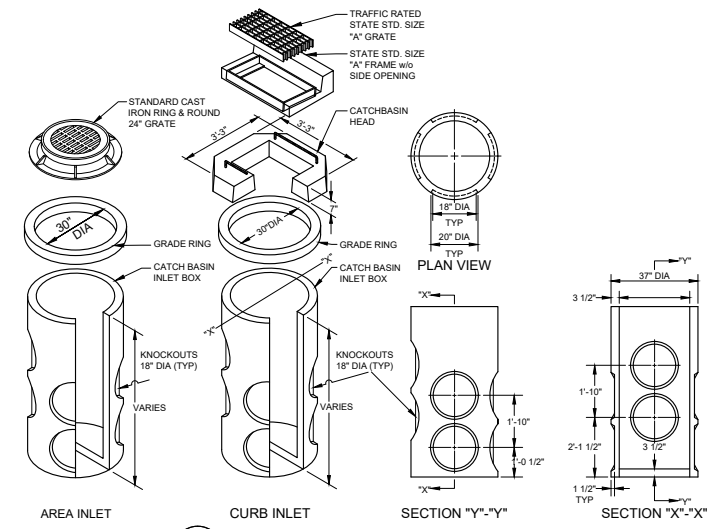


1 C0.2 POTABLE AND NON-POTABLE WATER LINE (NPWL) SEPARATION
N.T.S.

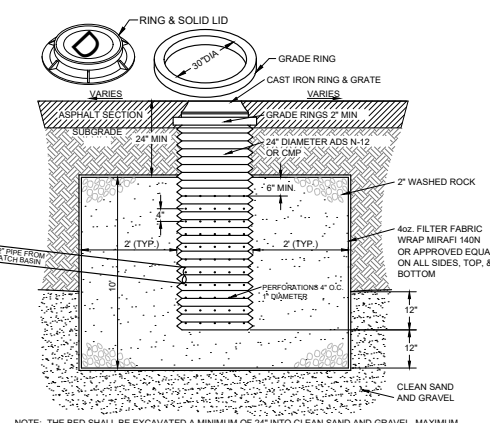


KETCHUM PUBLIC CONSTRUCTION SLURRY REQUIREMENT
 IN AREAS WHERE IT IS NECESSARY TO CUT THE ASPHALT PAVEMENT AND DIG A TRENCH FOR BURIAL OF CONDUIT CABLES OR OTHER CITY UTILITY, THE TRENCH SHALL BE BACKFILLED WITH A LEAN CONCRETE MIX TO THE BOTTOM OF FINISH SURFACE MATERIAL WITH THE FOLLOWING PROPORTIONS OF MATERIALS:
 COARSE AGGREGATE (9" MINUS)
 SAND
 PORTLAND CEMENT
 WATER
 NO COMPACTION, VIBRATION OR FINISHING IS REQUIRED. THE LEAN CONCRETE MIX SHALL BE STRUCK OFF AT OR BELOW THE ELEVATION OF THE PLANT MIX SURFACING WITH A SQUARE EDGE SHOVEL OR SIMILAR HAND TOOL. THE BACKFILL MIX SHALL BE ALLOWED TO SET FOR A MINIMUM OF 2 HOURS BEFORE THE PERMANENT PLANT MIX SURFACING IS PLACED TO COMPLETE THE TRENCH REPAIR. TEMPORARY PLACEMENT OF ASPHALT COLD MIX SURFACING MAY BE NECESSARY TO ACCOMMODATE TRAFFIC WITHIN THE FIRST 2 HOURS OF BACKFILL PLACEMENT PRIOR TO COMPLETING THE PERMANENT REPAIR.

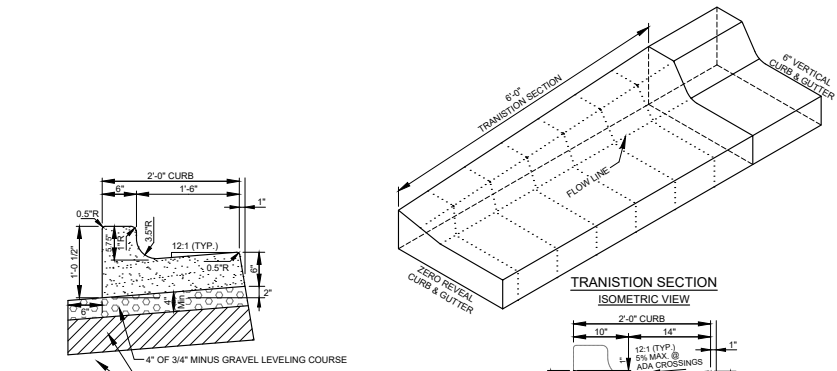
2 C0.2 TYPICAL TRENCH SECTION
N.T.S.



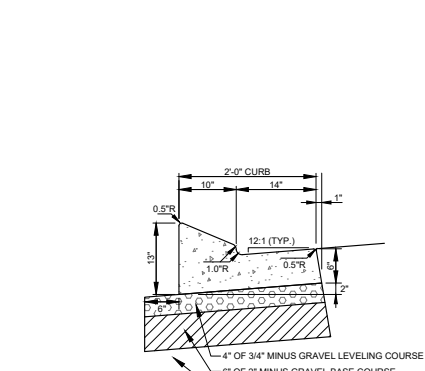
3 C0.1 30" DIAMETER CATCH BASIN
N.T.S.



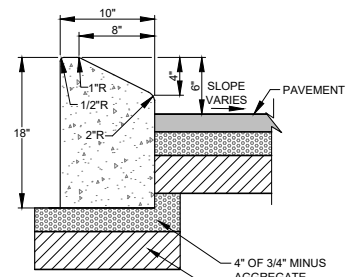
4 C0.2 DRYWELL DETAIL (6'X6')
N.T.S.



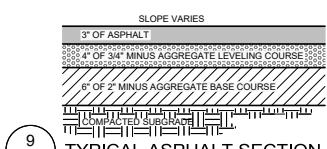
5 C0.2 6" CONCRETE VERTICAL CURB & GUTTER
N.T.S.



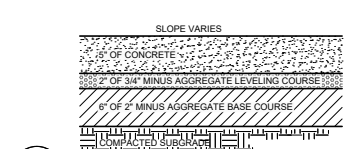
7 C0.2 MOUNTABLE CURB & GUTTER DETAIL
N.T.S.



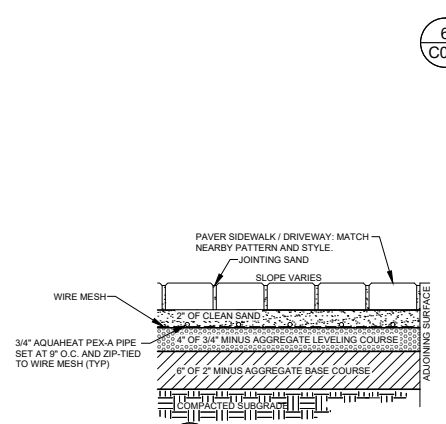
8 C0.2 MOUNTABLE CONCRETE CURB
N.T.S.



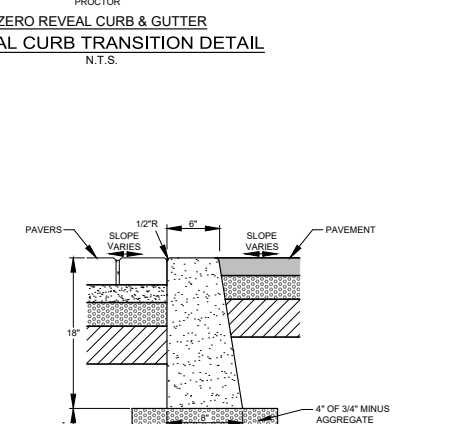
9 C0.2 TYPICAL ASPHALT SECTION
N.T.S.



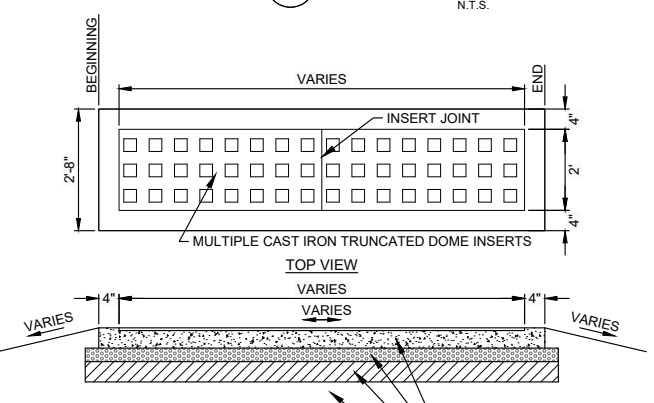
10 C0.2 TYPICAL CONCRETE SECTION
N.T.S.



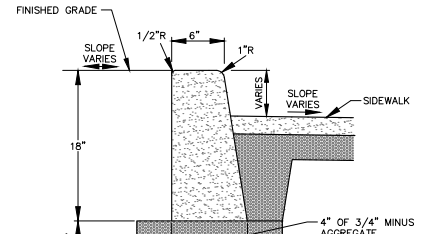
11 C0.2 PAVER DETAIL
N.T.S.



12 C0.2 CONCRETE CURB / PAVER EDGING
N.T.S.



13 C0.2 TRUNCATED DOME INSERT DETAIL AT PAVER SIDEWALK AREA
N.T.S.



14 C0.2 CONCRETE VERTICAL CURB
N.T.S.

PRELIMINARY NOT FOR CONSTRUCTION

THE KETCHUM BOUTIQUE HOTEL
 LOCATED WITHIN SECTION 18, T.4 N., R.18 E., B.M., CITY OF KETCHUM, BLAINE COUNTY, IDAHO
 PREPARED FOR PEG KETCHUM HOTEL LLC

PROJECT INFORMATION
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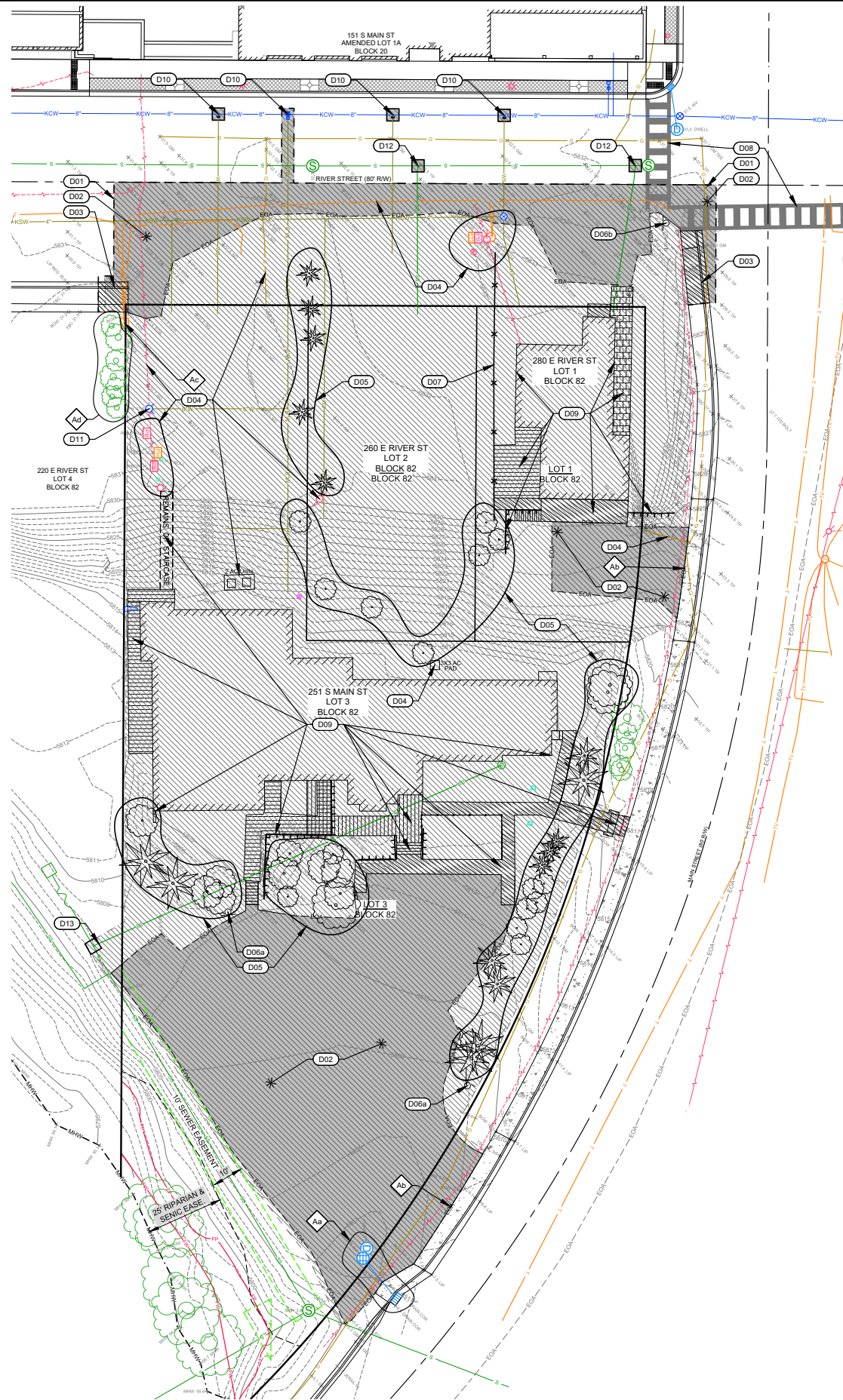
DESIGNED BY: C/JULIUS/M
 DRAWN BY: C/JULIUS/M
 CHECKED BY: C/JULIUS/M

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GALENA ENGINEERING, INC.
 Civil Engineers & Land Surveyors
 317 N. River Street
 Lewiston, ID 83501
 (208) 798-1205
 (208) 798-4612 fax
 email: galena@galena-engineering.com

NO.	DATE	BY	REVISIONS

C0.2



DEMOLITION SCHEDULE

- (D01) SAWCUT A MINIMUM OF 24" OF ASPHALT TO PROVIDE CLEAN VERTICAL EDGE.
- (D02) REMOVE AND DISPOSE OF ASPHALT.
- (D03) REMOVE AND DISPOSE OF CONCRETE CURB / SIDEWALK (UTILIZE EXISTING JOINTS WHERE POSSIBLE)
- (D04) CONTRACTOR TO COORDINATE THE REMOVAL AND/OR RELOCATION OF DRY UTILITIES, TO INCLUDE POWER, GAS, AND ALL COMMUNICATIONS.
- (D05) REMOVE AND DISPOSE OF TREE AND ROOT BALL SYSTEM
- (D06) REMOVE SIGN:
a. DISPOSE OF SIGN.
b. RETAIN SIGN. TO BE RELOCATED TO NEW SIDEWALK (SEE CORRESPONDING SIDEWALK DESIGN SHEET).
- (D07) REMOVE AND DISPOSE OF FENCE.
- (D08) OBLITERATE PAVEMENT MARKINGS
- (D09) DEMOLISH HOUSE / STRUCTURE AND ASSOCIATED RETAINING WALLS, SIDEWALKS, BOARD WALKS, AND STAIR CASES. CONTRACTOR TO ENSURE ALL CONNECTING UTILITIES ARE DISCONNECTED PRIOR TO DEMOLITION.
- (D10) CONTRACTOR TO LOCATE AND DISCONNECT WATER SERVICE AT WATER MAIN AND TURN OFF CURB STOP. CONTRACTOR SHALL NOTIFY KETCHUM WATER DEPARTMENT FOR INSPECTION PRIOR TO BACKFILLING. ROAD REPAIR SHALL CONFORM TO DETAIL 2 / C0.2. THE REMAINDER OF WATER SERVICE SHALL BE ABANDONED IN PLACE WITHIN RIGHT-OF-WAY.
- (D11) CONTRACTOR TO LOCATE AND DISCONNECT WATER LINE AT WATER MAIN AND INSTALL CAP WITH THRUST BLOCK. CONTRACTOR SHALL NOTIFY KETCHUM WATER DEPARTMENT FOR INSPECTION PRIOR TO BACKFILLING. THE ASSOCIATED VALVE BOX AND FIRE HYDRANT SHALL BE REMOVED AND THE REMAINDER OF WATER LINE SHALL BE ABANDONED IN PLACE WITHIN RIGHT-OF-WAY.
- (D12) CONTRACTOR TO LOCATE AND DISCONNECT SEWER SERVICE AT SEWER MAIN AND INSTALL PLUG PER KETCHUM WASTEWATER DIVISION STANDARDS. CONTRACTOR SHALL NOTIFY KETCHUM WASTEWATER DEPARTMENT FOR INSPECTION PRIOR TO BACKFILLING. ROAD REPAIR SHALL CONFORM TO DETAIL 2 / C0.2. THE REMAINDER OF SEWER SERVICE SHALL BE ABANDONED IN PLACE WITHIN RIGHT-OF-WAY.
- (D13) CONTRACTOR TO LOCATE AND DISCONNECT SEWER SERVICE AT SEWER MAIN AND INSTALL PLUG PER KETCHUM WASTEWATER DIVISION STANDARDS. CONTRACTOR SHALL NOTIFY KETCHUM WASTEWATER DEPARTMENT FOR INSPECTION PRIOR TO BACKFILLING. THE REMAINDER OF SEWER SERVICE SHALL BE ABANDONED IN PLACE WHERE APPLICABLE.

- (A) RETAIN AND PROTECT:
a. STORM DRAINAGE STRUCTURES
b. SIDEWALK
c. TELEPHONE RISER
d. TREES

DEMOLITION PLAN
THE KETCHUM BOUTIQUE HOTEL
 LOCATED WITHIN SECTION 18, T.4 N., R.18 E., B.M., CITY OF KETCHUM, BLAINE COUNTY, IDAHO
 PREPARED FOR PEG KETCHUM HOTEL LLC
 PROJECT INFORMATION
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DESIGNED BY: CTJ/LSMF
 DRAWN BY: CTJ/LSMF
 CHECKED BY:

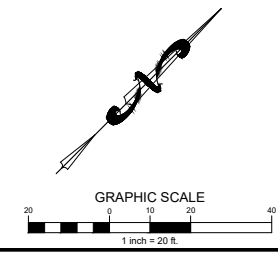
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 email: galena@galena-engineering.com

NO.	DATE	BY	REVISIONS

C1.0

PRELIMINARY
 NOT FOR
 CONSTRUCTION



GENERAL NOTES

1. THE CONTRACTOR SHALL OBTAIN ALL APPLICABLE EROSION CONTROL PERMITS PRIOR TO CONSTRUCTION.
2. THE CONTRACTOR SHALL INSTALL TEMPORARY SEDIMENT AND EROSION CONTROL DEVICES PRIOR TO AND CONCURRENT WITH ANY FILL OR GRADING ACTIVITIES IN ACCORDANCE WITH THE MOST CURRENT EDITION OF THE IDAHO DEPARTMENT OF ENVIRONMENTAL QUALITY CATALOG OF STORMWATER BEST MANAGEMENT PRACTICES FOR IDAHO CITIES AND COUNTIES AND THIS EROSION CONTROL AND BMP PLAN AND ASSOCIATED SWPPP DOCUMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING AND KEEPING A COPY OF THE ABOVE STANDARDS AND SPECIFICATIONS AND THE SWPPP DOCUMENTS ON SITE AT ALL TIMES DURING CONSTRUCTION.
3. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH ALL PROJECT PERMITS AND SWPPP DOCUMENTS. ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED AS SPECIFIED ON THIS PLAN OR AS DETERMINED BY THE ARCHITECT/ENGINEER AND MUST BE MAINTAINED IN EFFECTIVE OPERATING CONDITIONS THROUGHOUT THE LIFE OF THE PROJECT.
4. CONSTRUCTION ACTIVITY SHALL CEASE DURING PERIODS OF INTENSE RAINFALL AND SHALL NOT RECONVENE UNTIL THE EROSION OF SEDIMENT FROM THE SITE HAS BEEN SUFFICIENTLY STABILIZED.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING CONSTRUCTION VEHICLES LEAVING THE SITE TO PREVENT DUST, SILT AND DIRT FROM BEING RELEASED OR TRACKED OFF-SITE. CONSTRUCTION VEHICLES SHALL ENTER AND EXIT SITE ONLY THROUGH A PROPERLY INSTALLED CONSTRUCTION ENTRANCE/EXIT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY CLEAN-UP OF THE PUBLIC RIGHTS OF WAY OR ADJACENT PRIVATE PROPERTY FROM ANY TRACKING OR DEPOSITING OF MUD, DIRT, SAND, GRAVEL, ROCK OR DEBRIS.
6. BMPs SHALL BE INSPECTED AT LEAST EVERY 14 CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM EVENT THAT IS 0.25 INCHES OR GREATER TO IDENTIFY BMP EFFECTIVENESS AND IMPLEMENT REPAIRS OR DESIGN CHANGES AS SOON AS FEASIBLE. DEPENDING ON FIELD CONDITIONS. EQUIPMENT, MATERIALS AND WORKERS MUST BE AVAILABLE FOR RAPID RESPONSE TO FAILURES AND EMERGENCIES. ALL CORRECTIVE MAINTENANCE TO BMPs SHALL BE PERFORMED AS SOON AS POSSIBLE AFTER THE CONCLUSION OF EACH STORM, DEPENDING ON WORKER SAFETY.

PRIOR TO CONSTRUCTION ACTIVITIES

1. IMPLEMENT PERIMETER EROSION AND SEDIMENT CONTROL MEASURES AS SPECIFIED ON THIS PLAN (SILT FENCE, INLET PROTECTION, CONSTRUCTION ENTRANCE, ETC.)

DURING CONSTRUCTION ACTIVITIES AND WINTER SHUTDOWN AS NECESSARY

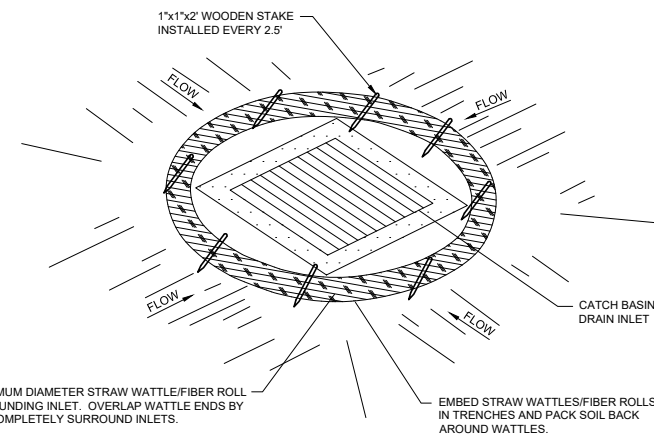
1. SPRAY WATER ON DISTURBED AREAS TO CONTROL DUST DURING DRY SEASON.
2. PROTECT NEWLY CONSTRUCTED STORM DRAINAGE FACILITIES PER DETAIL ON THIS SHEET.
3. AREAS WHERE GRADING IS COMPLETED TO FINISHED GRADES SHALL BE SEEDED AND MULCHED ACCORDING TO OWNER'S LANDSCAPING PLAN. COORDINATE THIS WORK WITH LANDSCAPE ARCHITECT.
4. DISTURBED AREAS LEFT DISTURBED OVER WINTER SHUTDOWN OR OTHER EXTENDED PERIODS OF TIME, SHALL BE PROTECTED FROM EROSION ACCORDING TO THE FOLLOWING:
DISTURBED AREAS WITH SLOPES OF 4:1 OR STEEPER SHALL BE SEEDED WITH "GRASSLAND WEST LOGRO" MIX AT 125 LBS/ACRE, OR APPROVED EQUIVALENT, FERTILIZE WITH 16/20/0/14 (SULFUR) AT 400 LBS/ACRE. MULCH WITH EITHER "S1 GEOSOLUTIONS LANDLOK S2" AT 2000 LBS/ACRE WITH 1% TACKIFIER OR WITH DRY STRAW SPREAD TO COMPLETELY COVER MOISTENED SOIL AND TRACKED INTO SOIL. "CAT TRACK" SLOPES PERPENDICULAR TO RUNOFF FLOW DIRECTION. CONTRACTOR SHALL FOLLOW MANUFACTURER'S APPLICATION INSTRUCTIONS FOR ALL SEED, MULCH, AND FERTILIZER MIXES. IF ADDITIONAL STEEP SLOPE PROTECTION IS NEEDED, INSTALL MATTING (BMP 18) AND FIBER ROLLS (BMP 35) PER THE CATALOG OF STORMWATER BEST MANAGEMENT PRACTICES FOR IDAHO CITIES AND COUNTIES, VOLUME 2.
5. AT A MINIMUM, THE CONTRACTOR'S "RESPONSIBLE PERSON" SHALL INSPECT THE SITE BEFORE AND AFTER STORM EVENTS AND AT 24 HOUR INTERVALS DURING EXTENDED STORMS.

UPON COMPLETION OF CONSTRUCTION ACTIVITIES

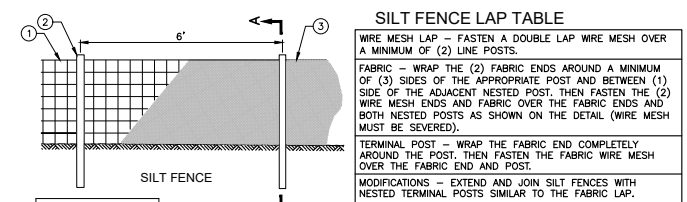
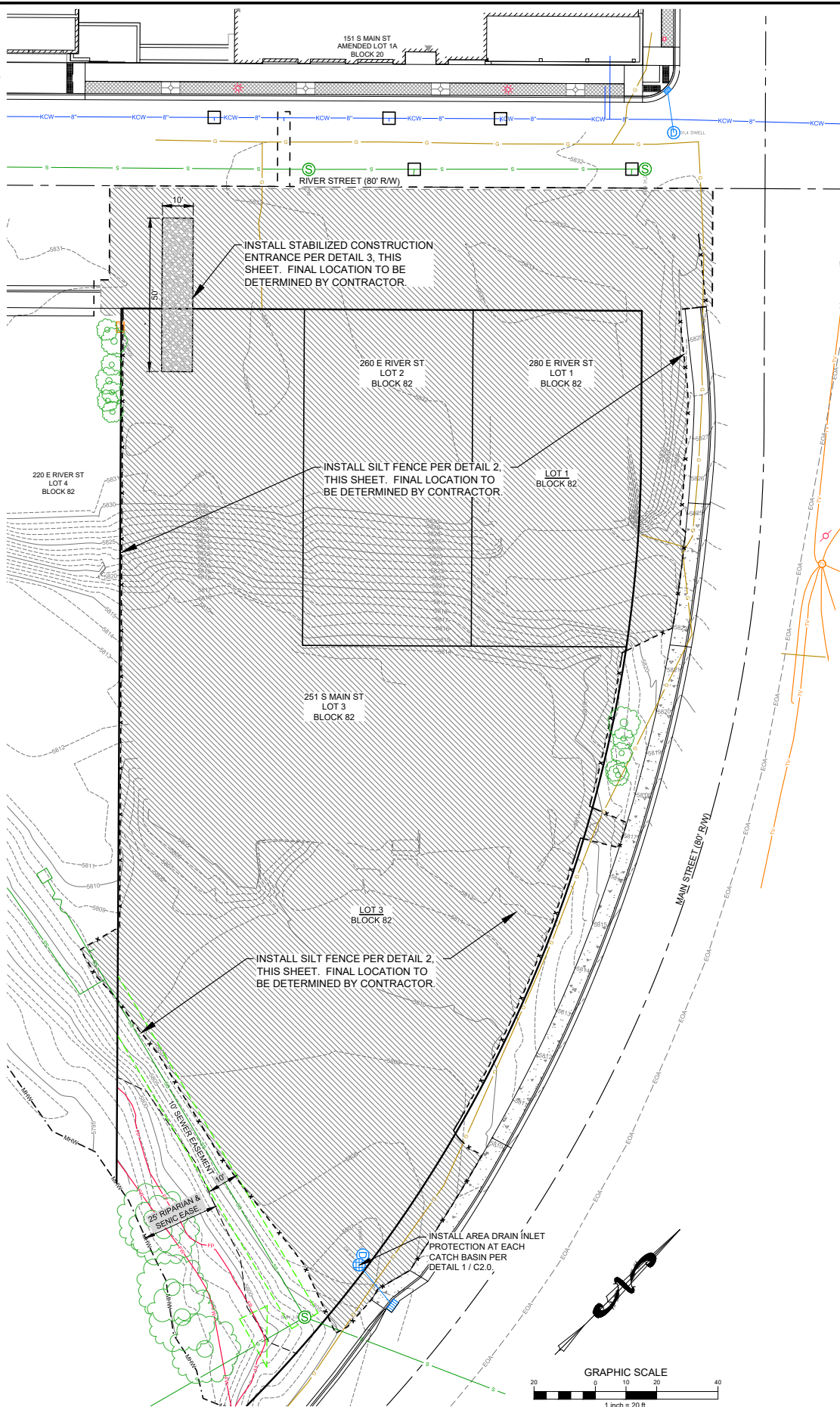
1. PERIODIC MAINTENANCE OF EROSION CONTROL MEASURES IS REQUIRED UNTIL STORM DRAINAGE FACILITIES ARE OPERATIONAL AND STABILIZING VEGETATION IS ESTABLISHED. SUCH MAINTENANCE INCLUDES CLEANING CATCH BASINS AND DRYWELLS, MAINTAINING CHECK DAMS AND OTHER BARRIERS, REMOVAL OF ACCUMULATED SILT IN SENSITIVE AREAS, AND STREET CLEANING. A TYPICAL SCHEDULE FOR SUCH MAINTENANCE WOULD BE ONCE A WEEK AND AFTER EACH SIGNIFICANT STORM EVENT.
2. UPON ESTABLISHMENT OF STABILIZING VEGETATION AND PERMANENT DRAINAGE FACILITIES OPERATIONS, REMOVE TEMPORARY EROSION CONTROL MEASURES AND CLEAR STREETS OF ACCUMULATED SILT, MUD AND DEBRIS.

NOTES:

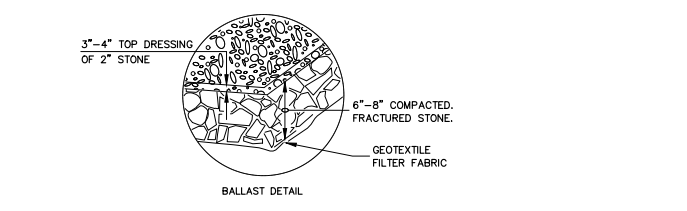
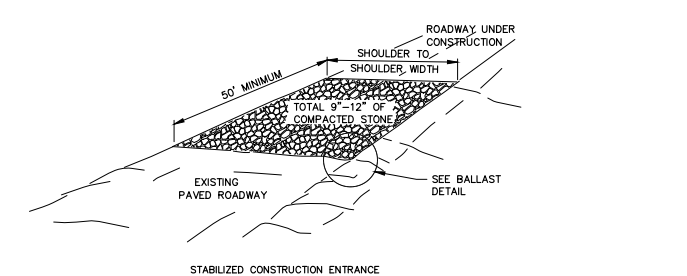
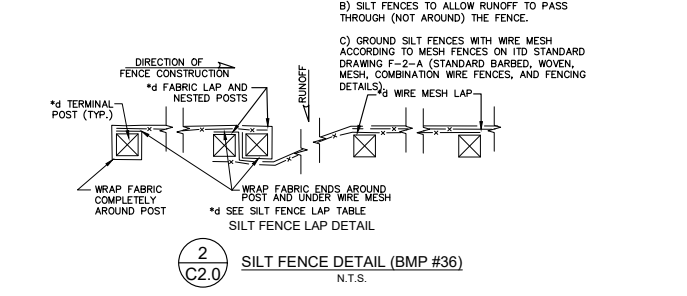
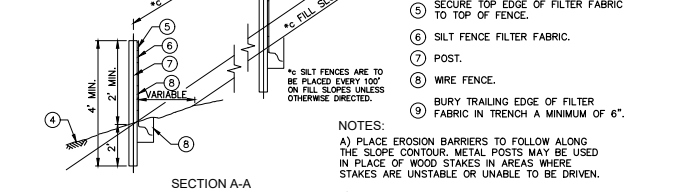
1. STRAW WATTLES/FIBER ROLLS TO BE PLACED AROUND INLETS INDICATED ON THE PLANS OR AS DIRECTED BY ENGINEER.
2. DIG TRENCHES APPROXIMATELY 2-4 INCHES DEEP BY 8-10 INCHES WIDE. THEN PLACE WATTLE BUNDLES IN TRENCH SO THAT THE ENDS OF THE BUNDLES OVERLAP AT LEAST 1'. THE OVERLAP SHOULD BE AS LONG AS NEEDED TO PERMIT STAKING SPECIFIED.
3. BUNDLES OF WATTLING SHALL BE STAKED THROUGH THE CENTER ON 2.5' CENTERS. PLACE EXTRA STAKES THROUGH THE BUNDLES AT EACH OVERLAP.
4. STAKES SHALL BE 1"x1"x2" WOODEN STAKES. DRIVE STAKES INTO A FIRM HOLD OF 1.5' DEEP. WHERE SOILS ARE SOFT USE LONGER STAKES.
5. PACK SOIL FIRMLY ALONG THE OUTSIDE OF THE INLET PROTECTION BY TAMPING OR WALKING ON SOIL.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING MAINTENANCE ITEMS UNTIL THE STRAW WATTLES ARE REMOVED. INSPECT REGULARLY AND AFTER EVERY STORM. MAKE ANY REPAIRS NECESSARY TO ENSURE THAT STRAW WATTLES ARE IN GOOD WORKING ORDER. REMOVE AND DISPOSE OF ANY ACCUMULATED SEDIMENT OFF-SITE. REPLACE STRAW WATTLES IF THEY BEGIN TO DETERIORATE.
7. INLET PROTECTION SHALL REMAIN IN PLACE AND OPERATIONAL UNTIL THE DRAINAGE AREA IS COMPLETELY STABILIZED AND COVERED WITH GRASS. THE CONTRACTOR SHALL REMOVE THE FIBER ROLLS AT THE REQUEST OF THE ENGINEER OR OWNER AFTER THE GRASS IS ESTABLISHED AND ACCEPTED BY THE ENGINEER OR OWNER.



1
C2.0
AREA DRAIN INLET PROTECTION DETAIL (BMP #31)
N.T.S.



EROSION BARRIER SPACING TABLE	
SLOPE RATIO	SPACING DIST. (MAX.)
2:1	45'
3:1	55'
4:1	75'
6:1	110'
10:1	220'
20:1	500'



- NOTES:**
1. STONE SIZE-USE 2" STONE OR RECLAIMED OR RECYCLED CONCRETE EQUIVALENT.
 2. LENGTH-AS REQUIRED, BUT NOT LESS THAN 50 FEET.
 3. THICKNESS-NOT LESS THAN 6 INCHES.
 4. WIDTH-10 FOOT MINIMUM. BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS.
 5. FILTER CLOTH-WILL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE.
 6. SURFACE WATER-ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED ACROSS THE ENTRANCE. IF PIPING IS IMPRACTICAL, A MOUNTABLE BERM WITH 5:1 SLOPES WILL BE PERMITTED.
 7. MAINTENANCE-THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.
 8. WASHING-WHEELS SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO PUBLIC RIGHTS-OF-WAY. WHEN WASHING IS REQUIRED IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.
 9. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.

3
C2.0
STABILIZED CONSTRUCTION ENTRANCE (BMP #5)
N.T.S.

**PRELIMINARY
NOT FOR
CONSTRUCTION**

STORM WATER POLLUTION PREVENTION PLAN

THE KETCHUM BOUTIQUE HOTEL

LOCATED WITHIN SECTION 18, T.4 N., R.18 E., B.M., CITY OF KETCHUM, BLAINE COUNTY, IDAHO

PREPARED FOR PEG KETCHUM HOTEL LLC

PROJECT INFORMATION
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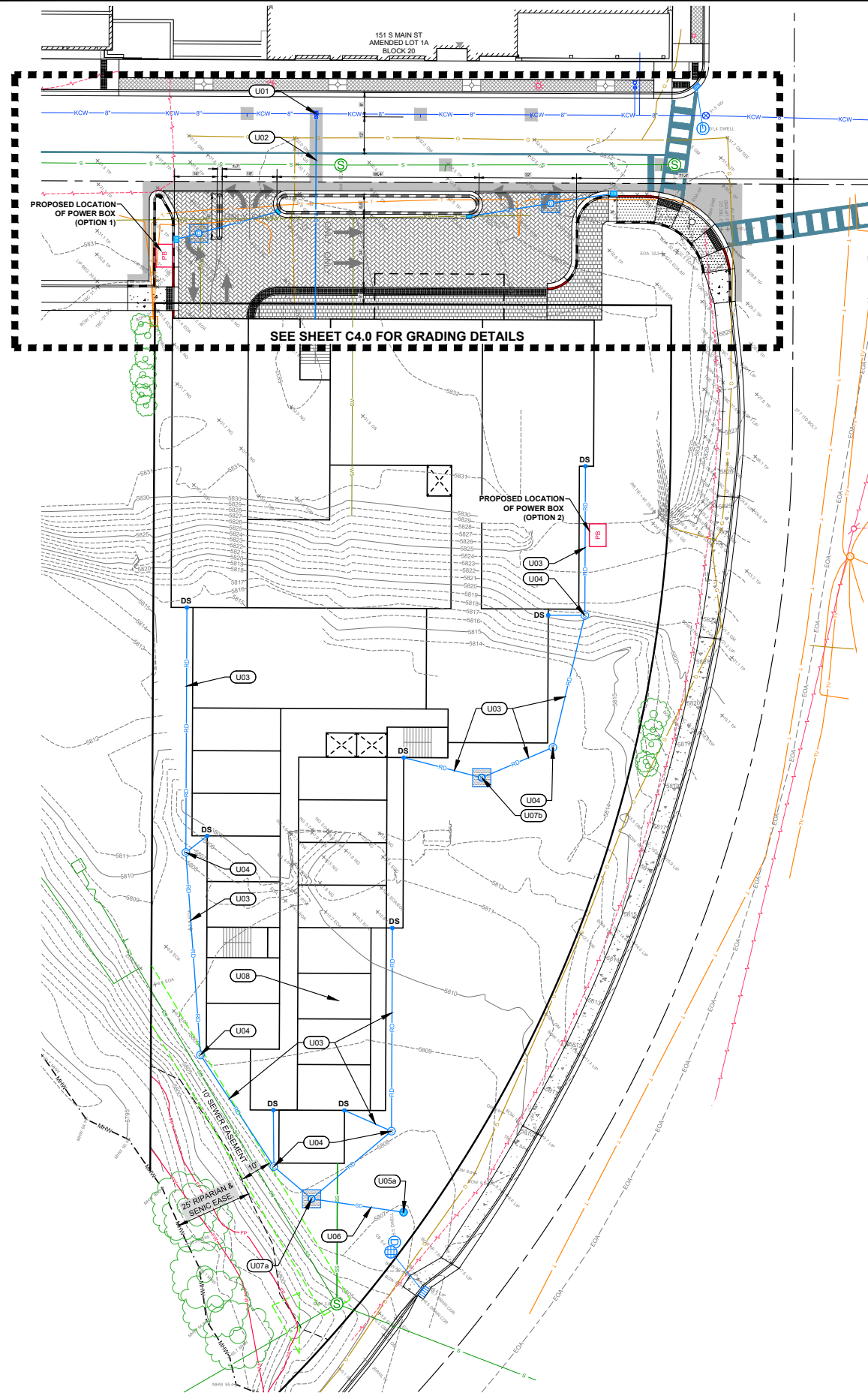
DESIGNED BY	CTJ/USMF
DRAWN BY	CTJ/USMF
CHECKED BY	CTJ/USMF

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Civil Engineers & Land Surveyors
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3875 3353
(208) 798-1705
(208) 798-4612 fax
email galena@galena-engineering.com

NO.	DATE	BY	REVISIONS

C2.0



UTILITY CONSTRUCTION SCHEDULE

- U01 INSTALL 6" D.I. TEE
8" GATE VALVE
W/ THRUST BLOCKS
- U02 INSTALL 6" WATER MAIN / SERVICE. SEE DETAIL 1
/ C0.2 FOR POTABLE AND NON-POTABLE WATER
LINE SEPARATION AND DETAIL 2 / C0.2 FOR
TRENCHING.
- U03 INSTALL 6" PVC ROOF DRAIN SYSTEM WITH
MINIMUM SLOPE OF 2.0%.
- U04 INSTALL 6" ROOF DRAIN CLEANOUT
- U05 INSTALL CATCH BASIN. SEE DETAIL 3 / C0.2.
a. RIM = 5806.91
INV. OUT = 5803.9
- U06 INSTALL 12" ADS N-12 STORM DRAIN PIPE WITH A
MINIMUM SLOPE OF 2.0%. SEE DETAIL 2 / C0.2
FOR TRENCHING.
- U07 CONSTRUCT DRYWELL. SEE DETAIL 4 / C0.2.
a. RIM = 5807.79
INV. IN = 5803.2
b. RIM = 5813.0
INV. OUT = 5809.0
- U08 INSTALL 8" PVC SEWER LINE / SERVICE.
CONNECT TO EXISTING SEWER MANHOLE.

UTILITY PLAN
THE KETCHUM BOUTIQUE HOTEL
 LOCATED WITHIN SECTION 18, T.4 N., R.18 E., B.M., CITY OF KETCHUM, BLAINE COUNTY, IDAHO
 PREPARED FOR PEG KETCHUM HOTEL LLC
 PROJECT INFORMATION
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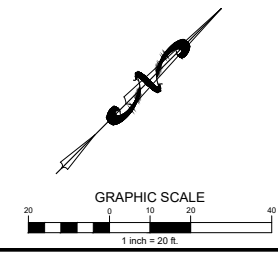
DESIGNED BY
 CTJ/JSMF
 DRAWN BY
 CTJ/JSMF
 CHECKED BY

REUSE OF DRAWINGS
 These drawings, or any portion thereof, shall not be used on any project or extension of any project without the written consent of GALENA ENGINEERING, INC.

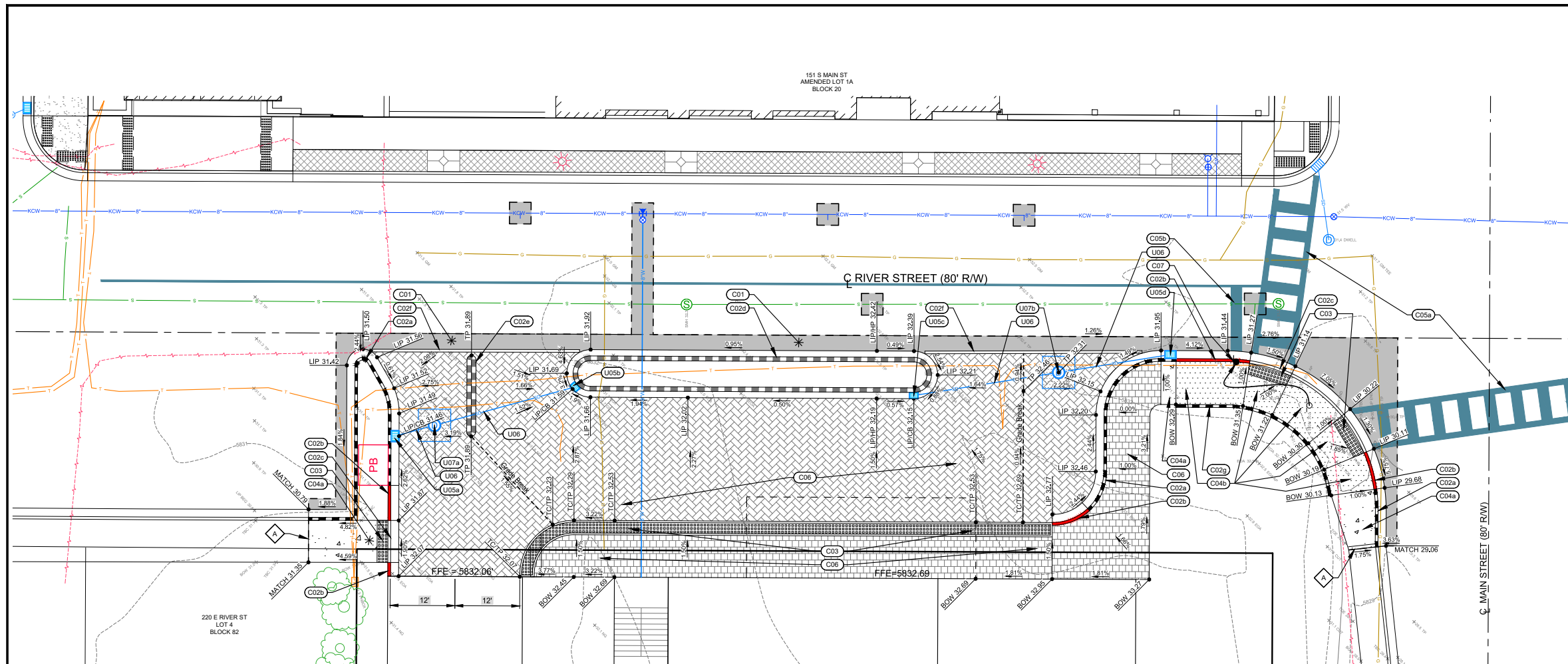
GALENA ENGINEERING, INC.
 Civil Engineers & Land Surveyors
 317 N. River Street
 Ketchum, ID 83841
 (208) 798-7205
 (208) 798-4612 fax
 email: galena@galena-engineering.com

NO.	DATE	BY	REVISIONS

C3.0



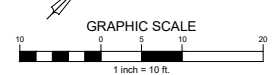
PRELIMINARY
 NOT FOR
 CONSTRUCTION



- UTILITY CONSTRUCTION SCHEDULE**
- U06 INSTALL CATCH BASIN. SEE DETAIL 3 / C0.2.
 - a. RIM = 5831.26
INV. OUT = 5828.2
 - b. RIM = 5831.39
INV. OUT = 5828.3
 - c. RIM = 5831.95
INV. OUT = 5828.9
 - d. RIM = 5831.68
INV. OUT = 5828.6
 - U07 INSTALL 12" ADS N-12 STORM DRAIN PIPE WITH A MINIMUM SLOPE OF 2.0%. SEE DETAIL 2 / C0.2 FOR TRENCHING.
 - U07 CONSTRUCT DRYWELL. SEE DETAIL 4 / C0.2.
 - a. RIM = 5831.60
INV. IN = 5827.7
 - b. RIM = 5832.21
INV. OUT = 5828.1

- CONSTRUCTION SCHEDULE**
- C01 CONSTRUCT ASPHALT ROADWAY / ASPHALT REPAIR. SEE DETAIL 9 / C0.2.
 - C02 CONSTRUCT CONCRETE CURB AND GUTTER.
 - a. 6" VERTICAL C&G PER DETAIL 5 / C0.2.
 - b. CURB TRANSITION PER DETAIL 6 / C0.2.
 - c. ZERO REVEAL CURB AND GUTTER PER DETAIL 6 / C0.2.
 - d. MOUNTABLE CURB AND GUTTER PER DETAIL 7 / C0.2.
 - e. MOUNTABLE CURB PER DETAIL 8 / C0.2.
 - f. CURB / PAVER EDGING PER DETAIL 12 / C0.2.
 - g. CURB AT BACK OF WALK. SEE DETAIL 14 / C0.2.
 - C03 INSTALL CITY OF KETCHUM APPROVED CAST IRON TRUNCATED DOME DETECTABLE WARNING INSERT. SEE DETAIL 13 / C0.2 FOR INSTALLATION IN AREAS WITHOUT CONCRETE SIDEWALK.
 - C04 CONSTRUCT CONCRETE SIDEWALK WIDTH AS SHOWN HEREON. SEE DETAIL 10 / C0.2.
 - a. FLAT WORK
 - b. ADA COMPLIANT RAMPS AND LANDING.
 - C05 INSTALL ROAD STRIPING / PAINT.
 - a. WHITE CROSSWALK STRIPING (12" WIDE BARS WITH 24" WIDE CROSS BARS WITH GLASS BEADS).
 - b. WHITE STOP BAR STRIPING (24" WIDE).
 - C06 INSTALL HEATED PAVER SIDEWALK / ROADWAY. SEE DETAIL 11 / C0.2.
 - C07 INSTALL STOP / STREET SIGN. USE PREVIOUSLY REMOVED SIGN.
 - △ MATCH EXISTING LINES AND GRADES

- LEGEND**
- | | | |
|-----------------------|---|---|
| Existing Items | <ul style="list-style-type: none"> -KCV-8" Ketchum City Water Line -W- Water Service -KCV-4" Abandoned Ketchum Spring Water -W- Abandoned Water Service -S- Sewer Line -SS- Sewer Service Conifer Tree Deciduous Tree ITD Bolt = Right-of-way Monument FD12 = Found 1/2" Rebar FD5/8 = Found 5/8" Rebar Found Aluminum Cap in Concrete Survey Control Found Nail in Stump Fire Hydrant Water Valve Frost Free Hydrant Catch Basin Drywell Sewer Manhole Sewer Cleanout Telephone Line Buried Power Line Overhead Power Line | <ul style="list-style-type: none"> Power Box Telephone Riser Cable Television Riser AC = Aluminum Cap AP = Angle Point BOLL = Bollard BOW = Back of Walk CC = Curb Cut COR = Corner EOA = Edge of Asphalt Line FH = Fire Hydrant FND = Found GM = Gas Main GS = Ground Shot INT = Intersection JERRAIL = Jersey Rail LIP = Lip of Gutter PB = Buried Power Line PBOX = Power Box PHB = Buried Telephone Line PHBOX = Telephone Riser PMTR = Power Meter PP = Power Pole SMH = Sewer Manhole TBC = Top Back of Curb TVB = Buried Cable Television Line TVBOX = Cable Television Riser |
|-----------------------|---|---|
- Proposed Items**
- | | |
|--|--|
| <ul style="list-style-type: none"> New Asphalt Asphalt To Be Removed Concrete Sidewalk ADA Pedestrian Approach Concrete 6" Vertical Curb Curb Transition Zero Reveal Curb & Gutter Retaining Wall Agfa Access Truncated Dome Sign Fire Hydrant Water Valve Grade Break 5' Contour Interval 1' Contour Interval Pavers Power Transformer | <ul style="list-style-type: none"> Concrete To Be Removed Area To Be Grubbed Drywell Storm Drain Roof Drain Catch Basin Down Spout Sawcut Line Approximate Limits Of Disturbance Road Paint Grade Spot Elevation Street Light Water Service / Main Sewer Service / Main TC = Top of Concrete TP = Top of Pavers |
|--|--|



PRELIMINARY
NOT FOR
CONSTRUCTION

RIGHT-OF-WAY IMPROVEMENT PLAN

THE KETCHUM BOUTIQUE HOTEL
LOCATED WITHIN SECTION 18, T.4 N., R.18 E., B.M., CITY OF KETCHUM, BLAINE COUNTY, IDAHO
PREPARED FOR PEG KETCHUM HOTEL LLC

PROJECT INFORMATION
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DESIGNED BY	CTJ/USMF
DRAWN BY	CTJ/USMF
CHECKED BY	CTJ/USMF

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email: galena@galena-engineering.com

NO.	DATE	BY	REVISIONS

C4.0

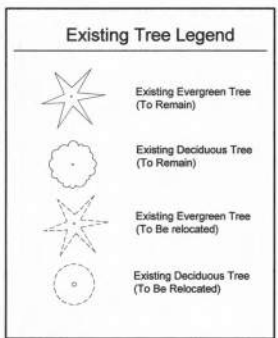
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EXHIBIT | 02.06

LANDSCAPE DRAWINGS

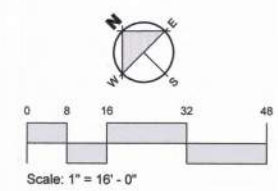
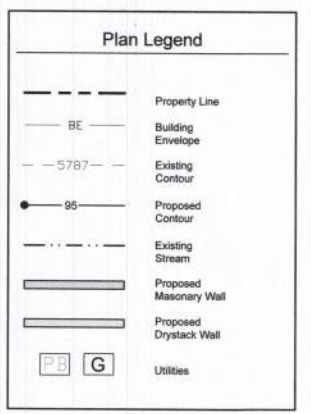
General Notes

1. Base map information taken from survey by Galena Engineering dated 06/03/19 and from on-site information. Architectural information provided by AJC Architects dated 06/03/19. Contractor shall verify conditions in the field prior to construction.
2. Landscape architect is not responsible for any deviation from these plans, unless such changes are authorized by the landscape architect in writing.
3. All existing utilities are underground. All new utilities shall be underground.
4. Site serviced by City of Ketchum.



Plant Legend

Qty	Abbr. Species	Size
13	Street Trees	4" Cal.
8	ABM Autumn Blaze Maple	
8	Shade Trees	3" Cal.
3	SM Silver Maple	
3	Conifer Trees	12' & 14'
3	CS Colorado Spruce	(1)14' & (2)12'
18	Medium Trees	2-1/2" Cal.
14	OA Quaking Aspen	(14) 2-1/2" Cal.
15	Flowering Trees	3" Cal.
5	CD Crabapple, Snowdrift	(5) 2-1/2" Cal.
142	Deciduous Shrubs	5 gal. & 1 gal.
10	BC Barberry, Crayon Pigmy	(10) 1 gal.
4	PC Peking Cotoneaster	(4) 5 gal.
31	ID Isanti Dogwood	(27) 5 gal.
13	Evergreen Shrubs	5 gal.
13	SY Spreading Yew	
725 sq.ft.	Perennial Flowers Various	Flats @ 8" O.C.
8,300 sq.ft.	Grasses Lawn Mix	Hydroseed
3,750 sq.ft.	Natural Grasses TBD	Hydroseed
615 sq.ft.	Tall/Ornamental Grasses TBD	1 gal. @ 12" O.C.



PRELIMINARY ONLY - NOT FOR CONSTRUCTION

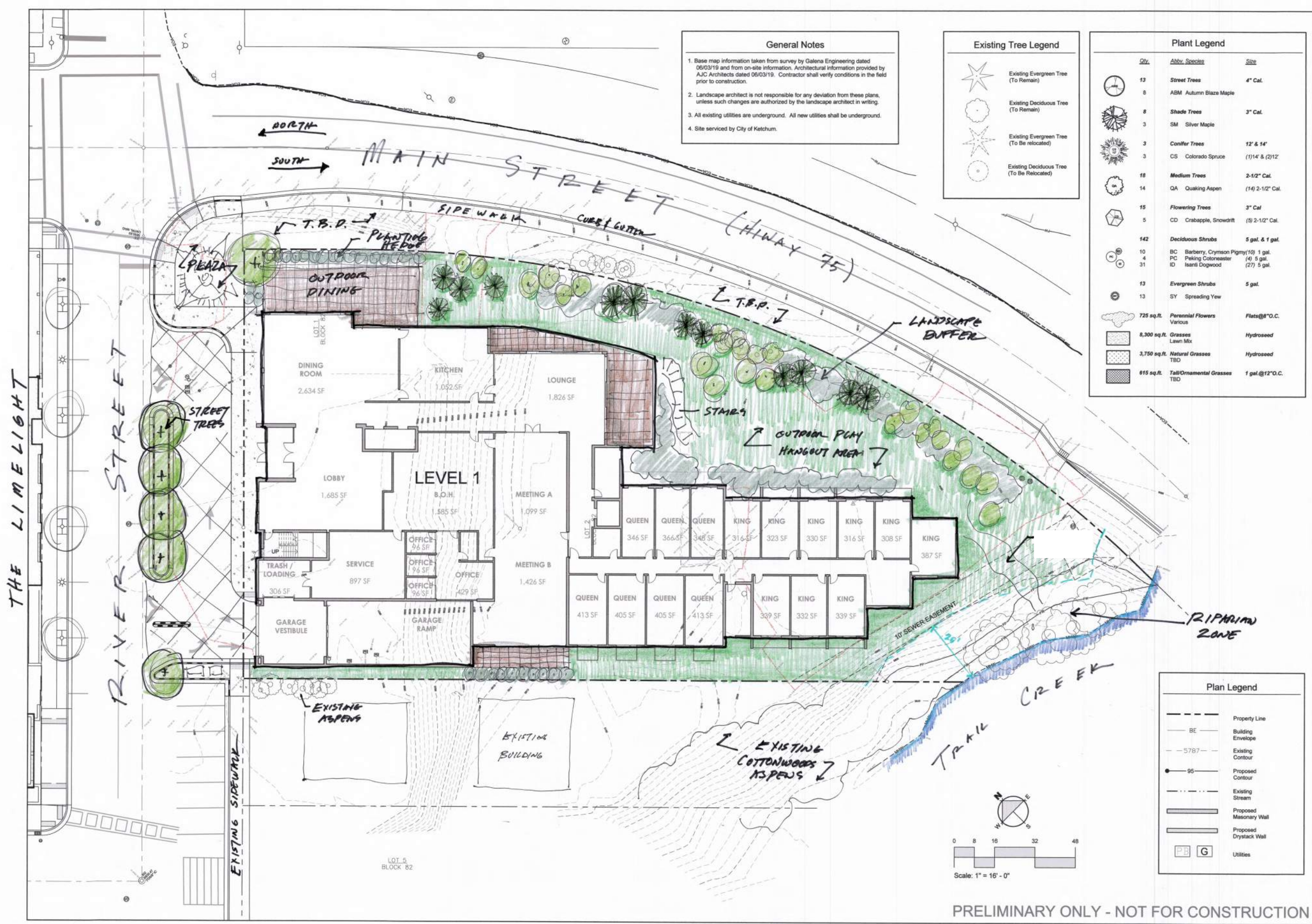


EXHIBIT | 02.07

EXTERIOR COLOR PALATE
SCHEMATIC MOOD BOARD



Ketchum Tribute Hotel

Exterior Inspiration Mood Board



EXHIBIT | 02.08

DARK SKY COMPLIANT FIXTURES

Project: _____
 Fixture Type: _____
 Location: _____
 Contact: _____

Two-Light Outdoor

Wall mounted • Damp location listed **PROGRESS LED**

Description:

5" up/down wall cylinder. The P5675 Series are ideal for a wide variety of interior and exterior applications including residential and commercial. The Cylinders feature a 120V alternating current source and eliminates the need for a traditional LED driver. This modular approach results in an encapsulated luminaire that unites performance, cost and safety benefits. Specify P860038 top cover lens for use in wet locations.

Specifications:

- Black (-31) (powder coat paint)
- Die cast and extruded aluminum Construction
- Powder coat finish.
- Die-cast aluminum construction with durable powder coated finish
- 795 lumens 46 lumens/watt per module
- 3000K color temperature, 90+ CRI
- Dimmable to 10% brightness (See Dimming Notes)
- Back plate covers a standard 4" octagonal recessed outlet box
- Mounting strap for outlet box included
- 6" of wire supplied
- Meets California Title 24 high efficacy requirements for outdoor use only

Performance:

Number of Modules	2
Input Power	17w per module
Input Voltage	120 V
Input Frequency	60 Hz
Lumens/LPW (Delivered)	830/24.4 (LM-79)
CCT	3000 K
CRI	90 CRI
Life (hours)	60,000 (L70/TM-21)
FCC	FCC Title 47, Part 15, Class B
Min. Start Temp	-30 °C
Max. Operating Temp	30 °C
Warranty	5 year warranty
Labels	cCSAus Damp location listed Meets California Title 24 high efficacy requirements for outdoor use only

P5675-31/30K

Images:



Dimensions:

Width: 5"
 Depth: 8-7/8"
 Height: 14"
 H/CTR: 8"

TUBE - model: WS-W26
LED Wall Mount



PRODUCT DESCRIPTION

Precise engineering using the latest energy efficient LED technology with a built-in reflector for superior optics; An appealing cylindrical profile with a powerful LED down light perfect for accent and wall wash lighting.

FEATURES

- IP65 Rated, ETL & cETL Wet Location Listed
- Mounts up or down
- Die-Cast Aluminum Construction
- Universal Voltage Input (120V - 277V)
- Dimming: ELV (120V) or 0-10V

WAC LIGHTING
Responsible Lighting®

Fixture Type:
 Catalog Number:
 Project: _____
 Location: _____

SPECIFICATIONS

Construction: Aluminum with etched glass.
Power: Integral driver in luminaire. 120V - 277V input.
Light Source: High output LED
Mounting: Mounts directly to junction box
Dimming: 0-10V Dimming: 100%-10%
 ELV Dimming: 100%-15% (120V only)
Finish: Brushed Aluminum (AL), Black (BK), Bronze (BZ), Graphite (GH), White (WT)
Color Temp: 3000K
CRI: 90
Rated Life: 70,000 hours
Standards: IP65 Rated, ETL & cETL Wet Location Listed, Dark Sky Friendly.

Beam	Photometry	Voltage	Watt	Lumens	Finish
Single Light	WS-W2605 Dark Sky Friendly Energy Star®	120V - 277V	16W	800	AL Brushed Aluminum BK Black BZ Bronze GH Graphite WT White
Double Light	WS-W2604	120V - 277V	30W	1750	



Example: **WS-W2604-BZ**

WAC Lighting www.waclighting.com Phone (800) 526.2588 • Fax (800) 526.2585	Headquarters/Eastern Distribution Center 44 Harbor Park Drive • Port Washington, NY 11050 Phone (516) 515.5000 • Fax (516) 515.5050	Western Distribution Center 1750 Archibald Avenue • Ontario, CA 91760 Phone (800) 526.2588 • Fax (800) 526.2585
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WAC Lighting retains the right to modify the design of our products at any time as part of the company's continuous improvement program. SEP 2017

RFS Design 158 LFP

Project Information

Quick Facts	
Applications:	Trail / Pathway / Street
Distribution Types:	IES Type 2, 3, 4, and 5
Color Temperature:	3,000K - 4,000K
Lumen Range	4,194 - 4,711
Mounting Height:	25'
Battery Type:	Long-Life Lithium Iron Phosphate (LFP)
Finish (Standard Color):	Black
Warranty:	10-Year System & 10-Year Battery

Overview

The RFS Design 158 LFP system is a popular choice for residential street lighting, parks, and for maximizing spacing on residential/intermediate pathways and bikeways. With its patented solar "skin", this light pole generates and safely stores its own energy. This creates a sleek appearance that is not possible with traditional flat panel systems. Because of its vertical orientation, the panel is inherently protected from hail damage and doesn't collect dirt and debris.

Not only will you hardly notice the solar collector, but you'll hardly notice the maintenance. This system includes our Lithium Phosphate battery packs which have been field tested in Arizona and carry a 10-year warranty.

The RFS Design 158 LFP is about more than aesthetics:

- Graffiti washes off the ETFE coating on the collector with soap and water.
- The battery compartment is protected with a high security bolt system.
- Since the collector doesn't use crystalline cells, it can literally take a beating.

Design

SolarOne® will support your planning or specification process with energy balance calculations, lighting design, and a shading evaluation specific to your project location. This Virtual Site Analysis (VSA) is custom for every project so you can see the details behind the recommendation.

Our analysis is based on industry standards such as IES & CIE, credible climate data sources like NASA & NREL, and over ten years of LED Solar Powered Lighting field experience with municipalities, military bases, and industrial facilities.

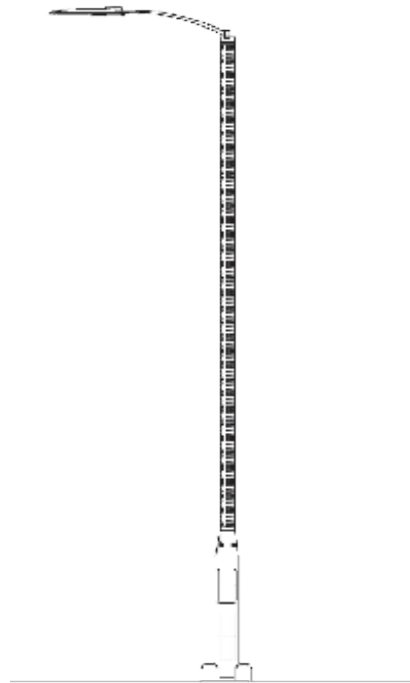
Product Matrix

RFS - D158 - L -

Model	Light Pattern Type	Color Temperature	Motion Detector	House Side Shield
	R2M - Type II Medium	4K - 4,000K	MD-Optional Adder	HS-Optional Adder
	R2S - Type II Short	3K - 3,000K	None-Leave Blank	None-Leave Blank
	R3M - Type III Medium			
	R3S - Type III Short			
	4 - Type IV			
	5 - Type V			

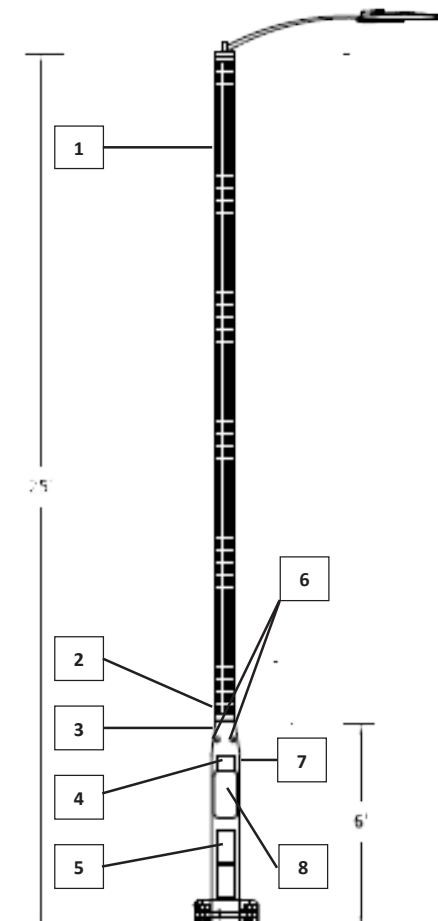
SolarOne® Solutions Inc • www.solarone.net
 220 Reservoir St - Suite 19 • Needham, MA 02494
 PH: 339-225-4530 • FAX: 339-225-4539
 © 2017

RFS Design 158 LFP

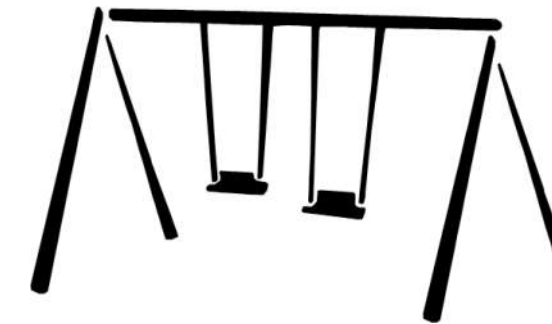


DATA SHEET 758-5009-00

Product Details



1. The patented approach to adhering the solar skin to the surface of the pole creates best in class aesthetics.
 - Solar collector is vandal resistant due to bypass diodes and durable ETFE covering.
 - Solar panel is certified to standard UL 1703.
 - Sun-facing, wrapped 2250 around body of pole.
 - Solar collector is self-cleaning due to vertical orientation.
2. Quality components built to last include marine grade wiring and conformal coating of the energy management system.
3. All components integrated inside a secured compartment within the base of the pole eliminating the need for an external battery box.
4. Patented computerized energy management system guarantees maximum battery life.
5. Long-Life, Lithium Iron Phosphate (LFP) batteries significantly lower operating and maintenance costs with an expected 10 - 12 year life.
6. Motion detection (optional).
7. T6 powder coated spun aluminum pole -170 mph wind rating.
8. High security access bolt with one of a kind encrypted access bit licensed only to you.



Thin Film Solar Panel



The thin-film solar panel wraps around the pole 2250 for maximum solar collection. Because of the patented design it looks like a traditional street light pole. Besides being visually attractive the panel is also extremely tough.

- ASTM tested to withstand the impact of 1 inch diameter balls at 52 mph.
- The Tefzel coating makes the collector able to withstand most forms of vandalism.
- Flexible material adheres to the pole with a commercial grade adhesive and retention strips with supporting anodized aluminum rivets.
- Vertical design reduces wind loading, is self-cleaning and avoids weight loads from snow.



SolarOne Solutions Inc • www.solarone.net
 220 Reservoir St - Suite 19 • Needham, MA 02494 • PH: 339-225-4530 • FAX: 339-225-4539
 © 2017

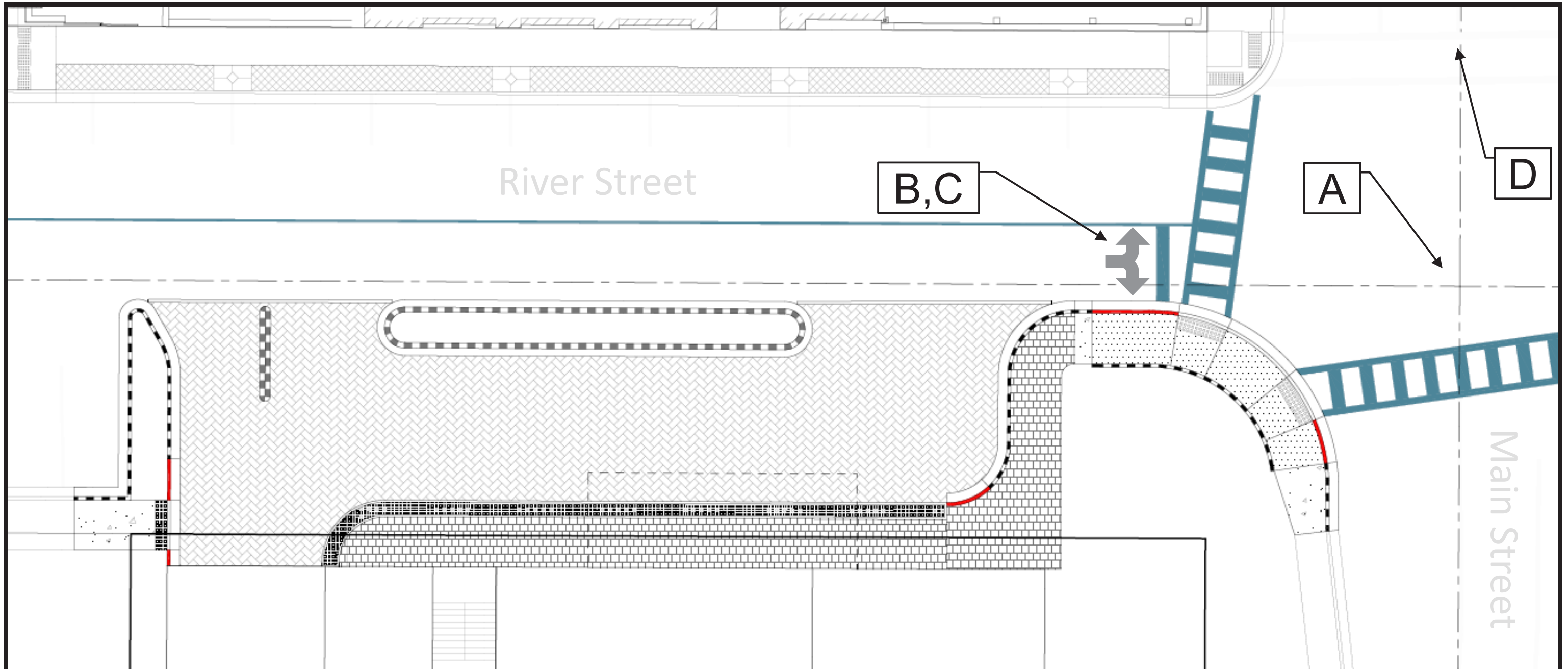
RFS Design 158 LFP

DATA SHEET 758-5009-00

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EXHIBIT | 02.09

TRAFFIC STUDY DIAGRAMS

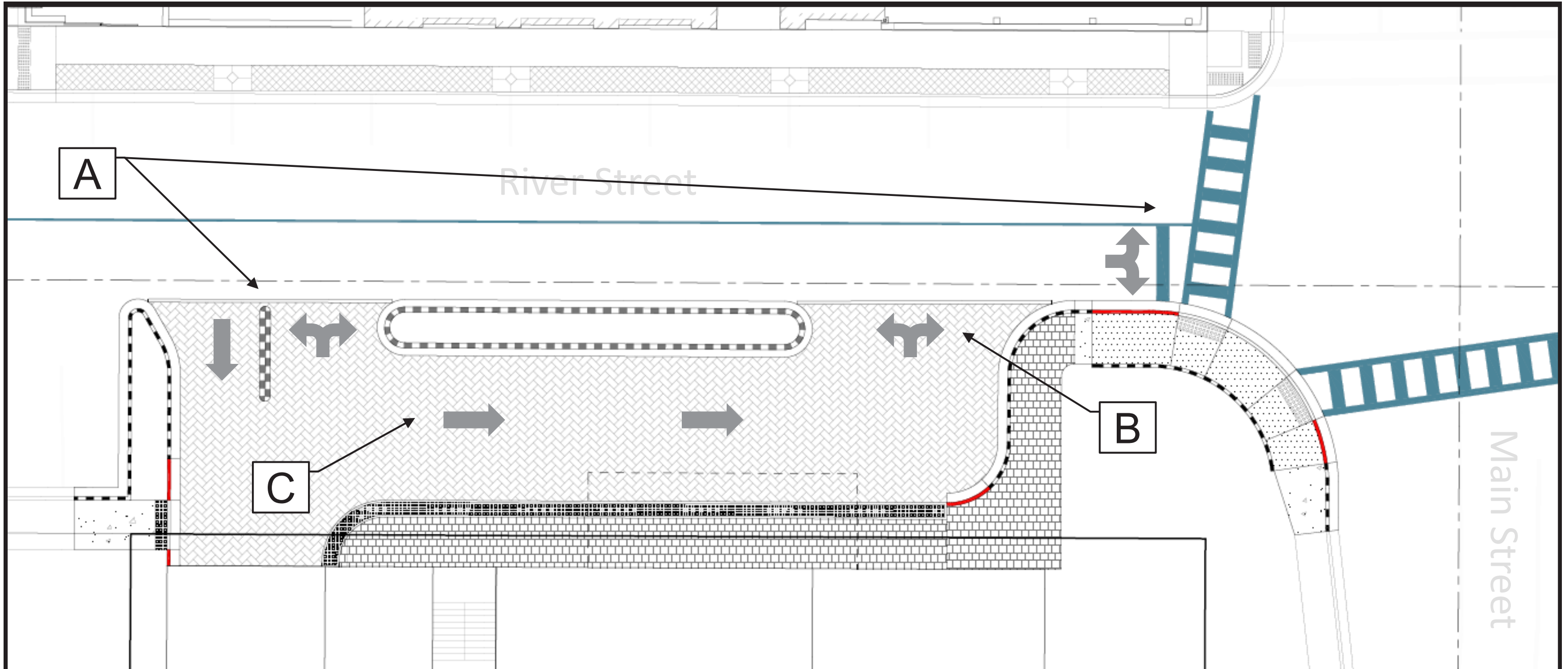


A. Near-term vehicle delays and queueing at River Street / Main Street at acceptable levels

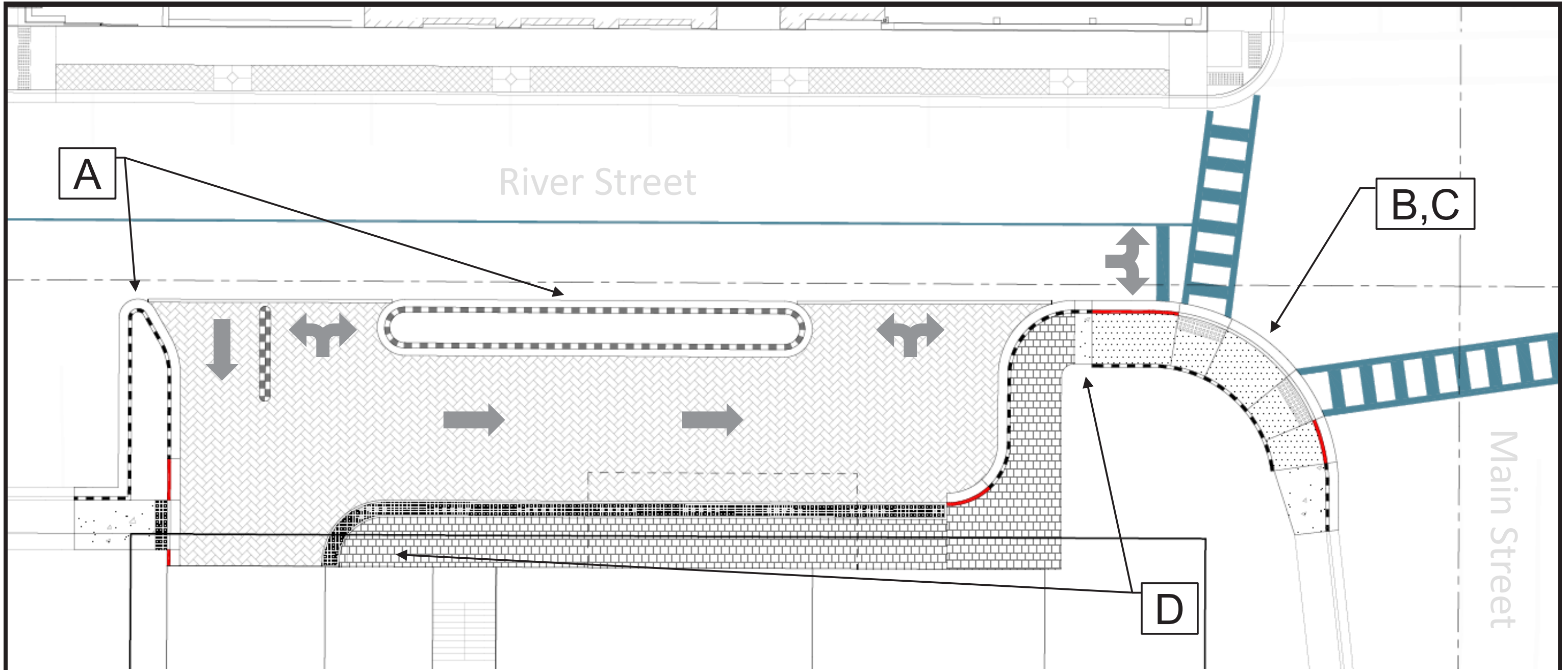
B. Left-turns from River Street onto Main Street will be difficult in future (2040) conditions with or without the project-generated traffic

C. The City may consider restricting left-turn movements at River Street / Main Street

D. Converting Main Street to a three-lane cross section will facilitate turn movements from River Street to Main Street.



- A. Distance between primary access and Main Street is maximized to minimize interference with traffic flow at intersection
- B. Secondary access is for egress movements only from the hotel drop off area
- C. Counterclockwise circulation pattern allows for right-side drop off / pickup



- A. Curb Line to Define Traveled Way
- B. Defined Curb Radius
- C. Pedestrian Ramps
- D. Complete Sidewalk Connection between Washington Avenue and River Street

Ketchum Gateway Hotel

Project-Related Improvements

HALES ENGINEERING
innovative transportation solutions

EXHIBIT | 02.10

PLAT MAP | LOT LINE ADJUSTMENT



City of Ketchum
Planning & Building

OFFICIAL USE ONLY
File Number:
Date Received:
By:
Fee Paid:
Approved Date:
Denied Date:
By:

Lot Line Shift Application

OWNER INFORMATION	
Owner Name:	PEG Ketchum Hotel LLC
Mailing Address:	180 N University Ave, Ste. 200, Provo, UT 84601
Phone:	801-655-1998
Email:	nblayden@pegcompanies.com
PROJECT INFORMATION	
Name of Proposed Plat:	Lot 3A, Block 82, Ketchum Townsite
Representative of Owner:	Sean Flynn / Galena Engineering
Phone:	(208) 788-1705
Mailing Address:	317 N River St Hailey ID 83333
Email:	sflynn@galena-engineering.com
Legal Land Description:	Lots 1, 2, 3, and fraction of 21 and 22, Block 82, Ketchum
Project Address:	260 East River Street
Number of Lots:	1
Total Land Area in Square Feet:	47,249
Overlay District:	<input checked="" type="checkbox"/> Flood <input type="checkbox"/> Mountain <input type="checkbox"/> Avalanche
Easements to be Dedicated on the Final Plat (Describe Briefly):	none
ATTACHMENTS	
Attachments Necessary to Complete Application:	
1. A copy of a current lot book guarantee and recorded deed to the subject property;	
2. One (1) copy of preliminary plat; and,	
3. A CD or email of an electronic (.pdf) of the plat.	

Applicant agrees in the event of a dispute concerning the interpretation or enforcement of the Lot Line Shift Application, in which the City of Ketchum is the prevailing party, to pay reasonable attorney fees, including attorney fees on appeal, and expenses of the City of Ketchum. I, the undersigned, certify that all information submitted with and upon this application form is true and accurate to the best of my knowledge and belief.

Sean Flynn

Signature of Owner/Representative
Sean Flynn / Galena Engineering

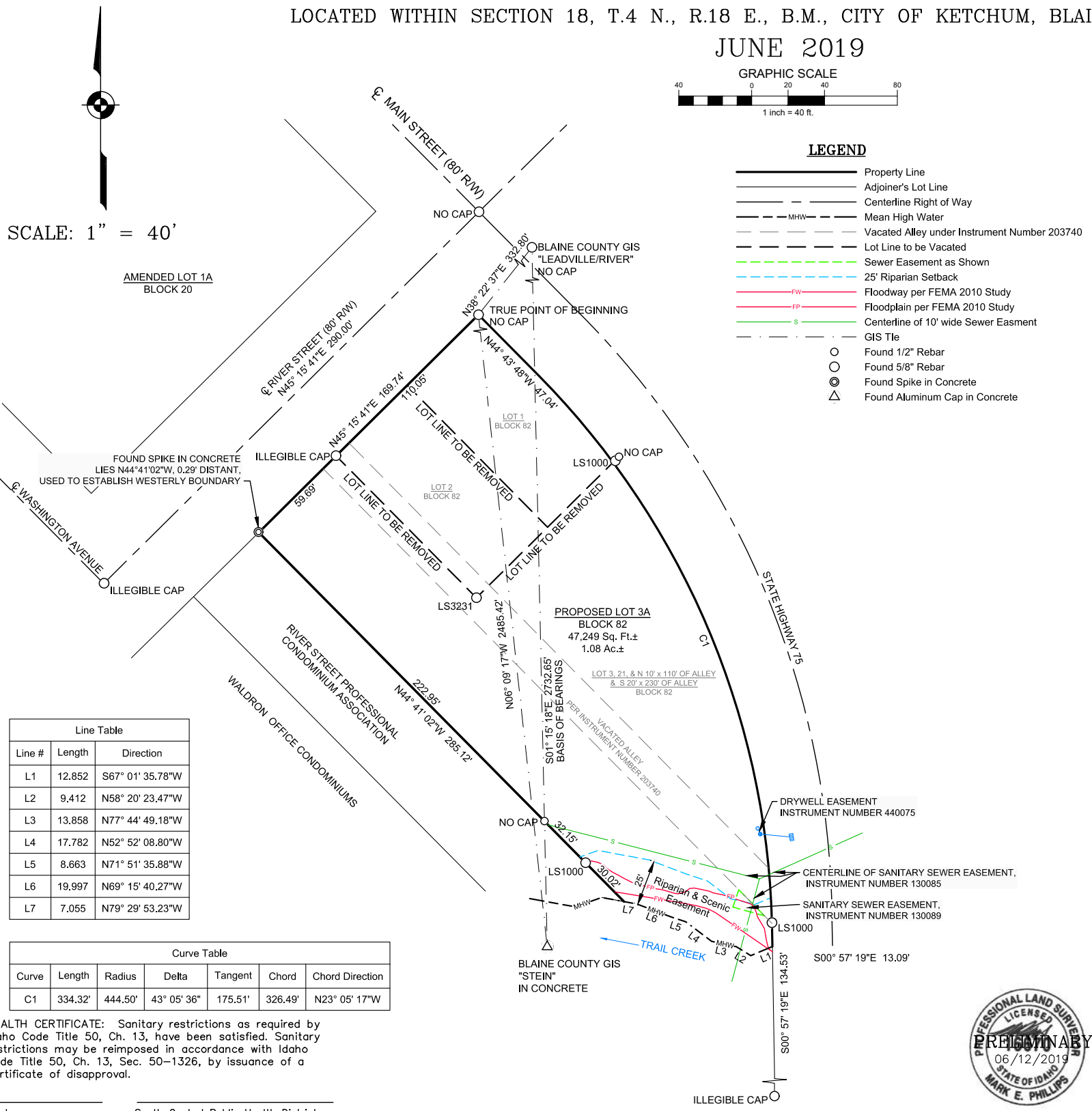
06/15/19

Date

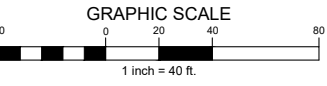
A PLAT SHOWING LOT 3A, BLOCK 82, KETCHUM TOWNSITE

WHEREIN THE LOT LINES OF LOTS 1 & 2, BLOCK 82, ARE VACATED, CREATING LOT 3A
LOCATED WITHIN SECTION 18, T.4 N., R.18 E., B.M., CITY OF KETCHUM, BLAINE COUNTY, IDAHO

JUNE 2019



SCALE: 1" = 40'



LEGEND

- Property Line
- Adjoiner's Lot Line
- - - Centerline Right of Way
- - - MHW
- - - Mean High Water
- - - Vacated Alley under Instrument Number 203740
- - - Lot Line to be Vacated
- - - Sewer Easement as Shown
- - - 25' Riparian Setback
- - - Floodway per FEMA 2010 Study
- - - Floodplain per FEMA 2010 Study
- - - Centerline of 10' wide Sewer Easement
- Found 1/2" Rebar
- Found 5/8" Rebar
- ⊙ Found Spike in Concrete
- △ Found Aluminum Cap in Concrete

Line Table		
Line #	Length	Direction
L1	12.852	S67° 01' 35.78"W
L2	9.412	N58° 20' 23.47"W
L3	13.858	N77° 44' 49.18"W
L4	17.782	N52° 52' 08.80"W
L5	8.663	N71° 51' 35.88"W
L6	19.997	N69° 15' 40.27"W
L7	7.055	N79° 29' 53.23"W

Curve Table						
Curve	Length	Radius	Delta	Tangent	Chord	Chord Direction
C1	334.32'	444.50'	43° 05' 36"	175.51'	326.49'	N23° 05' 17"W

HEALTH CERTIFICATE: Sanitary restrictions as required by Idaho Code Title 50, Ch. 13, have been satisfied. Sanitary restrictions may be reimposed in accordance with Idaho Code Title 50, Ch. 13, Sec. 50-1326, by issuance of a Certificate of disapproval.

Date _____ South Central Public Health District

SURVEY NARRATIVE & NOTES

1. The purpose of this survey is to vacate the lot lines of Lots 1 & 2, Block 82, Ketchum Townsite, creating Lot 3A, Block 82, Ketchum Townsite, and show the monuments found during the boundary retracement of the following parcels of land:

- Lot 1 in Block 82, of the Village of Ketchum, as shown on the certified copy of the official map thereof, recorded as Instrument No. 302967, records of Blaine County, Idaho,
- Lot 2 in Block 82 of the Village of Ketchum, as shown on the certified copy of the official map thereof, recorded as Instrument No. 302967, records of Blaine County, Idaho; together with the Northeastly one-half (1/2) of the alley in Block 82, adjacent and contiguous to Lot 2, Block 82, Original Townsite of Ketchum, Idaho,
- Lot 3 and all that portion of Lots 21 and 22 lying westerly of State Highway 75, in Block 82 of the Village of Ketchum, as shown on the certified copy of the official map thereof, recorded as Instrument No. 302967, records of Blaine County, Idaho; together with the Southwestly one-half (1/2) of the alley in Block 82, adjacent and contiguous to Lot 3, Block 82 and the Northeastly one-half (1/2) of the alley in Block 82, adjacent and contiguous to Lots 21 and 22, Original Townsite of Ketchum, Idaho.

The Boundary shown is based on found centerline monuments, found lot corner monuments, and the following documents:

- The Village of Ketchum, Instrument Number 302967, records of Blaine County, Idaho.
- The Record of Survey for Ketchum Medical Clinic, Instrument Number 315647, records of Blaine County, Idaho.
- The Idaho Transportation Department plans for the State Highway.

The Southerly Boundary along Trail Creek is a Riparian Boundary, based on the Village of Ketchum plat, which will accrete and relic in time, following the northerly bank of Trail Creek. The current Mean High Water is shown as the Riparian Boundary.

Additional documents used in the course of this survey include;

- River Street Professional Condominium Association, Instrument Number 269006, records of Blaine County, Idaho.
- Replat of Block 83, Original Town of Ketchum, Instrument Number 210798, records of Blaine County, Idaho.
- Record of Survey showing A Portion of Lot 22, Block 82, & Tax Lot 6072, Instrument Number 421738, records of Blaine County, Idaho.
- Trail Creek Crossing Condominiums, Instrument Number 536460, records of Blaine County, Idaho.
- Apsey Subdivision, Instrument Number 180600, records of Blaine County, Idaho.
- A Replat of Andora Villa No. 3 Common Area, Instrument Number 448860, records of Blaine County, Idaho.
- Andora Villa No. 3, Instrument Number 186909, records of Blaine County, Idaho.
- Record of Survey for Darryl McMillen, Instrument Number 189027, records of Blaine County, Idaho.
- State Highway Right of Way Deeds

2. At this time discrepancies exist along the State Highway 75 boundary based on found monuments and the above listed surveys. ITD does not currently have a Land Surveyor on staff to be able to resolve said discrepancies. Based on their schedule for an upcoming Highway improvement project, a Surveyor for ITD will be under contract by mid-July. When the ITD Surveyor is under contract, we will coordinate with them and possibly adjust the boundary along State Highway 75. The boundary shown hereon along State Highway 75 is in the most conservative location per all of the identified information. If a change is made in the future, it will push to the east, giving Lot 3A more area.

3. This survey does not purport to reflect any of the following which may be applicable to subject real property: natural hazards, encroachments, wetlands, easements, building setbacks, restrictive covenants, subdivision restrictions, zoning or any other land-use regulations.



LOT 3A, BLOCK 82
KETCHUM TOWNSITE

GALENA ENGINEERING, INC.
HAILEY, IDAHO

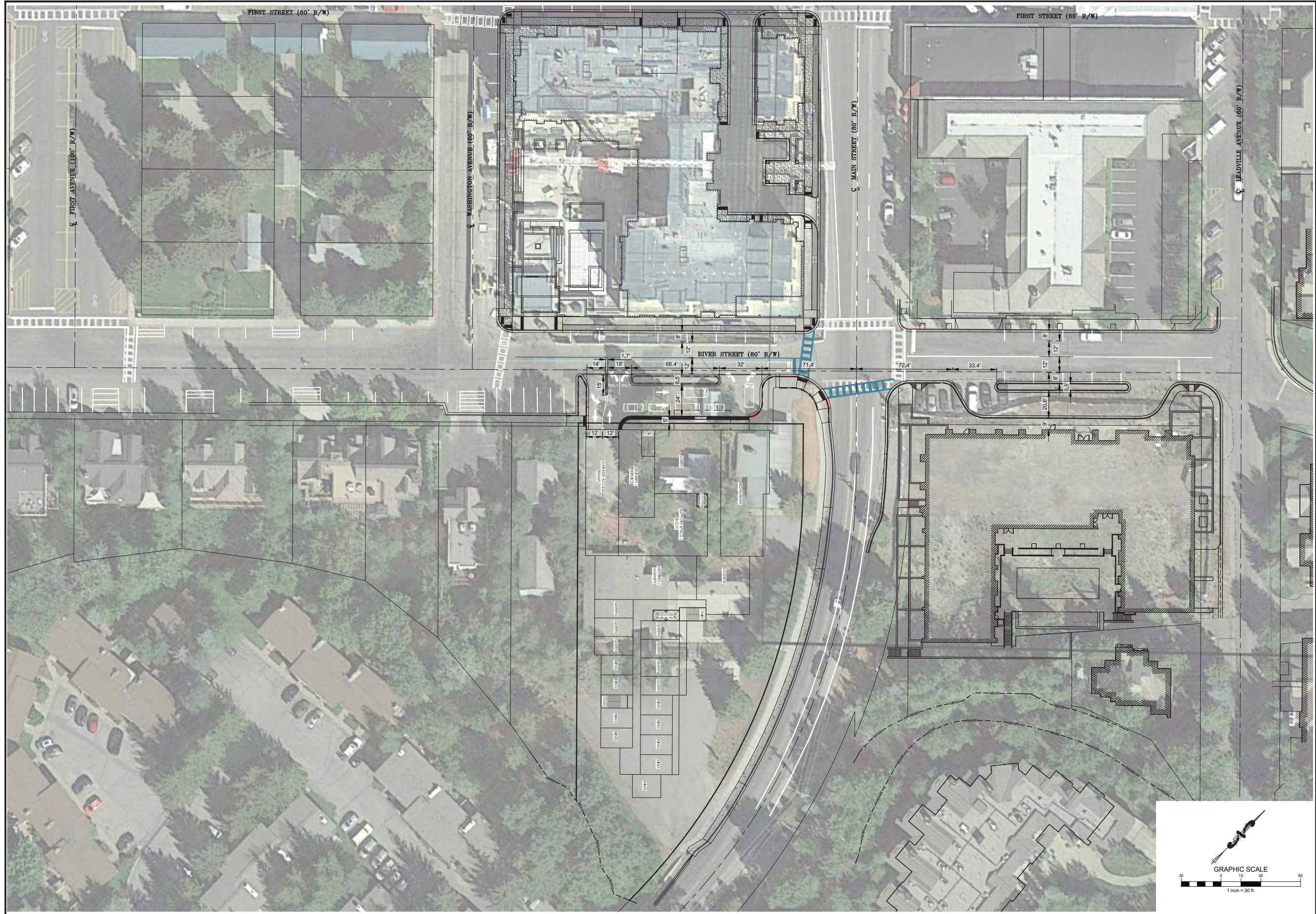
SHEET 1 OF 2

Job No. 6003-05

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EXHIBIT | 02.11

PUBLIC WAY IMPROVEMENTS



GATEWAY CORNER SITE MAP

THE KETCHUM GATEWAY HOTEL

LOCATED WITHIN SECTION 18, T. 4 N., R. 18 E., B.M., CITY OF KETCHUM, BLAINE COUNTY, IDAHO
PREPARED FOR PEG KETCHUM HOTEL LLC

PROJECT INFORMATION
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<p>REUSE OF DRAWINGS These drawings, or any portion thereof, shall not be used on any project or for any other purpose without the written agreement in writing with Galena Engineering, Inc.</p> <p>GALENA ENGINEERING, INC. Civil Engineers & Land Surveyors 317 N. River Street Ketchum, ID 83841 (208) 788-1705 (208) 788-4612 fax email: galena@galena-engineering.com</p>	<p>DESIGNED BY CT</p> <p>DRAWN BY SMF</p> <p>CHECKED BY</p>
---	---

NO.	DATE	BY	REVISIONS

EX1.0

PUBLIC WAY IMPROVEMENTS

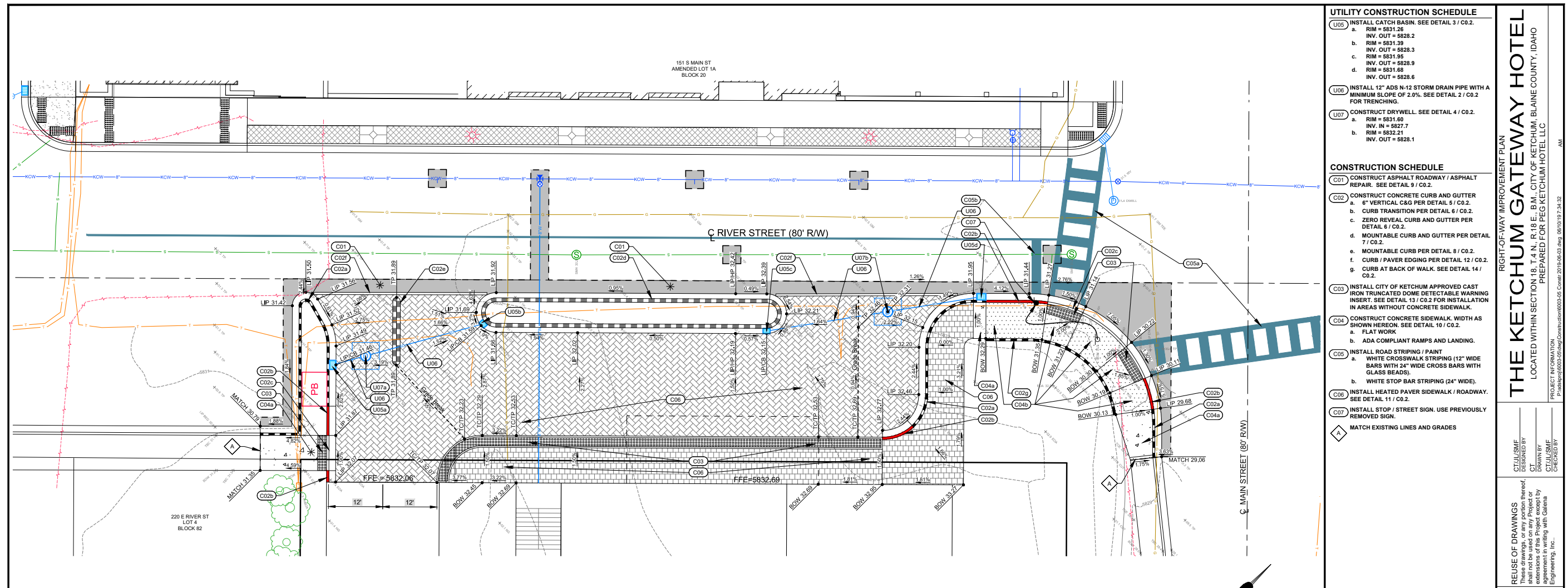
RIVER STREET

River Street is currently a low-traffic street with unimproved frontage and gravel surface parking along the hotel property. Hotel improvements will include a new guest drop-off area separated from the street with a fully landscaped island. This paved area will be heated with a radiant system, and fully maintained by the operations team of the hotel. The flow of this entry and corner was designed to mirror the recently approved hotel project on the opposite side of Main Street.

Current plans for outdoor dining and patio space on the corner of River and Main will be designed to screen the diners from traffic, and provide a comfortable transition from Ketchum City into the lobby of the hotel.

MAIN STREET | ID-75

Our project will be coordinating directly with Idaho Department of Transportation on planned construction improvements of Main Street / State Highway 75. Plans to widen the street and bridge in this location to provide dedicated turn lanes will be considered with all site improvements along our east property line and landscape terracing will be integrated with any grading revisions along this road.



UTILITY CONSTRUCTION SCHEDULE

U01 INSTALL CATCH BASIN. SEE DETAIL 3 / C0.2.

a. RIM = 5831.26
INV. OUT = 5828.2

b. RIM = 5831.39
INV. OUT = 5828.3

c. RIM = 5831.95
INV. OUT = 5828.9

d. RIM = 5831.68
INV. OUT = 5828.6

U02 INSTALL 12" ADS N-12 STORM DRAIN PIPE WITH A MINIMUM SLOPE OF 2.0%. SEE DETAIL 2 / C0.2 FOR TRENCHING.

U07 CONSTRUCT DRYWELL. SEE DETAIL 4 / C0.2.

a. RIM = 5831.60
INV. IN = 5827.7

b. RIM = 5832.21
INV. OUT = 5828.1

CONSTRUCTION SCHEDULE

C01 CONSTRUCT ASPHALT ROADWAY / ASPHALT REPAIR. SEE DETAIL 9 / C0.2.

C02 CONSTRUCT CONCRETE CURB AND GUTTER

a. 6" VERTICAL C&G PER DETAIL 5 / C0.2

b. CURB TRANSITION PER DETAIL 6 / C0.2

c. ZERO REVEAL CURB AND GUTTER PER DETAIL 6 / C0.2

d. MOUNTABLE CURB AND GUTTER PER DETAIL 7 / C0.2

e. MOUNTABLE CURB PER DETAIL 8 / C0.2

f. CURB / PAVER EDGING PER DETAIL 12 / C0.2

g. CURB AT BACK OF WALK. SEE DETAIL 14 / C0.2

C03 INSTALL CITY OF KETCHUM APPROVED CAST IRON TRUNCATED DOME DETECTABLE WARNING INSERT. SEE DETAIL 13 / C0.2 FOR INSTALLATION IN AREAS WITHOUT CONCRETE SIDEWALK.

C04 CONSTRUCT CONCRETE SIDEWALK. WIDTH AS SHOWN HEREON. SEE DETAIL 10 / C0.2.

a. FLAT WORK

b. ADA COMPLIANT RAMPS AND LANDING.

C05 INSTALL ROAD STRIPING / PAINT

a. WHITE CROSSWALK STRIPING (12" WIDE BARS WITH 24" WIDE CROSS BARS WITH GLASS BEADS).

b. WHITE STOP BAR STRIPING (24" WIDE).

C06 INSTALL HEATED PAVER SIDEWALK / ROADWAY. SEE DETAIL 11 / C0.2.

C07 INSTALL STOP / STREET SIGN. USE PREVIOUSLY REMOVED SIGN.

A MATCH EXISTING LINES AND GRADES

RIGHT-OF-WAY IMPROVEMENT PLAN

THE KETCHUM GATEWAY HOTEL

LOCATED WITHIN SECTION 18, T.4 N., R.18 E., B.M., CITY OF KETCHUM, BLAINE COUNTY, IDAHO

PREPARED FOR PEG KETCHUM HOTEL, LLC

PROJECT INFORMATION

DESIGNED BY: CTJLJ/SMF

DRAWN BY: CTJLJ/SMF

CHECKED BY: CTJLJ/SMF

REUSE OF DRAWINGS

These drawings, or any portion thereof, shall not be used on any project or extension of this project except by permission of the City of Ketchum, Idaho, or the City of Ketchum Engineering, Inc.

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EXHIBIT | 02.12

SOILS REPORT | PERC TEST

BUTLER ASSOCIATES, INC.

GEOTECHNICAL & CIVIL ENGINEERING & CONSULTING

P.O. Box 1034

Ketchum, Idaho 83340

Phone: 208.720.6432

Email: svgeotech@gmail.com

Eric Peterson

C/o PEG Development

180 N. University Ave.

Suite 200

Provo, UT 84601

E: epeterson@pegdev.com

O: 435-647-6344

December 7, 2018

RE: SOIL & FOUNDATION INVESTIGATION PROPOSAL

Proposed Gateway Project

251 South Main Street

Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite

Ketchum, Idaho

Dear Eric,

I have completed the authorized geotechnical investigation and report for your proposed residence on Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite in Ketchum, Idaho. You authorized the work with the signed proposal dated August 30, 2018.

This report summarizes the results of my field and laboratory testing and presents my geotechnical engineering opinions and recommendations. **It is my opinion that the site is suitable for the proposed Gateway Project supported by continuous and spread footings, retaining walls and slab-on-grade foundations on a structural fill foundation building pad and/or an approved native subgrade excavated into the existing slope.** I am providing the recommendations in this report for the preparation of the subgrade, structural fill building pad, foundation design, foundation drainage system, lateral loading, surface grading and drainage and general radon venting concepts.

I recommend that this office be retained to provide observations for the construction of the structural fill foundation building pad, foundation drainage system and any other recommendations presented in this report that are incorporated into the project design. This work will be performed on a time and material basis and is not included in this scope of services. A copy of this geotechnical report should be incorporated into the project construction documents. I appreciate this opportunity of working with you on this project. Please call me if you have any questions or comments.

Sincerely,

Steve Butler, P.E.



GEOTECHNICAL REPORT

Proposed Gateway Project

Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite

251 South Main Street

Ketchum, Idaho

Butler Associates, Inc.

P.O. Box 1034

Ketchum, Idaho 83340

December 7, 2018

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INTRODUCTION

This report represents the results of the soil and foundation engineering evaluation for the proposed Gateway Project on Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite in Ketchum, Idaho. The *Vicinity Map* shows the general location of the proposed project site.

The purpose of this evaluation was to assess the surface and subsurface soil and water conditions to prepare geotechnical engineering opinions and recommendations for the construction of the proposed residence. Before the subsurface investigation I reviewed the geotechnical report for the Limelight Hotel, Argyros Performing Arts Center and the proposed Auberge of Sun Valley Hotel located directly north, northwest and east of the site, respectively and geologic data pertinent to the site and general area. I performed a subsurface investigation by excavating nine test pits at the site using a track mounted excavator. The soils encountered in the test pits were visually identified and logged by a geotechnical engineer according to the Unified Soil Classification System and used to prepare this final report.

PROPOSED PROJECT

I understand that there are no completed building plans to review at this time but proposed project will probably consist of a steel, wood-frame and concrete five story, hotel that also includes two stories of underground parking excavated into the existing slope supported by continuous and spread footings, retaining walls and slab-on-grade construction. The elevation of the lowest garage slab-on-grade will be close to the elevation of the existing asphalt parking lot adjacent to Trail Creek. The parking garage will be accessed off of River Street and will be supported by slab-on-grade construction. I understand that the lowest footings will several feet above the elevation of Trail Creek of 5796 feet so the site will not have to be dewatered to construct the foundations.

The primary views from the site are Griffin Butte and the Wood River valley to the north, Trail Creek drainage and Dollar Mountain to the east, The Wood River Valley to the south and Bald Mountain to the west.

According to the Blaine County Parcel Information Map the property is generally "rectangular shaped" and totals approximately 1.05 acres in size. River Street borders the lot to the north, State Highway 75 to the east, Trail Creek to the south and Lot 4, Block 82, Ketchum Townsite to the west.

FIELD EXPLORATION

Nine test pits were excavated and observed at the site on October 26, 2018 using a track-mounted excavator. Although the site contained several existing structures, utilizes and temporary construction job-site trailers that restricted access I believe the soils exposed in the test pits represent the general classification and engineering properties of the native soils throughout the site. The test pits were excavated up to 8.5 feet below existing grade. The *Test Pit Site Plan* shows the property lines, existing building footprints, existing contours and test pit locations.

The soils in each test pit were evaluated and the soil profiles logged in the field by a geotechnical engineer in accordance with the Unified Soil Classification System (USCS). The *Test Pit Site Plan*, *Test Pit Site Plan Photos* and *Test Pit Logs* are presented on pages 10, 11-13 and 14-22, respectively. The USCS chart on page 23 should be used to interpret the terms on the test pit logs in this report.

At the conclusion of the subsurface evaluation, the test pits were loosely backfilled to match the existing ground surface. Any of the test pits located beneath areas proposed for foundations, terraces, walkways or driveways will need to be excavated and backfilled with structural fill in accordance with the *Site Preparation* section of this report.

SUBSURFACE CONDITIONS

The general soil profiles encountered in test pits revealed up to 7.0 feet of uncontrolled fill consisting of silty fine sand and gravel with trace roots & debris overlying native sand and gravel up to 8.5 feet below existing grade. The test pits were terminated after reaching several feet into the competent native sand and gravel soils, due to the consistency of the soil between the test pits and the similar soil profiles that were encountered in the foundation excavations for the projects referenced in the *Introduction* section.

The geology of this area is mapped on the "Geologic Map of the Hailey Quadrangle" as sand, gravel, cobble and boulder soils classified as alluvial soils deposited by Trail Creek and the Big Wood River. The uncontrolled fill was the result of the construction of the previous improvements.

OPINIONS AND RECOMMENDATIONS

General

It is the opinion of this office that the site is suitable from a geotechnical standpoint for the proposed development of the hotel project and underground parking garage excavated into the existing slope supported by an approved native subgrade or a free-draining structural fill foundation building pad constructed on an approved native subgrade after the existing structures are demolished and exported. All site runoff should be collected and directed to either a storm water retention system located under the parking garage slab-on-grade or in a series of drywells located south of the structure.

All structural fill to be placed for the foundation building pad, exterior terraces, walkways and driveways should be approved native or imported sand and gravel soils. The on-site moisture sensitive fine grain soils should not be used as structural fill. All structural fill should be placed as outlined in the *Structural Fill* section of this report. The recommendations contained in this report reflect my understanding of the existing surface and below grade conditions and reflect a straight-line interpolation and extrapolation of the subsurface conditions between and beyond test pit locations. However, the soil conditions may vary at the proposed site. The various soil conditions will not be known until the foundation excavation is complete and may cause changes to construction plans and/or costs.

Temporary Excavation Slopes

The native undisturbed, compact sand and gravel soils underlying the uncontrolled fill is classified as Class C soils by OSHA. Temporary excavation slopes in Class C soils should be 1.5':1.0' (horizontal to vertical). However, based on my experience with undisturbed native sand and gravel soils in the Wood River Valley the temporary excavation slopes can be excavated up to 0.75:1.0. This office should observe the temporary slopes to confirm the soil type and stability during the foundation excavation. If the native soils are stable I will complete a letter addressing OSHA recommendations and the temporary slopes that should be kept on file in the project office. The contractor should consider installing a

construction fence above the edge of the excavation to keep persons/animals from falling into the excavation. Any uncontrolled fill exposed in the temporary excavation slopes should be removed or excavated to 1.5:1.0.

Subgrade Preparation & Structural Fill Foundation Building Pad

Following are site preparation recommendations to be completed prior to approving the subgrade for footings and/or the construction of the structural fill foundation building pad to support the footings:

1. All test pits should be accurately located in the field prior to commencing with the excavation. Any test pit that is located beneath a proposed footing, slab-on-grade, terrace or walkway adjacent to the structure should be excavated and backfilled with structural fill in accordance with this report. This procedure should help reduce local settlement. The test pit locations are shown on the *Test Pit Site Plan*.
2. The building footprint, exterior terraces, walkways and limits of disturbance should be stripped of the uncontrolled fill, fine grain topsoil and organics to expose the underlying native undisturbed sand and gravel soils. The excavated material should be exported due to the limited space on the site.
3. The excavation to bottom of the footing subgrade should be completed. This office should observe the exposed footing subgrade to determine the suitability of the soils.
4. The exposed subgrade should be watered and compacted with multiple passes of a smooth drum roller to at least 95% of the maximum dry density of the material according to ASTM D-698. Any soft areas encountered once the subgrade is compacted should be over-excavated to expose a competent subgrade and backfilled with structural fill as outlined in the *Structural Fill* section.
5. Following compaction efforts the subgrade should be proof-rolled with a loaded dump truck or front loader for the final observation by this office. Once the subgrade has been determined to be compacted and free of unsuitable soils it will be approved for footings.

Structural Fill

Structural fill for the foundation building pad, retaining walls, walkways, exterior terraces and the driveway shall meet the following recommendations:

1. Structural fill should consist of approved washed fractured or rounded gravel, crushed sand and gravel (roadmix) or pitrun sand and gravel classified as GW, GM, GP, SW, SM, or SP as described in the Unified Soil Classification System chart presented after the test pit logs. On-site sand and gravel soils could be used as structural fill but should be approved by this office.
2. If native sand and gravel soils are used as structural or non-structural fill against the foundation walls at least 2 feet of imported 1"-2" dia. washed gravel should be installed from bottom of footing to top of footing to assist in subsurface runoff in reaching the foundation drains. See the *Retaining Wall Foundation Drainage Profile*.

3. Granular structural fill should have no more than 10% passing the No. 200 sieve and a cobble size of no larger than 8 inches.
4. Structural fill should be placed in uniform, maximum 10-inch deep, loose lifts and compacted to a minimum of 95% of the maximum dry density of the soil, as determined by ASTM D 698 (Standard Proctor). This assumes that heavy compaction equipment such as smooth-drum, vibratory rollers with a minimum drum weight of 5 tons is used. The depth of each lift could be specified in other sections of this report.
5. The maximum loose lift thickness should be reduced to 6 inches where smaller and/or lighter compaction equipment is used (i.e. WACKER jumping jack). A vibrating plate tamper can be used to compact 10" lifts of washed rock but should not be used to compact native silty clay.
6. ¾" minus crushed sand and gravel roadmix should be placed in 4"-6" loose lifts, watered and compacting with a jumping jack tamper, vibrating plate tamper or smooth drum roller.
7. The general contractor should arrange for this office to observe the compaction of each lift or establish a schedule to provide adequate documentation for the building department and allow the construction observation report to be stamped by an engineer.

Foundations

The native sand and gravel soils or an imported gravel structural fill foundation building pad constructed on an approved native subgrade will support continuous footings, spread footings and slab-on-grade construction based on the following parameters:

1. The allowable bearing pressure of the native sand and gravel or imported gravel structural fill foundation building pad constructed on an approved foundation subgrade is 5,000 pounds per square foot (psf).
2. Exterior footings should be at least 36 inches below finish grade to minimize the potential for frost heave.
3. Total and differential settlement is estimated to be less than one inch and ¼", respectively, for the structural fill building pad on an approved sand and gravel subgrade.
4. The recommended friction factor is 0.50 for the native sand and gravel or imported gravel foundation building pad.
5. The floor joists and sub-floor should be in-place prior to backfilling against the foundation walls unless directed otherwise by the structural engineer.
6. All footings should be constructed so that a line drawn from the edge of footings at a slope of 0.5 foot horizontal to 1.0 foot vertical to the undisturbed subgrade soil is not intercepted by non-structural fill or an open slope. See *Structural Fill/Foundation Subgrade Concepts Profile* for details.

Retaining Walls

The following design parameters assume that proper drainage will maintain a fully drained environment behind the walls for the life of the structure with a level backfill at least 10 feet behind the structure:

1. The recommended equivalent active lateral earth pressure is 35 pounds per cubic foot (pcf) equivalent fluid pressure (efp). Active pressure design is based on the top of the wall moving.
2. The recommended equivalent at-rest lateral earth pressure is 55 pounds per cubic foot (pcf) equivalent fluid pressure (efp). Active pressure design is based on the top of the wall moving.
3. The recommended equivalent passive lateral earth pressure is 500 pcf, efp.
4. The floor joists and sub-floor should be in-place prior to backfilling against the retaining walls or as directed by the structural engineer.
5. Footings adjacent to retaining walls should be structurally connected to the retaining walls.

All retaining walls should be waterproofed as follows:

1. Retaining walls should be covered with a waterproof membrane and a synthetic drainage mat that is installed to the bottom of footing. The drainage mat will both direct groundwater to the footing drain and will also protect the waterproofing membrane.
2. If washed, free-draining gravel is used as backfill against retaining walls then the synthetic drainage mat can be omitted and replaced with a cheaper protection board that will protect the waterproofing membrane as the free-draining gravel is installed.
3. A synthetic drainage mat is not necessary if free-draining gravel is used exclusively as backfill against the retaining walls since the gravel will allow groundwater to reach the footing drain to relieve hydrostatic pressures.
4. See the *Retaining Wall Drainage Profile* for waterproofing and drainage design concepts.

If the groundwater is allowed to rise above the base of the footings then the hydrostatic pressures will increase the lateral earth pressures by 62 pcf per vertical foot of wall.

All backfill should be placed as directed in the *Structural Fill* section.

1" to 2", rounded or fractured, washed drain rock has several advantages if used as structural fill against retaining walls. The lateral pressures against the retaining wall from smaller compaction equipment (i.e. vibrating plate tamper) used for compacting the washed gravel will be less than that of a hoe-pack or smaller smooth steel drum roller that should be used for compacting imported or on-site sand and gravel soils. The drain rock also provides an excellent free draining medium and eliminates the need (and cost) for a synthetic drainage mat. The washed gravel is not self-compacting and should be placed in 12-inch loose lifts and compacted with a vibrating plate tamper.

Structural fill for footings adjacent to retaining walls should be placed to provide an envelope under footings, patios and walkways so that a line drawn from the edge of footings or walkways at a slope of 0.5 foot horizontal to 1.0 foot vertical to the undisturbed subgrade soil is not intercepted by non-structural fill or an open slope. See the *Structural Fill-Foundation Subgrade Concepts* detail.

Soil Classification for Septic Design

The project will be served by the Ketchum Utilities Department so no test pits were excavated for designing a septic system drainfield.

Surface Grading and Drainage

This office has not reviewed the grading and drainage plan at the time this report was completed. The grading and drainage plan should incorporate the following concepts based on the soils encountered in the test pits.

1. I recommend that the surface adjacent to the proposed structure be sloped away from the foundation at a minimum of 2% to prevent ponding and to direct surface water runoff away from foundations, terraces, patios and walkways.
2. All roof down spouts, foundations drains, catch basins and surface runoff should be directed to the storm water system located either beneath the parking garage slab-on-grade or downslope (south) of the structure.
3. During the backfill operations a solid 6" or 8" diameter PVC pipe sloped at 2% should be installed around the entire foundation a minimum of 3 feet below finish grade (for frost protection). Stub-outs for downspouts and catch basins should be installed in the pipe for future hook-ups. See the *Retaining Wall Foundation Drainage Profile* for details.
4. All drain lines connected to the 6" PVC solid pipe should be covered with at least 2 feet of soil cover for frost protection. No drain lines should be installed under the driveway since the frost depth can extend to over 5 feet below grade.
5. All drywells shown on the landscape plan located upslope the structure should be converted to catch basins and connected to the solid PVC pipe.
6. The native sand and gravel soils have an infiltration rate of 2.0"/ minute. See the *Storm Water Drywell Profile* for details.
7. This office can complete and/or review a storm water system at your request.

Driveway, Terraces and Walkways

I recommend the following section for asphalt driveways, terraces and walkways of either pavers or exterior concrete slabs in order to minimize frost action and settlement. The driveway section is designed to allow for an exposed gravel driving surface during construction before the final asphalt driving surface is installed:

1. The hardscape alignments should be cut to at least 12" below finish grade and/or to remove all roots, organics, uncontrolled fill, disturbed native soils and topsoil. The exposed sand and gravel subgrade should be watered and compacted with a 5-ton vibratory smooth drum roller to at least 95% of the maximum dry density of the material according to ASTM D-698. Any soft areas should be excavated to a competent subgrade and replaced with compacted structural fill as outlined in the *Site Preparation* section.
2. All parking areas, terraces and walkways should be constructed so that a line drawn from the edge of walkways or driveways at a slope of 0.5 foot horizontal to 1.0 foot vertical to the undisturbed subgrade soil is not intercepted by non-structural fill or an open slope. See the *Structural Fill-Foundation Subgrade Concepts Profile*.
3. The 6 inches of imported pitrun sand and gravel or 2" minus crushed sand and gravel roadmix sub-base watered and compacted with multiple passes of a smooth drum roller to at least 95% of the maximum dry density of the soil as determined by ASTM Test D-698 (Standard Proctor). On-site native sand and gravel could be used as structural fill but should be reviewed by this office.

4 inches of ¾" minus, crushed sand and gravel roadmix compacted to at least 95% of the maximum dry density of the soil as determined by ASTM Test D-698 (Standard Proctor).
4. Pavers, asphalt or concrete.
5. Surface Driveway runoff should not be allowed to drain onto River Street. I recommend that a catch basin with a cast iron ring and grate be installed in the driveway to direct runoff to the storm water system. The catch basin located in the driveway will allow it to be plowed free of snow and ice.

A minimum of 4 inches of ¾", well graded, crushed sand and gravel (road mix) base course should be placed between the pit-run sub-base and the finish walking surface. This will provide a leveling course and distribute point loads. If the fill for the driveway, terraces, and walkways are completed before the finish surface is constructed any structural fill should be compacted if the surfaces are exposed over a winter since the material will experience frost heave and reach a loose state.

The ¾" minus roadmix should be installed just prior to placing the finish asphalt surface so it does not become contaminated during construction of the residence.

Seismicity

The general subsurface soil conditions are consistent with 2012 International Building Code. The latitude and longitude of the project site are 43.68°N and 114.36° W, respectively.

Site Soil Classification: Class C

Risk Category: I/II/III

S_s = 0.593 g

S₁ = 0.171 g

S_{ms} = 0.690 g

S_{m1} = 0.279 g

See the USGS Design Maps Summary Report for details.

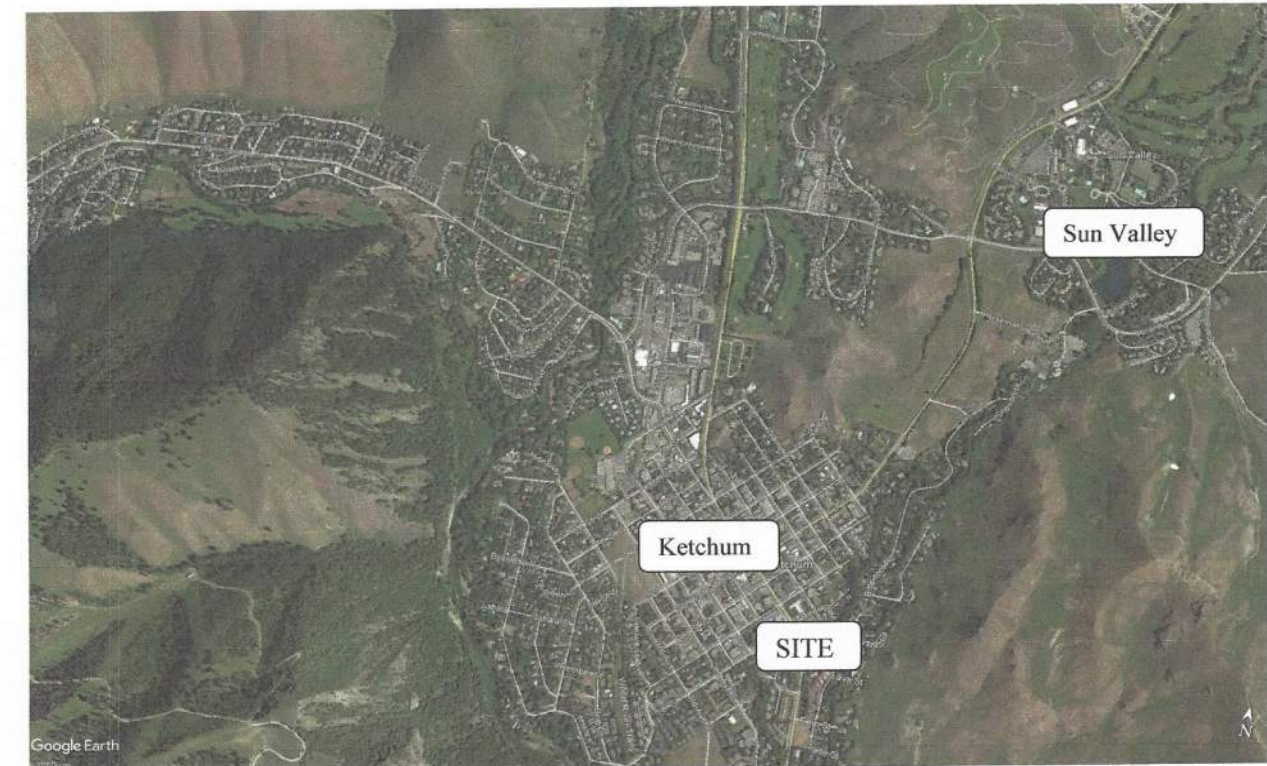
Radon Venting

This office is not qualified to complete a radon venting system design so the following venting concepts are guidelines. The radon system should be reviewed with a radon venting contractor.

Blaine County has a history of radon gas collecting in crawlspaces and under slab-on-grades. Radon gas is a byproduct of the natural breakdown of uranium that accumulates in improperly sealed basements and crawl spaces. These radon levels can exceed safety standards as set by the EPA. According to the State Radon Contact the most accurate testing results are gathered in the structure after construction. A 4-inch dia. perforated pipe placed in the leveling gravel beneath slab-on-grade construction can be connected to venting fans to reduce radon levels. It is important to create an airtight seal between all concrete slabs and adjacent walls. Consulting an experienced contractor or radon-venting specialist can ensure a fairly inexpensive system is installed during construction compared to potentially expensive remedial measures. See *Radon System Concepts Plan* for general design suggestions. I recommend installing sleeves in interior footings to allow the perforated pipe to be directed through the footings to the mechanical room for eventual venting through the roof.

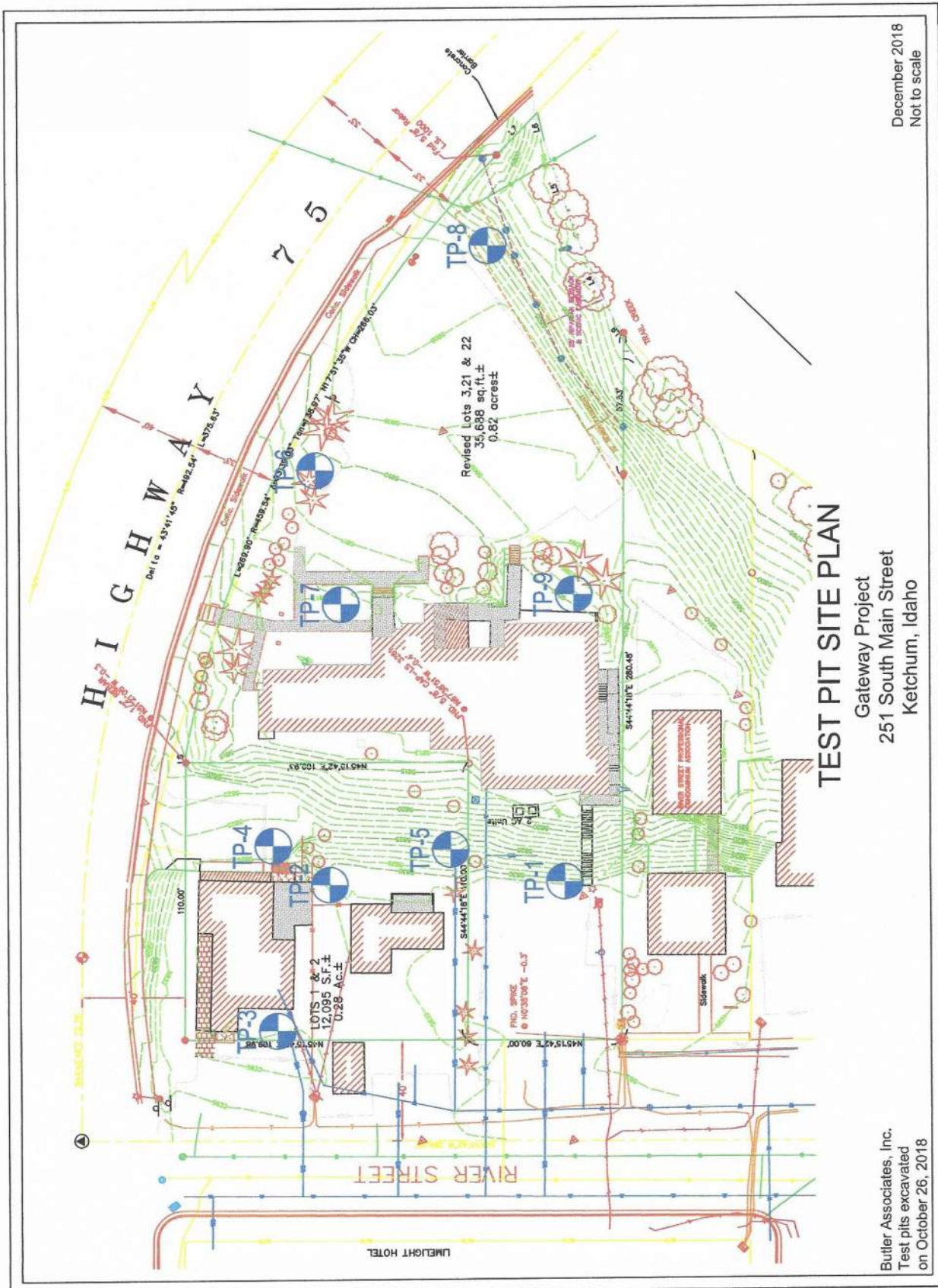
CONSTRUCTION OBSERVATION AND MONITORING

This report provides opinions and recommendations that are generally accepted geotechnical engineering principle and practices. I recommend that this office provide construction monitoring and observation services in order to ensure that the recommendations outlined in this report are followed and that the foundation drainage system and grading and drainage details are constructed properly. If this office is not retained to perform the recommended services, I cannot be responsible for soil engineering construction errors or omissions. The costs for the recommended services are not included with this report and would be incurred on a time and expense basis.



VICINITY MAP

Proposed Gateway Project
Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
251 South Main Street
Ketchum, Idaho
43° 40' 41" 114° 21' 42"



TEST PIT SITE PLAN PHOTO 1

Proposed Gateway Project
 Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
 251 South Main Street
 Ketchum, Idaho
 Image captured on September 28, 2018



TEST PIT SITE PLAN PHOTO 2

Proposed Gateway Project
Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
251 South Main Street
Ketchum, Idaho
Image captured on September 28, 2018



TEST PIT SITE PLAN PHOTO 3

Proposed Gateway Project
Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
251 South Main Street
Ketchum, Idaho
Image captured on September 28, 2018

EXPLORATORY TEST PIT #1
 Proposed Gateway Project
 Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
 251 South Main Street
 Ketchum, Idaho

DEPTH (Feet)	USCS SOIL CLASS	SOIL DESCRIPTION
0.0'-2.4'	GM	Silty fine SAND & GRAVEL, trace Debris & Roots (UNCONTROLLED FILL) Brown, loose dry.
2.4-3.0'	SM	Silty fine SAND & GRAVEL, trace Roots (NATIVE) Brown, loose, damp.
3.0'-7.5'	GW	SAND, GRAVEL, COBBLE & BOULDER, trace Silt & Clay (NATIVE) Brown, compact-dense, damp.

Test Pit completed on October 26, 2018.
 See *Test Pit Site Plan* for test pit location.
 The approximate test pit surface elevation is 5831 feet based on the topographic survey by Galena Engineering, Inc.
 No groundwater encountered.
 Test pit terminated at 7.5' below existing grade due to the consistency of the soils between the test pits after reaching several feet below typical crawlspace footing elevation.
 No soil sample retrieved.
 Sloughing of test pit walls in sand and gravel soils.
 Excavation equipment: DEERE track-mounted excavator.



EXPLORATORY TEST PIT #2
 Proposed Gateway Project
 Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
 251 South Main Street
 Ketchum, Idaho

DEPTH (Feet)	USCS SOIL CLASS	SOIL DESCRIPTION
0.0'-1.4'	SM	Silty fine SAND, little Gravel, trace Roots (NATIVE) Dark brown, loose dry.
1.4'-7.8'	GW	SAND, GRAVEL, COBBLE & BOULDER trace Silt, Roots to 4 feet (NATIVE) Brown, compact-dense, damp.

Test Pit completed on October 26, 2018.
 See *Test Pit Site Plan* for test pit location.
 The approximate test pit surface elevation is 5831 feet based on the topographic survey by Galena Engineering, Inc.
 No groundwater encountered.
 Test pit terminated at 7.8' below existing grade due to the consistency of the soils between the test pits after reaching several feet below typical crawlspace footing elevation.
 No soil sample retrieved.
 Sloughing of test pit walls in sand and gravel soils.
 Excavation equipment: DEERE track-mounted excavator.



EXPLORATORY TEST PIT #3
 Proposed Gateway Project
 Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
 251 South Main Street
 Ketchum, Idaho

DEPTH (Feet)	USCS SOIL CLASS	SOIL DESCRIPTION
0.0'-2.1'	SM	Silty fine SAND, little Gravel, trace Roots (UNCONTROLLED FILL?) Brown, loose dry.
2.1'-7.8'	GW	SAND, GRAVEL, COBBLE & BOULDER (NATIVE) Brown, compact-dense, dry.

Test Pit completed on October 26, 2018.
 See *Test Pit Site Plan* for test pit location.
 The approximate test pit surface elevation is 5833 feet based on the topographic survey by Galena Engineering, Inc.
 No groundwater encountered.
 Test pit terminated at 7.8' below existing grade due to the consistency of the soils between the test pits after reaching several feet below typical crawlspace footing elevation.
 No soil sample retrieved.
 Sloughing of test pit walls in sand and gravel soils.
 Excavation equipment: DEERE track-mounted excavator.



EXPLORATORY TEST PIT #4
 Proposed Gateway Project
 Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
 251 South Main Street
 Ketchum, Idaho

DEPTH (Feet)	USCS SOIL CLASS	SOIL DESCRIPTION
0.0'-3.5'	SM	Silty fine SAND, little Gravel, trace Roots (UNCONTROLLED FILL) Dark brown, loose dry. Sewer service pipe at 3.5'
3.5'-7.0'	GM	Silty SAND & GRAVEL (UNCONTROLLED FILL) Brown, compact-dense, damp.
7.0'-8.5'	GW	SAND, GRAVEL, COBBLE & BOULDER (NATIVE) Brown, compact-dense-dry.

Test Pit completed on October 26, 2018.
 See *Test Pit Site Plan* for test pit location.
 The approximate test pit surface elevation is 5824 feet based on the topographic survey by Galena Engineering, Inc.
 No groundwater encountered.
 Test pit terminated at 8.5' below existing grade due to the consistency of the soils between the test pits after reaching several feet below typical crawlspace footing elevation.
 No soil sample retrieved.
 Sloughing of test pit walls in sand and gravel soils.
 Excavation equipment: DEERE track-mounted excavator.



EXPLORATORY TEST PIT #5
 Proposed Gateway Project
 Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
 251 South Main Street
 Ketchum, Idaho

<u>DEPTH (Feet)</u>	<u>USCS SOIL CLASS</u>	<u>SOIL DESCRIPTION</u>
0.0'-1.4'	GM	Silty fine SAND, little Gravel & Roots (UNCONTROLLED FILL) Brown, loose dry.
1.4-6.0'	SM	Silty fine SAND, GRAVEL & COBBLE, occ. Boulder (NATIVE) Brown, loose, dry.
6.0'-8.5'	GW	SAND, GRAVEL, COBBLE & BOULDER (NATIVE) Brown, compact-dense, dry.

Test Pit completed on October 26, 2018.
 See *Test Pit Site Plan* for test pit location.
 The approximate test pit surface elevation is 5826 feet based on the topographic survey by Galena Engineering, Inc.
 No groundwater encountered.
 Test pit terminated at 8.5' below existing grade due to the consistency of the soils between the test pits after reaching several feet below typical crawlspace footing elevation.
 No soil sample retrieved.
 Sloughing of test pit walls in sand and gravel soils.
 Excavation equipment: DEERE track-mounted excavator.



EXPLORATORY TEST PIT #6
 Proposed Gateway Project
 Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
 251 South Main Street
 Ketchum, Idaho

<u>DEPTH (Feet)</u>	<u>USCS SOIL CLASS</u>	<u>SOIL DESCRIPTION</u>
0.0'-0.8'	GM	Silty fine SAND, little Gravel & Roots (UNCONTROLLED FILL) Brown, loose dry.
0.8'-6.9'	GW	SAND, GRAVEL, COBBLE & BOULDER (NATIVE) Brown, compact-dense, dry.

Test Pit completed on October 26, 2018.
 See *Test Pit Site Plan* for test pit location.
 The approximate test pit surface elevation is 5810 feet based on the topographic survey by Galena Engineering, Inc.
 No groundwater encountered.
 Test pit terminated at 6.9' below existing grade due to the consistency of the soils between the test pits after reaching several feet below typical crawlspace footing elevation.
 No soil sample retrieved.
 Sloughing of test pit walls in sand and gravel soils.
 Excavation equipment: DEERE track-mounted excavator.



EXPLORATORY TEST PIT #7
 Proposed Gateway Project
 Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
 251 South Main Street
 Ketchum, Idaho

<u>DEPTH (Feet)</u>	<u>USCS SOIL CLASS</u>	<u>SOIL DESCRIPTION</u>
0.0'-4.1'	GM	Silty fine SAND, little Gravel & Roots (UNCONTROLLED FILL) Brown, loose dry.
4.1'-7.2'	GW	SAND, GRAVEL, COBBLE & BOULDER (NATIVE) Brown, compact-dense, dry.

Test Pit completed on October 26, 2018.
 See *Test Pit Site Plan* for test pit location.
 The approximate test pit surface elevation is 5813 feet based on the topographic survey by Galena Engineering, Inc.
 No groundwater encountered.
 Test pit terminated at 7.2' below existing grade due to the consistency of the soils between the test pits after reaching several feet below typical crawlspace footing elevation.
 No soil sample retrieved.
 Sloughing of test pit walls in sand and gravel soils.
 Excavation equipment: DEERE track-mounted excavator.



EXPLORATORY TEST PIT #8
 Proposed Gateway Project
 Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
 251 South Main Street
 Ketchum, Idaho

<u>DEPTH (Feet)</u>	<u>USCS SOIL CLASS</u>	<u>SOIL DESCRIPTION</u>
0.0'-1.5'	GM	Silty fine SAND, little Gravel & Roots (UNCONTROLLED FILL) Brown, loose dry.
1.5'-7.7'	GW	SAND, GRAVEL, COBBLE & BOULDER (NATIVE) Brown, compact-dense, dry.

Test Pit completed on October 26, 2018.
 See *Test Pit Site Plan* for test pit location.
 The approximate test pit surface elevation is 5805 feet based on the topographic survey by Galena Engineering, Inc.
 No groundwater encountered.
 Test pit terminated at 8.5' below existing grade due to the consistency of the soils between the test pits after reaching several feet below typical crawlspace footing elevation.
 No soil sample retrieved.
 Sloughing of test pit walls in sand and gravel soils.
 Excavation equipment: DEERE track-mounted excavator.



EXPLORATORY TEST PIT #9
 Proposed Gateway Project
 Lots 1, 2, 3, 21 & 22, Block 82, Ketchum Townsite
 251 South Main Street
 Ketchum, Idaho

DEPTH (Feet)	USCS SOIL CLASS	SOIL DESCRIPTION
0.0'-6.4'	GM	Silty fine SAND, little Gravel & Roots (UNCONTROLLED FILL) Brown, loose dry.
6.4'-7.6'	GW	SAND, GRAVEL, COBBLE & BOULDER (NATIVE) Brown, compact-dense, dry.

Test Pit completed on October 26, 2018.

See *Test Pit Site Plan* for test pit location.

The approximate test pit surface elevation is 5809 feet based on the topographic survey by Galena Engineering, Inc.

No groundwater encountered.

Test pit terminated at 7.6' below existing grade due to the consistency of the soils between the test pits after reaching several feet below typical crawlspace footing elevation.

No soil sample retrieved.

Sloughing of test pit walls in sand and gravel soils.

Excavation equipment: DEERE track-mounted excavator.



SOILS CLASSIFICATION / LEGEND

RELATIVE DENSITY OR CONSISTENCY
UTILIZING STANDARD PENETRATION TEST VALUES

COHESIONLESS SOILS (a)			COHESIVE SOILS (b)		
Density (c)	N. blows/ft. (c)	Relative Density (%)	Consistency	N. blows/ft. (c)	Undrained (d) Shear Strength (psf)
Very loose	0 to 4	0 - 15	very soft	0 to 2	<250
Loose	4 to 10	15 - 35	soft	2 to 4	250-500
Compact	10 to 30	35 - 65	firm	4 to 8	500-1000
Dense	30 to 50	65 - 85	stiff	8 to 15	1000-2000
Very Dense	over 50	>85	very stiff Hard	15 to 30 over 30	2000-4000 >4000

- (a) Soils consisting of gravel, sand, and silt, either separately or in combination, possessing no characteristics of plasticity and exhibiting drained behavior.
 (b) Soils possessing the characteristics of plasticity and exhibiting undrained behavior.
 (c) Refer to text of ASTM D 1586-94 for a definition of N; in normally consolidated cohesionless soils Relative Density terms are based on N values corrected for overburden pressures.
 (d) Undrained shear strength = 1/2 unconfined compression strength.

COMPONENT DEFINITIONS BY GRADATION

COMPONENT	SIZE RANGE
Boulders	Above 12 inches
Cobbles	3 inches to 12 inches
Gravel	3 inches to No. 4 (4.75 mm)
Coarse gravel	3 inches to 3/4 inch
Fine gravel	3/4 inch to No. 4 (4.75mm)
Sand	No. 4 (4.75mm) to No. 200 (0.075mm)
Coarse sand	No. 4 (4.75) to No. 10. (2.0mm)
Medium sand	No. 10 (2.0mm) to No. 40 (0.42mm)
Fine sand	No. 40 (0.42) to No. 200 (0.075mm)
Silt & Clay	Smaller than No. 200 (0.075mm)

SILT & CLAY DESCRIPTIONS

DESCRIPTIONS	TYPICAL UNIFIED DESIGNATION
Silt	ML (non-plastic)
Clayey Silt	CL-ML (low plasticity)
Silty Clay	CL
Clay	CH
Plastic Silt	MH
Organic Soils	OL, OH, Pt

LABORATORY TESTS

TEST	DESIGNATION
Moisture	(1)
Density	D
Grain Size	G
Hydrometer	H
Atterberg Limits	(1)
Consolidation	C
Unconfined	U
UU Triax	UU
CU Triax	CU
CD Triax	CD
Permeability	P

(1) Moisture & Atterberg Limits

SAMPLES

SS	SPT Samplers
HD	Heavy Duty Spill Spoons
SH	Shelby Tube
P	Pitcher Sampler
B	Bulk
C	Cord

Unless otherwise noted, drive samples advanced with 140 lb. Hammer with 30 inch drop.

COMPONENT PROPORTIONS

DESCRIPTIONS	RANGE OF PROPORTION
Trace	0 - 5%
Little	5 - 12%
Some or Adjective (a)	12 - 30%
And	30 - 50%

(a) Use Gravelly, Sandy or Silty as appropriate.

UNIFIED SOIL CLASSIFICATION SYSTEM

MAJOR DIVISIONS	SYMBOL	TYPICAL NAMES	
COARSE GRAINED SOILS	GRAVELS	GW Well-Graded gravel	
	More than 50% of coarse fraction retained on No. 4 Sieve	CLEAN GRAVELS	GP Poorly-graded gravels
		GRAVELS WITH FINES	GM Gravel and Silt Mixtures
	more than 50% retained on No. 200 Sieve	CLEAN SANDS	GC Gravel and Clay Mixtures
			SW Well-graded Sands
		50% or more of coarse fraction passes No. 4 Sieve	SP Poorly-graded Sands
SANDS WITH FINES more than 12% fines			SM Sand and Silt Mixtures
FINE GRAINED SOILS	SILTS & CLAYS	SC Sand and Clay Mixtures	
		Liquid limit less than 50	INORGANIC	CL Low-plasticity Clays
	50% or more passes the No. 200 Sieve		ORGANIC	ML Non-plastic and Low-plasticity Silts
		SILTS & CLAYS	OL Organic Silt and Clay of Low plasticity.
	Liquid limit less than 50		INORGANIC	CH High Plasticity Clays
		ORGANIC	MH High Plasticity Silts
HIGHLY ORGANIC SOILS	PT	OH High-plasticity-Organic Clays High-plasticity-Organic Silts	
		Pt Peat, Muck and Other Highly Organic Soils	

USGS Design Maps Summary Report

User-Specified Input

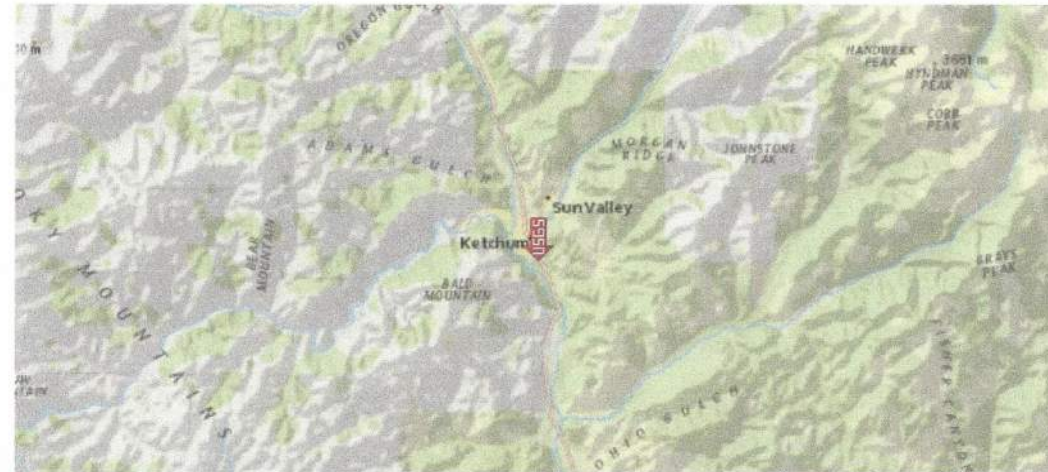
Report Title Gateway Project
Thu December 6, 2018 22:03:47 UTC

Building Code Reference Document 2012/2015 International Building Code
(which utilizes USGS hazard data available in 2008)

Site Coordinates 43.68°N, 114.36°W

Site Soil Classification Site Class C - "Very Dense Soil and Soft Rock"

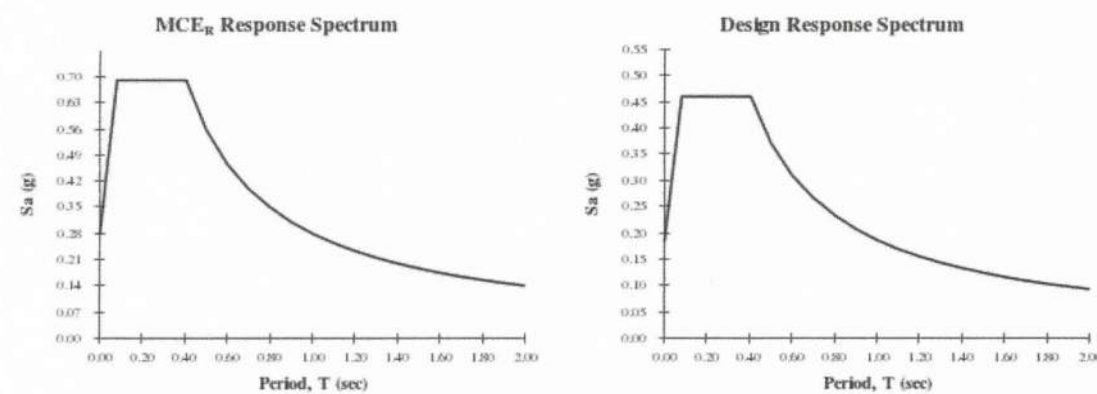
Risk Category I/II/III



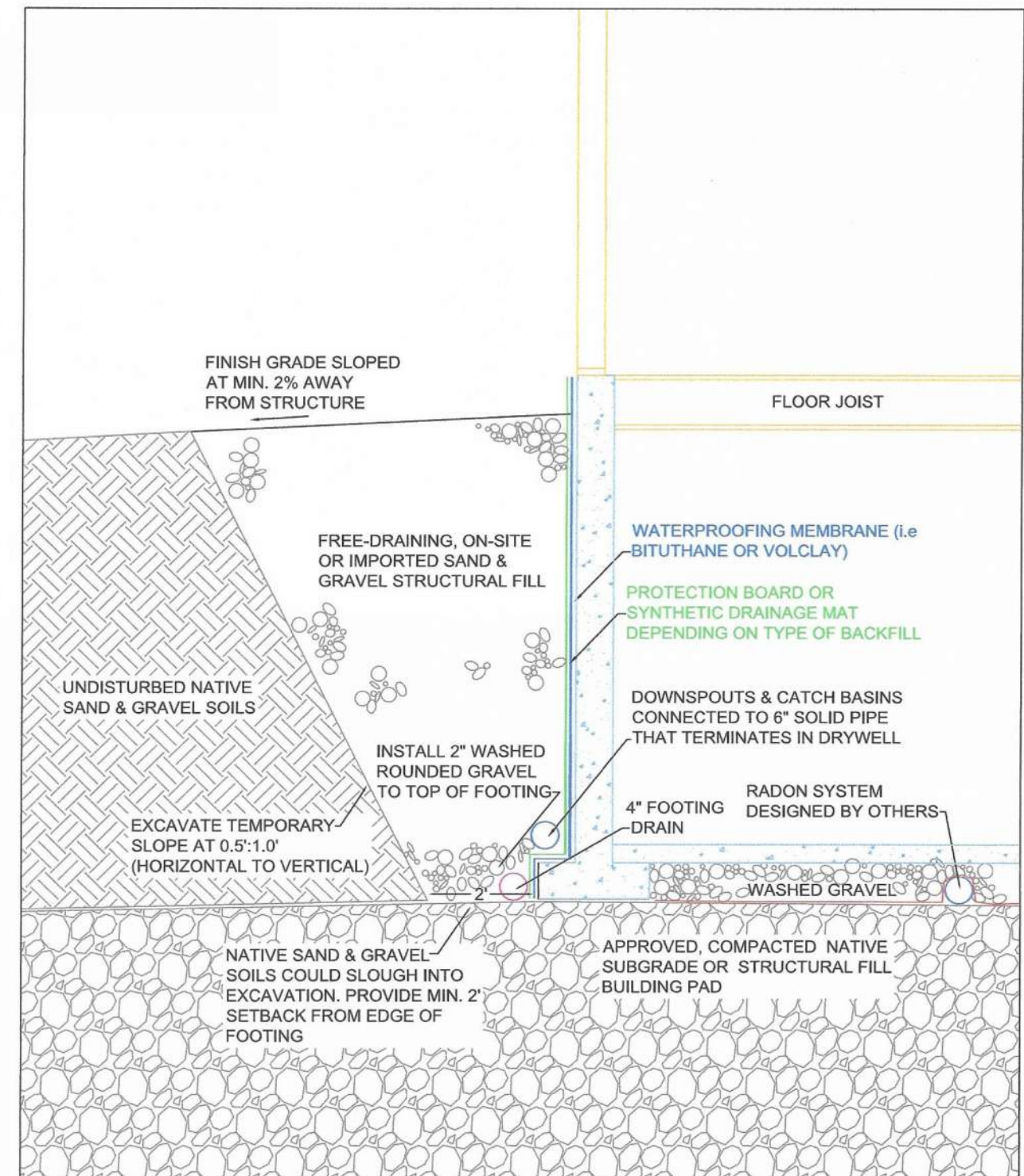
USGS-Provided Output

$S_s = 0.593 g$ $S_{MS} = 0.690 g$ $S_{DS} = 0.460 g$
 $S_1 = 0.171 g$ $S_{M1} = 0.279 g$ $S_{D1} = 0.186 g$

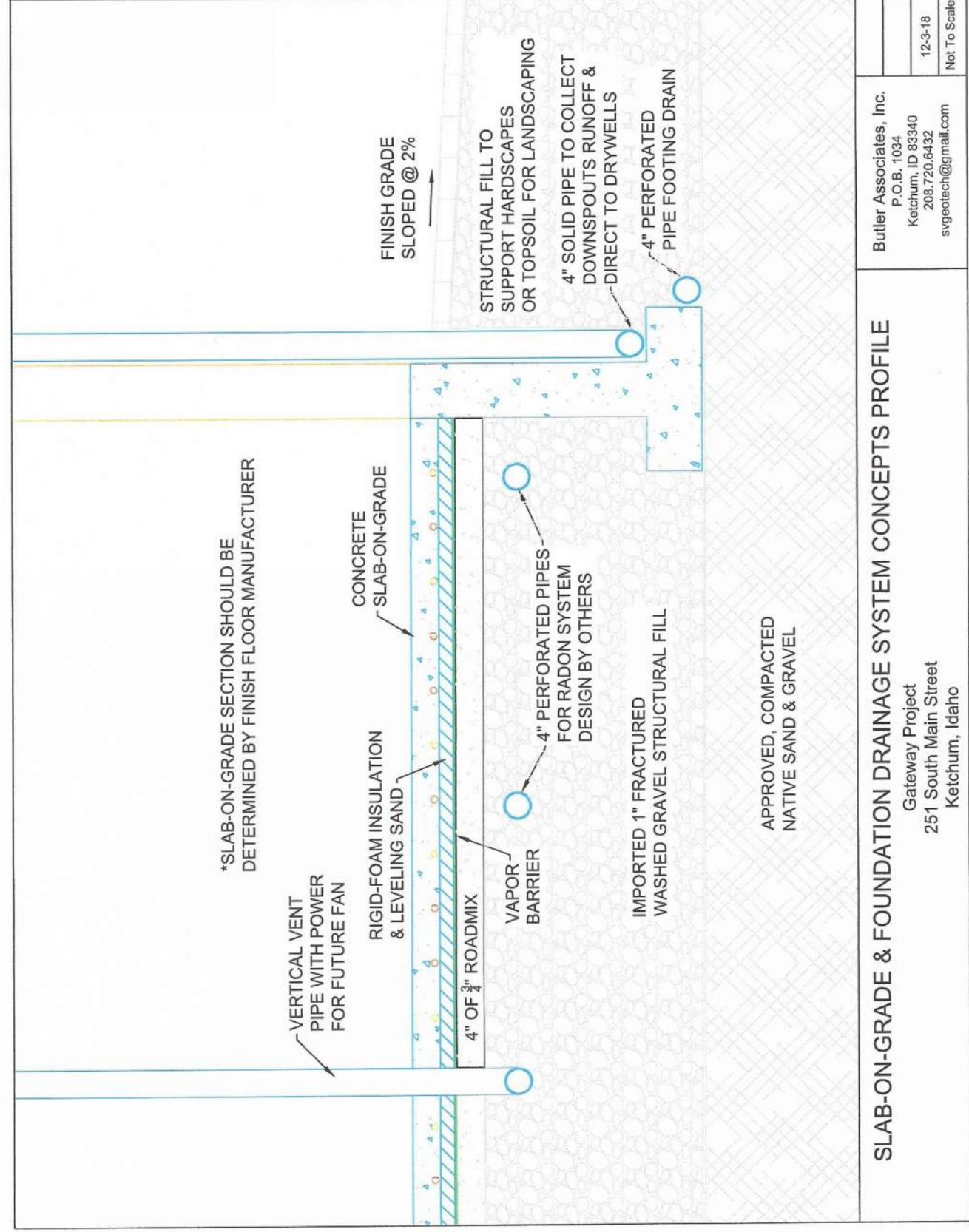
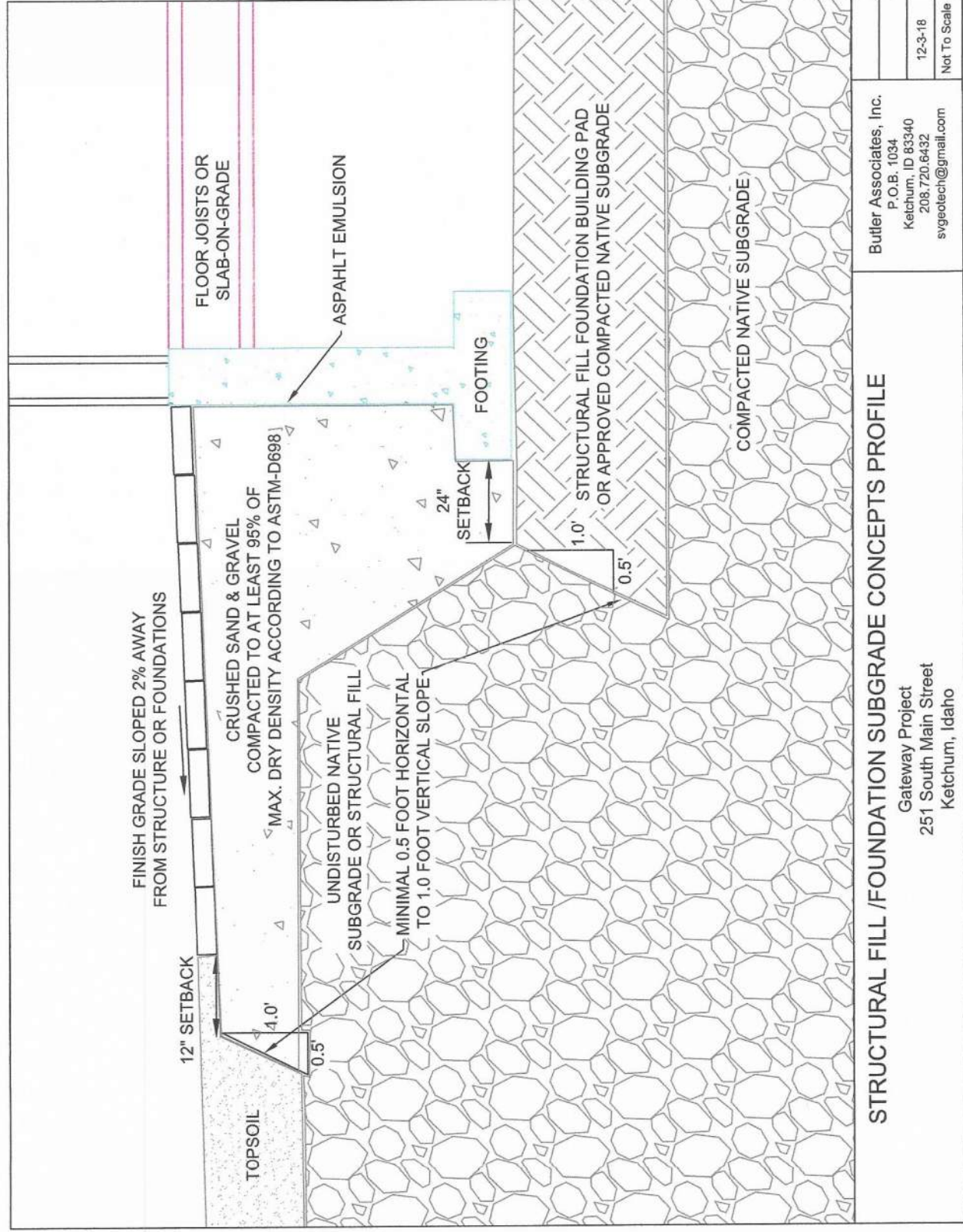
For information on how the S_s and S_1 values above have been calculated from probabilistic (risk-targeted) and deterministic ground motions in the direction of maximum horizontal response, please return to the application and select the "2009 NEHRP" building code reference document.

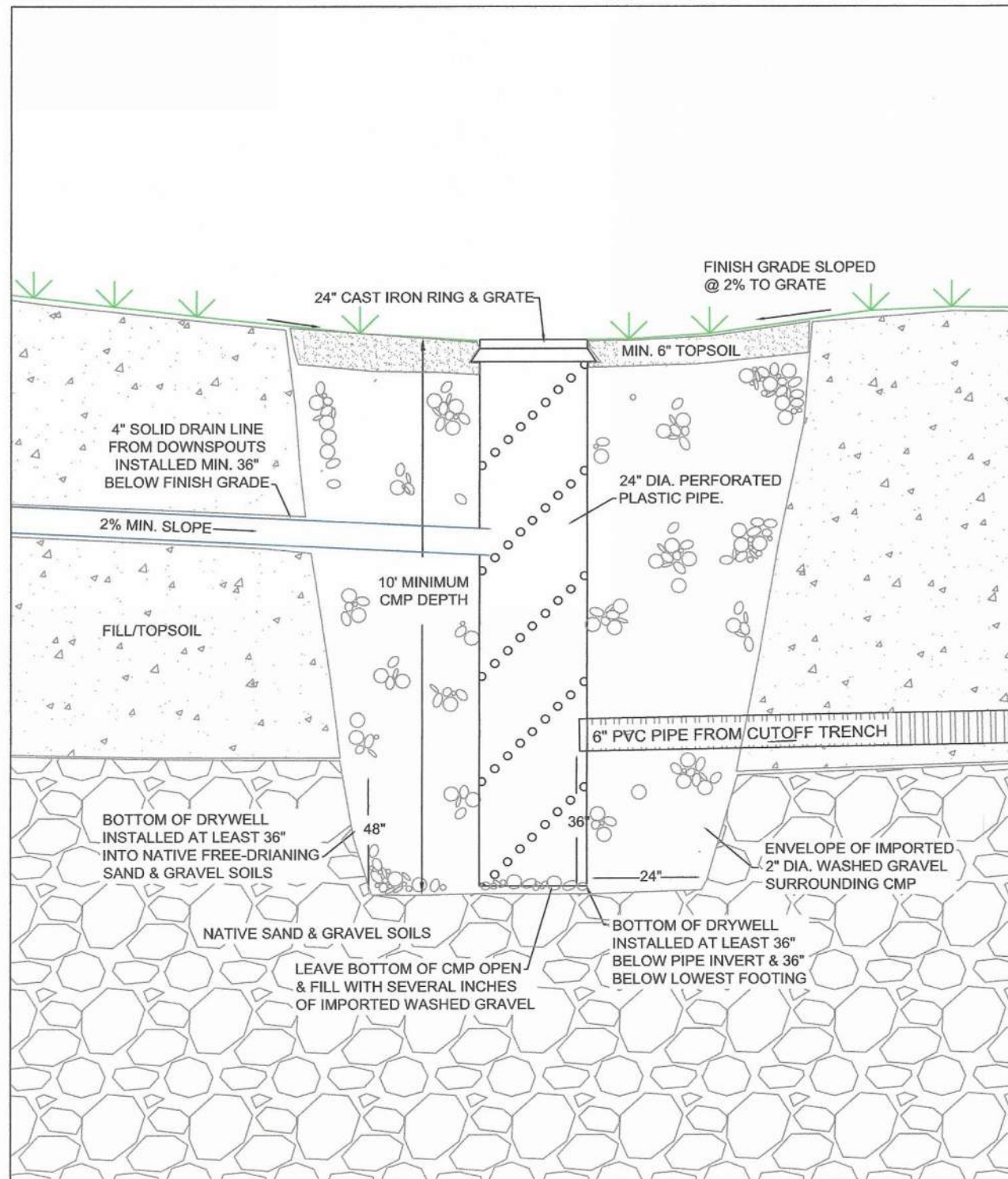


Although this information is a product of the U.S. Geological Survey, we provide no warranty, expressed or implied, as to the accuracy of the data contained therein. This tool is not a substitute for technical subject-matter knowledge.



RETAINING WALL DRAINAGE PROFILE		Butler Associates, Inc. P.O.B. 1034 Ketchum, ID 83340 208.720.6432 svgeotech@gmail.com	
Gateway Project 251 South Main Street Ketchum, Idaho			12-3-18
			Not To Scale



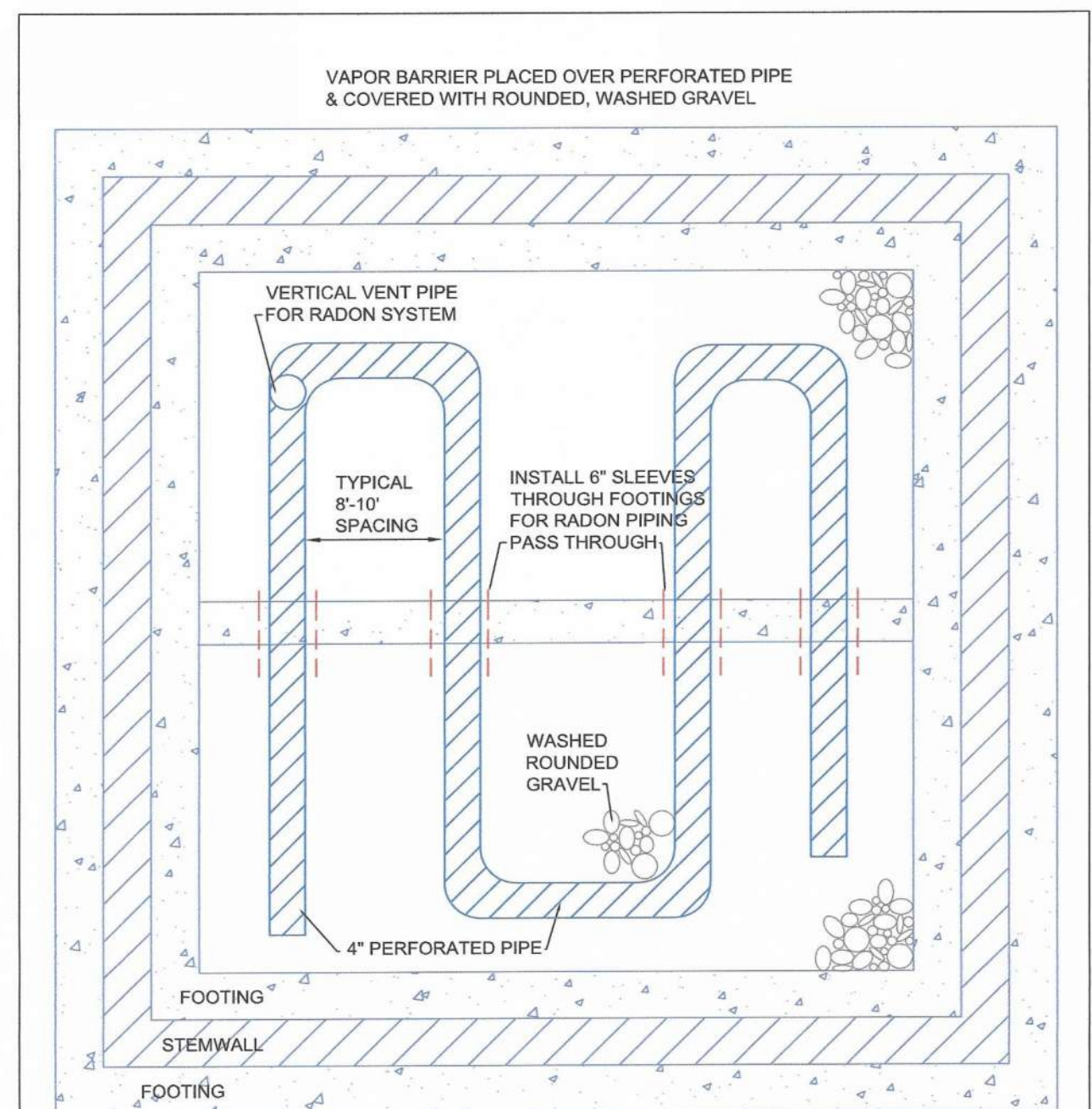


STORMWATER LANDSCAPE DRYWELL PROFILE

Gateway Project
251 South Main Street
Ketchum, Idaho

Butler Associates, Inc.
P.O.B. 1034
Ketchum, ID 83340
208.720.6432
svgeotech@gmail.com

12-3-18
Not To Scale



****RADON SYSTEM DESIGNED BY OTHERS**

RADON SYSTEM CONCEPTS PLAN

Gateway Project
251 South Main Street
Ketchum, Idaho

Butler Associates, Inc.
P.O.B. 1034
Ketchum, ID 83340
208.720.6432
svgeotech@gmail.com

12-3-18
Not To Scale

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EXHIBIT | 02.13

FLOOD PLAIN | RIPARIAN CORRIDOR

FLOOD PLAIN MANAGEMENT OVERLAY - APPLICATION NARRATIVE

This narrative is prepared to go along with the Floodplain Management Overlay Application for the Ketchum Boutique Hotel Project. This project is proposing to construct a 4-star hotel at the south-western corner of Main Street and River Street. The following items relate to this application:

- The applicant team is aware that there is a riparian area regulated by KMC Chapter 17.88 on the property. This 25'-0" riparian zone is shown on several drawings in the civil drawings set (C1.0, C2.0, and C3.0).

- The applicant's intent is to allow the riparian zone to remain in a natural, unaltered state.

- The applicant team is aware of all riparian regulations (no applications of pesticides / herbicides or fertilizers in riparian zone, application of herbicides or pesticides outside of the 25' riparian zone, but within 100'-0" of MHW has special regulations, etc.). The applicant will adhere to all applicable regulations, and if alteration of the riparian area is desired in the future, permits will be applied for

- No portion of the riparian area will be disturbed by any construction activity related to the project. There exists a 10'-0" sewer main easement immediately adjacent to the riparian zone for any building structures.

- The riparian setback will be protected during construction. As shown on sheet C2.0, by silt fencing. Temporary signage will also be erected to alert all contractors not to disturb the riparian area.



City of Ketchum
Planning & Building

Floodplain Management Overlay Application

OFFICIAL USE ONLY	
File Number:	
Date Received:	
By:	
Fee Paid:	
Approved Date:	
Denied Date:	
By:	

OWNER INFORMATION			
Project Name: Ketchum Gateway Hotel			
Owner Name: PEG Ketchum Hotel LLC			
Mailing Address: 180 N University Ave, Ste. 200, Provo, UT 84601			
Phone: 801-655-1998			
Email: nblayden@pegcompanies.com			
PROJECT INFORMATION			
Architect/Representative: Sean Flynn / Galena Engineering			
Phone: 208-788-1705			
Mailing Address: Sean Flynn / Galena Engineering			
Email: sflynn@galena-engineering.com			
Engineer of Record: Sean Flynn			
Engineer Email: sflynn@galena-engineering.com			
Legal Land Description: Lots 1, 2, 3, and fraction of 21 and 22, Block 82, Ketchum Townsite			
Project Address: 260 East River Street			
Lot Area: 47,249 square feet			
Zoning District: T. Tourist			
Anticipated Use: Hotel			
Value of Construction: \$			
TYPE OF CONSTRUCTION			
<input checked="" type="checkbox"/> New	<input type="checkbox"/> Remodel	<input type="checkbox"/> Addition	<input type="checkbox"/> Other, please explain:
<input type="checkbox"/> Waterways Design Review	<input checked="" type="checkbox"/> Floodplain Development	<input type="checkbox"/> Stream Alteration	
TOTAL FLOOR AREA			
Proposed		Existing	
Basement:			
1 st Floor:			
2 nd Floor:			
3 rd Floor:			
Decks:			
Mezzanine:			
Total:			
Building Coverage: 25,874 SF 55 %		Curb Cut: SF %	
PROPOSED SETBACKS			
Front: 5'	Side: 25' on highway, 5' on west side	Side:	Rear:
ADDITIONAL INFORMATION			
Building Height:		Parking Spaces Provided:	
Will Fill or Excavation Be Required? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No			
If Yes, Amount in Cubic Yards		Fill:	Excavation:
Will Existing Trees or Vegetation Be Removed? Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>			

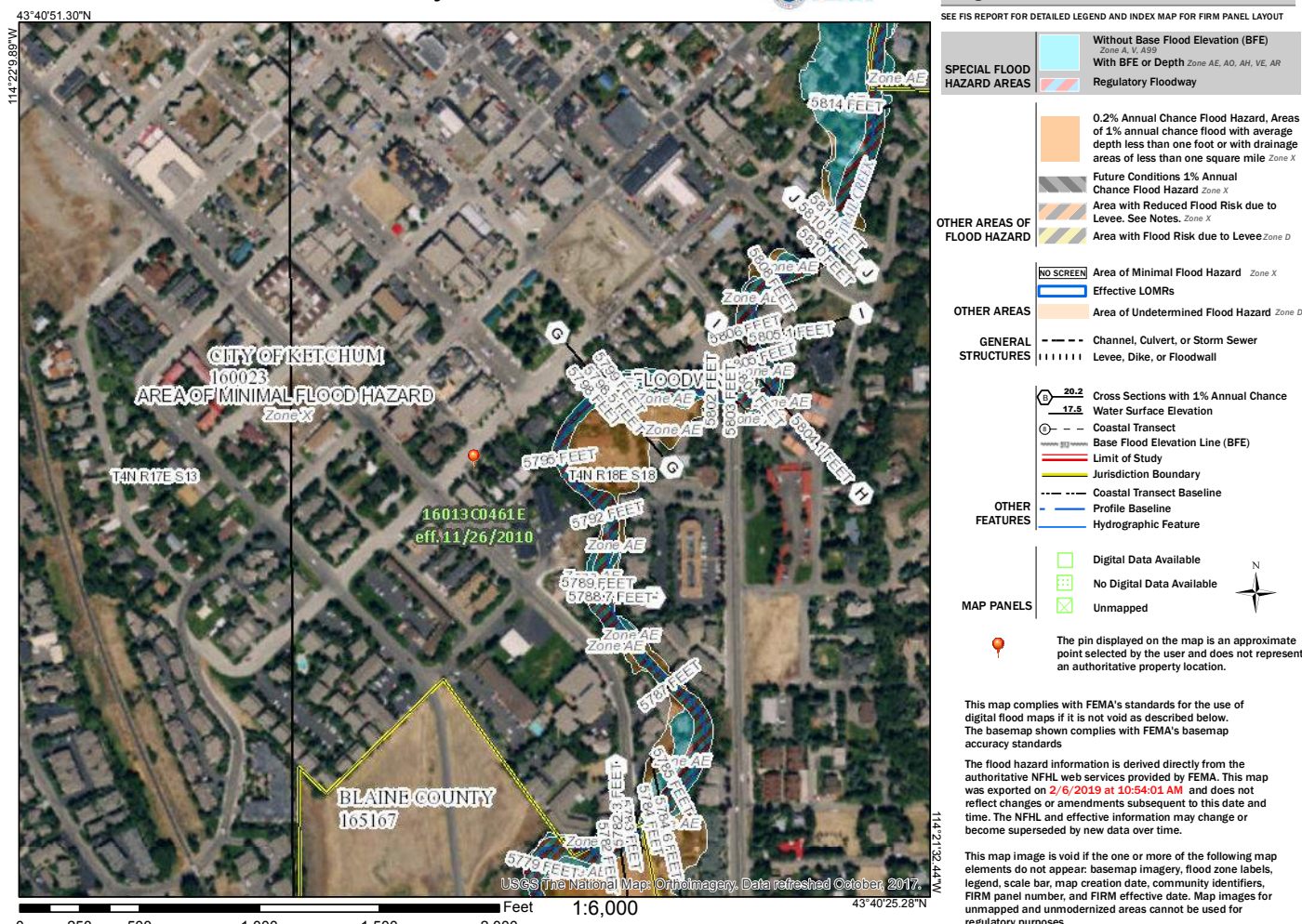
Applicant agrees in the event of a dispute concerning the interpretation or enforcement of the Floodplain Management Overlay Application, in which the City of Ketchum is the prevailing party, to pay reasonable attorney fees, including attorney fees on appeal, and expenses of the City of Ketchum. I, the undersigned, certify that all information submitted with and upon this application form is true and accurate to the best of my knowledge and belief.

Signature of Owner/Representative

Date

City of Ketchum Planning & Building Department
Floodplain Management Overlay Application

National Flood Hazard Layer FIRMette



FLOODPLAIN MANAGEMENT OVERLAY EVALUATION STANDARDS

Please provide a narrative to address each of the criteria below.

Criteria for Evaluation of Applications: The criteria for evacuation of floodplain development permit applications, waterways design review applications, and stream alteration permit applications shall be as follows:

1. Preservation or restoration of the inherent natural characteristics of the river and creeks and floodplain areas. Development does not alter river channel.
2. Preservation of riparian vegetation and wildlife habitat, if any, along the stream bank and within the required minimum twenty-five-foot (25') setback or riparian zone. No construction activities, encroachment or other disturbance (mowing, pruning, or any other landscape activity) into the twenty-five-foot (25') riparian zone, except for restoration, shall be allowed at any time without written city approval per the terms of this chapter.
3. No development, other than development by the city of Ketchum or development required for emergency access, shall occur within the twenty-five foot (25') riparian zone with the exception of approved stream stabilization work. The planning and zoning commission may approve access to property where no other primary access is available. Private pathways and staircases shall not lead into or through the riparian zone unless deemed necessary by the planning and zoning commission.
4. A landscape plan and time frame shall be provided to restore any vegetation within the twenty-five foot (25') riparian zone that is degraded, not natural or which does not promote bank stability.
5. New or replacement planting and vegetation shall include plantings that are low growing and have dense root systems for the purpose of stabilizing stream banks and repairing damage previously done to riparian vegetation. Examples of such plantings include: red osier dogwood, common chokecherry, serviceberry, elderberry, river birch, skunk bush sumac, Bebb's willow, Drummond's willow, little wild rose, gooseberry, and honeysuckle.
6. Landscaping and driveway plans to accommodate the function of the floodplain to allow for sheet flooding. Floodwater carrying capacity is not diminished by the proposal. Surface drainage is controlled and shall not adversely impact adjacent properties including driveways drained away from paved roadways. Culvert(s) under driveways may be required. Landscaping berms shall be designed to not dam or otherwise obstruct floodwaters or divert same onto roads or other public pathways.
7. Impacts of the development on aquatic life, recreation, or water quality upstream, downstream or across the stream are not adverse.
8. Building setback in excess of the minimum required along waterways is encouraged. An additional ten foot (10') building setback is encouraged to provide for yards, decks and patios outside the twenty-five foot (25') riparian zone.
9. The top of the lowest floor of a building located in the one percent (1%) annual chance floodplain shall be a minimum of twenty-four inches (24") above the base flood elevation of the subject property.
10. The backfill used around the foundation in the floodplain shall provide a reasonable transition to existing grade but shall not be used to fill the parcel to any greater extent. Compensatory storage shall be required for any fill placed within the floodplain. An LOMA-F shall be obtained prior to placement of any additional fill in the floodplain.
11. All new buildings shall be constructed on foundations that are approved by a licensed professional engineer.
12. Driveways shall comply with effective street standards; access for emergency vehicles has been adequately provided for.
13. Landscaping or revegetation shall conceal cuts and fills required for driveways and other elements of the development.
14. (Stream Alteration Only.) The proposal is shown to be a permanent solution and creates a stable situation.
15. (Stream Alteration Only.) No increase to the one percent (1%) annual chance floodplain upstream or downstream has been certified, with supporting calculations, by a registered Idaho hydraulic engineer.
16. (Stream Alteration Only.) The recreational use of the stream, including access along any and all public pedestrian/fisher's easements, and the aesthetic beauty shall not be obstructed or interfered with by the proposed work.
17. Where development is proposed that impacts any wetland, first priority shall be to move development from the wetland area. Mitigation strategies shall be proposed at time of application that replace the impacted wetland area with a comparable amount and/or quality of new wetland area or riparian habitat improvement.
18. (Stream Alteration Only.) Fish habitat shall be maintained or improved as a result of the work proposed.
19. (Stream Alteration Only.) The proposed work shall not be in conflict with the local public interest, including but not limited to, property values, fish and wildlife habitat, aquatic life, recreation and access to public lands and waters, aesthetic beauty of the stream and water quality.
20. (Stream Alteration Only.) The work proposed is for the protection of the public health, safety and/or welfare such as public schools, sewage treatment plant, water and sewer distribution lines and bridges providing particularly limited or sole access to areas of habitation.

APPLICATION CHECKLIST

Please utilize and submit the checklist on the following pages to ensure a complete application.

FLOODPLAIN MANAGEMENT OVERLAY APPLICATION CERTIFICATION OF COMPLETENESS

Use for:

- Floodplain Development Permit
- Waterways Overlay Design Review
- Stream Alteration Permit

Project Name:

Date:

Reviewed by:

DOCUMENTS

- Application form
- Evaluation criteria narrative
- Description of proposed development
- Specifications for building construction and materials, flood proofing, filling, grading, dredging, channel improvement/changes and utilities
- Elevation and/or flood proofing certification prepared by a professional engineer for existing and proposed residential and nonresidential structures located partially or wholly in the regulatory floodplain. Said floodproofing methods shall meet the criteria in subsection 17.88.060.B of the Ketchum Municipal Code.
- Copy of letter of map amendment based on fill (LOMA-F) application for any proposed fill in the floodplain. LOMA-F approval shall be obtained from FEMA prior to issuance of a floodplain development permit.

SETS OF PLANS

- One (1) copy of full-sized; One (1) 11x17 reduced; and CD, flash drive or email (.pdf) of plans

SITE SURVEY (certified by a licensed engineer or surveyor)

- Exterior boundary lines of the property together with dimensions
- Topographic survey of the real property at a minimum of one (1) foot contour intervals, significant hillsides may be a minimum of ten (10) foot contour intervals
- Location of any existing dwelling units, other structures, fill, storage of materials, drainage facilities and all improved areas (pavement) with dimensions thereof showing the setback of each structure from the nearest property line
- Location of existing channels and ditches and other significant natural features, boundaries of floodway and floodplain, including Base Flood Elevation (BFE) and other site specific information from the studies referred to in Ketchum Municipal Code. subsection 17.88.040.A.3

- Location and elevations of adjacent streets, water supply and sewer lines, including private wells and/or septic systems
- Elevation of the lowest floor (including basement) of all structures existing and proposed partially or wholly located in the one percent (1%) annual chance floodplain, including elevation to which any structure has been or will be floodproofed
- Identification of the riparian zone and the "mean high water mark," as defined in Ketchum Municipal Code
- Location of previous stream alterations upstream, downstream and along both banks from subject lot
- Location of drainage ways, intermittent and year-round, including potential overflow channels or channel movement
- Location and dimensions of easements, private and public, within and adjacent to the proposed project together with the purpose thereof
- Location of all existing trees to be preserved and significant trees to be removed
- Indication of any zoning district overlay which affects the property (floodplain, mountain overlay or avalanche)
- Location of existing structures on adjacent properties
- Plat map of the property certified by a licensed engineer or surveyor, except tax lots

SITE PLAN

- Vicinity map
- Proposed excavation or land fill including resulting slope grades for the building pad(s), driveways and any other element of the proposed development where excavation or fill will take place
- Drainage plan including offsite improvements such as borrow ditches and culverts and including a plan for on- and off-site improvements to provide for unobstructed conveyance of floodwaters
- Location of on-site parking spaces and access thereto, including the dimensions of the spaces and the width and length of access and curb cuts
- Location and dimensions of snow storage areas
- Location of dumpster and/or garbage and recycling can storage areas, including the dimensions and proposed fencing or other screening
- Location and type of any electrical power transformers, switches and/or sectors
- Location and type of all heating, ventilation, air conditioning and other mechanical units
- Drip line of all buildings
- Percentage of the lot coverage by proposed building and parking areas together with the total square footage of the parcel of property
- Location of all proposed structures (buildings) and all improved areas (pavement, sidewalk) with dimensions thereof showing the setback of each structure from the nearest property line
- Designation of the zoning district in which the project is located
- Location of any zoning district boundary line within the proposed project or the immediate vicinity thereof
- For any building in the floodplain with an area below the lowest floor that is below the base flood elevation and has a ceiling height of five feet (5') or greater, the building owner shall sign a non-conversion agreement, that shall run with the property, promising not to improve, finish or otherwise convert the area below the lowest floor to living area and granting the city the right to inspect the enclosed area at its discretion. Such agreement shall be recorded at Blaine County's recorder's office

ARCHITECTURAL PLANS

- Floor plans of all floors at not less than one-eighth (1/8) scale
- All exterior elevations
- Roof plan including direction of snow sliding and snow clips if applicable. Location and type of all

- Cross-section(s) of the property and proposed building adequately establishing the natural grade, finished grade, slope of land, slope of proposed accesses and grades to all public rights-of-way
- Location and type (cut sheets) of all exterior lighting
- Model or computer simulation renderings, if required at pre-application design review meeting

LANDSCAPE PLAN

- All existing vegetation over 2 inches in caliper, including size and species
- Proposed landscaping of the project including types, quantities and sizes of trees, shrubs, ground cover and other vegetation
- Proposed landscaping or other improvements within any public rights-of-way
- Location, type (materials and colors) and height of walls or fences
- Location of parking areas
- Location of vehicular and pedestrian circulation patterns, easements and proposed improvements with regard thereto
- Irrigation system for landscaping
- Drainage plan including off-site improvements

STREAM ALTERATIONS

- Copies of applications sent to the U.S. army corps of engineers (USACE) and Idaho department of water resources (IDWR) (stream alteration only). USACE and IDWR approvals shall be obtained prior to issuance of a stream alteration permit
- Cross section of proposed work (stream alteration only)
- Length of stream to be worked, type of work to be done, type of equipment to be used and starting and completion dates of work (stream alteration only)
- A valley cross section showing stream channel, floodway limits, elevations of adjacent land areas, IRF, proposed development, and high water information and a profile showing the slope of the bottom of the channel or flow line of the stream may be required upon review of all other material submitted (stream alteration only)
- A no net rise certificate, including supporting calculations, prepared by registered professional hydraulic engineer for any work proposed in the floodway

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EXHIBIT | 02.14

LIST OF ADJACENT PROPERTY OWNERS

300 ft adjoiners for Parcel Number:RPK0000082003A
 117 records returned in query

[Map It!](#)

You can save this file as .htm or .html and open it in a spreadsheet or copy and paste it into a spreadsheet.

Parcel Number	Owner 1	Owner 2	Mail Address Line 1	Mail Address Line 2	Mail Address Line 3	Mail Address Line 4	Mail Address Line 5	Property Address	Legal Desc 1	Legal Desc 2	Legal Desc 3	Legal Desc 4	Legal Desc 5
RPK0000001001A	KENTWOOD LLC	KENTWOOD PROPERTIES LLC	PO BOX 2172	KETCHUM ID 83340-0000				180 S MAIN ST	KETCHUM	LOT 1A & PARCEL A BLK 1			
RPK0000040001A	KETCHUM CITY OF		PO BOX 2315	KETCHUM ID 83340-0000				131 E RIVER ST	KETCHUM	LOTS 1 THRU 8 BLK 40			
RPK00000820010	PEG KETCHUM HOTEL LLC		180 N UNIVERSITY AVE STE 200	PROVO UT 84601-0000				280 E RIVER ST	KETCHUM	LOT 1 BLK 82	5500 SF OR .1263 AC		
RPK0000082002A	PEG KETCHUM HOTEL LLC		180 N UNIVERSITY AVE STE 200	PROVO UT 84601-0000				260 E RIVER ST	KETCHUM	LOT 2 BLOCK 82	10' X 110' OF ALLEY	6,600 SF OR .152 AC	
RPK0000082003A	PEG KETCHUM HOTEL LLC		180 N UNIVERSITY AVE STE 200	PROVO UT 84601-0000				251 S MAIN ST	KETCHUM	LOTS 3, 21, FR 22 BLK 82	N 10' X 110' OF ALLEY	S 20' X 230' OF ALLEY	
RPK0000082007A	O'GARA EDWARD F III TRUSTEE	O'GARA VICKIE S TRUSTEE	PO BOX 4964	KETCHUM ID 83340-0000				140 E RIVER ST	KETCHUM	AM LOT 7A BLK 82	13,200SF		
RPK0000082022A	BARBER JEFFREY A		BOX 2174	SUN VALLEY ID 83353-0000				280 S MAIN ST	KETCHUM	E FR PART LOT 22 BLK 82	& TL 7561 6750SF		
RPK00000830020	TRAIL CREEK FUND LLC		C/O EAGAN REAL ESTATE	PO BOX 84	SUN VALLEY ID 83353-0000			300 E RIVER ST	KETCHUM	REPLAT BLK 83	LOT 2	40,631SF	
RPK0300001001A	SLATTER TERRY D	SLATTER BARBARA A	4082 MEADOW RIDGE LANE	TWIN FALLS ID 83301-0000				309 ANDORA LN # 106	ANDORA VILLA INC T.H. #1	SUBLOT 1A			
RPK0300001002A	ROBINSON RICHARD S	ROBINSON KATHY ROTHGEB	PO BOX 1140	SUN VALLEY ID 83353-0000				309 ANDORA LN # 105	ANDORA VILLA INC T.H. #1	SUBLOT 2A			
RPK0300001003A	LEYDA PATRICIA		BOX 5835	KETCHUM ID 83340-0000				309 ANDORA LN # 104	ANDORA VILLA INC T.H. #1	SUBLOT 3A			
RPK0300001004A	MC CREARY DAVID F		428 3RD AVENUE #7	NEW YORK NY 10016-0000				309 ANDORA LN # 103	ANDORA VILLA INC T.H. #1	SUBLOT 4A			
RPK0300001005A	DICORI MATTHEW T TRUSTEE	DICORI NICOLE L TRUSTEE	4832 E 45TH ST	YUMA AZ 85365-0000				309 ANDORA LN # 102	ANDORA VILLA INC T.H. #1	SUBLOT 5A			
RPK0300001006A	PROUTY MARTHA L	PROUTY DAVID	3013 MOUNTAIN VIEW DR	BOISE ID 83704-6242				309 ANDORA LN # 101	ANDORA VILLA INC T.H. #1	SUBLOT 6A			
RPK0300001007B	JUNIPER COMMUNITY ASSOCIATION INC		PO BOX 5945	KETCHUM ID 83340-0000				309 ANDORA LN	ANDORA VILLA INC T.H. #1	COMMON AREA-LOT 7B			
RPK03010000000	JUNIPER COMMUNITY ASSOCIATION INC		PO BOX 5945	KETCHUM ID 83340-0000					ANDORA VILLA INC T.H. & CONDOS	FR LOT 5 TL 5537 & TL 4799	SWIMMING POOL		

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Blaine County GIS Map Services 300 ft Adjoiner Results

RPK03010020010	INGOLD ROBERT F JR TRUSTEE	ROBERT F INGOLD JR REVOC LIVING TRUST DATED 04- 19-12	1846 LEEWARD LN	NEWPORT BEACH CA 92660-3801				311 ANDORA LN UNIT 107	ANDORA VILLA INC T.H. #2	LOT 1 UNIT #107			
RPK03010020020	SPAR JOHN M		3709 OLD CONEJO RD	NEWBURY PARK CA 91320-0000				311 ANDORA LN UNIT 108	ANDORA VILLA INC T.H. #2	LOT 2 UNIT #108			
RPK03010020070	GMI LLC		1744 MORTIMER DR	BOISE ID 83712-0000				311 ANDORA LN UNIT 110	ANDORA VILLA INC T.H. #2	LOT 7 UNIT #110			
RPK03010020080	ANDERSEN HEIDI M		PO BOX 5024	KETCHUM ID 83340-0000				311 ANDORA LN UNIT 109	ANDORA VILLA INC T.H. #2	LOT 8 UNIT #109			
RPK03010020180	LOCKHART ROSEMARY		BOX 25260	PORTLAND OR 97298- 0000				311 ANDORA LN UNIT 124	ANDORA VILLA INC T.H. #2	LOT 18 UNIT #124			
RPK03010020190	PREKEGES MARIA G		PO BOX 2892	KETCHUM ID 83340-0000				311 ANDORA LN UNIT 125	ANDORA VILLA INC T.H. #2	LOT 19 UNIT #125			
RPK03010020200	J GARY HIRST LLC		5432 SE PARKWOOD LANE	MERCER ISLAND WA 98040-0000				311 ANDORA LN UNIT 126	ANDORA VILLA INC T.H. #2	LOT 20 UNIT #126			
RPK03010020210	JUNIPER COMM ASSOC		PO BOX 5945	KETCHUM ID 83340-0000				311 ANDORA LN	ANDORA VILLA INC T.H. #2	LOT 21 COMMON AREA			
RPK03290000010	SHOEMAKER ELIZABETH W TRUSTEE	ELIZABETH W SHOEMAKER SEPARATE PROPERTY TRUST	400 HOMEWOOD RD	LOS ANGELES CA 90049-0000				130 E RIVER ST	HOMES ON THE CREEK TOWNHOUSES	SUBLOT 1	7,741SF		
RPK03290000020	HERBERT GERALDINE A TRUSTEE	GERALDINE A HERBERT REVOCABLE TRUST A/A 5/5/93	PO BOX 479	KETCHUM ID 83340-0000				120 E RIVER ST	HOMES ON THE CREEK TOWNHOUSES	SUBLOT 2	8272SF		
RPK07030000000	LIMELIGHT KETCHUM LLC		C/O ASPEN SKIING COMPANY, LLC	MATT JONES	117 ASPEN BUSINESS CENTER	ASPEN CO 81611-0000			151 SO MAIN HOTEL & RESIDENCES	COMMON AREA			
RPK070300000H0	ASPEN SKIING COMPANY LLC		PO BOX 1248	ASPEN CO 81612-0000				151 S MAIN ST UNIT H	151 SO MAIN HOTEL & RESIDENCES	UNIT H	HOTEL		
RPK070300000R0	LIMELIGHT KETCHUM LLC		C/O ASPEN SKIING COMPANY, LLC	MATT JONES	117 ASPEN BUSINESS CENTER	ASPEN CO 81611-0000			151 S MAIN ST UNIT R	151 SO MAIN HOTEL & RESIDENCES	UNIT RET	RETAIL SPACE	
RPK07030004060	POPLAWSKI EDWARD J SUCCESSOR TRUSTEE	POPLAWSKI KIMBALL P SUCCESSOR TRUSTEE	C/O CORNERSTONE ADVISORS	225 108TH AVE NE STE 400	BELLEVUE WA 98004- 0000			151 S MAIN ST UNIT 406	151 SO MAIN HOTEL & RESIDENCES	UNIT 406			
RPK07030004090	KOFFLER SHAYLA		PO BOX 4312	KETCHUM ID 83340-0000				151 S MAIN ST UNIT 409	151 SO MAIN HOTEL & RESIDENCES	UNIT 409			
RPK07030004110	MURTAUGH MELINDA	MELINDA RENEE MURTAUGH 2014 TRUST	PO BOX 703	SUN VALLEY ID 83353- 0000				151 S MAIN ST UNIT 411	151 SO MAIN HOTEL & RESIDENCES	UNIT 411			

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	RENEE TRUSTEE													
RPK07030004120	FOUST MARY KATHERINE TRUSTEE	MARY K FOUST TRUST	111 CENTRAL AVE	SAUSALITO CA 94965-0000				151 S MAIN ST UNIT 412	151 SO MAIN HOTEL & RESIDENCES	UNIT 412				
RPK07030004140	LIMELIGHT 414 LLC		74 ALEJANDRA	ATHERTON CA 94027-0000				151 S MAIN ST UNIT 414	151 SO MAIN HOTEL & RESIDENCES	UNIT 414				
RPK07030004230	IDREC LLC		933 S HIGH ST	WEST CHESTER PA 19382-0000				151 S MAIN ST UNIT 423	151 SO MAIN HOTEL & RESIDENCES	UNIT 423				
RPK07030005010	COCOLO LLC		503 LARCHMONT DR	WYOMING OH 45215-0000				151 S MAIN ST UNIT 501	151 SO MAIN HOTEL & RESIDENCES	UNIT 501				
RPK07030005030	CLOTFELTER RICHARD C	CLOTFELTER JAN E	215 5TH AVE S APT C202	KIRKLAND WA 98033-9008				151 S MAIN ST UNIT 503	151 SO MAIN HOTEL & RESIDENCES	UNIT 503				
RPK07030005050	BIG BURN LLC		WASHINGTON LIMITED LIABILITY C	8880 GROAT POINT DR	MEDINA WA 98039-0000			151 S MAIN ST UNIT 505	151 SO MAIN HOTEL & RESIDENCES	UNIT 505				
RPK07030005070	CANYON RUN LLC		1114 STATE ST # 310	SANTA BARBARA CA 93101-0000				151 S MAIN ST UNIT 507	151 SO MAIN HOTEL & RESIDENCES	UNIT 507				
RPK07030005080	WHITMAN DOUGLAS		1565 WEBSTER ST	PALO ALTO CA 94301-0000				151 S MAIN ST UNIT 508	151 SO MAIN HOTEL & RESIDENCES	UNIT 508				
RPK07030005100	BARRIER JIMMY RAY TRUSTEE	BARRIER PATRICIA WALLER TRUSTEE	PO BOX 376	MEDINA WA 98039-0000				151 S MAIN ST UNIT 510	151 SO MAIN HOTEL & RESIDENCES	UNIT 510				
RPK07030005120	HPPW LLC		999 THIRD AVE STE 3050	SEATTLE WA 98104-0000				151 S MAIN ST UNIT 512	151 SO MAIN HOTEL & RESIDENCES	UNIT 512				
RPK07030005140	HEILY ANDREW OBRYAN	HEILY COURTNEY	920 LAKESIDE AVE S	SEATTLE WA 98144-0000				514 S MAIN ST UNIT 514	151 SO MAIN HOTEL & RESIDENCES	UNIT 514				
RPK07070030000	ANDORA VILLA CONDOS 3 OWNERS		PO BOX 3736	KETCHUM ID 83340-0000					ANDORA VILLA #3 CONDO	COMMON AREA				
RPK07070030010	SV QUARTERS LLC		6041 77TH AVE SE	MERCER ISLAND WA 98040-0000				305 ANDORA LN # 144	ANDORA VILLA #3 CONDO	UNIT 1	.0556			
RPK07070030020	HARLAMERT DANE C	HARLAMERT KAARI	PO BOX 3667	KETCHUM ID 83340-3667				305 ANDORA LN 143	ANDORA VILLA #3 CONDO	UNIT 2	.0556			
RPK07070030030	TALBOT GARY J TRUSTEE	LABIA YOLANDA V TRUSTEE	PO BOX 3360	TRUCKEE CA 96160-0000				305 ANDORA LN 142	ANDORA VILLA #3 CONDO	UNIT 3	.0556			
RPK07070030040	INMAN JOHN A	INMAN NICOLE S	BOX 2853	KETCHUM ID 83340-0000				305 ANDORA LN 141	ANDORA VILLA #3 CONDO	UNIT 4	.0556			
RPK07070030050	TOKAREFF ANNE J		PO BOX 4041	KETCHUM ID 83340-0000				305 ANDORA LN 140	ANDORA VILLA #3 CONDO	UNIT 5	.0556			
RPK07070030060	MONGE THOMAS R	MONGE CYNTHIA P	BOX 307	SUN VALLEY ID 83353-0000				305 ANDORA LN 139	ANDORA VILLA #3 CONDO	UNIT 6	.0556			
RPK07070030070	RENZ JAKOB T		PO BOX 4027	KETCHUM ID				305	ANDORA VILLA #3 CONDO	UNIT 7	.0556			

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				83340-0000				ANDORA LN 138	#3 CONDO			
RPK07070030080	TAGLIO TORY JONATHAN		BOX 6306	KETCHUM ID 83340-0000				305 ANDORA LN 137	ANDORA VILLA #3 CONDO	UNIT 8	.0556	
RPK07070030090	LUNDBERG LARRY L TRUSTEE	LARRY LEE LUNDBERG 2003 TRUST	665 S 16TH STREET	SAN JOSE CA 95112-0000				305 ANDORA LN 136	ANDORA VILLA #3 CONDO	UNIT 9	.0556	
RPK07070030100	COVAULT JERRY	COVAULT LOIS	7850 STEGNER DR	MISSOULA MT 59808-0000				305 ANDORA LN 127	ANDORA VILLA #3 CONDO	UNIT 10	.0556	
RPK07070030110	CASHMAN JONI	DUNHAM MICHAEL	BOX 3148	SUN VALLEY ID 83353-0000				305 ANDORA LN 128	ANDORA VILLA #3 CONDO	UNIT 11	.0556	
RPK07070030120	MULICK LANI A		BOX 1754	KETCHUM ID 83340-0000				305 ANDORA LN 129	ANDORA VILLA #3 CONDO	UNIT 12	.0556	
RPK07070030130	SELES-SHELTON WENDY ANNE		PO BOX 483 WAY	LEMONT PA 16851-0483				305 ANDORA LN 135	ANDORA VILLA #3 CONDO	UNIT 13	.0556	
RPK07070030140	BLACKBURN ROBERT S TRUSTEE	BLACKBURN ELAINE TRUSTEES	1240 HOOVER	CARLSBAD CA 92008-0000				305 ANDORA LN 134	ANDORA VILLA #3 CONDO	UNIT 14	.0556	
RPK07070030150	SCHUCKERT WILLIAM F	WOODARD WENDY TRUSTEES	15100 N 78TH WAY #207	SCOTTSDALE AZ 85260-0000				305 ANDORA LN 133	ANDORA VILLA #3 CONDO	UNIT 15	.0556	
RPK07070030160	LEVY SCOTT		PO BOX 504	KETCHUM ID 83340-0000				305 ANDORA LN 132	ANDORA VILLA #3 CONDO	UNIT 16	.0556	
RPK07070030170	HAMBRICK PETER CALVIN TRUSTEE	PETER & JUDY HAMBRICK FAMILY TRUST	5421 HERON BAY	LONG BEACH CA 90803-0000				305 ANDORA LN 131	ANDORA VILLA #3 CONDO	UNIT 17	.0556	
RPK07070030180	GERMER PROPERTIES LLC		C/O KATI GERMER	1682 E LAKEWOOD DR	HOLLADAY UT 84117-7518			305 ANDORA LN 130	ANDORA VILLA #3 CONDO	UNIT 18	.0556	
RPK07770000000	HABITAT 2000 CONDO OWNERS		219 S 1ST AVE STE 101	HAILEY ID 83333-0000					HABITAT 2000 CONDOS	COMMON AREA		
RPK077700A0010	GONZALEZ KARLA R	GONZALEZ FELIX JR	BOX 476	KETCHUM ID 83340-0000				601 S LEADVILLE AVE A1	HABITAT 2000 CONDO	UNIT 1-A	GAR 1-A .044728	
RPK077700A0020	HEBEIN FREDERICK J TRUSTEE	HEBEIN CYNTHIA TRUSTEE	6162 SANDY HILL LN	YORBA LINDA CA 92886-0000				601 S LEADVILLE AVE A2	HABITAT 2000 CONDO	UNIT 2-A	GAR 2-A .044728	
RPK077700A0030	FIRST IDAHO INVEST CO	MANNINGWIGHT, MANNING, INC	BOX 700	KETCHUM ID 83340-0000				601 S LEADVILLE AVE A3	HABITAT 2000 CONDOS	UNIT 3-A	GAR 3-A .044306	
RPK077700A0040	FIRST IDAHO INVEST CO.	MANNING WIGHT, MANNING, INC	BOX 700	KETCHUM ID 83340-0000				601 S LEADVILLE AVE A4	HABITAT 2000 CONDO	UNIT 4-A	GAR 4-A .044306	
RPK077700A0050	FIRST IDAHO INVEST CO.	MANNING WIGHT, MANNING, INC	BOX 700	KETCHUM ID 83340-0000				601 S LEADVILLE AVE A5	HABITAT 2000 CONDO	UNIT 5-A	GAR 5-A .048638	
RPK077700A0060	FIRST IDAHO INVEST CO.	MANNING WIGHT, MANNING, INC	BOX 700	KETCHUM ID 83340-0000				601 S LEADVILLE AVE A6	HABITAT 2000 CONDO	UNIT 6-A	GAR 6-A .048638	
RPK077700A0070	JELTS LLC		11579 BATTLE POINT DR	BAINBRIDGE ISLAND WA 98110-0000				601 S LEADVILLE AVE A7	HABITAT 2000 CONDO	UNIT 7-A	GAR 7-A .053622	

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RPK077700A0080	FIRST IDAHO INVEST CO.	MANNING WIGHT, MANNING, INC	BOX 700	KETCHUM ID 83340-0000			601 S LEADVILLE AVE A8	HABITAT 2000 CONDO	UNIT 8-A	GAR 8-A .053622		
RPK077700B0010	MANNING OLSEN	MANNING DAVIS	BOX 700	KETCHUM ID 83340-0000			601 S LEADVILLE AVE B1	HABITAT 2000 CONDO	UNIT 1-B	GAR 1-B .044728		
RPK077700B0020	FIRST IDAHO INVEST CO	MANNING WIGHT, MANNING, INC	BOX 700	KETCHUM ID 83340-0000			601 S LEADVILLE AVE B2	HABITAT 2000 CONDO	UNIT 2-B	GAR 2-B .044728		
RPK077700B0030	ADLER SHEILA MICKIE	HAXBY MICHAEL L	PO BOX 2709	SUN VALLEY ID 83353-0000			601 S LEADVILLE AVE B3	HABITAT 2000 CONDOS	UNIT 3-B	GAR 3-B .044306		
RPK077700B0040	FIRST IDAHO INVEST CO.	MANNING WIGHT, MANNING, INC	BOX 700	KETCHUM ID 83340-0000			601 S LEADVILLE AVE B4	HABITAT 2000 CONDO	UNIT 4-B	GAR 4-B .044306		
RPK077700B0050	CALLAHAN NANCY	LYTLE CAREY J	C/O MICHELE MESSMER	515 W EDWARDS AVE	NAMPA ID 83686-0000		601 S LEADVILLE AVE B5	HABITAT 2000 CONDO	UNIT 5-B	GAR 5-B .048638		
RPK077700B0060	FIRST IDAHO INVEST CO.	MANNING WIGHT, MANNING, INC	BOX 700	KETCHUM ID 83340-0000			601 S LEADVILLE AVE B6	HABITAT 2000 CONDO	UNIT 6-B	GAR 6-B .048638		
RPK077700B0070	FIRST IDAHO INVEST CO.	MANNING WIGHT, MANNING, INC	BOX 700	KETCHUM ID 83340-0000			601 S LEADVILLE AVE B7	HABITAT 2000 CONDO	UNIT 7-B	GAR 7-B .053622		
RPK077700B0080	FIRST IDAHO INVEST CO.	MANNING WILFORD, MANNING, INC	BOX 700	KETCHUM ID 83340-0000			601 S LEADVILLE AVE B8	HABITAT 2000 CONDO	UNIT 8-B	GAR 8-B .053622		
RPK077700C0010	SEIBEL SHELLEY L		BOX 2904	KETCHUM ID 83340-0000			601 S LEADVILLE AVE C1	HABITAT 2000 CONDO	UNIT 1-C	GAR 1-C .059466		
RPK077700C0020	HABITAT INVESTORS		BOX 700	KETCHUM ID 83340-0000			601 S LEADVILLE AVE C2	HABITAT 2000 CONDO	UNIT 2-C	GAR 2-C .059466		
RPK077700C0030	CARNES VICKI TRUSTEE	LITTLE WREN REVOCABLE TRUST	PO BOX 1812	KETCHUM ID 83340-0000			601 S LEADVILLE AVE C3	HABITAT 2000 CONDO	UNIT 3-C	GAR 3-C .057946		
RPK077700C0040	V & E LLC		13535 NW OVERTON	PORTLAND OR 97229-0000			601 S LEADVILLE AVE C4	HABITAT 2000 CONDO	UNIT 4-C	GAR 4-C .057946		
RPK08390000000	LA RESIDENCE HOMEOWNERS		C/O R.E. CRIST	PO BOX 2326	KETCHUM ID 83340-0000			LA RESIDENCE CONDOS	COMMON AREA			
RPK08390000010	BARITEAU JACK E JR TRUSTEE	JACK E BARITEAU JR SEPARATE PROPERTY TRUST UTA 10/2/96	PO BOX 84	SUN VALLEY ID 83353-0000			291 S LEADVILLE AVE A1	LA RESIDENCE CONDOS	UNIT 1 A .54	SEC 18 4N 18E		
RPK08390000020	GREGORY RITA ANN TRUSTEE	RITA A GREGORY TRUST	28562 OSO PKWY STE D-213	SANTA MARGARITA CA 92688-0000			291 S LEADVILLE AVE A2	LA RESIDENCE CONDOS	UNIT 2 A .46	SEC 18 4N 18E		
RPK08880000000	RIVER STREET PROF CONDO OWNER		219 S 1ST AVE STE 101	HAILEY ID 83333-0000				RIVER STREET PROFESSIONAL	COMMON AREA	SEC 18, 4N 18E		
RPK08880000010	220 E RIVER ST LLC		PO BOX 249	KETCHUM ID 83340-0000			220 E RIVER ST 1	RIVER STREET PROFESSIONAL	CONDOS ASSOCIATION	UNIT 1 50% SEC 18 4N 18E		
RPK08880000020	220 E RIVER ST LLC		PO BOX 249	KETCHUM ID 83340-0000			220 E RIVER ST 2	RIVER STREET PROFESSIONAL	CONDOS ASSOCIATION	UNIT 2 50%		

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RPK09520000010	KOZLOWSKI KIRSTIN T		PO BOX 1373	SUN VALLEY ID 83353- 0000				361 S LEADVILLE AVE 1	TRAIL CREEK CONDO AMENDED	UNIT 1 BLK 2	.13		
RPK09520000020	JOHN SPILLER & MARIANNE WALTERS REVOCABLE TRUST 1/9/18	WALTERS DAVID W	33 TWAIN HARTE LN	SAN RAFAEL CA 94901- 5258				361 S LEADVILLE AVE 2	TRAIL CREEK CONDO AMENDED	UNIT 2 BLK 2	.12 SEC 18 4N 18E		*HOLD SEE NOTES
RPK09520000030	HILL RYAN CHRISTOPHER	HILL RODNEY C	82 B SURFSIDE AVE	SURFSIDE CA 90743- 0000				361 S LEADVILLE AVE 3	TRAIL CREEK CONDO AMENDED	UNIT 3 BLK 2	.19 SEC 18 4N 18E		
RPK09520000040	NORTON DAVID M & BRENDA TRUSTEES	NORTON BRENDA K TRUSTEES	BOX 1535	KETCHUM ID 83340-0000				361 S LEADVILLE AVE 4	TRAIL CREEK CONDO AMENDED	UNIT 4 BLK 2	.17 SEC 18 4N 18E		
RPK09520000050	MCB REALTY WEST LLC		395 N VIRGINIA ST	SALT LAKE CITY UT 84103-0000				361 S LEADVILLE AVE 5	TRAIL CREEK CONDO AMENDED	UNIT 5 BLK 2	.16 SEC 18 4N 18E		
RPK09520010000	J PETERMAN DEVELOPMENT LLC A WISCONSIN LIMITED LIABILIT		2809 CURRY PKWY	MADISON WI 53713- 0000				341 S LEADVILLE AVE	TRAIL CREEK CONDOS AMENDED	BLOCK 1 14,992SF .34	SEC 18 4N 18E		
RPK09520020000	TRAIL CREEK OWNERS INC	C/O DAVID W WALTERS	33 TWAIN HARTE LANE	SAN RAFAEL CA 94901- 0000				361 S LEADVILLE AVE	TRAIL CREEK CONDOS AMENDED	BLOCK 2- COMMON AREA	23,906SF .55		
RPK09590000000	TRAIL CREEK LLC		BOX 2251	KETCHUM ID 83340-0000					TRAIL CREEK CROSSING CONDO	COMMON AREA			
RPK095900000A0	LEZAMIZ JOHN FAM LMT PTNSHIP		847 CANYON SPRINGS RD	TWIN FALLS ID 83301- 0000				321 S LEADVILLE AVE A	TRAIL CREEK CROSSING CONDO	UNIT A (COMM)	.80% SEC 18 4N 18E		
RPK095900000B0	ADOLPH ROBERT J	STRECKER JOAN E	9250 SE SHORELAND DR	BELLEVUE WA 98004- 6519				321 S LEADVILLE AVE 106	TRAIL CREEK CROSSING CONDO	UNIT B	8.63% SEC 18 4N 18E		
RPK095900000C0	MOODIE KATHLEEN MARIE	MOODIE JOHN T	921 E LAKE SAMMAMISH PKWY NE	SAMMAMISH WA 98074- 0000				321 S LEADVILLE AVE 204	TRAIL CREEK CROSSING CONDO	UNIT C	8.08% SEC 18 4N 18E		
RPK095900000D0	SAHLBERG JOHN THOMAS	SAHLBERG SUSAN CARLTON	2417 N 20TH ST	BOISE ID 83702-0000				321 S LEADVILLE AVE 203	TRAIL CREEK CROSSING CONDO	UNIT D	7.98% SEC 18 4N 18E		
RPK095900000E0	FORREST DON E	FORREST MARY GA	1404 NORWEGIAN WOOD CT	MANSFIELD TX 76063- 0000				321 S LEADVILLE AVE 101	TRAIL CREEK CROSSING CONDO	UNIT E	6.86% SEC 18 4N 18E		
RPK095900000F0	LEZAMIZ JOHN FAM LMT PTNSHIP		847 CANYON SPRINGS RD	TWIN FALLS ID 83301- 0000				321 S LEADVILLE AVE 201	TRAIL CREEK CROSSING CONDO	UNIT F	6.43 SEC 18 4N 18E		
RPK095900000G0	DRESSMAN FRANK C TRUSTEE	DRESSMAN LINDA L TRUSTEE	700 LA PENINSULA BLVD	TWIN DOLPHINS # 306	NAPLES FL 34113- 0000			321 S LEADVILLE AVE 202	TRAIL CREEK CROSSING CONDO	UNIT G	8.14% SEC 18 4N 18E		
RPK095900000H0	MASSEY PAMELA L TRUSTEE	PAMELA MASSEY FAMILY TRUST UTA 11/6/2018	1005 S HERON POINTE WAY	EAGLE ID 83616-0000				321 S LEADVILLE AVE 102	TRAIL CREEK CROSSING CONDO	UNIT H	7.88% SEC 18 4N 18E		
RPK095900000I0	KIEBURTZ STEVEN J	KIEBURTZ APRIL H	4015 E MADISON ST	SEATTLE WA 98112-4760				321 S LEADVILLE AVE 103	TRAIL CREEK CROSSING CONDO	UNIT I	8.00% SEC 18 4N 18E		
RPK095900000J0	TRAIL CREEK J LLC	HANSON ROBERT C TRUSTEE	C/O MELANIE HANSON	MELVICK LLC 1005 13TH ST	BELLINGHAM WA 98225-			321 S LEADVILLE	TRAIL CREEK CROSSING	UNIT J	7.95% SEC 18 4N 18E		

<https://maps.co.blaine.id.us/blaine/adjqprclnum.php?rp=RPK0000082003A>

6/7

6/17/2019

Blaine County GIS Map Services 300 ft Adjoiner Results

						0000		AVE 104	CONDO				
RPK095900000K0	HANSON SCOTT M	HANSON KAREN W	PO BOX 700	FOX ISLAND WA 98333-0700				321 S LEADVILLE AVE 105	TRAIL CREEK CROSSING CONDO	UNIT K	8.63% SEC 18 4N 18E		
RPK095900000L0	BENSON THOMAS	BENSON DELL-ANN	PO BOX 3819	KETCHUM ID 83340-0000				321 S LEADVILLE AVE 205	TRAIL CREEK CROSSING CONDO	UNIT L	7.93% SEC 18 4N 18E		
RPK095900000M0	DUNCAN DAVID D		3317 OCEAN BLVD	CORONA DEL MAR CA 92625-3257				321 S LEADVILLE AVE 206	TRAIL CREEK CROSSING CONDO	UNIT M	5.72% SEC 18 4N 18E		
RPK095900000N0	LIVINGSTON KEVIN		135 MARLIN AVE	MILL VALLEY CA 94941-0000				321 S LEADVILLE AVE 207	TRAIL CREEK CROSSING CONDO	UNIT N	6.97% SEC 18 4N 18E		
RPK096900000000	WALDRON OFFICE CONDO OWNERS		219 S 1ST AVE STE 101	HAILEY ID 83333-0000					WALDRON OFFICE CONDOS	COMMON AREA	SEC 18, 4N 18E		
RPK096900000010	FELTON PATRICIA LENTZ TRUSTEE	PATRICIA LENTZ FELTON LIVING TRUST	PO BOX 1674	KETCHUM ID 83340-0000				200 E RIVER ST 1	WALDRON OFFICE CONDOS	UNIT 1	.33 SEC 18, 4N 18E		
RPK096900000020	SAWTOOTH BOARD OF REALTORS INC		BOX 1937	KETCHUM ID 83340-0000				200 E RIVER ST 2	WALDRON OFFICE CONDOS	UNIT 2	.33 SEC 18, 4N 18E		
RPK096900000030	MONTANA GOES TO IDAHO LLC		PO BOX 1674	KETCHUM ID 83340-0000				200 E RIVER ST 3	WALDRON OFFICE CONDOS	UNIT 3	.33 SEC 18, 4N 18E		
RPK4N180180120	SHARBININ RICHARD J		PO BOX 3820	KETCHUM ID 83340-3820				307 S MAIN ST	KETCHUM	FR NWSW TL 4799 A	SEC 18 4N 18E		
RPK4N180180510	KETCHUM KORRAL LLC		BOX 1607	BELLEVUE WA 98009-0000				310 S MAIN ST	KETCHUM	FR NESW & SESW TL 2631	SEC 18 4N 18E		

<https://maps.co.blaine.id.us/blaine/adjqprclnum.php?rp=RPK0000082003A>

7/7

EXHIBIT | 02.15

WAIVER LIST

LIST OF WAIVERS

SETBACKS - 17.12.030.C

Typically 1' for ever 3' in building height for the sides and 15' for the front

The Ketchum Boutique Hotel is honoring a 25'-0" set-back on the East boundary along Highway 75, which also currently includes a +/- 20'-0" additional buffer from the Idaho Department of Transportation right-of-way for future improvements. Also the project is currently in-set from the riparian corridor and utility easement requirements along the Trail Creek boundary to the south. To address the larger scale of the Limelight hotel across River Street, the boutique hotel is currently using a 5 foot set-back along both the North and West property boundaries. With the current plan of improvements in the right-of-way, this impact will be minimized and reflect the context of the Limelight and Auberge hotels with more direct relationships.

LIST OF WAIVERS

FLOOR AREA RATIO - 17.124.040.A

Tourist zone typically allows 1.6 when community housing is included in the project

Current floor area ratio calculations are estimated at a value of 1.9. Ketchum City code allows a maximum value of 1.6 in the Tourist Zone.

Based on the slope of the lot, areas that could typically be considered basement (i.e. Employee Housing) are being included in the Floor Area Ratio (FAR) calculation. As the building is being stepped to accommodate the terrain, the floor area and building mass are appropriately scaled to the intent of the FAR code and Gateway Massing Study.

LIST OF WAIVERS

HEIGHT - 17.12.030.C

Tourist zone is typically 35'-0" maximum

Currently, the site is zoned as a 'T - Tourist zone', which allows hotels to be 4-stories along River Street.

Based on fitting into the context of the adjacent Limelight Hotel at 68'-0", and the proposed hotel across Main Street at 58'-0", we are proposing a waiver of the 50'-0" maximum, to allow a 60'-0" maximum height along River Street. This height will still allow the upper levels of the adjacent developments clear views over the top of the hotel, while providing nice views from the Boutique Hotel to the surroundings.

The massing of the building will terrace with the topography to below approximately 52'-0" across the site, with a portion of overlap / taller volume occurring at the fourth floor roof-top bar / patio space. The architecture will be minimal and simple in this location, and the amenity of a roof-top bar that will be provided to the community will hopefully be well received by the public.

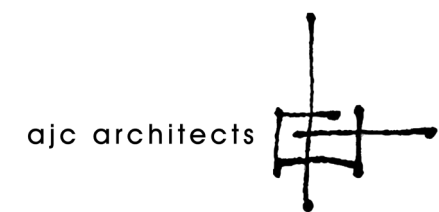
LIST OF WAIVERS

PUD ELIGIBLE LOT SIZE - 06.08.080.A

PUD minimum lot size is typically 3 acres

Current lot size requirements for a PUD application is 3.0 acres, however the Ketchum Boutique lot is currently 1.08 acres total. This lot has been slated for a larger hotel development in previous Gateway planning studies, and this waiver is needed to make development of this specific lot economically feasible. The density will not be excessive in comparison to adjacent lots on the River and Main intersection, and the proposed development will fit into the overall development master plan of the Gateway area.

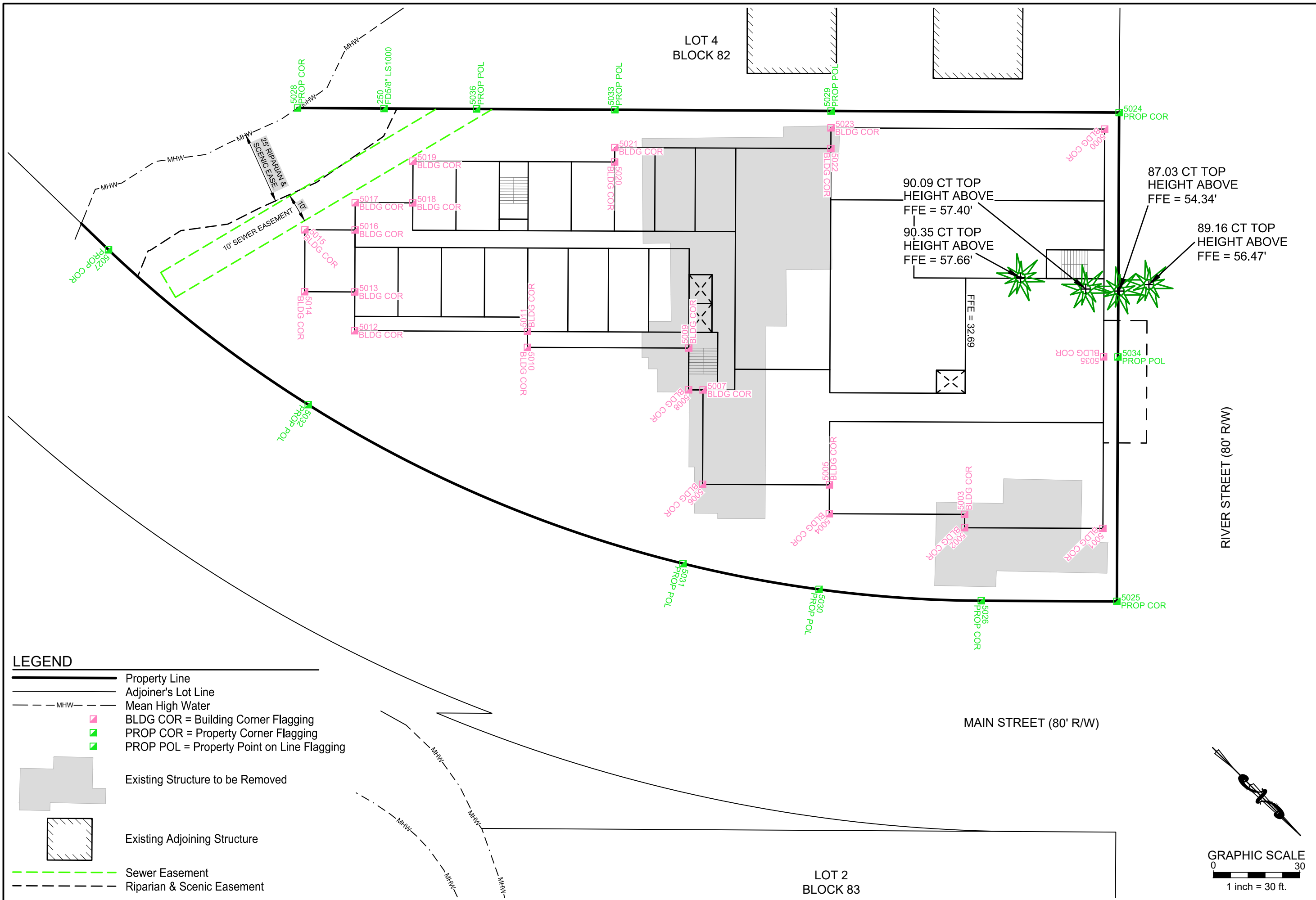
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703 east 1700 south | salt lake city, utah 84105
ph: 801.466.8818 | www.ajcarchitects.com



180 n university ave, suite 200 | provo, utah 84601
ph: 801.655.1998 | www.pegdev.com



- LEGEND**
- Property Line
 - Adjoinder's Lot Line
 - Mean High Water
 - BLDG COR = Building Corner Flagging
 - PROP COR = Property Corner Flagging
 - PROP POL = Property Point on Line Flagging
 - Existing Structure to be Removed
 - Existing Adjoining Structure
 - Sewer Easement
 - Riparian & Scenic Easement

THE KETCHUM BOUTIQUE HOTEL
 LOCATED WITHIN SECTION 18, T.4 N., R.18 E., B.M., CITY OF KETCHUM, BLAINE COUNTY, IDAHO
 PREPARED FOR PEG KETCHUM HOTEL LLC

PROJECT INFORMATION
 P:\skskproj\6003-05\dwg\Construction\6003-05 Const 2019-07-25.dwg 07/29/19 8:34:01 AM

DESIGNED BY	CT	DRAWN BY	SMF	CHECKED BY
REUSE OF DRAWINGS These drawings, or any portion thereof, shall not be used on any Project except by agreement in writing with Galena Engineering, Inc.				
GALENA ENGINEERING, INC. Civil Engineers & Land Surveyors 317 North River Street Hailey, Idaho 83333 (208) 786-1705 (208) 786-4612 fax email galena@galena-engineering.com				
NO.	DATE	BY	REVISIONS	
EX				







TRIBUTE
PORTFOLIO

{ Stay Independent. }

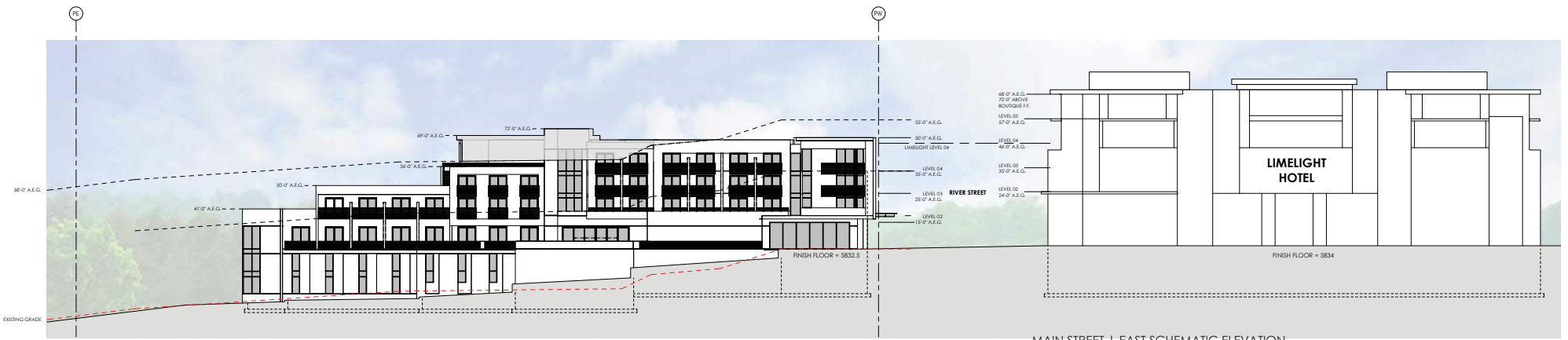


Comparative Hotel PUD Summary Chart

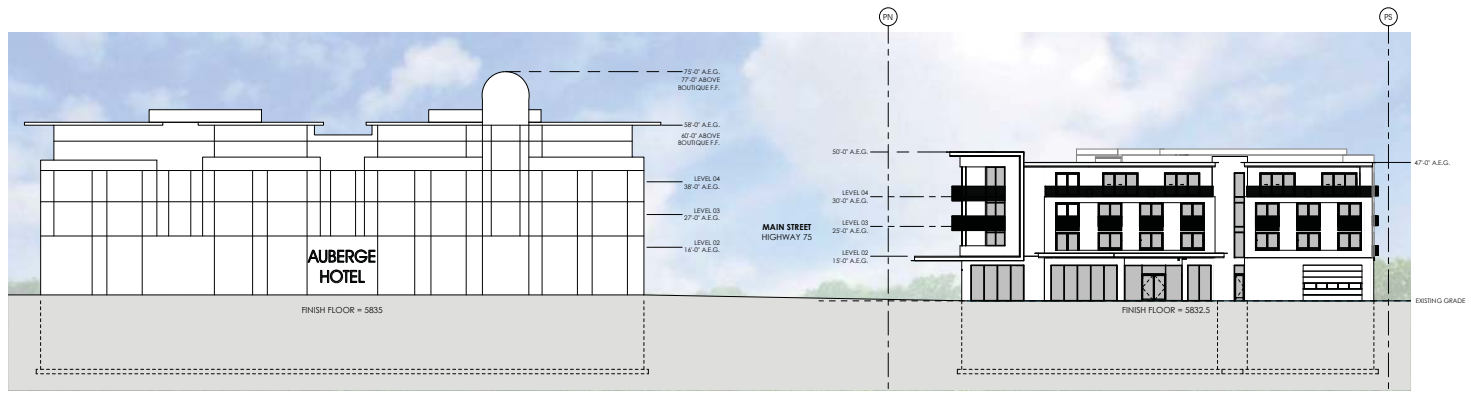
Project Name	Zoning District	Project Address or Location	Maximum number of stories visible above grade (no parking garages located completely below grade on all sides are included)	Maximum height	FAR of all approved buildings on subject development parcel	Development parcel size	Gross square footage of hotel / hotel core	Project Status
Ketchum Boutique Hotel	Tourist	251 S. Main Street (southwest corner of Main and River)	<u>6</u> from lowest grade on site (includes partial-story rooftop bar) <u>4</u> from River Street (partial-story rooftop bar does not have River Street frontage)	<u>72'</u> from lowest grade on site <u>58'</u> or less from River Street	1.9	1.08 acres	135,915 square feet (includes below grade/partially below grade parking)	Proposed
Limelight	Community Core	151 S. Main Street (northwest corner of Main and River)	<u>5</u>	<u>68'</u>	2.7	1.08 acres	231,400 square feet (includes below grade parking)	Complete
Bariteau hotel (formerly "Auberge")	Tourist	200 S. Main Street (southeast corner of Main and River)	<u>7</u> from lowest grade on site (includes partial-story rooftop observatory) <u>4</u> from River Street (partial-story rooftop observatory also has River Street frontage)	<u>103'</u> from lowest grade on site <u>58'</u> from River Street, not including observatory <u>75'</u> from River Street, including observatory	2.4	0.93 acres	149,325 square feet (includes below grade/partially below grade parking)	Under construction
River Run Master Plan (Sun Valley Resort)	Tourist	Existing skier pick up/drop off and parking lot on the east side of the Big Wood River	<u>Undefined</u> ("tent" diagram representing schematic building mass approved)	<u>110'</u>	1.4	21.76 acres*	250,000-300,000 square feet range for hotel core (excludes below grade parking)	Entitled, not yet started
Warm Springs Ranch Resort	Tourist	South of Warm Springs Road between Bald Mountain Road and Geezer Alley	<u>Undefined</u> ("tent" diagram representing schematic building mass approved)	<u>93'</u>	1.43	13.72 acres**	538, 151 for hotel core (excludes below grade parking)	Entitled, not yet started

* Represents the Hotel Core "block" of the River Run Master Plan

** Represents Block 1 of the Warm Springs Ranch Resort master development



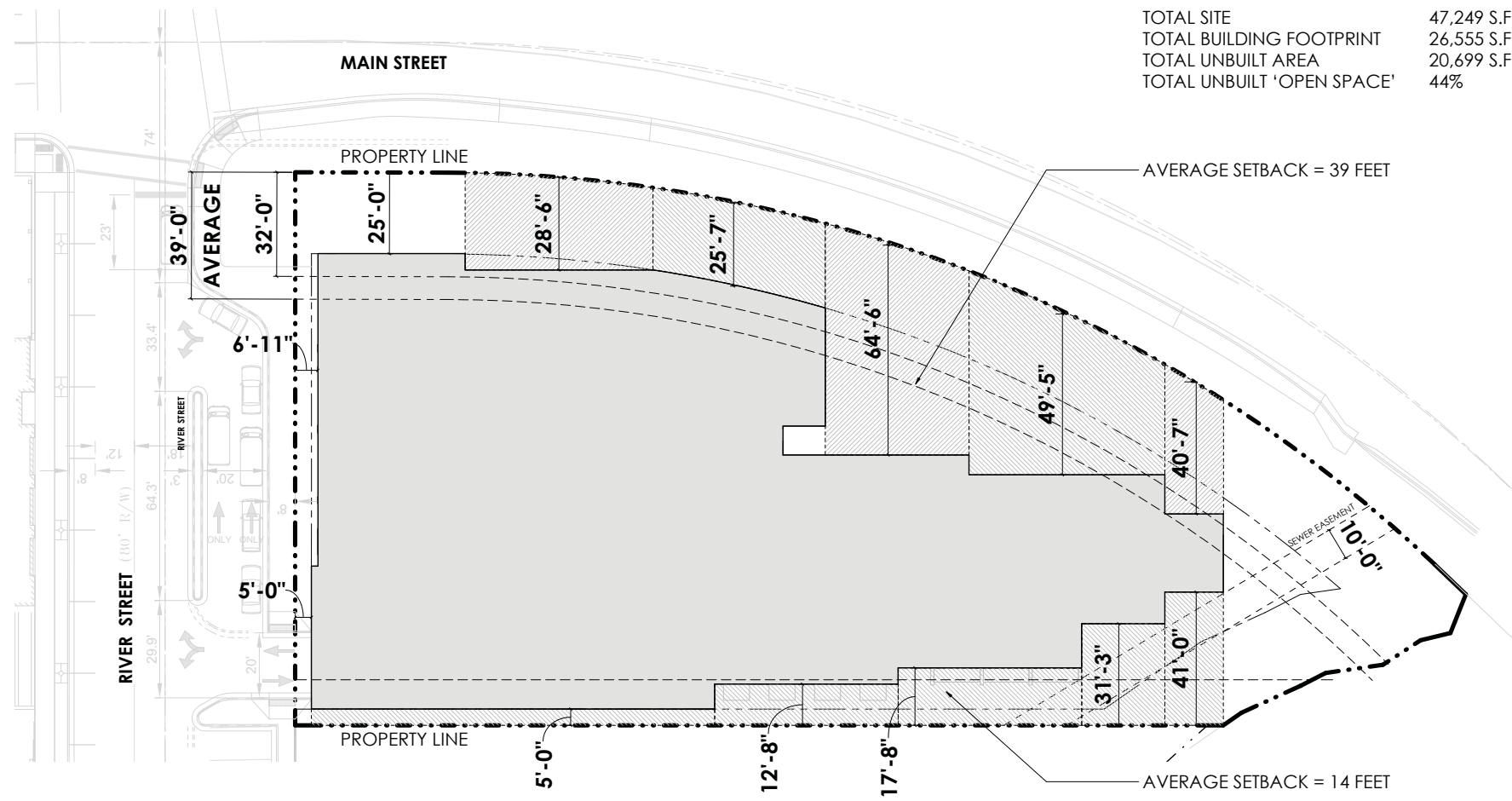
MAIN STREET | EAST SCHEMATIC ELEVATION
ADJACENT TO LIMELIGHT HOTEL



RIVER STREET | NORTH SCHEMATIC ELEVATION
ADJACENT TO APPROVED AUBERGE HOTEL

HEIGHT ANALYSIS - CONTEXTUAL ELEVATIONS

TOTAL SITE	47,249 S.F.
TOTAL BUILDING FOOTPRINT	26,555 S.F.
TOTAL UNBUILT AREA	20,699 S.F.
TOTAL UNBUILT 'OPEN SPACE'	44%



SETBACK 17.12.030.C

WAIVER

BUILDING HEIGHT

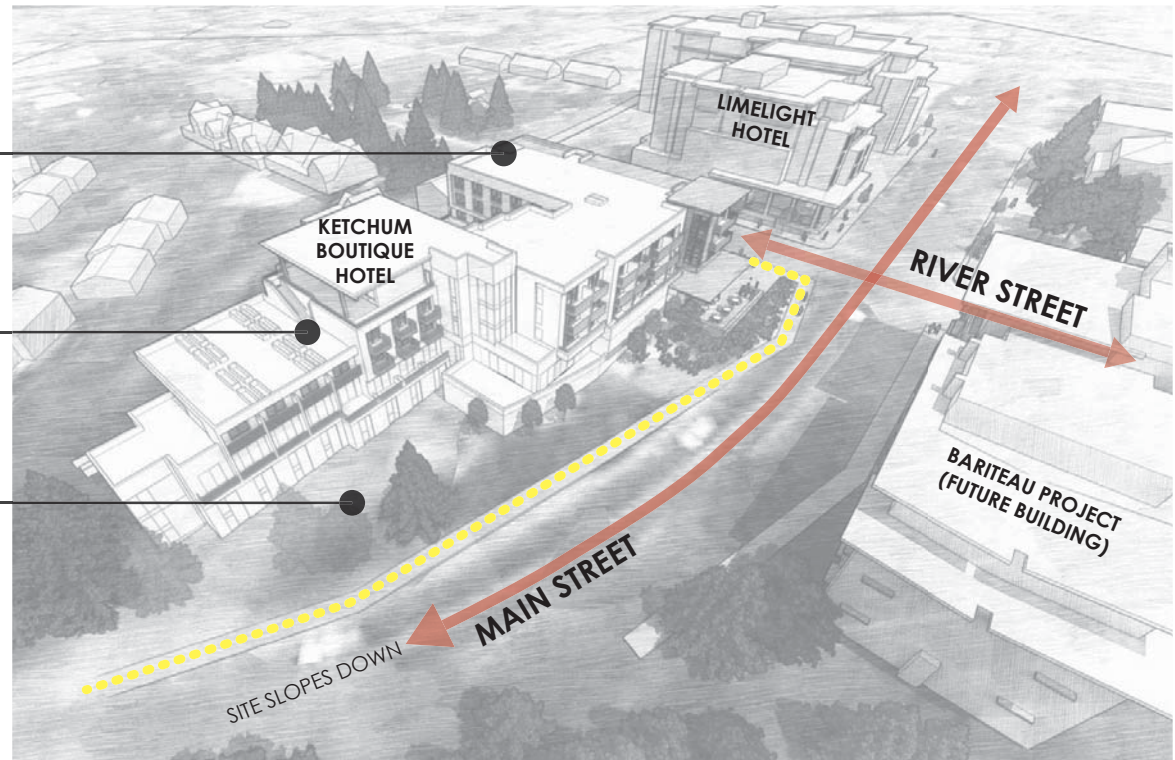
CURRENTLY DESIGNED, THE BOUTIQUE HOTEL IS PROPOSED TO BE A FOUR-STORY STRUCTURE ON RIVER STREET, AND STEP / TERRACE DOWN ALONG TRAIL CREEK.

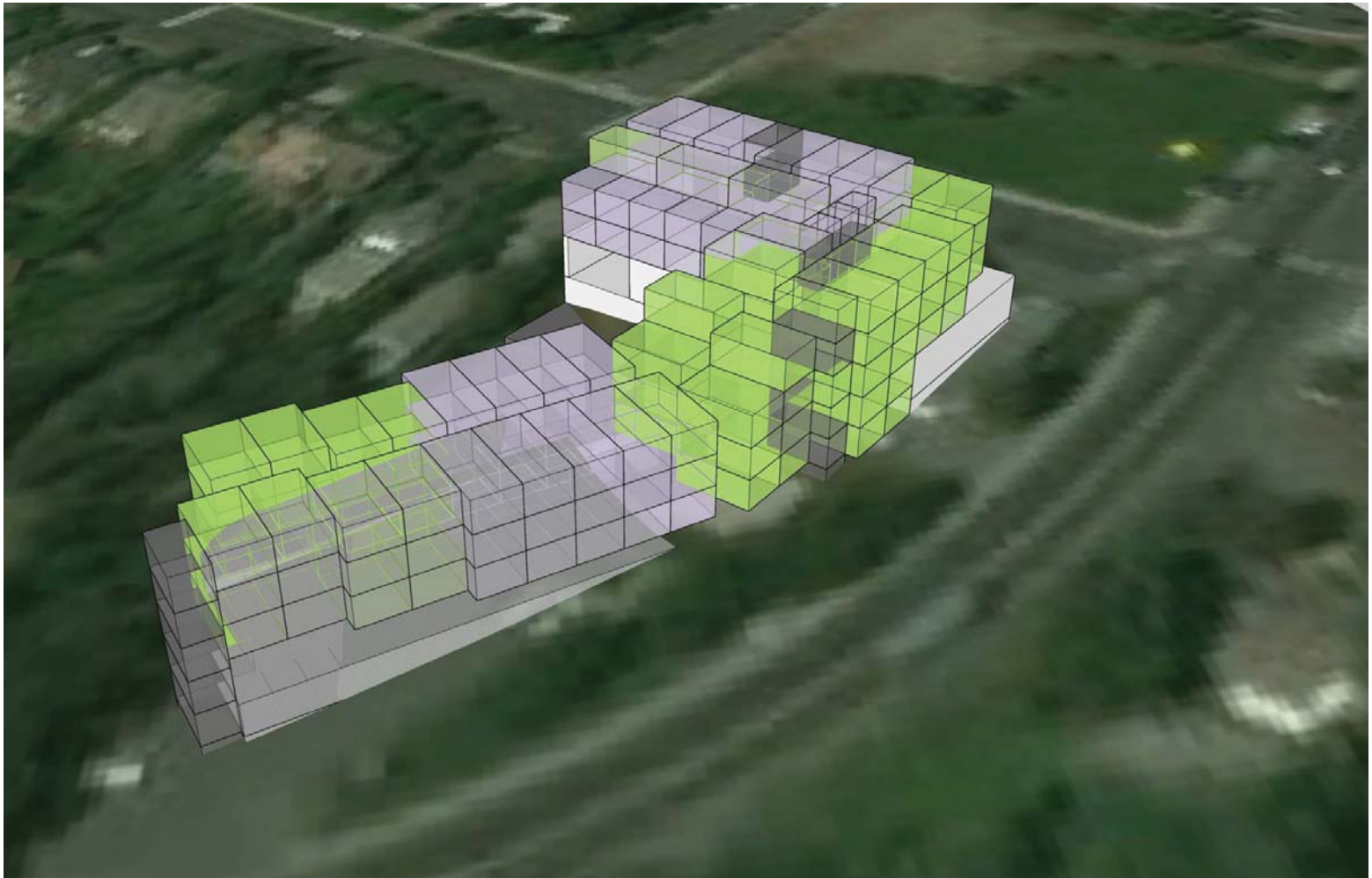
BUILDING MASS AND SCALE

IN RESPONSE TO THE 30-FOOT ELEVATION CHANGE CONDITION OF THE SITE, THE BUILDING MASS IS BEING BROKEN INTO SMALLER VOLUMES THAT TERRACE AND STEP WITH THE TOPOGRAPHY TO BREAK DOWN THE SCALE AS IT MOVES TOWARDS TRAIL CREEK.

STEEP SLOPING SITE

WITH STEEP SLOPES ENTERING TOWN ALONG MAIN STREET, THE BUILDING WILL TERRACE ALONG WITH THE CHANGE IN GRADE TO SOFTEN THE MASS AND CREATE SEAMLESS TRANSITION INTO DOWNTOWN.

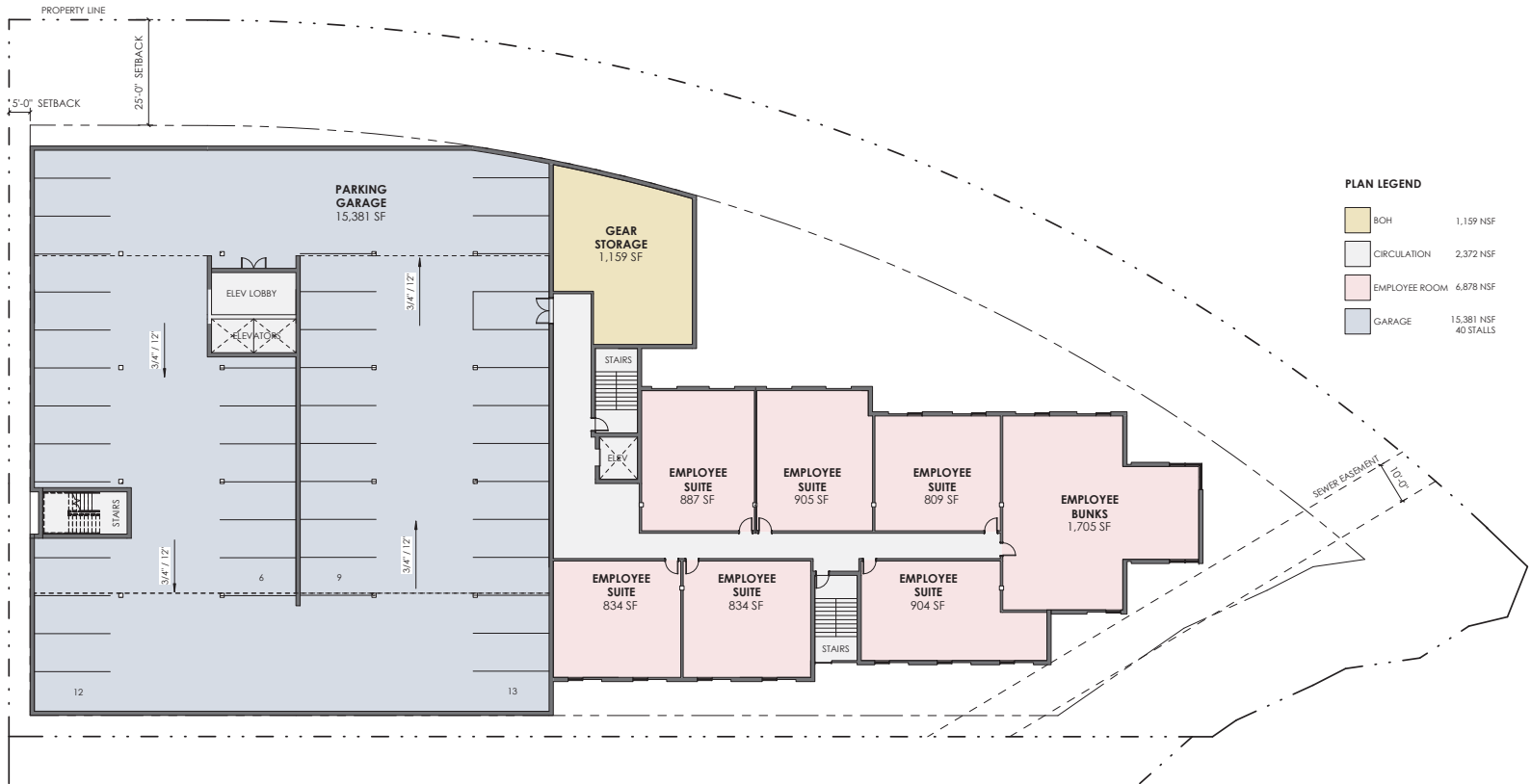




MASSING STUDY



MASSING STUDY - RESOLUTION



FLOOR PLANS



GARAGE SECTION



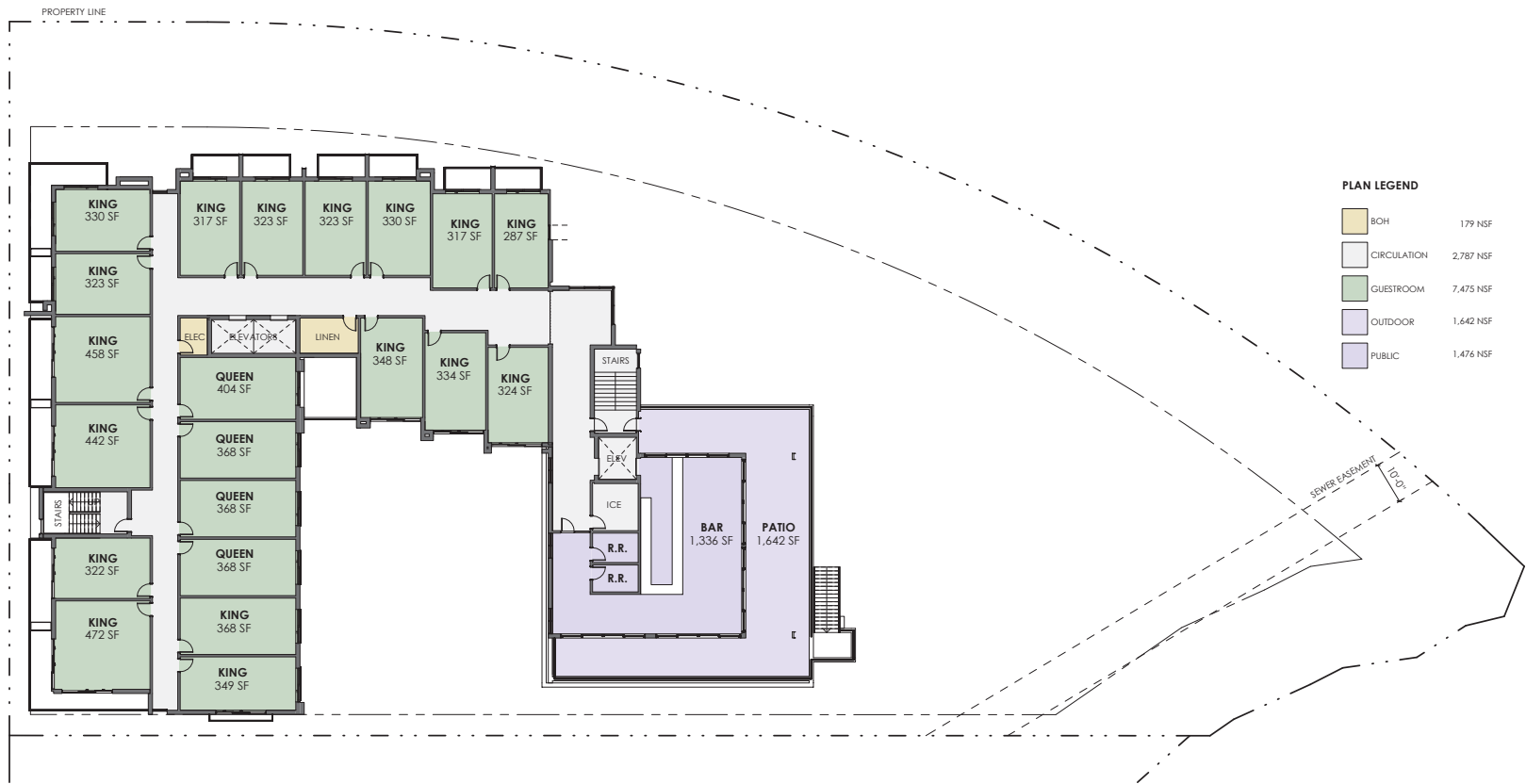
FLOOR PLANS



FLOOR PLANS



FLOOR PLANS



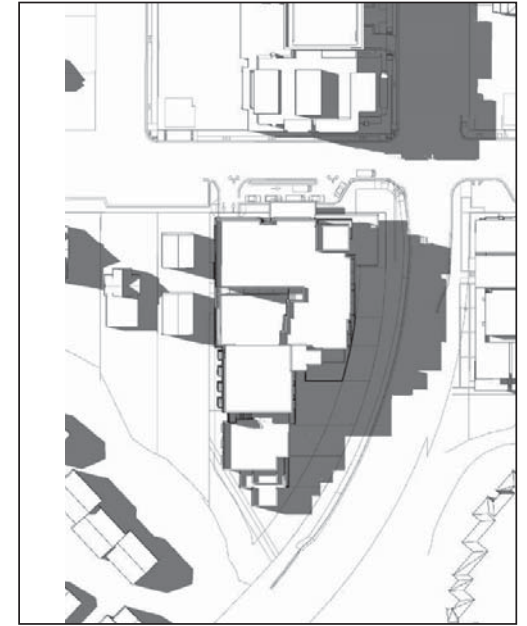
FLOOR PLANS



MARCH 21 - 9:00 AM



MARCH 21 - 12:00 PM



MARCH 21 - 3:00 PM

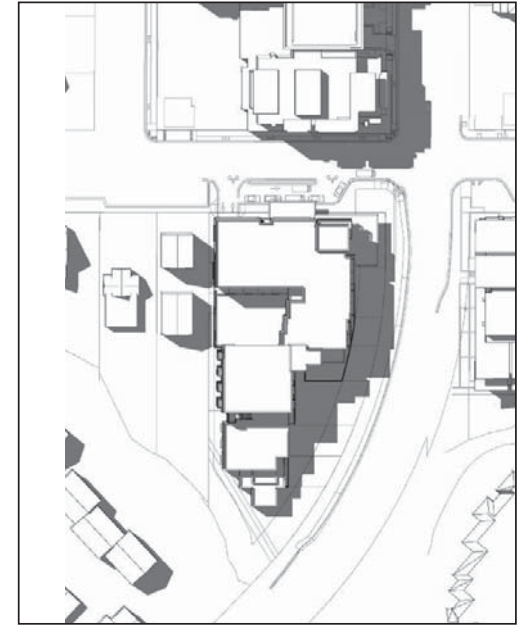
SHADOW STUDIES



JUNE 21 - 9:00 AM



JUNE 21 - 12:00 PM



JUNE 21 - 3:00 PM

SHADOW STUDIES



SEPT 21 - 9:00 AM

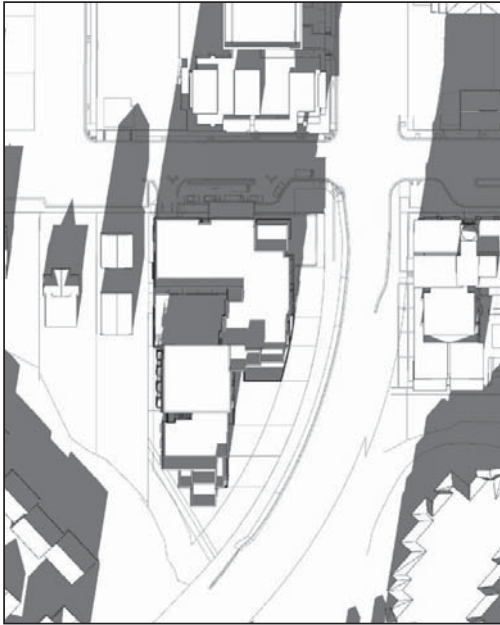


SEPT 21 - 12:00 PM



SEPT 21 - 3:00 PM

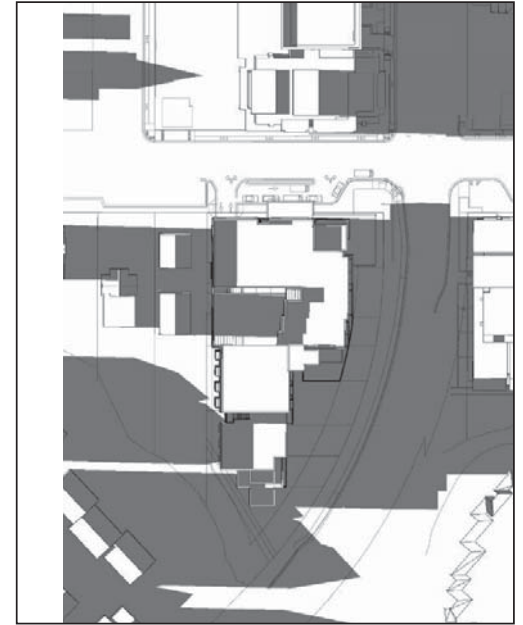
SHADOW STUDIES



DEC 21 - 9:00 AM



DEC 21 - 12:00 PM



DEC 21 - 3:00 PM

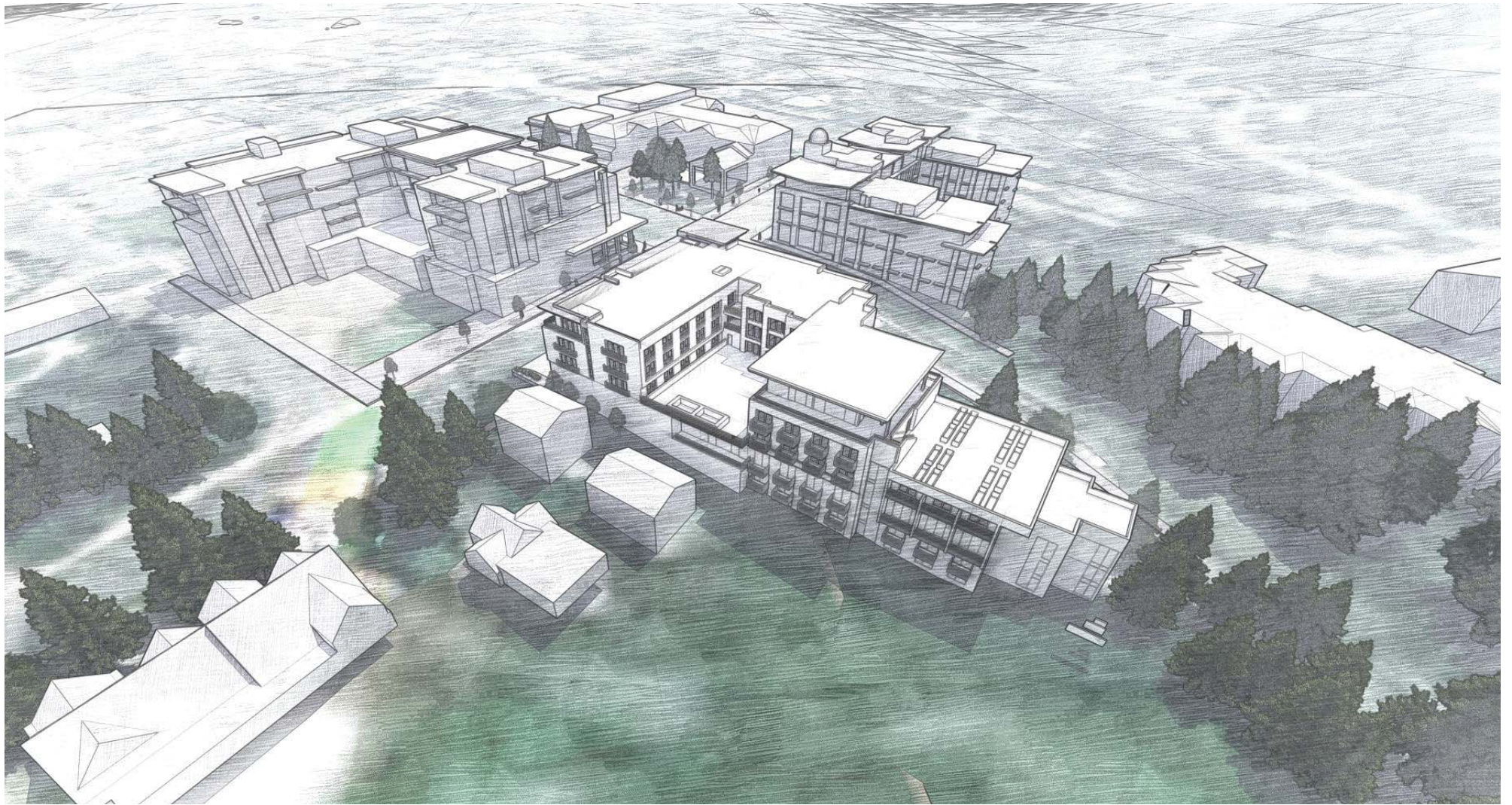
SHADOW STUDIES



PERSPECTIVE



PERSPECTIVE



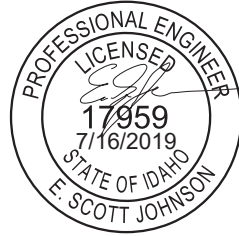
PERSPECTIVE



PERSPECTIVE

MEMORANDUM

Date: July 16, 2019
To: Nick Blayden, PEG Companies
From: Hales Engineering
Subject: **Ketchum Boutique Hotel Parking Study**



UT19-1441

This memorandum discusses the parking study completed for the proposed Ketchum Boutique Hotel located in Ketchum, Idaho. The study will identify the City of Ketchum parking requirements, the parking demand rates identified by the Institute of Transportation Engineers (ITE) and propose methods for reducing parking demand at the proposed hotel.

Project Description

The proposed Ketchum Boutique Hotel is located on the southwest quadrant of the Main Street (SH-75) / River Street intersection in Ketchum, Idaho. A vicinity map of the project site is shown in Figure 1. The proposed hotel will have 100 guestrooms, with an onsite restaurant, bar, meeting space, and other typical hotel amenities. The hotel will also have on-site employee housing to accommodate up to 30 employees. The site plan calls for 90 off-street parking spaces in an underground parking garage.

A market study for the proposed hotel projects that weekend occupancy at the hotel will range from 42% to 93% throughout the year, with an average occupancy of 72%.

City of Ketchum Parking Requirements

The City of Ketchum parking requirements set forth for this land use in the Ketchum Municipal Code (KMC), Title 17, Chapter 17.125 are as follows:

Restaurant/Bar - First 5,500 gross square feet are exempt from parking requirements. Parking required for additional square footage at a ratio of 1 space per 1,000 gross square feet.

Hotel (Excluding Restaurant/Bar) - 1 space per 1,000 gross square feet.

Employee Housing - 0 parking space required for units under 750 square feet, 1 parking space required for units 751-2,000 square feet, 2 parking spaces required for units 2,001 square feet or more.

The City also requires that the six on-street parking spaces that will be eliminated with the construction of this project be offset in the off-street parking garage.



Figure 1: Site vicinity map of the project in Ketchum, Idaho

Parking Demand Rate Calculations

Hales Engineering calculated the required parking according to the rates discussed in the previous section. These calculations are summarized in Table 1. As shown in Table 1, the planned uses for this site would require 87 parking spaces. The site plan for the project identifies a supply of 90 parking spaces on-site; therefore, the project is **overparked by 3 spaces**.

Table 1
Ketchum Gateway Hotel Parking Study
Required Parking Calculation

Use	Quantity	Unit	Total Required Spaces
Restaurant / Bar	6,435	Square Feet	1
Hotel	66,000	Square Feet	66
Employee Housing (751-2,000 Square Feet)	14	Units	14
On-Street Parking Offset	6	Spaces	6
Total			87

SOURCE: Hales Engineering, July 2019

Parking Demand Management

It may be desirable to reduce the parking demand on site further. This can be accomplished by providing amenities and services that eliminate the need for a hotel employee or guest to use a personal vehicle. Hales Engineering identifies the following methods to reduce parking demand on site:

Guest Shuttle – Providing a shuttle between the airport and the hotel allows guests to stay at the hotel without parking a personal vehicle on site. The shuttle can also be used to transport guests to / from various locations in the area, including dining, shopping, and recreation.

Employee Shuttle – An employee shuttle can be used to transport employees between the hotel and an offsite location, eliminating the need for employees to drive a personal vehicle to work.

Car Share Program – A car share program consists of having a vehicle on site that can be checked out by employees and guests as needed. This would provide employees and guests the mobility of a personal vehicle without bringing personal vehicles to the hotel site.

Subsidized Transit Passes – Providing a subsidized transit pass to employees living off site will allow them to travel to / from work without using a personal vehicle.

Carpool Program – Coordinating and/or incentivizing employee carpooling will reduce the number of employee vehicles needed to park on site.

Alternative Transportation – Providing facilities such as bicycle storage on site will allow employees to use alternative modes of transportation.

Conclusions and Recommendations

Hales Engineering makes the following conclusions regarding parking at the proposed Ketchum Boutique Hotel:

- Current plans call for 90 off-street parking spaces at the hotel (supply).
- KMC requires that 87 off-street parking spaces for the proposed land use (KMC requirement).
- It may be desirable to further reduce parking demand on site. Hales Engineering recommends the following measures to reduce on-site parking demand:
 - Guest Shuttle
 - Employee Shuttle
 - Car Share Program
 - Subsidized transit pass
 - Carpool Program
 - Alternative Transportation
- As shown, the 90 off-street parking spaces planned for this site will meet the KMC parking requirements.

If you have any questions regarding this memorandum, please feel free to contact us.

KETCHUM PLANNING STAFF

Ketchum Boutique Hotel Traffic Impact Study

Scott Johnson, PE, PTOE

July 29, 2019

Overview

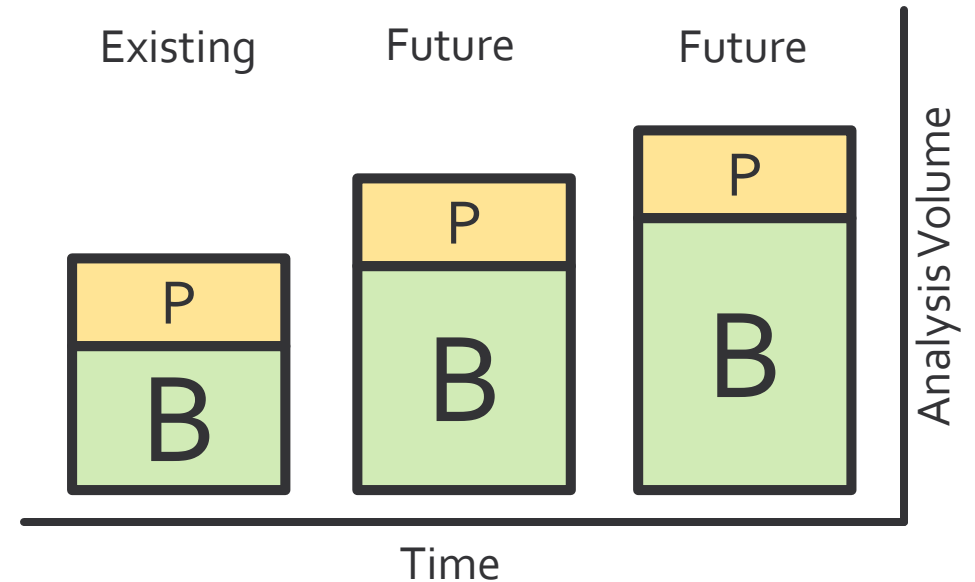
- Analysis Methodology
- Data Collection
- Trip Generation
- Microsimulation Analysis
- Analysis Results

Analysis Methodology

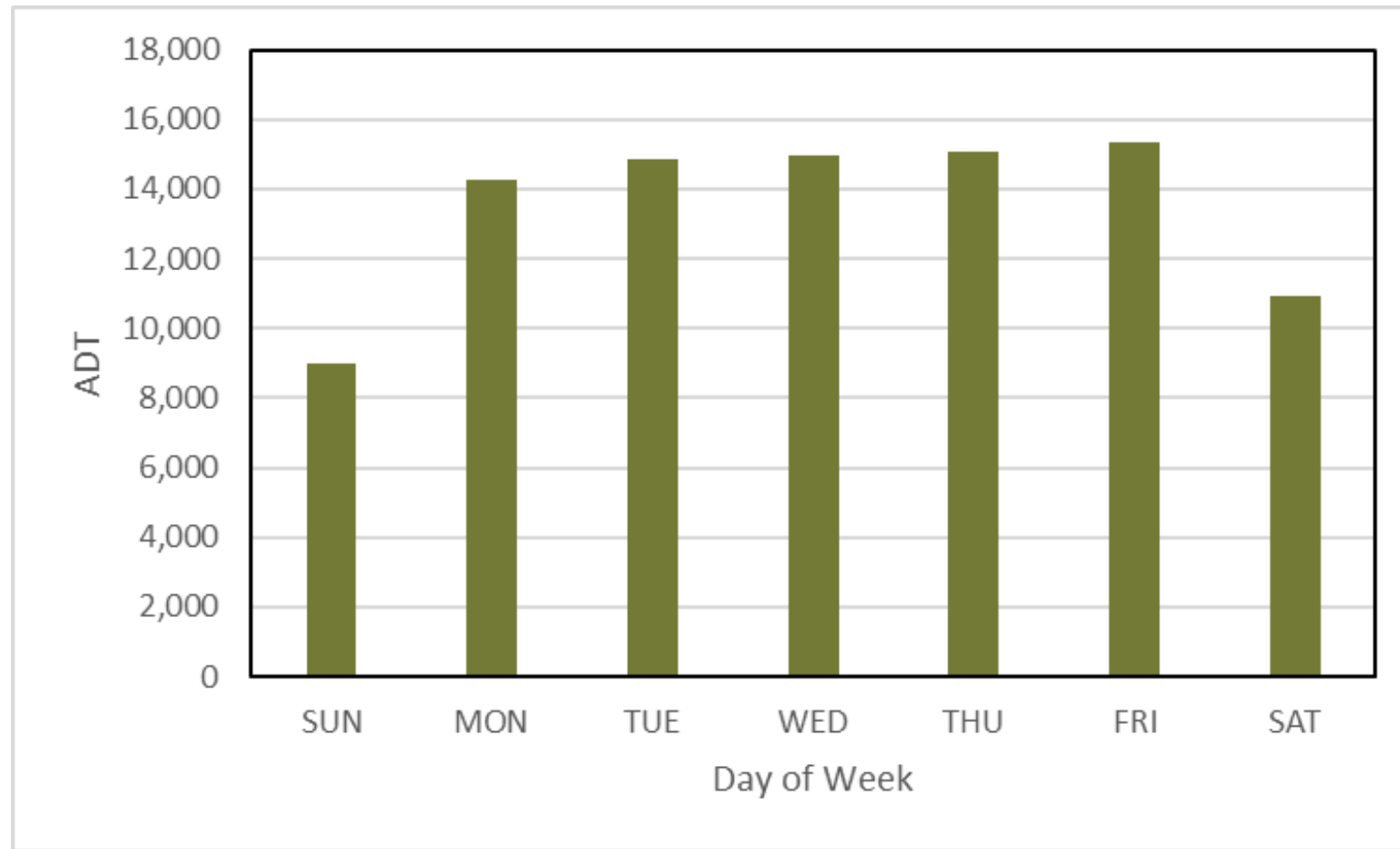
- Level of Service (LOS)
 - Describes operating performance of an intersection or roadway
 - *Highway Capacity Manual (HCM)*, 6th Edition, 2016
 - Calculated based on peak hour traffic conditions
 - Reported on a scale from A to F
 - Signalized, All-Way Stop, and Roundabout Intersections
 - Average delay (seconds/vehicle) on **all approaches**
 - Stop-Controlled Intersections
 - Average delay (seconds/vehicle) on the **worst approach**

Analysis Methodology

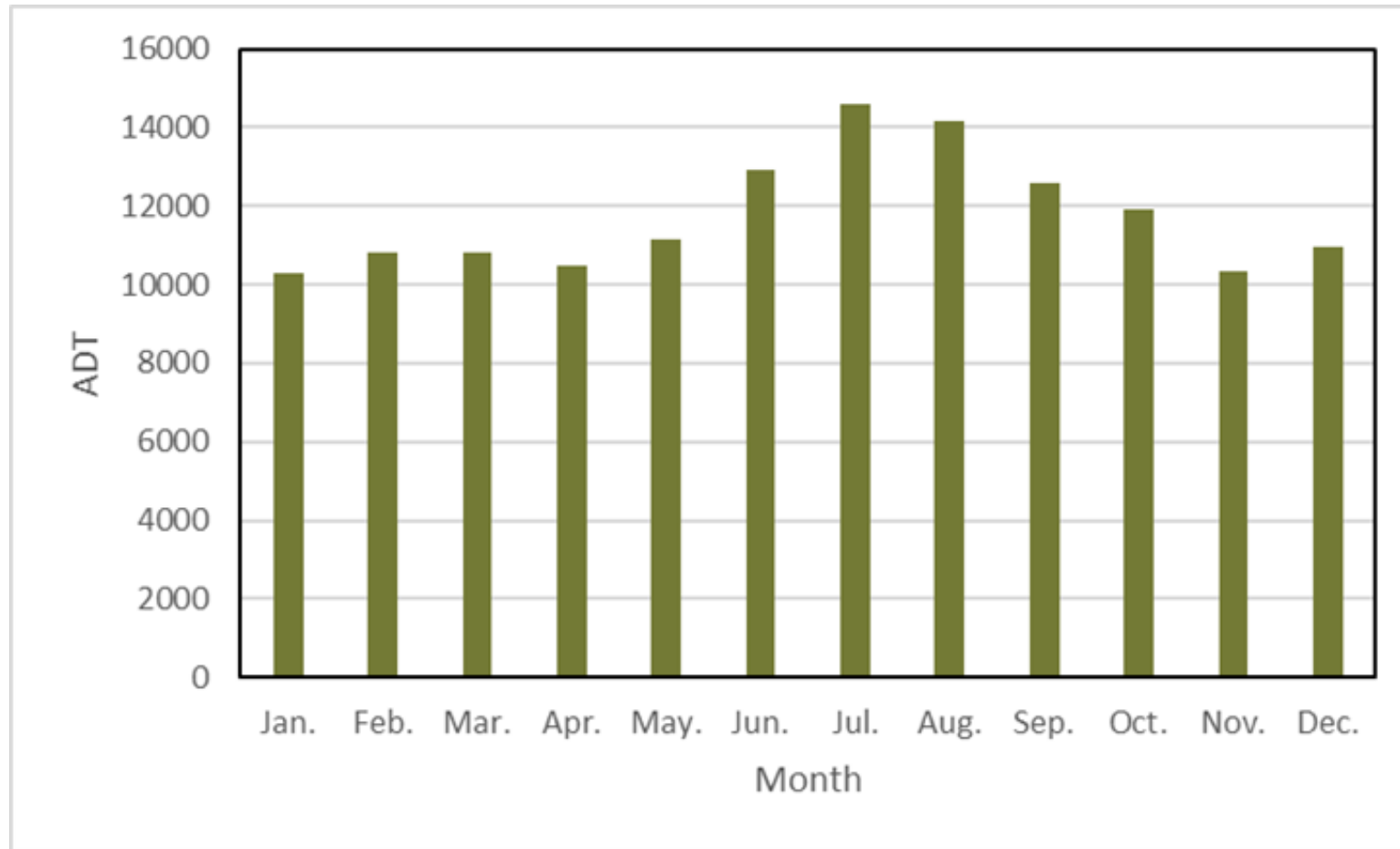
- Background Conditions
 - Existing traffic volumes/conditions or future volumes/conditions assuming background growth/improvements
- Plus Project Conditions
 - Conditions with traffic generated by the proposed project added to the roadway network



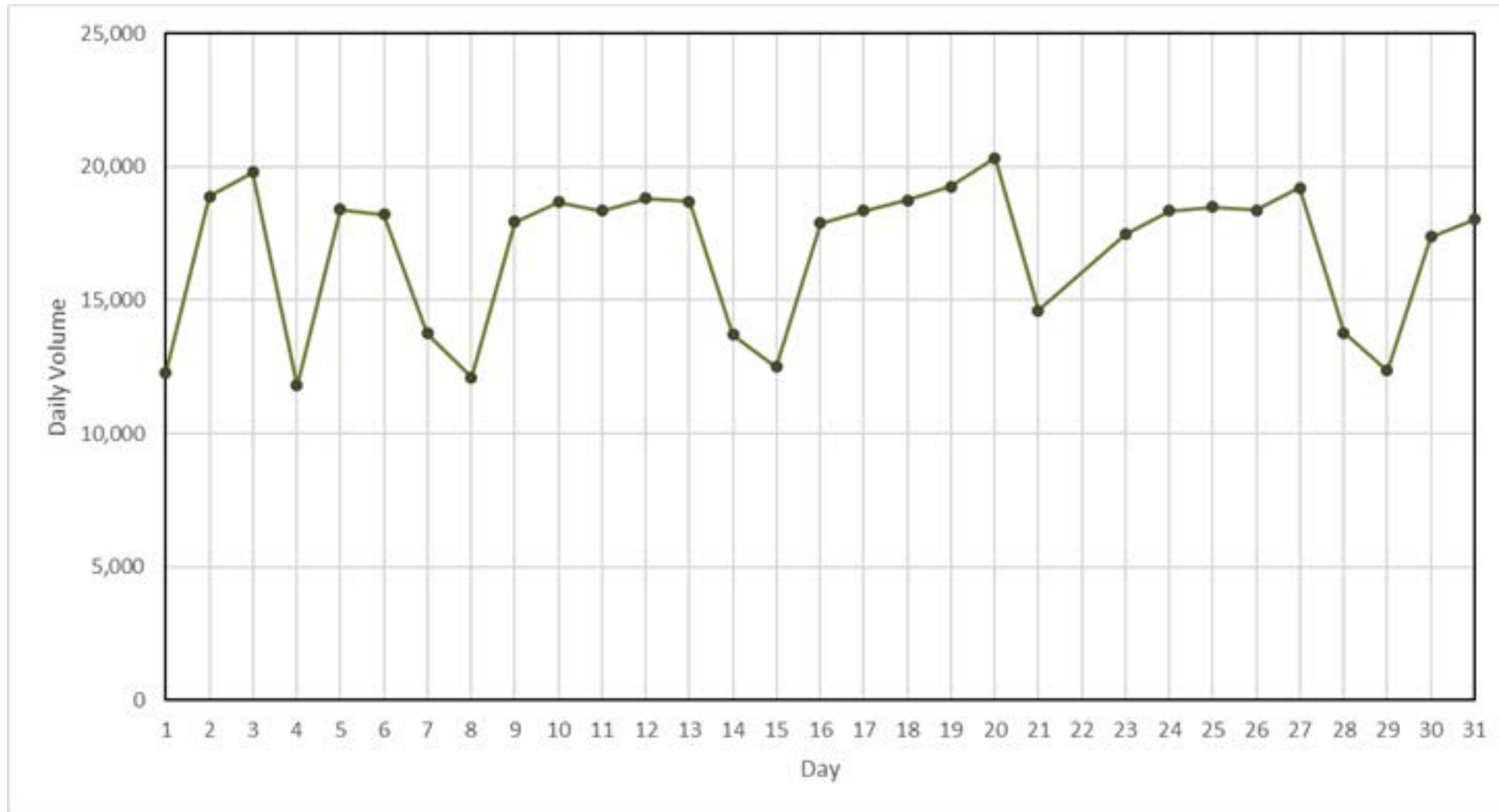
Day of the Week ADT for SH-75



Monthly ADT for SH-75



July 2018 Daily Volumes for SH-75



500 Highest Volume Hours on SH-75

**Idaho Transportation Department
500 Highest Hours for 2018**

Site names: 00068
County: Blaine
Fund Class: R Minor Arterial - Other
Location: SH-75 2.9 Mi. N of Bullion St

Seasonal Factor Grp: 4
Daily Factor Grp: 1
Axle Factor Grp: 3
Growth Factor Grp:

Rank	Volume	% AADT	% Pos	Date/Time	Day
1	1836	13.61	59.91	7/20/18 4:00 PM	Friday
2	1799	13.33	55.86	8/31/18 4:00 PM	Friday
3	1777	13.17	63.59	7/3/18 4:00 PM	Tuesday
4	1768	13.1	66.06	7/25/18 4:00 PM	Wednesday
5	1729	12.81	68.59	7/19/18 5:00 PM	Thursday
6	1725	12.78	61.86	7/27/18 4:00 PM	Friday
7	1710	12.67	69.94	6/28/18 5:00 PM	Thursday
8	1702	12.61	68.27	7/3/18 5:00 PM	Tuesday
9	1698	12.58	64.02	8/8/18 5:00 PM	Wednesday
10	1697	12.57	63.32	7/19/18 4:00 PM	Thursday
11	1690	12.53	63.96	7/19/18 4:00 PM	Thursday
12	1686	12.5	64.41	9/21/18 4:00 PM	Friday
13	1683	12.48	63.9	7/2/18 4:00 PM	Tuesday
14	1678	12.45	63.53	7/18/18 4:00 PM	Wednesday
15	1676	12.42	63.31	6/29/18 4:00 PM	Friday
16	1672	12.39	66.87	7/5/18 4:00 PM	Thursday
17	1671	12.38	67.92	6/26/18 4:00 PM	Tuesday
18	1670	12.38	67.84	7/31/18 4:00 PM	Tuesday
19	1670	12.38	66.89	7/26/18 4:00 PM	Thursday
20	1670	12.38	67.9	7/24/18 4:00 PM	Tuesday
21	1670	12.38	66.05	7/18/18 4:00 PM	Wednesday
22	1669	12.37	68.48	7/11/18 4:00 PM	Wednesday
23	1668	12.36	64.75	7/10/18 4:00 PM	Tuesday
24	1663	12.33	64.94	8/9/18 5:00 PM	Thursday
25	1662	12.32	66.25	8/1/18 5:00 PM	Wednesday
26	1659	12.3	69.68	7/11/18 5:00 PM	Wednesday
27	1657	12.28	62.34	8/2/18 4:00 PM	Thursday

Created Wed Jan 09 14:09:57 MST 2019 AV04P: Page 1 of 2

**Idaho Transportation Department
500 Highest Hours for 2018**

Site names: 00068
County: Blaine
Fund Class: R Minor Arterial - Other
Location: SH-75 2.9 Mi. N of Bullion St

Seasonal Factor Grp: 4
Daily Factor Grp: 1
Axle Factor Grp: 3
Growth Factor Grp:

Rank	Volume	% AADT	% Pos	Date/Time	Day
28	1657	12.28	66.63	8/9/18 4:00 PM	Thursday
29	1655	12.27	64.71	8/30/18 4:00 PM	Thursday
30	1653	12.25	66.91	7/12/18 4:00 PM	Thursday
35	1646	12.2	67.07	7/2/18 4:00 PM	Monday
40	1633	12.1	59.64	10/12/18 4:00 PM	Friday
45	1626	12.05	64.7	12/27/18 4:00 PM	Thursday
50	1622	12.02	70.28	7/10/18 5:00 PM	Tuesday
55	1612	11.95	62.97	8/3/18 4:00 PM	Friday
60	1607	11.91	67.21	7/17/18 4:00 PM	Tuesday
65	1597	11.84	70.32	6/14/18 4:00 PM	Thursday
70	1586	11.75	71.25	8/20/18 5:00 PM	Monday
75	1582	11.72	63.08	8/15/18 5:00 PM	Wednesday
80	1575	11.68	67.5	7/25/18 4:00 PM	Wednesday
85	1564	11.6	70.18	7/18/18 5:00 PM	Wednesday
90	1566	11.61	67.75	6/7/18 4:00 PM	Thursday
95	1559	11.55	66.52	10/5/18 4:00 PM	Friday
100	1555	11.52	70.48	5/16/18 4:00 PM	Wednesday
200	1465	10.86	71.88	12/11/18 4:00 PM	Tuesday
300	1407	10.43	73.13	1/10/18 4:00 PM	Wednesday
400	1364	10.11	74.41	8/7/18 8:00 AM	Tuesday
500	1329	9.85	70.2	5/21/18 5:00 PM	Monday

Created Wed Jan 09 14:09:57 MST 2019 AV04P: Page 2 of 2

- All occurred on a weekday (Mon-Fri)
- All but one occurred between 4:00 p.m. and 6:00 p.m.

Data Collection

- Peak Period Turning-Movement Counts
 - 7:00 – 9:00 a.m. and 4:00 – 6:00 p.m.
- Seasonal Adjustments
 - Adjusted to represent average weekday in July
 - Validated by comparing to data collected July 2018
- Future volume projections calculated using annual growth rate developed by HDR for Ketchum City.

Trip Generation

- Trip generation for the development was calculated using trip generation rates published in the Institute of Transportation Engineers (ITE), *Trip Generation*, 10th Edition, 2017.
 - Daily Trips
 - Morning Peak
 - Evening Peak

Trip Generation

**Table 3
Ketchum Boutique Hotel TIS
Trip Generation**

Weekday Daily									
Land Use ¹	# of Units	Unit Type	Trip Generation	% Entering	% Exiting	Trips Entering	Trips Exiting	Total Daily Trips	
Hotel (310)	100	Rooms	704	50%	50%	352	352	704	
Project Total Daily Trips						352	352	704	
Morning Peak Hour									
Land Use ¹	# of Units	Unit Type	Trip Generation	% Entering	% Exiting	Trips Entering	Trips Exiting	Total a.m. Trips	
Hotel (310)	100	Rooms	46	59%	41%	27	19	46	
Project Total a.m. Peak Hour Trips						27	19	46	
Evening Peak Hour									
Land Use ¹	# of Units	Unit Type	Trip Generation	% Entering	% Exiting	Trips Entering	Trips Exiting	Total p.m. Trips	
Hotel (310)	100	Rooms	50	51%	49%	26	25	51	
Project Total p.m. Peak Hour Trips						26	25	51	

1. Land Use Code from the Institute of Transportation Engineers (ITE) *Trip Generation*, 10th Edition, 2017.

SOURCE: Hales Engineering, July 2019

Microsimulation Analysis

- Synchro/SimTraffic Analysis Software
 - Stochastic Analysis
 - 10 Simulation Runs
 - Delay & Queuing Data Extracted

Microsimulation Analysis (Existing)



Microsimulation Analysis (Existing Plus Project)



Analysis Results

- River Street / Main Street (SH-75)
 - Existing Background → LOS D (WB Approach)
 - Existing Plus Project → LOS F (WB Approach)
 - Eliminate EB/WB Left Turns → LOS C (EB Approach)
 - Future Background → LOS F (WB Approach)
 - Eliminate EB/WB Left Turns → LOS C (EB Approach)
 - Future Plus Project → LOS C (EB Approach)

Questions?

Attachment B (continued)

PEG Ketchum Hotel, LLC Gateway Site Submittal Drawings, Narratives & Studies

3. New Maps and Information (since PZ approval on 8/12/19)

- Hales Engineering Access Memorandum, Dated 8/13/19
- In-Group Hospitality Parking Plan, Dated 8/26/19

Following Ketchum Tribute Design Update Drawings, Dated 10/1/19

- Tribute Portfolio
- Council Update
- Architectural Site Plan
- Landscape Plan
- Floor Plans (Each Level)
- Contextual Elevations (5 Drawings)
- Project Perspectives from NE, NW, SE, NE, N & Rooftop Bar
- Waiver Supporting Exhibits
 - Minimum Lot Size for PUD
 - Floor Area Ratio
 - Average Setbacks
 - Height / Contextual Elevations
- Planning & Zoning Comments
- PZ Response

MEMORANDUM

Date: August 13, 2019
To: Nick Blayden, PEG Companies
From: Hales Engineering
Subject: **Ketchum – Boutique Hotel Access Memorandum**

UT19-1441

This memorandum discusses access management practices on SH-75 in Ketchum, Idaho as they relate to the proposed Boutique Hotel on the corner of the intersection of Main Street (SH-75) and River Street.

Background

The proposed Ketchum Boutique Hotel, located on the southwest corner of the River Street / Main Street (SH-75) intersection, will consist of 100 rooms and related amenities (restaurant, bar, fitness center, etc.). The hotel is planned to have two access points, both on River Street. As part of the project, two existing driveway accesses on Main Street (SH-75) between River Street and the Trail Creek bridge will be eliminated.

Questions have been raised regarding the proposed accesses for the hotel development, specifically if having access on Main Street (SH-75) would be a better option.

Analysis

Hales Engineering evaluated potential access on to Main Street (SH-75) based on Idaho Transportation Department (ITD) access control standards, sight distance, and general best practices.

ITD Access Control

SH-75 through Ketchum is classified by ITD as a Regional Route. The posted speed limit on this segment is 25 mph. According to IDAPA 39.03.42 – Rules Governing Highway Right-of-Way Encroachments on State Rights-of-Way, the Area Type for this segment of SH-75 is “Urban.” Access spacing for a Regional route in an Urban area type with a speed limit of 35 mph or less is as follows according to IDAPA 39.03.42:

- Signalized Road Spacing 2,640 ft
- Public Road Spacing 660 ft
- Driveway Distance Upstream from Public Road Intersection 660 ft
- Driveway Distance Downstream from Public Road Intersection 250 ft
- Distance Between Unsignalized Accesses Other Than Public Roads 250 ft

The distance along Main Street (SH-75) between River Street and the southern edge of the project parcel is approximately 375 feet. The distance along Main Street (SH-75) between the southern edge of the project parcel and the next driveway to the south is approximately 195 feet. Based on these conditions, one access to the hotel site could be constructed on Main Street (SH-75) and still meet ITD access spacing guidelines.

A single access point on Main Street (SH-75) would not be conducive to hotel operations, and the change in grade over the site would not allow for a second access onto River Street would not function in tandem, as is necessary, with an access onto Main Street.

Sight Distance

Information regarding minimum sight distances at accesses and/or intersections is published in the AASHTO, *A Policy on Geometric Design of Highways and Streets*, 7th Edition, 2018. Minimum sight distance is the minimum distance a driver needs to be able to see (measured along the centerline of the travel lane) to safely evaluate the available gap in traffic and execute a turn movement from a stop-controlled approach onto a roadway.

For a design speed of 30 mph, the minimum required sight distance for a right-turn from a stop-controlled approach is 290 feet and the minimum required sight distance for a left-turn from a stop-controlled approach is 335 feet. This means that a driver stopped on a driveway approach waiting to execute a right-turn movement onto southbound Main Street (SH-75) would need to have an unobstructed view of the southbound lane between the access and a point 290 feet to the north. A driver waiting to execute a left-turn movement onto northbound Main Street (SH-75) would need to have an unobstructed view of the southbound lane between the access and a point 335 feet to the south.

The minimum sight distance for a right-turning vehicle would extend to a point past River Street and the minimum sight distance for a left-turning vehicle would extend to a point just north of Andora Lane.

Based on the speed and geometry of Main Street (SH-75) along the project frontage, providing adequate sight distance for an access onto Main Street (SH-75) from the hotel site will be problematic.

General Best Practices

General engineering best practices encourage that accesses be located on lower functionally classified roadways where possible. Higher functionally classified roadways are intended to provide mobility, or, facilitate the movement of vehicles from one area to another. Lower functionally classified roadways are intended to provide access and connect to higher functionally classified roadways.

Main Street (SH-75) is classified by ITD as a minor arterial. River street is not classified by ITD, and a functional classification map for Ketchum City streets is not readily available. However, based on the characteristics of River Street it would be in a lower functional class than Main Street (SH-75). Being in a lower functional classification makes River Street a more desirable location for project accesses than Main Street (SH-75).

An access to the hotel site on Main Street (SH-75) would be located on the inside of a horizontal curve. Accesses on the inside of a horizontal curve provide challenges to drivers since looking for gaps requires them to monitor an area with a range greater than 180°, where an access on a tangent section of roadway would require a driver to monitor an area with a range of 180° or less. This extra field of vision creates increased difficulty for drivers entering the roadway from the access.

Conclusions/Recommendations

Hales Engineering evaluated potential access on to Main Street (SH-75) based on Idaho Transportation Department (ITD) access control standards, sight distance, and general best practices in engineering. Based on these factors, it is recommended that access to the proposed hotel be from River Street.

If you have any questions regarding this memorandum, please feel free to contact us.

Monday August 26, 2019

To whom it may concern:

We at In-Group Hospitality (“**In-Group**”) are grateful for this opportunity to share our parking plan for the proposed Tribute Sun Valley (“**Tribute**”). The following is our four-part parking plan to address the needs of (i) day guests, (ii) employees housed on the property, (iii) overnight self-parking guests, and (iv) overnight valet parking.

Day Guests:

Our current design allows us to maintain in excess of the required six parking stalls for public parking, all within our secured garage. Because those stalls will fall within a secured area, to ensure that safety and security of our guests and their vehicles, our plan is to provide complimentary valet parking (up to 4hrs) to all day guests with validation from any Food and Beverage outlet or our Executive Office. Arriving guests will be provided a unique vehicle claim ticket that identifies the vehicle as a “day guest.” Combining this service with our valet parking should not only enable us to provide more than the required stalls but also encourage locals to dine, entertain, or conduct business in our hotel and amenity spaces.

Employee Housing:

In-Group’s parking plan intends to also provide every employee housed at the Tribute with one (1) parking stall, secured by key access. These stalls will be provided to those employees at no cost, with each such employee receiving their key as part of their on-boarding process.

Overnight Self-Parking:

Our plan also allows self-parking for overnight guests to be provided in conjunction with the room reservation, with the cost of parking included as one of the many amenities covered by the Tribute’s resort fee. Access will be granted via a guest room key programed at check-in. The key will only be valid for the dates of their stay and then automatically deactivate.

Overnight Valet Parking:

All guests will have the option to Valet park their vehicles under In-Group’s parking plan, at a nominal fee not covered in the resort fee noted above. Valet parking will include “in/out privileges”. We anticipate that our clientele will greatly appreciate this high level of service and convenience.

We are certain that our current parking action plan will encourage local visits to the Tribute and provide a welcome amenity to our guests.

Respectfully,

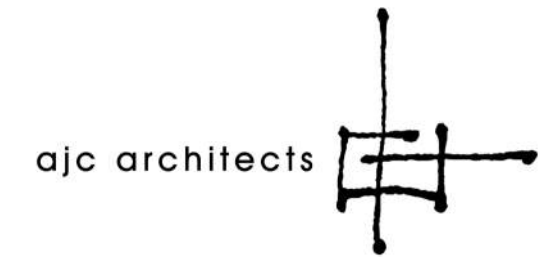

Joe Madera

Regional Director of Operations
In-Group Hospitality



KETCHUM TRIBUTE

Oct 1, 2019 | DESIGN UPDATE



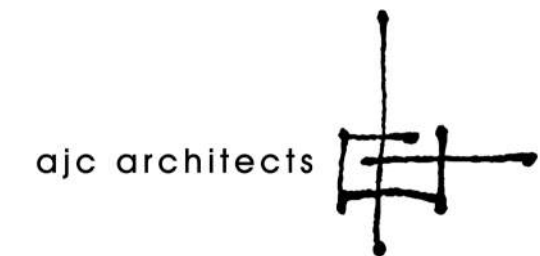
PROJECT UPDATE

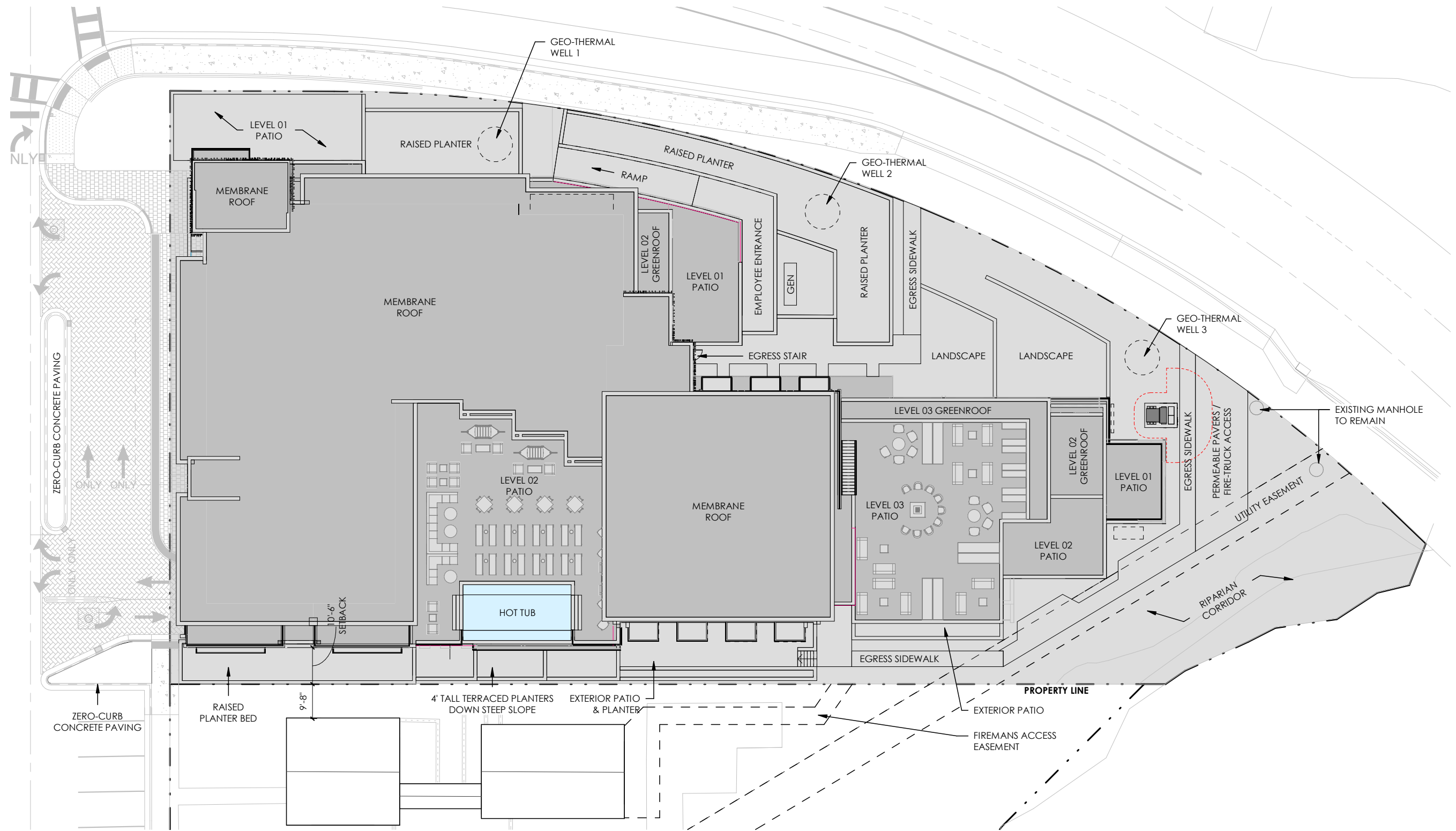
CITY COUNCIL,

ON JULY 29TH & 30TH OUR PROJECT TEAM PRESENTED CONCEPT DESIGN TO THE KETCHUM CITY PLANNING & ZONING COMMISSION FOR APPROVAL OF THE PRE-APPLICATION DESIGN SUBMITTAL.

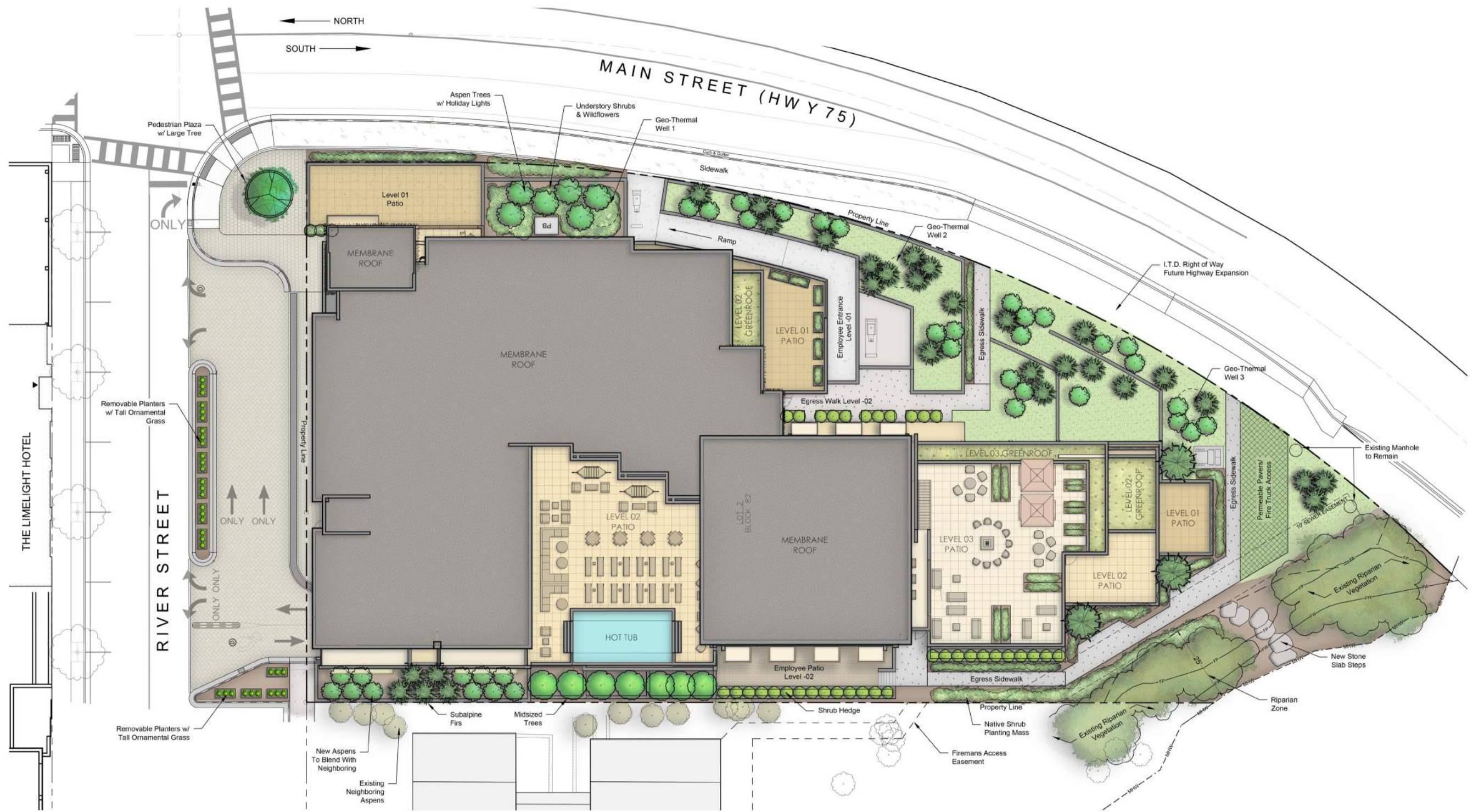
AT THIS MEETING THE KETCHUM BOUTIQUE HOTEL PROJECT WAS APPROVED TO MOVE TO A FULL P&Z DESIGN REVIEW SUBMITTAL, CONTINGENT ON KETCHUM CITY COUNCIL REVIEW AND APPROVAL OF SEVERAL CONDITIONS AND WAIVERS AS RECOMMENDED BY P&Z FOR APPROVAL. THIS REVIEW IS ON-GOING, AND IN THE INTEREST OF KEEPING PROJECT MOMENTUM, WE ARE SUBMITTING THIS DESIGN UPDATE PACKAGE FOR ADDITIONAL KETCHUM CITY FEEDBACK.

CURRENT PLANS, ELEVATIONS, AND RENDERS ARE INCLUDED ALONG WITH UPDATED DIAGRAMS FOR WAIVERS.

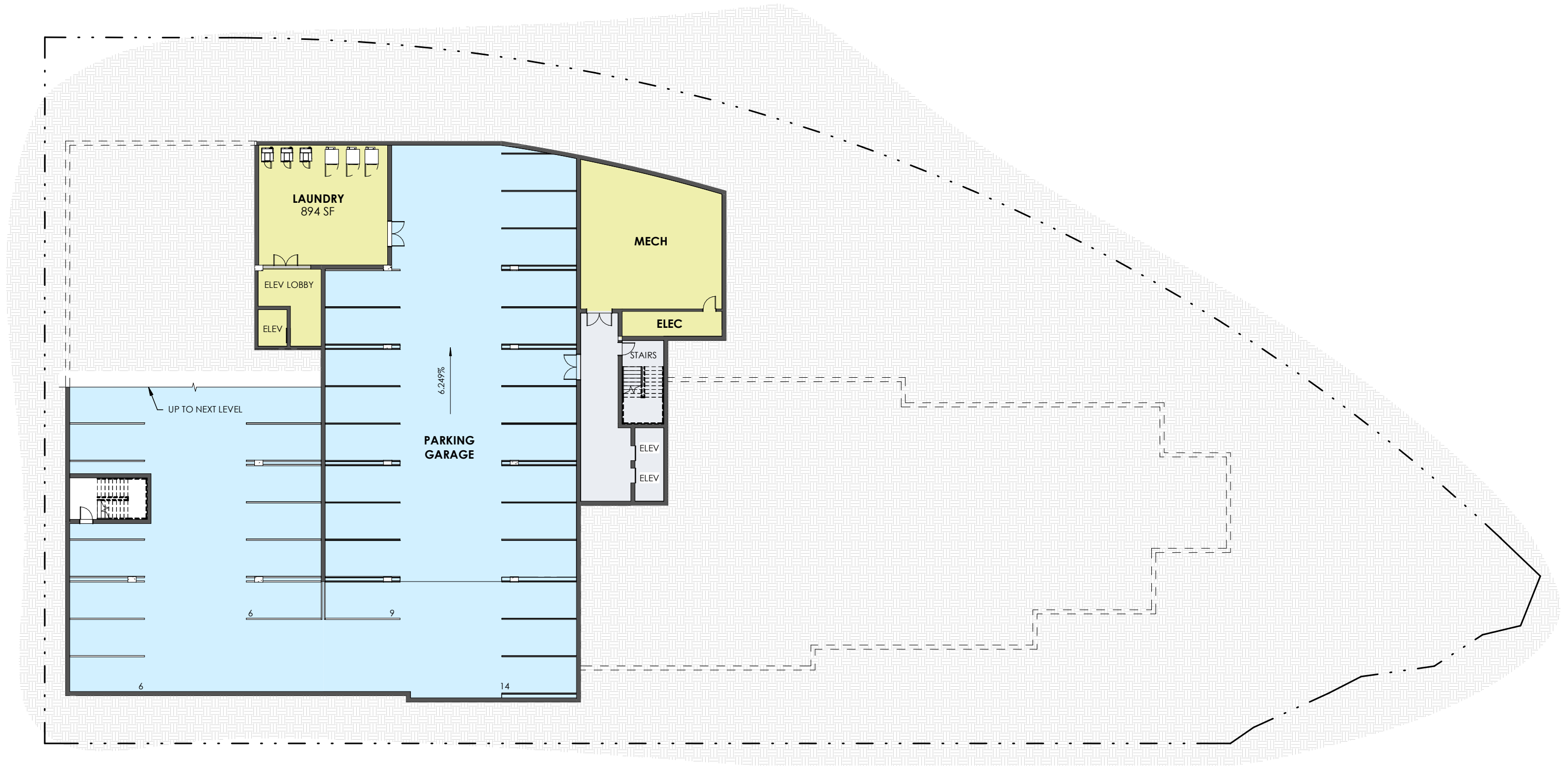




ARCHITECTURAL SITE PLAN



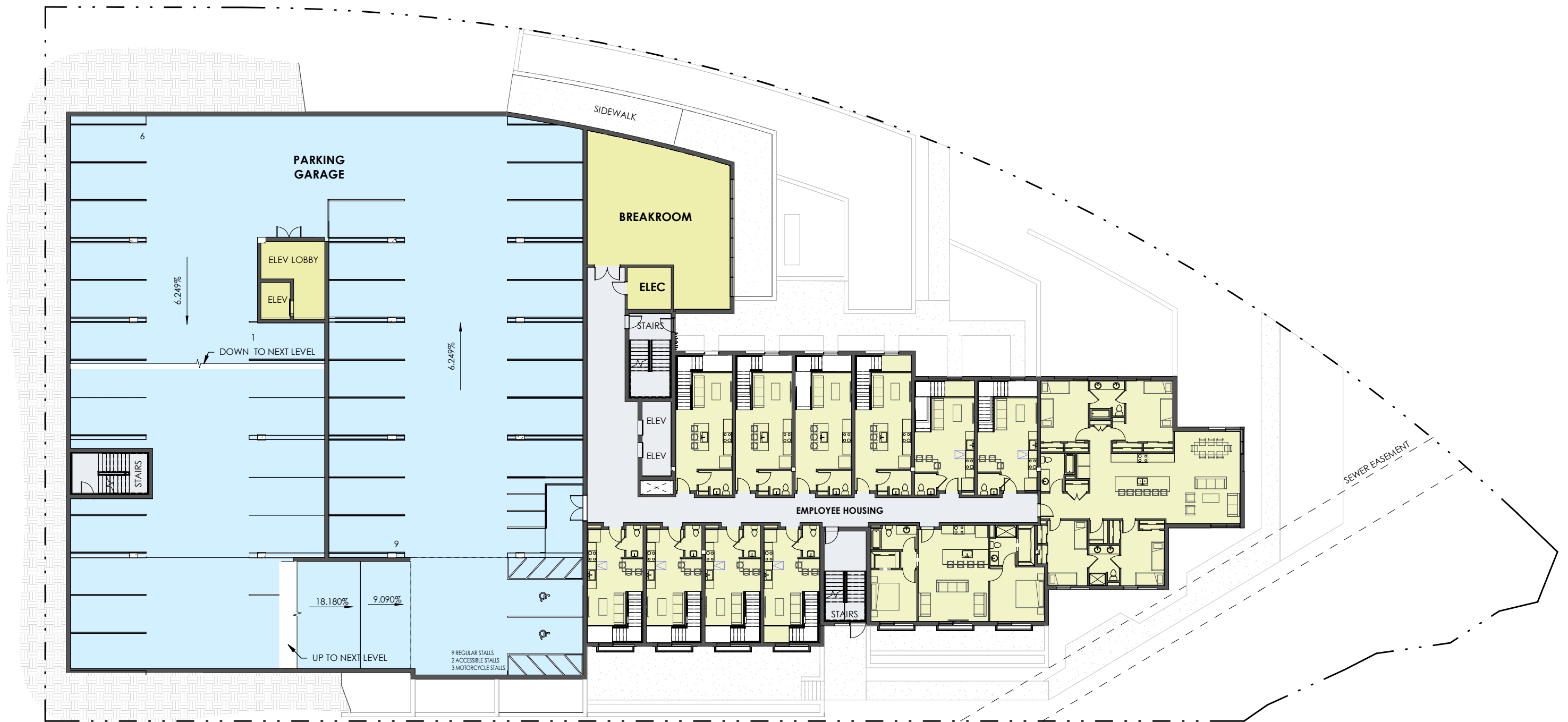
LANDSCAPE PLAN



LEVEL -03 | FLOOR PLAN

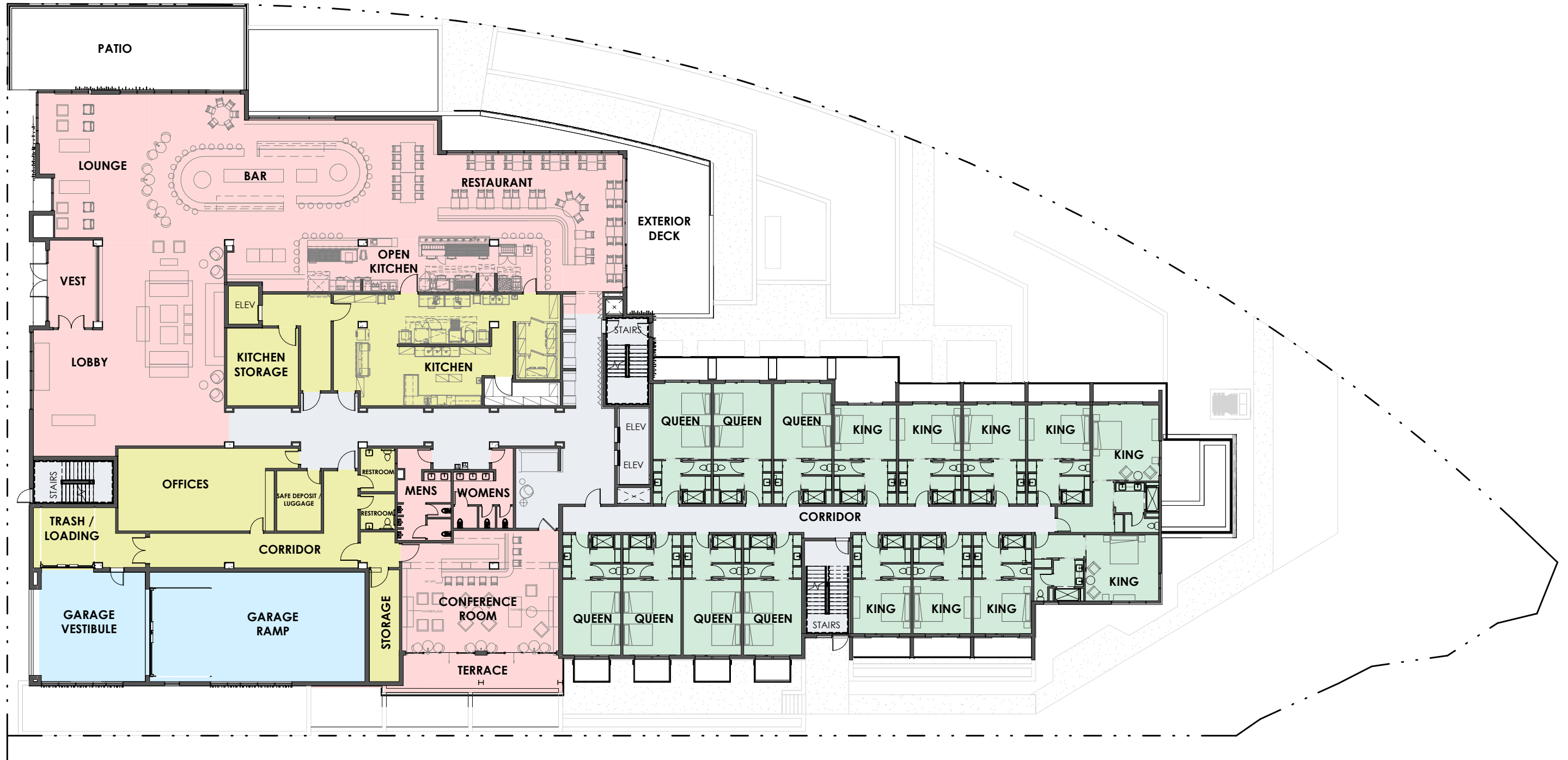


LEVEL -02 | FLOOR PLAN



LEVEL -01 | FLOOR PLAN

MAIN STREET



LEVEL 01 | FLOOR PLAN

TRIBUTE PORTFOLIO | KETCHUM, IDAHO

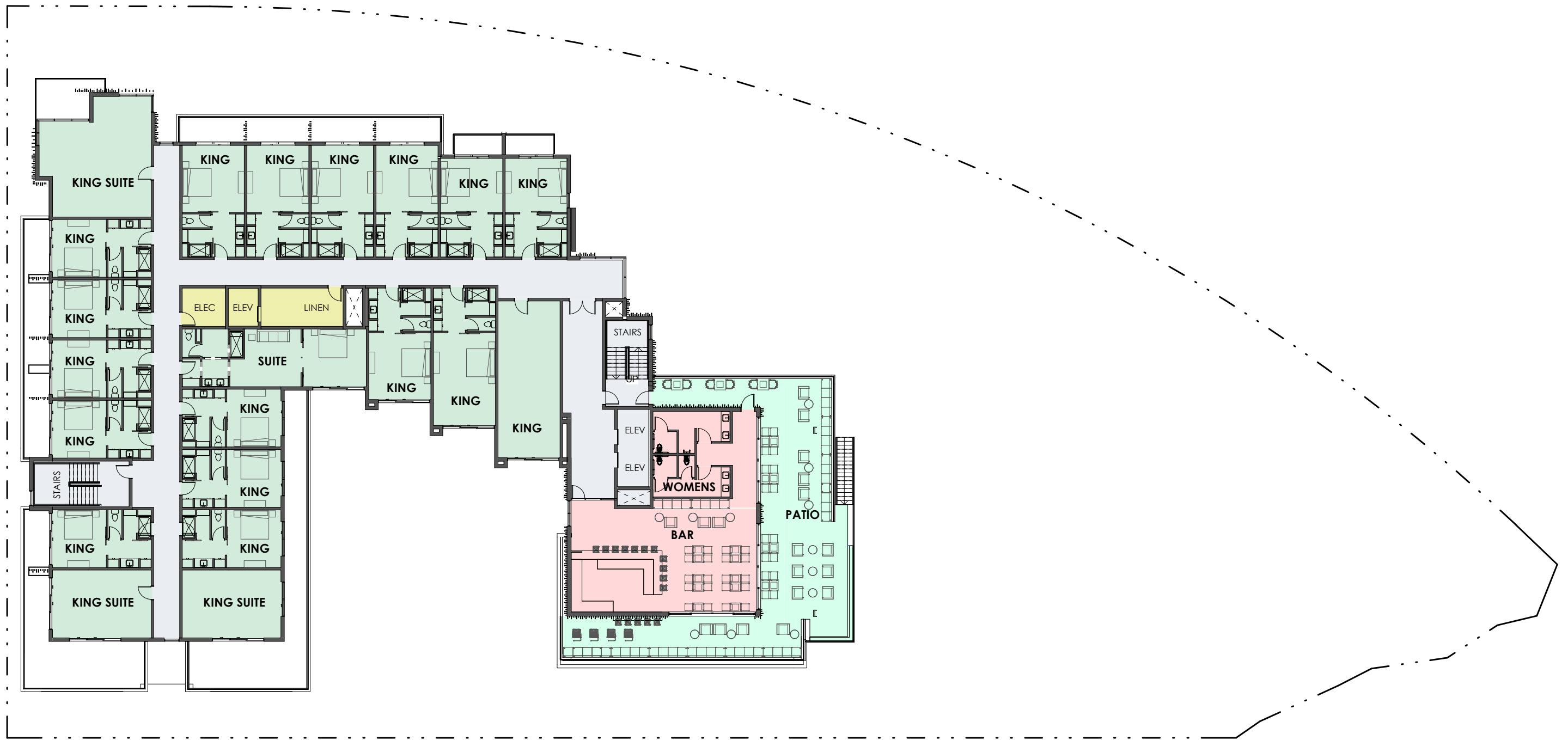


LEVEL 02 | FLOOR PLAN

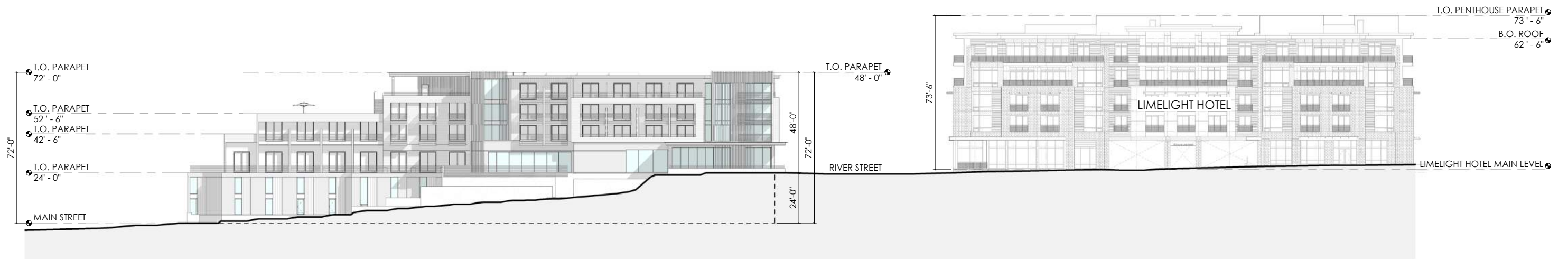
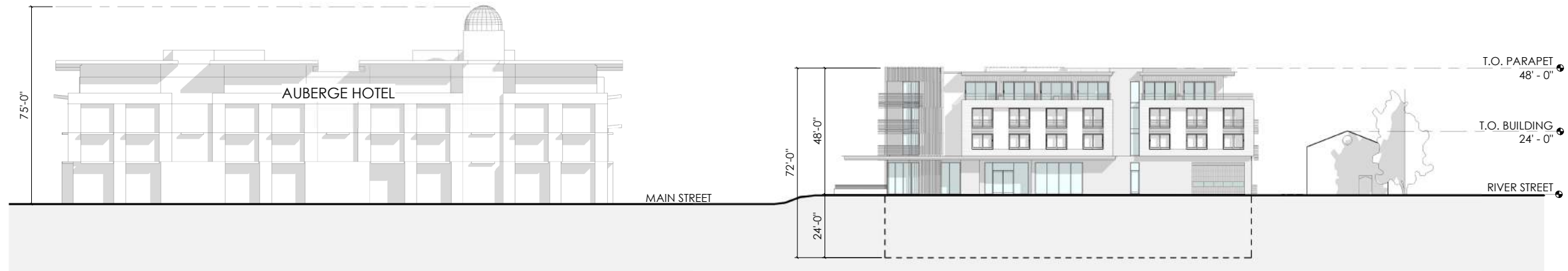


LEVEL 03 | FLOOR PLAN

TRIBUTE PORTFOLIO | KETCHUM, IDAHO



LEVEL 04 | FLOOR PLAN



CONTEXTUAL ELEVATIONS



EAST ELEVATION | HWY 75 | MAIN STREET

TRIBUTE PORTFOLIO | KETCHUM, IDAHO



WEST ELEVATION | TRAIL CREEK



SOUTH ELEVATION | TRAIL CREEK



WEST ELEVATION



PROJECT PERSPECTIVE | NORTH EAST

TRIBUTE PORTFOLIO | KETCHUM, IDAHO



PROJECT PERSPECTIVE | NORTH WEST

TRIBUTE PORTFOLIO | KETCHUM, IDAHO



PROJECT PERSPECTIVE | SOUTH EAST

TRIBUTE PORTFOLIO | KETCHUM, IDAHO



PROJECT PERSPECTIVE | NORTH EAST

TRIBUTE PORTFOLIO | KETCHUM, IDAHO



PROJECT PERSPECTIVE | NORTH

TRIBUTE PORTFOLIO | KETCHUM, IDAHO

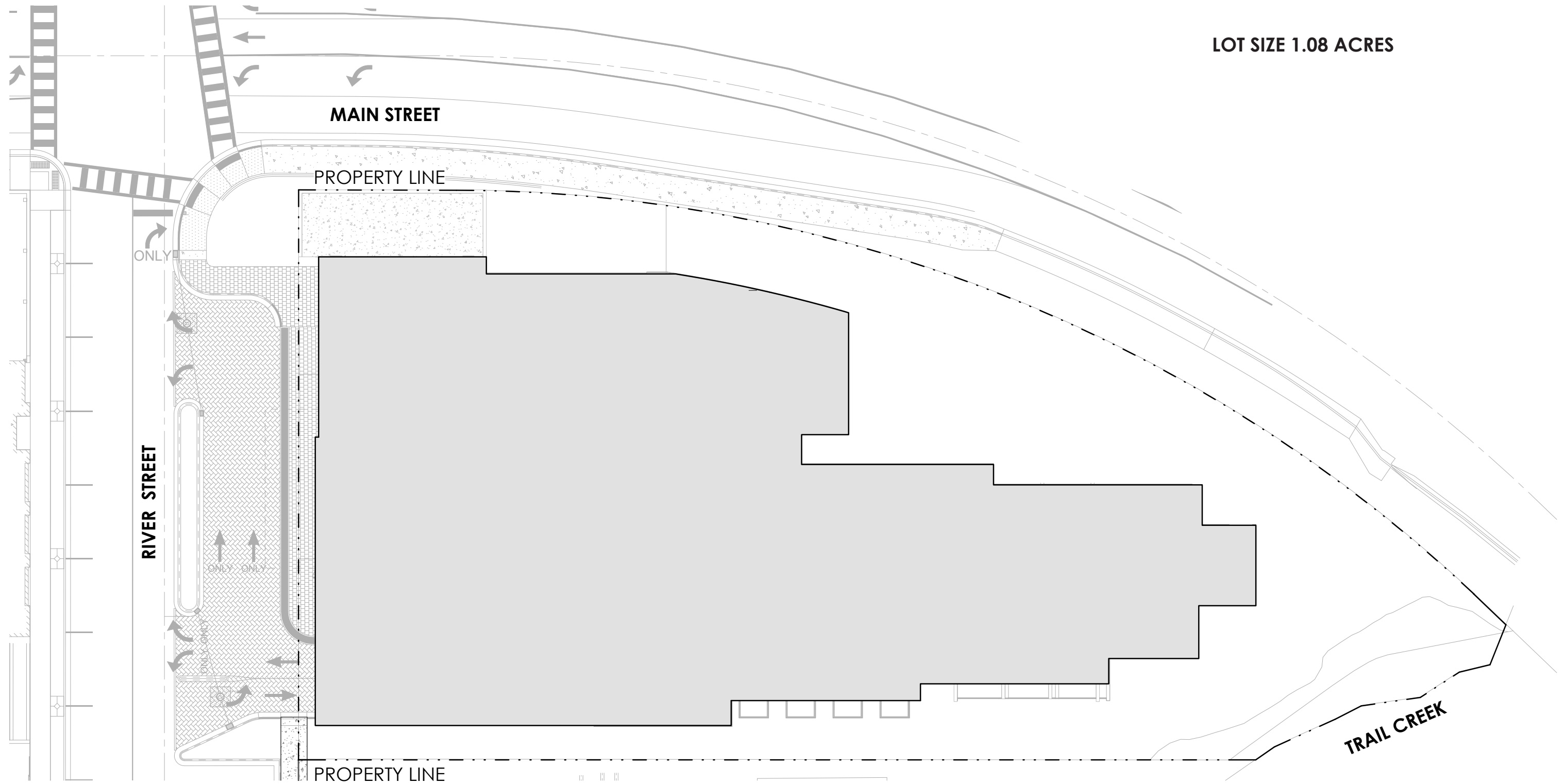


PROJECT PERSPECTIVE | ROOFTOP BAR

TRIBUTE PORTFOLIO | KETCHUM, IDAHO

WAIVERS | SUPPORTING EXHIBITS

TRIBUTE PORTFOLIO | **KETCHUM, IDAHO**



LOT SIZE 1.08 ACRES

MAIN STREET

PROPERTY LINE

ONLY

RIVER STREET

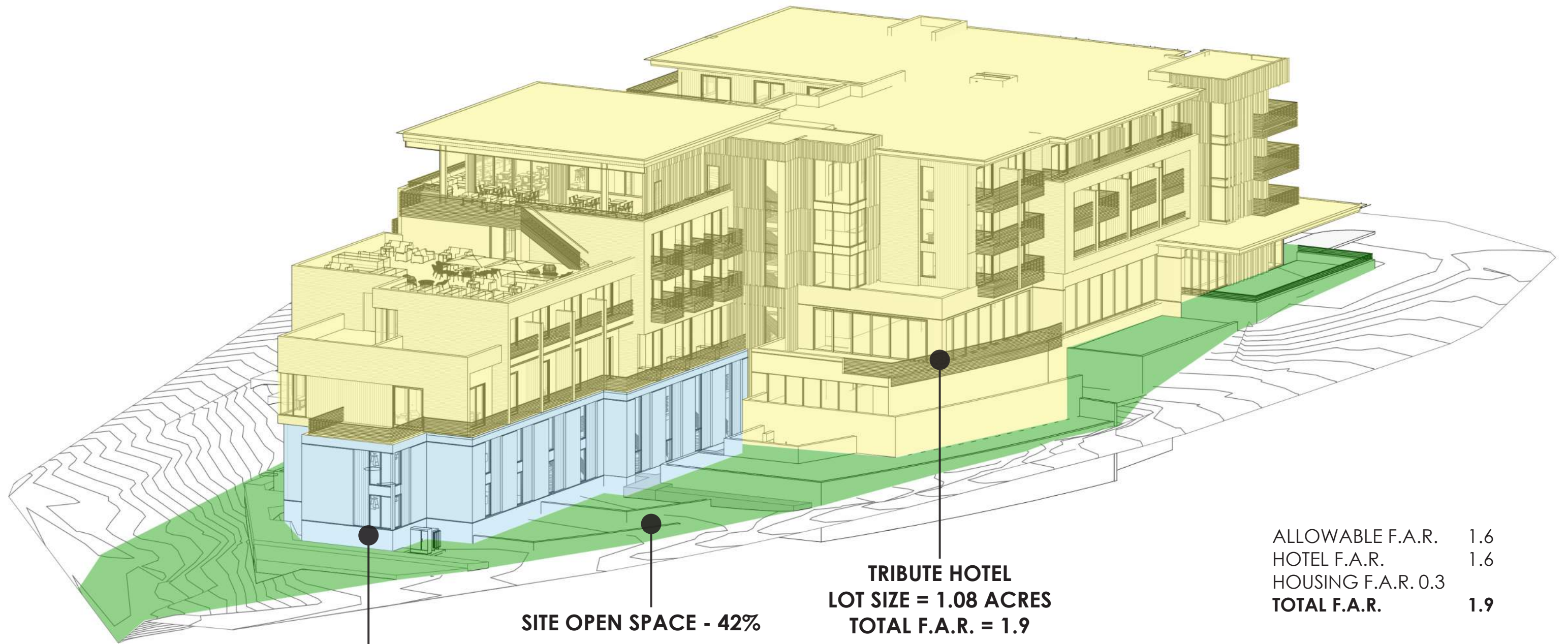
ONLY ONLY

PROPERTY LINE

TRAIL CREEK

WAIVER 1 | MINIMUM LOT SIZE FOR PUD

TRIBUTE PORTFOLIO | KETCHUM, IDAHO



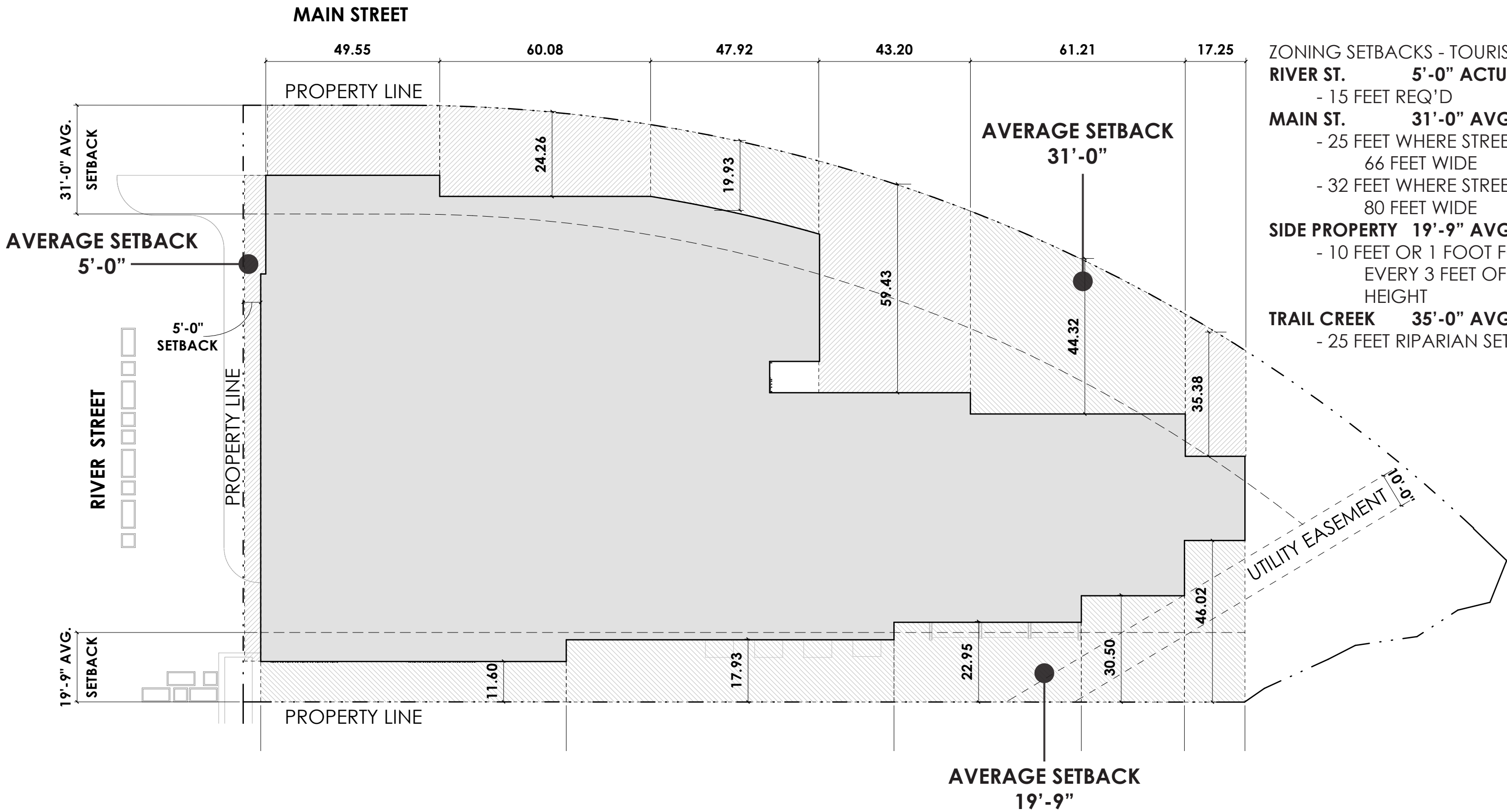
EMPLOYEE HOUSING COMPONENT

SITE OPEN SPACE - 42%

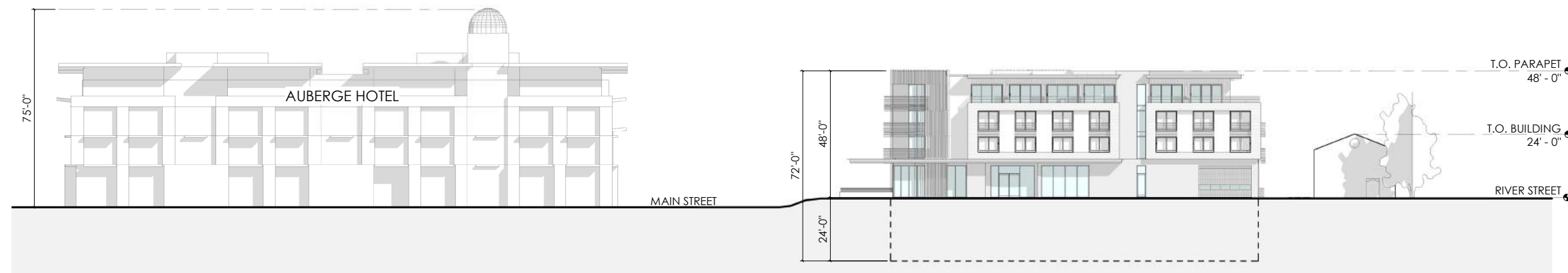
TRIBUTE HOTEL
LOT SIZE = 1.08 ACRES
TOTAL F.A.R. = 1.9

ALLOWABLE F.A.R.	1.6
HOTEL F.A.R.	1.6
HOUSING F.A.R. 0.3	
TOTAL F.A.R.	1.9

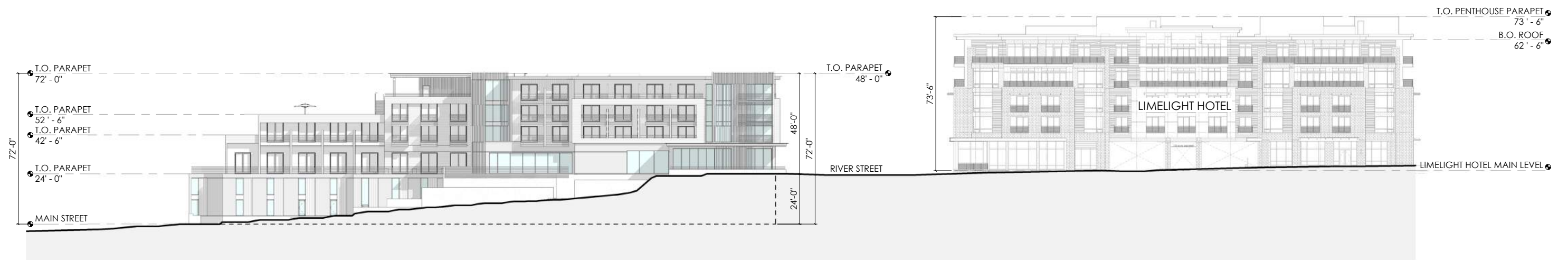
WAIVER 2 | FLOOR AREA RATIO



- ZONING SETBACKS - TOURIST
- RIVER ST. 5'-0" ACTUAL**
- 15 FEET REQ'D
 - MAIN ST. 31'-0" AVG.**
- 25 FEET WHERE STREET IS 66 FEET WIDE
- 32 FEET WHERE STREET IS 80 FEET WIDE
 - SIDE PROPERTY 19'-9" AVG.**
- 10 FEET OR 1 FOOT FOR EVERY 3 FEET OF HEIGHT
 - TRAIL CREEK 35'-0" AVG.**
- 25 FEET RIPARIAN SETBACK



RIVER STREET | NORTH SCHEMATIC ELEVATION
 ADJACENT TO APPROVED AUBERGE HOTEL



MAIN STREET | EAST SCHEMATIC ELEVATION
 ADJACENT TO LIMELIGHT HOTEL

WAIVER 4 | HEIGHT | CONTEXTUAL ELEVATIONS

NEIGHBORING SETBACK

P&Z REQUESTED THE DESIGN TEAM STUDY BUILDING'S RELATIONSHIP TO ADJACENT PROPERTY.

CORNER ELEMENT

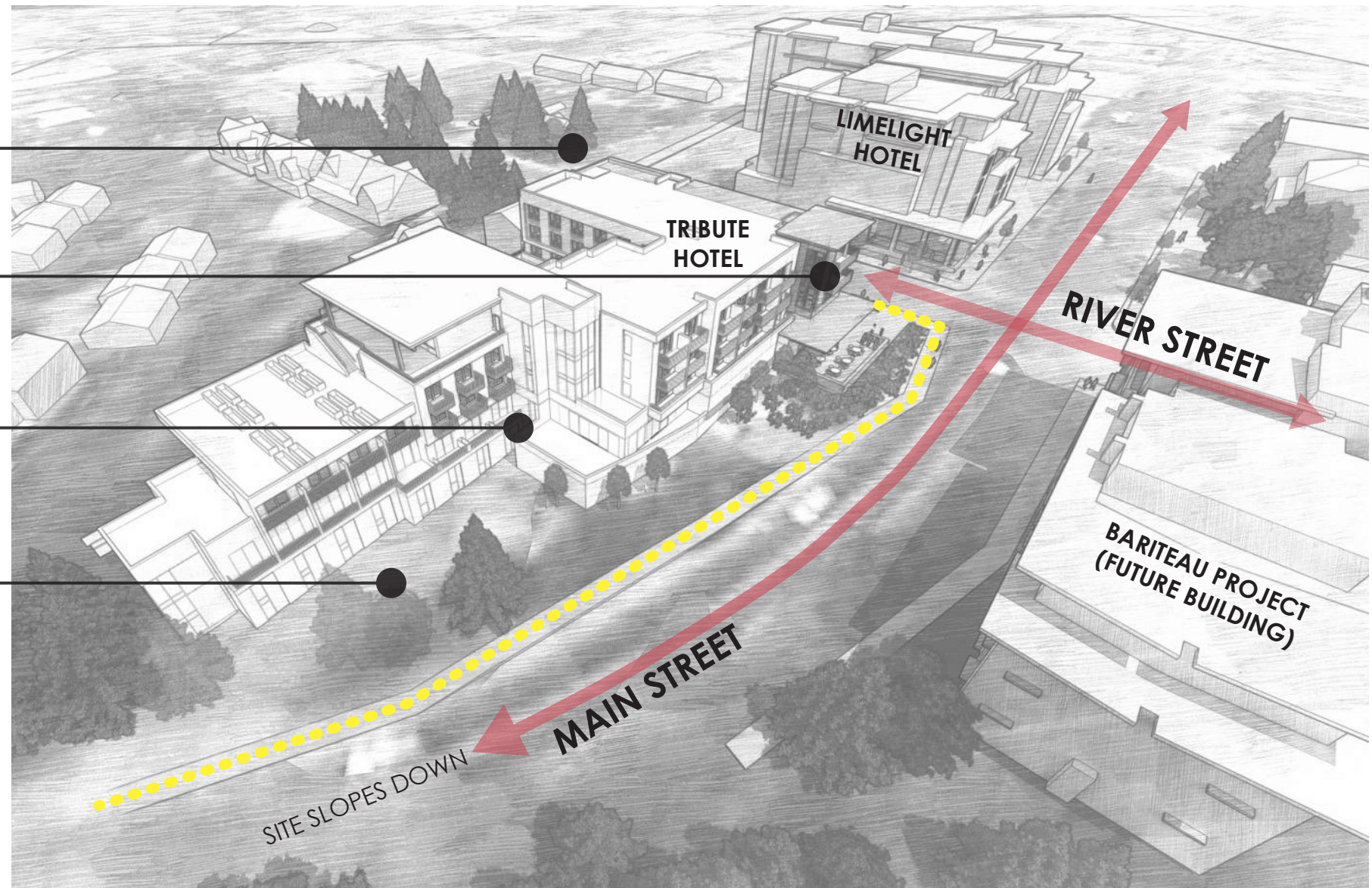
P&Z ASKED THE DESIGN TEAM TO SOFTEN THE ARCHITECTURAL PRESENCE OF THE CORNER ROOMS.

BUILDING SEPARATION / TRANSPARENCY

P&Z FELT THE CENTRAL BUILDING CONNECTION NEEDED A LIGHTER MORE TRANSPARENT DESIGN.

SITE & LANDSCAPE DEVELOPMENT

P&Z COMMENTED THAT ADDITIONAL ATTENTION WAS REQUIRED ON SITE DESIGN.



NEIGHBORING SETBACK

THE SETBACK HAS BEEN INCREASED TO 10'-6".
ADDITIONAL WINDOWS AND TERRACING HAVE BEEN
INTRODUCED TO THIS BUILDING ELEVATION TO RELATE TO
SCALE OF NEIGHBORING BUILDING FRONTAGE.

CORNER ELEMENT

THE CORNER ELEMENT HEIGHT HAS BEEN REDUCED AND
A MORE SUBTLE, SIMPLIFIED WOOD SCREEN FEATURE
REPLACED THE OVERHUNG MASSING PREVIOUSLY SHOWN.

WOODEN SCREEN ELEMENT

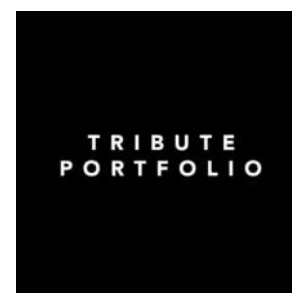
ADDITIONAL GLASS HAS BEEN ADDED TO THE CENTRAL
STAIR CORE, ALONG WITH A LIGHT VERTICAL WOOD
SCREEN TO SPLIT THE BUILDING MASSING

SITE & LANDSCAPE DEVELOPMENT

A SERIES OF TERRACES HAVE BEEN DESIGNED TO
INTEGRATE THE BUILDING INTO THE LANDSCAPE, MINIMIZE
THE VISUAL IMPACT OF THE GRADE CHANGE, AND HOUSE
PLANTINGS TO SCREEN BUILDING FACADES.



THANKS!



Attachment C

City department, agency & peer review letters/memos

From: Joyner, James M CIV USARMY CENWW (US) <James.M.Joyner@usace.army.mil>
Sent: Monday, July 15, 2019 3:21 PM
To: Participate <participate@ketchumidaho.org>
Subject: Ketchum Boutique Hotel - PEG Ketchum Hotel LLC (UNCLASSIFIED)

CLASSIFICATION: UNCLASSIFIED

To Whom it May Concern,

A review of the Ketchum Boutique Hotel project recently noticed by the City of Ketchum, indicates that Trail Creek and adjacent wetlands occur along the southern property boundary. A Department of the Army permit under Section 404 of the Clean Water Act may be required prior to the discharge of fill material into Trail Creek and/or its adjacent wetlands.

James M. Joyner
Sr. Regulatory Project Manager
US Army Corps of Engineers
Walla Walla District
Idaho Falls Regulatory Office
900 N Skyline Drive, Suite A
Idaho Falls, Idaho 83402

208-522-1676
james.m.joyner@usace.army.mil

CLASSIFICATION: UNCLASSIFIED

Ketchum Fire Department Preapplications Requirements

To: PEG Ketchum Hotel LLC / Nick Blayden
CC: Jeff Egan, Building Official
From: Tom Ancona, Assistant Chief & Fire Marshal
Date: June 24, 2019
Subject: Ketchum Boutique Hotel

The above project shall meet all 2012 International Fire Code requirements in addition to specific City Building and Fire Ordinances.

The buildings address numbers shall be placed in such a position to be plainly visible and legible from River Street. Numbers and letters shall be a minimum of four (4) inches tall, contrast with their background and be positioned a minimum of forty-eight (48) inches above final grade.

Vehicle parking and material storage during construction shall not restrict or obstruct public streets or access to any building. A **minimum** twenty-foot travel lane for emergency vehicle access shall be maintained clear and unobstructed at all times around the entire building as noted on the Construction Management Plan (V3). Once construction begins on the second floor and above, 26-foot aerial ladder truck access is required along one entire side of the building for evacuation of injured persons from upper floors. All required Fire Lanes, including within 15 feet of fire hydrants, shall be maintained clear and unobstructed at all times.

A written onsite emergency notification plan is required to be developed by the contractor's onsite Safety Officer to notify emergency services of an incident and to direct responders to the actual incident location on the site. The plan shall be reviewed and approved by the fire department prior to commencing work on the site.

An approved NFPA 13 Fire Sprinkler system is required throughout the entire building per 2012 IFC Section 903.3.1.1. and City of Ketchum Ordinance #1125 (www.ketchumfire.org). Fire sprinkler systems shall be annually tested and maintained per NFPA 25 with a report of the inspection forwarded to the fire department annually.

NOTE: One electronic set of fire sprinkler system plans, and two full size sets of fire sprinkler construction drawings shall be submitted to the Ketchum Fire Department in addition to the Idaho State Fire Marshals fire sprinkler plan submittal requirements. A separate Ketchum Fire Department Permit shall be obtained prior to installation of fire sprinkler systems. Inspections of fire sprinkler systems by the Fire Chief or an appointee are required. Inspections must be scheduled at least 48 hours in advance.

An approved Class I Standpipe system is required to be installed in all stairways. Fire hose connections shall be located on the intermediate landing of each floor including the basement level and roof levels. Fire hose connections to the standpipe system in the stairwells shall be gated 2 ½ inch NHT male couplings. The standpipe system shall be installed by the same contractor that installs the fire sprinkler system and shall meet the requirements of the most current Edition of NFPA Standard 14.

The fire department connection to the fire sprinkler system and the standpipe system shall be located on the River Street side of the building. The fire department connections to the fire sprinkler system and the standpipe systems shall be 2 ½ inch female (NHT) couplings.

An approved automatic smoke alarm system is required to be installed per IFC Section 907.2.11.2 and the requirements of NFPA 72. Two (2) sets of alarm system plans shall be submitted to the Ketchum Fire Department for approval and a Ketchum Fire Department permit is required prior to installation of alarm systems. In addition, all fire sprinkler flow switches and tamper switches shall be interconnected to the buildings alarm system. Inspections and testing of fire detection and notification systems by the Fire Chief or an appointee are required and shall be scheduled at least 48 hours in advance. Note: smoke detection is not required on the first floor in the lobby or restaurant areas.

The hotel shall prepare and Safety Evacuation Plan following the guidelines of Section 408.8 through 408.8.3 of the 2012 International Fire Code prior to the final inspection on the building.

A stairway shall be provided for roof access per Section 504.3 of the 2012 International Fire Code.

An approved Emergency Responder Radio system shall be installed throughout the building per 2012 IFC Section 510. The Emergency Responder Radio System shall provide acceptable radio coverage for all emergency responders in all locations within the building and around the building. Testing and approval of the system by the fire department is required prior to any occupation of the premises.

Fire extinguishers shall be installed and maintained per 2012 IFC Section 906 and NFPA Standard 10 both during construction and upon occupancy of the building.

Approved key boxes shall be installed, with the appropriate keys, for emergency fire department access in a location approved by the fire department. The key boxes shall be a Knox box brand and sized to accommodate keys to every door of the project. **Note: Up to four (4) key boxes may be required.**

An additional Fire hydrant shall be required on the corner of River Street and Main Street to meet the necessary for flow for the building.

All fire doors shall in emergency stairwells have signage "**Do Not Block Open**" per Section 703.2.1 of the 2012 International Fire Code.

Emergency Lighting Exit and Low-Level Exit signs shall be provided following Sections 1006 and 1011.2 of the 2012 International Fire Code.

Note: By the time the final set of plans are submitted for review the City of Ketchum may be following the 2015 International Fire Code.

Note: Additional requirements may be upon final plan review.

Inspections of fire department permit required installations shall be scheduled at least 48 hours in advance.

An 8 ½ by 11 color coded site map of this project shall be provided on paper and electronically to the fire department. This site map shall show the locations of gas shut-offs, power shut-offs, fire sprinkler riser rooms, fire department connections, alarm panels, Knox boxes, access doors, egress windows, stairways and any additional fire department requirements. Exact details for color coded "On-Sites" can be found at www.ketchumfire.org or by contacting Lieutenant Greg Martin at the Ketchum Fire Department.

Final inspections of all fire department permit required installations by the Fire Chief or an appointee are required and shall be scheduled at least 48 hours in advance. A Final Inspection Checklist can be found at www.ketchumfire.org.

Subject: Re: [EXTERNAL] PEG Ketchum Hotel
Date: Wednesday, September 18, 2019 at 3:23:11 PM Mountain Daylight Time
From: John Gaeddert
To: Joseph Meek
CC: Sherri Newland, Scott Malone, Nathan Jerke, Braley, Kordel, Brittany Skelton
Attachments: image001.png, Vitruvian Walkability Report.pdf

Joseph,

Thank you for your email and your recent presentation to the Ketchum Transportation Authority on ITD-recommendations for pedestrian improvements to SH75 through downtown Ketchum (e.g., HAWK and scramble system). Your presentation to the KTA was very helpful and, in fact, has helped inform our department that a more sophisticated HAWK system would be a superior alternative for both vehicles and pedestrians/bikes to the Rectangular Rapid Flashing Beacon (RRFB) that currently exists across SH75 at 4th street.

Two items:

- Attached is a report that Vitruvian did for the City of Ketchum in 2018. Of note is that Vitruvian recommends a RRFB at the north-side intersection of SH75 and River Street. Is your recommendation that if the City of Ketchum were to require of a nearby development (or developments) the installation of some type of system to improve pedestrian crossing at this intersection that the HAWK system (and not the RRFB system) be used?
- We have received conflicting recommendations (Slette letter of 9/10/19 and Hales Engineering Access Memorandum of 8/13/19) regarding best project access safety options for the proposed new hotel at the SW corner of the intersection of SH75/River Street. Do you have a professional recommendation for the traveling public, pedestrians/bikes and vehicles alike, for the proposed project access?

Thank you in advance for your continued assistance and insights.

My best, John

JOHN GAEDDERT, PhD | CITY OF KETCHUM

Director of Planning and Building

P.O. Box 2315 | 480 East Ave. N. | Ketchum, ID 83340

o: 208.726.7801 | f: 208.726.7812

jgaeddert@ketchumidaho.org | www.ketchumidaho.org

From: Joseph Meek <Joseph.Meek@itd.idaho.gov>
Date: Tuesday, September 17, 2019 at 2:01 PM
To: John Gaeddert <JGaeddert@ketchumidaho.org>, "Braley, Kordel" <kordel.braley@aecom.com>
Cc: Jeff Loomis <jloomis@galena-engineering.com>, Ryan Hales <ryan@halesengineering.com>, Sherri Newland <snewland@sandcassociates.com>, Scott Malone <ScottMalone@itdgov.onmicrosoft.com>,

Nathan Jerke <Nathan.Jerke@itd.idaho.gov>

Subject: RE: [EXTERNAL] PEG Ketchum Hotel

Hello John,

I want to clear up something from the Hales Engineering/ Ketchum – Boutique Hotel Memorandum. In the second paragraph under Background states: a better option.

This access being considered is not a better option to an access off of River Road. I believe that everyone sees this as an additional option or access. The accesses at River Road will not be altered or diminished.

Hales Engineering recognizes the state highway and its access restrictions. These restrictions are in place for the safety and operation of the highway.

Hales Engineering makes a good safety point about the lack of sight distance from that potential state highway access. The fact they show that there is not enough sight distance. This fault of an access will only be exacerbated by ice or snow. More distance would also be necessary for larger slow moving vehicles (service vehicles), such as a garbage truck or other service vehicles. Hales Engineering also speaks to the horizontal curve making proper sight at the access more of an issue.

It is odd to have a development use an access that that their engineer states as less than safe. It would also be a financial burden to have them build an access and infrastructure based on this access. Then have them make use of an alternate access during the construction of a bridge just south of River Street (ITD project). This makes changes to how they get product to and from service vehicles.

I am sure the City of Ketchum has many issues to weigh as well as the issue of safety.

Thank you,

Joseph Lafayette Meek III PE, PTOE, PTP

ITD D4 Traffic Engineer

208-886-7860



Your Safety • Your Mobility • Your Economic Opportunity

Attachment D

Public comment – please use following link:

<https://ketchumidaho.org/citycouncil/page/ketchum-boutique-hotel>

Attachment E

Signed Findings

1. Floodplain Development Permit
2. Plat / Readjustment of Lot Lines
3. Pre-Application Design Review reasoned statement [Placeholder - Full Design Review Findings]
4. PUD Conditional Use Permit and CUP
5. Development Agreement

Attachment E.1 - Floodplain Development Permit



City of Ketchum
Planning & Building

IN RE:)
)
PEG Ketchum Hotel, LLC) KETCHUM PLANNING AND ZONING COMMISSION
251 S. Main Street) FINDINGS OF FACT, CONCLUSIONS OF LAW AND
Floodplain Development/) DECISION
Waterways Design Review)
)
File Number: 19-062)

BACKGROUND FACTS

PROJECT: Ketchum Boutique Hotel

OWNER: PEG Ketchum Hotel, LLC

REPRESENTATIVE: Sean Flynn, Galena Engineering

LOCATION: 251 S. Main Street (Lots 3, 21, FR 22 Blk 82 N 10' x 110' of alley S 20' x 230' of alley, Ketchum Townsite), 260 E. River Street (Lot 2, Block 82, 10' x 110' of alley, Ketchum Townsite), and 280 E. River Street (Lot 1, Block 82, Ketchum Townsite)

ZONING: Tourist (T) with Floodplain Management Overlay with Floodplain, Floodway, and Waterways Design Review subdistricts

REQUEST: Floodplain Development Permit and Waterways Design Review

ASSOCIATED PERMITS: P19-061, P19-063, P19-064, P19-065, P19-066

PUBLIC NOTICE: Property owners within 300 feet and affected agencies were mailed on Friday, July 12, 2019. Public notice was published in the Idaho Mountain Express on Wednesday, July 17, 2019.

FINDINGS OF FACT

1. The City of Ketchum is a municipal corporation organized under Article XII of the Idaho Constitution and the laws of the State of Idaho, Title 50, Idaho Code. Under Chapter 65, Title 67 of the Idaho Code, the City is required to pass certain ordinances regarding land use, including a zoning ordinance.
2. The subject property, in particular the parcel addressed 251 S. Main Street, contains 100-year floodplain, floodway, and is adjacent to Trail Creek and therefore contains a regulated riparian zone. As such, the property is within the Floodplain Management Overlay with Floodplain, Floodway, and Waterways Design Review subdistricts. The southerly property boundary is the mean high-water mark on the north side of Trail Creek.

3. Pursuant to Zoning Code Title 17, Section 17.88.050(D)1, the administrator shall have the authority to consider and approve, approve with conditions, or deny applications for floodplain development permits and for waterways design review, but in accordance with 17.88.050(D)2 has the discretion to forward the application to the Planning and Zoning Commission for review during a noticed public meeting.

4. The subject parcels have previously been developed. No existing development encroaches into the riparian zone, floodplain, or floodway. Existing development consists of two vacant structures and an asphalt parking area. The asphalt parking area abuts a 10' wide sewer easement and the 25' riparian zone is adjacent to the southside of the 10' sewer easement. The 25' riparian zone is delineated on the plat associated with the hotel project (application P19-064) as a proposed riparian and scenic easement consistent with KMC § 17.88.040.D.2. The riparian zone is currently in a natural state and the north side of the bank, which is the southern boundary line of the subject property, does not contain man-made bank armoring or riprap. The 10' sewer easement combined with the 25' riparian easement provide a buffer ranging from 35' or more from the existing development and new development proposed on site (see civil sheet 3.0 in the applicant's submittal package dated July 29, 2019).

5. The applicant proposes to demolish and clear the existing development, including the asphalt surface parking area, from the site and to construct a new 100-room hotel, approximately 139,295 gross square feet in size. The hotel and all other site improvements are proposed to be located outside of the 1% annual chance floodplain, floodway, and riparian zone. Because of the sewer easement, the hotel will be located 35' or more from the mean high-water mark of Trail Creek. As indicated on the preliminary civil plans and preliminary landscape plan included in the applicant's submittal package dated July 29, 2019, all construction activity and all permanent site improvements will be located outside of the riparian zone, floodplain, and floodway.

6. As indicated in civil sheet 2.0, Storm Water Pollution Prevention Plan, measures will be taken to protect the adjacent waterway and riparian area. These measures include installation of a silt fence and inlet protection around the existing catch basin, development of a Storm Water Pollution Prevention Plan and Best Management Practices document for erosion control.

7. The applicant is put on notice that city review and approval of this application does not guarantee that flooding will not occur as the subject property is located partially within in the Special Flood Hazard Area (100-year floodplain). Purchasing flood insurance is recommended; standard property owner's insurance policies do not cover losses due to flooding.

Floodplain Development Requirements				
Chapter 17.88, Floodplain Management Overlay Zoning District (FP)				
Compliant			Standards and Staff Comments	
Yes	No	N/A	Regulation	City Standards and Staff Comments
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.040.D.3	Prior to issuance of any building permit for a structure located partially or wholly within the one percent (1%) annual chance floodplain, a preconstruction elevation certificate shall be completed by a registered professional engineer, architect or surveyor and submitted to the City of Ketchum building inspector. The elevation certificate form shall be made available from the City. The certificate shall again be completed by the owner and/or his/her agents and submitted to the City upon completion of the foundation and construction of the lowest floor, and again prior to issuance of a certificate of occupancy or a final inspection by the building inspector to evidence the as built elevation of the lowest floor including basement.
			Staff Comments	Civil sheet C3.0 best represents the location of the proposed building with respect to the one percent (1%) annual chance floodplain. As indicated on the plan, the building footprint is located entirely outside of the 1% annual chance floodplain. Therefore, Elevation Certificates are not required.

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.88.040.D.5	Prior to issuance of any floodplain development permit, the property owner or his or her authorized agent shall acknowledge by executed written affidavit that said property is located within the one percent (1%) annual chance floodplain as defined herein and that a violation of the terms of this article shall cause the City to seek legal remedies.
			<i>Staff Comments</i>	<i>The applicant is aware of this requirement and as a condition of approval, the affidavit shall be submitted prior to the Commission Chair signing the Findings of Fact and Conclusions of Law for this permit.</i>
17.88.060: PROVISIONS FOR FLOOD HAZARD REDUCTION				
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.050(E)1	Preservation or restoration of the inherent natural characteristics of the river and creeks and floodplain areas. Development does not alter river channel.
			<i>Staff Comments</i>	<p><i>The southern boundary of the property coincides with the mean high water mark of the north side of Trail Creek. Regulatory floodplain, including floodway, exists on the subject property along the southern boundary of the property and the 25'-wide riparian and scenic easement (riparian zone) is offset, landward, from the mean high water mark on the north side of Trail Creek. The riparian zone largely provides a buffer between the limit of the 1% annual chance floodplain and the remainder of the site, with the riparian buffer and floodplain boundary aligning and slightly overlapping only near the southeast corner of the site. An additional 10' wide sewer easement exists on the landward side of the riparian zone, which provides further distance between the developable portion of the property and the floodplain and Trail Creek.</i></p> <p><i>Civil sheet C 1.0 indicates existing development on the subject property with respect to the sewer easement, riparian zone, and the floodplain and floodway boundaries. Currently, two vacant buildings and an asphalt parking surface are located on the subject property. The asphalt parking surface extends directly to the north side of the 10' sewer easement. Therefore, the extent of existing development on the site is offset approximately 10' from the riparian zone and existing development is located entirely outside of the regulatory floodplain. The floodplain and riparian zone are characterized as being in an undisturbed, naturalized state.</i></p> <p><i>Civil sheet 2.0, Storm Water Pollution Prevention Plan, includes the following details: During demolition and clearance of the existing development a silt fence will be installed along the south, west, and east property. Along the southern property boundary the silt fence will be installed along the northern side of the 10' easement, therefore providing at least a 10' buffer from the riparian zone and an even larger buffer (ranging from 10' to 20'+) from the regulatory floodplain. Drain inlet protection will also be installed around the existing catch basin that retains the site's drainage. No encroachment into the floodplain or riparian zone will occur during demolition and clearance.</i></p> <p><i>The proposed new construction with respect to the floodplain and riparian zone is best indicated in Civil sheet 3.0, which includes the footprint of the proposed hotel building. The entirety of the hotel will be located outside of the riparian zone, which in turn means no portion of the hotel will be located within the regulatory floodplain. Additionally, the Landscape plan 1.0 indicates that all proposed landscaping for the project will occur outside of the riparian zone.</i></p> <p><i>Drainage improvements will be located outside of the riparian zone, and the portions of the site that are currently covered with asphalt will largely be replaced with landscaping (per the preliminary Landscape Plan), thereby reducing the impervious surface on the subject property.</i></p>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.88.050(E)2	Preservation or enhancement of riparian vegetation and wildlife habitat, if any, along the stream bank and within the required minimum twenty-five (25) foot setback or riparian zone. No construction activities, encroachment or other disturbance into the twenty five foot (25') riparian zone shall be allowed at any time without written City approval per the terms of this ordinance.
			<i>Staff Comments</i>	<i>As described in 17.88.060(E)1, no construction activity, encroachment, or other disturbance is proposed to occur within the riparian zone. The riparian zone is currently in a natural state and the new development is proposed to occur at 10 or more feet from the riparian zone boundary. No enhancement to the riparian zone is proposed at this time.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.88.050(E)3	No development, other than development by the City of Ketchum or development required for emergency access, shall occur within the twenty five foot (25') riparian zone with the exception of approved stream stabilization work. The Administrator may approve access to property where no other primary access is available. Private pathways and staircases shall not lead into or through the riparian zone unless deemed necessary by the Administrator.
			<i>Staff Comments</i>	<i>Consistent with the plans submitted as part of this application, no development is proposed in the riparian zone.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.88.050(E)4	A landscape plan and time frame shall be provided to restore any vegetation within the twenty five foot (25') riparian zone that is degraded, not natural or which does not promote bank stability.
			<i>Staff Comments</i>	<i>As described in 17.88.060(E)1, the riparian zone will be protected during demolition of the existing structures and for the duration of construction of the hotel. A restoration plan is not necessary at this time as disturbance to the riparian zone is not proposed. However, should the riparian zone inadvertently be disturbed, restoration shall be required, with a restoration plan submitted as part of an amendment to this permit.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.050(E)5	New or replacement planting and vegetation shall include plantings that are low growing and have dense root systems for the purpose of stabilizing stream banks and repairing damage previously done to riparian vegetation. Examples of such plantings include: red osier dogwood, common chokecherry, serviceberry, elderberry, river birch, skunk bush sumac, Beb's willow, Drummond's willow, little wild rose, gooseberry, and honeysuckle.
			<i>Staff Comments</i>	<i>No new or replacement plantings are required or are proposed.</i>

Compliant			Standards and Staff Comments	
Yes	No	N/A	Guideline	City Standards and Staff Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.88.050(E)6	Landscaping and driveway plans to accommodate the function of the floodplain to allow for sheet flooding. Floodwater carrying capacity is not diminished by the proposal. Surface drainage is controlled and shall not adversely impact adjacent properties including driveways drained away from paved roadways. Culvert(s) under driveways may be required. Landscaping berms shall be designed to not dam or otherwise obstruct floodwaters or divert same onto roads or other public pathways.
			<i>Staff Comments</i>	<i>Floodwater carrying capacity is not diminished by the proposal, which will improve site drainage (reduction of pervious surface) and direct all of its drainage to a on-site drywells/catch basins; site drainage shall not drain to Trail Creek. No landscaping berms are proposed and all proposed landscaping and vehicular accesses will be located outside of the floodplain.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.88.050(E)7	Impacts of the development on aquatic life, recreation, or water quality upstream, downstream or across the stream are not adverse.
			<i>Staff Comments</i>	<i>Adverse impacts on aquatic life, recreation, and water quality are not anticipated. The existing riparian zone will remain intact.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.88.050(E)8	Building setback in excess of the minimum required along waterways is encouraged. An additional ten foot (10') building setback is encouraged to provide for yards, decks and patios outside the twenty five foot (25') riparian zone.
			<i>Staff Comments</i>	<i>The required 25' riparian setback is being respected. Further, a 10' wide sewer easement exists beyond the riparian zone and will provide an additional buffer from the waterway.</i>

Compliant			Standards and Staff Comments	
Yes	No	N/A	Guideline	City Standards and Staff Comments
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.050(E)9	The top of the lowest floor of a building located in the one percent (1%) annual chance floodplain shall be a minimum of twenty four inches (24") above the base flood elevation of the subject property. (See section 17.88.060, figures 1 and 2 of this chapter.)
			Staff Comments	<i>This standard applies to buildings that are located partially or entirely within the 1% annual chance floodplain. This proposed new development will be located entirely outside of the 1% annual chance floodplain.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.88.050(E)10	The backfill used around the foundation in the floodplain shall provide a reasonable transition to existing grade but shall not be used to fill the parcel to any greater extent. Compensatory storage shall be required for any fill placed within the floodplain. An LOMA-F shall be obtained prior to placement of any additional fill in the floodplain.
			Staff Comments	<i>No fill is proposed in the floodplain; the proposed new development will be located entirely outside of the 1% annual chance floodplain. The standard for backfill around the foundation applies only to buildings that are located partially or entirely within the 1% annual chance floodplain.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.050(E)11	All new buildings shall be constructed on foundations that are approved by a licensed professional engineer.
			Staff Comments	<i>This standard applies to buildings that are located partially or entirely within the 1% annual chance floodplain. This proposed new development will be located entirely outside of the 1% annual chance floodplain.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.88.050(E)12	Driveways shall comply with effective street standards; access for emergency vehicles has been adequately provided for.
			Staff Comments	<i>The vehicular access to the hotel (ramp to a parking garage) and lower-level fire access will receive final approval from the Fire Marshal, City Engineer, and Streets Department Director prior to issuance of a building permit.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.88.050(E)13	Landscaping or revegetation shall conceal cuts and fills required for driveways and other elements of the development.
			Staff Comments	<i>A preliminary landscaping plan has been submitted. All area of the site disturbed during construction are proposed to be finished with landscaping.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.050(E)14	(Stream alteration) The proposal is shown to be a permanent solution and creates a stable situation.
			Staff Comments	<i>N/A, stream alteration is not proposed.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.050(E)15	(Stream alteration) No increase to the one percent (1%) annual chance floodplain upstream or downstream has been certified, with supporting calculations, by a registered Idaho hydraulic engineer.
			Staff Comments	<i>N/A, stream alteration is not proposed.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.050(E)16	(Stream alteration) The recreational use of the stream including access along any and all public pedestrian/fisher's easements and the aesthetic beauty shall not be obstructed or interfered with by the proposed work.
			Staff Comments	<i>N/A, stream alteration is not proposed.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.88.050(E)17	Where development is proposed that impacts any wetland, first priority shall be to move development from the wetland area. Mitigation strategies shall be proposed at time of application that replace the impacted wetland area with a comparable amount and/or quality of new wetland area or riparian habitat improvement.
			Staff Comments	<i>James Joyner, Senior Regulatory Project Manager with the U.S. Army Corps of Engineers, Walla Wall District, provided comment that wetlands exist along the southern property boundary, which is Trail Creek. However, staff discussed the location of the proposed development, which will occur only in areas of the site that have already been developed and will be located 35'+ from Trail Creek, with Mr. Joyner. As such, no change to the proposed project is necessary.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.050(E)18	(Stream alteration) Fish habitat shall be maintained or improved as a result of the work proposed.
			Staff Comments	<i>N/A, stream alteration is not proposed.</i>

Compliant			Standards and Staff Comments	
Yes	No	N/A	Guideline	City Standards and <i>Staff Comments</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.050(E)19	(Stream alteration) The proposed work shall not be in conflict with the local public interest, including, but not limited to, property values, fish and wildlife habitat, aquatic life, recreation and access to public lands and waters, aesthetic beauty of the stream and water quality. <i>Staff Comments</i> N/A, stream alteration is not proposed.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.050(E)20	(Stream alteration) The work proposed is for the protection of the public health, safety and/or welfare such as public schools, sewage treatment plant, water and sewer distribution lines and bridges providing particularly limited or sole access to areas of habitation. <i>Staff Comments</i> N/A, stream alteration is not proposed.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.060.A.1	A. General Standards: In all areas of special flood hazard, the following standards are required: 1. Anchoring: a. All new construction and substantial improvements shall be anchored to prevent flotation, collapse, or lateral movement of the structure resulting from hydrodynamic and hydrostatic loads, including the effects of buoyancy. b. All manufactured homes must likewise be anchored to prevent flotation, collapse or lateral movement, and shall be installed using methods and practices that minimize flood damage. Anchoring methods may include, but are not limited to, use of over the top or frame ties to ground anchors (reference the Federal Emergency Management Agency's "Manufactured Home Installation in Flood Hazard Areas" guidebook for additional techniques). <i>Staff Comments</i> N/A. This standard applies to buildings that are located partially or entirely within the 1% annual chance floodplain. This proposed new development will be located entirely outside of the 1% annual chance floodplain.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.060.A.2	2. Construction Materials And Methods: a. All structural and nonstructural building materials utilized at or below the base flood elevation must be flood resistant. Flood damage resistant materials must be used for all building elements subject to exposure to floodwaters, including floor joists, insulation, and ductwork. If flood damage resistant materials are not used for building elements, those elements must be elevated above the base flood elevation. This requirement applies regardless of the expected or historical flood duration. b. All new construction and substantial improvements shall be constructed using methods and practices that minimize flood damage. c. Electrical, heating, ventilation, plumbing and air conditioning equipment and other service facilities shall be designed and/or otherwise elevated or located so as to prevent water from entering or accumulating within the components during conditions of flooding. <i>Staff Comments</i> N/A. This standard applies to buildings that are located partially or entirely within the 1% annual chance floodplain. This proposed new development will be located entirely outside of the 1% annual chance floodplain.
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.060.A.3	3. Utilities: a. All new and replacement water supply systems shall be designed to minimize or eliminate infiltration of floodwaters into the system; b. New and replacement sanitary sewage systems shall be designed to minimize or eliminate infiltration of floodwaters into the systems and discharge from the systems into floodwaters; and c. On site waste disposal systems shall be located to avoid impairment to them or contamination from them during flooding. <i>Staff Comments</i> This standard applies to buildings that are located partially or entirely within the 1% annual chance floodplain. This proposed new development will be located entirely outside of the 1% annual chance floodplain
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.060.B.3.a	3. Nonresidential Construction: a. New construction and substantial improvement of any commercial, industrial, nonresidential portion of a mixed use or other nonresidential structure in any A1-30, AE and AH zone shall either have the top of the floor structure of the lowest floor, including basement, elevated to twenty four inches (24") above the base flood elevation or, together with attendant utility and sanitary facilities, shall: (1) Be floodproofed so that below the base flood level the structure is watertight with walls substantially impermeable to the passage of water;

Compliant			Standards and Staff Comments	
Yes	No	N/A	Guideline	City Standards and <i>Staff Comments</i>
				<p>(2) Have structural components capable of resisting hydrostatic and hydrodynamic loads and effects of buoyancy;</p> <p>(3) Be certified by a registered professional engineer or architect that the design and methods of construction are in accordance with accepted standards of practice for meeting provisions of this article based on their development and/or review of the structural design, specifications and plans. Such certifications shall be provided to the official as set forth in subsection 17.88.050D3c(2) of this chapter;</p> <p>(4) Nonresidential structures that are elevated, not floodproofed, must meet the same standards for space below the lowest floor as described in subsection B2c of this section; and</p> <p>(5) Applicants floodproofing nonresidential buildings shall be notified that flood insurance premiums will be based on rates that are one foot (1') below the floodproofed level (e.g., a building constructed to the base flood level will be rated as 1 foot below that level).</p>
			<i>Staff Comments</i>	<i>N/A. This standard applies to buildings that are located partially or entirely within the 1% annual chance floodplain. This proposed new development will be located entirely outside of the 1% annual chance floodplain.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.88.060.B.2.b	<p>b. All new construction and substantial improvement of nonresidential structures within AO zones shall:</p> <p>(1) Have the lowest floor (including basement) elevated above the highest adjacent grade at least twenty four inches (24") higher than the depth number specified in feet on the FIRM; or</p> <p>(2) Together with the attendant utility and sanitary facilities be completely floodproofed to that level to meet the floodproofing standard specified in subsection B3a of this section.</p>
			<i>Staff Comments</i>	<i>N/A. The subject property is in the AE zone, not the AO zone. Further, this standard applies to buildings that are located partially or entirely within the 1% annual chance floodplain. This proposed new development will be located entirely outside of the 1% annual chance floodplain.</i>

CONCLUSIONS OF LAW

1. The City of Ketchum is a municipal corporation organized under Article XII of the Idaho Constitution and the laws of the State of Idaho, Title 50, Idaho Code.
2. Under Chapter 65, Title 67 of the Idaho Code, the City has passed a land use ordinance, Title 17.
3. The Commission has the authority to hear the applicant's Floodplain Development and Waterways Design Review application pursuant to Chapter 17.88 of Ketchum Municipal Code Title 17.
4. The City of Ketchum Planning Department provided adequate notice of the time, place and summary of the applicant's proposal to be heard by the Commission for review of this application.
5. The project **does** meet the standards of approval under Chapter 17.88 of Zoning Code Title 17.

DECISION

THEREFORE, the Ketchum Planning & Zoning **approves** this application by PEG Ketchum Hotel LLC this Monday, August 12th, 2019, subject to the following conditions:

1. The term of floodplain development permit and waterways design review shall be twelve (12) months from the date that findings of fact, conclusions of law and decision are signed by the Administrator or upon appeal, the date the approval is granted by the Planning and Zoning Commission, subject to changes in zoning regulations. Application must be made for a building permit (if required) with the Ketchum Building Department during the twelve (12) month term. Once a building permit (if required) has been issued, the approval shall be valid for the duration of the building permit. Unless an extension is granted as set forth in KMC 17.88.050.G, failure to file a complete building permit application (if required) for a project in accordance with these provisions shall cause said approval to be null and void.
2. This Waterways Design Review and Floodplain Development Permit approval is based on the plans presented during the July 29th and July 30th 2019 meetings, and information presented and approved at the meeting on the date noted herein. Any building or site discrepancies which do not conform to the approved plans will be subject to removal;
3. Pursuant to Chapter 17.88.040.C, no chemicals or soil sterilants are allowed within 100 feet of the mean high-water mark. No pesticides, herbicides, or fertilizers are allowed within 25 feet of the mean high-water mark unless approved by the City Arborist. All applications of herbicides and/or pesticides within one hundred feet (100') of the mean high-water mark, but not within twenty-five feet (25') of the mean high-water mark, must be done by a licensed applicator and applied at the minimum application rates. Application times for herbicides and/or pesticides will be limited to two (2) times a year; once in the spring and once in the fall unless otherwise approved by the city arborist. The application of dormant oil sprays and insecticidal soap within the riparian zone may be used throughout the growing season as needed;
4. Prior to commencement of any construction activity on the subject parcel the silt fence and inlet protection shall be installed as indicated in civil sheet 2.0;
5. No maintenance, including the mowing, trimming, and removal of vegetation, and no construction activities, encroachment, or disturbance within the riparian zone shall take place without approval through an amendment to this permit, during the time period the permit is in effect, or through approval of a subsequent permit;
6. It shall be unlawful to dump, deposit or otherwise cause any trash, landscape debris or other material to be placed in any stream, channel, ditch, pond or basin that regularly or periodically carries or stores water.

Findings of Fact **adopted** this 12th day of August 2019.



Neil Morrow, Chair
Ketchum Planning & Zoning Commission

Attachment E.2 - Plat / Readjustment of Lot Lines



**City of Ketchum
Planning & Building**

IN RE:)	
)	
PEG Ketchum Hotel, LLC	)	KETCHUM CITY COUNCIL
Lot 3A, Block 82	)	FINDINGS OF FACT, CONCLUSIONS OF LAW AND
Subdivision	)	DECISION
)	
File Number: 19-064)	
)	

BACKGROUND FACTS

PROJECT: Lot 3A, Block 82, Ketchum Townsite

OWNER: PEG Ketchum Hotel LLC

REPRESENTATIVE: Galena Engineering

REQUEST: The applicant proposes to combine three parcels (251 S. Main Street – Ketchum Townsite Lots 3, 21, FR 22 Blk 82 N 10’ x 110’ of alley S 20’ x 230’ of alley, 260 E. River Street – Ketchum Townsite Lot 2 Block 82 10’ x 110’ of alley, and 280 E. River Street – Ketchum Townsite Lot 1 Block 82) into one lot, referenced herein as Lot 3A, Block 82, Ketchum Townsite (“new Lot 3A”).

PUBLIC NOTICE: Property owners within 300 feet and affected agencies were mailed on Friday, July 12, 2019. Public notice was published in the Idaho Mountain Express on Wednesday, July 17, 2019.

ZONING: The property is zoned Tourist (T) with Floodplain/Waterways Design Review overlay.

FINDINGS OF FACT

1. The applicant proposes to combine three parcels into a new Lot 3A, Block 82, Ketchum Townsite.
2. The proposal is subject to the City Subdivision requirements as set forth in Title 16, Chapter 16.04 of the Ketchum Municipal Code (“KMC”).
3. The application meets the definition of “readjustment of lot line” as set forth in Title 16, Chapter 16.04.030.L.

4. Applicant has submitted a Master Plan as part of its Planned Unit Development application, pursuant to Title 16, Chapter 16.08. Subject Master Plan includes a request for waiver or deferral of requirements (KMC §16.08.070.F).
5. Consistent with §16.08.070.F, Applicant requests city allow building permit issuance prior to final plat recordation and defer the requirements of KMC §16.04.030.H, which states that “No building permit shall be issued with regard to any parcel of land within a proposed subdivision until the final plat has been recorded.”
6. As set forth in the Applicant’s PUD application and Master Plan, waivers are requested to the following dimensional standard provisions: Floor Area Ratio (“FAR”), front and side yard setbacks, and height and four-story requirements.
7. Title 16, Chapter 16.04.020 defines Waiver as a “Modification of a relevant provision and regulation of this chapter not contrary to public interest or public health, safety or welfare, and due to physical characteristics of the particular parcel of land and not the result of actions of the subdivision where literal enforcement of this chapter would result in undue hardship. The granting of waiver(s) ... rests with the sound discretion of the commission and council, on a case by case basis.”
8. Details of the Applicant’s Master Plan are set forth in Attachment B. Included in Attachment B is Galena Engineers plat map showing a new Lot 3A, which combines the three existing lots owned by the Applicant within Block 82 of the Ketchum Townsite Plat into one larger lot. Subject map was prepared by Galena Engineers and was stamped by Mark Phillips on 6/12/2019.
9. New Lot 3A is 47,249 square feet and exceeds the minimum Tourist zone lot size of 8,000 square feet. Further, the property exceeds the eighty-foot (80’) minimum lot width established for the Tourist zone and includes the required twenty-five foot (25’) Riparian and Scenic Easement from the Ordinary High Water Mark (“OHWM”) established by the KMC for building setbacks along Trail Creek.
10. The following provides the Planning and Zoning Commission’s recommendations to the Ketchum City Council regarding the new Lot 3A subdivision plat within the City.

Table 1: Zoning Standards Analysis

Compliance with Zoning Standards				
Compliant			Standards and Staff Comments	
Yes	No	N/A	Guideline	City Standards and Staff Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.040	Minimum Lot Area: 9,000 square feet minimum.
			<i>Staff Comment</i>	<i>New Lot 3A is 47,249 square feet</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.040	Building Coverage
			<i>Staff Comment</i>	<i>A Planned Unit Development Application, pursuant to Title 16, Chapter 16.08 Project proposes a waiver to the FAR requirements and, subject to approval of the PUD application with conditions as noted herein, complies with this zoning standard.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.040	Minimum Building Setbacks Minimum: <i>Front: 15'</i>

				Side: > of 1' for every 2' in building height, or 10', whichever is greater Trail Creek/Rear: 25' State Highway 75: 25' to 32' (varies)
			Staff Comment	The Project proposes a waiver to the front & side yard setback requirements and, subject to approval of the PUD application with conditions as noted herein, complies with this provision of the Tourist zoning standard.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.040	Building Height Maximum Permitted: 35' or greater for hotels
			Staff Comment	The Project proposes a waiver to the height/4-story Tourist zone requirements for hotels and, subject to approval of the PUD application with conditions as noted herein, complies with this zoning standard
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.125.030.H	Curb Cut Permitted: A total of 35% of the linear footage of any street frontage can be devoted to access off street parking.
			Staff Comment	There are no curb cuts proposed along State Highway 75. The new configuration results in < 35% of the linear footage of street frontage devoted to access the off street parking within the parking garage.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.125.020.A.2 & 17.125.050	Parking Spaces Off-street parking standards of this chapter apply to any new development and to any new established uses.
			Staff Comment	As analyzed by staff and consistent with §17.125 of the KMC, the Project has adequate parking for the proposed uses on the property.

Table 2: Plat Requirements

Plat Requirements				
Compliant			Standards and Staff Comments	
Yes	No	N/A	City Code	City Standards and Staff Comments
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.C.1	The subdivider shall file with the administrator copies of the completed subdivision application form and preliminary plat data as required by this chapter.
			Staff Comments	The application has been reviewed and determined to be complete.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I	Contents Of Preliminary Plat: The preliminary plat, together with all application forms, title insurance report, deeds, maps, and other documents reasonably required, shall constitute a complete subdivision application. The preliminary plat shall be drawn to a scale of not less than one inch equals one hundred feet (1" = 100') and shall show the following:
			Staff Comments	All required materials for the Subdivision Plat application have been submitted.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .1	The scale, north point and date.
			Staff Comments	This standard has been met.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .2	The name of the proposed subdivision, which shall not be the same or confused with the name of any other subdivision in Blaine County, Idaho.
			Staff Comments	This standard has been met.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .3	The name and address of the owner of record, the subdivider, and the engineer, surveyor, or other person preparing the plat.
			Staff Comments	This standard shall be met with the Final Plat with the signed Certificate of Ownership.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .4	Legal description of the area platted.
			Staff Comments	This standard has been met.

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .5	The names and the intersecting boundary lines of adjoining subdivisions and parcels of property.
			<i>Staff Comments</i>	<i>This standard has been met.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .6	A contour map of the subdivision with contour lines and a maximum interval of five feet (5') to show the configuration of the land based upon the United States geodetic survey data, or other data approved by the city engineer.
			<i>Staff Comments</i>	<i>This standard has been met. The Subdivision Plat indicates contour lines at 1 ft intervals.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I.7	The scaled location of existing buildings, water bodies and courses and location of the adjoining or immediately adjacent dedicated streets, roadways and easements, public and private.
			<i>Staff Comments</i>	<i>This standard has been met.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .8	Boundary description and the area of the tract.
			<i>Staff Comments</i>	<i>The legal description appears on the Plat.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .9	Existing zoning of the tract.
			<i>Staff Comments</i>	<i>Each of the affected lots are located in the Tourist Zoning District.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .10	The proposed location of street rights of way, lots, and lot lines, easements, including all approximate dimensions, and including all proposed lot and block numbering and proposed street names.
			<i>Staff Comments</i>	<i>Subject items are reflected on the plat.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	16.04.030.I .11	The location, approximate size and proposed use of all land intended to be dedicated for public use or for common use of all future property owners within the proposed subdivision.
				<i>No land for common or public use is required or proposed.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .12	The location, size and type of sanitary and storm sewers, water mains, culverts and other surface or subsurface structures existing within or immediately adjacent to the proposed sanitary or storm sewers, water mains, and storage facilities, street improvements, street lighting, curbs, and gutters and all proposed utilities.
			<i>Staff Comments</i>	<i>The plat indicates the existing locations of all utilities. An encroachment permit will be required for all improvements to public right of way. A full utility plan will be required prior to final plat recordation and infrastructure construction.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .13	The direction of drainage, flow and approximate grade of all streets.
			<i>Staff Comments</i>	<i>These details are indicated. An encroachment permit will be required for all improvements to public right of way. A full utility plan will be required prior to final plat recordation and infrastructure construction.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .14	The location of all drainage canals and structures, the proposed method of disposing of runoff water, and the location and size of all drainage easements, whether they are located within or outside of the proposed plat.
			<i>Staff Comments</i>	<i>Drainage infrastructure exists within Idaho Transportation Department (ITD) right of way (ROW) near the SE corner of the property. This and related infrastructure are shown on the plat, including a drywell easement (instrument #440075) and two sanitary sewer easement (instrument #130085 and 130089). Also, a proposed new twenty-five foot (25') riparian and scenic easement is shown on the plat. Any work in this area is subject to separate floodplain rules and regulations.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .15	All percolation tests and/or exploratory pit excavations required by state health authorities.
			<i>Staff Comments</i>	<i>Applicant has submitted results of percolation and related tests.</i>

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	16.04.030.I .16	A copy of the provisions of the articles of incorporation and bylaws of homeowners' association and/or condominium declarations to be filed with the final plat of the subdivision.
			<i>Staff Comments</i>	<i>N/A</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .17	Vicinity map drawn to approximate scale showing the location of the proposed subdivision in reference to existing and/or proposed arterials and collector streets.
			<i>Staff Comments</i>	<i>This has been provided.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .18	The boundaries of the floodplain, floodway and avalanche zoning district shall also be clearly delineated and marked on the preliminary plat.
			<i>Staff Comments</i>	<i>New Lot 3A is located within the City's Floodplain Overlay District. The applicant has included an easement on the plat to protect this area.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .19	Building envelopes shall be shown on each lot, all or part of which is within a floodway, floodplain, or avalanche zone; or any lot that is adjacent to the Big Wood River, Trail Creek, or Warm Springs Creek; or any lot, a portion of which has a slope of twenty five percent (25%) or greater; or upon any lot which will be created adjacent to the intersection of two (2) or more streets.
			<i>Staff Comments</i>	<i>KMC §16.040.020 defines Building Envelope as, "the site for location of a structure delineated on a preliminary plat and final plat within which the entire building must be constructed. A building envelope shall conform to all minimum zoning ordinance requirements and requirements of this chapter." This application is a minor amendment to shift an interior boundary line in order to for the Applicant to not build over existing lot lines. Applicant has shown a new easement to assure no building is constructed within 25' of Trail Creek. A building envelope ("BE") will be reflected on the final plat consistent with the approved final Agreement for the Project. Subject BE will reflect the Council's final approval on FAR, corner lot radii sight line requirements, and front/side yard setbacks. The BE shall not encroach within 25' of Trail Creek or the edge of ITD ROW.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .20	Lot area of each lot.
			<i>Staff Comments</i>	<i>The areas of each lot are indicated on the Plat.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .21	Existing mature trees and established shrub masses.
			<i>Staff Comments</i>	<i>Existing mature trees and established shrub masses exist on the Property. Subject vegetation along Trail Creek helps provide important habitat and benefits to the stream. Mature trees along River Street provide a visual buffer for the old, dilapidated structures. A landscaping plan has been submitted for the Project, as well as a separate Floodplain Development Permit. The removal of existing mature trees and/or established shrub masses is subject to approval and adoption of the Applicant's landscape plan through the Design Review and/or Floodplain Development Permit process.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .22	A current title report shall be provided at the time that the preliminary plat is filed with the administrator, together with a copy of the owner's recorded deed to such property.
			<i>Staff Comments</i>	<i>A current title report and a copy of the both owners' recorded deed to the subject properties were included in the Plat application submittal.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.030.I .23	Three (3) copies of the preliminary plat shall be filed with the administrator.
			<i>Staff Comments</i>	<i>A digital copy for reproduction was submitted with the application. Therefore, Staff required only one (1) full size copy of the preliminary plat.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.040.A	Required Improvements: The improvements set forth in this section shall be shown on the preliminary plat and installed prior to approval of the final plat. Construction design plans shall be submitted and approved by the city engineer.

				<p>All such improvements shall be in accordance with the comprehensive plan and constructed in compliance with construction standard specifications adopted by the city. Existing natural features which enhance the attractiveness of the subdivision and community, such as mature trees, watercourses, rock outcroppings, established shrub masses and historic areas, shall be preserved through design of the subdivision.</p>
			Staff Comments	<p>The property is served by city water and sewer services, as well as other public and private utilities (TV, gas, electric, etc). Also serving new Lot 3A is an existing six-foot (6') wide sidewalk within ITD ROW. Subject sidewalk width is less than the current eight-foot (8') wide city sidewalk standard. Further, no sidewalks exist for the Property along River Street, which also has an eight-foot (8') wide sidewalk requirement. An encroachment permit from ITD as well as the City will be required for all improvements to public right of way, including the construction of a sidewalk not less than eight-feet (8') in width along River Street. As a condition of Plat approval, subject sidewalk shall be installed prior to final plat recordation unless otherwise approved by the Ketchum City Council.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.040.B	<p>Improvement Plans: Prior to approval of final plat by the commission, the subdivider shall file two (2) copies with the city engineer, and the city engineer shall approve construction plans for all improvements required in the proposed subdivision. Such plans shall be prepared by a civil engineer licensed in the state.</p>
			Staff Comments	<p>Subject plans are required.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.040.C	<p>Prior to final plat approval, the subdivider shall have previously constructed all required improvements and secured a certificate of completion from the city engineer. However, in cases where the required improvements cannot be constructed due to weather conditions or other factors beyond the control of the subdivider, the city council may accept, in lieu of any or all of the required improvements, a performance bond filed with the city clerk to ensure actual construction of the required improvements as submitted and approved. Such performance bond shall be issued in an amount not less than one hundred fifty percent (150%) of the estimated costs of improvements as determined by the city engineer. In the event the improvements are not constructed within the time allowed by the city council (which shall be one year or less, depending upon the individual circumstances), the council may order the improvements installed at the expense of the subdivider and the surety. In the event the cost of installing the required improvements exceeds the amount of the bond, the subdivider shall be liable to the city for additional costs. The amount that the cost of installing the required improvements exceeds the amount of the performance bond shall automatically become a lien upon any and all property within the subdivision owned by the owner and/or subdivider.</p>
			Staff Comments	<p>Subject plans and sidewalk improvement is required.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.040.D	<p>As Built Drawing: Prior to acceptance by the city council of any improvements installed by the subdivider, two (2) sets of as built plans and specifications, certified by the subdivider's engineer, shall be filed with the city engineer. Within ten (10) days after completion of improvements and submission of as built drawings, the city engineer shall certify the completion of the improvements and the acceptance of the improvements, and shall submit a copy of such certification to the administrator and the subdivider. If a performance bond has been filed, the administrator shall forward a copy of the certification to the city clerk. Thereafter, the city clerk shall release the performance bond upon application by the subdivider.</p>
				<p>Subject plans are required.</p>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.040.E	<p>Monumentation: Following completion of construction of the required improvements and prior to certification of completion by the city engineer, certain land survey monuments shall be reset or verified by the subdivider's engineer or surveyor to still be in place. These monuments shall have the size, shape, and type of material as shown on the subdivision plat. The monuments shall be located as follows:</p> <ol style="list-style-type: none"> 1. All angle points in the exterior boundary of the plat. 2. All street intersections, points within and adjacent to the final plat. 3. All street corner lines ending at boundary line of final plat. 4. All angle points and points of curves on all streets. 5. The point of beginning of the subdivision plat description.
			<i>Staff Comments</i>	<i>Monumentation required.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.040.F	<p>Lot Requirements:</p> <ol style="list-style-type: none"> 1. Lot size, width, depth, shape and orientation and minimum building setback lines shall be in compliance with the zoning district in which the property is located and compatible with the location of the subdivision and the type of development, and preserve solar access to adjacent properties and buildings. 2. Whenever a proposed subdivision contains lot(s), in whole or in part, within the floodplain, or which contains land with a slope in excess of twenty five percent (25%), based upon natural contours, or creates corner lots at the intersection of two (2) or more streets, building envelopes shall be shown for the lot(s) so affected on the preliminary and final plats. The building envelopes shall be located in a manner designed to promote harmonious development of structures, minimize congestion of structures, and provide open space and solar access for each lot and structure. Also, building envelopes shall be located to promote access to the lots and maintenance of public utilities, to minimize cut and fill for roads and building foundations, and minimize adverse impact upon environment, watercourses and topographical features. Structures may only be built on buildable lots. Lots shall only be created that meet the definition of "lot, buildable" in section 16.04.020 of this chapter. Building envelopes shall be established outside of hillsides of twenty five percent (25%) and greater and outside of the floodway. A waiver to this standard may only be considered for the following: <ol style="list-style-type: none"> a. For lot line shifts of parcels that are entirely within slopes of twenty five percent (25%) or greater to create a reasonable building envelope, and mountain overlay design review standards and all other city requirements are met. b. For small, isolated pockets of twenty five percent (25%) or greater that are found to be in compliance with the purposes and standards of the mountain overlay district and this section. 3. Corner lots shall have a property line curve or corner of a minimum radius of twenty five feet (25') unless a longer radius is required to serve an existing or future use. 4. Side lot lines shall be within twenty degrees (20°) to a right angle or radial line to the street line. 5. Double frontage lots shall not be created. A planting strip shall be provided along the boundary line of lots adjacent to arterial streets or incompatible zoning districts. Should a double frontage lot(s) be created out of necessity, then such lot(s) shall be reversed frontage lot(s). 6. Minimum lot sizes in all cases shall be reversed frontage lot(s).

				7. Every lot in a subdivision shall have a minimum of twenty feet (20') of frontage on a dedicated public street or legal access via an easement of twenty feet (20') or greater in width. Easement shall be recorded in the office of the Blaine County recorder prior to or in conjunction with recordation of the final plat.
			Staff Comments	<i>The Project complies with each of these requirements. A building envelope ("BE") will be reflected on the final plat consistent with the approved final Agreement for the Project. Subject BE will reflect the Council's final approval on FAR, corner lot radii sight line requirements, and front/side yard setbacks. The BE shall not encroach within 25' of Trail Creek or the edge of ITD ROW.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	16.04.040.G	<p>G. Block Requirements: The length, width and shape of blocks within a proposed subdivision shall conform to the following requirements:</p> <ol style="list-style-type: none"> 1. No block shall be longer than one thousand two hundred feet (1,200'), nor less than four hundred feet (400') between the street intersections, and shall have sufficient depth to provide for two (2) tiers of lots. 2. Blocks shall be laid out in such a manner as to comply with the lot requirements. 3. The layout of blocks shall take into consideration the natural topography of the land to promote access within the subdivision and minimize cuts and fills for roads and minimize adverse impact on environment, watercourses and topographical features. 4. Corner lots shall contain a building envelope outside of a seventy-five foot (75') radius from the intersection of the streets.
			Staff Comments	<i>This application does not create a new block. This requirement is not applicable. Notwithstanding, a Building Envelope ("BE") will be reflected on the final plat consistent with the approved final Agreement for the Project. Subject BE will reflect the Council's final approval on corner lot radii sight line requirements.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.040.H	<p>Street Improvement Requirements:</p> <ol style="list-style-type: none"> 1. The arrangement, character, extent, width, grade and location of all streets put in the proposed subdivision shall conform to the comprehensive plan and shall be considered in their relation to existing and planned streets, topography, public convenience and safety, and the proposed uses of the land; 2. All streets shall be constructed to meet or exceed the criteria and standards set forth in chapter 12.04 of this code, and all other applicable ordinances, resolutions or regulations of the city or any other governmental entity having jurisdiction, now existing or adopted, amended or codified; 3. Where a subdivision abuts or contains an existing or proposed arterial street, railroad or limited access highway right of way, the council may require a frontage street, planting strip, or similar design features; 4. Streets may be required to provide access to adjoining lands and provide proper traffic circulation through existing or future neighborhoods; 5. Street grades shall not be less than three-tenths percent (0.3%) and not more than seven percent (7%) so as to provide safe movement of traffic and emergency vehicles in all weather and to provide for adequate drainage and snow plowing; 6. In general, partial dedications shall not be permitted, however, the council may accept a partial street dedication when such a street forms a boundary of the proposed subdivision and is deemed necessary for the orderly development of the neighborhood, and provided the council finds it practical to require the dedication of the remainder of the right of way when the adjoining property is

				<p>subdivided. When a partial street exists adjoining the proposed subdivision, the remainder of the right of way shall be dedicated;</p> <p>7. Dead end streets may be permitted only when such street terminates at the boundary of a subdivision and is necessary for the development of the subdivision or the future development of the adjacent property. When such a dead end street serves more than two (2) lots, a temporary turnaround easement shall be provided, which easement shall revert to the adjacent lots when the street is extended;</p> <p>8. A cul-de-sac, court or similar type street shall be permitted only when necessary to the development of the subdivision, and provided, that no such street shall have a maximum length greater than four hundred feet (400') from entrance to center of turnaround, and all cul-de-sacs shall have a minimum turnaround radius of sixty feet (60') at the property line and not less than forty five feet (45') at the curb line;</p> <p>9. Streets shall be planned to intersect as nearly as possible at right angles, but in no event at less than seventy degrees (70°);</p> <p>10. Where any street deflects an angle of ten degrees (10°) or more, a connecting curve shall be required having a minimum centerline radius of three hundred feet (300') for arterial and collector streets, and one hundred twenty five feet (125') for minor streets;</p> <p>11. Streets with centerline offsets of less than one hundred twenty five feet (125') shall be prohibited;</p> <p>12. A tangent of at least one hundred feet (100') long shall be introduced between reverse curves on arterial and collector streets;</p> <p>13. Proposed streets which are a continuation of an existing street shall be given the same names as the existing street. All new street names shall not duplicate or be confused with the names of existing streets within Blaine County, Idaho. The subdivider shall obtain approval of all street names within the proposed subdivision from the commission before submitting same to council for preliminary plat approval;</p> <p>14. Street alignment design shall follow natural terrain contours to result in safe streets, usable lots, and minimum cuts and fills;</p> <p>15. Street patterns of residential areas shall be designed to create areas free of through traffic, but readily accessible to adjacent collector and arterial streets;</p> <p>16. Reserve planting strips controlling access to public streets shall be permitted under conditions specified and shown on the final plat, and all landscaping and irrigation systems shall be installed as required improvements by the subdivider;</p> <p>17. In general, the centerline of a street shall coincide with the centerline of the street right of way, and all crosswalk markings shall be installed by the subdivider as a required improvement;</p> <p>18. Street lighting may be required by the commission or council where appropriate and shall be installed by the subdivider as a requirement improvement;</p> <p>19. Private streets may be allowed upon recommendation by the commission and approval by the council. Private streets shall be constructed to meet the design standards specified in subsection H2 of this section;</p> <p>20. Street signs shall be installed by the subdivider as a required improvement of a type and design approved by the administrator and shall be consistent with the type and design of existing street signs elsewhere in the city;</p> <p>21. Whenever a proposed subdivision requires construction of a new bridge, or will create substantial additional traffic which will require construction of a new</p>
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				<p>bridge or improvement of an existing bridge, such construction or improvement shall be a required improvement by the subdivider. Such construction or improvement shall be in accordance with adopted standard specifications;</p> <p>22. Sidewalks, curbs and gutters may be a required improvement installed by the subdivider; and</p> <p>23. Gates are prohibited on private roads and parking access/entranceways, private driveways accessing more than one single-family dwelling unit and one accessory dwelling unit, and public rights of way unless approved by the city council.</p>
			<i>Staff Comments</i>	<i>The Project does not create a new street. With the exception of Street lighting these standards are not applicable. Street lighting in compliance with City standards is required of the Applicant consistent with this standard.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	16.04.040.I	<p>Alley Improvement Requirements: Alleys shall be provided in business, commercial and light industrial zoning districts. The width of an alley shall be not less than twenty feet (20'). Alley intersections and sharp changes in alignment shall be avoided, but where necessary, corners shall be provided to permit safe vehicular movement. Dead end alleys shall be prohibited. Improvement of alleys shall be done by the subdivider as required improvement and in conformance with design standards specified in subsection H2 of this section.</p>
			<i>Staff Comments</i>	<i>This proposal does not create a new alley. This standard is not applicable.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.040.J	<p>Required Easements: Easements, as set forth in this subsection, shall be required for location of utilities and other public services, to provide adequate pedestrian circulation and access to public waterways and lands.</p> <p>1. A public utility easement at least ten feet (10') in width shall be required within the street right of way boundaries of all private streets. A public utility easement at least five feet (5') in width shall be required within property boundaries adjacent to Warm Springs Road and within any other property boundary as determined by the city engineer to be necessary for the provision of adequate public utilities.</p> <p>2. Where a subdivision contains or borders on a watercourse, drainageway, channel or stream, an easement shall be required of sufficient width to contain such watercourse and provide access for private maintenance and/or reconstruction of such watercourse.</p> <p>3. All subdivisions which border the Big Wood River, Trail Creek and Warm Springs Creek shall dedicate a ten foot (10') fish and nature study easement along the riverbank. Furthermore, the council shall require, in appropriate areas, an easement providing access through the subdivision to the bank as a sportsman's access. These easement requirements are minimum standards, and in appropriate cases where a subdivision abuts a portion of the river adjacent to an existing pedestrian easement, the council may require an extension of that easement along the portion of the riverbank which runs through the proposed subdivision.</p> <p>4. All subdivisions which border on the Big Wood River, Trail Creek and Warm Springs Creek shall dedicate a twenty five foot (25') scenic easement upon which no permanent structure shall be built in order to protect the natural vegetation and wildlife along the riverbank and to protect structures from damage or loss due to riverbank erosion.</p> <p>5. No ditch, pipe or structure for irrigation water or irrigation wastewater shall be constructed, rerouted or changed in the course of planning for or constructing required improvements within a proposed subdivision unless same</p>

				<p>has first been approved in writing by the ditch company or property owner holding the water rights. A written copy of such approval shall be filed as part of required improvement construction plans.</p> <p>6. Nonvehicular transportation system easements including pedestrian walkways, bike paths, equestrian paths, and similar easements shall be dedicated by the subdivider to provide an adequate nonvehicular transportation system throughout the city.</p>
			<i>Staff Comments</i>	<i>A ten-foot (10') fishermen's easement needs to be shown on the plat in accordance with subsection 3 herein. The required twenty-five foot (25') riparian and scenic easement along Trail Creek is established in accordance with subsection 4. Standards #1,2, 5 & 6 are not applicable.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	16.04.040.K	<p>Sanitary Sewage Disposal Improvements: Central sanitary sewer systems shall be installed in all subdivisions and connected to the Ketchum sewage treatment system as a required improvement by the subdivider. Construction plans and specifications for central sanitary sewer extension shall be prepared by the subdivider and approved by the city engineer, council and Idaho health department prior to final plat approval. In the event that the sanitary sewage system of a subdivision cannot connect to the existing public sewage system, alternative provisions for sewage disposal in accordance with the requirements of the Idaho department of health and the council may be constructed on a temporary basis until such time as connection to the public sewage system is possible. In considering such alternative provisions, the council may require an increase in the minimum lot size and may impose any other reasonable requirements which it deems necessary to protect public health, safety and welfare.</p>
			<i>Staff Comments</i>	<i>N/A as the existing development connects to the public sewage system.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	16.04.040.L	<p>Water System Improvements: A central domestic water distribution system shall be installed in all subdivisions by the subdivider as a required improvement. The subdivider shall also be required to locate and install an adequate number of fire hydrants within the proposed subdivision according to specifications and requirements of the city under the supervision of the Ketchum fire department and other regulatory agencies having jurisdiction. Furthermore, the central water system shall have sufficient flow for domestic use and adequate fire flow. All such water systems installed shall be looped extensions, and no dead end systems shall be permitted. All water systems shall be connected to the municipal water system and shall meet the standards of the following agencies: Idaho department of public health, Idaho survey and rating bureau, district sanitarian, Idaho state public utilities commission, Idaho department of reclamation, and all requirements of the city.</p>
			<i>Staff Comments</i>	<i>N/A as water system improvements are existing.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	16.04.040.M	<p>Planting Strip Improvements: Planting strips shall be required improvements. When a predominantly residential subdivision is proposed for land adjoining incompatible uses or features such as highways, railroads, commercial or light industrial districts or off street parking areas, the subdivider shall provide planting strips to screen the view of such incompatible features. The subdivider shall submit a landscaping plan for such planting strip with the preliminary plat application, and the landscaping shall be a required improvement.</p>
			<i>Staff Comments</i>	<i>This standard is not applicable.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.040.N	<p>Cuts, Fills, And Grading Improvements: Proposed subdivisions shall be carefully planned to be compatible with natural topography, soil conditions, geology and hydrology of the site, as well as to minimize cuts, fills, alterations of topography,</p>

				<p>streams, drainage channels, and disruption of soils and vegetation. The design criteria shall include the following:</p> <ol style="list-style-type: none"> 1. A preliminary soil report prepared by a qualified engineer may be required by the commission and/or council as part of the preliminary plat application. 2. Preliminary grading plan prepared by a civil engineer shall be submitted as part of all preliminary plat applications. Such plan shall contain the following information: <ol style="list-style-type: none"> a. Proposed contours at a maximum of five foot (5') contour intervals. b. Cut and fill banks in pad elevations. c. Drainage patterns. d. Areas where trees and/or natural vegetation will be preserved. e. Location of all street and utility improvements including driveways to building envelopes. f. Any other information which may reasonably be required by the administrator, commission or council to adequately review the affect of the proposed improvements. 3. Grading shall be designed to blend with natural landforms and to minimize the necessity of padding or terracing of building sites, excavation for foundations, and minimize the necessity of cuts and fills for streets and driveways. 4. Areas within a subdivision which are not well suited for development because of existing soil conditions, steepness of slope, geology or hydrology shall be allocated for open space for the benefit of future property owners within the subdivision. 5. Where existing soils and vegetation are disrupted by subdivision development, provision shall be made by the subdivider for revegetation of disturbed areas with perennial vegetation sufficient to stabilize the soil upon completion of the construction. Until such times as such revegetation has been installed and established, the subdivider shall maintain and protect all disturbed surfaces from erosion. 6. Where cuts, fills, or other excavations are necessary, the following development standards shall apply: <ol style="list-style-type: none"> a. Fill areas shall be prepared by removing all organic material detrimental to proper compaction for soil stability. b. Fills shall be compacted to at least ninety five percent (95%) of maximum density as determined by AASHO T99 (American Association of State Highway Officials) and ASTM D698 (American standard testing methods). c. Cut slopes shall be no steeper than two horizontal to one vertical (2:1). Subsurface drainage shall be provided as necessary for stability. d. Fill slopes shall be no steeper than three horizontal to one vertical (3:1). Neither cut nor fill slopes shall be located on natural slopes of three to one (3:1) or steeper, or where fill slope toes out within twelve feet (12') horizontally of the top and existing or planned cut slope. e. Toes of cut and fill slopes shall be set back from property boundaries a distance of three feet (3'), plus one-fifth (1/5) of the height of the cut or the fill, but may not exceed a horizontal distance of ten feet (10'); tops and toes of cut and fill slopes shall be set back from structures at a distance of at least six feet (6'), plus one-fifth (1/5) of the height of the cut or the fill. Additional setback distances shall be provided as necessary to accommodate drainage features and drainage structures.
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			Staff Comments	<i>The Applicant has submitted a soils report and landscaping plan for the Project. As conditioned herein, prior to grading occurring on the new Lot 3A, City approval of the Applicant's grading, drainage and landscaping construction drawings is required. Subject construction drawings shall be consistent in concept with approved Design Review, Encroachment Permit, and related drawings.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.04.040.O	Drainage Improvements: The subdivider shall submit with the preliminary plat application such maps, profiles, and other data prepared by an engineer to indicate the proper drainage of the surface water to natural drainage courses or storm drains, existing or proposed. The location and width of the natural drainage courses shall be shown as an easement common to all owners within the subdivision and the city on the preliminary and final plat. All natural drainage courses shall be left undisturbed or be improved in a manner that will increase the operating efficiency of the channel without overloading its capacity. An adequate storm and surface drainage system shall be a required improvement in all subdivisions and shall be installed by the subdivider. Culverts shall be required where all water or drainage courses intersect with streets, driveways or improved public easements and shall extend across and under the entire improved width including shoulders.
			Staff Comments	<i>Prior to grading occurring on the new Lot 3A, City approval of the Applicant's grading, drainage and landscaping construction drawings is required. Subject construction drawings shall be consistent in concept with approved Design Review, Encroachment Permit, and related drawings.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	16.04.040.P	Utilities: In addition to the terms mentioned in this section, all utilities including, but not limited to, electricity, natural gas, telephone and cable services shall be installed underground as a required improvement by the subdivider. Adequate provision for expansion of such services within the subdivision or to adjacent lands including installation of conduit pipe across and underneath streets shall be installed by the subdivider prior to construction of street improvements.
			Staff Comments	<i>N/A as the subject property is served by existing utilities.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	16.04.040.Q	Off Site Improvements: Where the offsite impact of a proposed subdivision is found by the commission or council to create substantial additional traffic, improvements to alleviate that impact may be required of the subdivider prior to final plat approval, including, but not limited to, bridges, intersections, roads, traffic control devices, water mains and facilities, and sewer mains and facilities.
			Staff Comments	<i>No off-site improvements are required as a condition of platting new Lot 3A.</i>

CONCLUSIONS OF LAW

1. The City of Ketchum is a municipal corporation organized under Article XII of the Idaho Constitution and the laws of the State of Idaho, Title 50, Idaho Code.
2. Under Chapter 65, Title 67 of the Idaho Code, the City has passed a land use and subdivision ordinance, Title 16.
3. The City of Ketchum Planning Department provided adequate notice of the time, place and summary of the applicant's proposal to be heard by the Council for review of this application.

4. The proposed plat does meet the standards of approval under Title 16, Chapter 16.04, subject to conditions of approval.
5. Approval is granted for the combination of three parcels (251 S. Main Street – Ketchum Townsite Lots 3, 21, FR 22 Blk 82 N 10' x 110' of alley S 20' x 230' of alley, 260 E. River Street – Ketchum Townsite Lot 2 Block 82 10' x 110' of alley, and 280 E. River Street – Ketchum Townsite Lot 1 Block 82) into one lot, referenced as Lot 3A, Block 82, Ketchum Townsite, as stamped by Mark Phillips with Galena Engineers on 6/12/2019.


DECISION

THEREFORE, the Ketchum Planning & Zoning **recommends for approval** to the Ketchum City Council the request of PEG Ketchum Hotel LLC to reconfigure subject lots as depicted on the 6/12/2019 Plat prepared by Galena Engineers consistent with the following fifteen (15) conditions of approval.

1. Subject to City Council approval of the Master Plan and PUD, including deferment of the final plat Time Limitations set forth in KMC §16.04.030.H, the Applicant may be issued a building permit prior to final plat recordation and shall record the new Lot 3A plat concurrently with a Certificate of Occupancy being issued by the City for the Project;
2. The recorded plat shall show a minimum of two Blaine County Survey Control Monuments with ties to the property and an inverse between the two monuments. The Survey Control Monuments shall be clearly identified on the face of the map;
3. An electronic CAD file shall be submitted to the City of Ketchum prior to final plat signature by the City Clerk. The electronic CAD file shall be submitted to the Blaine County Recorder's office concurrent with the recording of the Plat containing the following minimum data:
 - a. Line work delineating all parcels and roadways on a CAD layer/level designated as "parcel";
 - b. Line work delineating all roadway centerlines on a CAD layer/level designated as "road"; and,
 - c. Line work that reflects the ties and inverses for the Survey Control Monuments shown on the face of the Plat shall be shown on a CAD layer/level designated as "control";
4. All information within the electronic file shall be oriented and scaled to Grid per the Idaho State Plane Coordinate System, Central Zone, NAD1983 (1992), U.S. Survey Feet, using the Blaine County Survey Control Network. Electronic CAD files shall be submitted in a ".dwg", ".dgn" or ".shp" format and shall be submitted digitally to the City on a compact disc. When the endpoints of the lines submitted are indicated as coincidental with another line, the CAD line endpoints shall be separated by no greater than 0.0001 drawing units;
5. The applicant shall provide a copy of the recorded final plat with its recorded instrument number to the Department of Planning and Building for the official file on the application;
6. All requirements of the Fire, Utility, Building, Planning and Public Works departments of the City of Ketchum shall be met. All public improvements shall meet the requirements of the Public Works Department;

7. All other provisions of Ketchum Municipal Code, Chapter 16, Subdivision Regulations, and all applicable ordinances rules and regulations of the city and other governmental entities having jurisdiction shall be complied with by the subdivision;
8. The Project proposes waivers to the floor area ratio, front and side yard setbacks, and height and four-story requirements and, subject to approval of the PUD application with conditions as noted herein, complies with each of the Tourist Zone dimensional standards for hotels;
9. An encroachment permit will be required for all improvements to public right of way. A full utility plan will be required prior to final plat recordation and infrastructure construction;
10. A twenty-five foot (25') riparian and scenic easement is shown on the plat. Any work in this area is subject to separate floodplain rules and regulations;
11. A ten-foot (10') fishermen's easement adjacent to the OHWM of Trail Creek needs to be shown and recorded on the plat;
12. A Building Envelope ("BE") will be reflected on the final plat consistent with the approved final Agreement for the Project. Subject BE will reflect the Council's final approval on FAR, corner lot radii sight line requirements, and front/side yard setbacks. The BE shall not encroach within 25' of Trail Creek or the edge of ITD ROW;
13. The removal of existing mature trees and/or established shrub masses is subject to approval and adoption of the Applicant's landscape plan through the Design Review and/or Floodplain Development Permit process;
14. An encroachment permit from ITD as well as the City will be required for all improvements to public right of way, including the construction of a sidewalk not less than eight-feet (8') in width along River Street. As a condition of Plat approval, subject sidewalk and street lighting to city standards shall be installed prior to final plat recordation unless otherwise approved by the Ketchum City Council; and
15. Prior to grading occurring on the new Lot 3A, City approval of the Applicant's grading, drainage and landscaping construction drawings is required. Subject construction drawings shall be consistent in concept with approved Design Review, Encroachment Permit, and related drawings.

Findings of Fact adopted this 12th day of August 2019.



Neil Morrow, Chair
Ketchum Planning & Zoning Commission

Attachment E.3 – Pre-Application Design Review reasoned statement

[Placeholder - Full Design Review Findings]



City of Ketchum
Planning & Building

IN RE:)	
)	
PEG Ketchum Hotel, LLC	)	KETCHUM PLANNING AND ZONING COMMISSION
251 S Main Street &	)	REASONED STATEMENT
260 & 280 E River St	)	MEMORIALIZING MOTION TO
Pre-Application Design Review	)	ADVANCE HOTEL PROJECT TO FINAL DESIGN REVIEW
)	
File Number: 19-061)	

PROJECT: Ketchum Boutique Hotel Pre-Application Design Review

FILE NUMBER: P19-061

OWNER/DEVELOPER: PEG Ketchum Hotel, LLC

REPRESENTATIVES: AJC Architects, Eggers Associates Landscape Architecture, Galena Engineering

REQUEST: Pre-Application Design Review for the development of a new 100-room hotel at the southwest corner of River Street and State Highway 75/Main Street. See Attachment B for Applicant Project Site Submittal Drawings, Narratives & Studies.

LOCATION: 251 S. Main Street – Ketchum Townsite Lots 3, 21, FR 22 Blk 82 N 10’ x 110’ of alley S 20’ x 230’ of alley, 260 E. River Street – Ketchum Townsite Lot 2 Block 82 10’ x 110’ of alley, and 280 E. River Street – Ketchum Townsite Lot 1 Block 82

NOTICE: Public notice is not required for Pre-Application Design Review. Property owners within 300 feet and affected agencies were mailed a public hearing notice regarding the hotel project on Friday, July 12, 2019. Public notice was published in the Idaho Mountain Express on Wednesday, July 17, 2019.

ZONING: Tourist (T) Zoning District

OVERLAY: Floodplain & Waterways Design Review Subdistricts

The subject Pre-Application Design Review is for the development of a new 100-room hotel at the southwest corner of River Street and State Highway 75/Main Street located in the Tourist (T) Zoning District with Floodplain Overlay. The project site currently consists of three lots which are proposed to be combined into a single parcel through Lot Line Shift Application P19-064. The total area of the combined parcel will be 47,248 sq ft. The two existing, vacant structures on the site will be removed, as will the existing asphalt surface parking lot. The property is bounded to the east by State Highway 75 (named Main Street as it passes through Ketchum city limits) and to the west by commercial mixed-use buildings. The Limelight Hotel is located directly north of the development site across River Street. Two commercial office buildings located at 220 E River Street are sited to the west of the hotel project development parcel. Adjacent development to the east across Main Street includes residential development including single-family residences, Trail Creek Crossings condominiums, and the Onyx at Leadville condominiums currently under construction.

Design Review is required for developing new non-residential projects, such as the proposed hotel, in all zoning districts. Prior to granting Design Review approval, the Planning & Zoning Commission must determine that (1) the project does not jeopardize the health, safety, or welfare of the public and (2) that the project conforms to all applicable standards and criteria set forth in Chapter 17.96 Design Review of Ketchum Municipal Code (KMC), Title 17 Zoning Regulations, and all applicable standards and ordinances as adopted and amended by the City of Ketchum. Hotels may exceed certain dimensional standards as specified in KMC §17.124.050.A through a planned unit development (Chapter 16.08 of KMC) and an associated subarea analysis (KMC §16.08.050) addressing impacts on the character and scale of the surrounding neighborhood. Pursuant to KMC §16.08.070.D, all Design Review standards shall be carefully analyzed and considered. This includes detailed analysis of building bulk, undulation, and other design elements. The site plan should be sensitive to the architecture and scale of the surrounding neighborhood.

Four (4) waivers are submitted for the Project. These include waivers to minimum lot size, setback (front and side yards), height, and floor area ratio (FAR) limitations. These waivers were requested by the Applicant consistent with KMC §16.04.120, §16.08.080 and §17.124.050, in part, as the literal enforcement of city code in the context of the special physical characteristics and conditions affecting the property would result in undue hardship. In particular, the Hotel site has a significant slope with a grade differential of approximately thirty-seven feet (37') from Trail Creek at the south end of the lot to the north end along River Street. The site is constrained by the river to the south and development is prohibited within the associated riparian area. The City also desires to setback structures from State Highway 75 (SH75) in this location to help preserve the entry to town and minimize shading of the highway during winter months. Further, the grade along SH75, future Idaho Transportation Department (ITD) bridge and highway expansion plans, and a desire for no access onto SH75 in this location create unique conditions for development. See the Planned Unit Development and Conditional Use Permit (File No. 19-063) *Findings of Fact, Conclusions of Law, Order of Decision and Recommendation to the City Council* for analysis regarding the hotel project's requested waivers and potential impacts to public health, safety, and welfare as well as recommended remedies. The Commission must balance the waivers requested due to the site specific constraints with the merits of the proposed design.

Pursuant to KMC §17.96.020, the purpose of Design Review is to maintain and enhance the appearance, character, beauty, and function of the City, to ensure that new development is complementary to the design of existing City neighborhoods, and to protect and enhance the economic base of the City of Ketchum. Design Review criteria and standards encourage new development to align with the community's vision for Ketchum as identified in the 2014 Comprehensive Plan—as an authentic mountain community with a world-class character, yet small town feel (Ketchum Comprehensive Plan, 7).

The hotel project is subject to Pre-Application Design Review per KMC §17.96.010.C.1. The purpose of Pre-Application Design Review is to allow the Commission to exchange ideas and give direction to the applicant on the preliminary design concept in relation to all Design Review criteria and evaluation standards (KMC §17.96.060). The Pre-Application phase is an opportunity for the Commission to provide input to the applicant in order to enhance the proposed architectural design, material selection, and compatibility with the surrounding neighborhood as well as provide direction to enhance the project's alignment with Ketchum's values and vision as identified in the Comprehensive Plan.

Design Review standards include criteria regarding street design (KMC §17.96.060.A), sidewalks (KMC §17.96.060.B), circulation design (KMC §17.96.060.G), drainage (KMC §17.96.060.C), and utilities (KMC §17.96.060.C). City Department comments, which include analyses of standards pertaining to public improvements, such as the installation of sidewalk, drainage, and utilities, have been included as Attachment C. Additional analysis related to circulation design and traffic impacts is contained in the Exhibit 4 of Attachment E addressing the PUD CUP criteria and standards. The purpose of the Pre-Application Design

Ketchum Boutique Hotel Pre-Application Design Review (P19-061)

Reasoned Statement

July 29th & 30th, 2019 Special Meeting of the Planning & Zoning Commission

City of Ketchum Planning & Building Department

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Review Reasoned Statement is (1) to provide an analysis of Design Review standards related to the proposed architectural and landscape designs as well as the preliminary design concept's compatibility with the adjacent neighborhood and the character of Ketchum, (2) to document the Commission's feedback to the applicant regarding the design concept, and (3) to memorialize the Commission's motion to recommend the hotel project to final Design Review.

ANALYSIS

Architectural Design Review Standards (KMC §17.96.060.F)

While located in the Tourist (T) Zoning District, the hotel project is sited directly adjacent to the Community Core, Ketchum's distinct downtown area. While elements of the community's existing physical design speak to the town's mining history and ski town heritage, Ketchum does not have one exclusive design aesthetic or architectural vernacular. Rather, Ketchum has developed over time to form a unique blend of eclectic design styles that range from historic mining sheds to alpine lodges to modern residential development. As identified in the 2014 Comprehensive Plan, Ketchum strives to protect and enhance those elements of the natural and built environment that contribute to its unique mountain resort community (Comprehensive Plan, 23). Protecting and enhancing the visual quality of the community including downtown entryways, undeveloped hillsides, and streetscapes is a key component of the community's vision (Comprehensive Plan, 23).

With more modern design characteristics, the proposed hotel project will complement new development within the downtown area, such as the Argyros Performing Arts Center. The proposed design creates a more urban, pedestrian oriented environment serving to invite the public and hotel guests into the proposed common areas including the restaurant, lounge, meeting rooms, and rooftop bar. With features that enhance the streetscape within the southern gateway area, the hotel project will enhance the vibrancy of downtown with pedestrian oriented design features that will allow hotel guests, the resident workforce, and the public to access and enjoy all of the amenities offered in downtown Ketchum. Utilizing window openings, balconies, patios, and terraces, the proposed design celebrates Ketchum's natural surroundings, hillsides, and water resources by encouraging an interface between outdoor areas and interior space.

The Planning & Zoning Commission values preserving Ketchum's mountain town character through the gateway corridor. During review of the Pre-Application, the Commission expressed support for the new hotel project as enhancing the vibrancy of downtown Ketchum. As opposed to other permitted uses in the Tourist (T) Zoning District, such as multi-family residential development, the hotel use offers public amenities such as the restaurant, lounge, and rooftop bar that may be enjoyed by the community as well as hotel guests.

KMC §17.96.060.F.1: Building(s) shall provide unobstructed pedestrian access to the nearest sidewalk and the entryway shall be clearly defined.

The subject property is a corner lot with street frontage along River Street and State Highway 75/Main Street. As a substantial improvement, the hotel project is required to install sidewalks equal to the length of the subject property lines adjacent to River and Main Streets. All sidewalks shall be constructed to City standards contained in KMC §12.04.030.M as well as all applicable City right-of-way standards. The proposed elevations as included within Exhibit 2.01 (Attachment B) of the applicant submittal drawings indicate the preliminary design concept of the entryway to the hotel lobby from River Street, which includes a porte-cochere providing weather protection for pedestrians accessing the sidewalk. All sidewalks are proposed to include a snowmelt system, which serves to further activate the corner of River and Main Streets by enhancing the pedestrian streetscape. The proposed front façade has been designed with ground floor windows and glass doors, which invites the public and hotel guests into the hotel. The Commission expressed concern regarding the traffic circulation at intersection of River and Main Streets and requested that the Applicant incorporate appropriate treatments in order to enhance pedestrian safety at this intersection.

Ketchum Boutique Hotel Pre-Application Design Review (P19-061)

Reasoned Statement

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KMC §17.96.060.F.2: The building character shall be clearly defined by use of architectural features.

Congruous with the natural topography of the development parcel, the mass of the building steps up the sloping grade from the south corner of the parcel adjacent to Trail Creek to the front property line at River Street. The vertical mass at the east elevation fronting Main Street is broken up by variation in roof heights, material differentiation, and a combination of horizontal and vertical elements. Projecting balconies and canopy elements serve to articulate breaks within the vertical plane. The circulation corridor design at the center of building serves as a prominent architectural feature anchoring the hotel to the site. Characterized with window openings at the east elevation fronting Main Street, the fenestration proposed with the circulation corridor serves as a visual invitation welcoming the public inside the hotel in order to enjoy the rooftop bar, restaurant, and lounge. The rooftop bar overhang element echoes the porte-cochere and overhangs proposed at the corner of Main & River Streets linking the public amenities proposed with the project including the restaurant, dining area, and lounge.

The Commission expressed concern regarding the height of the prominent tiered patio feature at the corner Main and River Streets. This element of the hotel design combines canopy elements associated with the public outdoor dining areas with three floors of patios that provide terraced outdoor living areas for certain suites. The Commission noted that the subject tower feature may be too imposing in relation to the pedestrian interface at this intersection. During the public hearing, concern was expressed regarding the rooftop bar in relation to noise and light pollution. KMC §17.132.030.H requires all exterior lighting be full cutoff fixtures with the light source fully shielded. Fixtures shielded underneath canopies must be flush mounted or side shielded. KMC §9.08.040.8 enumerates standards for noise levels permitted in the nighttime, daytime, and evening. The Commission encouraged the applicant to consider treatments to mitigate the impact of noise and light emanating for the proposed rooftop bar.

KMC §17.96.060.F.3: There shall be continuity of materials, colors and signing within the project.

Exhibit 2.07 of the applicant submittal (Attachment B) includes an exterior color palate and schematic mood board. The applicant has derived inspiration from Ketchum's mountainous backdrop and surrounding natural splendor. The material concept plan is characterized by natural materials including stone, steel, and wood with fenestration to celebrate the outside environment within interior common areas and living space. The Commission requested the Applicant incorporate material differentiation at all facades and in particular at the west façade in order to mitigate the impact of the reduced side yard setback.

KMC §17.96.060.F.4: Accessory structures, fences, walls and landscape features within the project shall match or complement the principal building.

No accessory structures have been proposed with the hotel project. Preliminary landscape drawings are included in Exhibit 02.06 (Attachment B) of the applicant submittal package. The applicant has proposed a landscape buffer adjacent to State Highway 75 comprised of shade and conifer trees that soften the façade at the east elevation. The preliminary landscape design concept includes a planting hedge serving to buffer the outdoor dining area from the adjacent street intersection. During review of the Pre-Application, the Commission as well as the public doubted whether the landscape buffer would sufficiently mitigate the noise impacts associated with traffic at the intersection. The Commission requested that the applicant provide a more detailed landscape plan in order to soften the design of the building façade and mitigate noise pollution.

The Pre-Application Design Review submittal did not indicate the proposed design of any signs related to the hotel and associated operations. Pursuant to KMC §17.127.030.B.1, a complete master signage plan must be submitted at the time of Design Review application for the new hotel.

KMC §17.96.060.F.5: Building walls shall provide undulation/relief, thus reducing the appearance of bulk and flatness.

As the building form terraces down towards Trail Creek following the contours of the development parcel, the façade is distinguished by a combination of horizontal and vertical elements, such as the balcony railing and window openings, providing additional relief to the staggered facade. Horizontal elements terminate at the southernmost portion of the building, which is characterized by large vertical windows providing views of adjacent Trail Creek. The proposed staggered façade design at all elevations incorporates fenestration through window and door openings, varying roof heights, and material differentiation to provide relief the bulk and mass of the hotel.

The Commission expressed concern regarding the proposed bulk, mass, and height of the building and the associated impact to view corridors, appropriateness of the proposed scale of the hotel and compatibly with the surrounding neighborhoods, and safety hazards resulting from the accumulation of ice and snow due to shadowing of the adjacent street. In order to mitigate these impacts, the Commission encouraged the Applicant to open the mass of the building in order to accommodate solar access and decrease impacts to view corridors.

The Commission called for more consideration to the design of the west façade at the northwest corner of the building due to the reduced side yard setback. The Commission requested the Applicant provide more undulation and relief to the façade design including more steps and terraced elements. The Commission critiqued the design for its lack of modulation and monolithic appearance and encourage more undulation, stepping of the facades, and material differentiation.

KMC §17.96.060.F.6: Building(s) shall orient toward their primary street frontage.

The building orients to the primary street frontages adjacent to River and Main Streets. Through large window openings that span the hotel entrance, the front elevation adjacent to River Street, and the corner of the east facade fronting Main Street as well as canopy elements overhanging public sidewalks, the hotel incorporates human scale design features fostering a pedestrian friendly environment. The Commission encourage the Applicant to incorporate design features and treatments that balance the mass of the building particularly at the northeast and northwest corners in order to provide more undulation and relief.

KMC §17.96.060.F.7: Garbage storage areas and satellite receivers shall be screened from public view and located off alleys.

As the development site is not located adjacent to an alley, the proposed garbage storage area is located within the building adjacent to River Street. The trash/loading area as depicted on the Main Level Floor Plan indicated within Exhibit 02.01 (Attachment B) of the applicant submittal drawings is fully enclosed within the building and screened from public view.

KMC §17.96.060.F.8: Building design shall include weather protection which prevents water to drip or snow to slide on areas where pedestrians gather and circulate or onto adjacent properties.

The building design includes a prominent porte-cochere at the entrance to the hotel lobby and cantilevered overhangs projecting over the public sidewalk, which will serve as weather protection and activate the corner of River and Main Streets with pedestrian oriented design. The project design incorporates a flat roof. The final Design Review application requires a roof plan indicating the setback of all proposed non-habitable structures and mechanical and electrical equipment affixed to the roof from the building façade.

Landscaping Design Review Standards (KMC §17.96.060.I)

Pursuant to KMC §17.96.060.I, landscaping shall serve to enhance and complement the neighborhood and townscape as well as provide a buffer between land uses. The preliminary landscape design concept

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incorporates a landscape buffer separating the hotel Main Street comprised of shade and conifer trees. Street trees are proposed to be incorporated into the streetscape design adjacent to River Street. All proposed street trees will require electrical outlets and irrigation and must maintain a minimum of 6 ft of clearance within the public sidewalk. Materials and vegetation types are required to be readily adaptable to the site's microclimate, soil conditions, and orientation. All trees, grasses, and perennials must be drought tolerant and native species are recommended.

Due to the requested waiver to reduce the side setback to 5 ft from the west interior property line, the Commission requested the applicant enhance the landscape design in order to provide a buffer between the hotel and the adjacent office buildings located at 220 E River Streets. The Commission called for the proposed landscape design to include a comprehensive strategy for the riparian area including a point of access for the public to access Trail Creek. The Commission noted the landscape plan should provide details for the proposed rooftop garden. The final landscape plan submitted with the final Design Review application shall be reviewed and approved by the City Arborist.

Public Amenities (KMC §17.96.060.J)

Public amenities are required to be provided wherever sidewalks are installed. Amenities may include, but are not limited to, benches and other seating, kiosks, bus shelters, trash receptacles, or art. The applicant is encouraged to propose public amenities that will enhance the human scale, pedestrian friendly design of the proposed hotel and that contribute to a high-quality public streetscape. All public amenities proposed to be installed shall meet all applicable City right-of-way standards and receive approval from the City Engineer and Streets Department.

CONCLUSIONS OF LAW

1. The City of Ketchum is a municipal corporation organized under Article XII of the Idaho Constitution and the laws of the State of Idaho, Title 50, Idaho Code.
2. Under Chapter 65, Title 67 of the Idaho Code, the City has passed a land use ordinance, Title 17.
3. The Commission has the authority to consider the Pre-Application Design Review in order to exchange ideas and give direction to the applicant on the design concept of a project pursuant to Chapter 17.96 of Ketchum Municipal Code Title 17.
4. The City of Ketchum Planning Department provided adequate notice of the time, place and summary of the applicant's proposal to be heard by the Commission for review of this application.

MOTION TO ADVANCE TO FINAL DESIGN REVIEW

THEREFORE, the Ketchum Planning & Zoning Commission moves to advance the hotel project to final Design Review provided that the Applicant specifically address the following through the final Design Review submittal:

- The Landscape Plan shall provide a comprehensive strategy for the riparian area including public access to Trail Creek, provide a substantial buffer between land uses and particularly between the adjacent office buildings to the west, provide relief and soften the design of the building façade, and mitigate noise pollution;
- The submittal shall specifically address traffic, circulation, and pedestrian safety as well as associated mitigation measures and remedies;
- In order to soften the hotel's impact to the adjacent property located at 220 E River Street, the Applicant shall incorporate more undulation, relief, and material differentiation at the west façade;

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- A detailed plan addressing lighting, noise, and signage shall be submitted for review with the final Design Review submittal;
- The submittal shall address sustainability measures related to building design and hotel operations as well as incorporate above code green standards; and
- The Applicant shall meaningfully address all feedback provided by the Planning & Zoning Commission, including critiques related to the bulk and mass of the building, the tiered outdoor terrace architectural feature at the northeast corner, and solar access with the final Design Review submittal package.

Reasoned Statement **adopted** this 12th day of August 2019.



Neil Morrow
Chair
Ketchum Planning & Zoning Commission

Attachment E.4 –PUD Conditional Use Permit and CUP

**BEFORE THE PLANNING AND ZONING COMMISSION
OF THE
CITY OF KETCHUM**

IN RE:	)	FILE NO. P19-063
)	
PEG KETCHUM HOTEL, LLC	)	FINDINGS OF FACT,
	)	CONCLUSIONS OF LAW, ORDER
Applicant for	)	OF DECISION AND
Planned Unit Development	)	RECOMMENDATION TO THE
Conditional Use Permit	)	CITY COUNCIL
)	

THIS MATTER coming on regularly for hearing at a special meeting of the Planning and Zoning Commission of the City of Ketchum (the "Commission"), which hearing was convened by the Commission at 4:30 p.m. on July 28, 2019 pursuant to Ketchum City Code ("KMC") sections 16.08.110 and 17.116.040 and this hearing was then continued to and reconvened by the Commission at 5:30 p.m. July 29, 2019, and which hearing was again continued to and reconvened by the Commission at 5:30 p.m. August 12, 2019 for the receipt and consideration and approval by the Commission of these Findings of Fact, Conclusions of Law and Order of Decision and Recommendation to the City Council.

The members of the Commission, do hereby make and set forth the Record of Proceedings, Findings of Fact, Conclusions of Law, and Order of Decision and Recommendation to the City Council as follows:

I. RECORD OF PROCEEDINGS

The record of the proceedings of the above-referenced matter consists of the following,

to-wit:

1.1 Exhibits Admitted into the Record from the Applicant, including Owner's Project Master Plan and Submittal Documents as follows:

- 1.1.1. Subject Real Property Legal Description**
- 1.1.2. Narrative 01.01 - Project Description**
- 1.1.3. Narrative 01.02 - Development Plan**
- 1.1.4. Narrative 01.03 – Project Analysis | Social Impact Study**
- 1.1.5. Narrative 01.05 – Development Schedule**
- 1.1.6. Narrative 01.06 – Hales Parking & Traffic Analyses**
- 1.1.7. Narrative 01.07 – Employee Housing Plan**
- 1.1.8. Narrative 01.08 – Subarea Analysis**
- 1.1.9. Narrative 01.09 – Contextual Hotel Components**
- 1.1.10. Narrative 01.11 – Standards Evaluation**
- 1.1.11. Exhibit 02.01 – Plans, Elevations & Sections**
- 1.1.12. Exhibit 02.02 – FAR calculations, plans, and sections**
- 1.1.13. Exhibit 02.03 – Height Analyses**
- 1.1.14. Exhibit 02.04 – Sun Studies**
- 1.1.15. Exhibit 02.05 – Civil Drawings C1.0 - C4.0**
- 1.1.16. Exhibit 02.06 – Landscape Drawings sheet L1.0**
- 1.1.17. Exhibit 02.07 – Exterior Color Palate**
- 1.1.18. Exhibit 02.08 – Dark Sky Compliant Fixtures**
- 1.1.19. Exhibit 02.09 – Traffic Study Diagrams**
- 1.1.20. Exhibit 02.10 – Plat Map | Lot Line Adjustment**
- 1.1.21. Exhibit 02.11 – ROW Improvements**
- 1.1.22. Exhibit 02.12 – Soils Report | Perc Test**
- 1.1.23. Exhibit 02.13 – Flood Plain | Riparian Corridor**
- 1.1.24. Exhibit 02.14 – List of Adjacent Property**
- 1.1.25. Exhibit 02.15 – Waiver List**
- 1.1.26. Galena Staking Exhibit**
- 1.1.27. AJC Architecture Virtual Reality Images**
- 1.1.28. AJC Architecture PowerPoint Presentations**
- 1.1.29. Hales Engineering PowerPoint Presentation**

1.2 Hearings:

- 1.2.1 The public hearing was commenced by the Commission, at 4:30 p.m. on July 28, 2019 followed with a site view of PEG Ketchum Hotel, LLC's (the "Applicant") subject real property of the above entitled Application located at 280 E. River Street Ketchum, Idaho and which hearing on said day was continued in City**

Council chambers at Ketchum City Hall 480 East Avenue North, Ketchum, Idaho for the receipt of testimony and comment from City staff, the Applicant and the public and which hearing was then continued to July 29, 2019 and reconvened at 5:30 p.m. a.m. at City Council chambers at City Hall for the continued receipt of testimony and comment from City staff, the Applicant and the public and for deliberation by the Commissioners and which hearing was again continued to August 12, 2019 and reconvened at 5:30 p.m. for the receipt and consideration and approval of these Findings of Fact, Conclusions of Law and Order of Decision and Recommendation to the City Council.

1.3 Witness Testimony:

1.3.1 Those who testified at the July 29, 2019 hearing are as follows, to-wit:

1.3.1.1 **City Staff including:** John Gaeddert, Brittany Skelton, Abby Rivin and William Gigray.

1.3.1.2 **Applicant including:** Nick Blayden, Justin Heppler, Sean Flynn, and Scott Johnson

1.3.1.3 **Public including:** Ed O’Gara, Ben Worst, Mark Pynn, Lars Guy, Bruce Hinkley, Bob Korb, John Salberg, Robert Adolph, and Susan Sahlberg

1.3.2 Those who testified at the July 30, 2019 hearing are as follows, to-wit:

1.3.2.1 **City Staff including:** John Gaeddert, Brittany Skelton, Abby Rivin and William Gigray.

1.3.2.2 **Applicant including:** Nick Blayden, Justin Heppler, and Scott Johnson.

1.3.2.2 **Public including:** Jima Rice, Pat Duggan, Ben Worst, Bruce Hinkley, Jerry Seiffert, Lars Guy. Lane Thompson, Mark Pynn, Harry Griffith, and Susan Sahlberg

1.3.3 No other witnesses appeared at the hearing on July 29, 2019 and July 30, 2019, which was then closed for the receipt of evidence and testimony by the Planning and Zoning Commission who then after duly reviewing testimony and exhibits, including written public and agency comment as set forth in **Attachments C and D**, proceeded to then deliberate.

DECISION

WHEREUPON THE COMMISSIONERS being duly informed and having reviewed the record, evidence, and testimony received and being fully advised in the premises, DO HEREBY MAKE THE FOLLOWING FINDINGS OF FACT, CONCLUSIONS OF LAW, AND ORDER OF DECISION AND RECOMMENDATION TO THE CITY COUNCIL, to-wit:

II. FINDINGS OF FACT

2.1 Findings Regarding Notice:

2.1.1 Notice Required: Notice has been given in accordance with the Law as required by KMC Sections 16.08.110 and 17.116.040.

2.1.2 Notice Provided:

2.1.2.1 Notice was published for the July 28, 2019 public hearing in the *Idaho Mountain Express*, the official newspaper, which has general circulation within the boundaries of the City of Ketchum.

<i>Newspaper</i>	<i>Date Published</i>
Idaho Mountain Express	Wednesday, July 17, 2019

2.1.2.2 Notice of the July 29-30, 2019 hearing was mailed on Friday, July 12, 2019 to the property owners within 300 feet of the subject real property and affected Agencies and was posted on the subject property on Friday, July 19, 2019.

2.2 Findings Regarding Applications Filed:

2.2.1 PEG Ketchum Hotel, LLC has submitted an Application for a Planned Unit Development of a Master Plan for a hotel development on a 1.08-acre site located at the southwest corner of the State Highway 75 and River Street intersection at 280 E. River, (the "Project Site").

2.2.2 The Application is subject to the City Planned Unit Development and Conditional Use Permit requirements as set forth in Titles 16 and 17 of the Ketchum Municipal Code (KMC).

2.2.3 KMC §16.08.020.B states:

The provisions for planned unit developments contained in this chapter are intended to encourage the total planning of developments. In order to provide the flexibility necessary to achieve the purposes of this chapter, specified uses may be permitted subject to the granting of a conditional use permit. Because of their unusual or special characteristics, PUD conditional uses require review and evaluation so that they may be located properly with respect to the purposes of this chapter, the comprehensive plan, and all other applicable ordinances, and with respect to their effects on surrounding properties and the community at large. In the event of conflict between this PUD chapter and any other ordinance of the city, this PUD chapter shall control. The review process prescribed in this chapter is intended to assure compatibility and harmonious development between conditional uses and surrounding properties and the city at large.

2.2.4 Applicant has submitted a Master Plan as part of its Planned Unit Development application, pursuant to Title 16, Chapter 16.08. Subject Master Plan (also herein referred to as the “Project”) includes a request for waiver or deferral of requirements (KMC §16.08.070.F).

2.2.5 As set forth in the Applicant’s PUD Application and Master Plan, waivers are requested to the following dimensional standards: Floor Area Ratio (FAR), front and side yard setbacks, and height and four-story requirements. Additionally, a waiver is requested for the PUD to occur on a Project Site with a minimum lot size of less than three (3) acres, which is permissible subject to stipulations set forth in KMC §16.08.080.A.

2.2.6 *Minimum Lot Size.* The Commission may waive the three (3) acre minimum lot size requirement consistent with KMC §16.08.080.A.4 as allowed for hotels. To do so, the Commission must find the Project meets the definition of hotel as set forth in KMC §17.08.020 and complies with the purpose of the Tourist zone as set forth in KMC §17.180 by providing the opportunity for tourist use. Additional relevant analysis is consistency of the Project with the Subarea Analysis and Gateway Study Excerpts, as set forth in Attachment A.

2.2.7 *Waiver.* Title 16, Chapter 16.04.020 defines Waiver as a:

Modification of a relevant provision and regulation of this chapter not contrary to public interest or public health, safety or welfare, and due to physical characteristics of the

particular parcel of land and not the result of actions of the subdivision where literal enforcement of this chapter would result in undue hardship. The granting of waiver(s) ... rests with the sound discretion of the commission and council, on a case by case basis.

Similarly stated relevant standards for the analysis of waiver requests are set forth in KMC §16.08.070.L and KMC §16.04.120.

- 2.2.8. *Four (4) waivers are submitted for the Project.*** These include waivers to minimum lot size, setback (front and side yards), height, and floor area ratio (FAR) limitations. These waivers were requested by the Applicant consistent with KMC §16.04.120, §16.08.080 and §17.124.050, in part, as the literal enforcement of city code in the context of the special physical characteristics and conditions affecting the property would result in undue hardship. In particular, the Hotel site has a large slope with a grade differential of approximately thirty-seven feet (37') from Trail Creek at the south end of the lot to the north end along River Street. The site is constrained by the river to the south and the City desires to setback structures from riparian and flood areas. The City also desires to setback structures from State Highway 75 (SH75) in this location to help preserve the entry to town and minimize shading of the highway during winter months. Further, the grade along SH75, future Idaho Transportation Department (ITD) bridge and highway expansion plans, and a desire for no access onto SH75 in this location create unique conditions for development.
- 2.2.9 *Floor Area Ratio.*** KMC §17.124.050.A states: "Hotels may exceed the maximum floor area ... requirements of this title subject to ... [a] Planned Unit Development ... which specifically outlines the waivers to bulk regulations requested." A subarea analysis is also required in the review process (KMC §17.124.050.A.2). The total developed gross floor area of the Project, as defined in KMC §17.08.020, is proposed to not exceed a FAR of 1.9 or approximately 88,917 gross floor area exclusive of basement areas and underground parking as shown in **Attachment B**. Total building area when each of the three (3) basement and parking garage levels and four (4) hotel stories are calculated in aggregate, as set forth in the PUD Findings and **Attachment B**, total approximately 135,915 square feet for the Project.
- 2.2.10 *Height.*** KMC§17.124.050.A states: "Hotels may exceed the ... height ... requirements of this title subject to ... [a] Planned Unit Development ... which specifically outlines the waivers to bulk regulations requested." A subarea analysis is also required in the review process (KMC §17.124.050.A.2). As noted, the Project Site has a large slope from Trail Creek at the south end of the lot to the north end along River Street. The hotel is proposed as a four-story structure on River Street that then stair steps and terraces down to three floors near Trail Creek. As depicted in the **Attachment B** Height Analysis, the maximum height of

the building along River Street does not exceed forty-seven feet (47') and the building scales down to forty-one feet (41') closer to the river on the south end of the property. The exception to this height analysis is in the center of the structure where 4-stories of hotel use are sandwiched between two public amenities (employee housing and a roof top bar for the public). At this more central site location, the existing grade drops at a fairly acute angle resulting in a portion of the building having a maximum height of seventy-two feet (72') as measured from existing grade (and a maximum of seventy-five feet based on the KMC definition of building height). In comparison to both the built Limelight hotel and approved Bariteau hotel on opposing corners, the height of the proposed Project will be lower and more closely align to the fourth-floor elevation of each of these buildings.

2.2.11 *Setback.* KMC§17.124.050.A, subsections 1 and 2, specifies that a PUD and Subarea Analysis process shall be used in the granting of waivers to bulk regulations for hotels. KMC §17.12.040 sets forth the following minimum front and rear yard setbacks: (A) Fifteen-foot (15') front setback on River Street; (B) One-foot (1') for every three-feet (3') of building height, but no less than ten-feet (10') for the west side setback; and (C) Twenty-five foot (25') to thirty-two foot (32') setbacks adjacent to State Highway 75 (SH75), as calculated based on the adjacent right of way width. The Project proposes a five-foot (5') front yard setback along River Street, a twenty-five-foot (25') setback along the SH75, and a five-foot (5') setback on the west-side of the building.

2.2.12 Details of the Project are set forth in **Attachment B** and include both narratives and maps. Narratives include a written project description, development plan, project analysis, social impact study, schedule, parking analysis, traffic study, employee housing plan, Subarea Analysis, and contextual hotel component analysis. Exhibit maps include plans, elevations, sections, sun study, height analysis, civil drawings, landscape plan, exterior color palate, dark sky compliant fixtures, traffic study diagrams, plat map, and public way improvements. Also provided are application forms, analyses of code compliance, soils report, and a waiver list.

2.3 Findings Regarding the Application for the Planned Unit Development Conditional Use Permit: The Planning and Zoning Commission having reviewed the Project Master Plan, as set forth in **Attachment B** to these Findings , as well as public comment, staff analyses, and agency/peer review/department inputs as set forth in **Attachments A, C and D** to these Findings and makes the following findings and recommendations as set forth in Tables 1, 2, and 3 below to the Ketchum City Council regarding the Planned Unit Development Conditional Use Permit as follows:

Table 1: PUD Standards of Evaluation Compliance Analysis

Planned Unit Development (PUD)

EVALUATION STANDARDS: 16.08.080

The standards set forth in this section shall apply to review of all PUD conditional use permit applications. The standards shall be used to review and evaluate the proposal in comparison to the manner of development and effects of permitted uses and standard development allowed on the property in question. Modification or waiver from certain standard zoning and subdivision requirements may be permitted subject to such conditions, limitations and/or additional development standards, pursuant to section 16.08.130 of this chapter, as the city council may prescribe to mitigate adverse impact at the proposed planned unit development, or to further the land use policies of the city, or to ensure that the benefits derived from the development justify a departure from such regulations. Where the city council determines that conditions cannot be devised to achieve the objectives, and/or the standards contained in this chapter are not met, applications for conditional use permits shall be denied. The city council shall make findings that each of the following evaluation standards have been met. The evaluation standards are as follows:

Compliant			City Standard & Finding	
Yes	No	N/A	KMC Section	City Standard & Findings
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.A	<p>Minimum lot size of three (3) acres. All land within the development shall be contiguous except for intervening waterways. Parcels that are not contiguous due to intervening streets are discouraged. However, the commission and the council may consider lands that include intervening streets on a case by case basis. The commission may recommend waiver or deferral of the minimum lot size, and the council may grant such waiver or deferral only for projects which: ....4. For a hotel which meets the definition of "hotel" in section 17.08.020, "Terms Defined", of this code, and conforms to all other requirements of section 17.18.130, "Community Core District (CC)", or section 17.18.100, "Tourist District (T)", of this code. Waivers from the provisions of section 17.18.130 of this code may be granted for hotel uses only as outlined in section 17.124.040 of this code. Waivers from the provisions of section 17.18.100 of this code may be granted for hotel uses only as outlined in section 17.124.040 of this code.</p>
			<i>Finding</i>	<p><i>The Project Site is approximately 1.08 acres and does not meet the minimum standard of (3) acres for a PUD. However, as noted herein below, the Commission finds that this requirement may be waived consistent with KMC §16.08.080.A.4 as allowed for hotels. Specifically, this Project:</i></p> <p><i>(A) Meets the definition of hotel as set forth in KMC §17.08.020. The Project consists of one hundred (100) rooms, includes on site food and beverage service with kitchen facilities, common reservation and cleaning services, meeting room space, combined utilities, on site management and reception services, access to all sleeping rooms through an inside lobby supervised by a person in charge no less than eighteen (18) hours per day, and adequate on site recreational facilities. There are no other residential uses proposed in connection with the hotel operation.</i></p> <p><i>(B) Complies with the purpose of the Tourist zone as set forth in KMC §17.180 by providing the opportunity for tourist use. Consistent with the sub-area analysis and Gateway Study Excerpts, as set</i></p>

Compliant			City Standard & Finding	
Yes	No	N/A	KMC Section	City Standard & Findings
				<p>forth in Exhibit A, the Project is compatible both in design and use with the surrounding uses and development.</p> <p>and</p> <p>(C) Allows the granting of waivers for hotel-related Tourist District Floor Area Ratio (FAR), setback, and height dimensional standards as outlined in KMC §17.124.040. The Project proposes to exceed the 0.5 Tourist Zone permitted Gross FAR as set forth in KMC §17.124.040.A and may exceed its FAR maximum in accordance with the pertinent code provisions allowing for fourth floor hotel uses, as set forth in KMC §17.124.040.B.3 and by reference KMC §17.124.050. In accordance with the aforementioned and also precedent (e.g., entitled Bariteau hotel site at 300 E. River Street across SH75 was also approved as a PUD on an approximately 0.9 acre site and the Limelight was approved as a PUD on an approximately 1.08 acre site), the Commission finds this evaluation standard to have been met.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.B	The proposed project will not be detrimental to the present and permitted uses of surrounding areas.
			16.08.080.D	The development shall be in harmony with the surrounding area.
			<i>Finding</i>	<p>As set forth in the <u>Attachment A</u> Gateway Study and the <u>Attachment B</u> Subarea Analysis, the proposed hotel is both by design and use consistent with envisioned plans for the corner of SH75 and River Street. Further, the proposed hotel project is consistent with current Tourist Zoning District zoning allowances for hotels. Each of the attendant uses, including restaurant/bar, meeting rooms, and employee housing are also permitted in the Tourist Zoning District. The site was defined as Site 2 in the 2007 Gateway Scale and Massing Study and was identified as a priority urban infill site for potential hotel development. As such, the Property is in the Ketchum Urban Renewal District (KURA) Revenue Allocation Area. The Project Site borders three other hospitality focused uses. The Limelight Hotel is located directly across River Street to the North. The Best Western Hotel is located diagonally across Main Street from the Project Site. The Planned Future Hotel by Bariteau is located directly across Main Street to the East. The two closest properties to the West are commercial office buildings, (220 and 200 East River Street). The site is bordered by commercial uses to its North, East, and West and is separated from the multifamily residential uses to the South by Trail Creek. Accordingly, the Commission finds this Project will (A) not be detrimental to the present and permitted uses of surrounding areas and (B) be in harmony with the surrounding area.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.C	The proposed project will have a beneficial effect not normally achieved by standard subdivision development.
			<i>Finding</i>	<p>The proposed hotel will benefit the city in ways not normally achieved by standard subdivision. These include public access to the river and, open space in excess of thirty-five percent (35%), and access to a 3,000 square foot bar patio terrace, which features landscaping and</p>

Compliant			City Standard & Finding	
Yes	No	N/A	KMC Section	City Standard & Findings
				<p><i>solar exposure unique for a built project. The Project will have significant economic and public amenity benefits to Ketchum that would not be achievable on this site without the PUD process due to the constraints created by the topography of the site (37' differential in grade between front property line on River and rear property line along Trail Creek), access constraints on the east side due to the east side bordering SH 75, and development constraints due to the south side of the property being Trail Creek. The provision of waivers through the PUD process allows the design of the building, interior layout, operations and programmatic aspects of the hotel to infuse economic and public benefits beyond what would be accomplished by hotel rooms alone.</i></p> <p><i>Economic benefits of the development include local option taxes generated by the 100 new hotel rooms that will be booked through the international reach of the internationally recognized hotel brand's reservation network. The hotel will feature a number of public amenities, including a street front restaurant and lounge, banquet/meeting rooms, and a roof top bar with panoramic views of Bald Mountain and Dollar Mountain; there is no other publicly accessible rooftop space in Ketchum city limits with a similarly large footprint (approximately 1,672 net square feet of roof-top Bar Patio on Level 04 and 3,008 net square feet of roof-top Bar Terrace on Level 03) or that has 280-degree views and is operational in all four seasons. The hotel will also provide on-site employee housing, with a minimum of 30 beds, in a mix of traditional apartments and dorm style apartment units.</i></p> <p><i>The hotel's inclusion of on-site employee housing will result in the project providing more on-site for employees than any other development in Ketchum city limits and the mix of housing unit styles (12 suites and 2 bunk rooms) will accommodate employees at different life stages and career stages (seasonal vs. long-term, full-time). Further, although the employee units are located on Lower Level 1 and Lower Level 2, which are partially below grade on the River Street portion of the building, because the grade of the site drops toward the south all employee units are above grade.</i></p> <p><i>If the rooftop bar and lower floor employee housing units were removed from the project (or if the employee housing were located in a basement) the benefits of this project to the community would be lessened; the employee housing and roof-top amenity comprise approximately 20,000 square feet of the approximately 140,000 gross square foot development. Due to the site constraints, the allowance for waivers from the typical standards of the code is what makes inclusion of these public benefits truly benefits, and is what makes these benefits possible.</i></p>

Compliant			City Standard & Finding	
Yes	No	N/A	KMC Section	City Standard & Findings
				<i>Accordingly, the Commission finds the PUD process as having a beneficial effect not normally achieved by standard development.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.D	The development shall be in harmony with the surrounding area.
			<i>Finding</i>	<i>The Commission finds this Project to be in harmony with the surrounding area. Details of this finding are presented jointly with KMC §16.08.080.B (above).</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	16.08.080.E	<p>1. Densities and uses may be transferred between zoning districts within a PUD as permitted under this chapter, provided, the aggregate overall allowable density of units and uses shall be no greater than that allowed in the zoning district or districts in which the development is located. Notwithstanding the above, the commission may recommend waiver or deferral of the maximum density and the council may grant additional density above the aggregate overall allowable density only for projects which construct community or employee housing and which:</p> <ul style="list-style-type: none"> a. Include a minimum of thirty percent (30%) of community or employee housing, as defined in section 16.08.030 of this chapter; and b. Guarantee the use, rental prices or maximum resale prices thereof based upon a method proposed by the applicant and approved by the Blaine County housing authority and/or the Ketchum city council. <p>2. Application for waiver or deferral of this criteria shall include a description of the proposed community or employee housing and the proposed guarantee for the use, rental cost or resale cost.</p>
			<i>Finding</i>	<i>N/A. The Applicant is not requesting any density transfers.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.F	<p>The proposed vehicular and nonmotorized transportation system:</p> <ul style="list-style-type: none"> 1. Is adequate to carry anticipated traffic consistent with existing and future development of surrounding properties. 2. Will not generate vehicular traffic to cause undue congestion of the public street network within or outside the PUD. 3. Is designed to provide automotive and pedestrian safety and convenience. 4. Is designed to provide adequate removal, storage and deposition of snow. 5. Is designed so that traffic ingress and egress will have the least impact possible on adjacent residential uses.

Compliant			City Standard & Finding	
Yes	No	N/A	KMC Section	City Standard & Findings
				<p>This includes design of roadways and access to connect to arterial streets wherever possible, and design of ingress, egress and parking areas to have the least impact on surrounding uses.</p> <p>6. Includes the use of buffers or other physical separations to buffer vehicular movement from adjacent uses.</p> <p>7. Is designed so that roads are placed so that disturbance of natural features and existing vegetation is minimized.</p> <p>8. Includes trails and sidewalks that create an internal circulation system and connect to surrounding trails and walkways.</p>
			<i>Finding</i>	<p><i>Attachment B</i> includes documents from the Applicant that address how vehicular and pedestrian traffic will circulate in and around the proposed Project. Foremost of these exhibits is the detailed traffic impact study (TIS) prepared by Hales Engineering, which AECOM (on behalf of the city) has provided a peer analysis. <i>Attachment B</i> also includes River Street Public ROW Encroachment details that feature the Applicant's circulation plan, sidewalk improvements, and proposed snowmelt system for the Project. Finally, <i>Attachment A</i> includes two excerpts of professional studies. The first is an excerpt from the Idaho Transportation District (ITD) Record of Decision (ROD) and proposed Fiscal Year 2025 (FY25) road improvements to State Highway 75 (SH75) adjacent the property between the Trail Creek Bridge and River Street. These include a 3-lane urban section with curb, gutter and sidewalk. Importantly, the middle lane features a left turn lane for north bound traffic on SH75 that would permit adequate queuing and protected westbound (WB) turning movement onto River Street and the Project. The second excerpt is from Vitruvian and references a city-sponsored recommendation to upgrade the unsignalized crossing at SH75/River Street (north-side of intersection between Limelight Hotel and the Best Western) with a Rectangular Rapid Flashing Beacon (RRFB) to enhance pedestrian safety. Also proposed to improve vehicular level of service (LOS) movements is making east bound (EB) and WB River Street at the intersection with SH75 right turn only movements (signing and striping required). To further reduce traffic and to meet City sustainability goals, as expressed throughout the Ketchum Comprehensive Plan, the operation of the hotel will integrate strategies to reduce vehicular impact on Ketchum's streets from this Project. These include strategies such as a Guest Shuttle (airport and to local destinations), Employee Car Share Program, and Employee Transit Passes. As conditioned herein, the Commission finds this standard to have been met. The Project will be adequately served by necessary vehicular and nonmotorized transportation systems.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.G	The plan is in conformance with and promotes the purposes and

Compliant			City Standard & Finding	
Yes	No	N/A	KMC Section	City Standard & Findings
				<p>goals of the comprehensive plan, zoning ordinance, and other applicable ordinances of the city, and not in conflict with the public interest:</p> <ol style="list-style-type: none"> 1. Pursuant to subsection 16.08.070D of this chapter, all of the design review standards in chapter 17.96 of this code shall be carefully analyzed and considered. This includes detailed analysis of building bulk, undulation and other design elements. The site plan should be sensitive to the architecture and scale of the surrounding neighborhood. 2. The influence of the site design on the surrounding neighborhood, including relationship of the site plan with existing structures, streets, traffic flow and adjacent open spaces, shall be considered. 3. The site design should cluster units on the most developable and least visually sensitive portion of the site.
			<i>Finding</i>	<p><i>As previously set forth in the findings for KMC §16.08.080 subsections B, D and F (above), the Project as conditioned, will be adequately served by necessary vehicular and nonmotorized transportation systems and will be in harmony with the surrounding area. The Project will pay applicable fees, from Local Option Taxes for construction materials to applicable building permit fees and impact fees for such items as water and sewer connections. Pursuant to KMC §16.08.070.D, all of the design review standards set forth in KMC §17.96 are conditionally attached to the City's approval of the Planned Unit Development and are memorialized in the Project Development Agreement. Staff has analyzed and the Commission has found, as noted in Attachment A, Staff Analysis Project Compliance with the Ketchum Comprehensive Plan subsection, that the Project both conforms with and promotes the purposes and goals of the comprehensive plan. As noted in <u>Attachment B</u> and the Applicant's site design drawings, Project massing has been carefully designed with a four-story bench design on River Street that terraces down (southward) to follow the topography drop from River Street to Trail Creek. Subject terraces then become gathering spots for guests and the public to enjoy the outdoor and take in the scenic views from the hotel. As noted by the Applicant, "the massing also provides for a façade that steps in and out of plane, which is enhanced by a layer balconies and articulation of those forms. The building pulls back over 35' from Trail Creek and has minimal visual impact on Forest Service Park." The building footprint near the front property line is confined to a 5' setback off of the River Street frontage where it has an appropriate relationship to the sidewalk and street scape. The footprint is then pulled back to respect the riparian setback along Trail Creek to minimize the impact of the new building adjacent to a natural feature. Site landscape design has been designed to complement the bench</i></p>

Compliant			City Standard & Finding	
Yes	No	N/A	KMC Section	City Standard & Findings
				<i>topography and creek bank features of the site." As conditioned herein, this standard has been met.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.H	The development plan incorporates the site's significant natural features.
			<i>Finding</i>	<i>Three significant natural features are recognized by the Commission, including: the site's location on a bench; Trail Creek along the south property line; and, the 360 degree scenic views from the site including Bald Mountain and Dollar Mountain. The hotel has been designed to step down, following the bench topography, creating rooftop terraces and public spaces that take advantage of solar orientation and available views. The landscape plan includes pedestrian pathways for hotel guest and the public to access Trail Creek viewpoint areas set outside of the riparian zone setback. As conditioned herein, this standard has been met and the Master Plan is found to properly incorporate the site's significant natural features.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.I	Substantial buffer planting strips or other barriers are provided where no natural buffers exist.
			<i>Finding</i>	<i>As noted in Table 2 and KMC §17.12.040, 21,825 square feet of the property will remain open space, which is approximately forty-six percent (46%) of the 47,249 square foot site. The Project has greater than the required thirty-five percent (35%) minimum open space set forth in the KMC for the Tourist Zoning District. Three notable buffer strips that benefit the public are proposed. The first is the twenty-five foot (25') setback from SH75/Main Street that will be landscaped. Subject setback, as set forth in Attachment B, averages thirty-nine feet (39'). Portions of this area are proposed to include an outdoor dining patio toward the intersection of Main Street and River Street and will have landscape and architectural barriers such as raised planters, raised water features, and architecturally integrated railings separating the dining patio from the street. The second buffer is a twenty-five foot (25') Riparian Easement along with a ten foot (10') Utility Easement that combine to create a thirty-five foot (35') setback from the property line adjacent Trail Creek. The third design element includes the placement of a buffer landscape island between the hotel's Porte Cochere drive along River Street. Given the significant public amenities integrated into the hotel design and invitation of the public into the building, the Commission finds a favorable exchange to exist and recommends the Ketchum City Council look favorably on the proposed River Street encroachment permit request by the applicant. This design element is subject to a separate Encroachment Permit application that will be reviewed by Council concurrently with the PUD. As conditioned herein, this standard has been met.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.J	Each phase of such development shall contain all the necessary elements and improvements to exist independently from proposed future phases in a stable manner.
			<i>Finding</i>	<i>As set forth in Attachment B, the Applicant proposes to develop the Project in a single phase. To assure that that the development contains all the necessary elements and improvements to exist in a stable manner, the Commission finds this standard (KMC §16.08.080.J) to be</i>

Compliant			City Standard & Finding	
Yes	No	N/A	KMC Section	City Standard & Findings
				<i>met, provided as a conditioned of the issuance of any Building Permit for the construction of the Project that an appropriate project completion assurance (e.g., an irrevocable letter of credit on a bank acceptable to the City in an amount equal to 130% of engineering estimates of the Master Plan) and relevant fees, such as building permit and impact fees, be provided to the satisfaction of the Council.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.K	<p>Adequate and usable open space shall be provided. The applicant shall dedicate to the common use of the homeowners or to the public adequate open space in a configuration usable and convenient to the residents of the project. The amount of usable open space provided shall be greater than that which would be provided under the applicable aggregate lot coverage requirements for the zoning district or districts within the proposed project. Provision shall be made for adequate and continuing management of all open spaces and common facilities to ensure proper maintenance.</p> <p><i>As previously noted, 21,825 square feet of the property will remain open space, which is approximately forty-six percent (46%) of the 47,249 square foot site. Further, subject rooftop bar also includes patio space plus an additional 3,000 square feet of landscaped terrace area devoted to public use. The open space provided exceeds the requirement by more than eleven percent (11%), which is an amount "greater than that which would be provided under the applicable aggregate lot coverage requirements for the zoning district or districts within the proposed project." The Commission finds that subject open space is both adequate and useable and complemented by the Project's addition of the outdoor roof top bar space with adjacent living garden terrace, which is available to the public and managed and maintained by the Project.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.L	<p>Location of buildings, parking areas and common areas shall maximize privacy within the project and in relationship to adjacent properties and protect solar access to adjacent properties.</p> <p><i>The Commission has reviewed the Applicant's response to this standard of evaluation, including reference to its sun study and height analysis/compatibility view drawings as set forth in Attachment B, and generally concurs with the finding that "The Ketchum Boutique Hotel is configured along a northwest spine that has allowed for the building's mass to be pulled back from the roadway view corridor leading to Main Street. All onsite parking is contained below grade and will have no visual impact on the site. The hotel features an interior courtyard located on level 2 that faces south, the courtyard will be hotel's 'private' exterior amenities space that is reasonably shielded from the view of most adjacent properties. The hotel features many architectural balcony elements that serve to create another layer of structure between the guests and the exterior, enhancing a sense of a perimeter of privacy in those guest rooms. The Sun Studies provided ... demonstrate that the massing of the hotel will have very minimal shade impact on adjacent buildings, only during the December studies</i></p>

Compliant			City Standard & Finding	
Yes	No	N/A	KMC Section	City Standard & Findings
				<i>do any shadows from the hotel intrude appreciably on any adjacent properties, and in those cases the shadow impacts from [the PEG Ketchum] hotel are not any more intrusive than the affected buildings have on their adjacent neighbors." As conditioned herein, the Commission finds this standard to have been met.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.M	<p>Adequate recreational facilities and/or daycare shall be provided. Provision of adequate on site recreational facilities may not be required if it is found that the project is of insufficient size or density to warrant same and the occupant's needs for recreational facilities will be adequately provided by payment of a recreation fee in lieu of such facilities to the city for development of additional active park facilities. On site daycare may be considered to satisfy the adequate recreational facility requirement or may be required in addition to the recreational facilities requirement.</p> <p><i>Programmed recreation facilities within the Project, as depicted in <u>Attachment B</u>, include a 1,039 square foot fitness center and a 3,277 square foot outdoor terrace, including hot tubs. The Commission finds these on-site guest amenities to adequately meet the recreational needs appropriate to the scale of the Project. In addition, the Commission finds that the proposed use, inclusive of the employee housing units, does not warrant the provision of on-site daycare services.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.N	<p>There shall be special development objectives and special characteristics of the site or physical conditions that justify the granting of the PUD conditional use permit.</p> <p><i>As noted in the Gateway Study set forth in <u>Attachment A</u>, the City of Ketchum has established special development objectives for the four corners surrounding the intersection of River Street/SH75. The Commission has reviewed and analyzed this Study, visited the site, and recognizes subject Project Site is on a bench with approximately 37 feet of grade change and without the PUD process would unlikely be developable as a hotel as it would have to have one building along River Street, and a second building at the bottom of the hotel accessible via SH75 Street. This latter access is not desirable for site visibility and safe ingress/egress. Accordingly, the Commission finds there to be special development objectives and special characteristics of the site and its physical conditions that justify the granting of the PUD conditional use permit.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.O	<p>The development will be completed within a reasonable time.</p> <p><i>As set forth in the Applicant's submittal, as set forth in <u>Attachment B</u>, a Spring 2020 construction start and an Autumn 2021 opening are proposed. Similar to the Commission's finding on KMC §16.08.080.J, the Commission finds this standard is met; provided that a project completion assurance agreement is entered into between the Applicant and City Council for the Project prior to the issuance of any Building Permit for the construction of the Project.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.P	<p>Public services, facilities and utilities are adequate to serve the proposed project and anticipated development within the appropriate service areas.</p>

Compliant			City Standard & Finding	
Yes	No	N/A	KMC Section	City Standard & Findings
			Finding	<i>Street, water, sewer, and fire personnel have met with the Applicant and found that adequate city services are available to serve the Project. See Attachment C for details on various departmental reviews. The Applicant and the City have also met with ITD regarding the Project and, as conditioned herein, is requesting improvements be installed by the Applicant at the intersection of SH75/River Street as a condition of Certificate of Occupancy. Formally, this will occur upon acceptance by ITD of a specific Encroachment Permit application submitted by the Applicant in conjunction with city recommendations to ITD for approval. Prior to building permit issuance, the Applicant will need will-serve letters from other utility providers (gas, electric, waste and recycling). To date, no issues of service have been identified. The payment of impact, local option tax, and building permit fees pursuant to approved city schedules are required. The Commission finds this standard has been met. Subject to the conditions set forth herein, public services, facilities and utilities are adequate to serve the Project and anticipated development within this area.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	16.08.080.Q	The project complies with all applicable ordinances, rules and regulations of the city of Ketchum, Idaho, except as modified or waived pursuant to this section
			Finding	<i>KMC §16.08.080 Subsections G and Q both stipulate that the Project conform with and promote the purposes of applicable ordinances and not conflict with the public interest. This Project involves six (6) interrelated permits (floodplain, subdivision, design review, PUD, CUP, and a development agreement), as well as encroachment permits that will be required for SH75 from ITD and for River Street from the Ketchum City Council. Each of these eight (8) sets of approvals, as well as future compliance of Project construction drawings with other city regulations, such as Building, Fire, and Green Building Codes are required of the Applicant. As conditioned herein, the Commission finds that this Project complies with all applicable rules and regulations of the City. The Commission makes this finding in recognition of its previous finding in favor of waiving the three (3) acre minimum PUD eligibility criteria as detailed under KMC §16.08.080.A as allowed for hotels. Further, the Commission makes this finding in recognition of the following Table 1 dimensional standard and project waiver analysis for the proposed FAR, height/story, and setbacks proposed for the Project. Further, as noted in general finding #3 herein, Ketchum's planned unit development ordinance is intended to encourage the total planning of developments, provide flexibility, and work with unusual or special characteristics of the land or a development project. Notably, KMC §16.08.020.B states, "[i]n the event of conflict between this PUD chapter and any other ordinance of the city, this PUD chapter shall control."</i>

Table 2: Tourist Zoning District Dimensional Standards and Project Waiver Analysis

Compliance with Zoning Standards

Compliant			City Standard & Finding	
Yes	No	N/A	KMC Section	City Standard & Findings
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.040	Minimum Lot Area: 9,000 square feet minimum.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.040	Building Coverage
			Finding	<i>As set forth in Attachment B, 21,825 square feet of the property will remain open space, which is approximately forty-six percent (46%) of the 47,249 square foot site. The Project has greater than the required thirty-five percent (35%) minimum open space set forth in the KMC for the Tourist Zoning District. The Commission finds that this standard has been met.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.124.040	Permitted Gross Floor Area Ratio: 0.5 or greater for hotels
			Finding	<i>The Commission finds the Project meets the definition of hotel as set forth in KMC §17.08 and, as a consequence, is eligible to exceed listed FAR consistent with the Commission's previous finding within Table 1, KMC §17.08.080, subsections B and D. A FAR of 1.9 is proposed for the hotel, which incorporates employee housing and other public amenities within the Project. Significantly, the Commission has reviewed the Attachment B Subarea Analysis and two Attachment A documents - the Gateway Study and a Comparative Hotel PUD Summary Chart. The Commission finds the proposed hotel is both by design and use consistent with envisioned plans for the corner of SH75 and River Street. The FAR of the Project is significantly less than the CC-Limelight Hotel and Tourist Zone Bariteau Project – neither of which incorporate community housing on the hotel site. The Project Site was defined as Site 2 in the 2007 Gateway Scale and Massing Study and was identified as a priority urban infill site for potential hotel development. As such, the Property is in the Ketchum Urban Renewal District (KURA) Revenue Allocation Area. The allowance of a 1.9 FAR, as herein conditionally approved by the Commission, is warranted due to special development objectives and special characteristics of the site and its physical conditions. In reaching this finding, the Commission finds that the proposed FAR, as stipulated, will not be detrimental to the public welfare, health and safety nor injurious to property owners in the immediate area.</i>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>17.124.040</p> <p><i>Finding</i></p>	<p>Permitted Gross Floor Area Ratio: 0.5 or greater for hotels</p> <p><i>The Commission finds the Project meets the definition of hotel as set forth in KMC §17.08 and, as a consequence, is eligible to exceed listed FAR consistent with the Commission's previous finding within <u>Table L</u>, KMC §17.08.080, subsections B and D. A FAR of 1.9 is proposed for the hotel, which incorporates employee housing and other public amenities within the Project. Significantly, the Commission has reviewed the <u>Attachment B</u> Subarea Analysis and two <u>Attachment A</u> documents - the Gateway Study and a Comparative Hotel PUD Summary Chart. The Commission finds the proposed hotel is both by design and use consistent with envisioned plans for the corner of SH75 and River Street. The FAR of the Project is significantly less than the CC-Limelight Hotel and Tourist Zone Bariteau Project – neither of which incorporate community housing on the hotel site. The Project Site was defined as Site 2 in the 2007 Gateway Scale and Massing Study and was identified as a priority urban infill site for potential hotel development. As such, the Property is in the Ketchum Urban Renewal District (KURA) Revenue Allocation Area. The allowance of a 1.9 FAR, as herein conditionally approved by the Commission, is warranted due to special development objectives and special characteristics of the site and its physical conditions. In reaching this finding, the Commission finds that the proposed FAR, as stipulated, will not be detrimental to the public welfare, health and safety nor injurious to property owners in the immediate area.</i></p> <p><i>Subject to the approval of the PUD application with conditions as noted herein, the Commission finds that the Project FAR warrants a waiver and, as a result, complies with this provision of the Tourist Zoning District.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>17.12.040</p> <p><i>Finding</i></p>	<p>Building Height Maximum Permitted: 35' or greater for hotels</p>
				<p><i>The Project proposes to exceed the thirty-five foot (35') height limit, which is permissible subject to the city's fourth floor hotel use allowance in the Tourist Zoning District provisions, as set forth in KMC §17.124.040.B.3 and by reference KMC §17.124.050.A and B.6. Evidence in support of the Project height waiver up to seventy-two feet (72') from existing grade (and up to seventy-five feet as measured by the KMC definition of building height) and an interpretation that the "hotel" does not exceed four floors are as follows:</i></p> <p>(A) <i>The Project site has a large slope from Trail Creek at the south end of the lot to the north end along River Street. The hotel is proposed as a four-story structure on River Street, and step / terrace down to three floors along Trail Creek.</i></p> <p>(B) <i>The KMC does not specify the maximum height of a four-story building. Historic references in the KMC, as well as the top floor plate of the adjoining Limelight Hotel show the hotel fourth floor to equal approximately fifty-eight feet (58').</i></p>

				<p>(C) <i>Maximum height of the building shall not exceed forty-seven feet (47') along River Street or forty-one feet (41') closer to the river on the south end of the property, as depicted in the <u>Attachment B</u> Height Analysis.</i></p> <p>(D) <i>During the transition where the four-story building along River Street steps down approximately thirty feet (30') toward Trail Creek, the forty-seven foot (47') high 4-story building reads like 6-stories at seventy-two feet (72') high. This is permissible consistent with KMC §16.08.020.B and desirable as follows: first, the height of the building at subject central location is below the fifty-eight (58') 4-story horizontal plane established by precedent and with the top of the fourth floor at the adjacent Limelight; second, the Commission recognizes that in this central location of the structure, that the 4-stories of hotel use are sandwiched between two public amenities (employee housing and a roof top bar for the public). The unique characteristics of the site at this location, where the existing grade drops quickly in the center of the site, result in a portion of the building having a taller element of seventy-two feet (72') as measured from existing grade. The Commission finds this consistent with general finding #3 herein and KMC §16.08.020.B, "[i]n the event of conflict between this PUD chapter and any other ordinance of the city, this PUD chapter shall control."</i></p> <p>(E) <i>In comparison to both the Limelight and approved Bariteau hotels on opposing corners, the height of the proposed Boutique Hotel is lower and more closely aligned to the fourth floor of each building.</i></p> <p>(F) <i>The <u>Attachment A</u> Gateway Study and <u>Attachment B</u> Subarea Analysis indicate that the proposed hotel is both by design and use consistent with envisioned plans for the corner of SH75 and River Street.</i></p> <p><i>and,</i></p> <p>(G) <i>Further, the proposed hotel project is consistent with current Tourist Zoning District zoning allowances for hotels. Each of the attendant uses, including restaurant/bar, meeting rooms, and employee housing are also permitted in the Tourist Zoning District. The Project proposes a height waiver for hotels in the Tourist Zone District and, subject to approval of the PUD application with conditions as noted herein, complies with this zoning standard.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>17.125.030.H</p>	<p>Curb Cut Permitted: <i>A total of 35% of the linear footage of any street frontage can be devoted to access off street parking.</i></p>

			Finding	<i>There are no curb cuts proposed along State Highway 75. The new configuration results in less than thirty-five percent (< 35%) of the linear footage of street frontage devoted to access the off street parking within the parking garage.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.125.020.A.2 & 17.125.050	Parking Spaces <i>Off-street parking standards of this chapter apply to any new development and to any new established uses.</i>
			Finding	<i>As analyzed by staff and consistent with KMC §17.125 and the Applicant's parking study by Hales Engineering dated July 16, 2019 (see Attachment B), the Project has adequate parking for the proposed uses on the property. The project is parked with ninety (90) on-site garage parking spaces. Of the ninety (90) spaces provided for the Project not less than six (6) spaces are reserved for public use and fourteen (14) spaces are reserved for employee housing use. As conditioned herein, the Project complies with this standard.</i>

Table 3: Conditional Use Permit Standards Analysis

Conditional Use Requirements				
EVALUATION STANDARDS: 17.116.030				
A conditional use permit shall be granted by the commission only if the applicant demonstrates that:				
Compliant			Standards and Staff Conclusions	
Yes	No	N/A	KMC Section	City Standards and Findings
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.116.030(A)	The characteristics of the conditional use will not be unreasonably incompatible with the types of uses permitted in the applicable zoning district. <i>The proposed hotel and each of the attendant uses within the Project, including restaurant/bar, meeting rooms, and employee housing, are permitted uses in the Tourist Zoning District. The characteristics of the conditional use for the Planned Unit Development CUP and the waivers approved herein pursuant to KMC §17.124.050 are compatible with the types of uses permitted in the Tourist Zoning District. The Commission finds this standard of evaluation has been met.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.116.030(B)	The conditional use will not materially endanger the health, safety and welfare of the community. <i>The Project will be served with essential public services and facilities, an acceptable level of service for traffic operations and pedestrian safety as set forth in the applicable findings noted in Table 1. As conditioned herein, the Commission finds this standard has been met.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.116.030(C)	The conditional use is such that pedestrian and vehicular traffic associated with the use will not be hazardous or conflict with existing and anticipated traffic in the neighborhood. <i>The Commission found in <u>Table 1</u> KMC §16.08.080.F herein above that the Project will be adequately served by necessary vehicular and nonmotorized transportation systems. This finding was made after reviewing Attachment B, which includes documents from the Applicant that address how vehicular and pedestrian traffic will circulate in and around the proposed Project. Foremost of these exhibits is the detailed traffic impact study (TIS) prepared by Hales Engineering, which</i>

Conditional Use Requirements				
EVALUATION STANDARDS: 17.116.030				
A conditional use permit shall be granted by the commission only if the applicant demonstrates that:				
Compliant			Standards and Staff Conclusions	
Yes	No	N/A	KMC Section	City Standards and Findings
				<p><i>AECOM (on behalf of the city) has provided a peer analysis. Attachment B also includes River Street Public ROW Encroachment details that feature the Applicant's circulation plan, sidewalk improvements, and proposed snowmelt system for the Project. Finally, Attachment A includes two excerpts of professional studies. The first is an excerpt from the Idaho Transportation District (ITD) Record of Decision (ROD) and proposed Fiscal Year 2025 (FY25) road improvements to State Highway 75 (SH75) adjacent the property between the Trail Creek Bridge and River Street. As conditioned herein, the Commission finds this standard to have been met. In particular, three off-site mitigation measures that will be required as a condition of development, including:</i></p> <p><i>(A) Developer to accommodate a northbound left-turn lane plus taper at River/Main. The developer will need to coordinate with ITD to determine where the west edge of SH-75 will be and whether ITD will accept temporary paving. The developer would install sidewalk, curb and gutter to the city's standard. AECOM to follow up with whether the southbound approach should also have a left-turn lane. AECOM also to follow up with some references in the study to back this up. In addition, it's understood that this will help prevent queuing and also be a safety improvement.</i></p> <p><i>(B) Developer to install "right-turn only" signs on the eastbound and westbound approaches (City would likely pay for the cost on the westbound approach).</i></p> <p><i>and,</i></p> <p><i>(C) At the discretion of the Ketchum City Council, the developer shall install rectangular rapid flashing beacon (RRFB) on the crosswalk on the north leg. No crosswalk required on the south leg.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.116.030(D)	<p>The conditional use will be supported by adequate public facilities or services and will not adversely affect public services to the surrounding area or conditions can be established to mitigate adverse impacts.</p> <p><i>Finding</i> <i>Consistent with the findings made for KMC §17.116.03 subsections B and C, the Commission finds this standard to have been met.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.116.030(E)	<p>The conditional use is not in conflict with the policies of the Comprehensive Plan or the basic purposes of this Section.</p> <p><i>Finding</i> <i>The proposed conditional use is supported by the following goals and policies of the 2014 Comprehensive Plan. Specific findings and analysis</i></p>

Conditional Use Requirements				
EVALUATION STANDARDS: 17.116.030				
A conditional use permit shall be granted by the commission only if the applicant demonstrates that:				
Compliant			Standards and Staff Conclusions	
Yes	No	N/A	KMC Section	City Standards and Findings
				<i>are as set forth in Attachment A. As noted herein, the proposed conditional does not conflict with the policies of the Comprehensive Plan or the basic purposes of Chapter 17.116 Conditional Uses.</i>

2.4 Findings Regarding Applicant’s PUD Bulk Area Waivers:

- 2.4.1 The Applicant’s Project includes waivers to the floor area ratio, front and side yard setbacks, and height and four-story requirements and, subject to compliance by the Applicant with conditions as noted herein, the Project complies with each of the Tourist Zone dimensional standards for hotels.
- 2.4.2 The proposed Planned Unit Development and Conditional Use Permit Application meets the standards of approval under KMC Title 16 and Title 17, subject to conditions of approval.
- 2.4.3 The Project may exceed the maximum floor area, height, setback or minimum lot size requirements of Title 17 KMC, subject to a planned unit development having been prepared for the Project’s proposed hotel and subject to approval by the City Council which outlines the waivers to bulk regulations requested.
- 2.4.4 All height and bulk Project limitations shall be in accordance with Tourist District except those items waived as an incident of the PUD Development Plan approval. The approved Project plans illustrate areas where buildings may exceed height and bulk limitations. As conditioned herein, the Commission refers to the zoning and subdivision waivers set forth in these PUD Findings.

**III.
CONCLUSIONS OF LAW**

The following are the legal principles that provide the basis for the Planning and Zoning Commissioners’ decisions and recommendations to the City Council which the Commissioners have applied to the facts presented at the hearing of the above entitled matter:

- 3.1 The City is a municipal corporation established in accordance with Article XII of the Constitution of the State of Idaho and Title 50 Idaho Code and is required and has exercised its authority pursuant to the Local Land Use Planning Act codified at Chapter

65 of Title 67 Idaho Code and pursuant to Chapters 3, 9 and 13 of Title 50 Idaho Code to enact the Ordinances and regulations which Ordinances codified in the Ketchum City Code (“KMC”) which are identified in Section II of Findings of Fact and which are herein restated as Conclusions of Law by this reference and which City Ordinances govern the Applicant’s Project Applications for the Development and use of the Project Site.

- 3.2** KMC section 16.08.120 C provides that prior to final approval of a PUD conditional use permit, the City Council may require a written agreement executed by the Applicant to secure performance of any requirement or condition to be imposed as part of the approval, including, but not limited to Development and may also require recordation of documents establishing and guaranteeing the operation and maintenance of the Project; and
- 3.3** The Project Applications, which includes waivers to the floor area ratio, front and side yard setbacks, and height and four-story requirements is governed under KMC Sections 16.08.020 B, 16.08.030, 16.08.040, 16.08.070, 16.08.080 and 17.124.050 are reviewed and considered by the Commission in accordance with the following:
- 3.3.1** In the event of a conflict Chapter 8 of Title 16 KMC controls over any other City ordinance; and
- 3.3.2** A planned unit development involves a development of land in which the standard land use regulations of the City may be modified or waived in order to promote beneficial development of an entire tract of land in conformance with an approved planned unit development conditional use permit accentuating usable open space, recreational uses, public amenities, community housing, and harmonious development with surrounding properties and the city at large; and
- 3.3.3** Any person wishing to develop a planned unit development shall comply with the requirements of chapter 8 of Title 16 KMC in addition to the zoning, subdivision and other applicable laws, ordinances, regulations and rules, subject to any modification or waiver granted as part of the planned unit development (PUD) conditional use permit; and
- 3.3.4** The Planning and Zoning Commission can make recommendations and the City Council has authority to grant waivers or deferrals of any of the requirements of sections 16.08.070 and 16.08.080 KMC on a case-by-case basis when the waiver or deferral will not be detrimental to the public welfare, health and safety nor injurious to property owners in the immediate area; and
- 3.3.5** The proposed Planned Unit Development and Conditional Use Permit meets the standards of approval under Title 16 and Title 17 KMC, subject to conditions of approval.

**IV.
DECISION and RECOMMENDATION TO CITY COUNCIL**

The Planning and Zoning Commissioners, having reviewed the above-entitled record, having listened to the arguments and presentations at the hearing and, being fully informed in the premises and further based upon the Findings of Fact and Conclusions of Law hereinabove set forth, **DO HEREBY DECIDE AND RECOMMEND TO THE CITY COUNCIL** approval of PEG Ketchum Hotel, LLC PUD Project Master Plan the Project as set forth in **Attachment B** subject to the following terms and conditions of approval:

- 4.1 Emergency Services Conditions:** The following are emergency services and safety terms and conditions:
- 4.1.1 Completion of Fire Improvements.** The City Building Official or the City Fire Marshal may withhold building and/or fire inspection approval for any phase of construction until all necessary components of the water and/or fire alarm system sufficient to provide protection for that portion of the Project are complete.
- 4.1.2 Fire Access During Construction.** Vehicle parking and material storage during Project construction shall not restrict or obstruct public streets or access to any building. Emergency vehicle access shall be maintained as required by the Fire Chief. Once construction begins on the second floor and above, 26-foot aerial ladder truck access is required along one entire side of the building, in a location approved by the Fire Marshal, for evacuation of injured persons from upper floors. All required Fire Lanes, including within 15 feet of fire hydrants, shall be maintained clear and unobstructed at all times.
- 4.1.3 Fire Code Requirements.** The Project shall comply with all the terms and conditions set forth in the Ketchum Fire Department Pre-application Requirements Memo dated June 24, 2019 from Tom Ancona, Assistant Chief & Fire Marshall, inclusive of subsequent amendments thereto, as well as all 2012 International Fire Code requirements and any additional specific City Building (Chapter 2 15.04 and 15.06) and Fire Ordinances (Chapter 15.08).
- 4.2 ROW Improvements Conditions:** The following ROW Improvements are required of the Applicant:
- 4.2.1 DIG.** The Applicant shall submit a Street and Alley Digging, Excavation, and Trenching (“DIG”) Permit application with an associated traffic control plan for

all construction work within the City ROW to be reviewed and approved by the City Streets Department.

4.2.2 TURP. The use of City right-of-way for construction which includes the closure of adjacent streets or sidewalks requires a Temporary Use of Right-of-Way Permit (“TURP”).

4.2.3 River Street Encroachment Permit Improvements. KMC §17.96.030.C states: “The City Council shall approval all permanent encroachments within the City-owned ROW associated with a development project.” Applicant has made application as a part of the Project to the City for a license to encroach into the River Street Public Right of Way (“River Street ROW”) for the following improvements: guest pick-up/drop-off, underground utilities, landscaping, street trees with decorative tree grates, public art, bike racks, sidewalks, pedestrian walkway lighting, and street lighting, and related improvements along River Street, pursuant to KMC §12.08.040. Further, the Applicant proposes that all walkway and driving surfaces within this encroachment area be hooked into its private boiler or similar snowmelt system within the Project mechanical room. The snow melt system is proposed to be operational every winter after the Hotel Operations begins operations. Final approval of the River Street ROW improvement plans is required and will is subject to review and approval by the Ketchum City Council through a separate encroachment agreement. If approved via separate City encroachment process, such order or decision on encroachment, including any and all conditions thereon, is hereby incorporated by reference and made a part of these findings.

4.2.4 SH75 Encroachment Permit. ITD has an approved Record of Decision (“ROD”) that includes a 3-lane section with a six-foot (6’) wide sidewalk abutting the Subject Real Property. Subject ITD improvements to the SH75 ROW are proposed to be installed by ITD in Fiscal Year 2025 with road work in the vicinity, at the earliest, occurring in October of 2025. The installation of these SH75 Improvements by ITD and, particularly, the striping of a dedicated left turn lane West Bound onto River Street with adequate queuing for cars turning onto River Street is important to traffic flow both along SH75 and into the Project.

4.2.4.1 Given the Applicant’s timeframe for construction and Certificate of Occupancy, subject SH75 work will not be conducted by ITD prior to the Developed Project and the commencement of Hotel Operations opening. Given that the City finds that a center turn lane with adequate queuing of approximately fifty to one hundred feet (50’ – 100’) is necessary for the SH75/River Street intersection to retain its current Level of Service (“LOS”) for vehicular car movement, therefore the Applicant shall file with ITD an application for an encroachment permit. The Applicant and City shall work together to attain approval from ITD for the construction and striping of a partial center turn near the River Street intersection north

of the Trail Creek bridge. The Applicant shall pay for engineering, traffic control and construction costs for subject SH75 improvements adjacent to the Project.

4.2.4.2 Further, to avoid excessive delays for East Bound traffic on River Street, the Applicant shall work with the City and ITD to install appropriate signage and improvements to allow only a right turn onto southbound SH75 at this intersection.

4.2.4.3 Pedestrian Safety. To help assure pedestrian safety and consistent with KMC, at the discretion of the Ketchum City Council upon the recommendation of the Ketchum Transportation Authority and the city's peer review engineering firm (AECOM), the Applicant shall work with the City and ITD to upgrade the unsignalized SH75 and River Street crossing (on north-side) to include a rectangular rapid flashing beacon (RRFB) crosswalk similar to 4th Street. The circulation design shall meet all standards as specified in KMC §17.96.060.G.

4.2.5 Letter of Credit. The Applicant shall provide an irrevocable letter of credit to the City for the aforementioned ROW Improvements affecting both the SH75 and River Street ROWs.

4.3 Terrace Walls. Construction of terrace walls or features of the outdoor dining patio with landscaping and architectural features adjacent to SH75 may be subject to future design review at the time the application is filed for approval at the discretion of the Administrator.

4.4 Time Limits: The following are the time limits that govern this Project:

4.4.1 Pursuant to KMC §17.96.090, the Design Review Permit No. P 19-0 is valid for twelve (12) months from the date of final decision on the associated Findings of Fact, Conclusions of Law, and Decision. The Application for the Project building permit must be filed within the time as specified in KMC §17.96.090(A)(2). Any extension shall only be as allowed and specified in KMC §17.96.090.

4.4.2 Unless extended by the Ketchum City Council, a building permit shall be issued within twelve (12) months from the date of the last issued Permit.

4.4.3 A certificate of occupancy shall be issued for the Project no later than 18 months after the building permit is issued unless the time for completion of the Project is extended by the City Council.

4.5 Certificate of Occupancy: No Certificate of Occupancy shall be issued for the use and occupancy of this Project until the following items are complete:

- 4.5.1 All Design Review elements of the Project have been completed and approved by the Planning & Building Department; and
 - 4.5.2 All proposed encroachments within the City's River Street right-of-way have been installed in accordance with the Project Master Plan and approved by the City Engineer; and
 - 4.5.3 All rooftop mechanical and electrical equipment is fully screened from public vantage points and approved by the Planning & Building Department; and
 - 4.5.4 The City's Fire, Utilities, Building, Arborist, Streets, and Planning Departments have conducted final inspections and authorized issuance of Certificate of Occupancy; and
 - 4.5.5 Prior to Certificate of Occupancy, a Parking Plan verifying public use, validation processes for determining parking charges (if any) for the public and the six (6) displaced public parking spaces, and other details at the discretion of the City, shall be provided and approved by Ketchum City Council for the Project Parking Garage.
- 4.6 **City Permit Performance Fees:** The Applicant shall be charged and shall pay the City Permit Performance Fees for the administration of the City's performance of the *Permit Conditions Acceptance Development Agreement*.
- 4.7 **Conditions to Applicant's Obligations.** The Applicant's obligations hereunder are conditioned upon (1) receiving all the referenced approvals from the City and (2) securing financing as provided in the *Permit Conditions Acceptance Development Agreement*.
- 4.8 **Drainage.** Project Drainage system plans shall be submitted to the City Engineer for review and approval. Pursuant to KMC §17.96.060.C, all storm water shall be retained on site, drainage improvements constructed shall be equal to the length of the Subject Real Property boundary lines, and all drainage facilities shall be constructed per City standards. All drainage improvements shall meet the applicable design criteria as specified in KMC §12.04.030.
- 4.9 **Utilities Plan:** The Applicant shall submit a Project Utility Plan indicating the location and size of water and sewer mains as well as gas, electric, TV and phone services (KMC §17.96.040.C.2c & KMC §17.96.060.D.1-3). Per KMC §17.96.060.D.2, utilities shall be located underground and utility, power, and communications lines within the Project Site should be concealed from public view.
- 4.10 **Employee Housing Units.** The Applicant shall either maintain or enter into a master lease with the Hotel Operator for apartment units within the Developed Project

containing 30 beds and thereby fulfill and satisfy the employee housing obligation of this Project consistent with the KMC. All leased apartment units must be subleased, assigned or otherwise made available to employees of the Hotel Operator on terms and conditions determined by it in the exercise of its discretion. The Applicant may enter into a master lease with the Hotel Operator for apartment units containing thirty (30) beds and thereby fulfill and satisfy the employee housing obligation of this Project consistent with KMC §17.124.050. All leased apartment units must be subleased, assigned or otherwise made available to employees of the Hotel Operator on terms and conditions determined by it in the exercise of its discretion.

- 4.11 Hotel Operations.** The core feature of the Project is a hotel building operated at an industry acknowledged Four Star Hotel Operations Standard. Adherence to a Four-Star Hotel Operations Standard, particularly during Peak Travel Season, affects the sufficiency of on-site parking and traffic circulation in the immediate vicinity of the Project and is a requirement of the occupancy and use of the Developed Project.
- 4.12 Lower Parking Demand and Traffic Impacts.** To assure that the Applicant and/or Hotel Operator provides guest shuttle, employee shuttle, car share program, transit passes, carpool program, and alternative transportation (such as bike storage for employees), the Applicant and/or Hotel Operator shall include in the irrevocable letter of credit a Lower Parking Demand and Traffic Impacts amount of \$ [REDACTED] for a period of not greater than five (5) years upon which the City Council may request a draw to cover the City's costs in the mitigation of lowering traffic impacts and/or parking demands associated with the Applicant and/or Hotel Operator's failure to comply.
- 4.13 Parking & Loading.** The Applicant shall present a Project Parking Plan for review and consideration by the Commission as part of its full Design Review Submittal. Prior to Certificate of Occupancy, a Project Parking Plan verifying public use, validation processes for determining parking charges (if any) for the public and the six (6) displaced public parking spaces, and other details at the discretion of the City, shall be provided and approved by Ketchum City Council for the Project Parking Garage. The Project has a total of 90 parking spaces. Of those 90 spaces, 66 spaces are required for the Four-Star Hotel Operations Standard, 14 spaces are required for employee housing, 1 space is required for the restaurant/bar, and 6 are required for the public to mitigate displaced public parking spaces from River Street. Of this total, not less than:
- 4.13.1** Fourteen (14) stalls shall be allocated for employee housing, inclusive of at least one (1) car share vehicle; and
- 4.13.2** Six (6) underground parking stalls will be available to the general public at no charge to the public while using the conference center, spa, restaurant and bar. A validation system may be employed by the Applicant and/or Hotel Operator with regard to the monitoring of public use of the six, free-of-charge, underground public spaces located in the Project Parking Garage; and

- 4.13.2** Guest shuttle, employee shuttle, car share program, transit passes, and bike storage shall be provided as a part of the Four-Star Hotel Operations Standard.
- 4.13.3** During and upon completion of the construction of the Project, delivery vehicles associated with the Project shall not interfere with the regular flow of traffic surrounding the Project Site. Delivery vehicles shall not block the regular flow of traffic along River Street.
- 4.14 Local Option Tax.** The Project shall be subject to the provisions of KMC Section 3.12, relating to local option taxes.
- 4.14.1 Beverage, Food & Retail Sales.** All retail, food and beverage sales on the Project Site and in the Project shall be subject to the local option tax.
- 4.14.2 Building Materials.** The Project and Project Site shall be subject to the local option tax on building materials.
- 4.14.3 Employee Housing.** The obligation to pay local option tax shall not apply to the rental of employee housing units.
- 4.14.4 Future Amendments to LOT Ordinance.** Any amendments to or repeal of Ketchum's Local Option Tax Ordinance and/or Idaho law relating to such local option taxes shall also apply to and modify this Section to the extent of such amendment(s) and/or repeal.
- 4.14.5 Hotel Rooms.** All hotel rentals in the Project Four-Star Hotel Operations Standard shall be subject to the local option tax, regardless of who makes the reservation, including independent third-party travel agencies or other independent parties.
- 4.14.6 Short-term rentals.** All non-hotel rentals, if any, shall be subject to the local option tax on short-term rentals.
- 4.15 Waivers.** Setbacks, FAR, and height for the Project shall comply with final Design Review for the Project as approved by the City. The final plans once approved and integrated into the Permit Conditions Acceptance Development Agreement by the Ketchum City Council illustrate areas where buildings may exceed height and bulk limitations. As conditioned herein above, Ketchum acknowledges the zoning and subdivision waivers set forth in the PUD Findings.
- 4.16** All requirements of the Fire, Utility, Building, Planning and Public Works departments of the City of Ketchum shall be met. All public improvements shall meet the requirements of the Public Works Department.
- 4.17** All other provisions of Ketchum Municipal Code, Chapters 16 and 17 and all applicable ordinances rules and regulations of the City and other governmental entities having jurisdiction shall be complied with by the Project.

- 4.18 Building Permit Requirements.** The building permit for the Project shall not be issued until:
- 4.18.1** The Project is subject to completion assurances and a letter of credit, which shall be detailed by the City Attorney and Finance Director and approved by the Ketchum City Council as provided in the Permit Conditions Acceptance Development Agreement governing this Project; and
 - 4.18.2** The project shall pay the plan check and building permit fees that are in effect at the time of plan check and building permit submittal. Prior to issuance of a building permit, project impact fees (police, fire, parks and streets) and water and sewer connection fees shall be paid in an amount not too exceed \$ _____ in accordance with the methodology established in the KMC Chapter 15.12 and any subsequent changes made prior to issuance of a building permit.
 - 4.18.3** Storm Water Management Pollution Prevention Plan (“SWPPP”) in accordance with local, state and federal laws and regulations is in place for the Project; and
 - 4.18.4** A detailed Project Construction Staging and Mitigation Plan which is consistent with the standards specified in Chapter 15.06 of KMC, including provisions for off-site parking for contractors, sub-contractors, and other trades associated with the construction of the Project, off-site storage of bulk materials, and required right of way encroachments during construction, shall be submitted and approved by the City Planning and Building Administrator prior to building permit approval.
 - 4.18.5** The Applicant has secured a will serve letter from Idaho, Clear Creek Disposal and other applicable public and private utility providers prior to issuance of a Building Permit.
 - 4.18.6** The River Street Encroachment Permit encroachment agreement shall be obtained.
 - 4.18.7** The Applicant shall cause to be issued in irrevocable letter of credit for the aforementioned Public ROW Improvements affecting both the SH75 and River Street ROWs. The amount of the financial guarantee shall be at 150% of engineering estimates for the guaranteed improvements. Partial and/or full release(s) of the letter of credit may be made upon: (i) Acceptance of subject River Street ROW improvements by the City; (ii) formal commencement of work by ITD of the SH75 ROW improvements adjacent the Subject Real Property and/or upon complete installation of the SH75 ROW improvements adjacent the Property.
 - 4.18.8** **Written Permit Conditions Agreement:** The Applicant has entered into the *Permit Conditions Acceptance Development Agreement* consistent in form with **Attachment E.**

Findings of Fact **adopted** this 12th day of August 2019.



Neil Morrow, Chair
Ketchum Planning & Zoning Commission

Attachment E.5 - Development Agreement

8/12/19

**Recording Requested By and
When Recorded Return to:**

City of Ketchum
Attn: City Clerk
P.O. Box 2315
Ketchum, Idaho 83340

SPACE ABOVE THIS LINE FOR RECORDER'S USE ONLY

City of Ketchum / PEG Ketchum Hotel, LLC

Permits Conditions Acceptance Development Agreement

Parties to the Agreement:

City of Ketchum	“City”	City Hall P.O. Box 2315 480 East Ave. N. Ketchum, Idaho 83340
PEG Ketchum Hotel, LLC	“Developer”	Attn: Nick Blayden 180 N. University Avenue, #200 Provo, Utah 84601
PEG Ketchum Hotel, LLC	“Owner”	Attn: Nick Blayden 180 N. University Avenue, #200 Provo, Utah 84601

8/12/19

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[insert commencing with final drafts]

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2. DESIGN REVIEW ORDINANCE
3. FLOODPLAIN MANAGEMENT OVERLAY ZONING ORDINANCE
4. SUBDIVISION ORDINANCE / LOT LINE SHIFT
5. PLANNED UNIT DEVELOPMENT ORDINANCE
6. CONDITIONAL USE PERMIT ORDINANCE
7. OFF STREET PARKING AND LOADING
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EXHIBIT B: PROJECT MASTER PLAN / OWNER SUBMITTAL DOCUMENTS:

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8/12/19

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- 2. PLAT / READJUSTMENT OF LOT LINES**
- 3. PRE-APPLICATION DESIGN REVIEW REASONED STATEMENT
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- 4. PUD CONDITIONAL USE PERMIT AND CUP**
- 5. DEVELOPMENT AGREEMENT**

Permits Conditions Acceptance Development Agreement

City of Ketchum/PEG Ketchum Hotel, LLC

THIS Permits Conditions Acceptance Development Agreement (this “**Agreement**”) is entered into by and between the CITY OF KETCHUM, a municipal corporation organized and existing under the laws of the state of Idaho, and PEG Ketchum Hotel, LLC, a Utah limited liability company authorized to do business in the state of Idaho as a foreign limited liability company (“**Developer**”) and (“**Owner**”).

NOW, THEREFORE, for good and valuable consideration, the receipt and sufficiency of which are hereby acknowledged and agreed, and in consideration of the premises and the mutual representations, covenants and agreements hereinafter contained, City and Developer/Owner represent, covenant and agree as follows:

SECTION 1 DEFINITIONS

For all purposes of this Agreement, the following words in **bold** print that appear in this Section have the definitions as herein provided in this Section unless the context of the term clearly requires otherwise and/or is not capitalized:

- 1.1 **Act:** Means and refers to the Local Land Use Planning Act as codified in Chapter 65 of Title 67, Idaho Code.
- 1.2 **Administrator:** Means and refers to the person appointed by the Mayor and City Council to interpret and administer Title 17 of the KMC also designated as the Director of Planning and Building.
- 1.3 **Agreement:** Means and refers to this “*Permits Conditions Acceptance Development Agreement.*”
- 1.4 **Applications:** Means and refers collectively to all of Owner’s applications to the City regarding the Development of the Subject Real Property in accordance with the Project Development Plan inclusive of: Pre-Application Design Review, Building Demolition Application [pending], Lot Line Shift Application, Floodplain Management Overlay Application, Planned Unit Development/Conditional Use Application, Design Review Application [pending] and Right of Way Encroachment Permit.
- 1.5 **Building Demolition Development Application:** Means and refers to the Developer/Owner’s application to the City for a demolition permit for buildings upon the Subject Real Property as the first part of City Development approval.
- 1.6 **Building Demolition Permit No. P 19-__:** Means and refers to that certain approved Permit issued to Developer/Owner by the Building Official subject to conditions pursuant

to the Building Demolition Development Application issued on ____ day of _____, 2019. [pending]

- 1.7 **Building Official:** Means and refers to the City Building Official.
- 1.8 **Building Permit:** Means and refers to the Building Permit/s issued by the Building Official for the construction of the Project Development Plan pursuant to Chapters 15.6 and 15.16 KMC.
- 1.9 **City:** Means and refers to the City of Ketchum, an Idaho Municipal Corporation and Party to this Agreement.
- 1.10 **City Administrative Review Fees:** Means and refers to the Administrative Review Fee, inclusive of use of outside consultants for plan checking and inspections or both for the City Services provided to the Developer/Owner for the City's administration of the Developer/Owner's Permits compliance. [need to clarify this process in city fee resolution]
- 1.11 **City Council:** Means and refers to the City Council of the City.
- 1.12 **City Project Development Plan Agent:** Means and refers to the City's _____ or their designee/s who acts on behalf of the City in the Section 4 approval process of the Project Development Plan Completion Assurance Process and Conditions and Section 6 Default process.
- 1.13 **Construction Plans:** Means and refers to the construction drawings for the Project Development Plan as approved by the City.
- 1.14 **Default Cure Remedy:** Means and refers to any remedy inclusive of a City draw on the letter of credit issued pursuant to Section 4 that will cure the default and will provide for the specific performance of the terms of this Agreement.
- 1.15 **Demolition Of Buildings Ordinance:** Means and refers to the Demolition Of Building Ordinance of the KMC Codified at Section 15.16.040, a true and correct copy of which is attached marked *Exhibit A*.
- 1.16 **Design Review Development Application:** Means and refers to the Developer/Owner's application to the City for design review approval of the proposed construction and Development by the Developer/Owner on the Subject Real.
- 1.17 **Design Review Ordinance:** Means and refers to the Design Review Ordinance of the KMC Codified at Section 17.96.010 et. seq., a true and correct copy of which is attached marked *Exhibit A*.
- 1.18 **Design Review Permit No. P 19-0__:** Means and refers to that certain approved Permit issued to Developer by the City Planning and Zoning Commission subject to conditions

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pursuant to the Design Review Development Application issued on the ____ day of _____, 2019. [pending]

- 1.19 Developed:** Means and refers to the completion of construction of the Project Development Plan as the development of the Project by the Developer/Owner in accordance with the provisions of this Agreement.
- 1.20 Developer:** Means and refers to PEG Ketchum Hotel, LLC, a Utah limited liability company, authorized to do business in the state of Idaho as a foreign limited liability company, which is developing the Project and is also the Project Owner and is a Party to this Agreement.
- 1.21 Developer/Owner:** Means and refers to both the Developer and the Owner as of the Effective Date, but subsequently if there are assignments of either the Developer and/or the Owner's Rights, pursuant to this Agreement, this term is inclusive of Developer/Owner assigns.
- 1.22 Developer/Owner's Representative:** Means and refers to the representatives for the Developer/Owner, which is Mr. Nick Blayden, AJC Architects/ Mr. Justin Heppler, BHD Structural Engineers/ Brett Goodman and is inclusive of any designee appointed by Developer as provided herein.
- 1.23 Development:** Means and refers to the process by which the Project is Developed.
- 1.24 Development Rights:** Means and refers to the Subject Real Property land use and development rights in connection with entitlements as provided in the Permits issued by the City in the process of the Applications as provided in this Agreement.
- 1.25 Development Standards Ordinance:** Means and refers to the Development Standards Ordinance of the KMC Codified at Section 17.124.010 et. seq. a true and correct copy of which is attached marked *Exhibit A*.
- 1.26 Effective Date:** Means and refers to the _____ day of _____, 2019.
- 1.27 Encroachment Permit City:** Means and refers to encroachment permit issued by the City as a part and parcel of the Project Development Plan.
- 1.28 Encroachment Permit ITD:** Means and refers to encroachment permit issued by the ITD as a part and parcel of the Project Development Plan.
- 1.29 Four Star Hotel Operations Standard:** Means and refers to the operation of the Developed Project as an upscale hotel providing guests with a luxury experience in a distinctive setting, including expanded amenities and exceptional service which includes the following characteristics:
- An impressive, well integrated and excellent architectural design; and

- A lobby area located away from main traffic areas with multiple conversation groupings and recognizable guest services; and
- A full service restaurant, separate lounge and bar area; and
- An onsite fitness center and hot tubs; and
- Conference rooms; and
- Turn down service, valet parking, baggage service, laundry service, newspaper service, room service, computer access, fitness center service, ice service, and concierge service; and
- Staff preparation and training to ensure integrate services from initial reservation call to departure.

- 1.30 Floodplain Management Application:** Means and refers to the Developer/Owner's application to the City for approval of the Developer/Owner's plan for floodplain management of the Developed Subject Real Property as the third part of City Development approval.
- 1.31 Floodplain Management Overlay Zoning Ordinance:** Means and refers to the Flood Damage Prevention Ordinance of the KMC Codified at Section 17.88.010 et. seq., a true and correct copy of which is attached marked *Exhibit A*.
- 1.32 Floodplain Management Permit No. P 19-062:** Means and refers to that certain City approved Permit issued to Developer/Owner by the City Planning and Zoning Department subject to conditions pursuant to the Floodplain Management Development Application issued on the ____ day of _____, 2019.
- 1.33 Hotel Operator:** Means and refers to the person and or entity that is in possession of the Developed Project and using and occupying the same as a Four Star Hotel Operations Standard.
- 1.34 Hotel Operations:** Means and refers to the occupation and use of the Developed Project in compliance with a Four Star Hotel Operations Standard.
- 1.35 International Codes:** Means and refers to any Building, Residential, Energy, Mechanical, Rules of Building Safety, Plumbing Code, National Electrical Code or other codes adopted by the Idaho Division of Building Safety and by the Idaho State Fire Marshall and/or by the City which govern the Development.
- 1.36 ITD:** means and refers to the Idaho Transportation Department, an administrative department of the state of Idaho.
- 1.37 KMC:** means and refers to the Ketchum Municipal Code, which is the codified ordinances of the City.
- 1.38 Level of Service:** Means and refers to an identified standard by which service is provided in the context of the terms appearance in this Agreement.

- 1.39 Lot Line Shift Development Application:** Means and refers to the Developer/Owner's application to the City for a Lot Line Shift of the existing Parcels of the Subject Real Property into one parcel as the second part of City Development approval.
- 1.40 Lot Line Shift Ordinance:** Means and refers to the Readjustment of Lot Line Procedures Lot Line Shift Ordinance of the KMC Codified at Section 16.04.030, a true and correct copy of which is attached marked *Exhibit D*.
- 1.41 Lot Line Shift Permit No. P 19-064:** Means and refers to that certain Lot Line Shift Permit issued to Developer by the approval of the City Council subject to conditions pursuant to the Lot Line Shift Development Application issued on ____ day of _____, 2019.
- 1.42 Master Plan:** Means and refers to the Master Plan filed by the Developer/Owner in support of their Project Development Plan, including the Planned Unit Development/Conditional Use Permit Application which depicts the Project proposed for construction.
- 1.43 Off Street Parking and Loading Ordinance:** Means and refers to the Off Street Parking and Loading Ordinance of the KMC Codified at Section 17.125.010 et. seq., a true and correct copy of which is attached marked *Exhibit A*.
- 1.44 Ordinances:** Means and refers collectively to the Demolition of Buildings Ordinance, Lot Line Shift Ordinance, Floodplain Management Overlay Zoning Ordinance, Planned Unit Development/Conditional Uses Ordinances, Design Review Ordinance, Off Street Parking and Loading Ordinance and the Development Standards Ordinance. This term may appear in this Agreement in the singular or the plural.
- 1.45 Owner:** Means and refers to PEG Ketchum Hotel, LLC, a Utah limited liability company, authorized to do business in the state of Idaho as a foreign limited liability company, who is the owner of the Subject Real Property on the Effective Date and Party to this Agreement, and after the Effective Date of this Agreement shall refer to the owner or owners of the Subject Real Property.
- 1.46 Peak Travel Season:** Means and refers to that period in each calendar year that commences on June 15th and continues to September 15th and then resumes on December 15th and end on the following President's Day holiday.
- 1.47 Parcels:** Means and refers to tract or parcels of distinctly legally described real property located within the Subject Real Property. This term may appear in this Agreement in the singular or the plural.
- 1.48 Party or Parties:** Means and refers to the City and/or the Developer/Owner, as the Parties to this Agreement, depending upon the context of the term as used in this Agreement.

1.49 Permits: Means and collectively refers to the following permits approved and issued by the City [inclusive of ITD issued encroachment permit] to the Developer/Owner for the Development of the Project as part and parcel of the Project Development Plan are inclusive of the following:

- River Street ROW Encroachment Permit City P 19-____; and [pending]
- State Highway 75 ROW Encroachment Permit ITD; and
- Building Demolition Permit No. P 19-____; and [pending]
- Lot Line Shift Permit No. P 19-064; and
- Floodplain Management Permit No. P 19-062; and
- Planned Unit Development/Conditional Use Permit No. P 19-063; and
- Design Review Permit No. P 19-____; and [pending]
- Waiver Applications granted by the City in conjunction with the above stated Permits; and
- Building Permits.

1.50 Planned Unit Development/Conditional Use Permit Application: Means and refers to the Developer/Owner's application to the City for approval of the proposed construction and development of the Project by the Developer/Owner as a planned unit development/conditional use.

1.51 Planned Unit Development/Conditional Use Permit No. P 19-063: Means and refers to that certain Planned Unit Development/Conditional Use Permit issued to Developer/Owner approved by the City Council subject to conditions pursuant to the Planned Unit Development/Conditional Use Permit Development Application issued on the ____ day of _____, 2019.

1.52 Planned Unit Development/Conditional Uses Ordinances: Means and refers to the Planned Unit Development Ordinance of the KMC codified at Section 16.08.060 et. seq., a true and correct copy of which is attached marked *Exhibit A* and the Conditional Uses Ordinance of the City Codified at Section 17.116 et. seq. of the KMC, a true and correct copy of which is attached marked *Exhibit A*.

1.53 Project: Means and refers to the intended development of the Subject Real Property in accordance with the Project Development Plan and subsequent occupation and use conduction Hotel Operations in accordance with a Four Star Hotel Operations Standard and as governed by this Agreement.

1.54 Project Development Plan: Means and refers to and is all-inclusive of all City approved Developer/Owner plans, specifications and Permits for the completion of the Developed Project and is inclusive of the following:

- July 29, 2019 submittal package:
 - Written Description of Project

- Narrative 01.02 – Development Plan
- Narrative 01.03 – Project Analysis | Social Impact Study
- Narrative 01.04 – Application of Standards 16.08.080
- Narrative 01.05 – Development Schedule
- Narrative 01.06 – Parking Analysis, July 16, 2019, Hales Engineering and Traffic Impact Study, July 12, 2019, Hales Engineering
- Narrative 01.07 – Employee Housing Plan
- Narrative 01.08 – Sub-Area Analysis
- Narrative 01.09 – Contextual Hotel Components
- Narrative 01.11 – Standards of Evaluation, Design Review, Conditional Use Permit, Development Agreement, and Hotels
- Exhibit 02.01 – Plans, June 18, 2019 | Elevations, July 2, 2019 | Sections, July 2, 2019
- Exhibit 02.02 – Floor Area Ratio (includes calculations, plans, and sections)
- Exhibit 02.03 – Height Analysis (includes Contextual Elevations, Invisible Plane Diagram, Existing Topography Diagram, Compatibility Views, Gateway Study Recommendations)
- Exhibit 02.04 – Sun Studies
- Exhibit 02.05 – Civil Drawings, sheets C0.1, C0.2, C1.0, C2.0, C3.0, and C4.0
- Exhibit 02.06 – Landscape Drawings sheet L1.0
- Exhibit 02.07 – Exterior Color Palate
- Exhibit 02.08 – Dark Sky Compliant Fixtures
- Exhibit 02.09 – Traffic Study Diagrams
- Exhibit 02.10 – Plat Map | Lot Line Adjustment
- Exhibit 02.11 – ROW Improvements

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- Exhibit 02.12 – Soils Report | Perc Test
- Exhibit 02.13 – Flood Plain | Riparian Corridor
- Exhibit 02.14 – List of Adjacent Property
- Exhibit 02.15 – Waiver List
- July 29, 2019 and July 30, 2019 hearing submittal documents
 - Galena Staking Exhibit
 - AJC Architecture Virtual Reality Images
 - AJC Architecture PowerPoint Presentation
 - Hales Engineering PowerPoint Presentation
- River Street Encroachment Permit [pending]
- Any additional documents submitted by Applicant for Council’s review of PUD/CUP etc [pending]
- Full Design Review submittal package to Planning & Zoning Commission [pending]

1.55 ROW: Means and refers to public right-of-way for streets and utilities either owned by the City or ITD.

1.56 ROW Improvements: Means and refers to ROW improvements.

1.57 Section: Means and refers to a Section of this Agreement.

1.58 SH75: Means and refers to State Highway 75 as it lies within the City under the ownership and jurisdiction of ITD.

1.59 Subject Real Property: Means and refers to 1.048 acres more or less located within the City and Blaine County, Idaho, legally described and depicted in *Exhibit B*.

- This definition is inclusive of the real property described in the Preliminary Plat prepared by Galena Engineering for the purpose of removing the lot line that bisects the Subject Real Property and will revise pursuant to this Agreement the legal description of record in Blaine County.

1.60 Term: Means and refers to the duration of this Agreement which is perpetual as is set forth herein unless the word “term” is not capitalized.

1.61 Waiver Applications: Means and refers to four (4) waiver Developer/Owner's applications to the City for approval of the following requirements:

- Floor Area Ratio; and
- Height/Stories; and
- Minimum Lot Size; and
- Setback

SECTION 2 RECITALS

The Parties recite and declare:

2.1 The Developer/Owner is the owner of the Subject Real Property which is within the City limits of the City; and

2.2 The Developer/Owner intends to develop its Project upon the Subject Real Property in accordance with the Project Development Plan and thereafter occupy and possess the same in the conduct of Hotel Operations. In order to accomplish its intentions, the Developer/Owner has filed the Applications which have resulted in the City's approval of the Permits which include terms and conditions of compliance by the Developer/Owner; and

2.3 The City is a municipal corporation established in accordance with Article XII of the Constitution of the State of Idaho and Title 50 Idaho Code and is required and has exercised its authority pursuant to the Local Land Use Planning Act codified at Chapter 65 of Title 67 Idaho Code and pursuant to Chapters 3, 9 and 13 of Title 50 Idaho Code to enact the Ordinances and regulations which Ordinances which govern the Developer/Owner's intentions for the Developed Project and use of the Subject Real Property; and

2.4 The City has the following authority pursuant to Idaho Code Section 50-335 which provides:

50-335. DESTRUCTION OF BUILDINGS INIMICAL TO SAFETY AND HEALTH. All cities in the state of Idaho shall have power to declare any building or structure to be a nuisance which, in the opinion of the city council, is so dilapidated or is in such condition as to menace the public health or the safety of persons or property on account of increased fire hazard or otherwise; and any council may cause the destruction or removal of any such building or structure at the expense of the person or persons, associations, corporations or copartnerships holding, owning or maintaining the same, and to levy a special

assessment as provided in section 50-1008 [, Idaho Code], on the land or premises whereon the nuisance is situated, to defray the cost or to reimburse the city for the cost of destruction or removal of said building or structure so declared to be a nuisance.

- 2.5 A failure of the Developer/Owner to construct and occupy the Project has great potential to result in a vacant and abandoned partially completed Project. The Project is located in a very visible and central location within the City. A vacant and abandonment Project, particularly due to its size and location, will become an eyesore, become a negative effect upon neighboring properties, tourism and the commerce and the health and welfare of the residents, and property owners of the City and would resultantly become a nuisance; and
- 2.6 KMC section 16.08.120 C provides that prior to final approval of a PUD conditional use permit, the City Council may require a written agreement executed by the Developer/Owner to secure performance of any requirement or condition to be imposed as part of the approval, including, but not limited to Development and may also require recordation of documents establishing and guaranteeing the operation and maintenance of the Project; and
- 2.7 A special consideration for the City to issue the Permits to the Developer/Owner for the Project Development Plan is the Developer Owner's assurance of financial performance of the timely completion of the Developed Project in accordance with the terms and conditions of this Agreement.
- 2.8 The City's approval of the Permits and granting to the Developer/Owner's Development Rights is conditioned upon and is in consideration of the Developer/Owner's acceptance of the terms and conditions of the Permits which acceptance is in accordance with and governed under the terms and conditions of this Agreement.

SECTION 3 PERMIT CONDITIONS

The Developer/Owner, by entering into this Agreement for its Term, does hereby accept and agrees to perform in accordance with and abide by the terms and conditions of the Permits which terms and conditions are herein stated and to abide by the terms and conditions of the issuance of a Building Permit which are also stated in Sections 4 and 5 of this Agreement as follows:

- 3.1 **Construction and Occupancy Time Limits:** The following are the time limits that govern construction and occupancy of this Project:
 - 3.1.1 **Design Review Permit:** Pursuant to KMC §17.96.090, the Design Review Permit is valid for twelve (12) months from the date of final decision on the associated Findings of Fact, Conclusions of Law, and Decision.

3.1.2 Building Permit: A completed application for the Project Development Plan building permit must be filed within twelve (12) months, the time as specified in KMC §17.96.090(A)(2). Any extension shall only be as allowed and specified in KMC §17.96.090.B.

3.1.2.1 Agreement Termination: In the event the Developer/Owner fails to complete the process of obtaining the issuance of the Building Permit for the Project within _____ months from the Effective Date, the Project Development Plan approvals and Permits and this Agreement shall then immediately terminate and be null and void.

3.1.3 Project Development Plan Phase Completion Timelines: The Developer/Owner shall complete the following Project Development Plan Phases in accordance with the following timelines following the issuance of Building Permit for the Project Development Plan: [detailed construction sequencing & approximate inspection dates – pending]

3.1.3.1 Demolition of existing structures on Subject Real Property: _____[date of completion]

3.1.3.2 Project site preparation after demolition including utility connection work: _____[date of completion]

3.1.3.3 Project foundation construction _____[date of completion]

3.1.3.4 Project framing including mechanical, fire sprinkler, electrical, windows and exterior doors _____[date of completion]

3.1.3.5 Project Roofing and siding -- HVAC _____[date of completion]

3.1.3.6 Project Interior /including utilities _____[date of completion]

3.1.3.7 Project construction outside of the hotel and Landscaping _____[date of completion]

3.1.4 Certificate of Occupancy: The following are Certificate of Occupancy issuance conditions:

3.1.4.1 Time Limits: A certificate of occupancy shall be issued for the Project no later than 365 days after the building permit is issued, unless an extension is granted in accordance with KMC §15.04.020.A.

3.1.4.2 Conditions Precedent: No Certificate of Occupancy shall be issued until the following items are complete:

- 3.1.4.2.1 All Design Review elements are complete to the satisfaction of the Planning & Building Department, and
- 3.1.4.2.2 All proposed encroachments within River Street right-of-way have been installed in accordance with the Approved Plans and to the satisfaction the City Engineer; and
- 3.1.4.2.3 All rooftop mechanical and electrical equipment is fully screened from public vantage points to the satisfaction of the Planning & Building Department, and
- 3.1.4.2.4 The Fire, Utilities, Building, Arborist, Streets, and Planning Departments have conducted final inspections and authorized issuance of a Certificate of Occupancy.
- 3.1.4.2.5 All River Street and SH75 ROW Improvements shall be completed in accordance with approved encroachment permit approvals to the satisfaction of the City Engineer and the ITD Engineer, respectively.

3.2 Building Permit Requirements. The following are the conditions precedent to the issuance of the Building Permit for the Project Development Plan:

- 3.2.1 **Project Development Plan Approval.** The Project Development Plan, including Final Design Review approval and the recordation of this Agreement, shall be approved by the City; and
- 3.2.2 **Building Demolition Permit Conditions:** The Developer/Owner right to proceed to demolish all or any portion of the existing structures on the Subject Real Property pursuant to the Building Demolition Permit approval shall follow the permit process as specified in KMC §15.16.040 including a 60 day waiting period for the demolition of historic buildings, prior to demolishing any historic structure on the Subject Real Property.
- 3.2.3 **Subdivision Lot Line Shift Development Application Conditions:** The Developer/Owner as part of the process for approval of the Project Development Plan has made application to the City for removal of the common lot lines shared between the three lots that comprise the Subject Real Property, pursuant to subsection L of KMC §16.04.030. As set forth in the Preliminary Plat by Galena Engineering, subject plat amendment will remove the lot line bisecting the Property into a new one-lot subdivision. The Lot Line Shift Permit No. P 19-064 approved final Plat Map reflecting such changes shall not be recorded and become effective until after all River Street ROW improvements and utilities have been installed to the satisfaction of the City Council as set forth in §16.04 of the City Subdivision Ordinance.

- 3.2.3.1 Deferment of Final Plat Recordation:** Subject to City Council approval of the Project Development Plan inclusive of the PUD, including deferment of the final plat Time Limitations set forth in KMC §16.04.030.H, the Applicant may be issued a Building Permit prior to final plat recordation and shall record the new Lot 3A plat concurrently with a Certificate of Occupancy being issued by the City for the Project.
- 3.2.3.2 City Department Requirements:** All requirements of the fire, utility, building, planning and streets and facilities departments of the City shall be met and all public improvements shall meet the requirements of the City Engineer.
- 3.2.3.3 Utility Plan:** A full utility plan will be required prior to final plat recordation and infrastructure construction.
- 3.2.3.4 Riparian and Scenic Easement:** A twenty-five foot (25') riparian and scenic easement is shown on the plat. Any work in this area is subject to separate floodplain rules and regulations; and
- 3.2.3.5 Fisherman's Easement:** A ten-foot (10') fishermen's easement adjacent to the OHWM of Trail Creek and access through the Subject Real Property shall be shown on the plat; and
- 3.2.3.6 Building Envelope:** A Building Envelope ("BE") will be reflected on the final plat consistent with the approved final Agreement for the Project. Subject BE will reflect the Council's final approval on FAR, corner lot radii sight line requirements, and front/side yard setbacks. The BE shall not encroach within 25' of Trail Creek or within 25' of the edge of ITD ROW; and
- 3.2.3.7 Removal Existing Vegetation:** The removal of existing mature trees and/or established shrub masses is subject to approval and adoption of the Applicant's landscape plan through the Design Review and/or Floodplain Development Permit process; and
- 3.2.3.8 Encroachment Permits:** An encroachment permit from ITD as well as the City will be required for all improvements to public right of way. As a condition of Plat approval, subject sidewalk and street lighting to city standards shall be installed prior to final plat recordation unless otherwise approved by the City Council; and
- 3.2.3.9 Drawing Approvals:** Prior to grading occurring on the new Lot 3A, City approval of the Applicant's grading, drainage and landscaping construction drawings is required. Subject construction drawings shall be consistent in concept with approved Design Review, Encroachment Permit, and related drawings.

- 3.2.4 Storm Water Management Pollution Prevention Plan (“SWPPP”) in accordance with local, state and federal laws and regulations is in place for the Project Development Plan; and the Project Development Plan includes a Construction Staging and Mitigation Plan consistent with the standards as specified in KMC §15.06 has been reviewed and approved by the City.
 - 3.2.5 A detailed Project Development Plan Construction Staging and Mitigation Plan which is consistent with the standards specified in Chapter 15.06 of KMC, including provisions for off-site parking for contractors, sub-contractors, and other trades associated with the construction of the Project, off-site storage of bulk materials, and required right of way encroachments during construction, shall be submitted and approved by Building Administrator prior to Building Permit approval.
 - 3.2.6 The Developer/Owner has secured a will serve letter from Idaho Power for the Project.
 - 3.2.7 The Developer/Owner has secured a will serve letter from Clear Creek Disposal prior to issuance of a Building Permit.
 - 3.2.8 The River Street Encroachment Permit from the City has been issued.
 - 3.2.9 The Developer/Owner has completed the process set forth in Section 4 with the issuance of the irrevocable letter of credit and the City Project Development Plan Agent has accepted and secured subject irrevocable letter of credit consistent with this Agreement. As set forth in §4.5 herein, the irrevocable letter of credit shall include: an amount equal to 130% of the approved Project Development Plan Bid; an amount equal to 150% of engineering estimates of the ROW Improvements; and, an amount to lower parking demand and traffic impacts.
- 3.3 **Emergency Services and Standards Conditions:** The following are emergency services and safety terms and conditions of the Project Development Plan construction:
- 3.3.1 **Completion of Fire Improvements.** The City Building Official or the City Fire Marshal may withhold building and/or fire inspection approval for any phase of construction of the Project Development Plan until all necessary components of the water and/or fire alarm system sufficient to provide protection for that portion of the Project are complete.
 - 3.3.2 **Fire Access During Construction.** Vehicle parking and material storage during Project construction shall not restrict or obstruct public streets or access to any building. Emergency vehicle access shall be maintained as required by the Fire Chief. Once construction begins on the second floor and above, 26-foot aerial ladder truck access is required along one entire side of the building, in a location approved by the Fire Marshal, for evacuation of injured persons from upper floors. All required Fire Lanes, including within 15 feet of fire hydrants, shall be maintained clear and unobstructed at all times.
 - 3.3.3 **Fire Code Requirements.** The Project shall comply with all the terms and conditions set forth in the City Fire Department Pre-application Requirements

Memo dated June 24, 2019 from Tom Ancona, Assistant Chief & Fire Marshall, inclusive of subsequent amendments thereto, as well as all 2012 International Fire Code requirements and any additional specific City Building (Chapter 2 15.04 and 15.06) and Fire Ordinances (Chapter 15.08).

3.4 Project Development Plan Construction Plans and Master Plan Conditions: The following are conditions of Construction Plans and the Master Plan approval for the Project Development Plan:

3.4.1 ROW Improvements and Use Conditions: The following are Project Development Plan ROW Improvements and use terms and conditions of Project Development Plan construction:

3.4.1.1 DIG Permit: The Developer/Owner shall submit a Street and Alley Digging, Excavation, and Trenching (“DIG”) Permit application with an associated traffic control plan for all construction work within the City ROW to be reviewed and approved by the Streets and Facility Department.

3.4.1.2 Temporary Use of Right-of-Way Permit: The use of City right-of-way for Project Development Plan construction which includes the closure of adjacent streets or sidewalks requires a Temporary Use of Right-of-Way Permit (“TURP”).

3.4.1.3 Public ROW Encroachments. Abutting the Subject Real Property are two public rights of way (“ROW”). To the east is State Highway 75 (“SH75”), which is owned and maintained by the Idaho Transportation Department (“ITD”). To the north is River Street, which is owned and maintained by the City. This Project qualifies as a substantial improvement, therefore the Project Development Plan must include the installation of sidewalks equal to the length of the subject Real Property boundary line adjacent to any public street (KMC §17.96.060). Prior to issuance of a Building Permit, the Developer/Owner shall obtain approval of all ROW encroachments within the ROW pursuant to a separate ROW encroachment agreement approved by the City Council.

3.4.1.3.1 SH75 Encroachment Permit. ITD has an approved Record of Decision (“ROD”) that includes a 3-lane section with a six-foot (6’) wide sidewalk abutting the Subject Real Property. Subject ITD improvements to the SH75 ROW are proposed to be installed by ITD in Fiscal Year 2025 with road work in the vicinity, at the earliest, occurring in October of 2025. The installation of these SH75 Improvements by ITD and, particularly, the striping of a dedicated left turn lane West Bound onto River Street with

adequate queuing for cars turning onto River Street is important to traffic flow both along SH75 and into the Project.

3.4.1.3.2 Center Turn Lane for SH 75/River Street Intersection: Given the Owner’s timeframe for construction and Certificate of Occupancy, subject SH75 work will not be conducted by ITD prior to the Developed Project and the commencement of Hotel Operations. Given that the City has determined that a center turn lane with adequate queuing of approximately fifty to one hundred feet (50’ – 100’) is necessary for the SH75/River Street intersection to retain its current Level of Service (“LOS”) for vehicular car movement, therefore the Developer/Owner shall file with ITD an application for an encroachment permit. The Developer/Owner and City shall work together to attain approval from ITD for the construction and striping of a partial center turn near the River Street intersection north of the Trail Creek bridge. The Developer/Owner shall pay for engineering, traffic control and construction costs for subject SH75 improvements adjacent the Project.

3.4.1.3.3 Signage & Striping: To avoid excessive delays for east bound traffic on River Street, subject to review and approval by ITD and the City, the Developer/Owner shall fund and install appropriate signage and improvements to allow only a right turn onto southbound SH75 as the intersection of SH75 and River Street.

3.4.2 Pedestrian Safety. To help assure pedestrian safety and consistent with KMC §§16.08.080 and 17.116.030, at the discretion of the Ketchum City Council upon the recommendation of the Ketchum Transportation Authority and the city’s peer review engineering firm (AECOM), the Developer/Owner shall work with the City and ITD to upgrade the unsignalized SH75 and River Street crossing (on north-side) to include a rectangular rapid flashing beacon (RRFB) crosswalk similar to 4th Street. The circulation design shall meet all standards as specified in KMC §17.96.060.G.

3.4.3 Terrace Walls. Developer/Owner plans for and construction of terrace walls on the Subject Real Property adjacent to SH75 and neighboring property to the west will be subject to future design review as provided by KMC at the time the application is filed for approval at the discretion of the Administrator.

3.4.4 Setback, FAR and Height Waivers Limitations: The approval of the Developer/Owner Waiver Applications which concern setbacks, FAR, and height of the Project shall comply with Final Design Review for the Project as approved by

the City. The final plans once approved and integrated into the Project Development Agreement by the Ketchum City Council illustrate areas where buildings may exceed height and bulk limitations. As conditioned herein above, City acknowledges the zoning and subdivision waivers set forth in the in the City Council Findings granting Planned Unit Development/Conditional Use Permit.

3.4.5 Floodplain. All conditions of Floodplain Development Permit P19-062 shall be adhered to. Any deviations to said permit shall be issued in writing through an amendment to the permit process in accordance with the terms and conditions and process of Section 5 of this Agreement.

3.4.5.1 At minimum, structures on the north side of Trail Creek shall be set back a minimum of twenty-five feet (25') from the mean high water mark. No unauthorized construction activity shall occur within the riparian setback. Riparian vegetation shall be maintained in its natural state.

3.4.5.2 The term of floodplain development permit and waterways design review shall be twelve (12) months from the date that findings of fact, conclusions of law and decision are signed by the Administrator or upon appeal, the date the approval is granted by the Planning and Zoning Commission, subject to changes in zoning regulations. Application must be made for a Building Permit (if required) with the City Building Department during the twelve (12) month term. Once a Building Permit (if required) has been issued, the approval shall be valid for the duration of the Building Permit. Unless an extension is granted as set forth in KMC 17.88.050.G, failure to file a complete Building Permit application (if required) for a project in accordance with these provisions shall cause said approval to be null and void.

3.4.5.3 This Waterways Design Review and Floodplain Development Permit approval is based on the plans, as referenced above, and information presented and approved at the meeting on the date noted herein. Any building or site discrepancies which do not conform to the approved plans will be subject to removal.

3.4.5.4 Pursuant to Chapter 17.88.040.C, no chemicals or soil sterilants are allowed within 100 feet of the mean high-water mark. No pesticides, herbicides, or fertilizers are allowed within 25 feet of the mean high-water mark unless approved by the City Arborist. All applications of herbicides and/or pesticides within one hundred feet (100') of the mean high-water mark, but not within twenty-five feet (25') of the mean high water mark, must be done by a licensed applicator and applied at the minimum application rates. Application times for herbicides and/or pesticides will be limited to two (2) times a year; once in the spring and once in the fall unless otherwise approved by the city arborist. The application of dormant oil sprays and insecticidal soap within the riparian zone may be used throughout the growing season as needed.

- 3.4.5.5** Prior to commencement of any construction activity on the Subject Real Property the silt fence and inlet protection shall be installed as indicated in civil sheet 2.0.
- 3.4.5.6** No maintenance, including the mowing, trimming, and removal of vegetation, and no construction activities, encroachment, or disturbance within the riparian zone shall take place without approval through an amendment to this permit, during the time period the permit is in effect, or through approval of a subsequent permit.
- 3.4.5.7** It shall be unlawful to dump, deposit or otherwise cause any trash, landscape debris or other material to be placed in any stream, channel, ditch, pond or basin that regularly or periodically carries or stores water.
- 3.4.5.8** This Waterways Design Review and Floodplain Development Permit approval is based on the plans, as referenced above, and information presented and approved at the meeting on the date noted herein. Any building or site discrepancies which do not conform to the approved plans will be subject to removal.
- 3.4.5.9** Pursuant to Chapter 17.88.040.C, no chemicals or soil sterilants are allowed within 100 feet of the mean high-water mark. No pesticides, herbicides, or fertilizers are allowed within 25 feet of the mean high-water mark unless approved by the City Arborist. All applications of herbicides and/or pesticides within one hundred feet (100') of the mean high-water mark, but not within twenty-five feet (25') of the mean high water mark, must be done by a licensed applicator and applied at the minimum application rates. Application times for herbicides and/or pesticides will be limited to two (2) times a year; once in the spring and once in the fall unless otherwise approved by the city arborist. The application of dormant oil sprays and insecticidal soap within the riparian zone may be used throughout the growing season as needed.
- 3.4.5.10** Prior to commencement of any construction activity on the Subject Real Property the silt fence and inlet protection shall be installed as indicated in civil sheet 2.0.
- 3.4.5.11** No maintenance, including the mowing, trimming, and removal of vegetation, and no construction activities, encroachment, or disturbance within the riparian zone shall take place without approval through an amendment to this permit, during the time period the permit is in effect, or through approval of a subsequent permit.
- 3.4.5.12** It is unlawful to dump, deposit or otherwise cause any trash, landscape debris or other material to be placed in any stream, channel, ditch, pond or basin that regularly or periodically carries or stores water.

- 3.4.6 Utilities Plan:** As a part of the Construction Plans the Developer/Owner shall submit Project utilities plan indicating the location and size of water and sewer mains as well as gas, electric, TV and phone services (KMC §17.96.040.C.2c & KMC §17.96.060.D.1-3). Per KMC §17.96.060.D.2, utilities shall be located underground and utility, power, and communications lines within the Project Development Plan site should be concealed from public view.
- 3.4.7 Water & Sewer:** Developer/Owner shall engineer, construct and extend, at its sole expense, the City water and sewer system improvements throughout the Project.
- 3.4.7.1 Connection Fee Credit.** Owner requests water and sewer service from the City to the Subject Real Property and the City agrees to provide such water and sewer service at the same fees as charged to equivalent users of City's water and sewer systems in accordance with the fee methodology in effect at the issuance of the Building Permit. Final fee calculations shall credit, as applicable, services abandoned with the demolition of the previous mixed use buildings on Subject Real Property.
- 3.4.7.2 Improvements.** All such improvements shall be designed and constructed in accordance with the standards of, and Construction Plans and specifications approved by, the State of Idaho, Department of Health and Welfare, Division of Environmental Quality, and City.
- 3.4.7.3 As-built Drawings.** All construction plans shall be approved by the City and as-built drawings provided to the City upon acceptance of the Project improvements by the City.
- 3.4.8 Drainage.** Project Drainage system Construction Plans shall be submitted to the City Engineer for review and approval. Pursuant to KMC §17.96.060.C, all storm water shall be retained on site, drainage improvements constructed shall be equal to the length of the Subject Real Property boundary lines, and all drainage facilities shall be constructed per City standards. All drainage improvements shall meet the applicable design criteria as specified in KMC §12.04.030.
- 3.4.9 Landscaping.** Developer/ Owner shall install all landscaping and related improvements for the Project located on private property in accordance with Design Review requirements which includes the detailed landscape Construction Plans as submitted as part of the Design Review Permit. The landscape Construction Plans shall include the following, which is not exhaustive: species sizes, quantities and location; significant stepping and retaining walls between the Subject Real Property and State Highway 75; in compliance with all floodplain and riparian regulations. Pursuant to KMC §17.96.060.I, landscape materials and vegetation types specified shall be readily adaptable to the microclimate, soil conditions, orientation and aspect, and shall serve to enhance and complement the townscape. All trees, shrubs, grasses, and perennials shall be drought tolerant. Any landscaping or improvements

within the ROW shall be installed consistent with the approved ROW Encroachment Agreement.

- 3.4.10 Lighting.** All Project lighting shall comply with the City Dark Sky Standards, Chapter 17, KMC. The Design Review Permit includes the required exterior lighting plan identifying the location, height, type, lumen output, and specifications for fixtures (KMC §17.96.040.C.2h). All exterior lighting fixtures shall be full cutoff fixtures with the light source fully shielded and an associated color temperature not to exceed 2700 Kelvin (KMC §17.132.030). Street lights shall be installed within the ROW subject to the review and approval of the City Engineer.
- 3.4.11 Employee Housing Units.** The Developer/Owner shall use and maintain the Subject Real Property for Hotel Operations and shall either maintain or enter into a master lease with the Hotel Operator for apartment units within the Developed Project containing thirty (30) beds and thereby fulfill and satisfy the employee housing obligation of this Project consistent with KMC §17.124.050. All leased apartment units must be subleased, assigned or otherwise made available to employees of the Hotel Operator on terms and conditions determined by it in the exercise of its discretion.
- 3.4.12 Hotel Operations.** The core intended feature of the Developed Project is a hotel building being used for Hotel Operations at an industry acknowledged Four Star Hotel Operations Standard. Adherence to a Four Star Hotel Operations Standard, particularly during Peak Travel Season, affects the sufficiency of on-site parking and traffic circulation in the immediate vicinity of the Project and is a requirement of the Hotel Operator's occupancy and use of the Developed Project.
- 3.4.13 Lower Parking Demand and Traffic Impacts.** To assure that the Developer/Owner and/or Hotel Operator provides guest shuttle, employee shuttle, car share program, transit passes, carpool program, and alternative transportation (such as bike storage for employees), the Developer/Owner and/or Hotel Operator shall include in the irrevocable letter of credit a Lower Parking Demand and Traffic Impacts amount of \$___ for a period of not greater than five (5) years upon which the City Council may request a draw to cover the City's costs in the mitigation of lowering traffic impacts and/or parking demands associated with the Developer/Owner and/or Hotel Operator's failure to comply. The process to be followed for a request for a draw, pursuant to this Section on the letter of Credit is governed chronologically first by Section 4 and then Section 5 of this Agreement.
- 3.4.14 Parking & Loading.** The Applicant shall present a Project Parking Plan for review and consideration by the Commission as part of its full Design Review Submittal. Prior to Certificate of Occupancy, a Project Parking Plan verifying public use, validation processes for determining parking charges (if any) for the public and the six (6) displaced public parking spaces, and other details at the discretion of the City, shall be provided and approved by Ketchum City Council for the Project Parking Garage. The Project Development Plan has a total of 90

parking spaces. Of those 90 spaces, 66 spaces are required for the Four Star Hotel Operations Standard, 14 spaces are required for employee housing, 1 space is required for the restaurant/bar, and 6 are required for the public to mitigate displaced public parking spaces from River Street. Of this total, not less than:

- 3.4.14.1** Fourteen (14) stalls shall be allocated for Hotel Operations employee housing, inclusive of at least one (1) car share vehicle; and
- 3.4.14.2** Six (6) underground parking stalls will be available to the general public at no charge to the public while using the conference center, spa, restaurant and bar. A validation system may be employed by the Developer/Owner and/or Hotel Operator with regard to the monitoring of public use of the six, free-of-charge, underground public spaces located in the Project Parking Garage; and
- 3.4.14.3** Guest shuttle, employee shuttle, car share program, transit passes, carpool program, alternative transportation (bike storage) – shall be provided as a part of the Four Star Hotel Operations Standard.
- 3.4.14.4** During and upon completion of the construction of the Project Development Plan, delivery vehicles associated with the Project shall not interfere with the regular flow of traffic surrounding the Subject Real Property. Delivery vehicles shall not block the regular flow of traffic along River Street.

3.5 Planned Unit Development Conditional Use Permit Conditions: Planned Unit Development/Conditional Use Permit No. P 19-063 approval includes the following conditions and other conditions as provided in this Agreement:

- 3.5.1 Project Height Limitations:** The maximum height of the Project shall comply with the provisions of Section 3.4.4.
- 3.5.2 Final Plat Time Limitation:** The Project final plat time limitations are set forth in Section 3.2.3.
- 3.5.3 Certificate of Occupancy Conditions:** No Certificate of Occupancy shall be issued until the Certificate of Occupancy Conditions [Section 3.1.4 of this Agreement] have been complied with. xxx
- 3.5.4 Drainage system plans:** The drainage system conditions are set forth in Section 3.4.8.
- 3.5.5 Utility plans:** The Utility Construction Plans conditions are set forth in Section 3.4.6.
- 3.5.6 Hotel Operations Employee Housing:** The Hotel Operations employee housing conditions are set forth in Section 3.4.11.

- 3.5.7 Hotel Operations:** The Hotel Operations Conditions are set forth in Section 3.4.12.
- 3.5.8 Parking Spaces:** The Developed Project and Hotel Operations parking space conditions are set forth in Section 3.4.13.
- 3.5.9 Letter of Credit Conditions and Process:** The letter of credit issuance and process Project completion assurance conditions are set forth in Section 4.
- 3.5.10 Public ROW Improvements and Use:** Public ROW improvements and use conditions are set forth in Section 3.4.1.
- 3.5.11 Emergency Services and Safety Conditions:** The emergency services conditions and safety conditions as set forth in Section 3.3.
- 3.5.12 City Department Requirements:** All requirements of the Fire, Utility, Building, Planning and Streets and Facilities departments of the City shall be met. All public improvements shall meet the requirements of the Public Works Department.
- 3.5.13 KMC Compliance Conditions:** All other provisions of KMC, Chapters 16 and 17 and all applicable ordinances rules and regulations of the City and other governmental entities having jurisdiction shall be complied with in the Project Development process.
- 3.6 Loading Requirements.** During and upon completion of the construction of the Project Development Plan, delivery vehicles associated with the Project shall not interfere with the regular flow of traffic surrounding the Subject Real Property. Delivery vehicles shall not block the regular flow of traffic along River Street.
- 3.7 Utilities and Warranty.** All Project Development Plan utilities, including water, sewer, gas, cable, phone and electric shall be installed underground within the street rights-of-way. This includes on site and off site utilities: no above-ground utility lines are permitted on the Subject Real Property. Detailed engineered construction drawings and specifications for construction of such improvements shall be prepared by Developer/Owner and approved by the City prior to construction. Prior to acceptance of any such improvements to be dedicated to City, the City shall inspect and approve same and Developer/Owner shall provide the City with “as built” drawings thereof. The Developer/Owner warrants to the best of its knowledge the “as built drawings” are substantially correct and Developer/Owner shall, for a period of one (1) year from the date of the City’s receipt of said drawings, be liable and hold the City harmless for any damage proximately caused by reason of a material error in said drawings.

 - 3.7.1 Transfer of Warranties.** Developer/Owner agrees to assign any warranties accruing to it and arising out of construction of the improvements described in this

Section remaining in effect at the time such improvements are transferred and/or dedicated to and accepted by the City.

- 3.8 Certificate of Occupancy:** No Certificate of Occupancy shall be issued for the use and occupancy of this Project until the following items are complete:
- 3.8.1** All Design Review elements of the Project Development Plan have been completed and approved by the Planning & Building Department; and
 - 3.8.2** All proposed encroachments within the City’s River Street right-of-way have been installed in accordance with the Project Development Plan and approved by the City Engineer; and
 - 3.8.3** All rooftop mechanical and electrical equipment is fully screened from public view and approved by the Planning & Building Department; and
 - 3.8.4** The City’s Fire, Utilities, Building, Arborist, Streets, and Planning Departments have conducted final inspections and authorized issuance of a Certificate of Occupancy.
- 3.9 City Administrative Review Fees:** The Developer/Owner shall be charged and shall pay the City Administrative Review Fees for the administration of the Developer/Owner’s and the City’s performance of this Agreement.
- 3.10 Conditions to Owner’s Obligations.** Owner’s obligations hereunder are conditioned upon (1) receiving all the referenced approvals from the City and (2) securing financing. If either of those conditions (or part of one) is not satisfied, then this Agreement shall no longer be valid.
- 3.11 Local Option Tax.** Hotel Operations shall be subject to the provisions of KMC Section 3.12 (the City “Local Option Tax Ordinance”) and any amendments or recodifications thereof relating to local option taxes inclusive of the following:
- 3.11.1 Beverage, Food & Retail Sales.** All retail, food and beverage sales on the Subject Real Property and in the Project shall be subject to the local option tax.
 - 3.11.2 Building Materials.** The Project and Subject Real Property building materials shall be subject to the local option tax on building materials.
 - 3.11.3 Employee Housing.** The obligation to pay local option tax shall not apply to the rental of employee housing units.
 - 3.11.4 Future Amendments to LOT Ordinance.** Any amendments to or repeal of Local Option Tax Ordinance and/or Idaho law relating to such local option taxes shall also apply to and modify this Section to the extent of such amendment(s) and/or repeal.

3.11.5 Hotel Rooms. All hotel rentals shall be subject to the local option tax, regardless of who makes the reservation, including independent third party travel agencies or other independent parties.

3.11.6 Short-term rentals. All non-hotel rentals, if any, shall be subject to the local option tax on short-term rentals.

SECTION 4 PROJECT DEVELOPMENT PLAN COMPLETION ASSURANCE PROCESS AND CONDITIONS

4.1 This Section contains the terms and conditions of the process to assure that the Developed Project is constructed in compliance with the Project Development Plan and completed in accordance with the Project Development Plan Phase Condition Timelines and the performance by the City of the provisions of this section shall be conducted by the City Project Development Plan Agent.

4.1.1 In the event the City Project Development Plan Agent designates a designee/s the same shall be in writing and a copy thereof shall be provided to the Developer/owner.

4.2 Within Sixty Days of the Effective Date the Developer/Owner shall obtain and provide to the City Project Development Plan Agent an irrevocable bid or commitments from licensed contractor/s [and construction managers if desired] to enter into construction contract with the Developer/Owner to construct the Project Development Plan in accordance with the Project Development Plan Phase Condition Timelines (the "Project Development Plan Bid") which bid or commitments includes all of the following:

4.2.1 Each separate and total bid cost; and

4.2.2 The respective schedules of values apportioned to the various divisions or phases of the Project Development Plan Work with each line item containing a schedule of monetary price values such that the total of all items shall equal the total bid cost; and

4.2.3 A reasonable liquidated damages penalty for failure to timely construct; and

4.2.4 A Developer/Owner option for failure to timely construct to secure another contractor to timely construct the Project Development Plan; and

4.2.5 Performance bond and a payment bond equal to the total bid costs to assure the timely completion of the construction of the Project Development Plan; and

4.2.6 Permission to allow City Project Development Plan Agent or their designee to inspect as may be reasonably needed in compliance with this Agreement.

4.3 The Developer/Owner shall then provide a copy of the Project Development Plan Bid to the City Project Development Plan Agent who shall review the same and determine (within 28 days) whether or not it is:

- Complete; and
- Complies with this Agreement; and
- Has been submitted by a reasonably responsible bidder.

4.3.1 The City Project Development Plan Agent shall, within the 28 days, conduct a reasonable inspection and provide a written report to the Developer/Owner either approving the Project Development Plan Bid or specify any failures and identifying what is needed to obtain compliance and approval.

4.4 Subsequent to the City Project Development Plan Agent's approval of the Project Development Plan Bid, the Developer/Owner shall enter into a contract/s for the Project Development Plan construction (the "Project Development Plan Construction Contract") in accordance with the approved Project Development Plan Bid.

4.5 Letter of Credit: In coordination with the entering into the Project Development Plan Construction Contract the Developer/Owner shall cause to be issued an irrevocable letter of credit on a bank, acceptable to the City Finance Manager and in a form acceptable to the City Project Development Plan Agent and the City Attorney which complies with the following which shall govern its use:

4.5.1 In an amount equal to 130% of the approved Project Development Plan Bid [the additional 30% to cover contingencies, and City Administrative Review Fees costs] and additionally:

4.5.2 An amount equal to 150% of engineering estimates of the ROW Improvements; and

4.5.3 A Lower Parking Demand and Traffic Impacts amount of \$_____.

4.5.4 For a period of sixty-four (64) months from the date of issue as a financial guarantee to the City

4.5.5 Which letter of credit shall provide for and accommodate the following process:

4.5.5.1 Requests for partial releases of the amount of letter of credit shall be made by the Developer/Owner to the City Project Development Plan Agent in writing in amounts equal to an approved request for payment in accordance with the Project Development Plan Construction Contract. In the event the request involves Partial release involving ROW Improvements the same may be made when there has been an acceptance of subject River Street ROW Improvements by the City; and when there has been formal commencement of work by ITD of the SH75 ROW Improvements adjacent the Subject Real Property; and/or upon complete installation of the SH75 ROW Improvements adjacent the Subject Real Property.

4.5.5.2.3 In the event of an approval by the Parties the Bank shall be notified in writing of the approval and the final release subject to any authorized draw by the City.

4.5.5.3 The City may draw upon the letter of credit in accordance with the following process:

4.5.5.3.1 In the circumstance of City Council decision finding a default on the part of the Developer/Owner, the request is made in writing by the City Project Development Plan Agent together with the following proof with a copy to the Developer/Owner:

- Certified copy of the City Council's Findings of Fact, Conclusions of Law and Order of Decision finding a default of this Agreement by the Developer/Owner which order of decision authorizes a draw from the letter of credit; and
- In the even the City Project Development Plan Agent draws on the letter of credit, the funds received shall then be place by the City Finance Manager in the City's Custodial Holding Fund for expenditure in accordance with the City Council's Findings of Fact, Conclusions of Law and Order of Decision and order finding a default in the enforcement of the specific performance of this Agreement.

4.5.5.3.2 In the circumstance of an approved partial release and Developer/Owner consent to an authorized City Administrative Review Fees draw by the City the request is made in writing by the City Project Development Plan Agent together with the following proof with a copy to the Developer/Owner:

- The Developer/Owner's written authorization of the draw on the line of credit.

4.6 City Project Development Plan Agent: The City officials, who are involved in the process of approval of the Developer/Owner's performance of the terms and conditions of this Agreement, shall notify the City Project Development Plan Agent of the timely performance or default of the Developer/Owner and the Project Development Plan Agent shall coordinate with the Developer/Owner on such matters in the process of the completion of the Developed Project and subsequent Developer/Owner performance matters governed under this Agreement.

- 4.6.1 In the event the City Project Development Plan Agent appoints a designee, written notice of the name and contact information and authority designated shall be immediately provided to the Developer/Owner.

**SECTION 5
PROJECT DEVELOPMENT PLAN MODIFICATIONS SUBSEQUENT TO BUILDING PERMIT
ISSUANCE**

- 5.1 The construction of the Project pursuant to the Building Permit issue shall substantially comply with the Project Development Plan and comply with any other applicable KMC and approved plan submittals required and relied upon for the issuance of the Building Permit.
- 5.2 The Administrator is authorized to approve minor modifications, as defined by KMC §17.08.020, to the Project Development Plan.
- 5.3 Any material change to the Project Development Plan, after the Building Permit has been issued, must be applied for by Developer/Owner in accordance with the follow process and authorization:
 - 5.3.1 The proposed change shall be submitted in writing as a Design Review request to the Planning & Building Department for review by the Administrator; and
 - 5.3.2 A written description shall be included which depicts all proposed modifications and all changes which are clearly indicated on the associated drawings; and
 - 5.3.3 Pursuant to KMC §17.96.030.A, the Administrator reviews the Design Review requests and determines whether the request must be reviewed by the Administrator or by the Planning and Zoning Commission for recommendation to the City Council of denial or approval of the request; and
 - 5.3.4 The recommendation of the Administrator or the Planning and Zoning Commission, as the case may be, is submitted to the City Council who shall conduct a public hearing and provide notice in accordance with the PUD approval process to decide the matter as the final administrative action of the City.

**SECTION 6
DEFAULT**

- 6.1 **Enforcement of Terms and Conditions of the Agreement.** The enforcement of the terms and conditions of this Agreement and the Permits issued by the City, excepting any terms and conditions which are based upon International Codes under the jurisdiction of the Building Official or the Fire Marshall, are as follows:

- 6.1.1 International Code Defaults:** The failure of the Developer/Owner, or the failure of the City to comply or perform, in accordance with the terms and conditions of this Agreement which involve conditions of Permits governed under International Codes shall be processed in accordance with the provisions of the applicable International Code involved.
- 6.1.2 All Other Defaults:** Otherwise the failure of the Developer/Owner, or the failure of the City to comply or perform, in accordance with the terms and conditions of this Agreement or the terms and conditions of any Permit which is the subject of this Agreement, shall be a Default of this Agreement (“Default”) and processed as follows:
- 6.1.2.1 City Default Claims.** A claim of Default by the City will be made by the City Project Development Plan Agent may be made against the Developer/ Owner.
- 6.1.2.2 Developer, Owner Default Claims:** A claim of Default may be made by the Developer/Owner against the City.
- 6.1.3 Claimant and Accused.** For purposes of this Section of the Agreement, a claim of Default is made by a (“**Claimant**”) against an (“**Accused**”).
- 6.1.4 Written Default Notice of Intent.** The Claimant shall serve the Accused with a Written Default Notice of Intent (“**Notice of Intent**”).
- 6.1.4.1** The Notice of Intent shall state the factual and legal basis for the claim of Default, the actions required to be taken by the Accused to cure the claim of Default and shall state the Default Cure Remedy that will be sought if the Default is not cured and a demand that the Accused respond in writing, within a reasonable stated time, as to whether or not the Accused consents to comply with the Notice of Intent or denies the claim of Default. The reasonable time frame shall depend upon the exigencies surrounding the matters and facts set forth in said Notice of Intent.
- 6.1.4.2** The Accused shall have a minimum of fourteen (14) days to remedy to the Notice of Intent.
- 6.1.4.3** The Notice of Intent shall be served as follows upon:
- **Developer:** by U.S. Mail to the address herein designated by Developer; and
 - **Owner:** by U.S. Mail to the address of its registered agent; and
 - **City:** by U.S. Mail to the address herein designated by the City.

6.1.5 Notice to Show Cause. In the event the Accused fails to correct and remedy a Notice of Intent, within the reasonable time designated in the Notice of Intent, to the satisfaction of the Claimant, the Claimant shall then request the City Council to proceed to set a hearing and provide written notice of the hearing to show cause to the Accused why the Claimant's Default Cure Remedy to cure the claim of Default as identified in the Notice of Intent should not be ordered.

6.1.5.1 The written notice of the hearing to show cause shall be served upon the Claimant and the Accused at least twenty-eight (28) days in advance of the hearing.

6.1.5.2 At the hearing to show cause, the Accused may present evidence as to why it or they are not in Default.

6.1.5.3 Following any presentation of evidence by the Accused and any rebuttal by the Claimant and any other interested persons, the City Council shall determine the matter and issue Findings of Fact, Conclusions of Law and an Order of Decision in accordance with the evidence presented at the Show Cause hearing.

6.1.5.4 The Findings of Fact, Conclusions of Law and Order of Decision issued by the City Council shall be the final administrative remedy of any claim of Default under this Agreement and the Parties may thereafter seek legal action in a court of competent jurisdiction for any legal or equitable remedy, including, without limitation, declaratory relief and or specific performance of this Agreement as the case may be, but the Parties shall not be entitled to consequential damages in any such action.

6.1.6 Prevailing Party. In the event any Party shall file suit or action at law or equity to interpret or enforce this Agreement, the provisions of Idaho Code Section 12-117, or any subsequent amendment or recodification of the same, shall apply to the determination of the prevailing Party and the award of reasonable attorney's fees, witness fees and other reasonable expenses.

SECTION 7 SHARED LEGAL DEFENSE OF THIS AGREEMENT

7.1 Shared Agreement Legal Defense Costs. In the event that any legal or equitable action or other proceeding is instituted by a third-party challenging the validity of any provision of this Agreement, the Parties will cooperate in defense of such action or proceeding. The City and the Developer may agree to select mutually agreeable legal counsel to defend such action or proceeding with the Parties sharing equally in the cost of such joint legal counsel, or each Party may select its own legal counsel at each Party's expense. All

other costs of such defense(s) shall be shared equally by the Parties. Each Party retains the right to pursue its own independent legal defense.

**SECTION 8
NOTICES AND FILINGS**

8.1 Manner of Serving. All notices, filings, consents, approvals and other communications provided for herein or delivered in connection herewith shall be validly delivered, filed, made, or served if in writing and delivered personally or delivered by a nationally recognized overnight courier or sent by certified United States Mail, postage prepaid, return receipt requested, if to:

City:

City Project Development Plan Agent
P.O. Box 2315
480 East Ave. N.
Ketchum, Idaho 83340

Developer:

PEG Ketchum Hotel, LLC
Attn: Nick Blayden
180 N. University Avenue, No. 200
Provo, Utah 84601

With a copy to:

William F. Gigray, III
WHITE PETERSON
5700 E. Franklin Rd., Suite 200
Nampa, ID 83687

Owner:

PEG Ketchum Hotel, LLC
Attn: Nick Blayden
180 N. University Avenue, No. 200
Provo, Utah 84601

or to such other addresses as either Party hereto may from time to time designate in writing and delivery in a like manner.

8.2 Mailing Effective. Notices, filings, consents, approvals and communication given by mail shall be deemed delivered immediately if personally delivered, 24 hours following deposit with a nationally recognized courier, or 72 hours following deposit in the U.S. mail, postage prepaid and addressed as set forth above.

**SECTION 9
DEVELOPER/OWNER ASSIGNMENT OF AGREEMENT RIGHTS**

9.1 Developer/Owner Assignment: The assignment of any of the Developer/Owner's rights and obligations of this Agreement shall in accordance with the following:

9.1.1 Complete Assignment of Developer/Owner's rights. A total assignment of the Developer/Owner's rights and obligations under this Agreement in connection with all undeveloped portions of the Subject Real Property shall be assigned subject to written consent of the City Council which shall not be unreasonably withheld, conditioned or delayed subject only to the following conditions:

9.1.1.1 Prior written notice from the Developer/Owner to the City Council together with the identification of the proposed assignee together with the proposed assignee's proof of their intentions and ability to perform and be bound to the conditions of this Agreement inclusive to provisions of Section 4 of this Agreement; and

9.1.1.2 Developer/Owner is not in Default of this Agreement or the Assignee tenders to the City a guarantee of the Assignee's performance of the Developer/Owner's Default upon assignment; and

9.1.1.3 The total assignment by the Developer/Owner shall be by a written instrument including the acceptance of the assignee to the terms and conditions of this Agreement, and the City Council's written consent and shall then be recorded in the official records of Blaine County, Idaho, expressly assigning such rights and obligations.

9.1.1.4 In the event of such total assignment of the Developer's Owner's rights and obligations hereunder, the Developer/Owner's liability under this Agreement shall then terminate.

9.1.2 Successors and Assigns. Notwithstanding any other provisions of this Agreement, the Developer/Owner may assign all or part of the Developer's or Owner's rights and duties under this Agreement as collateral to any financial institution from which the Developer/Owner has borrowed funds for use in Development of the Project. Such an assignment shall not relieve the Developer/Owner from any subsequent obligations of this Agreement.

SECTION 10 MISCELLANEOUS

10.1 Agreement runs with the Subject Real Property. The burdens of this Agreement for the Term of this Agreement and are binding upon, and the benefits inure to, all successors in interest of the Parties to this Agreement and constitute covenants that run with the Subject Real Property. Each commitment and restriction of this Agreement on the Subject Real Property shall be a burden on the Subject Real Property and shall be appurtenant to and for the benefit of the Subject Real Property and shall run with the land.

- 10.1.1** This Agreement shall be binding on the Developer and the Owner, and their respective heirs, administrators, executors, agents, legal representatives, successors, and assigns.
- 10.2 Agreement Amendment.** This Agreement may only be amended in accordance with the following process:

 - 10.2.1** An amendment may be proposed by a Party; and
 - 10.2.2** A proposed amendment must be in writing and include this entire Agreement as then existing and shall therein include a strikethrough of any language to be deleted and underline of any new language of the proposed amendment; and
 - 10.2.3** A proposed Amendment shall contain Statement of Purpose (which shall include a statement of how the Parties will be affected by the amendment); the Party to contact for information; the amended Agreement text; and
 - 10.2.4** City approval of a proposed Amendment must be processed in the same manner as the affected provision or provisions of the Agreement were originally approved subject to final approval of the City Council.
- 10.3 Choice of Law.** This Agreement shall be construed in accordance with the laws of the state of Idaho in effect on the Effective Date. Any action brought in connection with this Agreement shall be brought in a court of competent jurisdiction located in Blaine County, Idaho.
- 10.4 Construction.** All Parties hereto have either been represented by separate legal counsel or have had the opportunity to be so represented. Thus, in all cases, the language herein shall be constructed simply in accord with its fair meaning and not strictly for or against a Party, regardless of whether such Party prepared or caused the preparation of this Agreement.
- 10.5 Counterparts.** This Agreement may be executed in two or more counterparts, each of which shall be deemed an original, but all of which together constitute one and the same instrument. The signature pages from one or more counterparts may be removed from such counterparts and such signature pages all attached to a single document so that the signatures of all Parties may be physically attached to a single document.
- 10.6 Entire Agreement.** This Agreement constitutes the entire agreement between the Parties, except for the Permits and or approvals issued pursuant to this Agreement, pertaining to the subject matter hereof. All prior and contemporaneous agreements, representations and understandings of the Parties, oral or written, are hereby superseded and merged herein. No modification or amendment to this Agreement of any kind whatsoever shall be made or claimed by Developer/Owner or City shall have any force or effect whatsoever unless the same shall be endorsed in writing and signed by the Party

against which the enforcement of such modification or amendment is sought, and then only to the extent set forth in such instrument. Such approved amendment shall be recorded in the Official Records of Blaine County, Idaho.

10.7 Exhibits and Recitals. Any exhibit attached hereto shall be deemed to have been incorporated herein with the same force and effect as if fully set forth in the body hereof. The Recitals set forth at the beginning of this Agreement are hereby acknowledged and incorporated herein and the Parties hereby confirm the accuracy thereof. The Definitions set forth prior to the Recitals are hereby acknowledged and incorporated herein.

10.8 Further Acts. Each of the Parties shall promptly execute and deliver all such documents and perform all such acts as reasonably necessary, from time to time, to carry out the matters contemplated by this Agreement.

10.9 Good Standing; Authority. Each of the Parties represents to the other as follows:

10.9.1 Developer/Owner. Developer/Owner represents that it is a Utah limited liability company duly qualified to do business in Idaho; and

10.9.2 City. City represents that it is an Idaho municipal corporation in the state of Idaho; and

10.9.3 Authority. Each Party represents to the other that the individual(s) executing this Agreement on behalf of the Parties are authorized and empowered to bind the Party on whose behalf each such individual is signing.

10.10 Headings. This Agreement shall be construed according to its fair meaning and as if prepared by both Parties hereto. Table of Contents, titles and captions are for convenience only and shall not constitute a portion of this Agreement. As used in this Agreement, masculine, feminine or neuter gender and the singular or plural number shall each be deemed to include the others wherever and whenever the context so dictates.

10.11 Names and Plans. Developer/Owner shall be the sole owner of all names, titles, plans, drawings, specifications, ideas, programs, designs and work products of every nature at any time developed, formulated or prepared by or at the request of the Developer/Owner in connection with the Project Development Plan and the Project; provided, however, that in connection with any conveyance of portions of the Subject Real Property to the City, such rights pertaining to the portions of the Subject Real Property so conveyed shall be assigned to the City to the extent that such rights are assignable.

10.12 No Partnership; Third-Parties. It is hereby specifically understood, acknowledged and agreed that neither the City nor the Developer/Owner shall be deemed to be an agent of the other for any purpose whatsoever. It is not intended by this Agreement to, and nothing contained in this Agreement shall, create any partnership, joint venture or other arrangement between the Developer/Owner and the City. No term or provision of this Agreement is intended to, or shall, be for the benefit of any third-party, person, firm,

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organization or legal entity not a Party hereto, and no such other third-party, person, firm, organization or legal entity shall have any right to cause of action hereunder.

10.13 Obligation to Complete Development. The obligation of the Developer/Owner to complete any part or all of the Development of the Project within a specific time line, phasing schedule or other schedules, or any other plan, are provided in this Agreement as required as a condition of the Permits.

10.14 Parties' Intent. It is the Parties' express intention that the terms and conditions be construed and applied as provided herein, to the fullest extent possible. It is the Parties' further intention that, to the extent any such term or condition is found to constitute an impermissible restriction of the police power of the City, such term or condition shall be construed and applied in such lesser fashion as may be necessary to not restrict the police power of the City.

10.15 Recordation. After its execution, this Agreement shall be recorded in the real property records of Blaine County, Idaho by the City.

10.16 Severability. If any provision of this Agreement is declared void or unenforceable, such provision shall be severed from this Agreement, which shall otherwise remain in full force and effect.

10.17 Time of Essence. Time is of the essence in implementing the terms of this Agreement.

10.18 Waiver. No delay in exercising any right or remedy shall constitute a waiver by either Party thereof, and no waiver by the City or the Developer of the breach of any covenant or condition of this Agreement shall be construed as a waiver of any preceding or succeeding breach of the same or any other covenant or condition of this Agreement.

10.19 Agreement Term. The Term of this Agreement is effective upon the Effective Date and is thereafter perpetual unless terminated pursuant to Section 3.1.2.1 of this Agreement.

IN WITNESS WHEREOF, the Parties hereto, having been duly authorized, have executed this Development Agreement to be effective on the Effective Date.

CITY:

DEVELOPER:

CITY OF KETCHUM, Idaho, a municipal corporation organized and existing under the laws of the state of Idaho

PEG Ketchum Hotel, LLC

By: The PEG Ketchum Hotel, L.L.C., an Utah limited liability company,

By: _____
Neil Bradshaw, Mayor

By: _____
Nick Blayden, Manager

8/12/19

Attest:

By: _____
Robin Crotty, City Clerk

**CITY ATTORNEY APPROVAL AS TO
FORM AND AUTHORITY:**

The foregoing Agreement has been received by the undersigned attorney, who has opined that it is in proper form and within the power and authority granted under the laws of the state of Idaho to the City of Ketchum

OWNER:

PEG Ketchum Hotel, LLC, an Utah limited liability company

By: _____
Nick Blayden, Manager

Wm. F. Gigray, III, City Attorney

STATE OF IDAHO)
) ss.
COUNTY OF BLAINE)

On this ____ day of _____, 2019, before me, the undersigned, a Notary Public in and for said State, personally appeared **Neil Bradshaw**, known or identified to me to be the Mayor of the City of Ketchum, the municipal corporation that executed the instrument or the person who executed the instrument on behalf of said municipal corporation, and acknowledged to me that such municipal corporation executed the same.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal the day and year in this certificate first above written.

[seal]

Notary Public for Idaho
My Commission expires: _____

8/12/19

STATE OF _____)
) ss.
COUNTY OF _____)

On this ____ day of _____, 2019, before me, the undersigned, a Notary Public in and for said State, personally appeared **Nick Blayden**, Manager of PEG Ketchum Hotel, L.L.C., an Utah limited liability company, the limited liability company that executed the instrument, or the person who executed the instrument on behalf of said limited liability company, and acknowledged to me that such limited liability company executed the same.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal the day and year in this certificate first above written.

[seal]

Notary Public for _____
My Commission expires: _____

W:\Work\K\Ketchum, City of 24892\Gateway Hotel Development Proposal .015\Permit Acceptance Agreement\Permit Conditions Acceptance Agreement 8-12-19 lh.docx

8/12/19

EXHIBIT A
Ordinances

EXHIBIT A

8/12/19

EXHIBIT B
Project Development Plans

EXHIBIT B

8/12/19

EXHIBIT C
Approved Findings

EXHIBIT C

Planning & Zoning Commission Public Comments

https://ketchumidaho.org/sites/default/files/fileattachments/planning_amp_building/project/37921/all_comments_received_by_pz_7.15.19_to_9.11.19.pdf