



**City of Ketchum  
Planning & Building**

**KETCHUM FIRE STATION DESIGN REVIEW  
STAFF REPORT UPDATE  
KETCHUM PLANNING AND ZONING COMMISSION  
SPECIAL MEETING OF FEBRUARY 24, 2020**

**PROJECT:** Ketchum Fire Station

**FILE NUMBER:** P20-013

**APPLICATION:** Design Review

**REPRESENTATIVE:** Cole Architects

**OWNER:** City of Ketchum

**LOCATION:** 107 Saddle Road (Portion of Tax Lot 6689)

**ZONING:** Tourist (T)

**OVERLAY:** None

**NOTICE:** A public hearing notice for the project was mailed to all owners of property adjacent to the project site on January 22<sup>nd</sup>, 2020. The public hearing notice was published in the Idaho Mountain Express on January 22<sup>nd</sup>, 2020. The Planning & Zoning Commission moved to continue review of the application during their meeting on February 10<sup>th</sup>, 2020.

**REVIEWER:** Abby Rivin, Associate Planner

**BACKGROUND**

At the February 10<sup>th</sup> meeting, the Planning & Zoning Commission conducted a site visit and considered the fire station project plans, the applicant's presentation, and public comment. During deliberations, the Commission approved the architectural design of the building including the proposed materials. The applicant has proposed 2 small changes to the project plans: (1) a window has been removed at the rear elevation to address a neighbor's privacy concerns and (2) the carport height has decreased to 15 feet. These minor modifications do not noticeably alter the building design.

The Commission requested the applicant provide the traffic and circulation study, parking plans, and improve the landscaping. These materials are attached as Exhibit A to the Staff Report. These additions provide more evidence to support the project's compliance with all Design Review standards.

**TRAFFIC & CIRCULATION**

*Traffic shall flow safely within the project and onto adjacent streets. Traffic includes vehicle, bicycle, pedestrian and equestrian use. Consideration shall be given to adequate sight distances and proper signage (Ketchum Municipal Code §17.96.060.G3).*

Neighboring Highway 75, Saddle Road, and Warm Springs Road, the fire station site is centrally located with easy access to main routes that connect to all areas of the City. The circulation plan includes drive-through apparatus bays and sight windows that enhance safety while minimizing the fire station’s impact to neighborhood traffic.



Figure 1: Sight Window Example

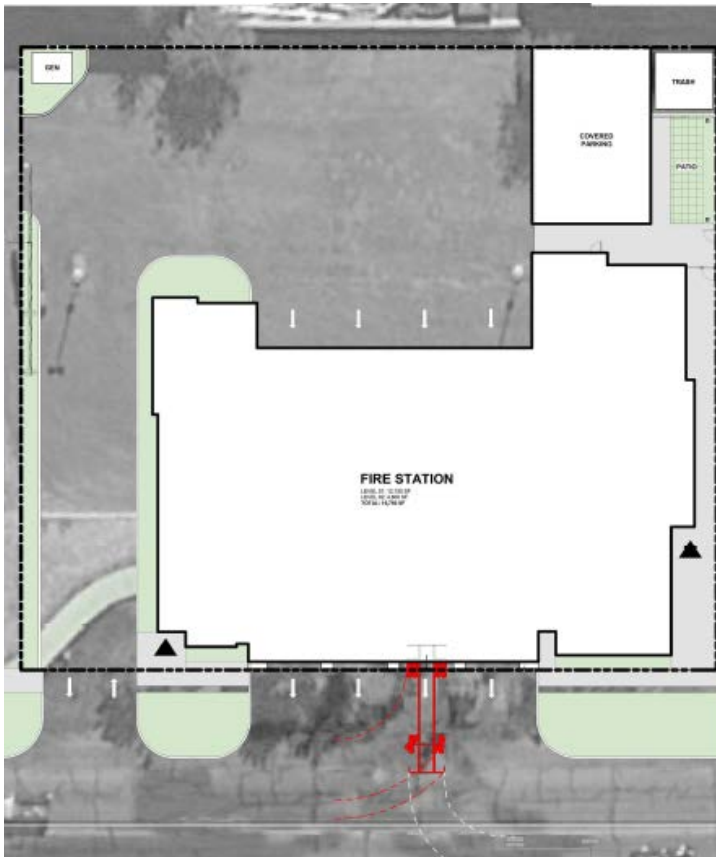


Figure 2: Turning Radius Diagram

The traffic and circulation study recommends additional safety features to reduce conflicts between emergency response and neighborhood traffic. These features include signs, devices, and policies to ensure the safety of pedestrians, bicyclists, drivers, and emergency responders. Staff recommends the Commission provide recommendations to the Ketchum City Council regarding the study’s proposed safety treatments. All encroachments, such as signs, within the public right-of-way associated with a development project require review and approval by the Ketchum City Council (Ketchum Municipal Code §17.96.030.C). The Commission recommendations regarding the traffic and circulation study and associated safety features may be memorialized through optional condition of approval #11.

The applicant has provided a diagram showing the turning radius for the largest fire truck

exiting onto Saddle Road. The turning radius diagram demonstrates that emergency responders will be able to exit the fire station onto Saddle Road safely.

## LANDSCAPE

*Landscaping is required for all projects. Landscape materials and vegetation types specified shall be readily adaptable to a site's microclimate, soil conditions, orientation and aspect, and shall serve to enhance and complement the neighborhood and townscape. All trees, shrubs, grasses and perennials shall be drought tolerant. Native species are recommended but not required. Landscaping shall provide a substantial buffer between land uses, including, but not limited to, structures, streets and parking lots. The development of landscaped public courtyards, including trees and shrubs where appropriate, shall be encouraged (Ketchum Municipal Code §17.96.060.I).*

The landscape plan is included on Sheets L1.00 and L1.50 of the project plans. The applicant has added nineteen more plants to augment the landscaping. The proposed landscaping will soften the fire station's hard lines and rectangular mass. Bordering the front property line, the tall grasses and flowering plants enhance the welcoming entryway to the fire station. The living trellis buffers the fire station from the Northwood Place apartments. The grasses, flowering plants, and shrubs complement the fire station's modern design without obstructing lines of sight needed to ensure traffic safety.

## PARKING

*Non-residential uses in the T-Zone require 1 parking space per 1,000 gross square with the additional exclusion of common and public areas (Ketchum Municipal Code §17.125.040).*

The Ketchum Fire Station is 17,507 gross square feet generating a parking demand of 18 parking spaces. The shared parking areas provide 100 spaces on-site to accommodate the fire station's parking demand. Parking for over 320 vehicles may be accommodated through on-street and off-site public parking spaces within the area. At the Commission's request, the applicant has provided a parking diagram showing the YMCA future expansion. The parking diagram shows the 150 on-site parking spaces that will be provided if the YMCA expands in the future. More than enough parking spaces are provided in the area to meet the needs of all users including the fire station and the YMCA.

## STAFF RECOMMENDATION

Staff recommends that the Commission consider the additional materials (Exhibit A), the project plans, the applicant's presentation, and any public comment received, deliberate, and approve the Ketchum Fire Station Design Review. The draft Findings of Fact, Conclusions of Law, and Decision have been included as Exhibit B.

## RECOMMENDED MOTION

"I move to approve the Ketchum Fire Station Design Review subject to conditions 1-10/conditions 1-11 and authorize the Chair to sign the Findings of Fact, Conclusions of Law, and Decision"

## RECOMMENDED CONDITIONS OF APPROVAL

1. This Design Review approval is subject to all comments and conditions as described in the City Department Comments (Table 2), the Zoning and Dimensional Standards Analysis including the off-street parking requirements subject to existing agreements between the City and the YMCA (Table 3), and the Design Review Standards Analysis (Table 4).
2. This Design Review approval is based on the plans and information presented and approved at the meeting on the date noted herein. Building Permit plans for all on-site improvements must conform to the approved Design Review plans unless otherwise approved in writing by the Planning and Zoning Commission or Administrator. Any building or site discrepancies which do not conform to the approved plans will be subject to removal.
3. All governing ordinances, requirements, and regulations of the Fire Department (2012 International Fire Code and local Fire Protection Ordinance No.1125), Building Department (2012 International Building Code, the 2012 International Residential Code, and Title 15 of Ketchum Municipal Code), Utilities Department, Street Department (Title 12 of Ketchum Municipal Code), and the City Engineer shall be met prior to Certificate of Occupancy.
4. The applicant shall submit final civil drawings prepared by an engineer registered in the State of Idaho to include specification for the ROW, utilities, and drainage improvements to be reviewed and approved by the City Engineer, Streets, and Utilities departments prior to issuance of a Building Permit for the project.
5. The term of Design Review approval shall be twelve (12) months from the date that the Findings of Fact, Conclusions of Law, and Decision are adopted by the Commission or upon appeal, the date the approval is granted by the Council subject to changes in zoning regulations (KMC §17.96.090).
6. All Design Review elements shall be completed prior to issuance of a Certificate of Occupancy for the building.
7. All exterior lighting on the property shall be in compliance with Ketchum Municipal Code, Chapter 17.132, Dark Skies, and shall be inspected by Planning Staff and approved prior the issuance of a Certificate of Occupancy for the building.
8. The project shall comply with the requirements of §17.124.040 Development Standards as adopted on the date a Building Permit is submitted for the project.
9. Prior to issuance of a Building Permit for the project, the applicant shall submit a construction management plans, which addresses each of the standards as set forth in Ketchum Municipal Code, Chapter 15.06 Construction Activity Standards.
10. In addition to the requirements set forth in this Design Review approval, this project shall comply with all applicable local, state, and federal laws.
11. OPTIONAL Planning & Zoning Commission recommendations to Ketchum City Council regarding the traffic and circulation study and safety treatments.

## EXHIBITS:

- A. Response to February 10, 2020 Planning and Zoning Commission Comments
- B. Draft Findings of Fact, Conclusions of Law, and Decision
- C. Ketchum Fire Station: Staff Report: Planning & Zoning Commission Meeting of February 10, 2020

Ketchum Fire Station Design Review

Staff Report UPDATE

Special Planning & Zoning Commission Meeting of February 24, 2020

**City of Ketchum Planning & Building Department**

Page 4 of 4

Exhibit A:  
Response  
to  
February 10, 2020  
Planning and Zoning  
Commission  
Comments



City of Ketchum  
City Hall

## Response to February 10, 2020 Planning and Zoning Commission Comments

The following information is provided in response to the Planning and Zoning Commission comments made at the February 10, 2020 meeting.

### Updated Building 3-D Perspective

- A revised perspective to reflect the proposed design and landscaping. (Attachment A)

### Landscape Plans

- Revised landscape plans showing the plant locations and plant type (Attachment B).

### Building Plans

- Revised elevation and floor plans showing the height of the rear carport and removal of a window at rear elevation of the project (Attachment C)

### Fire Truck/Engine Turning Radius

- Attachment D is a diagram showing the turning radius for the largest vehicle, the aerial ladder truck. As shown, the truck stays within the northbound lane when exiting north on Saddle Road. There is no conflict with southbound vehicles on Saddle Road.

### City and YMCA Parking Agreement

- Attachment E is the diagram showing over 170 on-site parking spaces if the YMCA expands.
- Attachment F is a copy of the Addendum and Amendment to the YMCA/City Parking Agreement.
- Approval of the fire station design review permit will not result in any conflict with the Agreement.

### Draft Traffic Analysis

- Attachment G is the preliminary draft of the traffic analysis. The analysis recommends consideration of fire station warning signs with flashing borders to be activated when an emergency vehicle is leaving the fire station.
- Two types of signs would be considered, one targeted at bicyclists/pedestrians and one targeted at drivers.



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PROJECT INFORMATION



KETCHUM FIRE  
STATION

107 SADDLE ROAD, KETCHUM, ID 83353

ISSUES

PHASE DESIGN REVIEW

DATE FEBRUARY 24, 2020

JOB NUMBER 18-022

MARK	DATE	DESCRIPTION

SHEET NAME

3D PERSPECTIVE

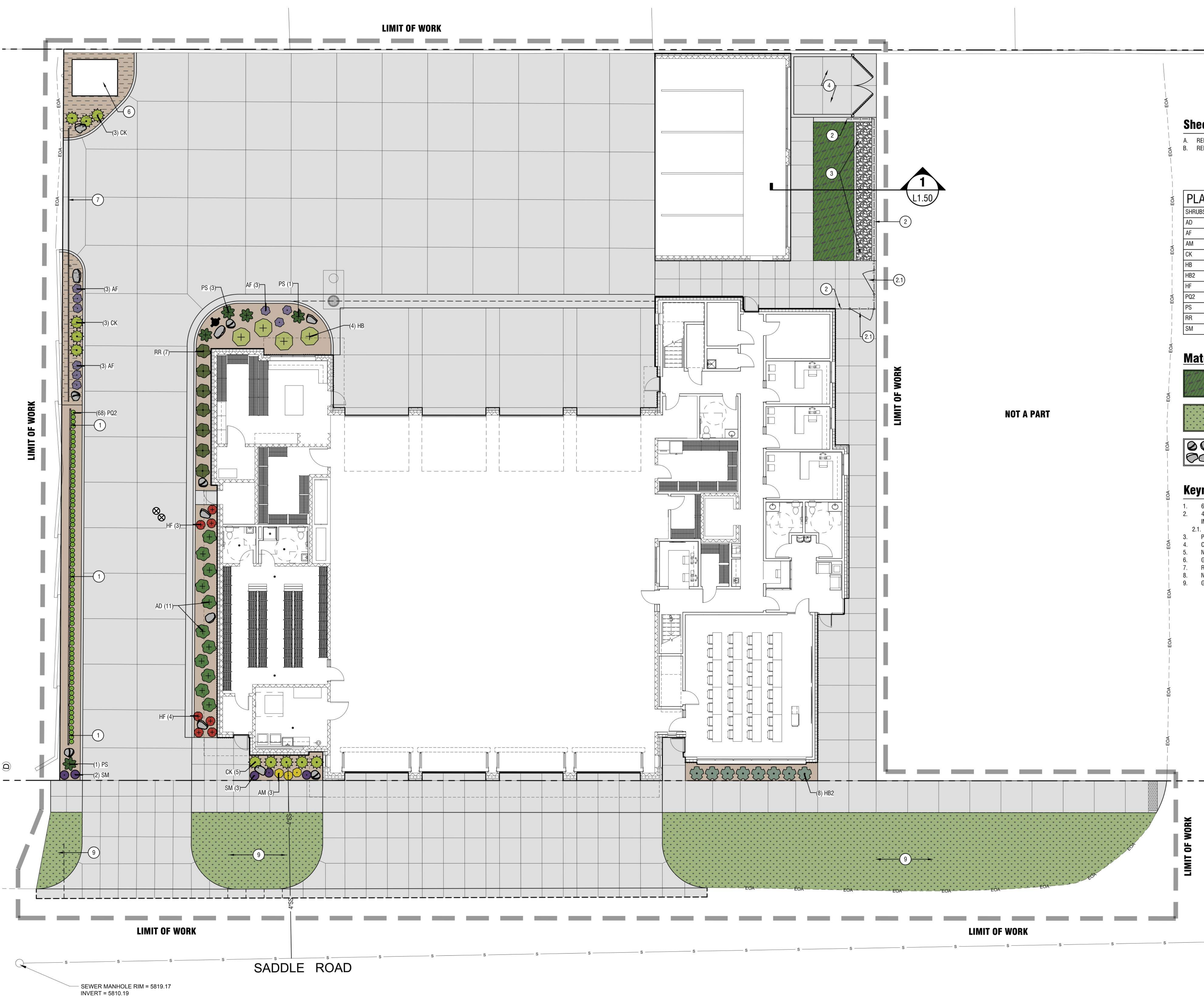
SHEET NUMBER

G0.20

## ATTACHMENT B



LIMIT OF WORK



**Sheet Notes:**

- A. REFER TO SHEET L1.50 FOR LANDSCAPE AND IRRIGATION NOTES.
- B. REFER TO SHEET L0.00 FOR EXISTING LANDSCAPE.

**PLANT SCHEDULE**

SHRUBS	BOTANICAL / COMMON NAME	SIZE	QTY
AD	ASTILBE X ARENDSII 'DEUTSCHLAND' / DEUTSCHLAND FALSE SPIRAEA	3 GAL.	11
AF	AGASTACHE X 'BLUE FORTUNE' / BLUE FORTUNE HYSSOP	1 GAL.	8
AM	ACHILLEA X 'MOONSHINE' / MOONSHINE YARROW	1 GAL.	3
CK	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / FEATHER REED GRASS	3 GAL.	8
HB	HYDRANGEA PANICULATA 'BOBO' / BOBO HYDRANGEA	5 GAL.	4
HB2	HELICTOTRICHON SEMPERVIRENS 'BLUE OATS' / BLUE OAT GRASS	1 GAL.	8
HF	HEUCHERA X 'FIRE CHIEF' / FIRE CHIEF CORAL BELLS	1 GAL.	7
PQ2	PARTHENOCISSUS QUINQUEFOLIA / VIRGINA CREEPER	1 GAL.	68
PS	PINUS MUGO 'SLOWMOUND' / MUGO PINE	5 GAL.	4
RR	RHAMNUS FRANGULA 'RON WILLIAMS' / FINE LINE FERN LEAF BUCKTHORN	5 GAL.	7
SM	SALVIA X SYLVESTRIS 'MAY NIGHT' / MAY NIGHT SAGE	1 GAL.	11

**Material Legend:**

- SYNTHETIC TURF - SEE DETAIL 3/L1.50.
- TURF SOD
- LARGE DARK GREY - TAN BOULDERS, 2-3' DIA. TYP. X 1'-2' HIGH.
- 3-IN DEPTH 4-IN - 6-IN ROUND RIVER ROCK. ROCK SHALL BE FREE OF FINES AND LESS THAN 2-IN IN SIZE.
- 3-IN DEPTH 1-IN CRUSHED DECORATIVE ROCK MULCH. COLOR: DARK TAN - GREY.

**Keynotes:**

- 1. 6-FT TALL FREE STANDING GREENSCREEN TRELLISING SYSTEM - SEE IMAGE 5/L1.50.
- 2. 4-FT TALL ONE INCH HORIZONTAL SLAT FENCE BY LANDSCAPE FORMS. COLOR: STORMCLOUD. SEE IMAGE 4/L1.50.
- 2.1. 5-FT WIDE GATE.
- 3. PATIO STRUCTURE POSTS AND STRUCTURE BY ARCHITECTS.
- 4. CMU TRASH ENCLOSURE.
- 5. NOT USED.
- 6. GENERATOR BY MECHANICAL.
- 7. ROLLING GATE.
- 8. NOT USED.
- 9. GRASS DRAINAGE SWALE.



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**PROJECT INFORMATION**



**KETCHUM FIRE STATION**  
107 SADDLE ROAD, KETCHUM, ID 83353

**ISSUES**

PHASE	25% DESIGN DEVELOPMENT
DATE	FEBRUARY 14, 2020
JOB NUMBER	18-022
MARK   DATE   DESCRIPTION	

**SHEET NAME**

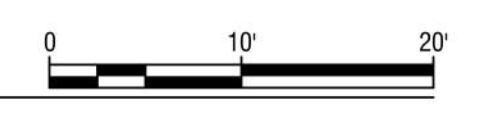
**LANDSCAPE PLAN**

**SHEET NUMBER**

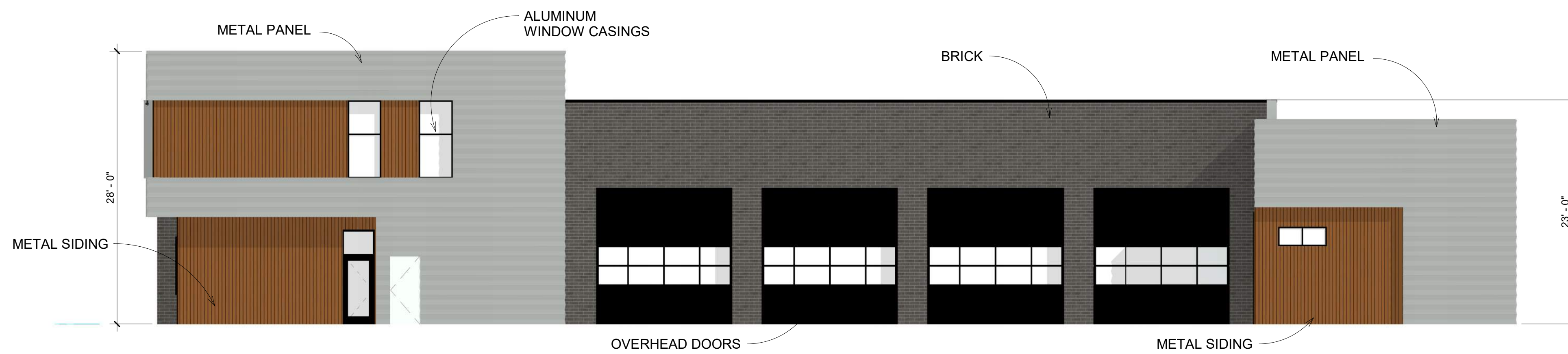
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SEWER MANHOLE RIM = 5819.17  
INVERT = 5810.19

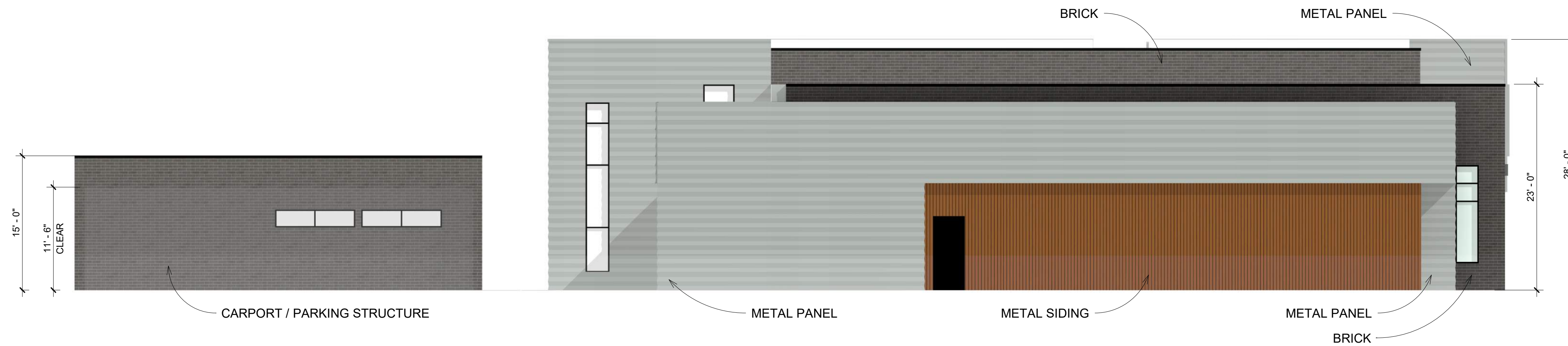
**DD-Landscape Plan**  
Horizontal Scale: 1" = 10'



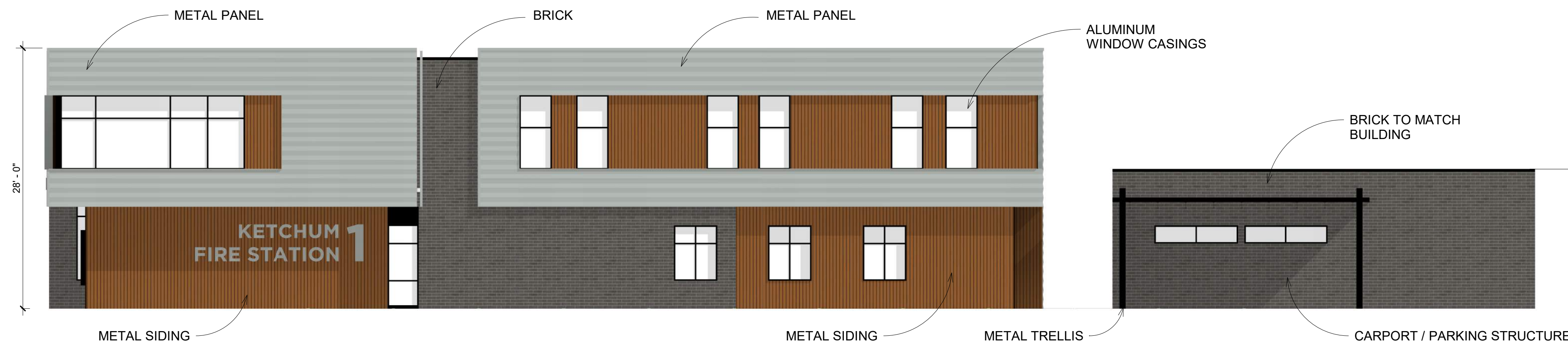
## ATTACHMENT C



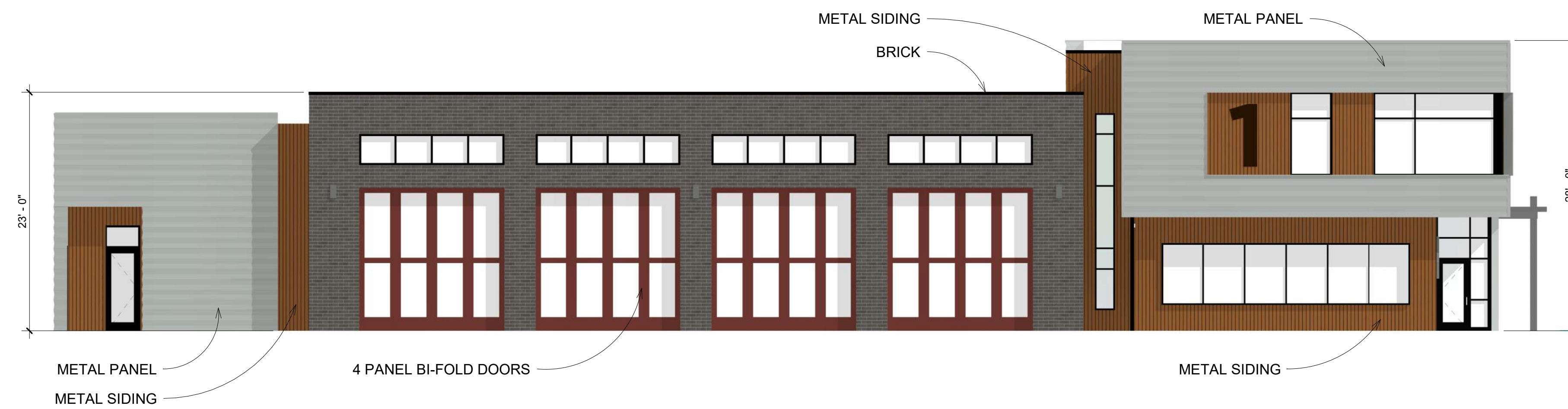
**1 EAST ELEVATION**  
SCALE | 1/8" = 1'-0"



**2 NORTH ELEVATION**  
SCALE | 1/8" = 1'-0"



**3 SOUTH ELEVATION**  
SCALE | 1/8" = 1'-0"



**4 WEST ELEVATION**  
SCALE | 1/8" = 1'-0"

**GENERAL NOTES**

- A. GENERAL NOTES APPLY TO ALL DRAWING SETS.
- B. FIELD VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS. NOTIFY ARCHITECT IMMEDIATELY IF ANY CONFLICTS OR DISCREPANCIES OCCUR BEFORE PROCEEDING WITH WORK.
- C. CROSS REFERENCES SHOWN ON DRAWINGS DO NOT NECESSARILY INDICATE ALL LIKE CONDITIONS AND DO NOT LIMIT APPLICATION OF ANY DRAWING OR DETAIL. WHERE SPECIFIC DIMENSIONS, DETAILS, OR DESIGN INTENT CANNOT BE DETERMINED, CONSULT THE ARCHITECT PRIOR TO PROCEEDING WITH WORK. IF DESIGN INTENT REMAINS UNCLEAR THEN PROVIDE MOST EXPENSIVE OPTION IN BID.
- D. DIMENSIONS ARE TO GRID LINE OR FACE OF STRUCTURAL MEMBER UNLESS OTHERWISE NOTED. DOOR & WINDOW OPENING DIMENSIONS ARE TO ROUGH OPENING OR CENTERLINE OF OPENING. CONCRETE AND BRICK DIMENSIONS ARE GIVEN TO THE FACE OF CONCRETE OR MASONRY AND TO THE FACE OF ROUGH OPENINGS.



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PROJECT INFORMATION



**KETCHUM FIRE STATION**

107 SADDLE ROAD, KETCHUM, ID 83353

ISSUES

PHASE DESIGN REVIEW

DATE FEBRUARY 24, 2020

JOB NUMBER 18-022

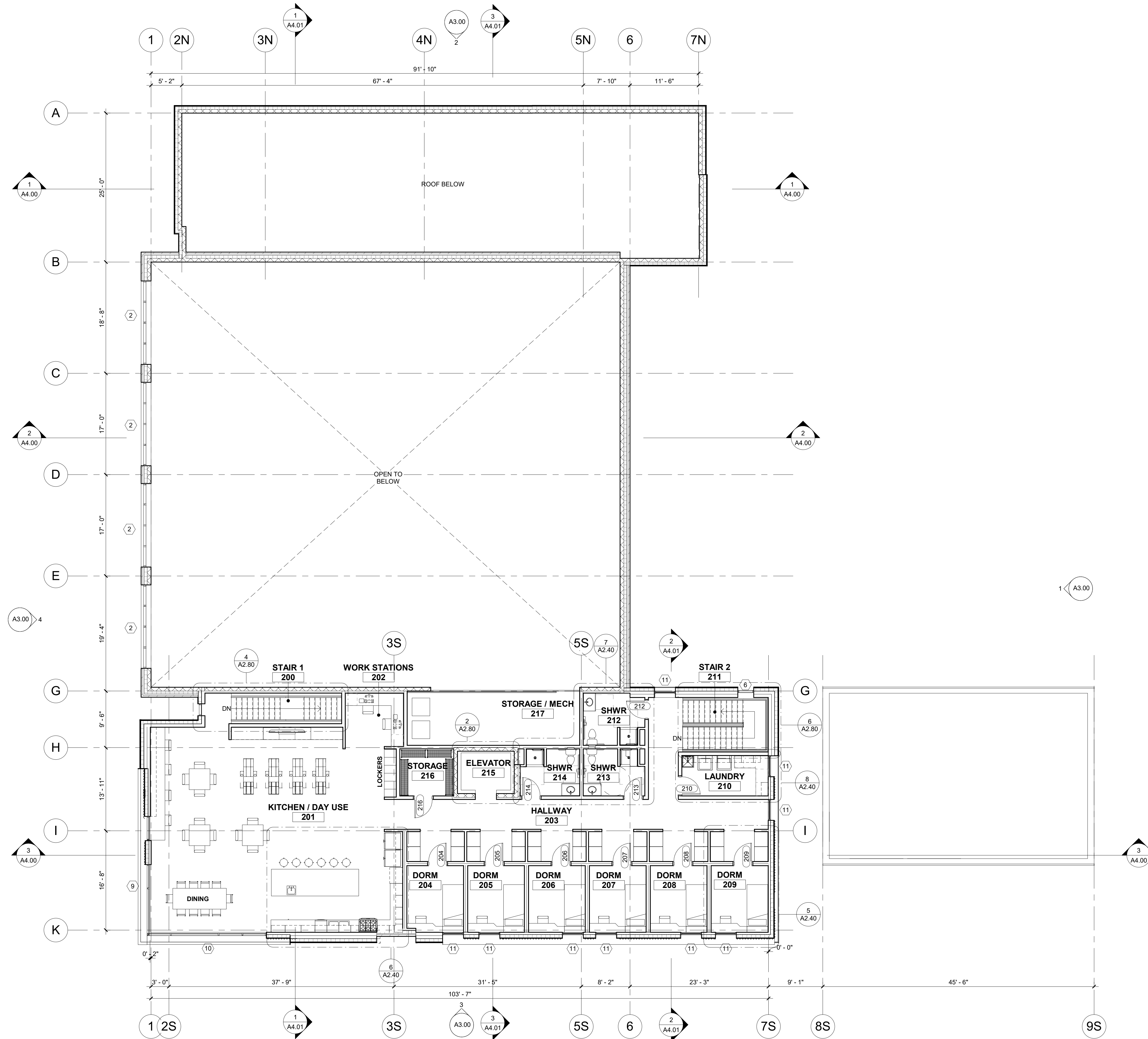
MARK	DATE	DESCRIPTION

SHEET NAME

**EXTERIOR ELEVATIONS**

SHEET NUMBER

**A3.01**



### GENERAL NOTES

- A. FIELD VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS. NOTIFY ARCHITECT IMMEDIATELY IF ANY CONFLICTS OR DISCREPANCIES OCCUR BEFORE PROCEEDING WITH WORK.
- B. CROSS REFERENCES SHOWN ON DRAWINGS DO NOT NECESSARILY INDICATE ALL LIKE CONDITIONS AND DO NOT LIMIT APPLICATION OF ANY DRAWING OR DETAIL WHERE SPECIFIC DIMENSIONS, DETAILS, OR DESIGN INTENT CANNOT BE DETERMINED. CONSULT THE ARCHITECT PRIOR TO PROCEEDING WITH WORK. IF DESIGN INTENT REMAINS UNCLEAR THEN PROVIDE MOST EXPENSIVE OPTION IN BID.
- C. PROVIDE SOLID BLOCKING IN WALLS FOR ALL WALL HUNG EQUIPMENT. BLOCKING TO MEET OR EXCEED MANUFACTURER'S RECOMMENDATIONS. FASTEN EQUIPMENT TO WALLS PER MANUFACTURER'S RECOMMENDATIONS. PROVIDE BLOCKING FOR, BUT NOT LIMITED TO THE FOLLOWING: MILLWORK, RAILING, FIRE EXTINGUISHER ACCESSORIES, WALL MOUNTED PLUMBING FIXTURES, SHELVING, ELECTRICAL EQUIPMENT, RESTROOM ACCESSORIES AND DISPLAY ITEMS.
- D. CAULK ALL COUNTERTOPS, BACKSPASHES AND CABINETS AT LOCATIONS WHERE THEY MEET WALLS. SEAL ALL CUT-OUTS IN COUNTERTOPS.
- E. ERECT DUST PROOF PARTITIONS AS REQUIRED BY OWNER TO PROTECT ADJACENT AREAS.
- F. SEAL AROUND ALL MECHANICAL AND ELECTRICAL EQUIPMENT PENETRATIONS AT WALLS. AT RATED WALLS USE A U.L. APPROVED FIRE-STOPPING MATERIAL.
- G. PROVIDE 4" DOOR JAMBS FROM FACE-OF-FINISH AT ADJACENT WALLS ON THE HINGE SIDE OR UNLESS OTHERWISE INDICATED.
- H. DIMENSIONS ARE TO GRID LINE OR FACE OF STRUCTURAL MEMBER UNLESS OTHERWISE NOTED. DOOR & WINDOW OPENING DIMENSIONS ARE TO ROUGH OPENING OR CENTERLINE OF OPENING. CONCRETE AND BRICK DIMENSIONS ARE GIVEN TO THE FACE OF CONCRETE OR MASONRY AND TO THE FACE OF ROUGH OPENINGS.
- I. WHEN APPLICABLE - UNLESS NOTED OTHERWISE, USE CDX PLYWOOD FOR EXTERIOR AND CONCEALED APPLICATIONS AND AB PLYWOOD FOR EXPOSED INTERIOR APPLICATIONS.

### KEYNOTES #

- 1. xx



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### PROJECT INFORMATION



### KETCHUM FIRE STATION

107 SADDLE ROAD, KETCHUM, ID 83353

### ISSUES

PHASE	DESIGN REVIEW	
DATE	FEBRUARY 24, 2020	
JOB NUMBER	18-022	
MARK	DATE	DESCRIPTION

### SHEET NAME

**FLOOR PLAN - LEVEL 02**

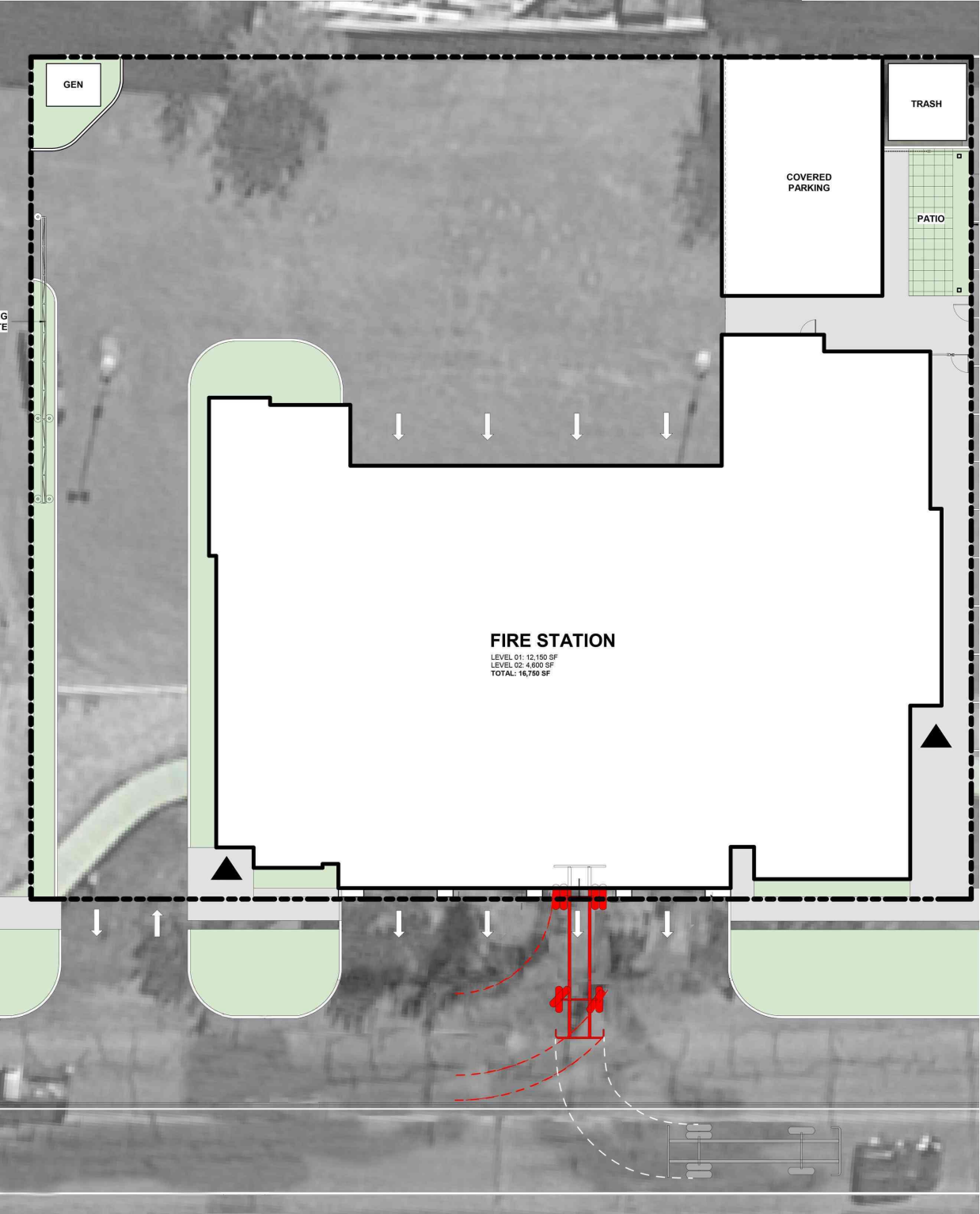
### SHEET NUMBER

**A2.02**

ATTACHMENT D

# KETCHUM ID FD SITE PLAN

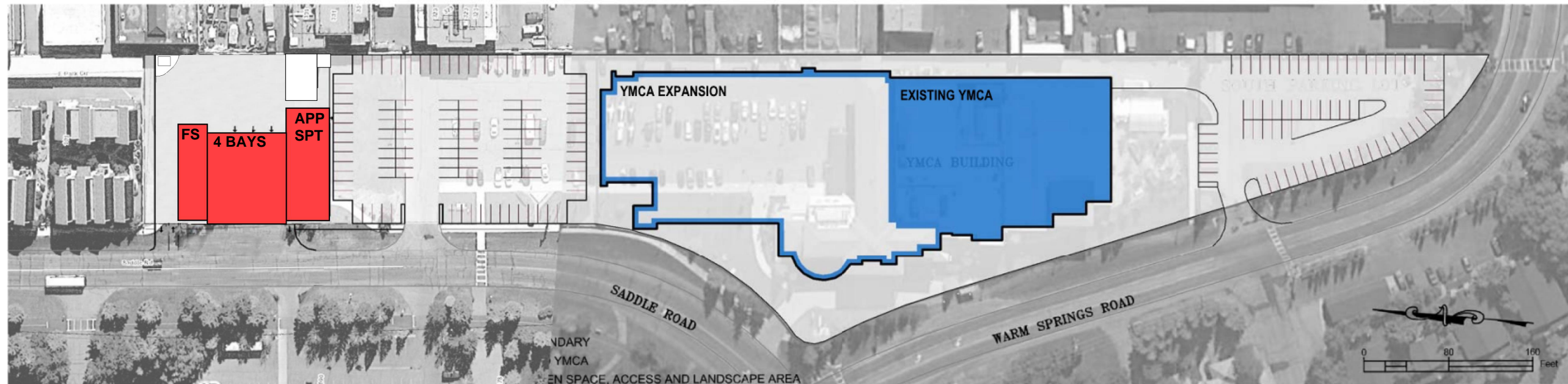
02/20/2020



ATTACHMENT E

## SITE PLAN - CONTEXT

02/20/2020



### 1 SITE PLAN - CONTEXT

Scale: 1" = 100'-0"

- EXISTING YMCA
- YMCA EXPANSION
- PROPOSED FIRE STATION



ATTACHMENT F

**ADDENDUM AND AMENDMENT TO PARKING AGREEMENT**

This ADDENDUM AND AMENDMENT is made and entered into this 4<sup>th</sup> day of November, 2019, by and between the City of Ketchum, a municipal corporation of the State of Idaho ("Lessor" or "City"), and Wood River Community Young Men's Christian Association, Inc., dba Wood River Community YMCA, an Idaho nonprofit corporation ("Lessee" or "YMCA").

- A. The City leases certain real property to the YMCA ("Leased Property") under the Lease Agreement, dated March 16, 2005, with a First Amendment to Lease, dated April 25, 2013 ("Lease"). The Parties also entered into a Parking Agreement, dated October 12, 2006, ("Parking Agreement") to address parking requirements and joint use arrangements in connection with the lease.
- B. The Parking Agreement covers parking on the Development Parcel, being all remaining property within the identified Park and Ride Lot not leased to the YMCA, and upon which certain joint parking uses were allowed pending future development by City. The YMCA relies upon the availability of sufficient public parking on the City land not leased to the YMCA.
- C. The City is pursuing potential development of a fire station ("City Development") which will impact the Development Parcel with respect to public parking availability. The City desires to minimize impact on the public parking also available to the YMCA. The YMCA supports the City Development and desires to come to an arrangement to accommodate the changing needs of the Parties.
- D. The YMCA is planning for expansion and further development of YMCA facilities as contemplated in the Lease. The City supports the YMCA and its mission in the community, and desires to come to an arrangement to accommodate the changing needs of the Parties.
- E. The Parties desire to continue the existing Lease and Parking Agreement with certain addendums and amendments to address changing needs and clarify the duties of the Parties.

Therefore, the Parties agree to further addend and amend the Lease Agreement, as previously amended, and the Parking Agreement, as follows:

1. Continued Use of Development Parcel for YMCA Parking. The City will continue to permit the YMCA to jointly use public parking facilities on the Development Parcel for the YMCA's parking requirements as generally contemplated within the Lease and Parking Agreement. In the event of any inconsistency between this Addendum and Amendment and the Parking Agreement or Lease this Addendum and Amendment shall govern. Except as amended by this Addendum and Amendment the Lease and Parking Agreement are ratified and affirmed.

**2. Vicinity Parking.**

The Parties have identified 150 parking spaces as the number of spaces reasonably necessary for the YMCA to continue to generally serve its patrons and fulfill its mission. In consideration of the ongoing cooperative efforts and support between the City and YMCA the City will maintain public parking available for YMCA non-exclusive use within the Vicinity of the YMCA. The Vicinity, for purposes of this Addendum and Amendment, shall be the area as identified on Exhibit A.

Upon completion of the City Development, the City will, at no required expense to the YMCA, develop, manage, and provide public parking in the Vicinity in the amount of at least one-hundred and fifty (150) on-site, on-street, or off-street public parking spaces or other equivalent parking facilities, which the Parties mutually agree are sufficient to ensure that the YMCA reasonably has access to public parking spaces for its ordinary and routine use.

The Parties understand that retaining a certain level of parking more proximate to the YMCA facilities is also desirable. Therefore, to preserve such proximity, at least one-hundred (100) of such public parking spaces will be located On-Site. "On-Site" is defined as illustrated by Exhibit B hereto.

At the time of a building permit for the contemplated YMCA expansion, the City will, at no required expense to the YMCA, increase the development, management, and provision of public parking to at least two-hundred (200) public parking spaces or equivalent parking facilities within the Vicinity. The commitment for On-Site spaces will increase to one-hundred and fifty (150) spaces. The definition of "On-Site" at this time will be expanded to include the additional area illustrated on Exhibit C hereto.

The Parties understand that unique circumstances, special events, and non-routine events will occur and will not be considered interference with the routine provision of generally available public parking.

The Parties understand and agree that ADA compliant spaces will be included to the extent required by law. The Parties also understand and agree that at the time of this Addendum and Amendment these commitments will not include compact car spaces, and that the Parties shall work cooperatively to address changing automobile and parking habits over the course of the Lease.

- 3. No Designated or Committed Parking.** The Parties understand and agree that all public parking developed by the City shall be open and available to general public use. None of the On-Site public parking spaces are hereby designated to, reserved, or committed solely to the YMCA.
- 4. Ongoing Cooperative Efforts; Reservation of City Rights.** The Parties understand and agree that this Addendum and Amendment is a result of the designated City Development. The Parties acknowledge and agree the City Development is dependent on the electorate's approval of the bond measure on November 5, 2019 which approval is an express condition precedent to the rights and obligations of the Parties hereunder. In the event the bond measure is not approved this Addendum and Agreement will be deemed as without effect and in which case neither Party shall have any further rights or obligations hereunder. Subject to the terms and conditions of this Addendum and

Amendment, the City reserves all general rights to provide, manage, and maintain public parking under its municipal powers. The Parties will endeavor to maintain a cooperative and collaborative effort in addressing parking needs for future development.

This Addendum shall be effective as of the later date of execution below.

CITY OF KETCHUM

WOOD RIVER COMMUNITY YOUNG MEN'S  
CHRISTIAN ASSOCIATION, INC.

  
\_\_\_\_\_  
Neil Bradshaw, Mayor

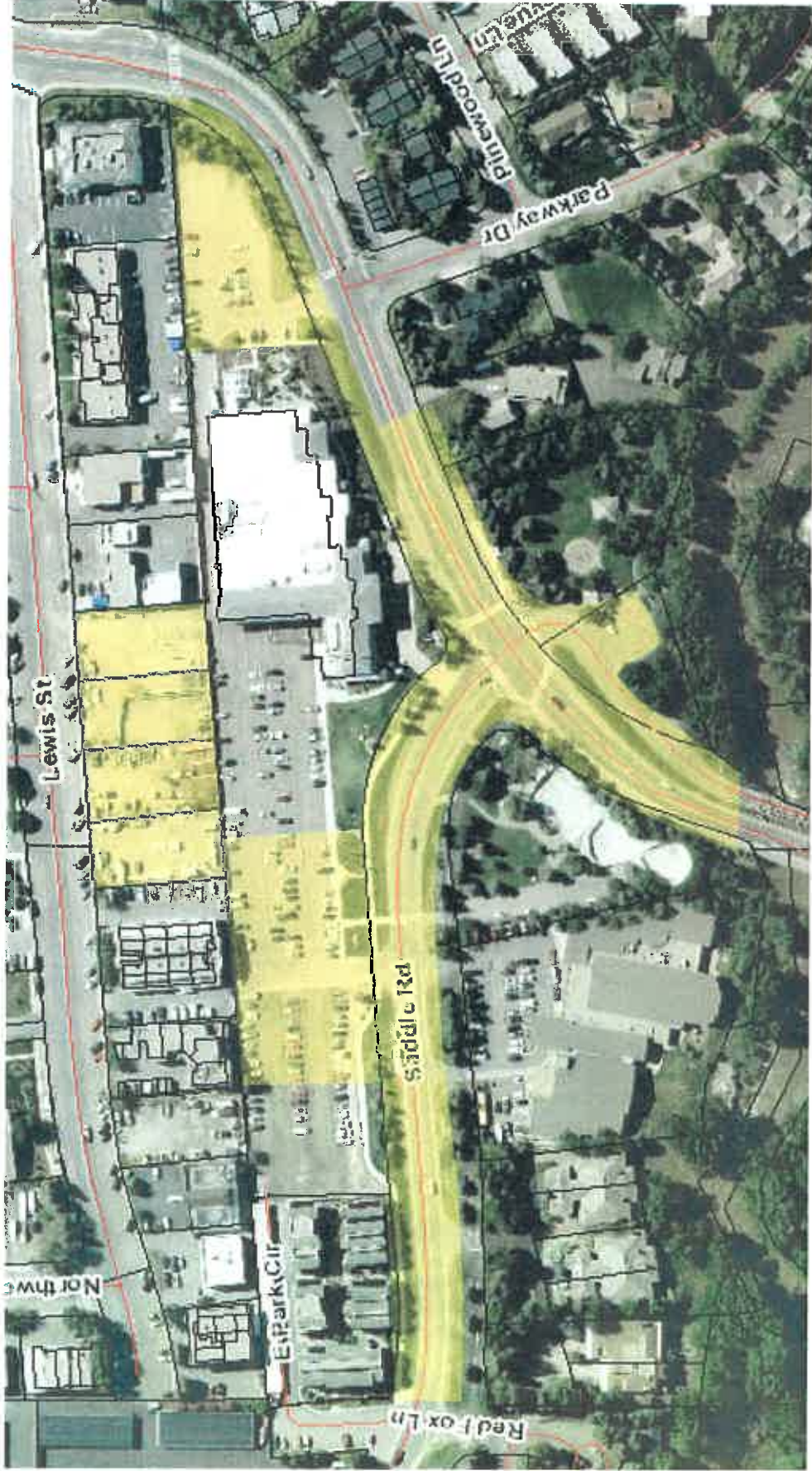
Date: 11/4/2019

  
\_\_\_\_\_  
John Dondero, Board Chair

Date: 11/4/2019

Attest:   
\_\_\_\_\_  
Robin Crotty, City Clerk

# ADDENDUM and AMENDMENT EXHIBIT A



**Legend**  
Vicinity

# ADDENDUM and AMENDMENT EXHIBIT B



## Legend

 On-site location for 100 parking spaces prior to YMCA expansion

# ADDENDUM and AMENDMENT EXHIBIT C



## Legend

On-site parking location for 150 parking spaces after YMCA expansion

## ATTACHMENT G



**Project name: Ketchum Fire Station  
Traffic Study**

**Project ref: 60610668**

**From: Kordel Braley, PE, PTOE; Daniel  
Helms, PE, PTOE, RSP<sub>21</sub>**

**Date:**  
February 20, 2020

**To: Suzanne Frick, City Administrator, City  
of Ketchum**

# Memo

On behalf of the City of Ketchum, AECOM has reviewed plans for a proposed fire station to be located on the east side of Saddle Road approximately 400 feet north of Warm Springs Road. AECOM's review focused primarily on the transportation and safety impacts of the proposed fire station at surrounding intersections and pedestrian/trail crossings.

The purpose of this memo is to evaluate existing conditions, potential risks, and mitigation measures. This analysis does not provide final design. No new data were collected as part of this analysis. AECOM has relied on data provided by the City and its partners.

## Existing Conditions

- Two transit stops near the Fire Station (Warm Springs YMCA and Warm Springs at Parkside) generate, on average, 37 and 32 boardings per day, respectively, based on daily spot counts. Assuming these are doubled to account for alighting, the average number of pedestrians per hour generated from these stops is only 4 and 3, respectively, assuming uniform arrivals and departures throughout the day.
- While no pedestrian crossing data were provided, the area surrounding the proposed fire station includes several pedestrian generators including YMCA, Rotary Park, the Guy Coles Skate Park, Big Wood School, Ernest Hemingway School, and multiple trails.
- The intersection of Warm Spring Road and Highway 75 currently has Opticom.
- Traffic counts collected in May 2018 for the intersection of Warm Springs Road and 10<sup>th</sup> Street were provided by the City and show a significant number of trips on Warm Springs Road (nearly 1,000 vehicles per hour during peak periods). Counts for Saddle Road were not available. Qualitatively, both of these streets would appear to be important routes to the transportation network for this part of Ketchum.

## Fire Station Operations

- According to data from the City, the fire department responded to 1,019 calls in 2019, which averages to approximately 20 calls per week.
- Based on discussions with the fire department, it is estimated that the egress distribution from the fire station would likely be 1 to 2 calls per week west on Warm Springs Road, 10 to 11 calls per week east on Warm Springs Road, and 7 to 8 calls per week north on Saddle Road.

## Mitigation Measures

Because of the random nature of emergency vehicle deployment, as well as the fact that emergency responders are trained professionals (as opposed to average drivers), an Intelligent Transportation System (ITS) solution could be considered as a mitigation strategy to address concerns to lessen the opportunity of vulnerable road user and emergency vehicle interaction. The ITS solution would be considered lower cost, as opposed to infrastructure improvements such as grade separation. Additional infrastructure improvements may be warranted based on typical traffic conditions, but this would require additional data collection efforts.

- A system can be installed that includes Fire Station Warning signs (W11-8) with flashing borders and/or flashing beacons to be activated when an emergency vehicle is leaving the fire station (see Figures 1 and 2). The signs would be deployed in strategic locations (such as trail crossings, locations near the Big Wood School or the YMCA, near transit stops, and on Saddle Road and Warm Springs Road, see Figure 3) that would alert pedestrians and bicyclists to the presence of an emergency vehicle.<sup>1</sup> Smaller signs are recommended for pedestrian/bicyclist applications.

## Infrastructure Considerations

Other infrastructure considerations include the following:

- Solar Power – Most devices can be energized using solar power. However, site specific evaluations will need to be performed at each location to ensure appropriate levels of sun are available to consistently operate the devices. Hard-wired power will be required for locations where sun light cannot be readily captured.
- Opticom – In order to make the system work automatically, Opticom is recommended. A receiver could be located directly across the street from the fire station garage door so the system can be activated when the doors open and the emergency vehicle's system is first activated. A back-up activation system could also be placed within the fire station, at the dispatch center, and/or in each emergency vehicle (see Figure 2).

## Operational Mitigation Measures

Operations mitigation measures could also be considered including:

- By policy, the fire department could mandate all, or nearly all, emergency vehicles travel north on Saddle Road to SH-75, instead of travelling east on Warm Springs Road. This would remove nearly all conflicts with emergency vehicles at the sensitive intersection locations along Warm Springs Road. According to data from the fire department, using this alternative route would add approximately 0.4 miles to a response south of Warm Springs Road/Main Street but only add about 30 seconds. It is outside of our expertise to weigh in on the merits and costs of this type of operational change. If the fire department did make this change, it would significantly reduce conflicts on Warm Springs Road.
- The fire department should maintain a strict policy against volunteers responding to the fire station in an unsafe manner.

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<sup>1</sup> A variety of vendors and systems are available. AECOM does not advocate for specific manufactures. Manufacture specifications should be considered in the design of any implemented system.

## Exhibits<sup>2</sup>

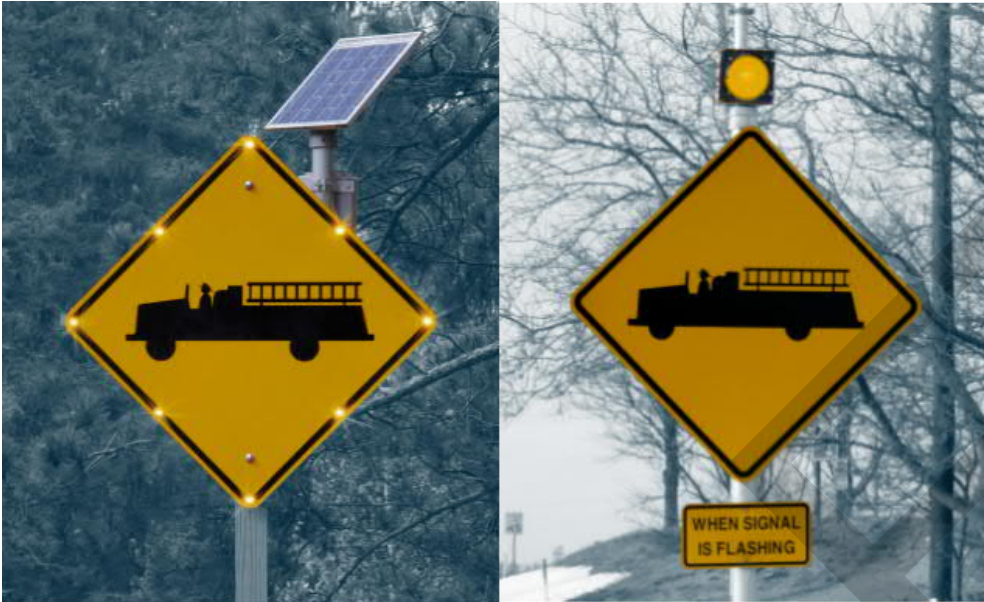


Figure 1. Fire station warning signs with border flashers or beacons.<sup>3</sup>



Figure 2. Sample manual activation systems.<sup>3</sup>

<sup>2</sup> A variety of vendors and systems are available. AECOM does not advocate for specific manufactures. Manufacture specifications should be considered in the design of any implemented system.

<sup>3</sup> <https://www.tapconet.com/product/emergency-vehicle-warning-system#overview>

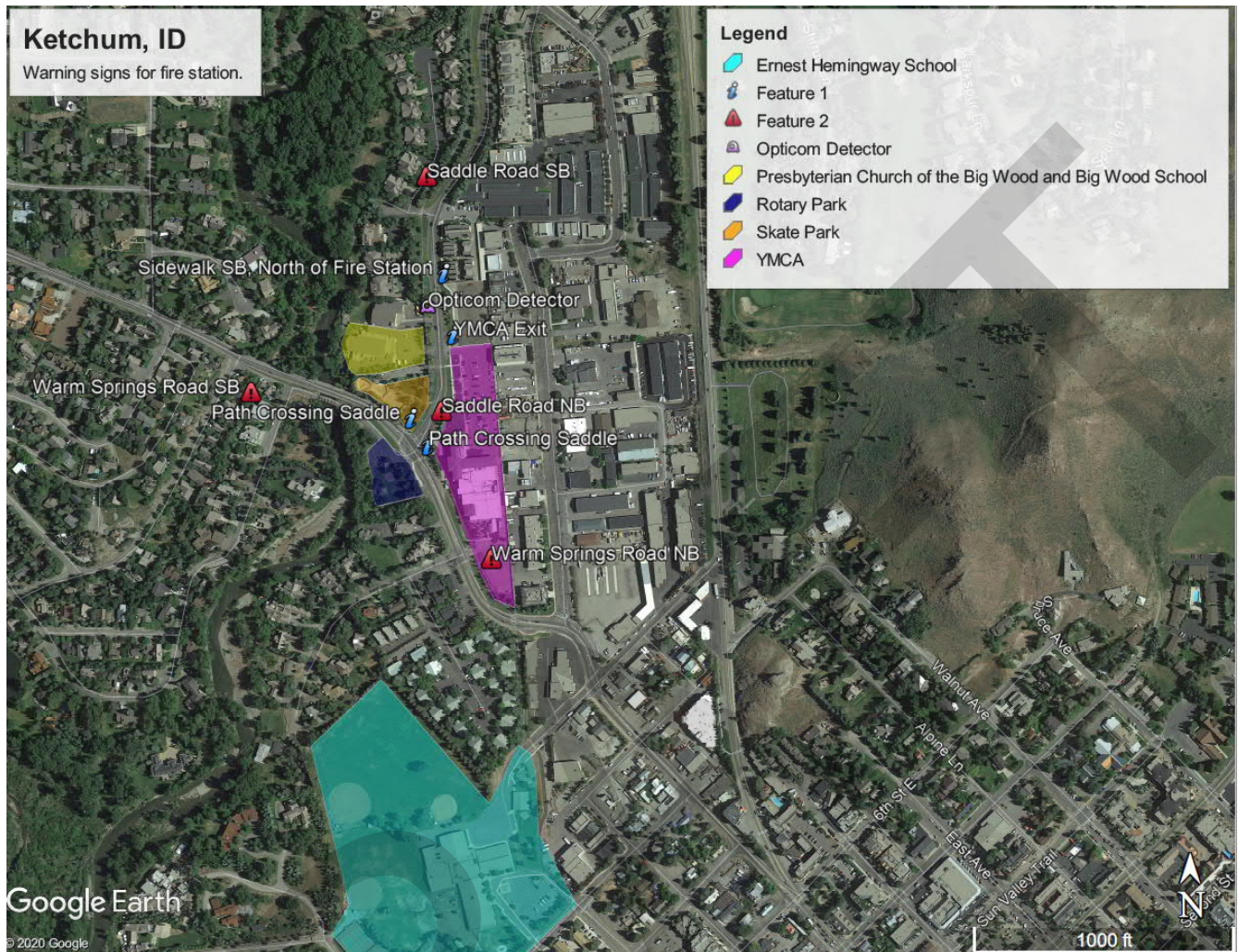


Figure 3. Conceptual layout of warning sign locations that warn drivers and pedestrians/bicyclists.

Exhibit B:

Draft

Findings of Fact, Conclusions of  
Law, and Decision



City of Ketchum  
Planning & Building

KETCHUM FIRE STATION DESIGN REVIEW  
STAFF REPORT  
KETCHUM PLANNING AND ZONING COMMISSION  
REGULAR MEETING OF FEBRUARY 10, 2020

PROJECT: Ketchum Fire Station

FILE NUMBER: P20-013

APPLICATION: Design Review

REPRESENTATIVE: Cole Architects

OWNER: City of Ketchum

LOCATION: 107 Saddle Road (Portion of Tax Lot 6689)

ZONING: Tourist (T)

OVERLAY: None

NOTICE: A public hearing notice for the project was mailed to all owners of property adjacent to the project site on January 22<sup>nd</sup>, 2020. The public hearing notice was published in the Idaho Mountain Express on January 22<sup>nd</sup>, 2020.

REVIEWER: Abby Rivin, Associate Planner



Figure 1: PUD Parcel Tax Lot 6689

**FIRE STATION LOCATION**

The new fire station is located on a portion of a larger parcel owned by the City within a Planned Unit Development (PUD) in the Tourist Zone. The PUD includes the Northwood Place apartments, the YMCA, and shared public parking areas all on the same parcel. The fire station site area is 15% of the PUD area with over 150 feet of street frontage along Saddle Road.

The neighborhood connects residential, commercial, recreational, and industrial development. A recreation hub, the multi-use path connects the YMCA to Guy Coles Skate Park and Rotary Park. Across Saddle Road, a townhome development extends north from the Church of the Big Wood.

Industrial buildings, including the Fire Training Facility on Lewis Street, border the east side of the PUD.

The project location and site area meet the Fire Department’s operational needs. The Future Land Use Map designates the parcel as Public/Quasi-Public for community services, utilities, and government buildings like the proposed fire station. The City-owned parcel is one of only four properties within the City designated for this type of essential facility. The other Public/Quasi-Public properties are developed with the Community Library, Post Office, and the Wastewater Treatment Plant.

### PROJECT COMPLIES WITH DESIGN REVIEW STANDARDS



Figure 2: Fire Station Rendering by Cole Architects

The fire station design balances multiple discordant functions, including living quarters, apparatus and equipment storage, maintenance, decontamination facilities, and administrative offices and training rooms. Occupied by personnel in continuous 24-hour shifts, the fire station design must balance

these different activities while maintaining a healthy and safe environment. The building layout and site design organizes similar functions within distinct areas—the apparatus bays at the center of the station separate the operations and maintenance area from the administrative offices, public meeting room, and residential areas. The orientation and placement of rectangular building shapes facilitate this functional arrangement. The modern design refashions traditional materials like brick to augment visual interest. Details like the red trim across the apparatus-bay windows allude to more traditional firehouse design.

### PARKING & TRAFFIC CIRCULATION

By Highway 75, Saddle Road, and Warm Springs, the fire station is centrally located with easy access to main routes that connect to all areas of the City. The circulation plan includes drive-through apparatus bays and sight windows that enhance safety while minimizing the fire station’s impact to neighborhood traffic. The City has engaged a professional traffic engineer to identify safety concerns or circulation conflicts. Any improvements necessary to ensure the safety of pedestrians, bicyclists, drivers, and emergency responders will be included in the project plan.

The shared parking areas within the PUD contain 220 public parking spaces. The parking spaces are unrestricted and shared between multiple users. Instead of restricting spaces to an exclusive use, sharing spaces satisfies parking demand by flexibly accommodating various uses during different times of the day. More than enough parking spaces are provided in the PUD to meet the needs of all users including the fire station parking demand.

**STAFF RECOMMENDATION**

Staff recommends that the Commission consider the project plans, the applicant’s presentation, and any public comment received, deliberate, and approve the Ketchum Fire Station Design Review.

**ANALYSIS**

Staff analysis is provided in Tables 1 through 4 including: (1) the project’s alignment with the 2014 Comprehensive Plan, (2) City Department comments, (3) adherence zoning and dimensional standards, and (4) evaluation of Design Review criteria. Project plans are attached as Exhibit A. The *Response to Planning and Zoning Commission Comments on the Fire Station Pre-Application Design Review* included as Exhibit B provides additional information regarding the site selection process, shared parking area, employee housing considerations, and traffic circulation.

**Table 1: Comprehensive Plan Analysis**

A critical facility that will support the future growth of Ketchum, the new fire station is an essential facility needed to sustain our vibrant mountain town. Providing fire protection and emergency medical services, the Fire Department preserves and enhances quality of life for Ketchum residents and visitors. The 2014 Comprehensive Plan identified the construction of a new, modern fire station as the department’s highest priority. By protecting life and property, the Fire Department provides the foundation to support our community vision and shared values. The fire station is consistent with the uses, goals, and policies listed below as specified within the 2014 Comprehensive Plan.

SUPPORTING SECTION	COMPLIANCE WITH THE 2014 COMPREHENSIVE PLAN & STAFF COMMENTS
<p>Land Use Category: Public/Quasi Public</p>	<p><b>PRIMARY USES</b> The Public/Quasi-Public category includes uses related to community services, such as schools, libraries, community centers and gardens, arts/culture, hospitals, government, utilities, cemeteries, and places of worship.</p> <p><i>The fire station is a public facility that delivers an essential service to the community. <u>The project site is one of only four properties within the City designated as an appropriate location for this type of use.</u> The remaining Public/Quasi-Public properties are developed with the Community Library, Post Office, and the Wastewater Treatment Plant.</i></p>
<p>Future Land Use</p>	



Infill and Redevelopment to Accommodate Growth	<i>Ketchum needs a new fire station to sustain our vibrant community and support future growth. As first responders to fires, public safety and medical emergencies, and technical rescues, the Fire Department protects the lives and property of The fire station is a critical facility needed to deliver essential services to protect the community. A vacant and underutilized property within a built-out area, the project site is strategically located for emergency response near major transportation corridors. As Ketchum continues to grow, the Fire Department must respond to increasing demands for services. The fire station is necessary to support the future growth of Ketchum.</i>
<b>A Strong and Diverse Economy</b>	
Policy E-2(b) Quality of Life Infrastructure	<i>The Fire Department keeps Ketchum dynamic, resilient, and economically viable. By enhancing delivery of fire protection and emergency services, the fire station project is critical to Ketchum’s economic success.</i>
<b>Community Design and Neighborhoods</b>	
Policy CD-1.3 Compatible Infill and Redevelopment Projects	<i>The fire station is contextually appropriate located in a neighborhood at the intersection of commercial, industrial, and residential development.</i>
Policy CD-1.5 High-Quality and Sustainable Design Principles for Public Buildings and Public Outdoor Spaces	<i>Centrally located, the fire station is sited near main routes to serve Ketchum residents and visitors. The project plans include a community room to host public meetings and events.  The fire station will be a LEED certified building.</i>
Policy CD-2.5 Energy and Water Efficiency in New Development	<i>The fire station will be a LEED certified building to ensure high performance standards for energy and water conservation.</i>
<b>Natural Resource Stewardship</b>	
Policy NR6.1 City Leadership in Energy Efficiency and Conservation	<i>The City of Ketchum will serve as an example by continuing to improve energy efficiency and GHG reductions in City facilities and operations.</i>
Policy NR6.2 Energy Alternatives	<i>The City should implement policies and programs that enhance opportunities for individuals, businesses, and public organizations to conserve energy and convert to renewable resources. The City should support energy conservation in City buildings, vehicles, operations and processes through its own policies, and provide information about techniques for energy efficiency.</i>
<b>Public Safety and Utilities</b>	

Goal PSU-1	Ketchum will proactively ensure the safety and welfare of residents by providing high levels of police, fire, and emergency response services. In addition, the City will promote community awareness and involvement with crime prevention and code enforcement. A primary objective is to provide and maintain reliable funding to protect investment in existing fire and police facilities and ensure appropriate levels of service.
Policy PSU-1.3 Fire and Police Service Standards	Future fire and police facility requirements should be determined using level of service (LOS) standards. Level of service should be defined as the optimum level of service desired from the service provider and may differ from the current LOS.
Policy PSU-3.2 Channeling Capital Improvements	The fire station project provides a safety-related capital improvement.

**Table 2: City Department Comments**

City Department comments are based on the project concept as proposed with the Design Review project plans. All City Departments shall review and approve the project through the Building Permit application process. All comments pertaining to the Design Review drawings are subject to change. All right-of-way improvements must be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.

City Department Comments
<i>All City Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project.</i>
<p><b>Fire Department:</b></p> <ul style="list-style-type: none"> <li>• It is the General Contractor’s responsibility to understand and adhere to all Fire Protection Ordinance #1125 requirements in addition to any and all other City of Ketchum requirements in effect at the time of Building Permit issuance. Failure to comply with all local ordinances and codes may result in project work stoppage as well as criminal penalties.</li> <li>• The above project shall meet all 2012 International Fire Code requirements in addition to specific City Building and Fire Ordinances.</li> <li>• Approved address numbers shall be placed in such a position to be plainly visible and legible from the road fronting the property. Numbers and letters shall be a minimum of four (4) inches tall, contrast with their background and be positioned a minimum of forty-eight (48) inches above final grade.</li> <li>• Vehicle parking and material storage during construction shall not restrict or obstruct public streets or access to any building. A minimum twenty-foot travel lane for emergency vehicle access shall be maintained clear and unobstructed at all times. All required Fire Lanes, including within 15 feet of fire hydrants, shall be maintained clear and unobstructed at all times.</li> <li>• An approved automatic fire sprinkler system shall be installed throughout the building per City of Ketchum Ordinance #1125 (<a href="http://www.ketchumfire.org">www.ketchumfire.org</a>) and the National Fire Protection Association Standard 13. An approved fire sprinkler flow bell, Knox box and Fire Department Connection shall be installed in an approved location visible to approaching firefighters. Water service lines to</li> </ul>

structures shall be hydraulically calculated for size to meet fire sprinkler flow requirements. Fire sprinkler systems shall be annually tested and maintained per NFPA 25. An approved fire department connection and flow bell shall be installed in a location approved by the fire department and the system shall be supervised by an approved alarm system.

Note: The entire building shall be protected by an NFPA 13 Fire Sprinkler system.

- NOTE: One electronic set of fire sprinkler system plans must be submitted to the Ketchum Fire Department as well as the State Fire Marshal's office and a Ketchum Fire Department Permit must be obtained prior to installation of fire sprinkler systems. Inspections of fire sprinkler systems by the Fire Chief or an appointee are required. Inspections must be scheduled at least 48 hours in advance.
- An approved monitored fire sprinkler alarm system shall be installed per City of Ketchum Ordinance #1125 ([www.ketchumfire.org](http://www.ketchumfire.org)) and the requirements of NFPA 72. Two (2) sets of alarm system plans shall be submitted to the Ketchum Fire Department for approval and a permit is required prior to installation of alarm systems. Inspections of fire detection systems by the Fire Chief or an appointee are required and shall be scheduled at least 48 hours in advance.
- An approved access roadway per 2012 International Fire Code Appendix D ([www.ketchumfire.org](http://www.ketchumfire.org)) shall be installed prior to any combustible construction on the site. The road shall be a minimum of twenty (20) feet in width and capable of supporting an imposed load of at least 75,000 pounds.
- Fire extinguishers shall be installed and maintained per 2012 IFC Section 906 both during construction and upon occupancy of the building.
- An approved key box shall be installed, with the appropriate keys, for emergency fire department access in a location approved by the fire department. The key box shall be a Knox box brand and sized to accommodate keys to every door of the project.
- An 8 ½ by 11 color coded site map of this project shall be provided on paper and electronically to the fire department. This site map shall show the locations of gas shut-offs, power shut-offs, fire sprinkler riser rooms, fire department connections, alarm panels, Knox boxes, access doors, egress windows, stairways and any additional fire department requirements. Exact details for color coded "On-Sites" can be found at [www.ketchumfire.org](http://www.ketchumfire.org).
- Final inspections of all fire department permit required installations by the Fire Chief or an appointee are required and shall be scheduled at least 48 hours in advance. A Final Inspection Checklist can be found at [www.ketchumfire.org](http://www.ketchumfire.org).
- Fire Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project.

**City Engineer & Streets Department:**

- All drainage shall be retained on site including water from any roof drains (KMC §17.96.060.C.1). Any roof drain locations will need to be shown on project plans submitted with the Building Permit application.
- All construction for the project must comply with the standards set forth in Ketchum Municipal Code, Chapter 15.06 Construction Activity Standards. The applicant shall submit a Construction Activity Plan addressing all applicable activities (KMC §15.06.030), including how materials will be off-loaded at the site, plan for coordinating with neighbors on temporary closures, temporary traffic control, and construction fencing with appropriate screening, to be reviewed and approved prior to issuance of a Building Permit for the project. Pursuant to KMC §15.06.030.A.2,

the applicant shall provide notice of the project, construction schedule, and general contractor’s contact information to all neighbors with properties adjacent to the project site.

- The Building Permit plans and construction drawings shall meet all applicable sections of Chapter 12 of Ketchum Municipal Code.
- The plans for the ROW improvements must be prepared by a professional engineer licensed in Idaho (KMC §12.04.020. Reconstruction of the city sidewalk is required. Specifications for the ROW improvements shall be indicated on the civil drawings submitted with the Building Permit application to be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.
- Recommended separation distance between a dry well and water main is 25’. If a dry well is installed closer than 25’ to the water main install 20 mil HDPE groundwater barrier along the water main side of drywell. Barrier should extend of 18 inches below the water main pipe. Minimum distance between a dry well and water main must be 10’.
- Utility locations (electrical, gas) are not permitted in public ROW. If utility upgrades are necessary the applicant will need to coordinate upgrades with Idaho Power and Intermountain Gas. All utilities need to be shown on the plans including the location and size of water and sewer mains and services, gas, electric, TV and phone.
- Drainage improvements constructed shall be equal to the length of the subject property lines adjacent to any public street or private street (KMC §17.96.060C).
- The applicant shall submit a Street and Alley Digging, Excavation, and Trenching (“DIG”) Permit application with an associated traffic control plan for all construction work within the City right-of-way to be reviewed and approved by the Streets Department. The use of City right-of-way for construction including the closure of adjacent streets or sidewalks requires a Temporary Use of Right-of-Way Permit (“TURP”).
- Final civil drawings for all associated ROW improvements shall be submitted with the Building Permit application to be verified, reviewed, and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.

**Utilities:**

- The applicant will be responsible for installing connections to the water and sewer system.
- Requirements and specifications for the water and sewer connections will be verified, reviewed, and approved by the Utilities Department prior to issuance of a Building Permit for the project.

**Building:**

- The building must meet the 2012 International Building Code and Title 15 Buildings and Construction of Ketchum Municipal Code.
- Building Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project.

**Planning and Zoning:**

Comments are denoted within the analysis of the project’s compliance with the Comprehensive Plan, zoning and dimensional standards, and Design Review evaluation standards.

**Table 3: Zoning and Dimensional Standards Analysis**

Zoning and Dimensional Standards Analysis				
Compliant			Standards and Staff Comments	
Yes	No	N/A	Guideline	City Standards and <i>Staff Comments</i>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	Minimum Lot Area
			<i>Staff Comments</i>	Required: 8,000 square feet minimum  Existing: Tax Lot 6689 Total: 249,150 square feet Fire Station Project Site: 35,139 square feet
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	Minimum Open Space
			<i>Staff Comments</i>	Required: 35%/30% (Footnote 5: 5% open site area may be used for private decks or patios and walkways subject to Design Review approval)  <b>Proposed with Ketchum Fire Station:</b> YMCA and Greenhouse Building Footprint: 69,282 square feet Northwood Place & Carport Building Footprint: 23,402 square feet Ketchum Fire Station & Carport Building Footprint: 14,810 square feet Total Building Footprint: 107,494 square feet Total Open Space: 141,656 square feet Open Space: 57% (141,656square feet open space/249,150 square feet Tax Lot 6689 area)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	Floor Area Ratios and Community Housing
			<i>Staff Comments</i>	Floor Area Ratio Permitted in Tourist (T) Zoning District FAR Permitted in T Zone: 0.5 FAR Permitted with Inclusionary Housing Incentive: 1.6  Proposed Floor Area Ratio Existing YMCA and Northwood Place Gross Floor Area: 128,806.5 square feet (YMCA 91,374 gross square feet + Northwood Place 37,432.5 gross square feet)  Proposed Ketchum Fire Station Gross Floor Area: 17, 507 gross square feet  Total Gross Floor Area of YMCA, Northwood Place, Ketchum Fire Station: 146,313.5 gross square feet Floor Area Ratio Proposed: 0.59 (146,313.5 gross square feet/249,150 square feet TL 6689 area)  Increase Above 0.5 Permitted FAR: 21,738.5 square feet (146,313.5 gross floor area-124,575 permitted 0.5 gross floor area) 20% of Increase: 4,348 square feet 15% Reduction for Net Livable Floor Area: 3,696 square feet  Northwood Place is an affordable apartment complex with 32 deed restricted community housing units. Comprised of five buildings, the

				total floor area of Northwood Place is 33,188 square feet, which satisfies the community housing contribution.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>17.12.030</b>	<b>Minimum Building Setbacks</b>
			<i>Staff Comments</i>	<p>Minimum:  <i>Front: 15 feet</i>  <i>West Side Setback Along Saddle Road Waiver (PUD CUP 04-008): 0 feet</i>  <i>East Side Setback Along Interior Property Line Waiver (PUD CUP 09-023): 0 feet</i>  <i>Rear: &gt; 1 per 3 feet in building height, but no less than 10 feet</i>  <i>Setback Along Warm Springs Road with Waiver: 20 feet</i></p> <p><i>The PUD designates the Warm Springs Road boundary as the front property line.</i></p> <p>Proposed:  <i>Front (South/Warm Springs): Greater than 15 feet</i>  <i>Rear (North/Park Circle): Greater than 15 feet</i>  <i>Side with Waiver (East/Interior): 0 feet</i>  <i>Side with Waiver (West/Saddle Road): 0 feet</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>17.12.030</b>	<b>Building Height</b>
			<i>Staff Comments</i>	<p><b>Maximum Permitted in Tourist Zone: 35 feet</b>  <b>Maximum Permitted with PUD Waiver: 45 feet-6 inches</b></p> <p>Proposed:  <i>The fire station is 29 feet tall.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>17.125.030H</b>	<b>Curb Cut</b>
			<i>Staff Comments</i>	<p><b>Required:</b>  <i>A total of 35% of the linear footage of any street frontage can be devoted to access to off street parking.</i></p> <p><b>Existing:</b>  <i>YMCA (PUD CUP 04-008 &amp; Design Review CR04-013): 5%</i>  <i>Northwood Place (PUD CUP 09-023 &amp; Design Review R09-003): 38 feet/27% Park Circle frontage</i></p> <p><b>Proposed:</b>  <i>As indicated on the Site Plan (Sheet C2.00), 2 curb cuts are proposed along Saddle Road to facilitate two-way traffic for command vehicles and fire apparatus to exit and enter the fire station. The 2 curb cuts are separated by a grass median. The north access serves as the main entrance and exit for staff vehicles and fire trucks. During an emergency response, firefighters will exit the station from the apparatus-bay curb cut.</i></p>

				<i>Tax Lot 6689 has 1,747 feet of street frontage along Park Circle, Saddle Road, and Warm Springs Road. The property has 890 feet of street frontage along Saddle Road. The 2 curb cuts have a combined width of 147.5 feet. An existing 30-foot curb cut along Saddle Road serves as the main access to the YMCA. The total width of the driveway accesses along Saddle Road will be 177.5 feet or 20% of the street frontage along Saddle Road.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.125.040	<b>Parking Spaces</b>
			<i>Staff Comments</i>	<p><i>Off-street parking standards apply to any new development and to any new established uses.</i></p> <p><b>Required:</b> <i>Non-Residential Use in T Zone: 1 parking space per 1,000 gross square feet (Footnote 1: Refer to definition of floor area, gross and with the additional exclusion of common and public areas).</i></p> <p><b>Proposed:</b> <i>Ketchum Fire Station is 17,507 gross square feet generating a parking demand of 18 parking spaces.</i> <i>The shared parking areas within the PUD contain 220 public parking spaces. More than enough parking spaces are provided in the PUD to meet the needs of all users including the fire station parking demand.</i></p>

Table 4: Design Review Standards Evaluation

Design Review Improvements and Standards (KMC §17.96.060)				
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.A1 Streets	The applicant shall be responsible for all costs associated with providing a connection from an existing city street to their development.
			<i>Staff Comments</i>	<i>Tax Lot 6689 is bordered by Park Circle to the north, Saddle Road to the west, and Warm Springs to the south. A driveway access along Saddle Road at the north end of the project site provides two-way traffic to the fire station. The apparatus bay curb cut is reserved for emergency response.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.A2 Streets	All street designs shall be approved by the City Engineer.
			<i>Staff Comments</i>	<i>No changes are proposed to the street design or lanes of travel within the Saddle Road right-of-way.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.B1 Sidewalks	All projects under 17.96.010(A) that qualify as a “Substantial Improvement” shall install sidewalks as required by the Public Works Department.
			<i>Staff Comments</i>	<i>The fire station is new construction and qualifies as a substantial improvement project requiring the installation of sidewalks. A concrete path extends the length of the PUD connecting Northwood</i>

				<p><i>Place to the YMCA. The path within the project site will be relocated within the Saddle Road right-of-way to align with the existing path.</i></p> <p><i>Final civil drawings for all associated ROW improvements shall be submitted with the Building Permit application to be verified, reviewed, and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.</i></p> <p><i>See Table 2 for comments and conditions from the City Engineer &amp; Streets Department.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.B2 Sidewalks	<p>Sidewalk width shall conform to the City's right-of-way standards, however the City Engineer may reduce or increase the sidewalk width and design standard requirements at their discretion.</p>
			<i>Staff Comments</i>	<p><i>All ROW improvements shall be constructed to City ROW standards. The proposed width of the path is 7 feet to match the existing sidewalk to the north and south. The City Engineer may reduce or increase the sidewalk width and design standard requirements.</i></p> <p><i>Final civil drawings for all associated ROW improvements shall be submitted with the Building Permit application to be verified, reviewed, and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project. See Table 2 for review comments and conditions from the City Engineer &amp; Streets Department.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.B3 Sidewalks	<p>Sidewalks may be waived if one of the following criteria is met:</p> <ol style="list-style-type: none"> <li>The project comprises an addition of less than 250 square feet of conditioned space.</li> <li>The City Engineer finds that sidewalks are not necessary because of existing geographic limitations, pedestrian traffic on the street does not warrant a sidewalk, or if a sidewalk would not be beneficial to the general welfare and safety of the public.</li> </ol>
			<i>Staff Comments</i>	<p><i>N/A the existing pathway must be relocated within the Saddle Road right-of-way.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.B4 Sidewalks	<p>The length of sidewalk improvements constructed shall be equal to the length of the subject property line(s) adjacent to any public street or private street.</p>
			<i>Staff Comments</i>	<p><i>The sidewalk will be constructed across the length of the project site along Saddle Road.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.B5 Sidewalks	<p>New sidewalks shall be planned to provide pedestrian connections to any existing or future sidewalks adjacent to the site. In addition, sidewalks shall be constructed to provide safe pedestrian access to and around a building.</p>
			<i>Staff Comments</i>	<p><i>The new sidewalk along Saddle Road will connect to a path that leads to the fire station's public entrance. The sidewalk extends the length</i></p>



				<i>of the PUD and connects Northwood Place to the YMCA. This circulation system connects to the BCRD Wood River Trail multi-use path, commonly referred to as the bike path, to the west across Saddle Road through existing crosswalks.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.B6 Sidewalks	The City may approve and accept voluntary cash contributions in-lieu of the above described improvements, which contributions must be segregated by the City and not used for any purpose other than the provision of these improvements. The contribution amount shall be one hundred ten percent (110%) of the estimated costs of concrete sidewalk and drainage improvements provided by a qualified contractor, plus associated engineering costs, as approved by the City Engineer. Any approved in-lieu contribution shall be paid before the City issues a certificate of occupancy.
			<b>Staff Comments</b>	<i>N/A. Staff does not recommend a voluntary cash contribution in-lieu of improvements for this project.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.C1 Drainage	All storm water shall be retained on site.
			<b>Staff Comments</b>	<i>All storm water shall be retained on site including water from roof drains. Preliminary drainage plans are indicated on Sheets C2.0 and C3.0 of the Design Review submittal drawings. The proposed drainage plan includes a system of drywells and grass drainage swales.</i>  <i>Prior to issuance of a Building Permit for the project, the applicant shall submit a final drainage plan indicating grading, catch basins, piping, and drywells (KMC §17.96.040.C.2b &amp; KMC §17.96.060.C.1-4) prepared by a civil engineer licensed in the state to be submitted for review and approval by the City Engineer and Streets Department. See Table 2 for City Department comments including City Engineer and Streets Department conditions.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.C2 Drainage	Drainage improvements constructed shall be equal to the length of the subject property lines adjacent to any public street or private street.
			<b>Staff Comments</b>	<i>See above analysis for Ketchum Municipal Code §17.96.060C1. All drainage improvements are required to meet City standards.</i>  <i>All drainage improvements shall be indicated on civil plans prepared by an Idaho licensed engineer and require review and approval from the City Engineer &amp; Streets Department prior to issuance of a Building Permit for the project.</i>  <i>See Table 2 for review comments and conditions from the City Engineer &amp; Streets Department.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.C3 Drainage	The City Engineer may require additional drainage improvements as necessary, depending on the unique characteristics of a site.

			<i>Staff Comments</i>	<i>A final drainage plan prepared by a civil engineer licensed in the state of Idaho shall be submitted with the Building Permit application to be reviewed and approved by the City Engineer and the Streets Department. The City Engineer may require additional drainage improvements as necessary.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.C.4 Drainage	Drainage facilities shall be constructed per City standards.
			<i>Staff Comments</i>	<i>All drainage facilities within the project site and the public right-of-way shall meet City standards. Final drainage specifications must be included with the civil drawings submitted with the Building Permit application to be reviewed and approved by the City Engineer &amp; Streets Department.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.D1 Utilities	All utilities necessary for the development shall be improved and installed at the sole expense of the applicant.
			<i>Staff Comments</i>	<i>Sheet C4.00 of the project plans show utility connections and locations. All utilities necessary for the fire station must be improved and installed at the sole expense of the applicant. Prior to issuance of a Building Permit, the applicant must secure a will-serve letter from Idaho Power. Final plans will be reviewed and approved by the Utilities Department prior to issuance of a Building Permit for the project. See Table 2 for review comments and conditions from the Utilities Department.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.D2 Utilities	Utilities shall be located underground and utility, power, and communication lines within the development site shall be concealed from public view.
			<i>Staff Comments</i>	<i>All utilities within the development site shall be underground and concealed from public view. See above analysis for Ketchum Municipal Code §17.96.060D1. The generator located at the northeast corner of the project site must be fully screened from public view.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.D3 Utilities	When extension of utilities is necessary all developers will be required to pay for and install two (2") inch SDR11 fiber optical conduit. The placement and construction of the fiber optical conduit shall be done in accordance with city of Ketchum standards and at the discretion of the City Engineer.
			<i>Staff Comments</i>	<i>The applicant is aware of this requirement to install services for high-speed internet to the site. The applicant will work with the City Engineer to identify the location of a fiber line to serve the project.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.E1 Compatibility of Design	The project's materials, colors and signing shall be complementary with the townscape, surrounding neighborhoods and adjoining structures.
			<i>Staff Comments</i>	<i>The exterior elevations (Sheet A3.00) show the proposed materials, colors, and signage. The façade design alternates gray horizontal metal panels with brown vertical metal siding. Projecting towards</i>

				<p><i>Saddle Road, the four apparatus bays are distinguished from the rest of the fire station with black brick veneer and glass-panel garage doors. The modern building alludes to traditional firehouse design through red detailing framing the garage doors. Integrated with the architecture of the building, the Ketchum Fire Station 1 sign at the south elevation directs attention to the public entryway. In addition to the red firehouse doors, the 1 sign at the front elevation anchors the building as a fire station.</i></p> <p><i>Residential, commercial, and light industrial development border the project site and PUD. Most of the surrounding neighborhood is developed with the exception of a few vacant properties in the light industrial area. To the east of the project site, the light industrial buildings are characterized by more simple design styles that combine stucco, concrete, and brick. Directly north of the project site, the Northwood Place façade design uses stone veneer to differentiate from the horizontal siding. Across Saddle Road, buildings including the church and townhomes integrate river rock, stone veneer, stucco, and wood siding into the façade design. The alternation of brick veneer with board-and-batten siding provides visual interest to the YMCA building.</i></p> <p><i>The materials proposed for the fire station are compatible with the surrounding neighborhood and nearby buildings. The design approach refashions traditional materials combined with contemporary finishes to augment visual interest. The new fire station will add modern style to the eclectic buildings within the neighborhood. The adaptation of traditional materials like the brick complements the more conventional design styles in the neighborhood.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.E2 Compatibility of Design	<p>Preservation of significant landmarks shall be encouraged and protected, where applicable. A significant landmark is one which gives historical and/or cultural importance to the neighborhood and/or community.</p>
			<i>Staff Comments</i>	<i>N/A. No significant landmarks of historical or cultural importance have been identified on the property. The project site has been used for parking and snow storage.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.E3 Compatibility of Design	<p>Additions to existing buildings, built prior to 1940, shall be complementary in design and use similar material and finishes of the building being added to.</p>
			<i>Staff Comments</i>	<i>N/A This standard does not apply because the fire station is new construction.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.F1 Architectural	<p>Building(s) shall provide unobstructed pedestrian access to the nearest sidewalk and the entryway shall be clearly defined.</p>

			<p><i>Staff Comments</i></p> <p><i>The project plans include a path that borders the south side of fire station and directs pedestrians to the public entrance from the shared parking area. This pathway leads to the sidewalk that extends from Northwood Place to the YMCA. This internal circulation system will connect to the bike path across Saddle Road through existing crosswalks.</i></p> <p><i>The fire station accommodates diverse spaces from living quarters to fire truck storage to maintenance areas to administrative offices. These different fire station functions are separated within the building. The apparatus bays at the center of the station separate the administrative areas and firefighter living quarters from areas dedicated to emergency response operations, maintenance, and storage.</i></p> <p><i>The design of the entryway at the south elevation includes large windows and doors inviting the public into the facility. The entryway is distinguished from the rest of the fire station by brown metal siding and signage. The emergency response operations area incorporates with fewer windows and uses different materials combining the gray metal panels with the black brick.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p><b>17.96.060.F2 Architectural</b></p> <p>The building character shall be clearly defined by use of architectural features.</p> <p><i>Staff Comments</i></p> <p><i>The fire station is characterized by modern design elements with simple building forms and functional floor plans. The orientation and placement of rectangular building shapes with flat roofs at varying heights define the fire station’s functional arrangement.</i></p> <p><i>Separating the operations and maintenance areas from the administrative offices and living quarters, the apparatus bays at the center of the station are defined by the glass-paneled garage doors. These windows will introduce natural light into the apparatus bays while providing pedestrians a unique view of the stored fire engines.</i></p> <p><i>The southern portion of the building is distinguished from the rest of the fire station through its size, shape, and placement. With significant fenestration, the south façade design welcomes the public into the fire station. The pattern of material placement provides texture to the box building form. The vertical placement of the brown metal siding provides visual relief from the horizontal metal panels and black brick.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p><b>17.96.060.F3 Architectural</b></p> <p>There shall be continuity of materials, colors and signing within the project.</p>

			<i>Staff Comments</i>	<i>The exterior walls of the building utilize the same metal and brick materials and gray, black, and brown color scheme. Highlighting the apparatus bays, the use of the firehouse red is limited to the apparatus-bay garage doors. The consistent pattern of material placement and colors across all building walls connects the diverse functions of the fire station into one cohesive design.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.F4 Architectural	<p>Accessory structures, fences, walls and landscape features within the project shall match or complement the principal building.</p> <p><i>Staff Comments</i></p> <p><u>Accessory Structures</u>  <i>The 1,360 square foot carport at the southeast corner of the project site uses the same black brick material as the fire station. A row of rectangular windows at the carport's south elevation provides relief from the black brick veneer. The four parking spaces in the carport will be reserved for staff and command vehicles. A covered patio area will extend to the south of the carport. Utilizing the same material and color palette, the carport and patio match the fire station design theme.</i></p> <p><u>Fences &amp; Gates</u>  <i>In the 1990s, a 5-foot concrete retaining wall was constructed along the side property line to separate Tax Lot 6689 from the industrial area to the east. In addition to the existing CMU wall, the project includes a 4-foot tall patio fence, a 6-foot tall trellis, and a rolling gate (Sheets L1.00 &amp; L1.50). The patio fence will not only enhance privacy, but also limits public access to the Fire Department operations area. Comprised of horizontal slats, the patio fence will match the color of the carport and covered patio. The trellis system separates the fire station from the Northwood Place apartments. Plants attach to panels so that the freestanding trellis will transform into a living fence. Multi-functional, the green trellis will enhance screening and soften the fire station's black, brown, and gray walls.</i></p> <p><u>Landscape</u>  <i>The landscape plan is included on Sheets L1.00 and L1.500 of the project plans. Grass medians border Saddle Road and separate the northern driveway access from the apparatus-bay curb cut reserved for emergency response. Grasses and flowering plants are proposed along the front façade at either side of the center apparatus bays. Softening the building's hard edges, the plants invite and welcome the public into the fire station. Flowering plants and shrubs border the north side of the building softening the gray and brown metals panels. Boulders are incorporated throughout the landscape design providing visual interest.</i></p>

☒	☐	☐	17.96.060.F5 Architectural	Building walls shall provide undulation/relief, thus reducing the appearance of bulk and flatness.
			<i>Staff Comments</i>	<i>The fire station design uses rectangular shapes in different forms to reduce the flatness of building walls. The gray metal panels, black brick, brown metal siding, and window openings are all unique rectangular elements in different sizes and orientations. For example, the gray metal panels are placed horizontally and the brown metal siding is aligned vertically. This contrast of rectangular shapes provides visual interest to the box-shaped building. In addition to this rectangular arrangement, color variation and multiple windows and doors to breathe life into the fire station. The varying roof heights define the separate areas of the building. Projecting beyond the rest of the building, the firehouse red detailing highlight framing the glass panels focuses attention to the apparatus bays.</i>
☒	☐	☐	17.96.060.F6 Architectural	Building(s) shall orient towards their primary street frontage.
			<i>Staff Comments</i>	<i>The front façade of the building orients towards Saddle Road.</i>
☒	☐	☐	17.96.060.F7 Architectural	Garbage storage areas and satellite receivers shall be screened from public view and located off alleys.
			<i>Staff Comments</i>	<i>The concrete trash enclosure is located at the southeast corner of the project site. In addition to the concrete trash enclosure, the carport and fenced patio further screen the dumpster from public view.</i>  <i>Prior to issuance of a Building Permit for the project, the applicant shall submit a will serve letter from Clear Creek Disposal to the Planning &amp; Building Department.</i>  <i>The project plans do not include any satellite receivers.</i>
☒	☐	☐	17.96.060.F8 Architectural	Building design shall include weather protection which prevents water to drip or snow to slide on areas where pedestrians gather and circulate or onto adjacent properties.
			<i>Staff Comments</i>	<i>Enhancing weather protection, the flat roof form will prevent water from dripping or snow from sliding from the building. At the main entrance, the second floor cantilevers beyond the first floor to cover the pedestrian pathway.</i>
☒	☐	☐	17.96.060.G1 Circulation Design	Pedestrian, equestrian and bicycle access shall be located to connect with existing and anticipated easements and pathways.
			<i>Staff Comments</i>	<i>The project plans include a path that directs pedestrians to the public entrance from the shared parking area. This pathway leads to the sidewalk that extends from Northwood Place to the YMCA. This</i>

				<i>internal circulation system connects to the bike path across Saddle Road through existing crosswalks.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.G2	Awnings extending over public sidewalks shall extend five (5') feet or more across the public sidewalk but shall not extend within two (2') feet of parking or travel lanes within the right of way.
			Circulation Design	<i>N/A. No awnings are proposed to extend across the public sidewalk.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.G3	Traffic shall flow safely within the project and onto adjacent streets. Traffic includes vehicle, bicycle, pedestrian and equestrian use. Consideration shall be given to adequate sight distances and proper signage.
			<i>Staff Comments</i>	<p><i>Neighboring Highway 75, Saddle Road, and Warm Springs Road, the fire station site is centrally located with easy access to main routes to all service delivery areas. A driveway access off Saddle Road at the northwest corner of the site will facilitate two-way traffic to and from the fire station. The curb cut along the four apparatus bays will be reserved for emergency response. Sight line windows are incorporated into the building design that allow emergency responders to view any pedestrians or bicyclists approaching the station on the sidewalk and any obstructions in front of the apparatus bays.</i></p> <p><i>The City has engaged a traffic engineering firm to identify safety concerns and circulation conflicts between neighborhood traffic and Fire Department emergency response operations and identify any mitigation measures. These mitigation measures may include warning signals, traffic control devices, and alert systems. If the traffic study identifies circulation conflicts, the City will implement any mitigation measures necessary to ensure the safety of pedestrians, bicyclists, and drivers in the area.</i></p> <p><i>A comprehensive analysis of traffic circulation is included within Exhibit B of the Staff Report.</i></p> <p><i>Prior to issuance of a Building Permit for the project, the City Engineer and Streets Department shall review the civil drawings to ensure adequate sight distances and proper signage for the proposed driveway access.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.G4	Curb cuts and driveway entrances shall be no closer than twenty (20') feet to the nearest intersection of two or more streets, as measured along the property line adjacent to the right of way. Due to site conditions or current/projected traffic levels or speed, the City Engineer may increase the minimum distance requirements.

			<i>Staff Comments</i>	<p>The project site is located over 250 ft from Saddle Road's intersection with Park Circle and over 400 ft from the Saddle Road and Warm Springs Road intersection. Clear sight lines are provided to the north and south of the fire station driveways.</p> <p>Prior to issuance of a Building Permit for the project, the City Engineer and Streets Department shall review the civil drawings to ensure adequate sight distances and proper signage for the proposed parking access.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.G5 Circulation Design	<p>Unobstructed access shall be provided for emergency vehicles, snowplows, garbage trucks and similar service vehicles to all necessary locations within the proposed project.</p> <p><i>Staff Comments</i></p> <p>Clear sight lines are provided to the north and south of the fire station driveways. Sight line windows are incorporated into the building design that allow emergency responders to view any pedestrians or bicyclists approaching the station on the sidewalk and any obstructions in front of the apparatus bays. The north driveway access facilitates two-way traffic for staff vehicles to enter and exit the station. The curb cut at the apparatus bays is reserved for emergency response.</p> <p>Garbage trucks will access the trash enclosure from the shared parking area to the south of the station. As the circulation areas will include a snowmelt system, snowplows will not need to remove snow from the site.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.H1 Snow Storage	<p>Snow storage areas shall not be less than thirty percent (30%) of the improved parking and pedestrian circulation areas.</p> <p><i>Staff Comments</i></p> <p>As permitted by KMC §17.96.060.H4, the traffic and pedestrian circulation areas will include a snowmelt system instead of a snow storage area.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.H2 Snow Storage	<p>Snow storage areas shall be provided on-site.</p> <p><i>Staff Comments</i></p> <p>N/A. As indicated on Sheet C2.00 of the project plans, the fire station site will be snow-melted as permitted pursuant to KMC §17.96.060.H4.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.H3 Snow Storage	<p>A designated snow storage area shall not have any dimension less than five (5') feet and shall be a minimum of twenty-five (25) square feet.</p> <p><i>Staff Comments</i></p> <p>As permitted by KMC §17.96.060.H4, the project site will include a snowmelt system instead of a snow storage area.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.H4 Snow Storage	<p>In lieu of providing snow storage areas, snow melt and hauling of snow may be allowed.</p> <p><i>Staff Comments</i></p> <p>The fire station paved circulation areas will include a snowmelt system instead of snow storage. Snow-melting all circulation areas will enhance Fire Department operations and emergency response.</p>



☒	☐	☐	17.96.060.11 Landscaping	<b>Landscaping is required for all projects.</b>
			<i>Staff Comments</i>	<i>The landscape plan is included on Sheets L1.00 and L1.50 of the project plans. The grasses, flowering plants, and shrubs complement the fire station's modern design.</i>
☒	☐	☐	17.96.060.12 Landscaping	Landscape materials and vegetation types specified shall be readily adaptable to a site's microclimate, soil conditions, orientation and aspect, and shall serve to enhance and complement the neighborhood and townscape.
			<i>Staff Comments</i>	<i>The landscape materials and vegetation types shall be readily adaptable to the site's microclimate, soil conditions, orientation, and aspect. The proposed landscaping will soften the fire station's hard lines and rectangular mass. Bordering the front property line, the tall grasses and flowering plants augment the welcoming entryway to the fire station.</i>
☒	☐	☐	17.96.060.13 Landscaping	All trees, shrubs, grasses and perennials shall be drought tolerant. Native species are recommended but not required.
			<i>Staff Comments</i>	<i>All proposed landscape materials and vegetation types shall be drought tolerant. The applicant is encouraged to select native species.</i>
☒	☐	☐	17.96.060.14 Landscaping	Landscaping shall provide a substantial buffer between land uses, including, but not limited to, structures, streets and parking lots. The development of landscaped public courtyards, including trees and shrubs where appropriate, shall be encouraged.
			<i>Staff Comments</i>	<i>The proposed landscaping is appropriate for the fire station project. The living trellis buffers the fire station from the Northwood Place apartments. An existing retaining wall separates the project site from the light industrial area.</i>
☒	☐	☐	17.96.060.J1 Public Amenities	Where sidewalks are required, pedestrian amenities shall be installed. Amenities may include, but are not limited to, benches and other seating, kiosks, bus shelters, trash receptacles, restrooms, fountains, art, etc. All public amenities shall receive approval from the Public Works Department prior to design review approval from the Commission.
			<i>Staff Comments</i>	<i>The project will realign the existing sidewalk that extends from Northwood Place to the YMCA. The width does not provide sufficient clearance to allow public amenities within the sidewalk design while still maintaining a clear path of travel as required by ADA. Further, sight constraints limit available locations to install amenities as any obstruction would conflict with vehicle ingress and egress into the site. Due to associated site constraints and potential conflicts with Fire Department operations, the fire station project does not propose any exterior public amenities such as benches. The community room and public restrooms provided within the fire station satisfy the public amenity requirement.</i>

## RECOMMENDED MOTION

"I move to approve the Ketchum Fire Station Design Review subject to conditions."

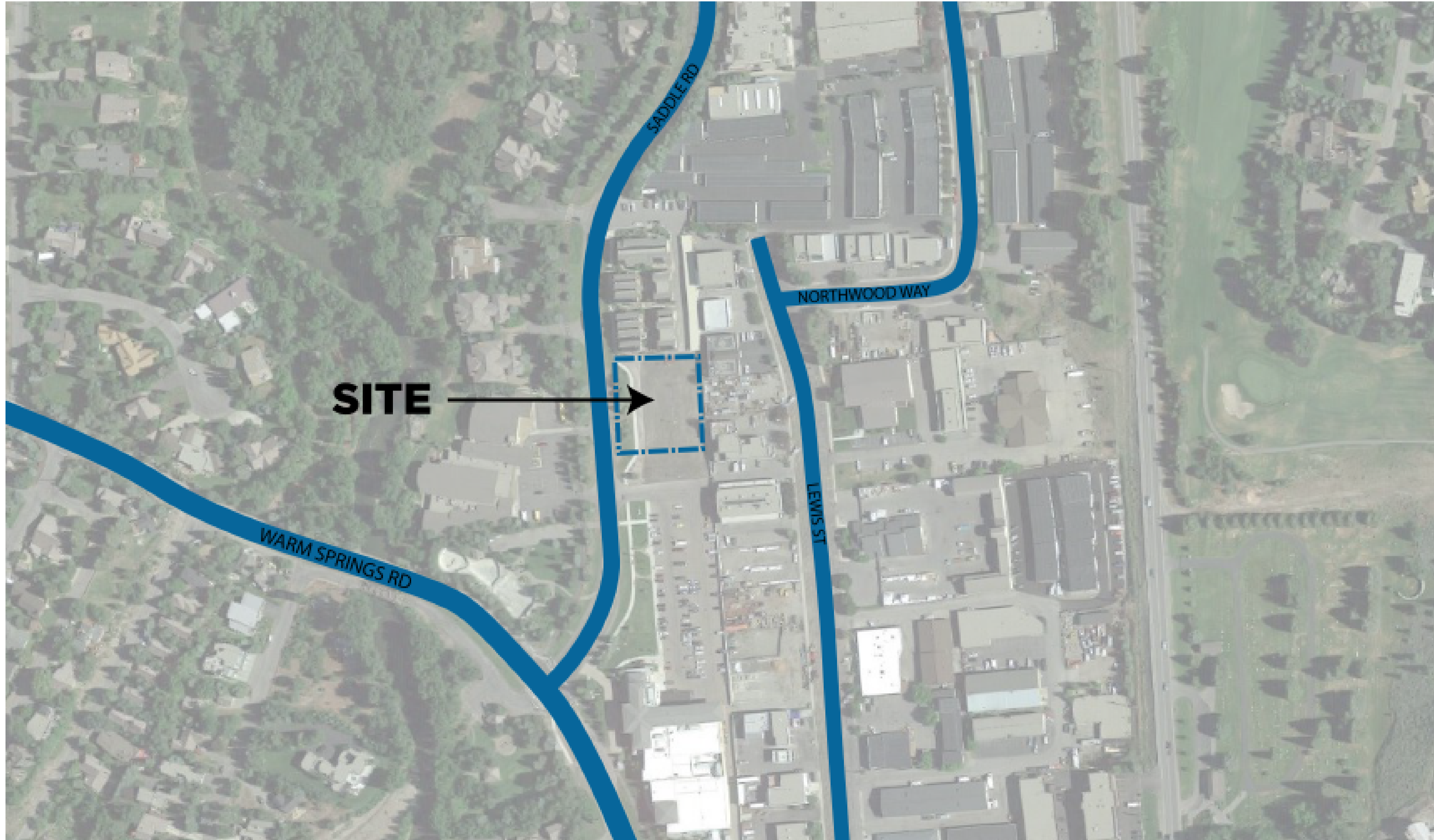
## RECOMMENDED CONDITIONS OF APPROVAL

1. This Design Review approval is subject to all comments and conditions as described in Tables 2, 3, and 4.
2. This Design Review approval is based on the plans and information presented and approved at the meeting on the date noted herein. Building Permit plans for all on-site improvements must conform to the approved Design Review plans unless otherwise approved in writing by the Planning and Zoning Commission or Administrator. Any building or site discrepancies which do not conform to the approved plans will be subject to removal.
3. All governing ordinances, requirements, and regulations of the Fire Department (2012 International Fire Code and local Fire Protection Ordinance No.1125), Building Department (2012 International Building Code, the 2012 International Residential Code, and Title 15 of Ketchum Municipal Code), Utilities Department, Street Department (Title 12 of Ketchum Municipal Code), and the City Engineer shall be met prior to Certificate of Occupancy.
4. The applicant shall submit final civil drawings prepared by an engineer registered in the State of Idaho to include specification for the ROW, utilities, and drainage improvements to be reviewed and approved by the City Engineer, Streets, and Utilities departments prior to issuance of a Building Permit for the project.
5. The term of Design Review approval shall be twelve (12) months from the date that the Findings of Fact, Conclusions of Law, and Decision are adopted by the Commission or upon appeal, the date the approval is granted by the Council subject to changes in zoning regulations (KMC §17.96.090).
6. All Design Review elements shall be completed prior to issuance of a Certificate of Occupancy for the building.
7. All exterior lighting on the property shall be in compliance with Ketchum Municipal Code, Chapter 17.132, Dark Skies, and shall be inspected by Planning Staff and approved prior the issuance of a Certificate of Occupancy for the building.
8. The project shall comply with the requirements of §17.124.040 Development Standards as adopted on the date a Building Permit is submitted for the project.
9. Prior to issuance of a Building Permit for the project, the applicant shall submit a construction management plans, which addresses each of the standards as set forth in Ketchum Municipal Code, Chapter 15.06 Construction Activity Standards.
10. In addition to the requirements set forth in this Design Review approval, this project shall comply with all applicable local, state, and federal laws.

## EXHIBITS:

- A. Project Plans
- B. Response to Planning and Zoning Commission Comments on the Fire Station Pre-Application Design Review

# Exhibit A: Project Plans



**COLE**  
ARCHITECTS

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ARCHITECTURE + PLANNING + DESIGN

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206.522.3620

STAMP

**NOT FOR  
CONSTRUCTION**

CONSULTANT

PROJECT INFORMATION



**KETCHUM FIRE  
STATION**

107 SADDLE ROAD, KETCHUM, ID 83353

ISSUES

PHASE DESIGN REVIEW

DATE FEBRUARY 3, 2020

JOB NUMBER 18-022

MARK	DATE	DESCRIPTION

SHEET NAME

VICINITY MAP

SHEET NUMBER

**G0.10**

**NOT FOR  
CONSTRUCTION**

CONSULTANT

PROJECT INFORMATION



**KETCHUM FIRE  
STATION**

107 SADDLE ROAD, KETCHUM, ID 83353

ISSUES

PHASE DESIGN REVIEW

DATE FEBRUARY 3, 2020

JOB NUMBER 18-022

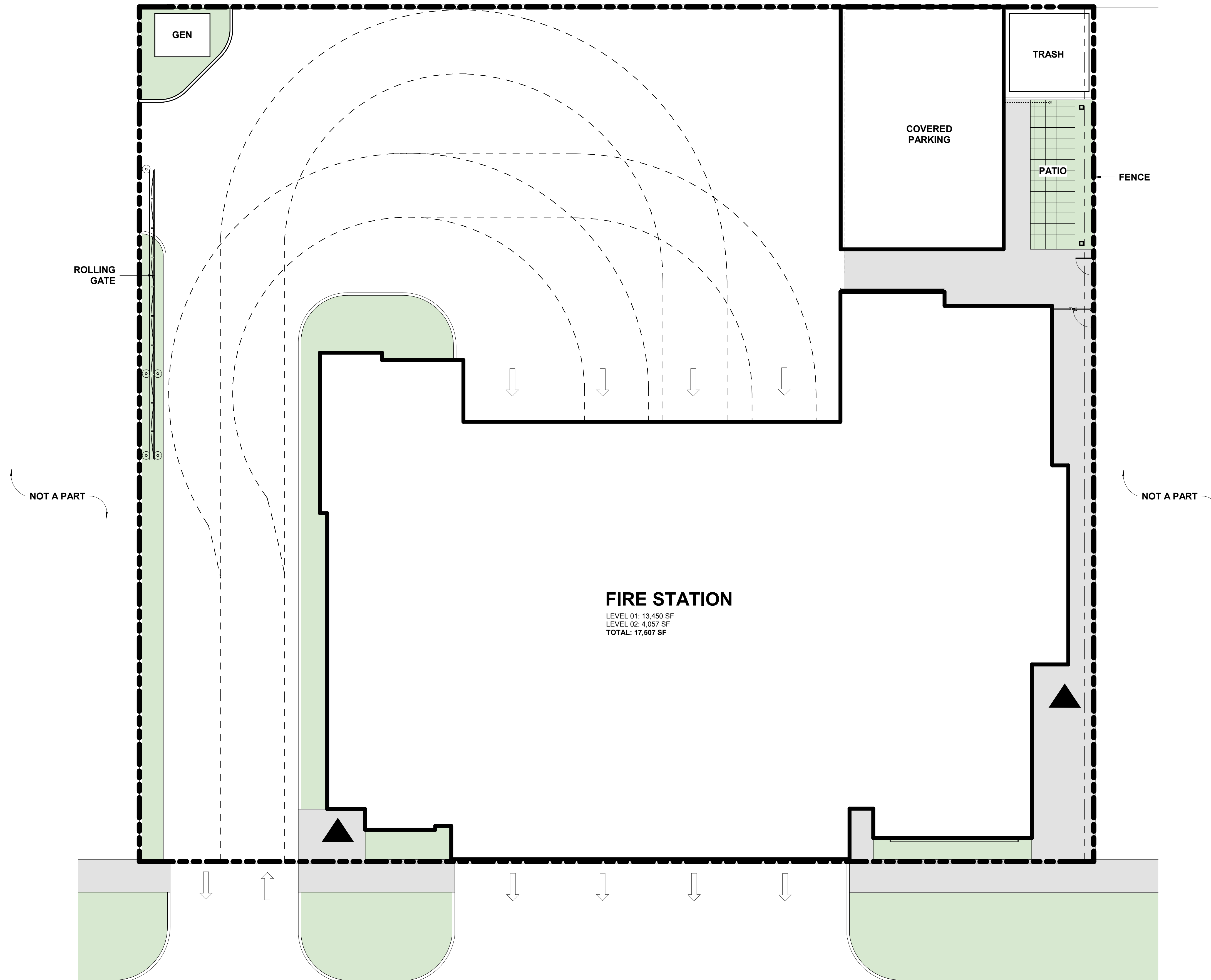
MARK	DATE	DESCRIPTION

SHEET NAME

**ARCHITECTURAL  
SITE PLAN**

SHEET NUMBER

**A1.00**





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PROJECT INFORMATION



**KETCHUM FIRE  
STATION**

107 SADDLE ROAD, KETCHUM, ID 83353

ISSUES

PHASE	DESIGN REVIEW	
DATE	FEBRUARY 3, 2020	
JOB NUMBER	18-022	
MARK	DATE	DESCRIPTION

SHEET NAME

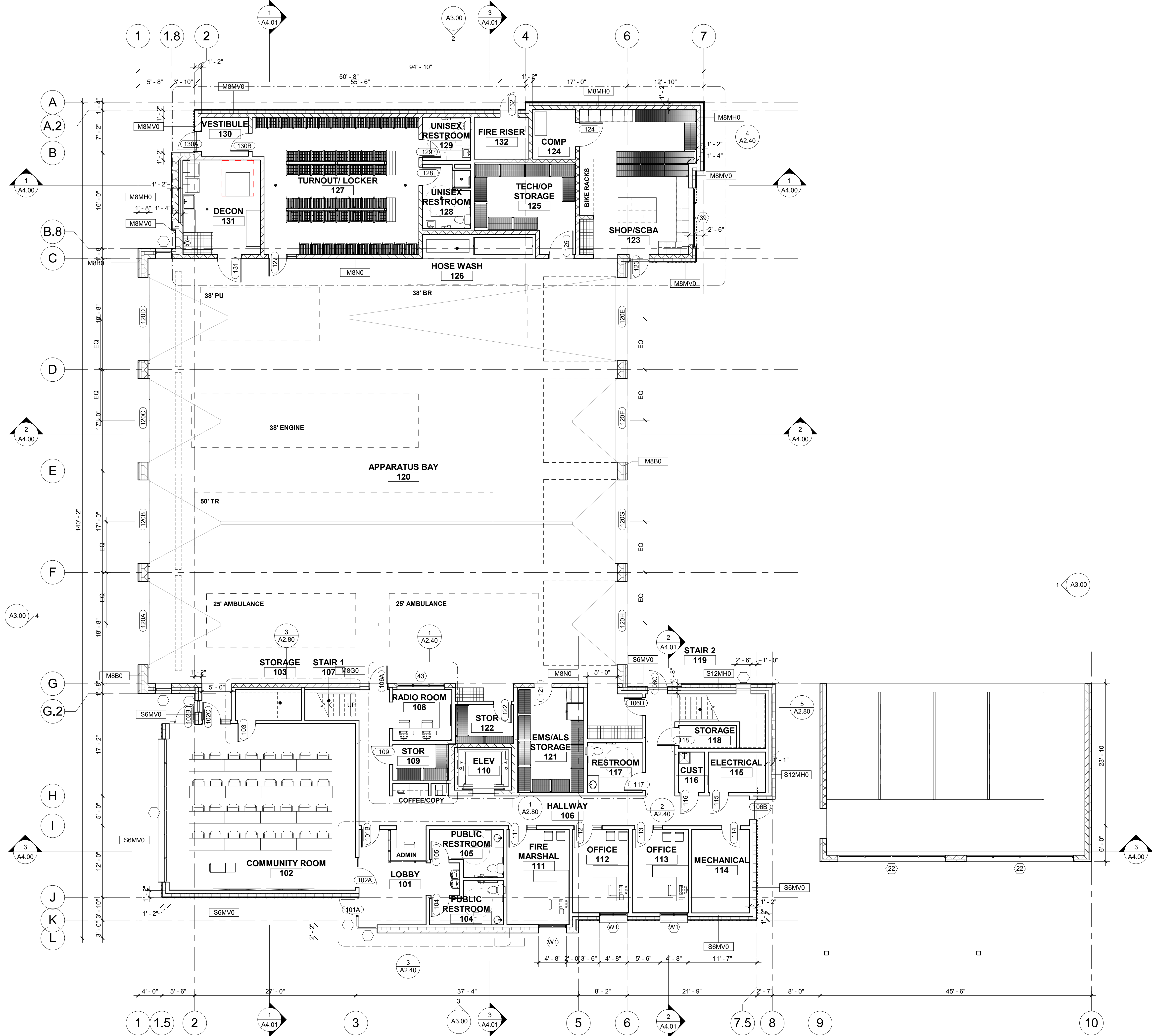
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LEVEL 01**

SHEET NUMBER

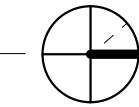
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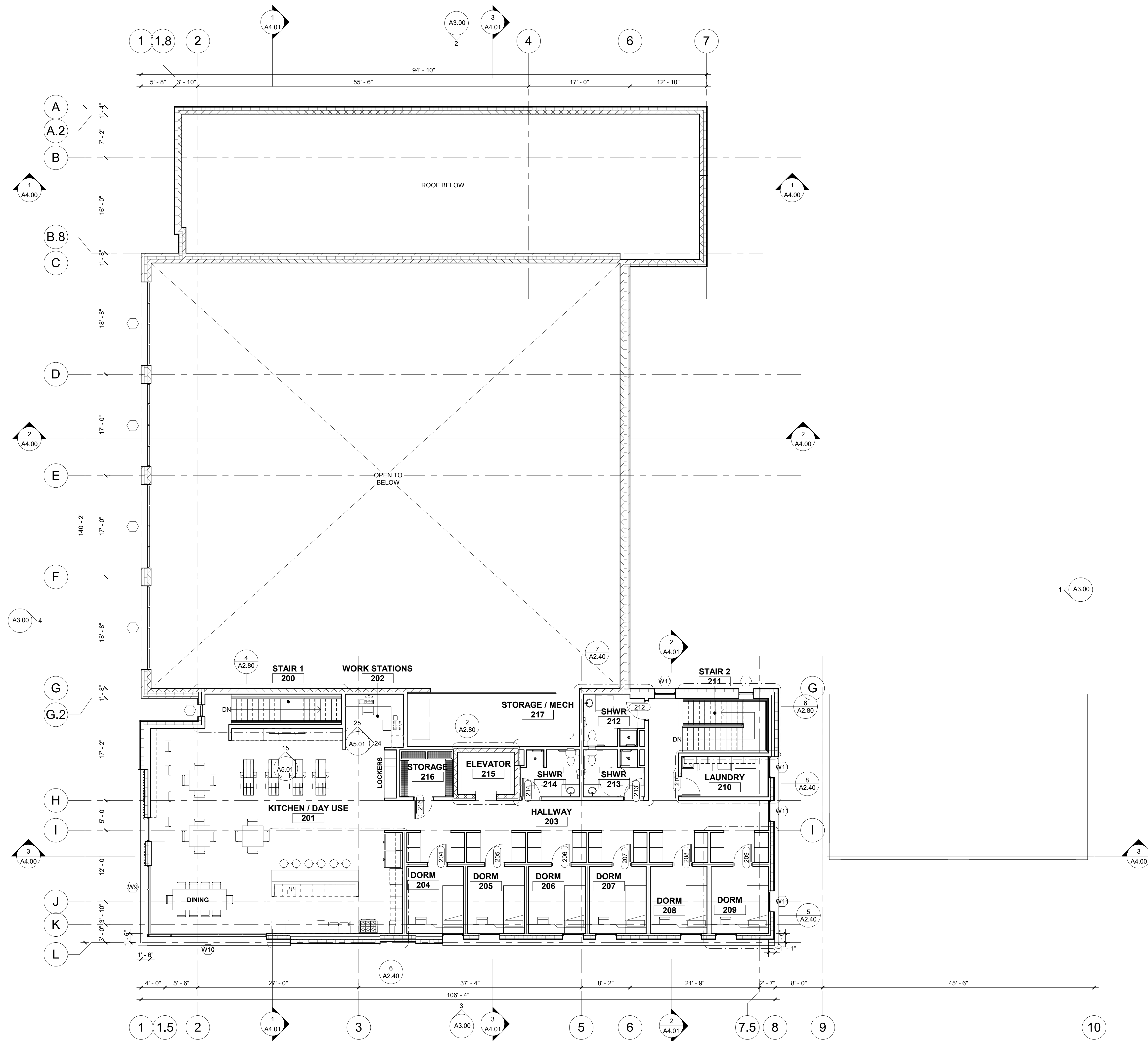
**GENERAL NOTES**

- A. FIELD VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS. NOTIFY ARCHITECT IMMEDIATELY IF ANY CONFLICTS OR DISCREPANCIES OCCUR BEFORE PROCEEDING WITH WORK.
- B. CROSS REFERENCES SHOWN ON DRAWINGS DO NOT NECESSARILY INDICATE ALL LIKE CONDITIONS AND DO NOT LIMIT APPLICATION OF ANY DRAWING OR DETAIL. WHERE SPECIFIC DIMENSIONS, DETAILS, OR DESIGN INTENT CANNOT BE DETERMINED, CONSULT THE ARCHITECT PRIOR TO PROCEEDING WITH WORK. IF DESIGN INTENT REMAINS UNCLEAR THEN PROVIDE MOST EXPENSIVE OPTION IN BID.
- C. PROVIDE SOLID BLOCKING IN WALLS FOR ALL WALL HUNG EQUIPMENT. BLOCKING TO MEET OR EXCEED MANUFACTURER'S RECOMMENDATIONS. FASTEN EQUIPMENT TO WALLS PER MANUFACTURER'S RECOMMENDATIONS. PROVIDE BLOCKING FOR, BUT NOT LIMITED TO THE FOLLOWING: MILLWORK, RAILING, FIRE EXTINGUISHER ACCESSORIES, WALL MOUNTED PLUMBING FIXTURES, SHELVING, ELECTRICAL EQUIPMENT, RESTROOM ACCESSORIES AND DISPLAY ITEMS.
- D. CAULK ALL COUNTERTOPS, BACKSPASHES AND CABINETS AT LOCATIONS WHERE THEY MEET WALLS. SEAL ALL CUT-OUTS IN COUNTERTOPS.
- E. ERECT DUST PROOF PARTITIONS AS REQUIRED BY OWNER TO PROTECT ADJACENT AREAS.
- F. SEAL AROUND ALL MECHANICAL AND ELECTRICAL EQUIPMENT PENETRATIONS AT WALLS. AT RATED WALLS USE A U.L. APPROVED FIRE-STOPPING MATERIAL.
- G. PROVIDE 4" DOOR JAMBS FROM FACE-OF-FINISH AT ADJACENT WALLS ON THE HINGE SIDE OR UNLESS OTHERWISE INDICATED.
- H. DIMENSIONS ARE TO GRID LINE OR FACE OF STRUCTURAL MEMBER UNLESS OTHERWISE NOTED. DOOR & WINDOW OPENING DIMENSIONS ARE TO ROUGH OPENING OR CENTERLINE OF OPENING. CONCRETE AND BRICK DIMENSIONS ARE GIVEN TO THE FACE OF CONCRETE OR MASONRY AND TO THE FACE OF ROUGH OPENINGS.
- I. WHEN APPLICABLE - UNLESS NOTED OTHERWISE, USE CDX PLYWOOD FOR EXTERIOR AND CONCEALED APPLICATIONS AND AB PLYWOOD FOR EXPOSED INTERIOR APPLICATIONS.



**1ST FLOOR PLAN**  
SCALE | 1/8" = 1'-0"





### GENERAL NOTES

- A. FIELD VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS. NOTIFY ARCHITECT IMMEDIATELY IF ANY CONFLICTS OR DISCREPANCIES OCCUR BEFORE PROCEEDING WITH WORK.
- B. CROSS REFERENCES SHOWN ON DRAWINGS DO NOT NECESSARILY INDICATE ALL LIKE CONDITIONS AND DO NOT LIMIT APPLICATION OF ANY DRAWING OR DETAIL WHERE SPECIFIC DIMENSIONS, DETAILS, OR DESIGN INTENT CANNOT BE DETERMINED. CONSULT THE ARCHITECT PRIOR TO PROCEEDING WITH WORK. IF DESIGN INTENT REMAINS UNCLEAR THEN PROVIDE MOST EXPENSIVE OPTION IN BID.
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- D. CAULK ALL COUNTERTOPS, BACKSPASHES AND CABINETS AT LOCATIONS WHERE THEY MEET WALLS. SEAL ALL CUT-OUTS IN COUNTERTOPS.
- E. ERECT DUST PROOF PARTITIONS AS REQUIRED BY OWNER TO PROTECT ADJACENT AREAS.
- F. SEAL AROUND ALL MECHANICAL AND ELECTRICAL EQUIPMENT PENETRATIONS AT WALLS. AT RATED WALLS USE A U.L. APPROVED FIRE-STOPPING MATERIAL.
- G. PROVIDE 4" DOOR JAMBS FROM FACE-OF-FINISH AT ADJACENT WALLS ON THE HINGE SIDE OR UNLESS OTHERWISE INDICATED.
- H. DIMENSIONS ARE TO GRID LINE OR FACE OF STRUCTURAL MEMBER UNLESS OTHERWISE NOTED. DOOR & WINDOW OPENING DIMENSIONS ARE TO ROUGH OPENING OR CENTERLINE OF OPENING. CONCRETE AND BRICK DIMENSIONS ARE GIVEN TO THE FACE OF CONCRETE OR MASONRY AND TO THE FACE OF ROUGH OPENINGS.
- I. WHEN APPLICABLE - UNLESS NOTED OTHERWISE, USE CDX PLYWOOD FOR EXTERIOR AND CONCEALED APPLICATIONS AND AB PLYWOOD FOR EXPOSED INTERIOR APPLICATIONS.

### KEYNOTES #

- 1. XX



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PROJECT INFORMATION



**KETCHUM FIRE STATION**

107 SADDLE ROAD, KETCHUM, ID 83353

ISSUES

PHASE	DESIGN REVIEW	
DATE	FEBRUARY 3, 2020	
JOB NUMBER	18-022	
MARK	DATE	DESCRIPTION

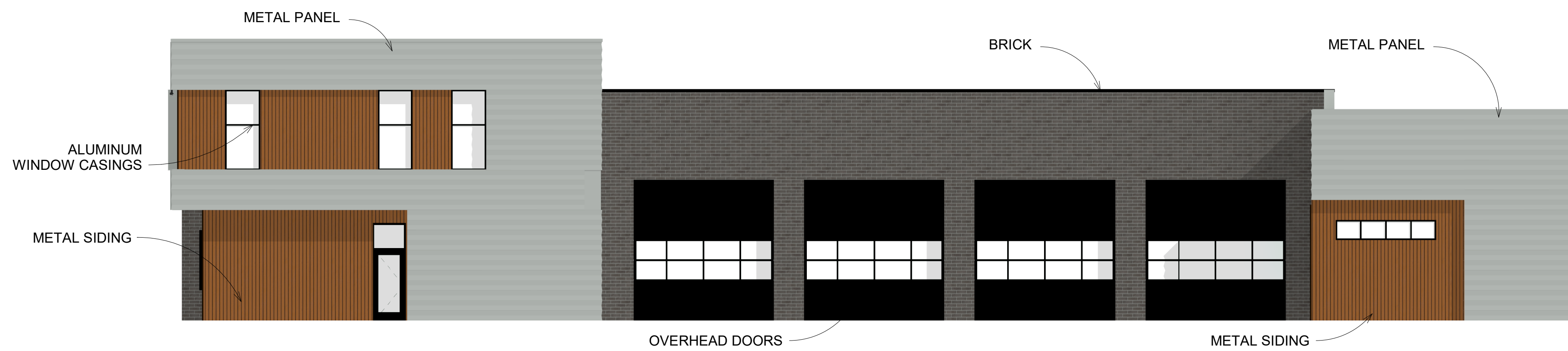
SHEET NAME

**FLOOR PLAN - LEVEL 02**

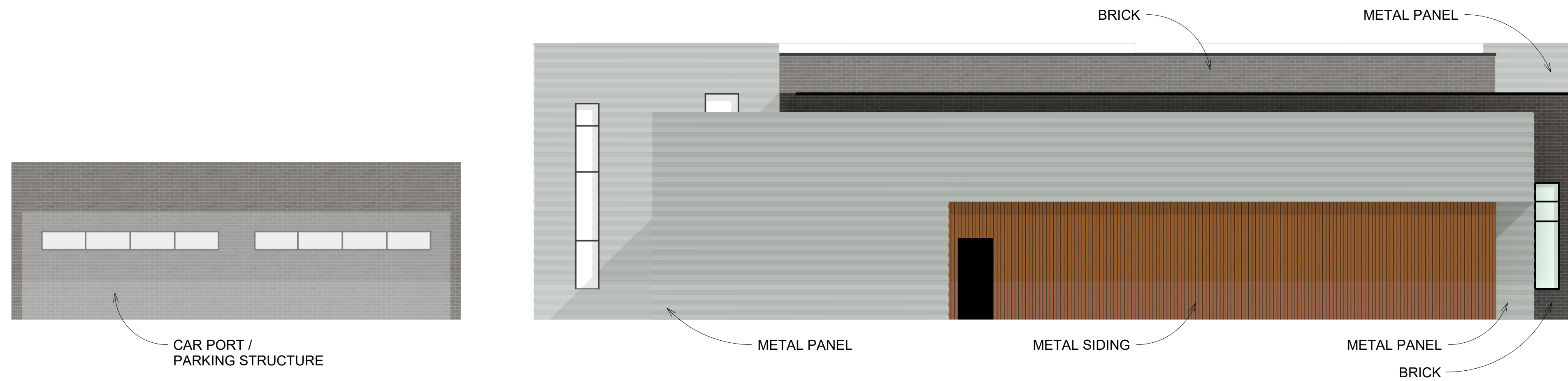
SHEET NUMBER

**A2.02**

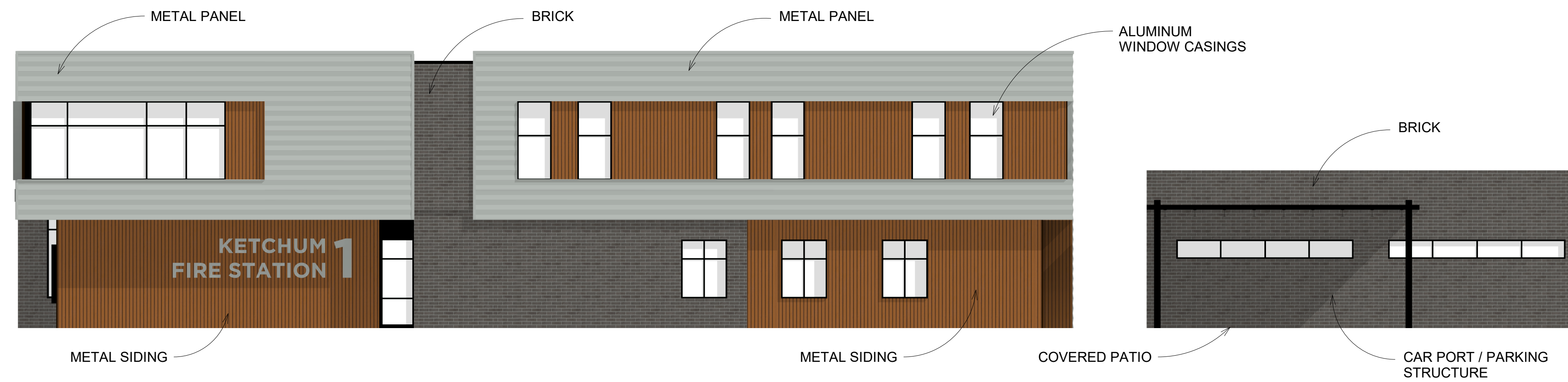
**1 2ND FLOOR PLAN**  
SCALE | 1/8" = 1'-0"



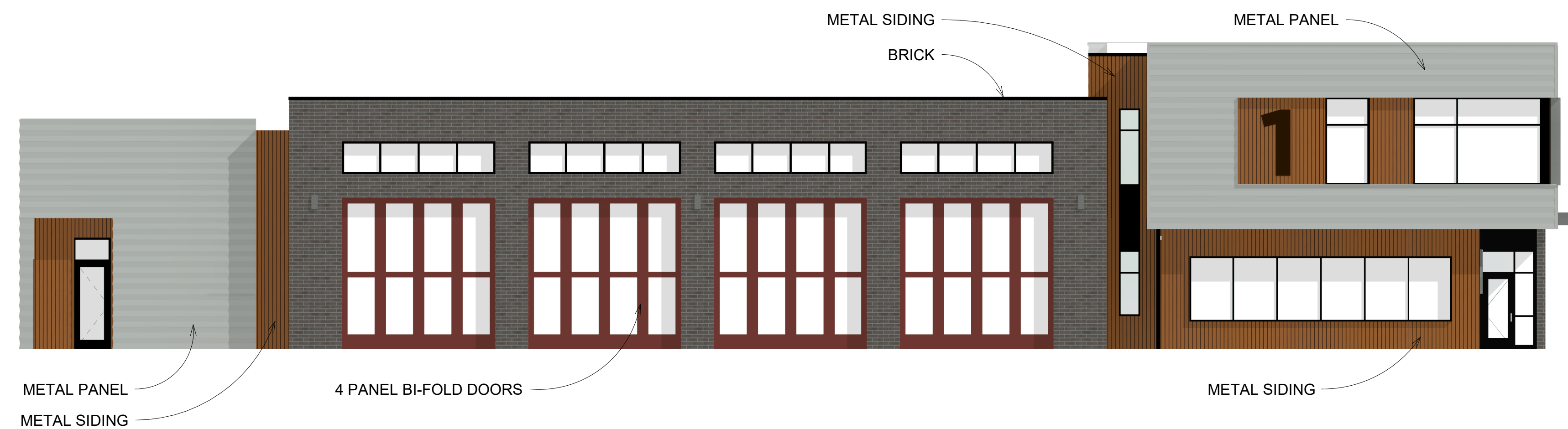
**1 EAST ELEVATION**  
SCALE | 1/8" = 1'-0"



**2 NORTH ELEVATION**  
SCALE | 1/8" = 1'-0"



**3 SOUTH ELEVATION**  
SCALE | 1/8" = 1'-0"



**4 WEST ELEVATION**  
SCALE | 1/8" = 1'-0"

## GENERAL NOTES

- A. GENERAL NOTES APPLY TO ALL DRAWING SETS.
- B. FIELD VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS. NOTIFY ARCHITECT IMMEDIATELY IF ANY CONFLICTS OR DISCREPANCIES OCCUR BEFORE PROCEEDING WITH WORK.
- C. CROSS REFERENCES SHOWN ON DRAWINGS DO NOT NECESSARILY INDICATE ALL LIKE CONDITIONS AND DO NOT LIMIT APPLICATION OF ANY DRAWING OR DETAIL. WHERE SPECIFIC DIMENSIONS, DETAILS, OR DESIGN INTENT CANNOT BE DETERMINED, CONSULT THE ARCHITECT PRIOR TO PROCEEDING WITH WORK. IF DESIGN INTENT REMAINS UNCLEAR THEN PROVIDE MOST EXPENSIVE OPTION IN BID.
- D. DIMENSIONS ARE TO GRID LINE OR FACE OF STRUCTURAL MEMBER UNLESS OTHERWISE NOTED. DOOR & WINDOW OPENING DIMENSIONS ARE TO ROUGH OPENING OR CENTERLINE OF OPENING. CONCRETE AND BRICK DIMENSIONS ARE GIVEN TO THE FACE OF CONCRETE OR MASONRY AND TO THE FACE OF ROUGH OPENINGS.



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PROJECT INFORMATION



**KETCHUM FIRE  
STATION**

107 SADDLE ROAD, KETCHUM, ID 83353

ISSUES

PHASE DESIGN REVIEW

DATE FEBRUARY 3, 2020

JOB NUMBER 18-022

MARK	DATE	DESCRIPTION

SHEET NAME

**EXTERIOR  
ELEVATIONS**

SHEET NUMBER

**A3.00**





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KETCHUM FIRE  
STATION

107 SADDLE ROAD, KETCHUM, ID 83353

ISSUES

PHASE DESIGN REVIEW

DATE FEBRUARY 3, 2020

JOB NUMBER 18-022

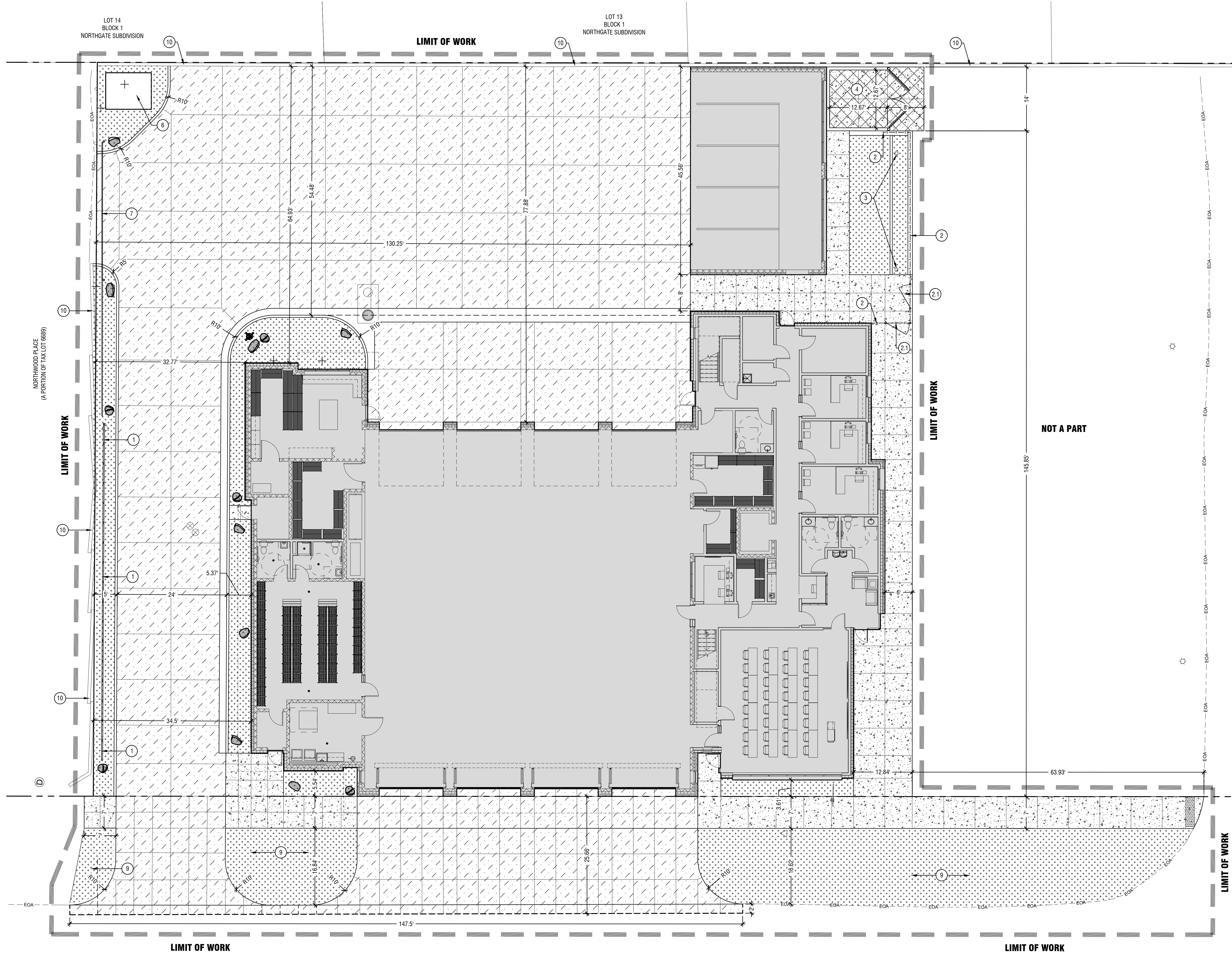
MARK	DATE	DESCRIPTION

SHEET NAME

3D PERSPECTIVE

SHEET NUMBER

G0.20



**Sheet Notes:**

- A. SNOW MELT SYSTEM WILL BE UTILIZED FOR THIS SITE.
- B. REFER TO SHEET C3.00 FOR GRADING INFORMATION.
- C. REFER TO SHEET C4.00 FOR UTILITY INFORMATION.

**Property Info:**

SUBJECT PROPERTY: 35,139 SQ. FT. (DEVELOPED AREA)  
ZONING DISTRICT: TOURIST

**Material Legend:**

	6" CONCRETE PAVING		STANDARD CONCRETE FLATWORK
	REINFORCED CONCRETE FLATWORK		LANDSCAPE AREA - REFER TO SHEET L1.00 FOR MORE INFORMATION.

**Keynotes:**

- CALL OUT NUMBERS COORDINATED TO NUMBERED NOTES BELOW.
1. 6-FT TALL FREE STANDING GREENSCREEN TRELLISING SYSTEM - SEE IMAGE 5/L1.50.
  2. 4-FT TALL ONE INCH HORIZONTAL SLAT FENCE BY LANDSCAPE FORMS. COLOR: STORMCLOUD. SEE IMAGE 4/L1.50.
  - 2.1. 5-FT WIDE GATE.
  3. PATIO STRUCTURE POSTS AND STRUCTURE BY ARCHITECTS.
  4. CMU TRASH ENCLOSURE.
  5. NOT USED
  6. GENERATOR BY MECHANICAL.
  7. ROLLING GATE
  8. NOT USED.
  9. GRASS DRAINAGE SWALE
  10. RETAIN AND PROTECT EXISTING WALL.



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**PROJECT INFORMATION**



**KETCHUM FIRE STATION**

107 SADDLE ROAD, KETCHUM, ID 83353

**ISSUES**

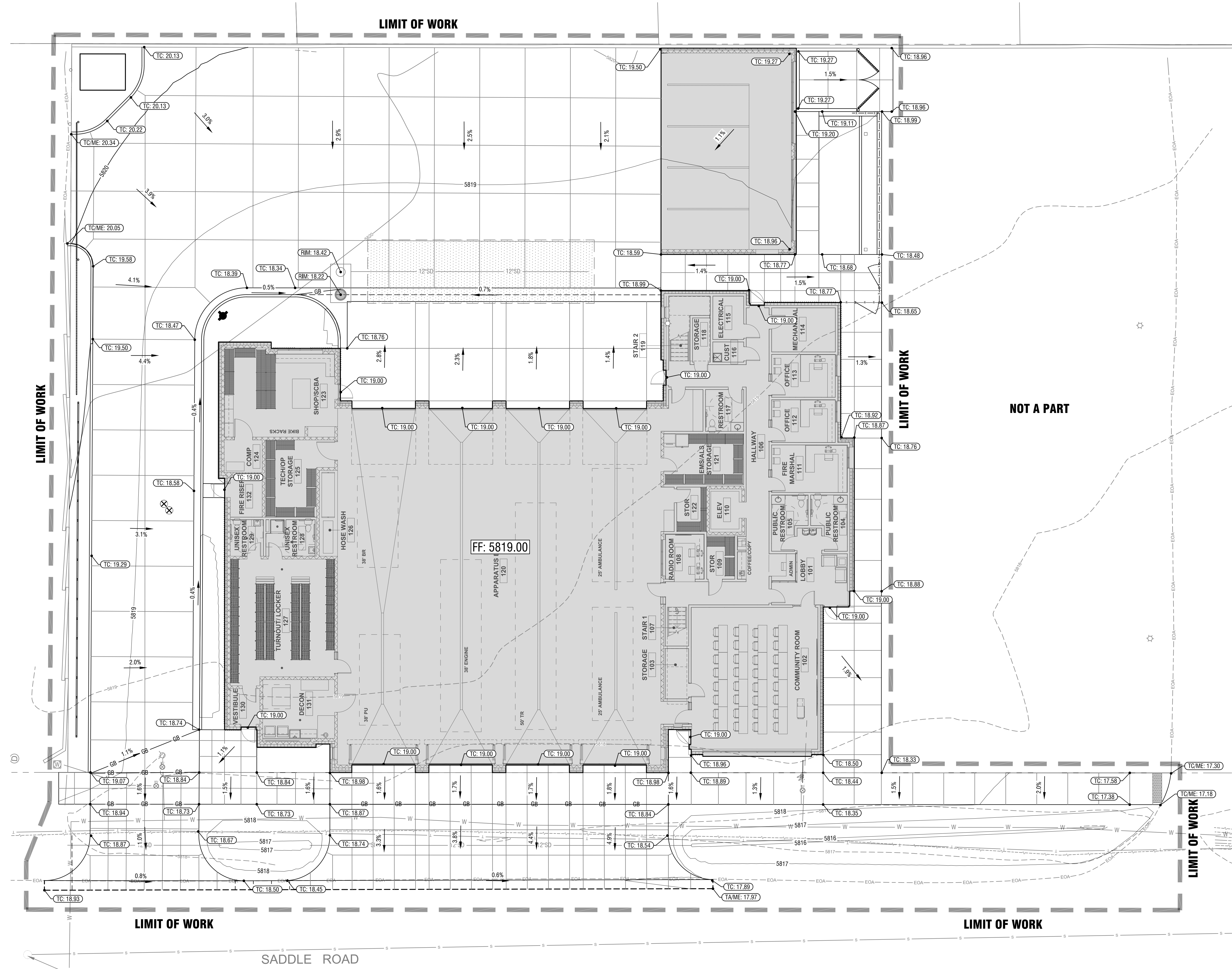
PHASE	DESIGN REVIEW	
DATE	FEBRUARY 3, 2020	
JOB NUMBER	18-022	
MARK	DATE	DESCRIPTION

**SHEET NAME**

**SITE PLAN**

**SHEET NUMBER**

**C2.00**



- Sheet Notes:**
- A. EXISTING AND PROPOSED CONTOURS ARE AT A 1-FT INTERVAL.
  - B. LONGITUDINAL SLOPE OF ALL SIDEWALKS SHALL NOT EXCEED 5%. CROSS SLOPE SHALL NOT EXCEED 2%. SLOPES WITHIN PEDESTRIAN RAMPS SHALL NOT EXCEED 12:1 SLOPE IN ANY DIRECTION. FLATWORK ADJACENT TO THE BUILDING SHALL NOT EXCEED 2.0% CROSS SLOPE OR HAVE CROSS SLOPE LESS THAN 1.0%.
  - C. ADD 2500 FEET TO ALL SPOT ELEVATIONS FOR ACTUAL ELEVATION.
  - D. BUILDING FINISH FLOOR ELEVATION 2561.50' REFERS TO ARCHITECTURAL ELEVATION 100'-0".
  - E. SPOT ELEVATIONS INDICATE TOP OF CONCRETE SURFACE UNLESS NOTED OTHERWISE AS FOLLOWS:
    - E.A. FF = FINISH FLOOR
    - E.B. FL = FLOW LINE
    - E.C. ME = MATCH EXISTING
    - E.D. RIM = RIM OF STRUCTURE
    - E.E. TA = TOP OF ASPHALT
    - E.F. TBC = TOP BACK OF CURB
    - E.G. TC = TOP OF CONCRETE
    - E.H. FG = FINISH GRADE
  - F. SET FLAT WORK ADJACENT TO BUILDING FLUSH WITH FINISH FLOOR AT DOOR OPENING AND UNIFORMLY TRANSITION TO REVEAL 2" OF STEM WALL UNLESS INDICATED OTHERWISE. SLOPES SHALL NOT EXCEED 2% IN ANY DIRECTION ADJACENT TO THE BUILDING.



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**KETCHUM FIRE STATION**

107 SADDLE ROAD, KETCHUM, ID 83353

**ISSUES**

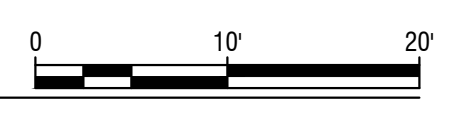
PHASE	DESIGN REVIEW	
DATE	FEBRUARY 3, 2020	
JOB NUMBER	18-022	
MARK	DATE	DESCRIPTION

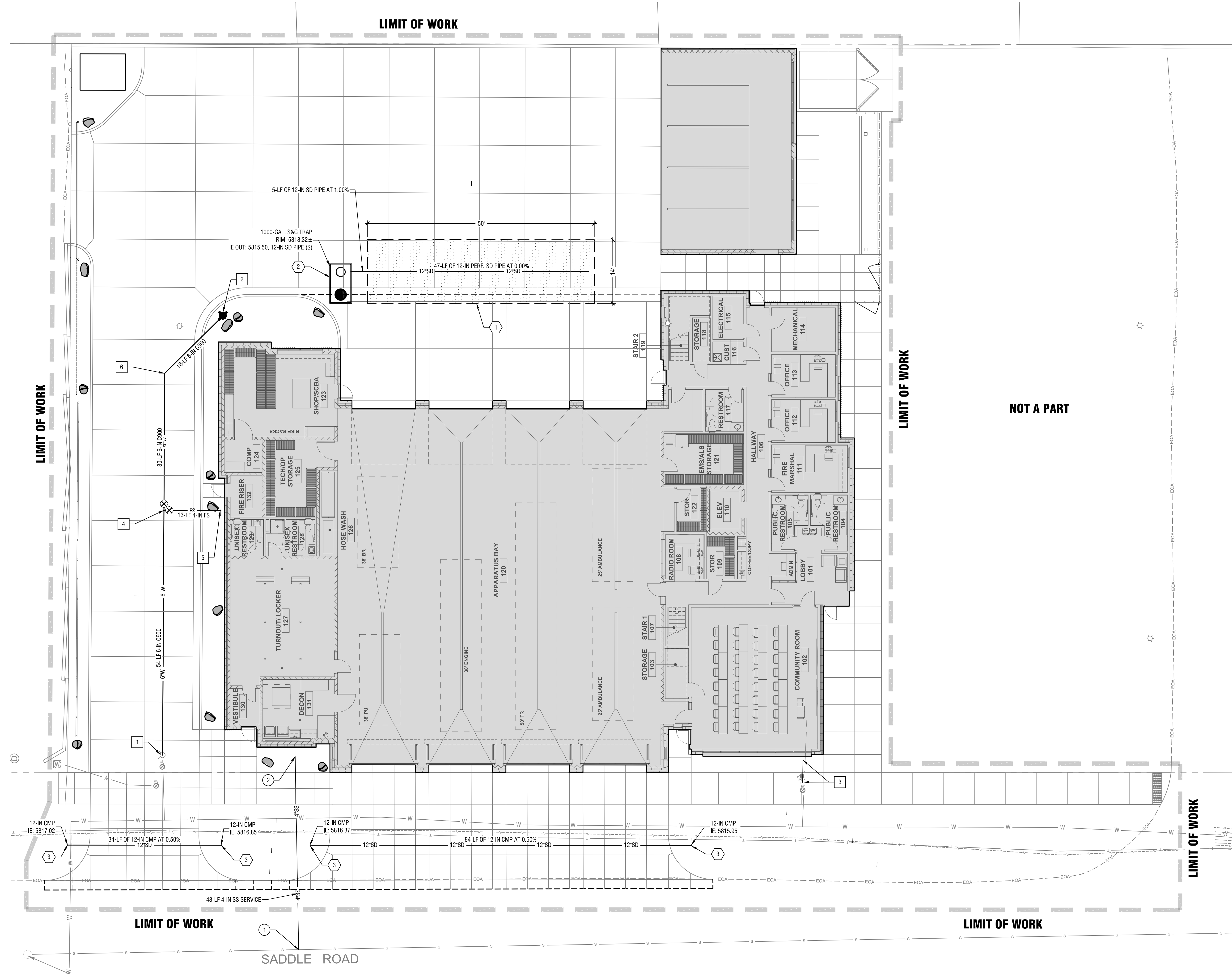
**SHEET NAME**

**GRADING PLAN**

**SHEET NUMBER**

**C3.00**





**Sheet Notes:**

- ALL CONSTRUCTION SHALL BE COMPLETED IN ACCORDANCE WITH THE LATEST EDITION OF THE IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION AND THE CITY OF KETCHUM REVISIONS TO THE ISPMC.
- CONTRACTOR IS RESPONSIBLE FOR VERIFYING LOCATION AND ELEVATION OF ALL TIE IN POINTS PRIOR TO CONSTRUCTION AND SHALL CONTACT THE DESIGN ENGINEER WITH ANY DISCREPANCIES.
- BOTH DOMESTIC AND FIRE SERVICE LINES REQUIRE A BACKFLOW PREVENTION DEVICE AT THE POINT OF ENTRY TO ANY BUILDING. THE TYPE OF DEVICE IS CONTINGENT UPON THE DEGREE OF HAZARD AND MUST MEET IDAHO DEQ STANDARDS. SEE MECHANICAL PLANS FOR BACK FLOW PREVENTION DEVICE.
- PIPE LENGTHS SPECIFIED ARE FROM FITTING TO FITTING OR FITTING TO CONNECTION POINT AND ARE SHOWN FOR CONVENIENCE ONLY AS ACTUAL INSTALLED LENGTH MAY VARY FROM PLAN.

**Water Keynotes:**

- REMOVE EXISTING FIRE HYDRANT AND REINSTALL AT NEW LOCATION.
- RE-INSTALL FIRE HYDRANT ASSEMBLY PER ISPMC SD-404.
- INSTALL WATER METER PER CITY OF KETCHUM STANDARDS. COORDINATE CONTINUATION WITHIN STRUCTURE PER MECHANICAL PLAN.
- INSTALL FIRE-SERVICE STUB IN COORDINATION WITH BUILDING FIRE SPRINKLER DESIGN. INSTALL APPROPRIATELY SIZED FITTINGS AND THRUST BLOCK PER ISPMC SD-403. COORDINATE SIZES WITH FIRE SPRINKLER DESIGN. APPROXIMATE 4-INCH LINE.
- COORDINATE CONTINUATION OF FIRE SERVICE WITHIN STRUCTURE PER FIRE SPRINKLER DESIGN.
- INSTALL 45° BEND AND THRUST BLOCK PER ISPMC SD-403.

**Sewer Keynotes:**

- TAP EXISTING SEWER MAIN. COORDINATE CONNECTION WITH THE CITY OF KETCHUM.
- COORDINATE CONTINUATION OF FIRE SERVICE WITHIN STRUCTURE PER FIRE SPRINKLER DESIGN.

**Storm Drain Keynotes:**

- INSTALL 14" x 50' L X 7' D SEEPAGE BED.
- INSTALL OLDCASTLE PRECAST 1000 GALLON SAND AND GREASE TRAP OR APPROVED EQUIVALENT.
- INSTALL FLARE END SECTION.

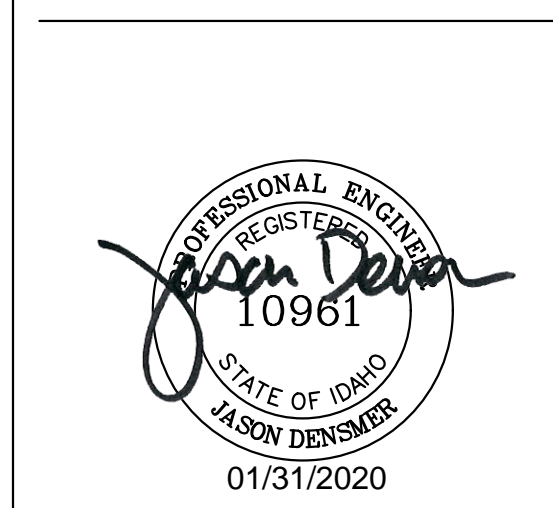


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**KETCHUM FIRE STATION**

107 SADDLE ROAD, KETCHUM, ID 83353

**ISSUES**

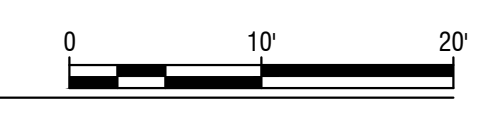
PHASE	DESIGN REVIEW	
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MARK	DATE	DESCRIPTION

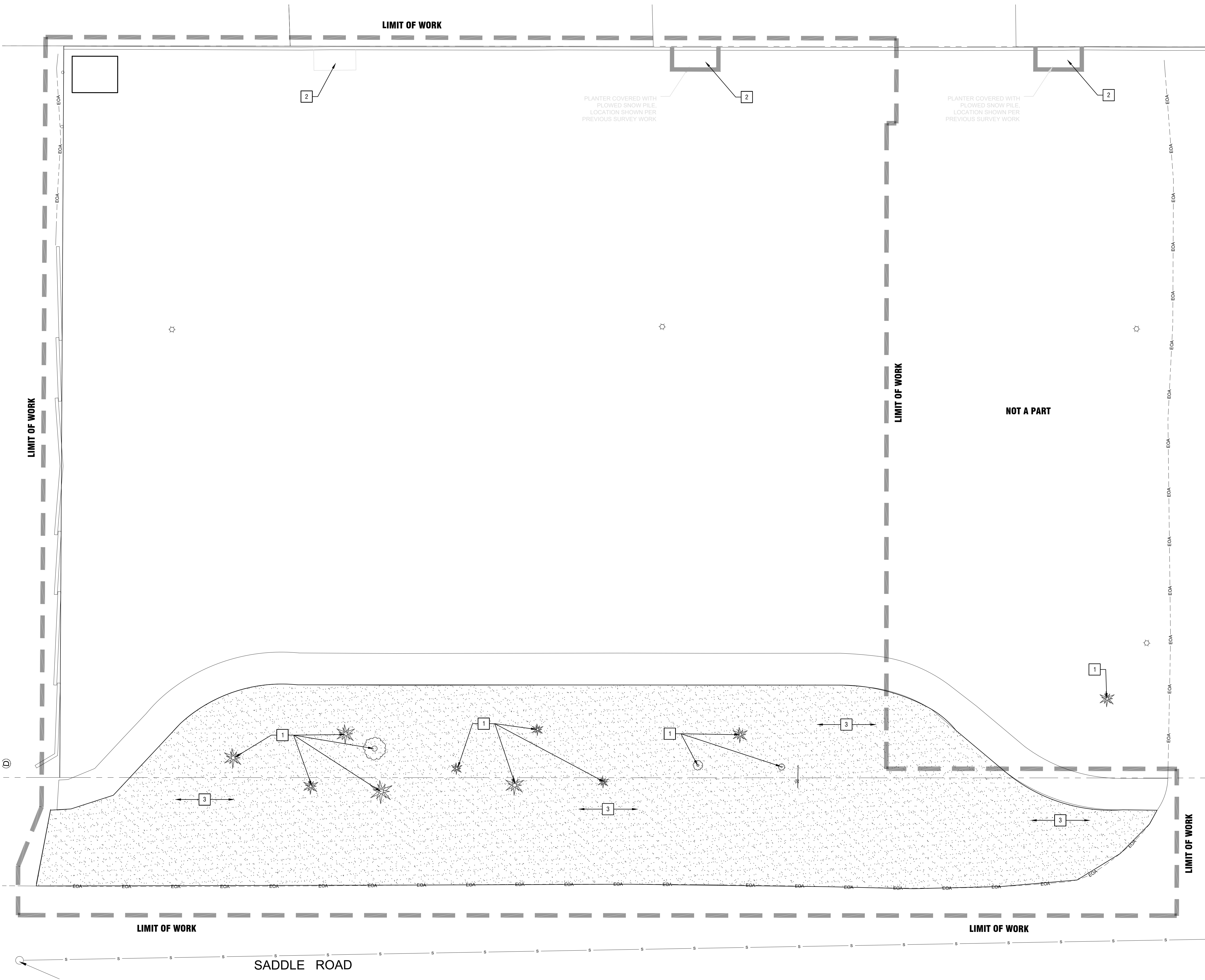
**SHEET NAME**

**UTILITIES PLAN**

**SHEET NUMBER**

**C4.00**





**Sheet Notes:**

- 1. REFER TO L1.00 FOR PROPOSED LANDSCAPE.

**Material Legend:**



**Keynotes:**

- 1. REMOVE EXISTING TREE.
- 2. REMOVE EXISTING WALL, PLANTER AND VEGETATION IN PLANTER.
- 3. REMOVE EXISTING LAWN.

# CALLOUT NUMBERS COORDINATED TO NUMBERED NOTES BELOW.

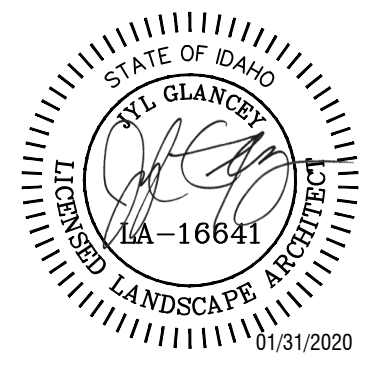


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**KETCHUM FIRE STATION**

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**ISSUES**

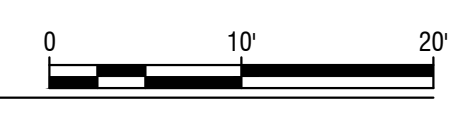
MARK	DATE	DESCRIPTION

**SHEET NAME**

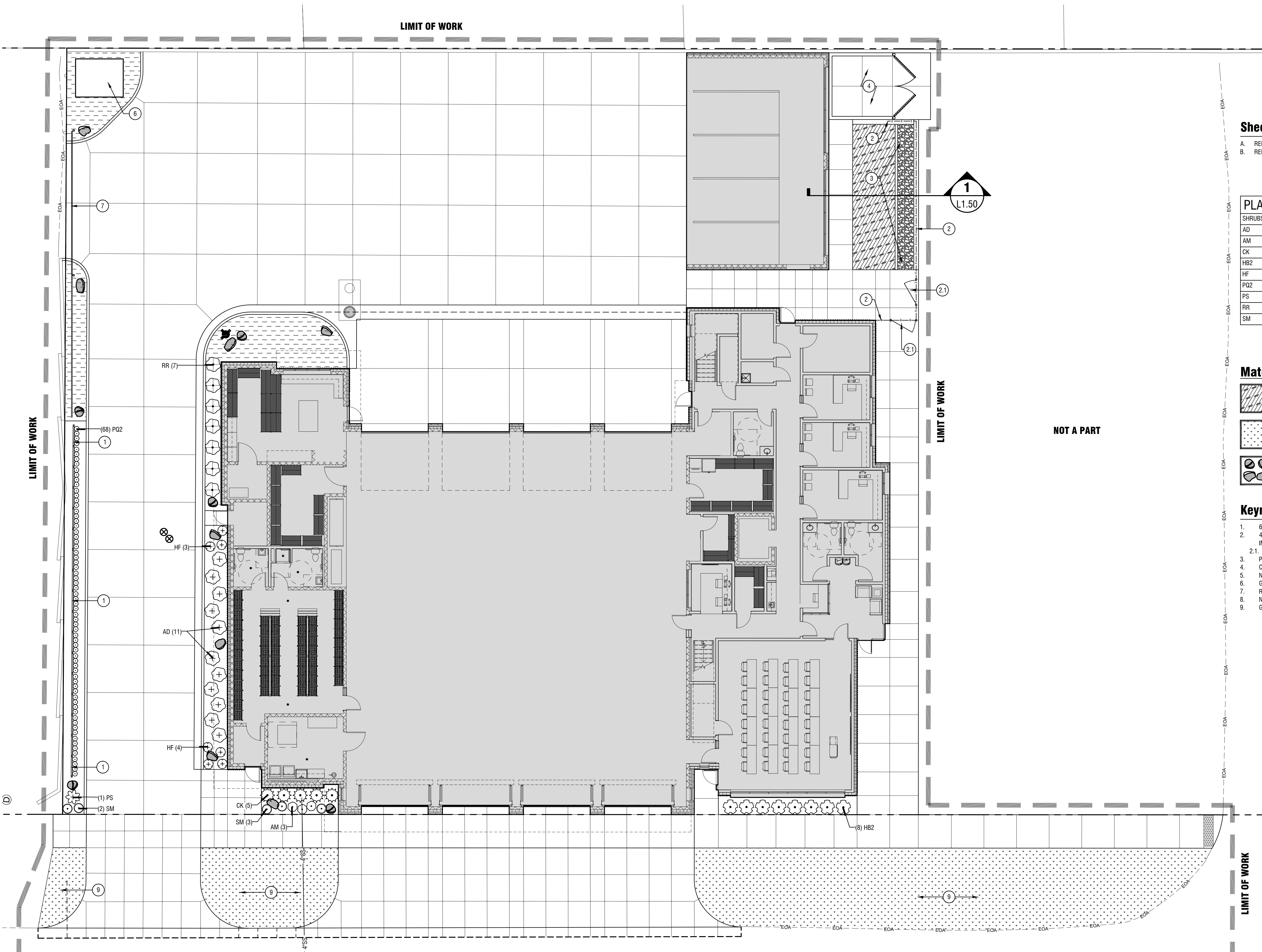
**EXISTING LANDSCAPE**

**SHEET NUMBER**

**L0.00**



LIMIT OF WORK



Sheet Notes:

- A. REFER TO SHEET L1.50 FOR LANDSCAPE AND IRRIGATION NOTES.
- B. REFER TO SHEET L0.00 FOR EXISTING LANDSCAPE.

PLANT SCHEDULE

SHRUBS	BOTANICAL / COMMON NAME	SIZE	QTY
AD	ASTILBE X ARENDSII 'DEUTSCHLAND' / DEUTSCHLAND FALSE SPIRAEA	3 GAL.	11
AM	ACHILLEA X 'MOONSHINE' / MOONSHINE YARROW	1 GAL.	3
CK	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / FEATHER REED GRASS	3 GAL.	5
HB2	HELIOTRICHON SEMPERVIRENS 'BLUE OATS' / BLUE OAT GRASS	1 GAL.	8
HF	HEUCHERA X 'FIRE CHIEF' / FIRE CHIEF CORAL BELLS	1 GAL.	7
PO2	PARTHENOCISSUS QUINQUEFOLIA / VIRGINA CREEPER	1 GAL.	68
PS	PINUS MUGO 'SLOWMOUND' / MUGO PINE	5 GAL.	1
RR	RHAMNUS FRANGULA 'RON WILLIAMS' / FINE LINE FERN LEAF BUCKTHORN	5 GAL.	7
SM	SALVIA X SYLVESTRIS 'MAY NIGHT' / MAY NIGHT SAGE	1 GAL.	5

Material Legend:

- SYNTHETIC TURF - SEE DETAIL 3/L1.50.
- TURF SOD
- LARGE DARK GREY - TAN BOULDERS, 2-3' DIA. TYP. X 1'-2' HIGH.
- 3-IN DEPTH 4-IN - 6-IN ROUND RIVER ROCK. ROCK SHALL BE FREE OF FINES AND LESS THAN 2-IN IN SIZE.
- 3-IN DEPTH 1-IN CRUSHED DECORATIVE ROCK MULCH. COLOR: DARK TAN - GREY.

Keynotes:

- 1. 6-FT TALL FREE STANDING GREENSCREEN TRELLISING SYSTEM - SEE IMAGE 5/L1.50.
- 2. 4-FT TALL ONE INCH HORIZONTAL SLAT FENCE BY LANDSCAPE FORMS. COLOR: STORMCLOUD. SEE IMAGE 4/L1.50.
- 2.1. 5-FT WIDE GATE.
- 3. PATIO STRUCTURE POSTS AND STRUCTURE BY ARCHITECTS.
- 4. CMU TRASH ENCLOSURE.
- 5. NOT USED.
- 6. GENERATOR BY MECHANICAL.
- 7. ROLLING GATE.
- 8. NOT USED.
- 9. GRASS DRAINAGE SWALE.

CALLOUT NUMBERS COORDINATED TO NUMBERED NOTES BELOW.



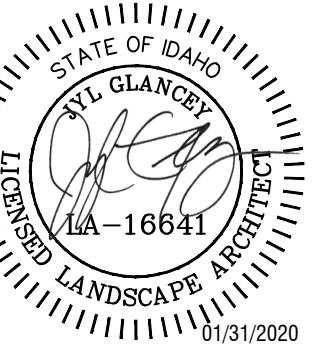
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KETCHUM FIRE STATION

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ISSUES

PHASE	DESIGN REVIEW
DATE	FEBRUARY 3, 2020
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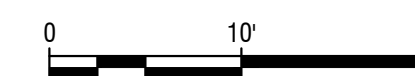
LANDSCAPE PLAN

SHEET NUMBER

L1.00

Design Review-Landscape Plan

Horizontal Scale: 1" = 10'



SEWER MANHOLE RIM = 5819.17  
INVERT = 5810.19

NOT A PART

LIMIT OF WORK

LIMIT OF WORK

LIMIT OF WORK

LIMIT OF WORK

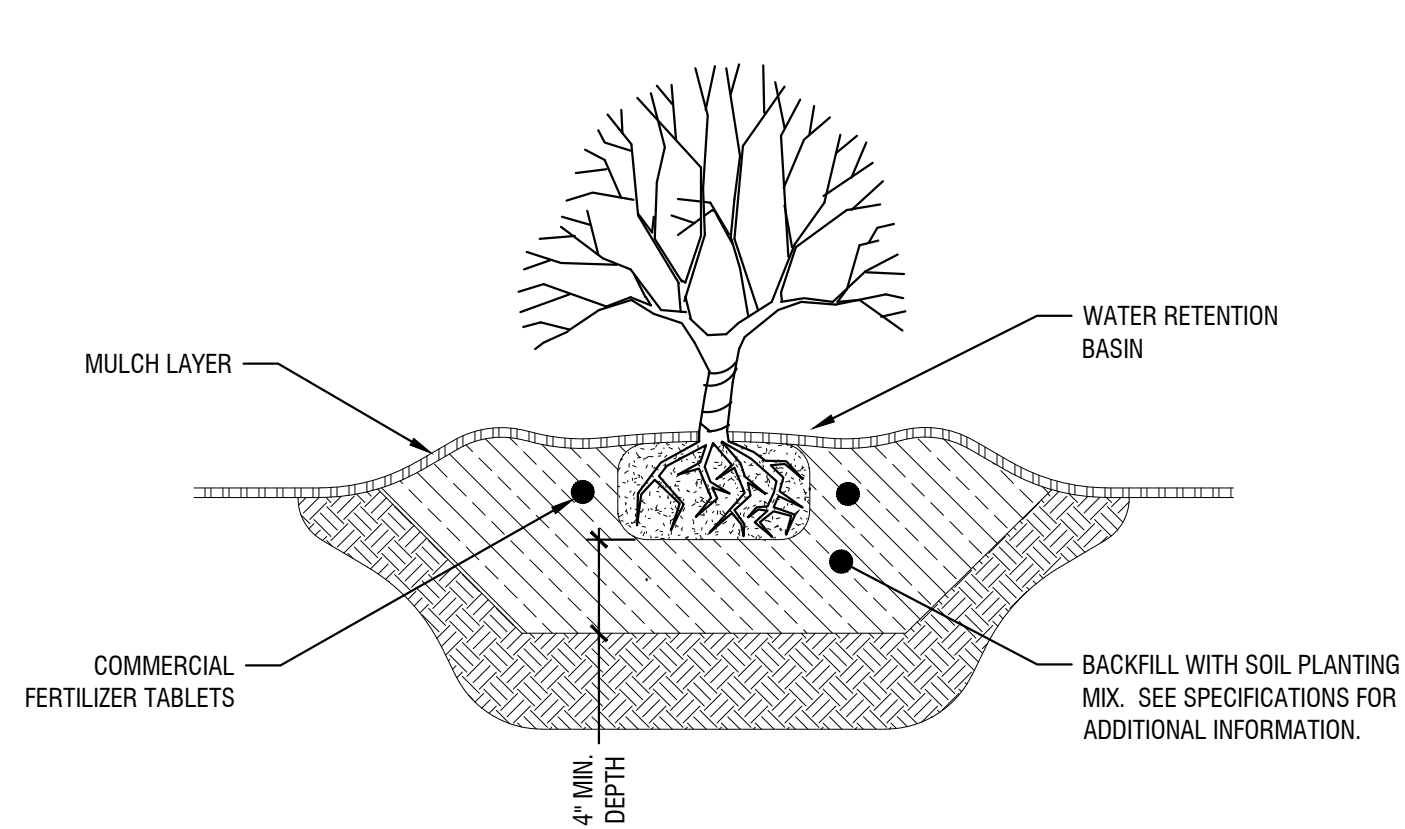
SADDLE ROAD

**Landscape Notes:**

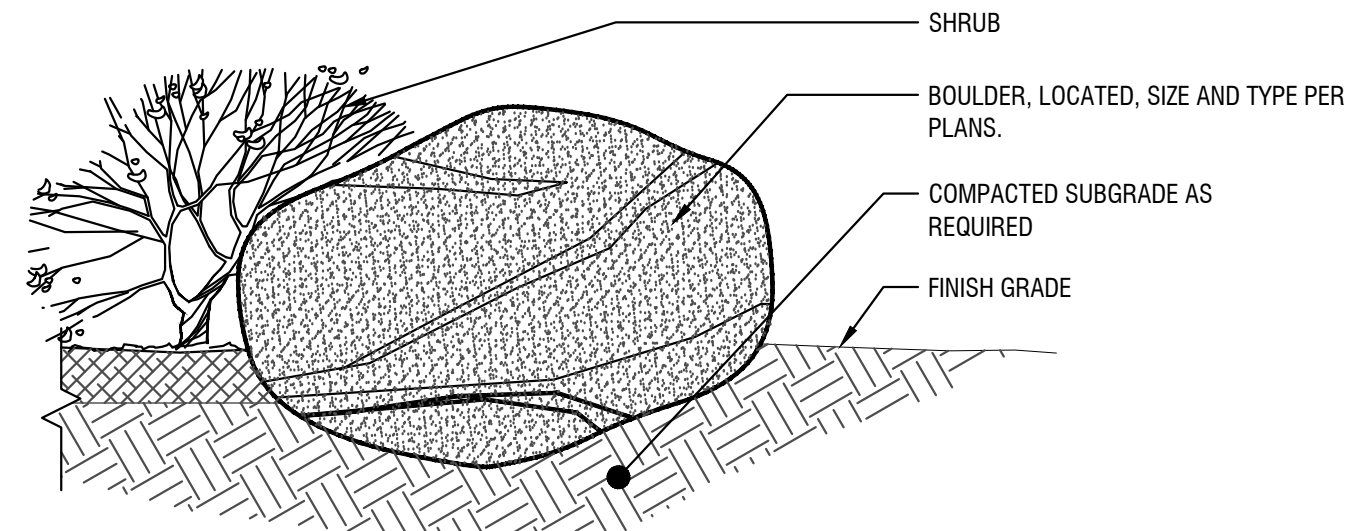
- A. CONTRACTOR SHALL REPORT TO LANDSCAPE ARCHITECT ALL CONDITIONS WHICH IMPAIR AND/OR PREVENT THE PROPER EXECUTION OF THIS WORK, PRIOR TO BEGINNING WORK.
- B. FINISH GRADES TO BE SMOOTH AND EVEN GRADIENTS WITH POSITIVE DRAINAGE IN ACCORDANCE WITH SITE GRADING PLAN. REMOVE RIDGES AND FILL DEPRESSIONS, AS REQUIRED TO MEET FINISH GRADES. PLACE 3" OF BARK MULCH, UNLESS SPECIFIED OTHERWISE, OVER SUBGRADE SOIL TO ACHIEVE FINISH GRADE. FINISH GRADE RELATED TO ADJACENT SITE ELEMENTS SHALL BE:
  - B.A. 1-INCH BELOW TOP OF ADJACENT PAVEMENT, VALVE BOX, VAULT, ETC.
  - B.B. 3-INCHES BELOW TOP OF CURB UNLESS NOTED OTHERWISE.
- C. ALL PLANTING BEDS SHALL HAVE A MINIMUM OF 18" OF TOPSOIL. SOD AREAS A MINIMUM OF 12" OF TOPSOIL. SPREAD, COMPACT AND FINE GRADE TOPSOIL TO A SMOOTH AND UNIFORM GRADE.
- D. RE-USE EXISTING SURFACE TOPSOIL WHERE POSSIBLE. VERIFY SUITABILITY OF SURFACE SOIL TO PRODUCE TOPSOIL MEETING REQUIREMENTS AND AMEND WHEN NECESSARY. TOPSOIL SHALL BE A LOOSE, FRIABLE, SANDY LOAM, CLEAN AND FREE OF TOXIC MATERIALS, NOXIOUS WEEDS, WEED SEEDS, ROCKS, GRASS OR OTHER FOREIGN MATERIAL AND A PH OF 5.5 TO 7.0. IF ON-SITE TOPSOIL DOES NOT MEET THESE MINIMUM STANDARDS, CONTRACTORS ARE RESPONSIBLE TO EITHER: A) PROVIDE APPROVED IMPORTED TOPSOIL, OR B.) IMPROVE ON-SITE TOPSOIL WITH METHODS APPROVED BY LANDSCAPE ARCHITECT. SUPPLEMENT WITH IMPORTED TOPSOIL WHEN QUANTITIES ARE INSUFFICIENT. CLEAN TOPSOIL OF ROOTS, PLANTS, SODS, STONES, CLAY LUMPS AND OTHER EXTRANEOUS MATERIALS HARMFUL TO PLANT GROWTH.
- E. IF IMPORTED TOPSOIL FROM OFF-SITE SOURCES IS REQUIRED, PROVIDE NEW TOPSOIL THAT IS FERTILE, FRIABLE, NATURAL LOAM, SURFACE SOIL, REASONABLY FREE OF SUBSOIL, CLAY LUMPS, BRUSH, WEEDS AND OTHER LITTER, AND FREE OF ROOTS, STUMPS, STONES LARGER THAN 2 INCHES IN ANY DIMENSION, AND OTHER EXTRANEOUS OR TOXIC MATTER HARMFUL TO PLANT GROWTH.
- F. OBTAIN TOPSOIL FROM LOCAL SOURCES OR FROM AREAS HAVING SIMILAR SOIL CHARACTERISTICS TO THAT FOUND AT PROJECT SITE. OBTAIN TOPSOIL ONLY FROM NATURALLY, WELL-DRAINED SITES WHERE TOPSOIL OCCURS IN A DEPTH OF NOT LESS THAN 4 INCHES.
- G. ALL LANDSCAPE AREAS SHALL BE WEED FREE AT THE TIME OF LANDSCAPE INSTALLATION. REMOVE ALL ROOTS, WEEDS, ROCKS AND FOREIGN MATERIAL ON THE SURFACE.
- H. NEW SHRUB PLANTING. SEE DETAIL 1/L1.50.
- I. ALL PLANT MATERIAL SHALL CONFORM TO THE AMERICAN NURSERYMAN STANDARDS FOR TYPE AND SIZE SHOWN. PLANTS WILL BE REJECTED IF NOT IN A SOUND AND HEALTHY CONDITION.
- J. ALL PLANT MATERIAL SHALL BE GUARANTEED FOR A PERIOD OF ONE YEAR BEGINNING AT THE DATE OF SUBSTANTIAL COMPLETION. REPLACE ALL PLANT MATERIAL FOUND DEAD OR NOT IN A HEALTHY CONDITION IMMEDIATELY WITH THE SAME SIZE AND SPECIES AT NO COST TO THE OWNER.

**Automatic Underground Irrigation Notes:**

- A. ALL LANDSCAPED AREAS SHALL HAVE AN AUTOMATIC UNDERGROUND SPRINKLER SYSTEM WHICH ENSURES COMPLETE COVERAGE AND PROPERLY ZONED FOR REQUIRED WATER USES.
- B. EACH HYDROZONE IS TO BE IRRIGATED WITH SEPARATE INDIVIDUAL STATIONS.
- C. POP-UP SPRINKLER HEADS SHALL HAVE A MINIMUM RISER HEIGHT OF 18" AT PLANTER BEDS, 6" AT LAWN AREAS.
- D. PLANTER BEDS ARE TO HAVE DRIP IRRIGATION SYSTEMS - WITH DRIP CONTROL ZONE KIT AND 150 MESH FILTER (MIN.).
- E. ELECTRONIC WATER DISTRIBUTION/ TIMING CONTROLLERS ARE TO BE PROVIDED. MINIMUM CONTROLLER REQUIREMENTS ARE AS FOLLOWS:
  - E.A. PRECISE INDIVIDUAL STATION TIMING
  - E.B. RUN TIME CAPABILITIES FOR EXTREMES IN PRECIPITATION RATES
  - E.C. AT LEAST ONE PROGRAM FOR EACH HYDROZONE
  - E.D. SUFFICIENT MULTIPLE CYCLES TO AVOID WATER RUN-OFF
  - E.E. POWER FAILURE BACKUP FOR ALL PROGRAMMED INDIVIDUAL VALVED WATERING STATIONS WILL BE DESIGNED AND INSTALLED TO PROVIDE WATER TO RESPECTIVE HYDRO-ZONES.
- F. INDIVIDUAL VALVED WATERING STATIONS WILL BE DESIGNED AND INSTALLED TO PROVIDE WATER TO RESPECTIVE HYDRO-ZONES.
- G. THE IRRIGATION SYSTEM SHALL BE DESIGNED TO PROVIDE 100% COVERAGE WITH HEAD TO HEAD SPACING OR TRIANGULAR SPACING AS APPROPRIATE.
- H. SPRINKLER HEADS SHALL BE ADJUSTED TO REDUCE OVERSPRAY ONTO IMPERVIOUS SURFACES SUCH AS SIDEWALKS, DRIVEWAYS, AND PARKING AREA.
- I. EACH VALVE SHALL BE INSTALLED IN A VALVE BOX LARGE ENOUGH TO ALLOW FOR MAINTENANCE AND REMOVAL. ONLY ONE VALVE PER BOX.

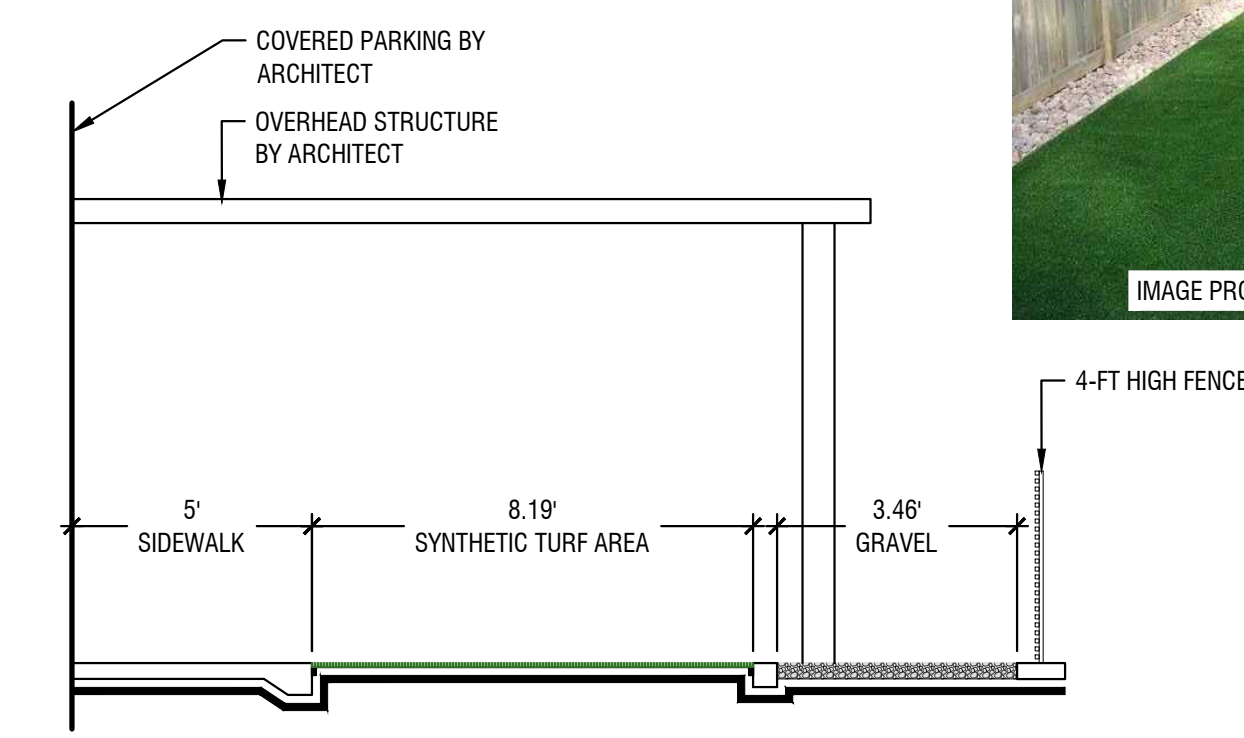


**1 Shrub Planting**  
Scale: NTS



- NOTES:
1. NOTIFY LANDSCAPE ARCHITECT WHEN PLACING BOULDERS FOR APPROVAL.
  2. PLACE BOULDERS PRIOR TO INSTALLATION OF IRRIGATION SYSTEM.
  3. CLEAN ALL BOULDERS OF DIRT AND LOOSE DEBRIS.
  4. WHEN PLACING BOULDERS, BURY 1/4 TO 1/3 OF BOULDER BELOW FINISH GRADE.
  5. DO NOT SCAR OR DAMAGE BOULDERS.

**2 Boulder Installation**  
Scale: NTS



**3 Patio Section**  
Scale: 1/4" = 1'



**4 Patio Fence**  
Scale: NTS



**5 Greenscreen Trellis System**  
Scale: NTS

**STAMP**



**CONSULTANT**



**PROJECT INFORMATION**



**KETCHUM FIRE STATION**

107 SADDLE ROAD, KETCHUM, ID 83353

**ISSUES**

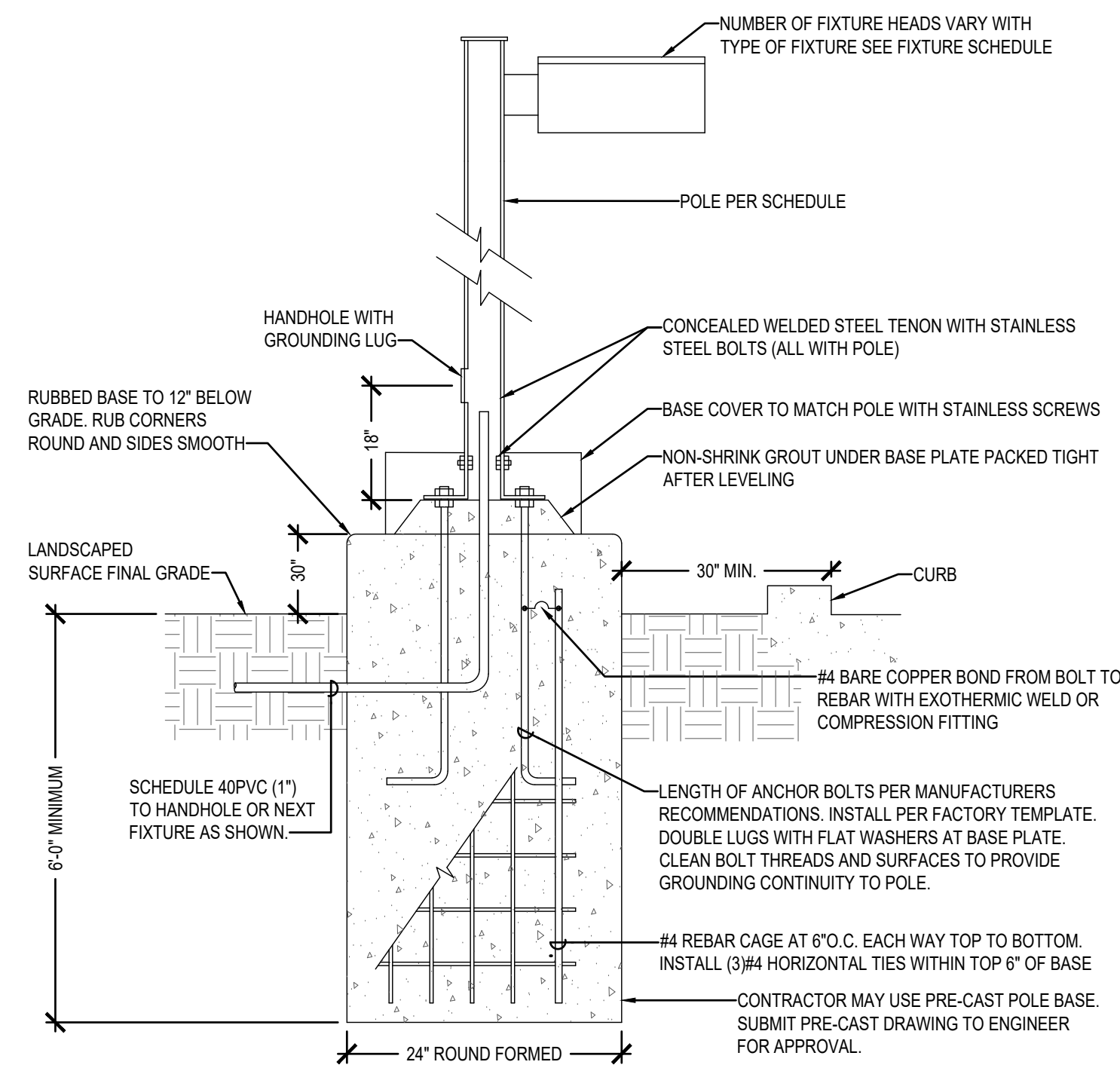
PHASE	DESIGN REVIEW	
DATE	FEBRUARY 3, 2020	
JOB NUMBER	18-022	
MARK	DATE	DESCRIPTION

**SHEET NAME**

**LANDSCAPE DETAILS**

**SHEET NUMBER**

**L1.50**



**DETAIL NOTES**

1. INCREASE THE SPECIFIED HEIGHT OF THE POLE BY 2'-0" TO ENSURE UNIFORM FIXTURE MOUNTAIN HEIGHT WHERE THE 6" BASE IS USED.
2. PROTECTED AREA IMPLIES THE POLE IS INSTALLED AT LEAST 30' FROM PARKING OR DRIVE AREAS.

**SITE LIGHTING POLE BASE DETAIL**  
NTS

**ELECTRICAL LEGEND - LIGHTING**

- ⊕ RECESSED LIGHT FIXTURE
- ◻ POLE LIGHT 1 HEAD WITH POLE
- ⊞ EXTERIOR WALL PACK

**LIGHTING FIXTURE SCHEDULE (19-366)**

TYPE	DESCRIPTION	MTG.	LAMPS	WATTAGE	MANUFACTURER
P1	LED VISUAL COMFORT LENS ON +12'-0" POLE AND +2'-6" CONCRETE BASE	POLE MOUNTED	LED 11,000 LUMENS 2700K	85	LITHONIA NO. RAD1 LED SERIES
RR1	LED 4" ROUND RECESSED CAN LIGHT	RECESSED	LED 1000 LUMENS 2700K	10.6	LITHONIA NO. LDN4 SERIES
WB1	ARCHITECTURAL WALL SCONCE WITH VISUAL COMFOR LENS	WALL MOUNTED	LED 3000 LUMENS 2700K	25	LITHONIA NO. WST LED SERIES
WB2	ARCHITECTURAL WALL SCONCE WITH VISUAL COMFOR LENS	WALL MOUNTED	LED 1500 LUMENS 2700K	11	LITHONIA NO. WST LED SERIES
WB3	ARCHITECTURAL WALL SCONCE WITH VISUAL COMFOR LENS	WALL MOUNTED	LED 6000 LUMENS 2700K	50	LITHONIA NO. WST LED SERIES

**LIGHTING CONTROLS NOTES**

THE EXTERIOR BUILDING LIGHTING AND POLE LIGHTS WILL BE ROUTED THROUGH THE LIGHTING CONTROL PANEL AND PHOTOCCELL. THE LIGHTING CONTROL PANEL WILL TURN LIGHTS ON AND OFF BASED ON A TIMED SCHEDULE AND THE PHOTOCCELL WILL TURN LIGHTS ON AND OFF BASED ON LIGHT CONDITIONS. THE EXTERIOR LIGHTS AND POLE LIGHTS WILL ALSO BE EQUIPPED WITH FACTORY INSTALLED MODULES TO ALLOW THE FIXTURES TO BE DIMMED AT A PRESET TIME TO REDUCE LIGHT LEVELS. FOR EXAMPLE FROM DUSK TO 11:00 PM AND 5:00 AM TO 7:00 AM THE FIXTURES TO BE DIMMED AT A PRESET TIME TO REDUCE LIGHT LEVELS. FROM 11:00 PM TO 5:00 AM THE FIXTURES COULD BE DIMMED TO 30% LIGHT OUTPUT. THESE FIXTURE WILL ALSO BE CONNECTED TO THE EMERGENCY ALARM SYSTEM IN THE BUILDING. UPON ACTIVATION OF THE ALARM DURING THE DIMMING PRESET TIME, THESE FIXTURES WOULD RETURN TO FULL LIGHT OUTPUT.

**STAMP**



**CONSULTANT**



234 S. WHISPERWOOD WAY, BOISE, ID 83709  
PH: (208) 384-0985  
MUSGROVE PROJECT NUMBER: 19-366

**PROJECT INFORMATION**



**KETCHUM FIRE STATION**

107 SADDLE ROAD, KETCHUM, ID 83353

**ISSUES**

PHASE DESIGN REVIEW

DATE FEBRUARY 3, 2020

JOB NUMBER 18-022

MARK	DATE	DESCRIPTION

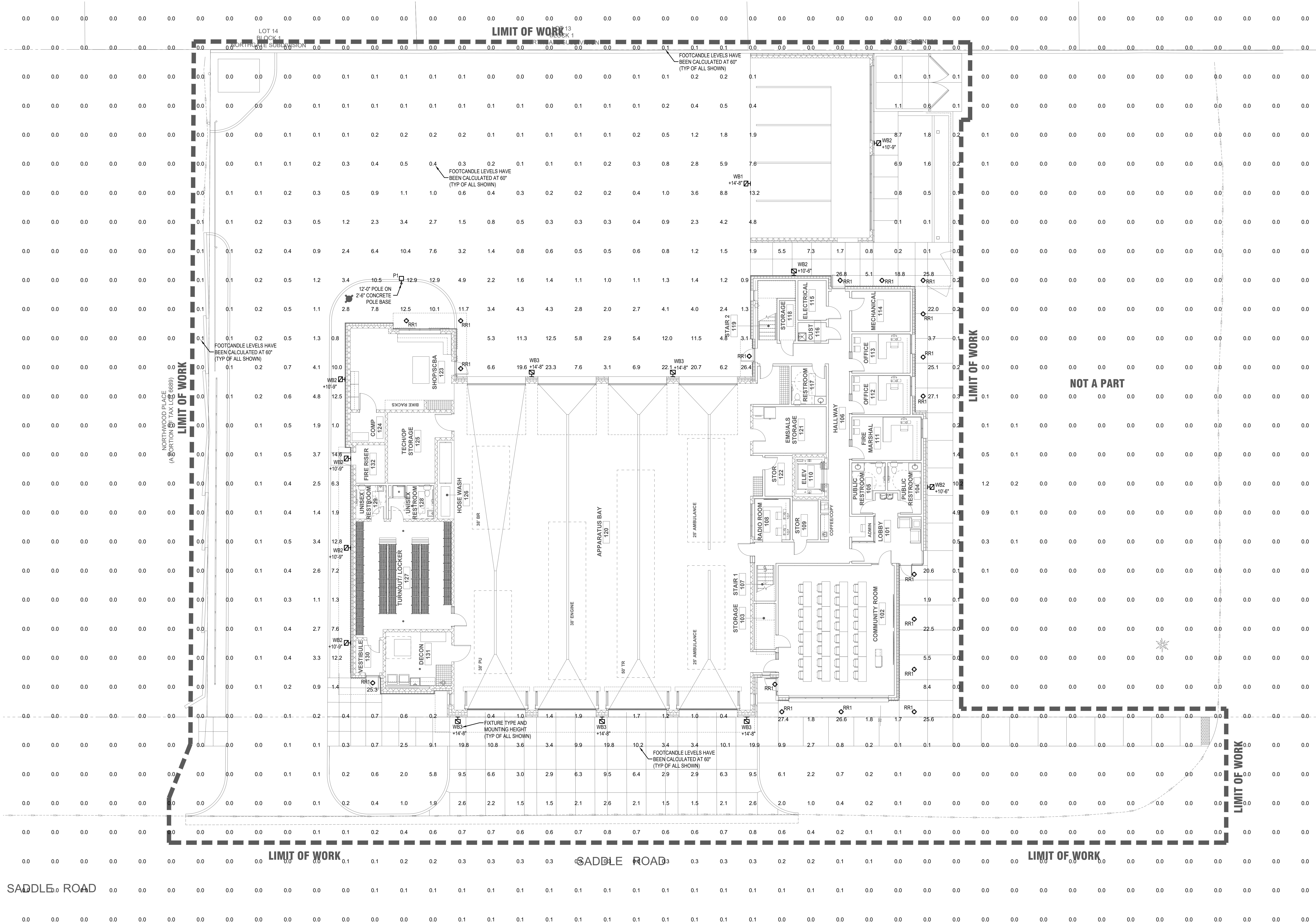
**SHEET NAME**

**ELECTRICAL DETAILS AND NOTES**

**SHEET NUMBER**

**E0.00**





**1 SITE LIGHTING PHOTOMETRIC PLAN**  
SCALE | 1" = 10'-0"



1008 W Main St  
Boise, Idaho 83702  
208.345.1800  
colearchitects.net  
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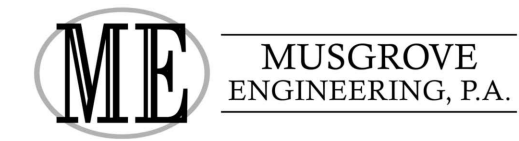
ARCHITECTURE + PLANNING + DESIGN

6211 Roosevelt Way NE  
Seattle, WA 98115  
206.522.3820

STAMP



CONSULTANT



234 S. WHISPERWOOD WAY, BOISE, ID 83709  
PH: (208) 384-0885  
MUSGROVE PROJECT NUMBER: 19-366

PROJECT INFORMATION



**KETCHUM FIRE STATION**

107 SADDLE ROAD, KETCHUM, ID 83353

ISSUES

PHASE	DESIGN REVIEW	
DATE	FEBRUARY 3, 2020	
JOB NUMBER	18-022	
MARK	DATE	DESCRIPTION

SHEET NAME

**PHOTOMETRIC SITE PLAN**

SHEET NUMBER

**E1.00**





## Exhibit B:

Response to Planning and  
Zoning Commission Comments  
on the Fire Station Pre-  
Application Design Review

# Response to Planning and Zoning Commission Comments on the Fire Station Pre-Application Design Review

## Site Location

While the selection of the fire station site location is not part of the design review process, the following information is provided in response to Commission comments made at the January 13, 2020 meeting.

Between 2001 and 2016, the City evaluated different locations for a new fire station. Sites considered consisted of the following locations:

- City property on Lewis Street
- City property adjacent to the YMCA (north and south properties)
- Stock Building site
- City Hall site

In September 2017 a new site evaluation process was initiated. A group of 11 people that included police and fire personnel, real estate and development professionals, and residents of Ketchum evaluated sites in and around Ketchum and assessed feasibility. All known feasible sites were evaluated. Attachment A provides a summary of the sites evaluated. The group identified 22 locations. These 22 locations were further evaluated for accessibility and other factors, including response time.

Initially, the City site south of the YMCA was identified as a preferred location. However, after public review and comment, that site was no longer under consideration. Two sites emerged as preferred locations, the City owned dirt lot north of the YMCA and the private property at 290 and 298 Northwood Way. The Northwood Way properties were available for purchase for \$3,036,000. Of the two sites, the City Council selected the dirt lot north of the YMCA.

The selected site is suitable with or without consolidation of the north valley fire agencies. The site is centrally located with access to Highway 75, Saddle Road into the City of Sun Valley, and Warm Springs Road.

Unlike some of the other sites considered, this location provides enough area for drive-through apparatus bays. Without drive-through bays, vehicles would have to back into the apparatus bay.

Throughout the 2019 Fire Bond campaign and at the 20 public meetings and open houses, the Saddle Road site was presented as the preferred location. After the bond passed, the City Council conducted a public meeting to review any other proposed locations. No other locations were presented and based on the input from the public and Fire Chief, the Council approved the Saddle Road location.

## Parking

As shown in Exhibit A, the fire station is located within a larger parcel owned by the City of Ketchum, it is not a separate parcel.



EXHIBIT A

The parcel contains approximately 170 public parking spaces between the YMCA and fire station and 50 spaces in the parking lot south of the YMCA. Over 100 vehicles can be parked on the street along Saddle Road and in and around Rotary Park. In total, the area can accommodate over 320 vehicles in the surrounding area not including the Presbyterian Church parking lot.

Parking in the area is all shared public parking. No single user has exclusive use or dedicated parking for their use. The City has determined that shared parking is the most efficient, sustainable and cost-effective way to provide parking for the various users in the area. This approach takes advantage of the fact that most parking spaces are only used part time by a particular user. Providing dedicated parking for any single user will result in under-utilized parking and is counter to the transportation and mobility goals of the City.

The parking demands of the fire station will fluctuate depending on the type of emergency call. The existing fire station is in the downtown core adjacent to the most congested and high demand parking area in Ketchum. Firefighters responding to a call must drive into the downtown and typically find parking on the street. While this is not ideal, to date, this practice has not impacted response times. The availability and access to parking for the new fire station is far superior to existing conditions.

On average, there are 2.7 incidents per day. The maximum number of firefighters responding to a major incident is 20. On Tuesday evenings, there would be an average of 16 vehicles coming to either the training facility on Lewis Street and/or the station for training. Training typically occurs between 5-8pm.

Daily, there will be 2-4 firefighters at the station in addition to the Fire Chief, Assistant Chief and Fire Clerk. The two chiefs drive command vehicles which will likely be parked within the fire station bays. Worst case, there is a demand for 5 parking spaces for the full-time staff during the day and as noted above, 20 spaces for other firefighters responding to a major call and approximately 16 spaces during Tuesday training sessions. Based on the available parking around

the site, there will be ample shared parking spaces to meet the parking demand of the station. There are over 60 public spaces within close proximity to the station.

Until the YMCA expands, the number of spaces reasonably necessary to support the YMCA, as identified by the YMCA, is 150 spaces. Assuming the YMCA is at capacity, the Presbyterian Church has an event and there is a major fire call, and all 320 parking places are occupied, the Fire Chief will facilitate parking for firefighters on the fire station site. There is space to park vehicles along the north driveway and in the rear of the station. While it is highly unlikely all uses will have peak parking at the same time, there is a plan in place to handle this situation if it occurs.

The availability of parking is an important issue and based on a careful analysis, the city has determined there is more than enough shared parking to accommodate the needs of all users in the area. In addition to the available parking, the site is located at the intersection of the two highest capacity bus routes (Valley and Blue Route) and within close proximity of the Wood River Multi-Use Path offering ample alternative transportation opportunities. Based on the availability of parking and alternative transportation options, the city is not prepared to dedicate public parking spaces for exclusive use of any use in the area, including the fire station.

### Housing

While the decision to include housing within the project is not part of the design review process, the following provides background information on the topic.

The availability of employee housing is a critical issue in the City of Ketchum. Without employees to plow and repair our streets, operate the water and wastewater system, maintain our parks and public facilities, and respond to fire and medical calls, the city simply cannot function and provide critical services. Providing employee housing is a top priority for the Mayor and City Council.

Constructing permanent housing as part of the fire station was contemplated in the early stages of the project development. After input from the full-time firefighters and an evaluation of project costs, it was determined permanent employee housing at this location was not attractive to employees and was expensive to build. Recognizing there is a critical need for employee housing, the Mayor and City Council have put all efforts towards developing over 50 units of community housing at the present city hall site. This project will accommodate employees and, as proposed, is expected to be more cost effective.

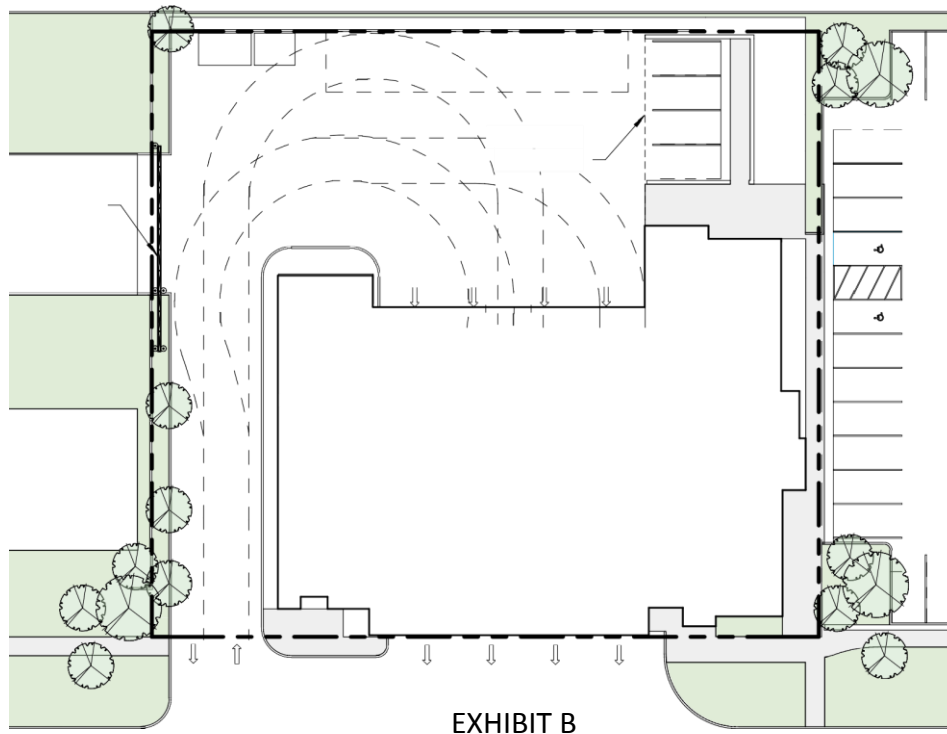
In the event there are funds and fire personnel demand to construct permanent housing units on the fire station site, the carport could readily be removed and replaced with housing units and parking. The option exists for future housing on the fire station site.

### Traffic Circulation

As mentioned at the January 13<sup>th</sup> meeting, the City has hired AECOM to conduct a traffic analysis to determine if there will be safety and circulation conflicts between pedestrians, vehicles and emergency vehicles and responders within the general area. If conflicts are identified, mitigation

measures will be implemented. This study is underway and will be presented to the City Council for consideration.

With respect to the circulation in and around the site, there will be a curb cut on the North end of the property providing two-way traffic to the rear of the fire station (Exhibit B). Command and utility vehicles and some staff vehicles will ingress and egress the facility from that north entrance. With current activity, this would be between 10 and 20 vehicles per day entering and exiting. In addition, this entrance will also be the entrance for fire and EMS apparatus. Some EMS apparatus may exit the facility from this location, however emergency response will primarily be from the curb cut at the apparatus bays.



There will be four apparatus bays fronting Saddle Road. Fire and EMS apparatus will be responding to emergencies from these bays. As identified above, on average, there are 2.7 incidents per day and 1.4 apparatus responding to each incident. This amounts to 3.8 apparatus responding daily to incidents. In addition, fire apparatus will be exiting on non-emergency tasks, which currently averages 2 to 4 trips on an average day. Our best estimate is that the number of non-emergency vehicle trips will increase slightly to a total of approximately 5 non-emergency vehicle trips on an average day. Vehicles exiting from the apparatus bays will be almost exclusively re-enter from the north driveway.

Currently 8.1 volunteer trips are made to the station each day. These occur over the course of the day and are not normally at the same time. The maximum number of firefighters responding to a major incident is 20. On Tuesday evenings, there would be an average of 16 vehicles coming to the



station for training. On joint training events, there may also be 2 to 4 fire apparatus coming from other fire departments.

Volunteers and off-duty firefighters may enter the north entrance, park in the joint parking lot south of the fire station or along Saddle Road.

From the emergency apparatus entrance, there are clear sight lines both to the north and south.

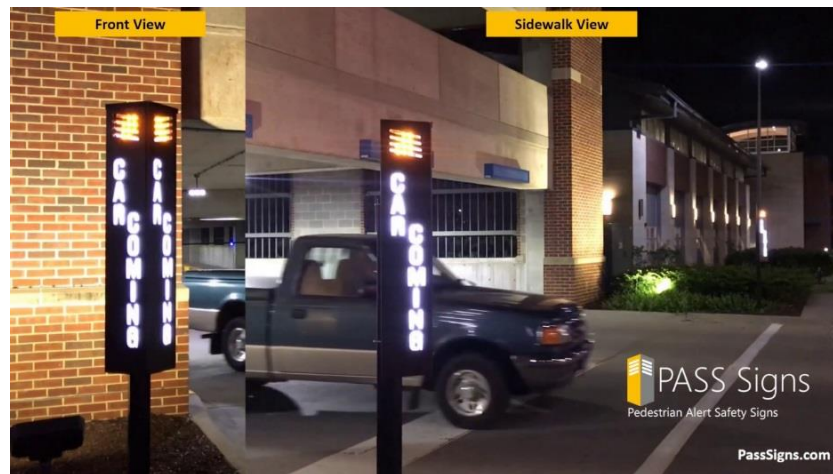
AECOM is conducting an analysis to determine if additional traffic and pedestrian control devices are necessary. Should it be determined control devices should be installed, systems such as those shown in Exhibit C could be considered.



STC optically-activated fire station warning signal

#### EXHIBIT C

The station is designed with sight line windows that will allow apparatus drivers to view pedestrians or bicyclists on sidewalks approaching the station as well as in front of the apparatus. In addition, if determined necessary by AECOM, there are systems to alert pedestrians and bicyclists of vehicles existing the driveway. Exhibit D provides some examples.



#### EXHIBIT D



EXHIBIT D

Even during emergency response, all apparatus will be required to come to a complete stop prior to pulling on to the roadway. Fire and EMS apparatus will also be required to activate emergency warning lights on the apparatus at all times, and sirens, when appropriate.

Volunteers and off-duty staff are not permitted by law to exceed speed limits or ignore traffic rules. This is also strictly enforced by a “no-tolerance” policy of the fire department.

There is adequate space in the rear courtyard for fire and EMS apparatus to turn around, and adequate space for freight delivery and other service uses.

The safety of first responders arriving and departing during emergencies along with the public around the station will be thoroughly evaluated by professional traffic engineers. If conflicts are identified, the City Council will implement measures to ensure the safety of all users in the area.

#### Design Comments

The Commission expressed concern over the dark color of the exterior brick. The graphic submitted made the brick appear to be darker than the actual appearance. At the February 10<sup>th</sup> meeting, samples of brick options will be provided for Commission review and approval.

ATTACHMENT A

# CITY-OWNED LOTS

# CURRENT CITY HALL LOT



Location: NE Corner of East Avenue & 5<sup>th</sup> Street

Legal: Lot 3A, Block 45

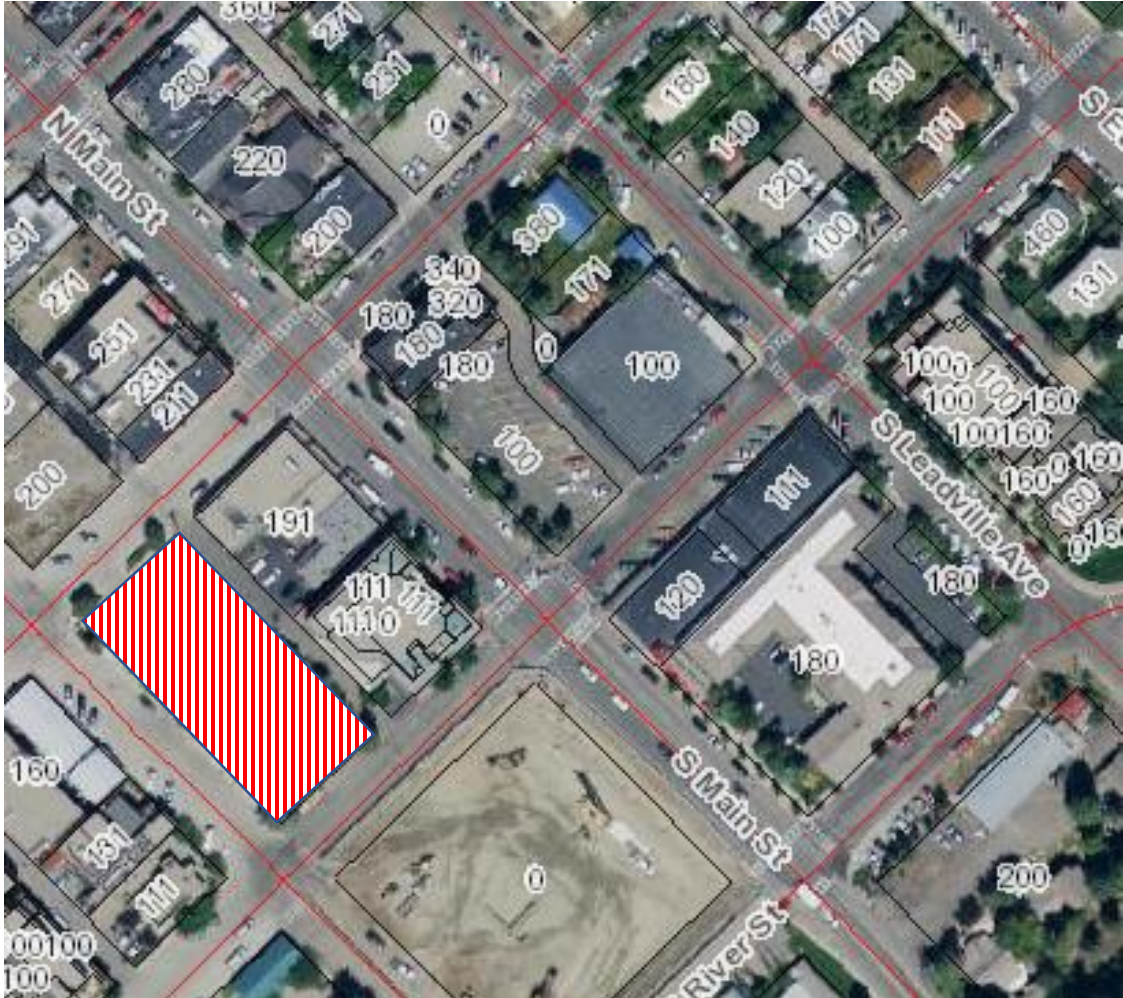
Size: 0.379 acres

Dimensions (WxD): Approx. 110' x 150'

Ownership: City of Ketchum

Market Value: N/A

# 1<sup>ST</sup> & WASHINGTON LOT



Location: Washington Avenue (btwn 1<sup>st</sup> & 2<sup>nd</sup> Streets)

Legal: Lot 5-8, Block 19

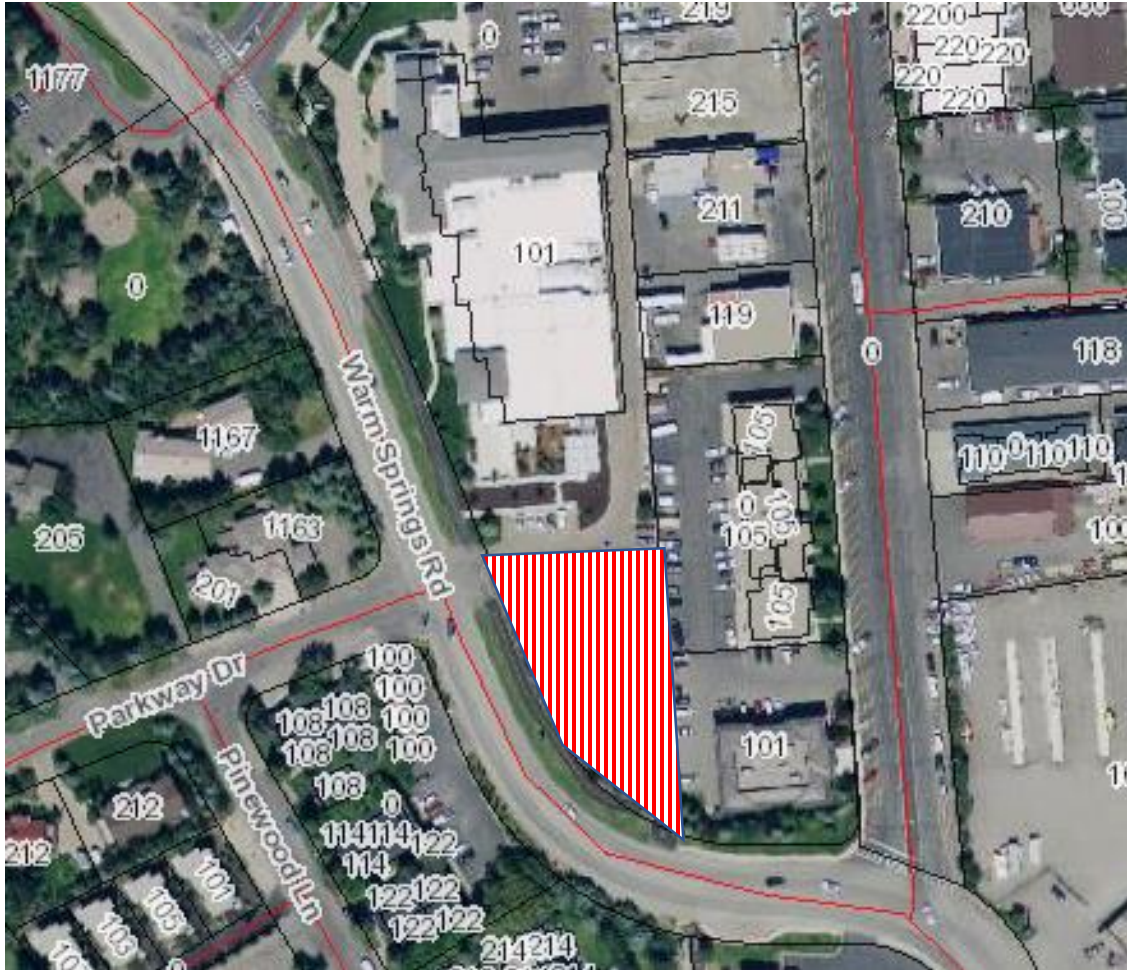
Size: 0.505 acres

Dimensions (WxD): Approx 220' x 100'

Ownership: City of Ketchum/KURA

Market Value: N/A

# YMCA SOUTH LOT



Location: Warm Springs Road

Legal: Sec 12 & 13 4N 17E

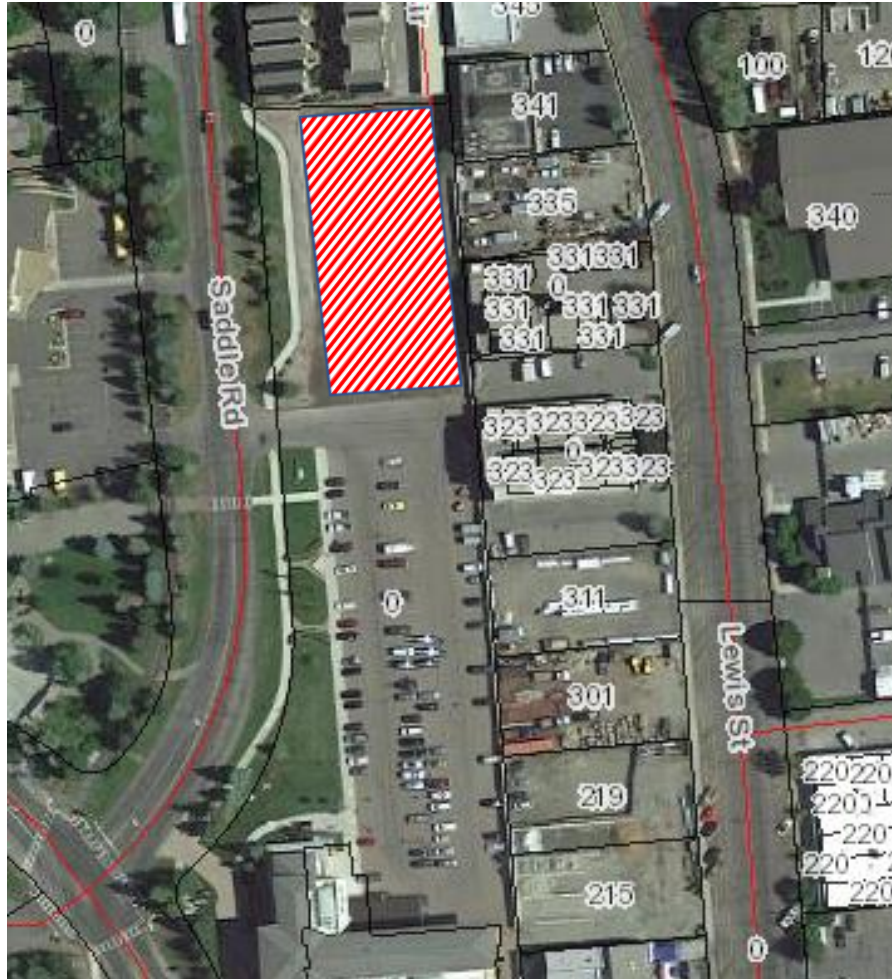
Size: 0.55 acres

Dimensions (WxD): Approx. 200' x 120'

Ownership: City of Ketchum

Market Value: N/A

# YMCA NORTH LOT



Location: Warm Springs Road

Legal: Sec 12 & 13 4N 17E

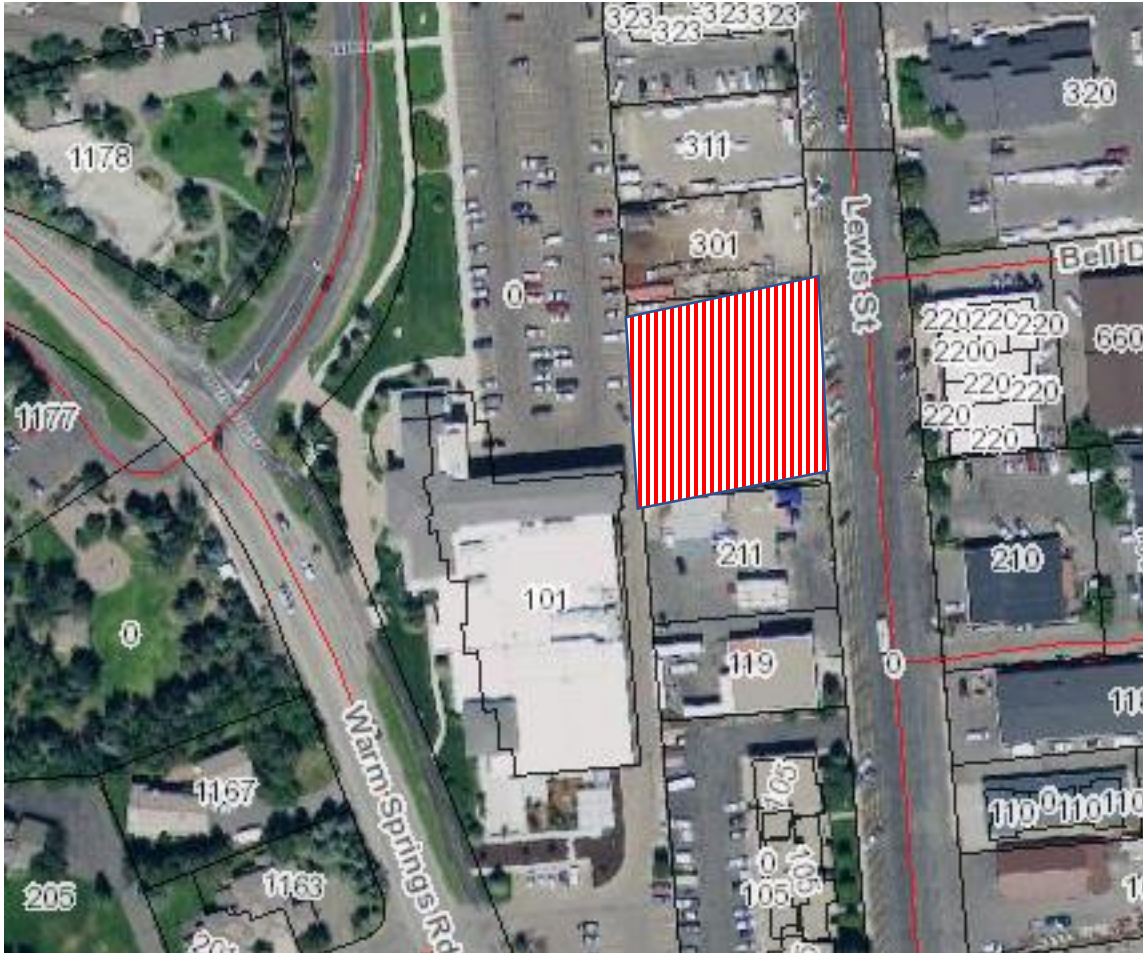
Size: 0.58 acres

Dimensions (WxD): Approx. 240' x 110'

Ownership: City of Ketchum

Market Value: N/A

# CITY-OWNED LEWIS STREET LOTS



Location: Lewis Street

Legal: Lot 7, Block 1 and Lot 6, Block 1

Size: 0.55 acres

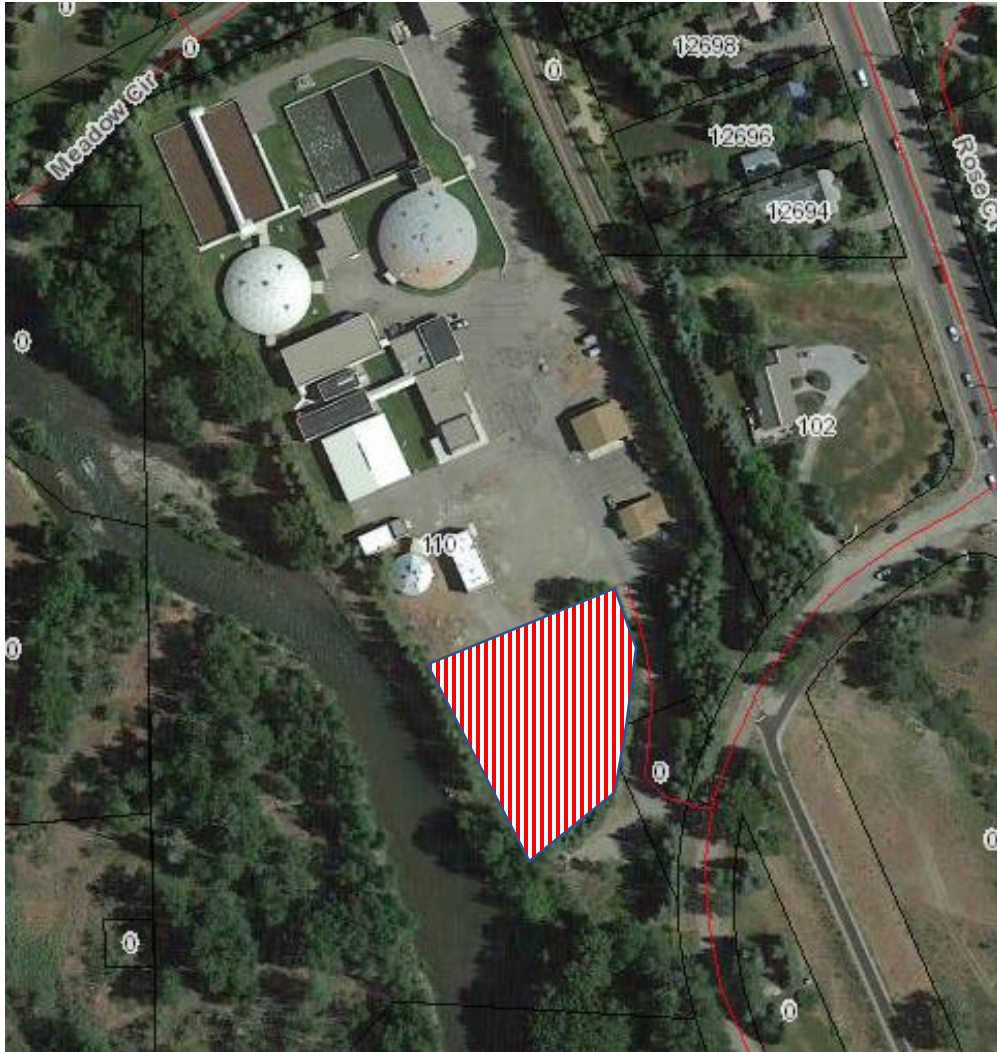
Dimensions (WxD): Approx 160' x 150'

Ownership: City of Ketchum

Market Value: N/A



# WATER TREATMENT PLANT LOT



Location: Water Treatment Plant

Legal: N/A

Size: Approx. 0.83 acres

Dimensions (WxD): Approx. 160' x 180'

Ownership: City of Ketchum

Market Value: N/A

# FOOT OF 9<sup>TH</sup> STREET LOT



Location: West End of 9<sup>th</sup> Street

Legal: N/A

Size: Approx. 0.15 acres

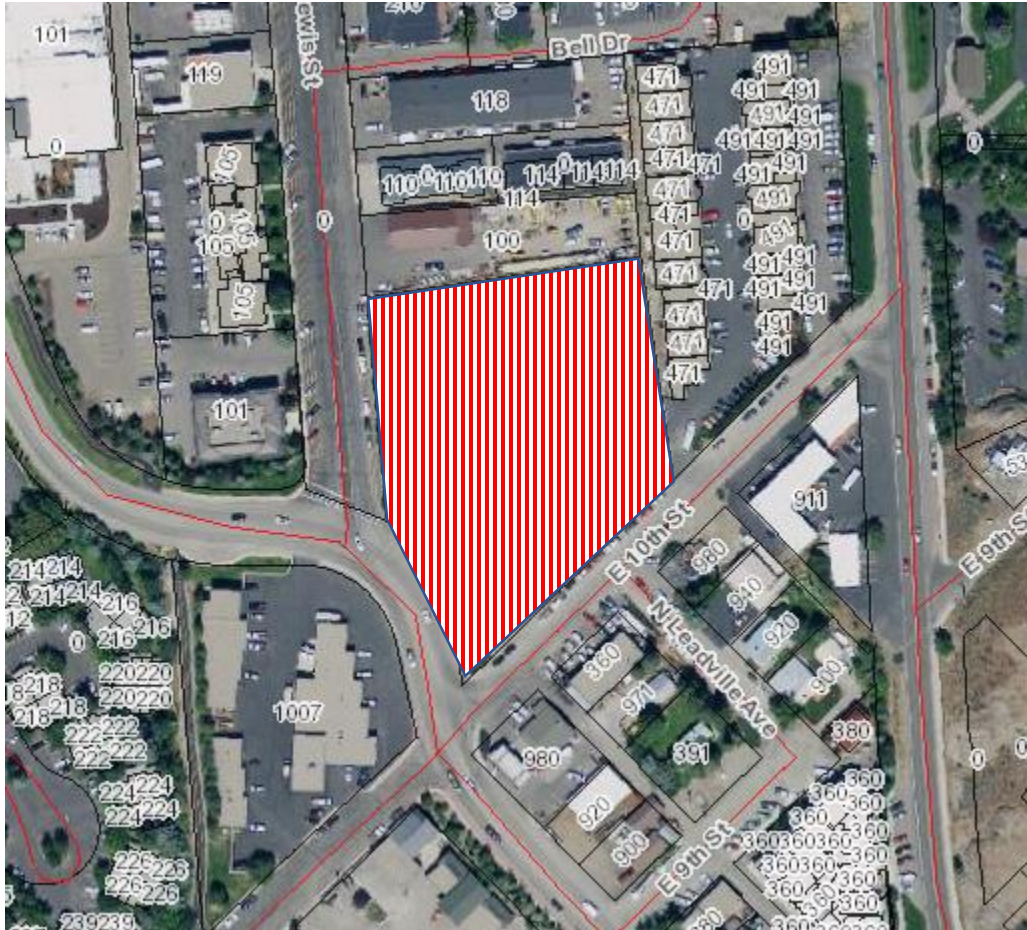
Dimensions (WxD): Approx. 80' x 65'

Ownership: City of Ketchum

Market Value: N/A

PRIVATELY-OWNED LOTS

# STOCK BUILDING SUPPLY LOT



Location: Lewis Street & Warm Springs Road.

Legal: Sec 13 4N 17E

Size: 2.432 acres

Dimensions (WxD): Approx. 340' x 305'

Ownership: Private (Warm Springs & 10th LLC).

Market Value: \$2,969,723

# 1<sup>ST</sup> AVENUE BETWEEN 4<sup>TH</sup> & 5<sup>TH</sup> STREET LOT



Location: 1<sup>st</sup> Avenue (btwn 4<sup>th</sup> & 5<sup>th</sup> Streets)

Legal: Lot 2 & 3A, Block 56

Size: 0.381 acres

Dimensions (WxD): Appox. 110' x 150'

Ownership: Private (431-471 N 1<sup>st</sup> Avenue LLC)

Market Value: \$1,625,330

# 2<sup>ND</sup> AVENUE & 2<sup>ND</sup> STREET LOT



Location: SW Corner of 2<sup>nd</sup> Avenue & 2<sup>nd</sup> Street

Legal: Lots 1 & 2, Block 62

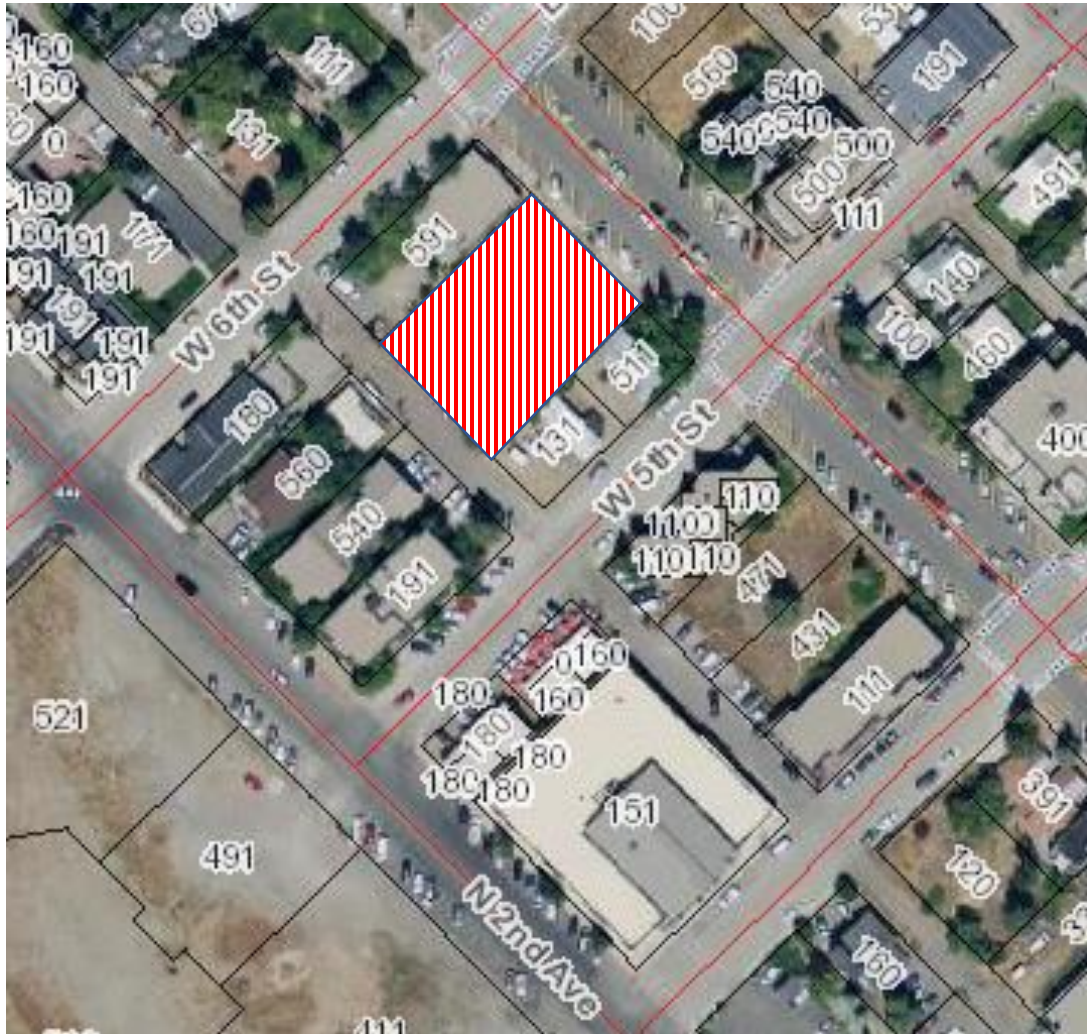
Size: 0.379 acres

Dimensions (WxD): Approx. 110' x 150'

Ownership: Private (Brien Stuart MD Trustee)

Market Value: \$1,056,000

# 1<sup>ST</sup> AVENUE LOT (HOSPICE & MT. EXPRESS)



Location: 1<sup>st</sup> Avenue (btwn 5<sup>th</sup> & 6<sup>th</sup> Streets)

Legal: Lots 2 & 3, Block 55

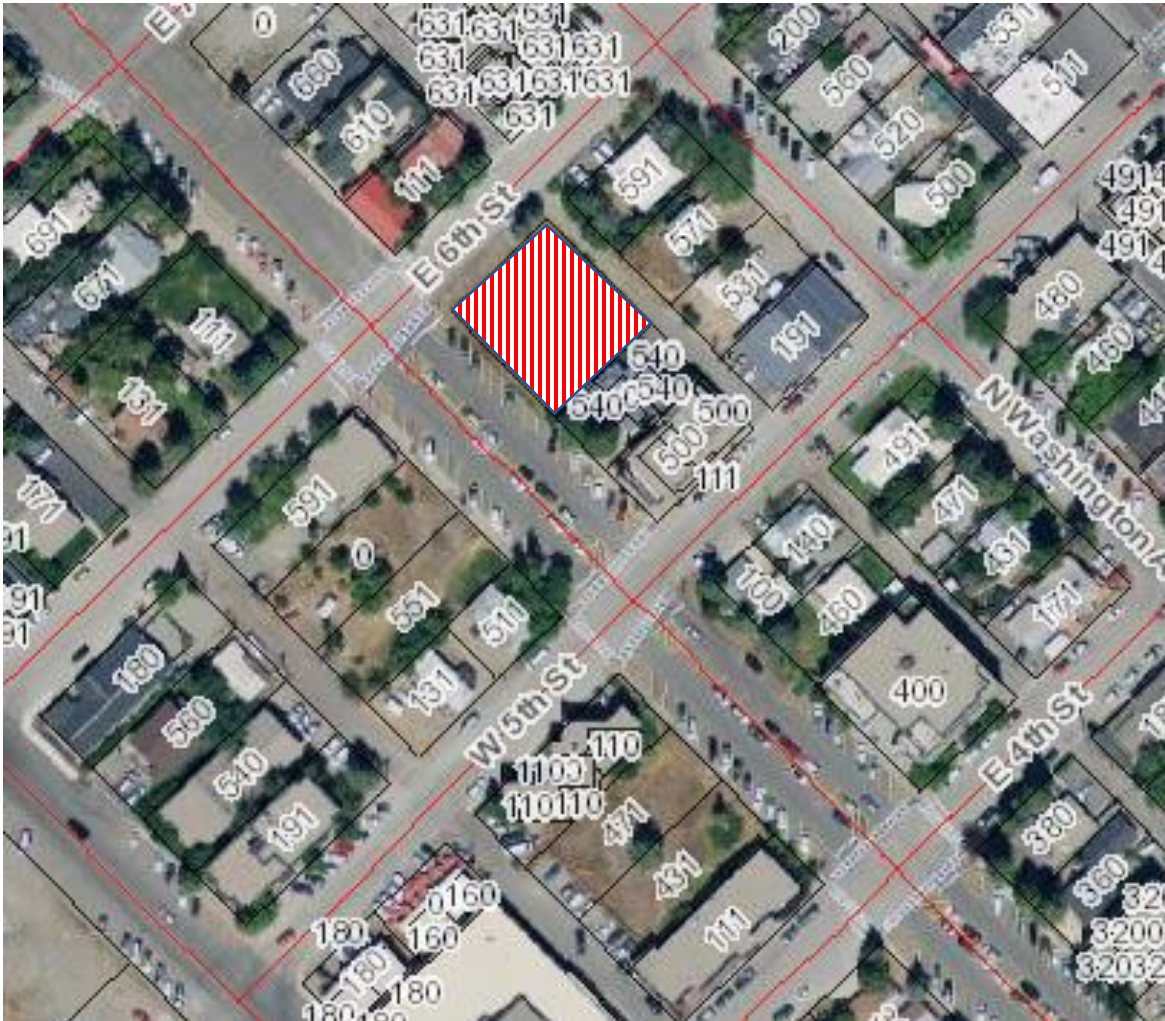
Size: 0.378 acres

Dimensions (WxD): Approx. 110' x 150'

Ownership: Private (Hardy Foundation Inc.)

Market Value: \$1,062,728

# 1<sup>st</sup> AVENUE AND 6<sup>TH</sup> STREET LOT



Location: SE Corner of 1<sup>st</sup> Avenue & 6<sup>th</sup> Street

Legal: Lot 5 & 6, Block 35

Size: 0.252 acres

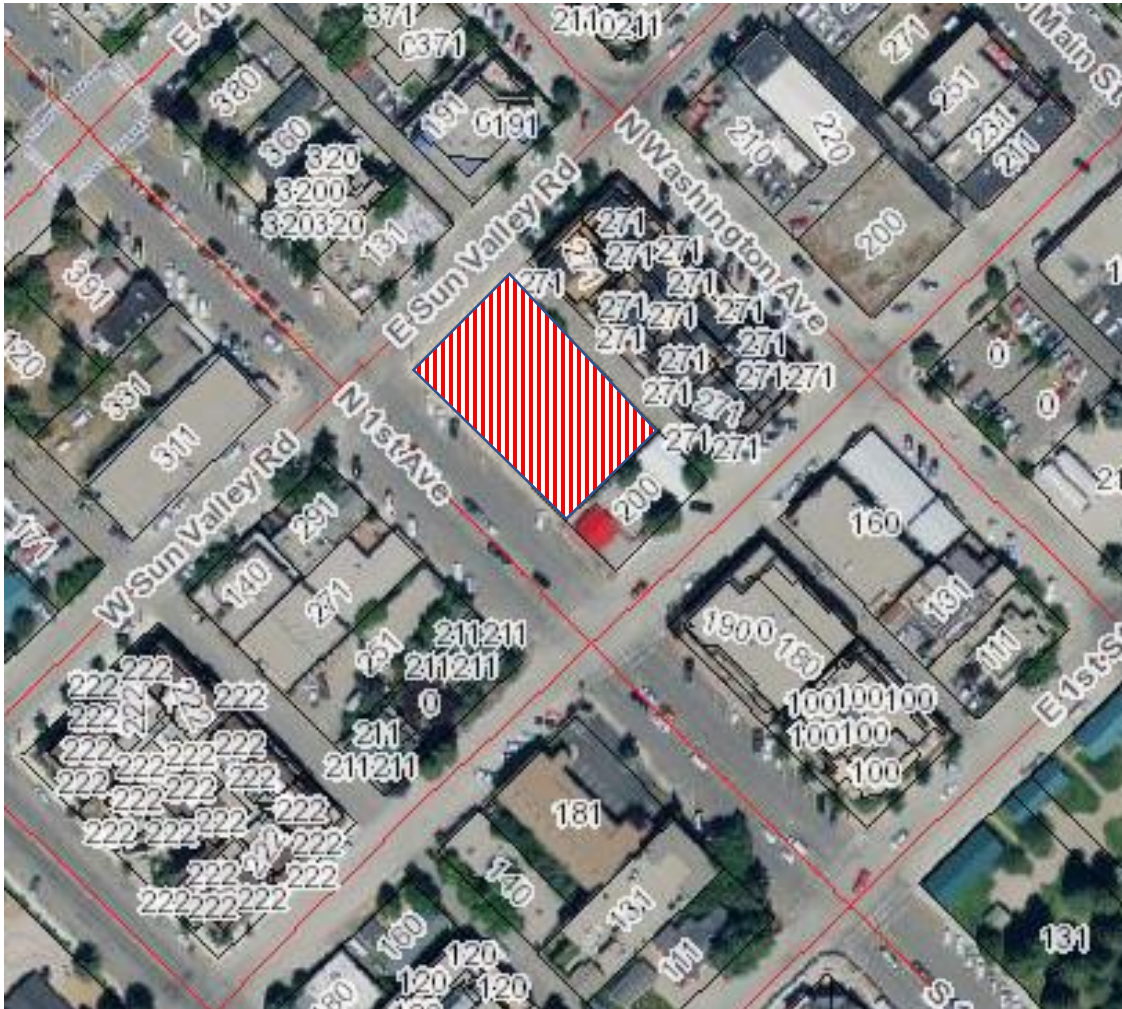
Dimensions (WxD): Approx. 110' x 100'

Ownership: Private (Jack Bariteau)

Market Value: \$880,000



# SUN VALLEY ROAD AND 1<sup>ST</sup> AVENUE LOT



Location: SE Corner of 1<sup>st</sup> Avenue & SV Road

Legal: Lot 5A, Block 38

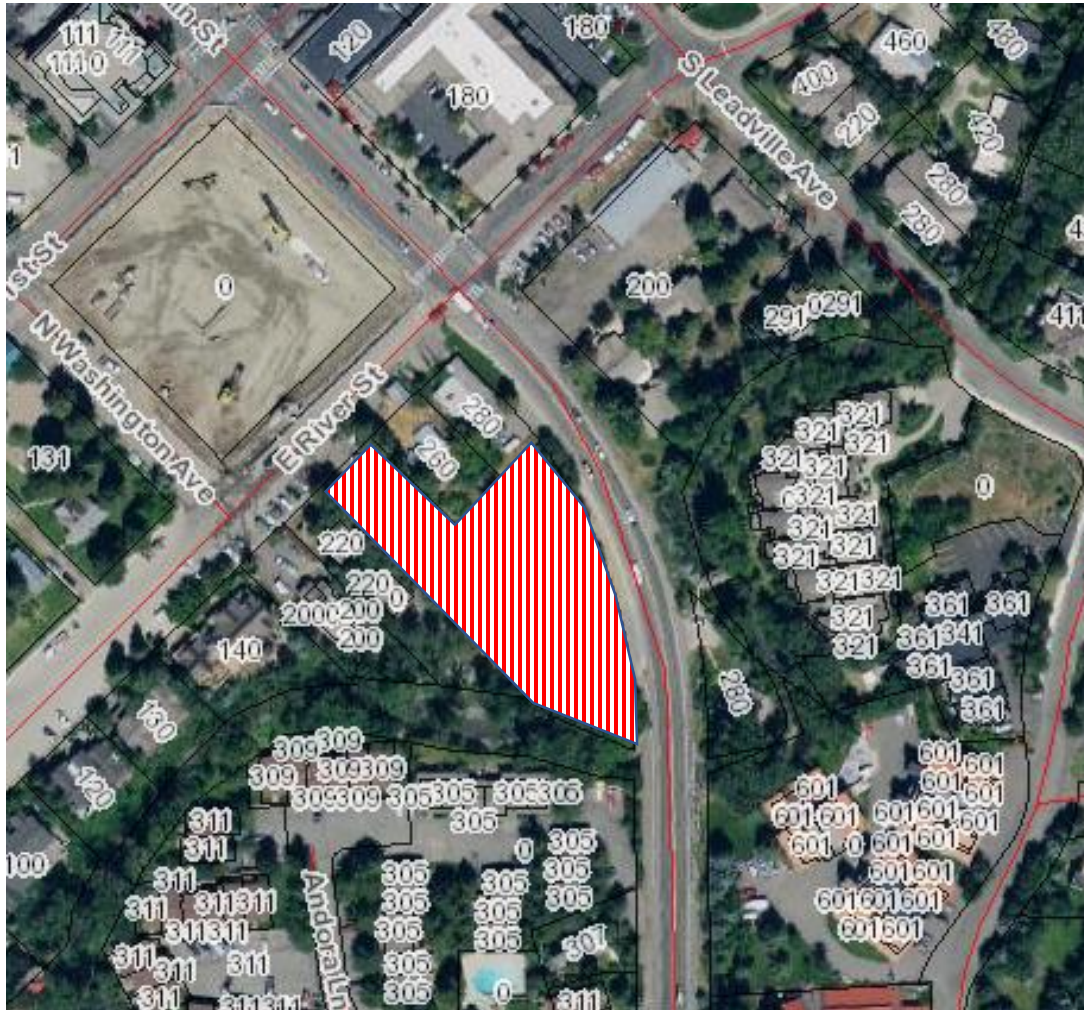
Size: 0.38 acres

Dimensions (WxD): Approx. 165' x 100'

Ownership: Private (260 1<sup>st</sup> LLC)

Market Value: \$1,617,000

# GATEWAY BUILDING LOT



Location: Main Street, South of River

Legal: Lots 3, 21, 22, Block 82

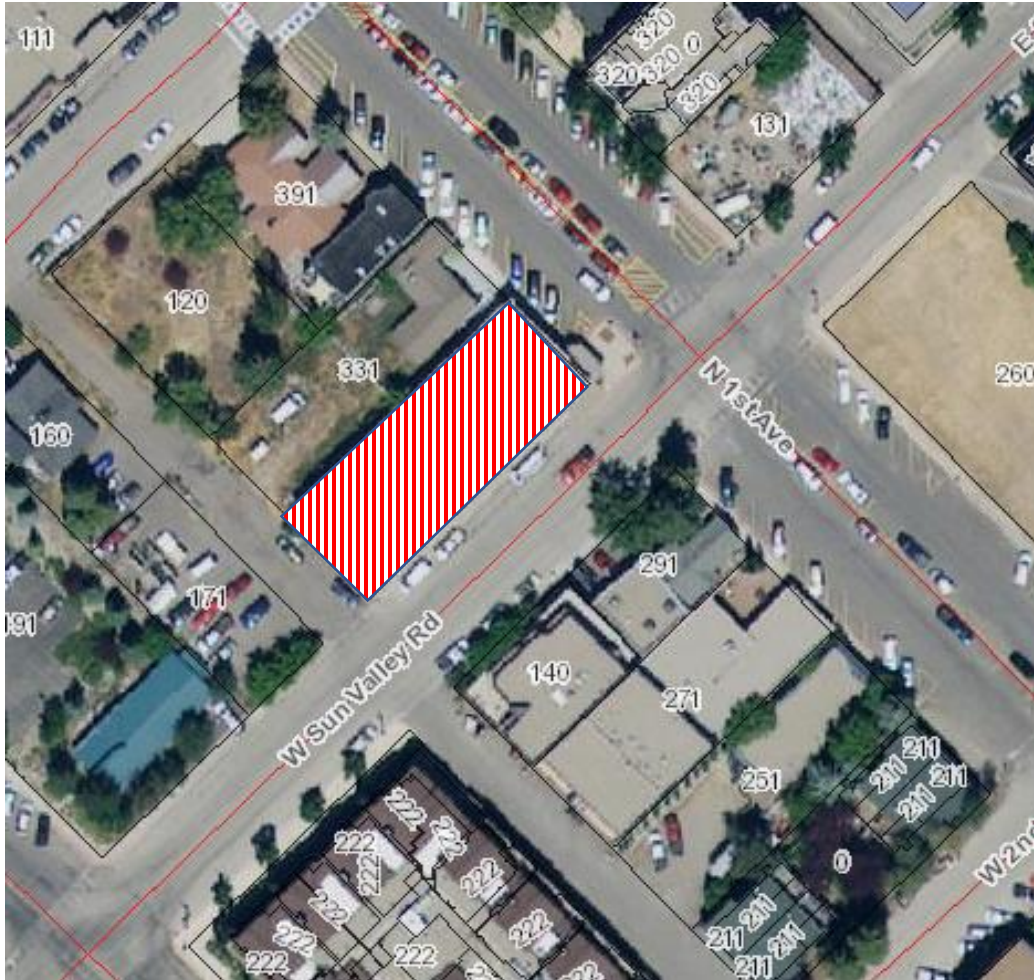
Size: 0.774 acres

Dimensions (WxD):

Ownership: Private (Idaho Banking Company, C/O DL Evans)

Market Value: \$2,421,785

# OLD POST OFFICE LOT



Location: NW Corner of 1<sup>st</sup> Avenue & SV Road

Legal: Lot 4, Block 57

Size: 0.189 acres

Dimensions (WxD): Approx. 50' x 150'

Ownership: Private (Geneva Plaza LLC)

Market Value: \$1,144,788

# HOT DOG HILL LOT



Location: NE Corner of Main Street & 4<sup>th</sup> Street

Legal: Lots 1 & 2, Block 5

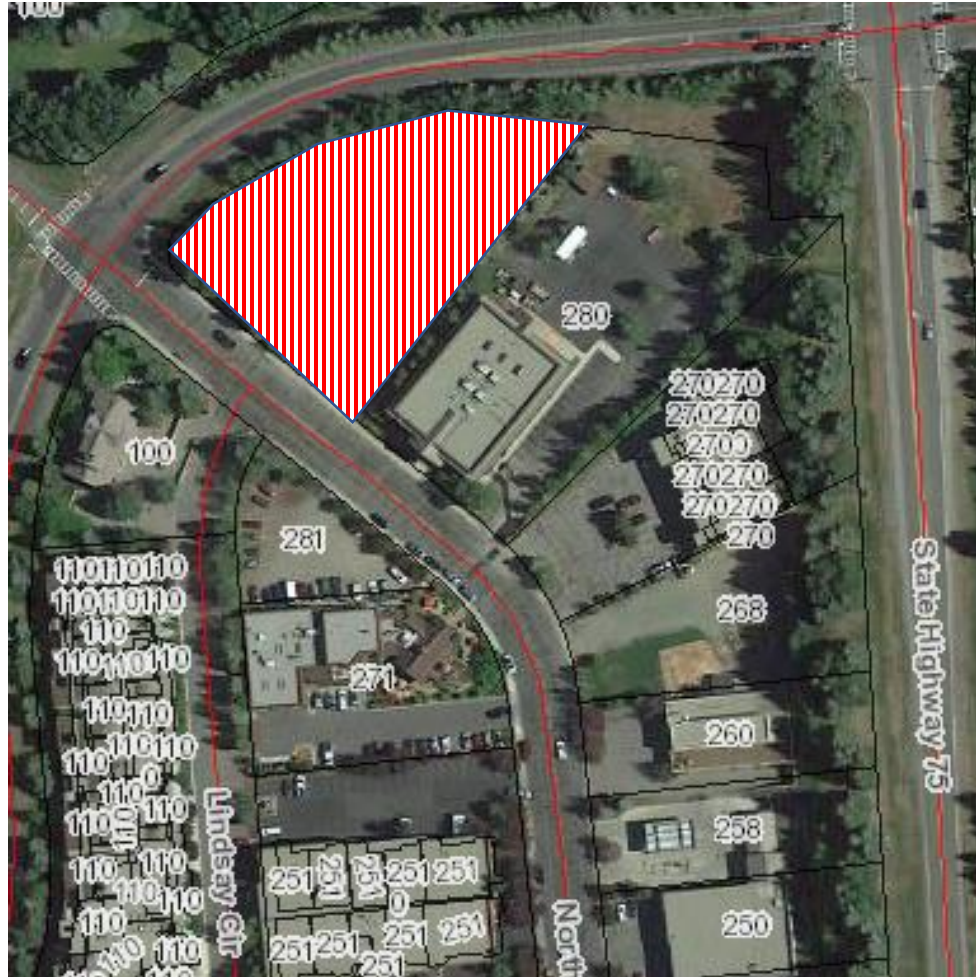
Size: 0.252 acres

Dimensions (WxD): Approx. 110' x 100'

Ownership: Private (440 North Main LLC).

Market Value: \$1,210,000

# BARSOTTI LIGHT INDUSTRIAL LOT



Location: SE Corner of Saddle & Northwood Roads

Legal: Lots 23 & 24

Size: 1.096 acres

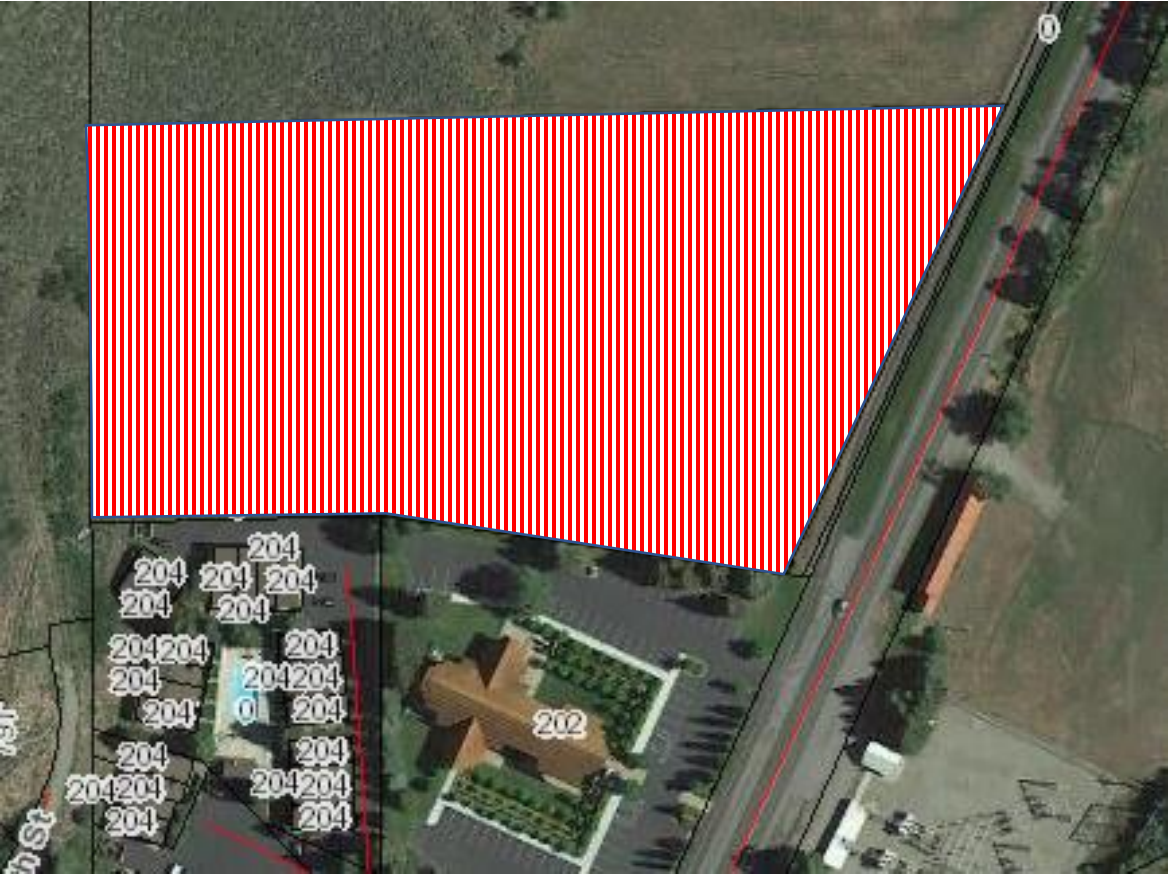
Dimensions (WxD): Approx 210' x 200'

Ownership: Private (Barsotti Investment Company LLC)

Market Value: \$1,388,246

ORGANIZATION-OWNED LOTS

# FESITVAL FIELD LOT



Location: Sun Valley Road (East of Town)

Legal: Sec 18 4N 18E

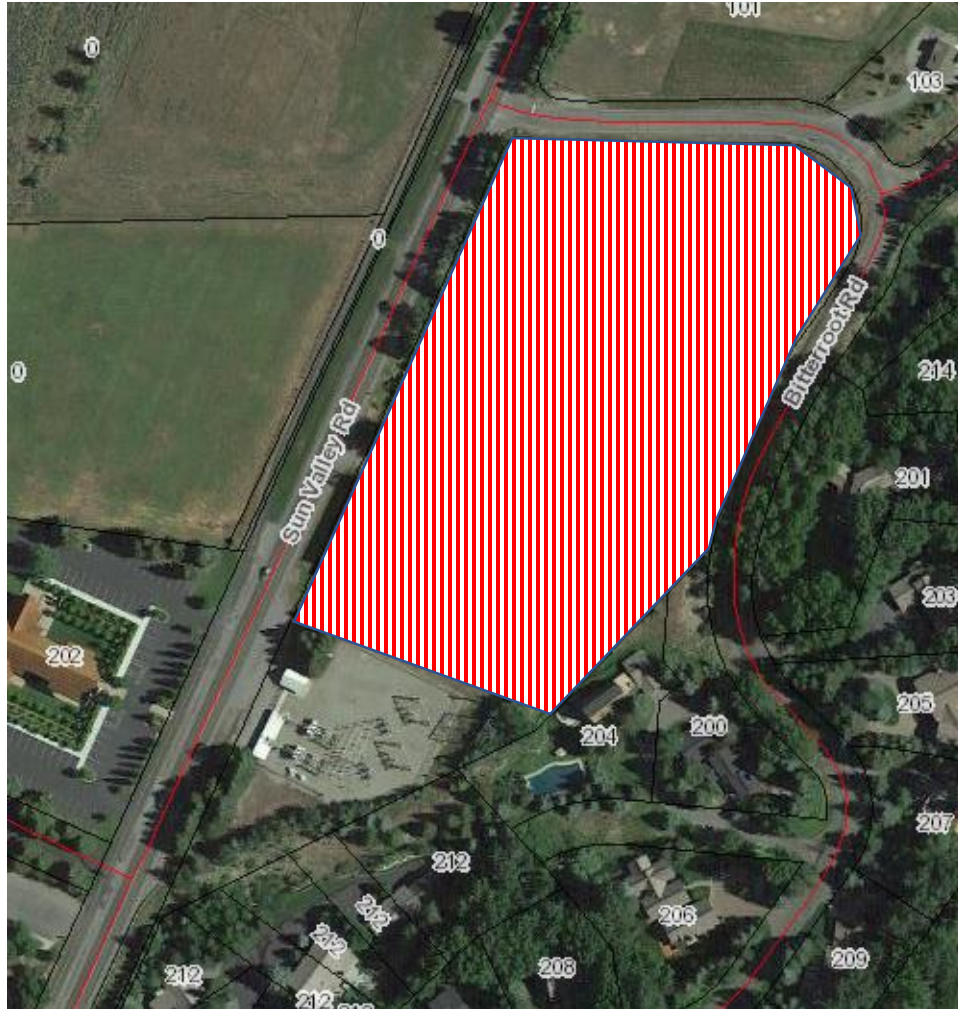
Size: 5.0 acres

Dimensions (WxD): Approx. 300' x 630'

Ownership: Private (City of Sun Valley)

Market Value:

# RED BARN LOT



Location: Sun Valley Road (east of town)

Legal: Bitter Root Sub, Lot 3, Block 2

Size: Approx. 7.0 acres

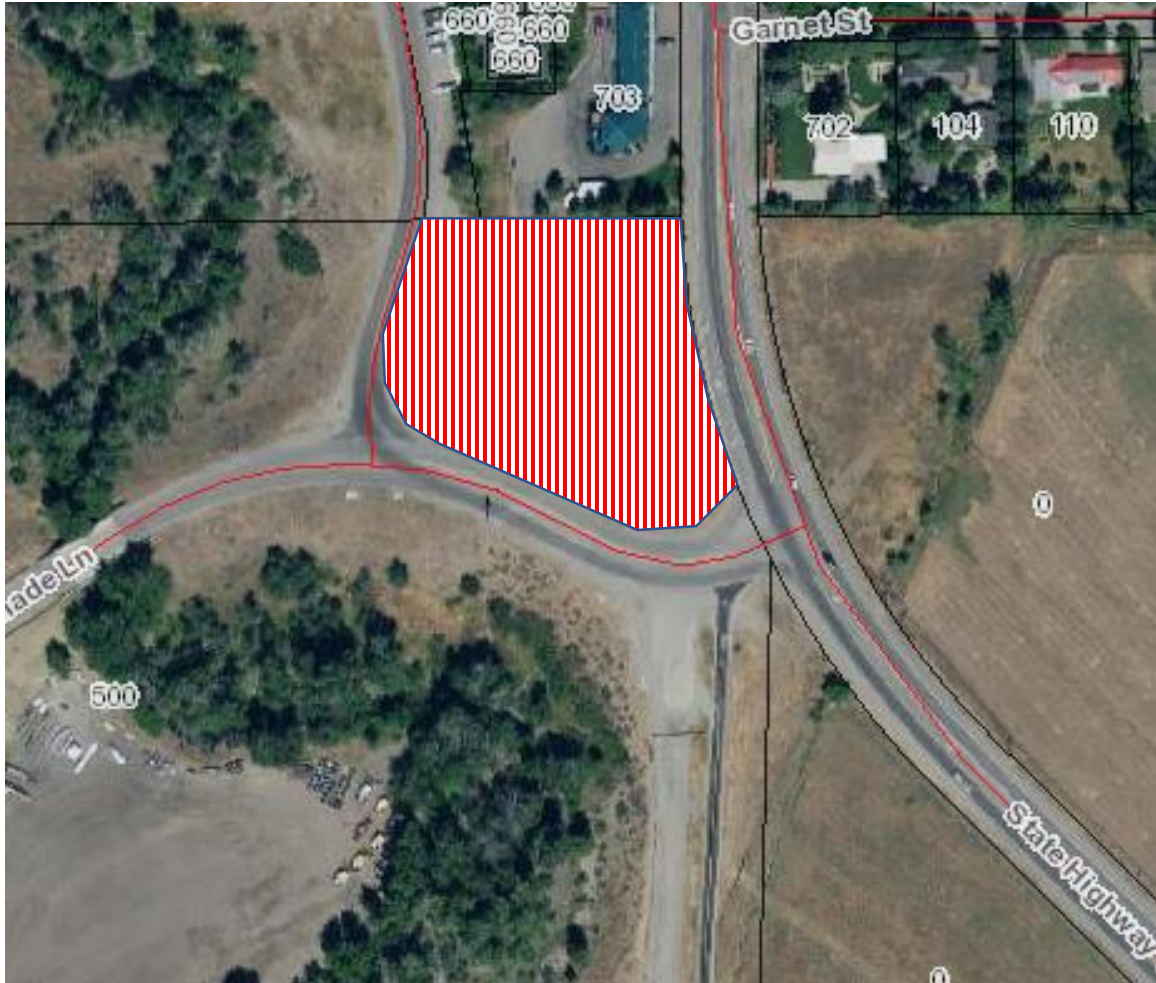
Dimensions (WxD): Approx. 450' x 375'

Ownership: Private (Sun Valley Company)

Market Value: Unknown



# 2<sup>ND</sup> AVENUE & SERENADE LANE LOT



Location: 2<sup>nd</sup> Avenue @ Serenade Lane

Legal: N/A

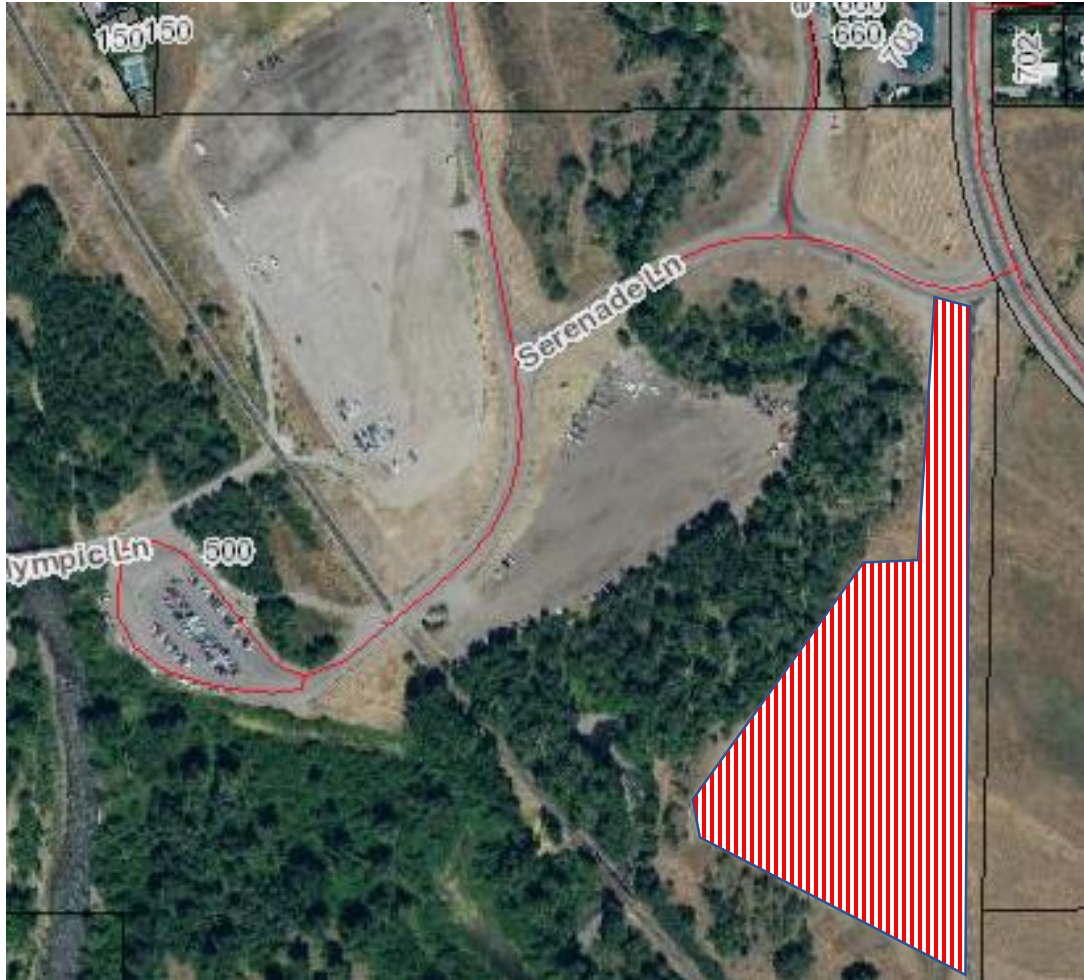
Size: Approx. 1.3 acres

Dimensions (WxD): Approx. 210' x 230'

Ownership: Private (Sun Valley Company)

Market Value: Unknown

# SNOW DUMP LOT



Location: Serenade Lane

Legal: N/A

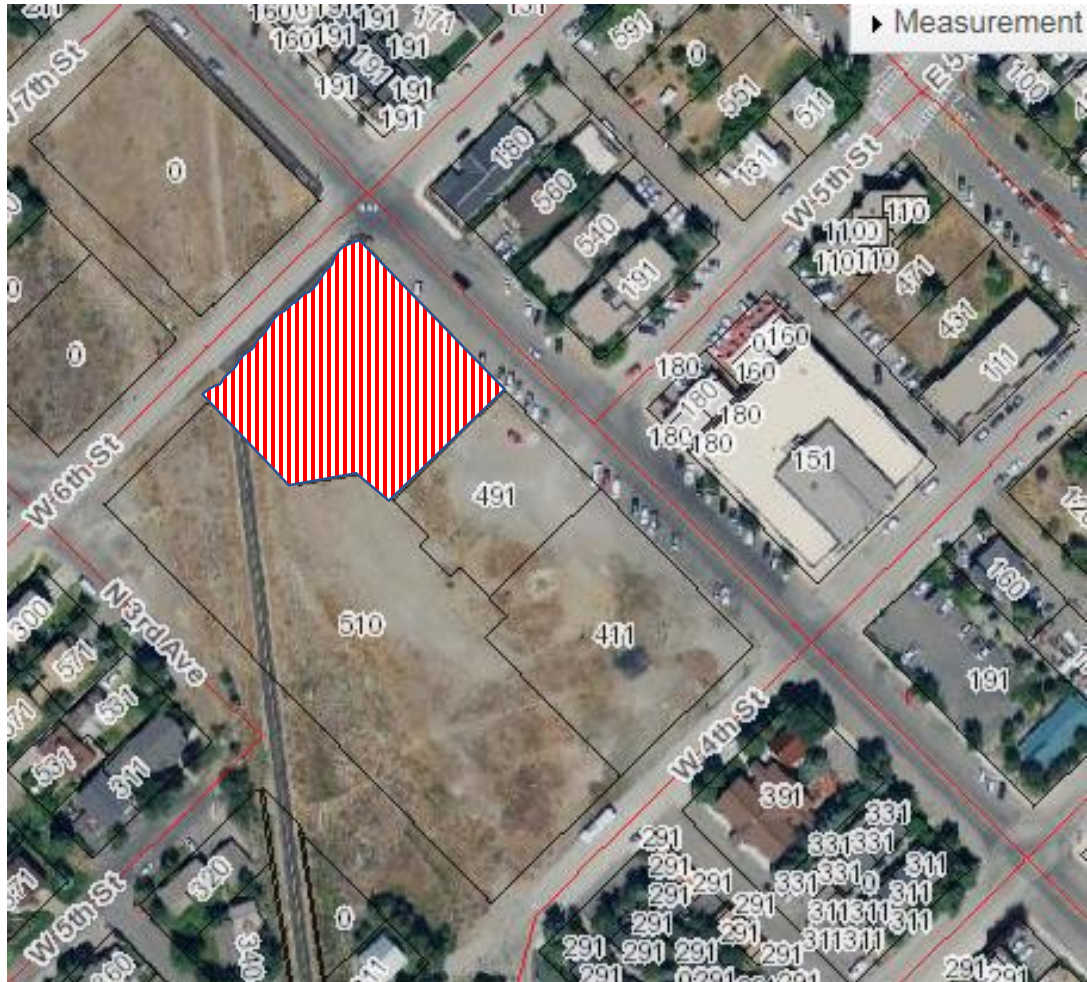
Size: 3.75 acres

Dimensions (WxD): 325' x 500'

Ownership: Private (Sun Valley Company)

Market Value: Unknown

# SIMPLOT LOT



Location: NW Corner of 6<sup>th</sup> Street & 2<sup>nd</sup> Avenue.

Legal: Lot 3A, Block 1

Size: 0.72 Acres

Dimensions (WxD): Approx. 175' x 190'

Ownership: Private (Simplot Ketchum Properties LLC).

Market Value: \$1,717,960

## Exhibit C:

Ketchum Fire Station: Staff  
Report: Planning & Zoning  
Commission Meeting of  
February 10, 2020



City of Ketchum  
Planning & Building

IN RE:	)	
	)	
Ketchum Fire station	)	<b>KETCHUM PLANNING AND ZONING COMMISSION</b>
Design Review	)	<b>FINDINGS OF FACT, CONCLUSIONS OF LAW, AND</b>
Date: February 24, 2020	)	<b>DECISION</b>
	)	
File Number: P20-013	)	

FINDINGS REGARDING APPLICATION FILED

PROJECT: Ketchum Fire Station

FILE NUMBER: P20-013

APPLICATION: Design Review

PRE-APPLICATION: The Commission reviewed the Pre-Application (File Number P20-005) and moved to advance the project to final Design Review on January 13<sup>th</sup>, 2020.

REPRESENTATIVE: Cole Architects

OWNER: City of Ketchum

LOCATION: 107 Saddle Road (Portion of Tax Lot 6689)

ZONING: Tourist (T)

OVERLAY: None

NOTICE: A public hearing notice for the project was mailed to all owners of property adjacent to the project site on January 22<sup>nd</sup>, 2020. The public hearing notice was published in the Idaho Mountain Express on January 22<sup>nd</sup>, 2020. The Planning & Zoning Commission moved to continue review of the application during their meeting on February 10<sup>th</sup>, 2020.

FINDINGS REGARDING PROJECT LOCATION

The new fire station is located on a portion of a larger parcel owned by the City within a Planned Unit Development (PUD) in the Tourist Zone. The PUD includes the Northwood Place apartments, the YMCA, and shared public parking areas all on the same parcel. The fire station site area is 15% of the PUD area with over 150 feet of street frontage along Saddle Road.

The neighborhood connects residential, commercial, recreational, and industrial development. A recreation hub, the multi-use path connects the YMCA to Guy Coles Skate Park and Rotary Park. Across Saddle Road, a townhome development extends north from the Church of the Big Wood. Industrial buildings, including the Fire Training Facility on Lewis Street, border the east side of the PUD.

The project location and site area meet the Fire Department’s operational needs. The Future Land Use Map designates the parcel as Public/Quasi-Public for community services, utilities, and government buildings like the proposed fire station. The City-owned parcel is one of only four properties within the City designated for this type of essential facility. The other Public/Quasi-Public properties are developed with the Community Library, Post Office, and the Wastewater Treatment Plant.

#### FINDINGS REGARDING PROJECT COMPLIANCE WITH DESIGN REVIEW STANDARDS

The fire station design balances multiple discordant functions, including living quarters, apparatus and equipment storage, maintenance, decontamination facilities, and administrative offices and training rooms. Occupied by personnel in continuous 24-hour shifts, the fire station design must balance these different activities while maintaining a healthy and safe environment. The building layout and site design organizes similar functions within distinct areas—the apparatus bays at the center of the station separate the operations and maintenance area from the administrative offices, public meeting room, and residential areas. The orientation and placement of rectangular building shapes facilitate this functional arrangement. The modern design refashions traditional materials like brick to augment visual interest. Details like the red trim across the apparatus-bay windows allude to more traditional firehouse design.

#### FINDINGS REGARDING PARKING & TRAFFIC CIRCULATION

By Highway 75, Saddle Road, and Warm Springs, the fire station is centrally located with easy access to main routes that connect to all areas of the City. The circulation plan includes drive-through apparatus bays and sight windows that enhance safety while minimizing the fire station’s impact to neighborhood traffic. The City has engaged a professional traffic engineer to identify safety concerns or circulation conflicts. Any improvements necessary to ensure the safety of pedestrians, bicyclists, drivers, and emergency responders will be included in the project plan.

The shared parking areas within the PUD contain 220 public parking spaces. The parking spaces are unrestricted and shared between multiple users. Instead of restricting spaces to an exclusive use, sharing spaces satisfies parking demand by flexibly accommodating various uses during different times of the day. More than enough parking spaces are provided in the PUD to meet the needs of all users including the fire station parking demand.

#### TABLE 1: FINDINGS REGARDING COMPLIANCE WITH COMPREHENSIVE PLAN

A critical facility that will support the future growth of Ketchum, the new fire station is an essential facility needed to sustain our vibrant mountain town. Providing fire protection and emergency medical services, the Fire Department preserves and enhances quality of life for Ketchum residents

Ketchum Fire Station Design Review

Planning & Zoning Commission Meetings of February 10 & 24, 2020

Findings of Fact, Conclusions of Law, and Decision

**City of Ketchum Planning & Building Department**

and visitors. The 2014 Comprehensive Plan identified the construction of a new, modern fire station as the department's highest priority. By protecting life and property, the Fire Department provides the foundation to support our community vision and shared values. The fire station is consistent with the uses, goals, and policies listed below as specified within the 2014 Comprehensive Plan.

SUPPORTING SECTION	COMPLIANCE WITH THE 2014 COMPREHENSIVE PLAN & <i>COMMISSION FINDINGS</i>
<p>Land Use Category: Public/Quasi Public</p>	<p><b>PRIMARY USES</b> The Public/Quasi-Public category includes uses related to community services, such as schools, libraries, community centers and gardens, arts/culture, hospitals, government, utilities, cemeteries, and places of worship.</p> <p><i>The fire station is a public facility that delivers an essential service to the community. <u>The project site is one of only four properties within the City designated as an appropriate location for this type of use.</u> The remaining Public/Quasi-Public properties are developed with the Community Library, Post Office, and the Wastewater Treatment Plant.</i></p>
<b>Future Land Use</b>	
<p>Infill and Redevelopment to Accommodate Growth</p>	<p><i>Ketchum needs a new fire station to sustain our vibrant community and support future growth. As first responders to fires, public safety and medical emergencies, and technical rescues, the Fire Department protects the lives and property of The fire station is a critical facility needed to deliver essential services to protect the community. A vacant and underutilized property within a built-out area, the project site is strategically located for emergency response near major transportation corridors. As Ketchum continues to grow, the Fire Department must respond to increasing demands for services. The fire station is necessary to support the future growth of Ketchum.</i></p>
<b>A Strong and Diverse Economy</b>	
<p>Policy E-2(b) Quality of Life Infrastructure</p>	<p><i>The Fire Department keeps Ketchum dynamic, resilient, and economically viable. By enhancing delivery of fire protection and emergency services, the fire station project is critical to Ketchum's economic success.</i></p>
<b>Community Design and Neighborhoods</b>	
<p>Policy CD-1.3 Compatible Infill and Redevelopment Projects</p>	<p><i>The fire station is contextually appropriate located in a neighborhood at the intersection of commercial, industrial, and residential development.</i></p>

Policy CD-1.5 High-Quality and Sustainable Design Principles for Public Buildings and Public Outdoor Spaces	<i>Centrally located, the fire station is sited near main routes to serve Ketchum residents and visitors. The project plans include a community room to host public meetings and events.</i>  <i>The fire station will be a LEED certified building.</i>
Policy CD-2.5 Energy and Water Efficiency in New Development	<i>The fire station will be a LEED certified building to ensure high performance standards for energy and water conservation.</i>
<b>Natural Resource Stewardship</b>	
Policy NR6.1 City Leadership in Energy Efficiency and Conservation	The City of Ketchum will serve as an example by continuing to improve energy efficiency and GHG reductions in City facilities and operations.
Policy NR6.2 Energy Alternatives	The City should implement policies and programs that enhance opportunities for individuals, businesses, and public organizations to conserve energy and convert to renewable resources. The City should support energy conservation in City buildings, vehicles, operations and processes through its own policies, and provide information about techniques for energy efficiency.
<b>Public Safety and Utilities</b>	
Goal PSU-1	Ketchum will proactively ensure the safety and welfare of residents by providing high levels of police, fire, and emergency response services. In addition, the City will promote community awareness and involvement with crime prevention and code enforcement. A primary objective is to provide and maintain reliable funding to protect investment in existing fire and police facilities and ensure appropriate levels of service.
Policy PSU-1.3 Fire and Police Service Standards	Future fire and police facility requirements should be determined using level of service (LOS) standards. Level of service should be defined as the optimum level of service desired from the service provider and may differ from the current LOS.
Policy PSU-3.2 Channeling Capital Improvements	The fire station project provides a safety-related capital improvement.

**TABLE 2: FINDINGS REGARDING CITY DEPARTMENT COMMENTS**

City Department comments are based on the project concept as proposed with the Design Review project plans. All City Departments shall review and approve the project through the Building Permit application process. All comments pertaining to the Design Review drawings are subject to change.



All right-of-way improvements must be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.

**City Department Comments**

*All City Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project.*

**Fire Department:**

- It is the General Contractor’s responsibility to understand and adhere to all Fire Protection Ordinance #1125 requirements in addition to any and all other City of Ketchum requirements in effect at the time of Building Permit issuance. Failure to comply with all local ordinances and codes may result in project work stoppage as well as criminal penalties.
- The above project shall meet all 2012 International Fire Code requirements in addition to specific City Building and Fire Ordinances.
- Approved address numbers shall be placed in such a position to be plainly visible and legible from the road fronting the property. Numbers and letters shall be a minimum of four (4) inches tall, contrast with their background and be positioned a minimum of forty-eight (48) inches above final grade.
- Vehicle parking and material storage during construction shall not restrict or obstruct public streets or access to any building. A minimum twenty-foot travel lane for emergency vehicle access shall be maintained clear and unobstructed at all times. All required Fire Lanes, including within 15 feet of fire hydrants, shall be maintained clear and unobstructed at all times.
- An approved automatic fire sprinkler system shall be installed throughout the building per City of Ketchum Ordinance #1125 ([www.ketchumfire.org](http://www.ketchumfire.org)) and the National Fire Protection Association Standard 13. An approved fire sprinkler flow bell, Knox box and Fire Department Connection shall be installed in an approved location visible to approaching firefighters. Water service lines to structures shall be hydraulically calculated for size to meet fire sprinkler flow requirements. Fire sprinkler systems shall be annually tested and maintained per NFPA 25. An approved fire department connection and flow bell shall be installed in a location approved by the fire department and the system shall be supervised by an approved alarm system.  
Note: The entire building shall be protected by an NFPA 13 Fire Sprinkler system.
- NOTE: One electronic set of fire sprinkler system plans must be submitted to the Ketchum Fire Department as well as the State Fire Marshal’s office and a Ketchum Fire Department Permit must be obtained prior to installation of fire sprinkler systems. Inspections of fire sprinkler systems by the Fire Chief or an appointee are required. Inspections must be scheduled at least 48 hours in advance.
- An approved monitored fire sprinkler alarm system shall be installed per City of Ketchum Ordinance #1125 ([www.ketchumfire.org](http://www.ketchumfire.org)) and the requirements of NFPA 72. Two (2) sets of alarm system plans shall be submitted to the Ketchum Fire Department for approval and a permit is required prior to installation of alarm systems. Inspections of fire detection systems by the Fire Chief or an appointee are required and shall be scheduled at least 48 hours in advance.

- An approved access roadway per 2012 International Fire Code Appendix D ([www.ketchumfire.org](http://www.ketchumfire.org)) shall be installed prior to any combustible construction on the site. The road shall be a minimum of twenty (20) feet in width and capable of supporting an imposed load of at least 75,000 pounds.
- Fire extinguishers shall be installed and maintained per 2012 IFC Section 906 both during construction and upon occupancy of the building.
- An approved key box shall be installed, with the appropriate keys, for emergency fire department access in a location approved by the fire department. The key box shall be a Knox box brand and sized to accommodate keys to every door of the project.
- An 8 ½ by 11 color coded site map of this project shall be provided on paper and electronically to the fire department. This site map shall show the locations of gas shut-offs, power shut-offs, fire sprinkler riser rooms, fire department connections, alarm panels, Knox boxes, access doors, egress windows, stairways and any additional fire department requirements. Exact details for color coded “On-Sites” can be found at [www.ketchumfire.org](http://www.ketchumfire.org).
- Final inspections of all fire department permit required installations by the Fire Chief or an appointee are required and shall be scheduled at least 48 hours in advance. A Final Inspection Checklist can be found at [www.ketchumfire.org](http://www.ketchumfire.org).
- Fire Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project.

**City Engineer & Streets Department:**

- All drainage shall be retained on site including water from any roof drains (KMC §17.96.060.C.1). Any roof drain locations will need to be shown on project plans submitted with the Building Permit application.
- All construction for the project must comply with the standards set forth in Ketchum Municipal Code, Chapter 15.06 Construction Activity Standards. The applicant shall submit a Construction Activity Plan addressing all applicable activities (KMC §15.06.030), including how materials will be off-loaded at the site, plan for coordinating with neighbors on temporary closures, temporary traffic control, and construction fencing with appropriate screening, to be reviewed and approved prior to issuance of a Building Permit for the project. Pursuant to KMC §15.06.030.A.2, the applicant shall provide notice of the project, construction schedule, and general contractor’s contact information to all neighbors with properties adjacent to the project site.
- The Building Permit plans and construction drawings shall meet all applicable sections of Chapter 12 of Ketchum Municipal Code.
- The plans for the ROW improvements must be prepared by a professional engineer licensed in Idaho (KMC §12.04.020). Reconstruction of the city sidewalk is required. Specifications for the ROW improvements shall be indicated on the civil drawings submitted with the Building Permit application to be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.
- Recommended separation distance between a dry well and water main is 25’. If a dry well is installed closer than 25’ to the water main install 20 mil HDPE groundwater barrier along the water main side of drywell. Barrier should extend of 18 inches below the water main pipe. Minimum distance between a dry well and water main must be 10’.

<ul style="list-style-type: none"> <li>Utility locations (electrical, gas) are not permitted in public ROW. If utility upgrades are necessary the applicant will need to coordinate upgrades with Idaho Power and Intermountain Gas. All utilities need to be shown on the plans including the location and size of water and sewer mains and services, gas, electric, TV and phone.</li> <li>Drainage improvements constructed shall be equal to the length of the subject property lines adjacent to any public street or private street (KMC §17.96.060C).</li> <li>The applicant shall submit a Street and Alley Digging, Excavation, and Trenching (“DIG”) Permit application with an associated traffic control plan for all construction work within the City right-of-way to be reviewed and approved by the Streets Department. The use of City right-of-way for construction including the closure of adjacent streets or sidewalks requires a Temporary Use of Right-of-Way Permit (“TURP”).</li> <li>Final civil drawings for all associated ROW improvements shall be submitted with the Building Permit application to be verified, reviewed, and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.</li> </ul>
<p><b>Utilities:</b></p> <ul style="list-style-type: none"> <li>The applicant will be responsible for installing connections to the water and sewer system.</li> <li>Requirements and specifications for the water and sewer connections will be verified, reviewed, and approved by the Utilities Department prior to issuance of a Building Permit for the project.</li> </ul>
<p><b>Building:</b></p> <ul style="list-style-type: none"> <li>The building must meet the 2012 International Building Code and Title 15 Buildings and Construction of Ketchum Municipal Code.</li> <li>Building Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project.</li> </ul>
<p><b>Planning and Zoning:</b>  Comments are denoted within the analysis of the project’s compliance with the Comprehensive Plan, zoning and dimensional standards, and Design Review evaluation standards.</p>

TABLE 3: FINDINGS REGARDING ZONING AND DIMENSIONAL STANDARDS

Zoning and Dimensional Standards Analysis				
Compliant			Standards and Commission Findings	
Yes	No	N/A	Guideline	City Standards and <i>Commission Findings</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	Minimum Lot Area
			<i>Commission Findings</i>	Required: 8,000 square feet minimum  Existing: Tax Lot 6689 Total: 249,150 square feet Fire Station Project Site: 35,139 square feet
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	Minimum Open Space
			<i>Commission Findings</i>	Required: 35%/30% (Footnote 5: 5% open site area may be used for private decks or patios and walkways subject to Design Review approval)

				<p><b>Proposed with Ketchum Fire Station:</b>  <i>YMCA and Greenhouse Building Footprint: 69,282 square feet</i>  <i>Northwood Place &amp; Carport Building Footprint: 23,402 square feet</i>  <i>Ketchum Fire Station &amp; Carport Building Footprint: 14,810 square feet</i>  <i>Total Building Footprint: 107,494 square feet</i>  <i>Total Open Space: 141,656 square feet</i>  <i>Open Space: 57% (141,656square feet open space/249,150 square feet Tax Lot 6689 area)</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	<p><b>Floor Area Ratios and Community Housing</b></p> <p><i>Commission Findings</i></p> <p>Floor Area Ratio Permitted in Tourist (T) Zoning District  FAR Permitted in T Zone: 0.5  FAR Permitted with Inclusionary Housing Incentive: 1.6</p> <p><b>Proposed Floor Area Ratio</b>  Existing YMCA, Future YMCA Expansion, and Northwood Place Gross Floor Area: 128,806.5 square feet (YMCA with Future Expansion 91,374 gross square feet + Northwood Place 37,432.5 gross square feet)</p> <p>Proposed Ketchum Fire Station Gross Floor Area: 17, 507 gross square feet</p> <p>Total Gross Floor Area of YMCA with Future Expansion, Northwood Place, Ketchum Fire Station: 146,313.5 gross square feet  Floor Area Ratio Proposed: 0.59 (146,313.5 gross square feet/249,150 square feet TL 6689 area)</p> <p>Increase Above 0.5 Permitted FAR: 21,738.5 square feet (146,313.5 gross floor area-124,575 permitted 0.5 gross floor area)  20% of Increase: 4,348 square feet  15% Reduction for Net Livable Floor Area: 3,696 square feet</p> <p>Northwood Place is an affordable apartment complex with 32 deed restricted community housing units. Comprised of five buildings, the total floor area of Northwood Place is 33,188 square feet, which satisfies the community housing contribution.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	<p><b>Minimum Building Setbacks</b></p> <p><i>Commission Findings</i></p> <p>Minimum:  <i>Front: 15 feet</i>  <i>West Side Setback Along Saddle Road Waiver (PUD CUP 04-008): 0 feet</i>  <i>East Side Setback Along Interior Property Line Waiver (PUD CUP 09-023): 0 feet</i></p>

				<p><i>Rear: &gt; 1 per 3 feet in building height, but no less than 10 feet</i></p> <p><i>Setback Along Warm Springs Road with Waiver: 20 feet</i></p> <p><i>The PUD designates the Warm Springs Road boundary as the front property line.</i></p> <p><b>Proposed:</b>  <i>Front (South/Warm Springs): Greater than 15 feet</i>  <i>Rear (North/Park Circle): Greater than 15 feet</i>  <i>Side with Waiver (East/Interior): 0 feet</i>  <i>Side with Waiver (West/Saddle Road): 0 feet</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	<p><b>Building Height</b></p> <p><b>Commission Findings</b></p> <p><b>Maximum Permitted in Tourist Zone: 35 feet</b>  <b>Maximum Permitted with PUD Waiver: 45 feet-6 inches</b></p> <p><b>Proposed:</b>  <i>The fire station is 29 feet tall.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.125.030H	<p><b>Curb Cut</b></p> <p><b>Commission Findings</b></p> <p><b>Required:</b>  <i>A total of 35% of the linear footage of any street frontage can be devoted to access to off street parking.</i></p> <p><b>Existing:</b>  <i>YMCA (PUD CUP 04-008 &amp; Design Review CR04-013): 5%</i>  <i>Northwood Place (PUD CUP 09-023 &amp; Design Review R09-003): 38 feet/27% Park Circle frontage</i></p> <p><b>Proposed:</b>  <i>As indicated on the Site Plan (Sheet C2.00), 2 curb cuts are proposed along Saddle Road to facilitate two-way traffic for command vehicles and fire apparatus to exit and enter the fire station. The 2 curb cuts are separated by a grass median. The north access serves as the main entrance and exit for staff vehicles and fire trucks. During an emergency response, firefighters will exit the station from the apparatus-bay curb cut.</i></p> <p><i>Tax Lot 6689 has 1,747 feet of street frontage along Park Circle, Saddle Road, and Warm Springs Road. The property has 890 feet of street frontage along Saddle Road. The 2 curb cuts have a combined width of 147.5 feet. An existing 30-foot curb cut along Saddle Road serves as the main access to the YMCA. The total width of the driveway accesses along Saddle Road will be 177.5 feet or 20% of the street frontage along Saddle Road.</i></p>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.125.040	<b>Parking Spaces</b>
			<i>Commission Findings</i>	<p><i>Off-street parking standards apply to any new development and to any new established uses.</i></p> <p><b>Required:</b>  <i>Non-Residential Use in T Zone: 1 parking space per 1,000 gross square feet (Footnote 1: Refer to definition of floor area, gross and with the additional exclusion of common and public areas).</i></p> <p><b>Proposed:</b>  <i>The Ketchum Fire Station is 17,507 gross square feet generating a parking demand of 18 parking spaces. The shared parking areas provide 100 spaces on-site to accommodate the fire station’s parking demand. Parking for over 320 vehicles may be accommodated through on-street and off-site public parking spaces within the area. At the Commission’s request, the applicant has provided a parking diagram showing the YMCA future expansion. The parking diagram shows the 150 on-site parking spaces that will be provided if the YMCA expands in the future. More than enough parking spaces are provided in the area to meet the needs of all users including the fire station and the YMCA.</i></p>

TABLE 4: FINDINGS REGARDING DESIGN REVIEW STANDARDS

Design Review Improvements and Standards (KMC §17.96.060)				
Yes	No	N/A	City Code	City Standards and <i>Commission Findings</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.A1 Streets	The applicant shall be responsible for all costs associated with providing a connection from an existing city street to their development.
			<i>Commission Findings</i>	<i>Tax Lot 6689 is bordered by Park Circle to the north, Saddle Road to the west, and Warm Springs to the south. A driveway access along Saddle Road at the north end of the project site provides two-way traffic to the fire station. The apparatus bay curb cut is reserved for emergency response.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.A2 Streets	All street designs shall be approved by the City Engineer.
			<i>Commission Findings</i>	<i>No changes are proposed to the street design or lanes of travel within the Saddle Road right-of-way.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.B1 Sidewalks	All projects under 17.96.010(A) that qualify as a “Substantial Improvement” shall install sidewalks as required by the Public Works Department.
			<i>Commission Findings</i>	<i>The fire station is new construction and qualifies as a substantial improvement project requiring the installation of sidewalks. A</i>

				<p>concrete path extends the length of the PUD connecting Northwood Place to the YMCA. The path within the project site will be relocated within the Saddle Road right-of-way to align with the existing path.</p> <p>Final civil drawings for all associated ROW improvements shall be submitted with the Building Permit application to be verified, reviewed, and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.</p> <p>See Table 2 for comments and conditions from the City Engineer &amp; Streets Department.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.B2 Sidewalks	<p>Sidewalk width shall conform to the City's right-of-way standards, however the City Engineer may reduce or increase the sidewalk width and design standard requirements at their discretion.</p> <p><i>Commission Findings</i></p> <p>All ROW improvements shall be constructed to City ROW standards. The proposed width of the path is 7 feet to match the existing sidewalk to the north and south. The City Engineer may reduce or increase the sidewalk width and design standard requirements.</p> <p>Final civil drawings for all associated ROW improvements shall be submitted with the Building Permit application to be verified, reviewed, and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project. See Table 2 for review comments and conditions from the City Engineer &amp; Streets Department.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.B3 Sidewalks	<p>Sidewalks may be waived if one of the following criteria is met:</p> <ol style="list-style-type: none"> <li>The project comprises an addition of less than 250 square feet of conditioned space.</li> <li>The City Engineer finds that sidewalks are not necessary because of existing geographic limitations, pedestrian traffic on the street does not warrant a sidewalk, or if a sidewalk would not be beneficial to the general welfare and safety of the public.</li> </ol> <p><i>Commission Findings</i></p> <p>N/A the existing pathway must be relocated within the Saddle Road right-of-way.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.B4 Sidewalks	<p>The length of sidewalk improvements constructed shall be equal to the length of the subject property line(s) adjacent to any public street or private street.</p> <p><i>Commission Findings</i></p> <p>The sidewalk will be constructed across the length of the project site along Saddle Road.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.B5 Sidewalks	<p>New sidewalks shall be planned to provide pedestrian connections to any existing or future sidewalks adjacent to the site. In addition,</p>

				sidewalks shall be constructed to provide safe pedestrian access to and around a building.
			<i>Commission Findings</i>	<i>The new sidewalk along Saddle Road will connect to a path that leads to the fire station's public entrance. The sidewalk extends the length of the PUD and connects Northwood Place to the YMCA. This circulation system connects to the BCRD Wood River Trail multi-use path, commonly referred to as the bike path, to the west across Saddle Road through existing crosswalks.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.B6 Sidewalks	The City may approve and accept voluntary cash contributions in-lieu of the above described improvements, which contributions must be segregated by the City and not used for any purpose other than the provision of these improvements. The contribution amount shall be one hundred ten percent (110%) of the estimated costs of concrete sidewalk and drainage improvements provided by a qualified contractor, plus associated engineering costs, as approved by the City Engineer. Any approved in-lieu contribution shall be paid before the City issues a certificate of occupancy.
			<i>Commission Findings</i>	<i>N/A. The Commission does not recommend a voluntary cash contribution in-lieu of improvements for this project.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.C1 Drainage	All storm water shall be retained on site.
			<i>Commission Findings</i>	<i>All storm water shall be retained on site including water from roof drains. Preliminary drainage plans are indicated on Sheets C2.0 and C3.0 of the Design Review submittal drawings. The proposed drainage plan includes a system of drywells and grass drainage swales.</i>  <i>Prior to issuance of a Building Permit for the project, the applicant shall submit a final drainage plan indicating grading, catch basins, piping, and drywells (KMC §17.96.040.C.2b &amp; KMC §17.96.060.C.1-4) prepared by a civil engineer licensed in the state to be submitted for review and approval by the City Engineer and Streets Department. See Table 2 for City Department comments including City Engineer and Streets Department conditions.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.C2 Drainage	Drainage improvements constructed shall be equal to the length of the subject property lines adjacent to any public street or private street.
			<i>Commission Findings</i>	<i>See above analysis for Ketchum Municipal Code §17.96.060C1. All drainage improvements are required to meet City standards.</i>  <i>All drainage improvements shall be indicated on civil plans prepared by an Idaho licensed engineer and require review and approval from the City Engineer &amp; Streets Department prior to issuance of a Building Permit for the project.</i>



				See Table 2 for review comments and conditions from the City Engineer & Streets Department.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.C3 Drainage	The City Engineer may require additional drainage improvements as necessary, depending on the unique characteristics of a site.
			<i>Commission Findings</i>	<i>A final drainage plan prepared by a civil engineer licensed in the state of Idaho shall be submitted with the Building Permit application to be reviewed and approved by the City Engineer and the Streets Department. The City Engineer may require additional drainage improvements as necessary.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.C.4 Drainage	Drainage facilities shall be constructed per City standards.
			<i>Commission Findings</i>	<i>All drainage facilities within the project site and the public right-of-way shall meet City standards. Final drainage specifications must be included with the civil drawings submitted with the Building Permit application to be reviewed and approved by the City Engineer &amp; Streets Department.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.D1 Utilities	All utilities necessary for the development shall be improved and installed at the sole expense of the applicant.
			<i>Commission Findings</i>	<i>Sheet C4.00 of the project plans show utility connections and locations. All utilities necessary for the fire station must be improved and installed at the sole expense of the applicant. Prior to issuance of a Building Permit, the applicant must secure a will-serve letter from Idaho Power. Final plans will be reviewed and approved by the Utilities Department prior to issuance of a Building Permit for the project. See Table 2 for review comments and conditions from the Utilities Department.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.D2 Utilities	Utilities shall be located underground and utility, power, and communication lines within the development site shall be concealed from public view.
			<i>Commission Findings</i>	<i>All utilities within the development site shall be underground and concealed from public view. See above analysis for Ketchum Municipal Code §17.96.060D1. The generator located at the northeast corner of the project site must be fully screened from public view.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.D3 Utilities	When extension of utilities is necessary all developers will be required to pay for and install two (2") inch SDR11 fiber optical conduit. The placement and construction of the fiber optical conduit shall be done in accordance with city of Ketchum standards and at the discretion of the City Engineer.

			<i>Commission Findings</i>	<i>The applicant is aware of this requirement to install services for high-speed internet to the site. The applicant will work with the City Engineer to identify the location of a fiber line to serve the project.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.E1 Compatibility of Design	The project's materials, colors and signing shall be complementary with the townscape, surrounding neighborhoods and adjoining structures.
			<i>Commission Findings</i>	<p><i>The exterior elevations (Sheet A3.00) show the proposed materials, colors, and signage. The façade design alternates gray horizontal metal panels with brown vertical metal siding. Projecting towards Saddle Road, the four apparatus bays are distinguished from the rest of the fire station with black brick veneer and glass-panel garage doors. The modern building alludes to traditional firehouse design through red detailing framing the garage doors. Integrated with the architecture of the building, the Ketchum Fire Station 1 sign at the south elevation directs attention to the public entryway. In addition to the red firehouse doors, the 1 sign at the front elevation anchors the building as a fire station.</i></p> <p><i>Residential, commercial, and light industrial development border the project site and PUD. Most of the surrounding neighborhood is developed with the exception of a few vacant properties in the light industrial area. To the east of the project site, the light industrial buildings are characterized by more simple design styles that combine stucco, concrete, and brick. Directly north of the project site, the Northwood Place façade design uses stone veneer to differentiate from the horizontal siding. Across Saddle Road, buildings including the church and townhomes integrate river rock, stone veneer, stucco, and wood siding into the façade design. The alternation of brick veneer with board-and-batten siding provides visual interest to the YMCA building.</i></p> <p><i>The materials proposed for the fire station are compatible with the surrounding neighborhood and nearby buildings. The design approach refashions traditional materials combined with contemporary finishes to augment visual interest. The new fire station will add modern style to the eclectic buildings within the neighborhood. The adaptation of traditional materials like the brick complements the more conventional design styles in the neighborhood.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.E2 Compatibility of Design	Preservation of significant landmarks shall be encouraged and protected, where applicable. A significant landmark is one which gives historical and/or cultural importance to the neighborhood and/or community.

			<i>Commission Findings</i>	<i>N/A. No significant landmarks of historical or cultural importance have been identified on the property. The project site has been used for parking and snow storage.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.E3 Compatibility of Design	Additions to existing buildings, built prior to 1940, shall be complementary in design and use similar material and finishes of the building being added to.
			<i>Commission Findings</i>	<i>N/A This standard does not apply because the fire station is new construction.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.F1 Architectural	Building(s) shall provide unobstructed pedestrian access to the nearest sidewalk and the entryway shall be clearly defined.
			<i>Commission Findings</i>	<p><i>The project plans include a path that borders the south side of fire station and directs pedestrians to the public entrance from the shared parking area. This pathway leads to the sidewalk that extends from Northwood Place to the YMCA. This internal circulation system will connect to the bike path across Saddle Road through existing crosswalks.</i></p> <p><i>The fire station accommodates diverse spaces from living quarters to fire truck storage to maintenance areas to administrative offices. These different fire station functions are separated within the building. The apparatus bays at the center of the station separate the administrative areas and firefighter living quarters from areas dedicated to emergency response operations, maintenance, and storage.</i></p> <p><i>The design of the entryway at the south elevation includes large windows and doors inviting the public into the facility. The entryway is distinguished from the rest of the fire station by brown metal siding and signage. The emergency response operations area incorporates with fewer windows and uses different materials combining the gray metal panels with the black brick.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.F2 Architectural	The building character shall be clearly defined by use of architectural features.
			<i>Commission Findings</i>	<p><i>The fire station is characterized by modern design elements with simple building forms and functional floor plans. The orientation and placement of rectangular building shapes with flat roofs at varying heights define the fire station's functional arrangement.</i></p> <p><i>Separating the operations and maintenance areas from the administrative offices and living quarters, the apparatus bays at the center of the station are defined by the glass-paneled garage doors. These windows will introduce natural light into the apparatus bays while providing pedestrians a unique view of the stored fire engines.</i></p>

				<p><i>The southern portion of the building is distinguished from the rest of the fire station through its size, shape, and placement. With significant fenestration, the south façade design welcomes the public into the fire station. The pattern of material placement provides texture to the box building form. The vertical placement of the brown metal siding provides visual relief from the horizontal metal panels and black brick.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.F3 Architectural	<p>There shall be continuity of materials, colors and signing within the project.</p>
			<i>Commission Findings</i>	<p><i>The exterior walls of the building utilize the same metal and brick materials and gray, black, and brown color scheme. Highlighting the apparatus bays, the use of the firehouse red is limited to the apparatus-bay garage doors. The consistent pattern of material placement and colors across all building walls connects the diverse functions of the fire station into one cohesive design.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.F4 Architectural	<p>Accessory structures, fences, walls and landscape features within the project shall match or complement the principal building.</p>
			<i>Commission Findings</i>	<p><u>Accessory Structures</u>  <i>The 1,360 square foot carport at the southeast corner of the project site uses the same black brick material as the fire station. A row of rectangular windows at the carport’s south elevation provides relief from the black brick veneer. The four parking spaces in the carport will be reserved for staff and command vehicles. A covered patio area will extend to the south of the carport. Utilizing the same material and color palette, the carport and patio match the fire station design theme.</i></p> <p><u>Fences &amp; Gates</u>  <i>In the 1990s, a 5-foot concrete retaining wall was constructed along the side property line to separate Tax Lot 6689 from the industrial area to the east. In addition to the existing CMU wall, the project includes a 4-foot tall patio fence, a 6-foot tall trellis, and a rolling gate (Sheets L1.00 &amp; L1.50). The patio fence will not only enhance privacy, but also limits public access to the Fire Department operations area. Comprised of horizontal slats, the patio fence will match the color of the carport and covered patio. The trellis system separates the fire station from the Northwood Place apartments. Plants attach to panels so that the freestanding trellis will transform into a living fence. Multi-functional, the green trellis will enhance screening and soften the fire station’s black, brown, and gray walls.</i></p> <p><u>Landscape</u></p>

				<p><i>The landscape plan is included on Sheets L1.00 and L1.500 of the project plans. Grass medians border Saddle Road and separate the northern driveway access from the apparatus-bay curb cut reserved for emergency response. Grasses and flowering plants are proposed along the front façade at either side of the center apparatus bays. Softening the building's hard edges, the plants invite and welcome the public into the fire station. Flowering plants and shrubs border the north side of the building softening the gray and brown metals panels. Boulders are incorporated throughout the landscape design providing visual interest.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.F5 Architectural	<p>Building walls shall provide undulation/relief, thus reducing the appearance of bulk and flatness.</p> <p><i>Commission Findings</i></p> <p><i>The fire station design uses rectangular shapes in different forms to reduce the flatness of building walls. The gray metal panels, black brick, brown metal siding, and window openings are all unique rectangular elements in different sizes and orientations. For example, the gray metal panels are placed horizontally and the brown metal siding is aligned vertically. This contrast of rectangular shapes provides visual interest to the box-shaped building. In addition to this rectangular arrangement, color variation and multiple windows and doors to breathe life into the fire station. The varying roof heights define the separate areas of the building. Projecting beyond the rest of the building, the firehouse red detailing highlight framing the glass panels focuses attention to the apparatus bays.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.F6 Architectural	<p>Building(s) shall orient towards their primary street frontage.</p> <p><i>Commission Findings</i></p> <p><i>The front façade of the building orients towards Saddle Road.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.F7 Architectural	<p>Garbage storage areas and satellite receivers shall be screened from public view and located off alleys.</p> <p><i>Commission Findings</i></p> <p><i>The concrete trash enclosure is located at the southeast corner of the project site. In addition to the concrete trash enclosure, the carport and fenced patio further screen the dumpster from public view.</i></p> <p><i>Prior to issuance of a Building Permit for the project, the applicant shall submit a will serve letter from Clear Creek Disposal to the Planning &amp; Building Department.</i></p> <p><i>The project plans do not include any satellite receivers.</i></p>

☒	☐	☐	17.96.060.F8 Architectural	Building design shall include weather protection which prevents water to drip or snow to slide on areas where pedestrians gather and circulate or onto adjacent properties.
			<i>Commission Findings</i>	<i>Enhancing weather protection, the flat roof form will prevent water from dripping or snow from sliding from the building. At the main entrance, the second floor cantilevers beyond the first floor to cover the pedestrian pathway.</i>
☒	☐	☐	17.96.060.G1 Circulation Design	Pedestrian, equestrian and bicycle access shall be located to connect with existing and anticipated easements and pathways.
			<i>Commission Findings</i>	<i>The project plans include a path that directs pedestrians to the public entrance from the shared parking area. This pathway leads to the sidewalk that extends from Northwood Place to the YMCA. This internal circulation system connects to the bike path across Saddle Road through existing crosswalks.</i>
☐	☐	☒	17.96.060.G2	Awnings extending over public sidewalks shall extend five (5') feet or more across the public sidewalk but shall not extend within two (2') feet of parking or travel lanes within the right of way.
			Circulation Design	<i>N/A. No awnings are proposed to extend across the public sidewalk.</i>
☒	☐	☐	17.96.060.G3 Circulation Design	Traffic shall flow safely within the project and onto adjacent streets. Traffic includes vehicle, bicycle, pedestrian and equestrian use. Consideration shall be given to adequate sight distances and proper signage.
			<i>Commission Findings</i>	<i>Neighboring Highway 75, Saddle Road, and Warm Springs Road, the fire station site is centrally located with easy access to main routes to all service delivery areas. A driveway access off Saddle Road at the northwest corner of the site will facilitate two-way traffic to and from the fire station. The curb cut along the four apparatus bays will be reserved for emergency response. Sight line windows are incorporated into the building design that allow emergency responders to view any pedestrians or bicyclists approaching the station on the sidewalk and any obstructions in front of the apparatus bays.</i>  <i>The City has engaged a traffic engineering firm to identify safety concerns and circulation conflicts between neighborhood traffic and Fire Department emergency response operations and identify any mitigation measures. These mitigation measures may include warning signals, traffic control devices, and alert systems. If the traffic study identifies circulation conflicts, the City will implement any</i>

				<p><i>mitigation measures necessary to ensure the safety of pedestrians, bicyclists, and drivers in the area.</i></p> <p><i>Prior to issuance of a Building Permit for the project, the City Engineer and Streets Department shall review the civil drawings to ensure adequate sight distances and proper signage for the proposed driveway access.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.G4 Circulation Design	<p>Curb cuts and driveway entrances shall be no closer than twenty (20') feet to the nearest intersection of two or more streets, as measured along the property line adjacent to the right of way. Due to site conditions or current/projected traffic levels or speed, the City Engineer may increase the minimum distance requirements.</p>
			<i>Commission Findings</i>	<p><i>The project site is located over 250 ft from Saddle Road's intersection with Park Circle and over 400 ft from the Saddle Road and Warm Springs Road intersection. Clear sight lines are provided to the north and south of the fire station driveways.</i></p> <p><i>Prior to issuance of a Building Permit for the project, the City Engineer and Streets Department shall review the civil drawings to ensure adequate sight distances and proper signage for the proposed parking access.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.G5 Circulation Design	<p>Unobstructed access shall be provided for emergency vehicles, snowplows, garbage trucks and similar service vehicles to all necessary locations within the proposed project.</p>
			<i>Commission Findings</i>	<p><i>Clear sight lines are provided to the north and south of the fire station driveways. Sight line windows are incorporated into the building design that allow emergency responders to view any pedestrians or bicyclists approaching the station on the sidewalk and any obstructions in front of the apparatus bays. The north driveway access facilitates two-way traffic for staff vehicles to enter and exit the station. The curb cut at the apparatus bays is reserved for emergency response.</i></p> <p><i>Garbage trucks will access the trash enclosure from the shared parking area to the south of the station. As the circulation areas will include a snowmelt system, snowplows will not need to remove snow from the site.</i></p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.H1 Snow Storage	<p>Snow storage areas shall not be less than thirty percent (30%) of the improved parking and pedestrian circulation areas.</p>
			<i>Commission Findings</i>	<p><i>As permitted by KMC §17.96.060.H4, the traffic and pedestrian circulation areas will include a snowmelt system instead of a snow storage area.</i></p>

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.H2 Snow Storage	Snow storage areas shall be provided on-site.
			<i>Commission Findings</i>	<i>N/A. As indicated on Sheet C2.00 of the project plans, the fire station site will be snow-melted as permitted pursuant to KMC §17.96.060.H4.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060.H3 Snow Storage	A designated snow storage area shall not have any dimension less than five (5') feet and shall be a minimum of twenty-five (25) square feet.
			<i>Commission Findings</i>	<i>As permitted by KMC §17.96.060.H4, the project site will include a snowmelt system instead of a snow storage area.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.H4 Snow Storage	In lieu of providing snow storage areas, snow melt and hauling of snow may be allowed.
			<i>Commission Findings</i>	<i>The fire station paved circulation areas will include a snowmelt system instead of snow storage. Snow-melting all circulation areas will enhance Fire Department operations and emergency response.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.I1 Landscaping	<b>Landscaping is required for all projects.</b>
			<i>Commission Findings</i>	<i>The landscape plan is included on Sheets L1.00 and L1.50 of the project plans. The grasses, flowering plants, and shrubs complement the fire station's modern design.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.I2 Landscaping	Landscape materials and vegetation types specified shall be readily adaptable to a site's microclimate, soil conditions, orientation and aspect, and shall serve to enhance and complement the neighborhood and townscape.
			<i>Commission Findings</i>	<i>The landscape materials and vegetation types shall be readily adaptable to the site's microclimate, soil conditions, orientation, and aspect. The proposed landscaping will soften the fire station's hard lines and rectangular mass. Bordering the front property line, the tall grasses and flowering plants augment the welcoming entryway to the fire station.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.I3 Landscaping	All trees, shrubs, grasses and perennials shall be drought tolerant. Native species are recommended but not required.
			<i>Commission Findings</i>	<i>All proposed landscape materials and vegetation types shall be drought tolerant. The applicant is encouraged to select native species.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.I4 Landscaping	Landscaping shall provide a substantial buffer between land uses, including, but not limited to, structures, streets and parking lots. The development of landscaped public courtyards, including trees and shrubs where appropriate, shall be encouraged.
			<i>Commission Findings</i>	<i>The proposed landscaping is appropriate for the fire station project. The living trellis buffers the fire station from the Northwood Place apartments. An existing retaining wall separates the project site from the light industrial area.</i>



<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060.J1 Public Amenities	Where sidewalks are required, pedestrian amenities shall be installed. Amenities may include, but are not limited to, benches and other seating, kiosks, bus shelters, trash receptacles, restrooms, fountains, art, etc. All public amenities shall receive approval from the Public Works Department prior to design review approval from the Commission.
			<i>Commission Findings</i>	<i>The project will realign the existing sidewalk that extends from Northwood Place to the YMCA. The width does not provide sufficient clearance to allow public amenities within the sidewalk design while still maintaining a clear path of travel as required by ADA. Further, sight constraints limit available locations to install amenities as any obstruction would conflict with vehicle ingress and egress into the site. Due to associated site constraints and potential conflicts with Fire Department operations, the fire station project does not propose any exterior public amenities such as benches. The community room and public restrooms provided within the fire station satisfy the public amenity requirement.</i>

#### CONCLUSIONS OF LAW

1. The City of Ketchum is a municipal corporation established in accordance with Article XII of the Constitution of the State of Idaho and Title 50 Idaho Code and is required and has exercised its authority pursuant to the Local Land Use Planning Act codified at Chapter 65 of Title 67 Idaho Code and pursuant to Chapters 3, 9 and 13 of Title 50 Idaho Code to enact the Ordinances and regulations, which Ordinances are codified in the Ketchum City Code (“KMC”) and are identified in the Findings of Fact and which are herein restated as Conclusions of Law by this reference and which City Ordinances govern the Applicant’s Mountain Overlay Design Review Application for the development and use of the project site.
2. The Commission has authority to hear the applicant’s Design Review Application pursuant to Chapter 17.96 of Ketchum Code Title 17.
3. The City of Ketchum Planning Department provided adequate notice for the review of this application.
2. The Design Review application is governed under Ketchum Municipal Code Chapters 17.96, 17.124, 17.08, 17.12, 17.18, and 17.128.
3. The Design Review application for the fire station meets the standards of approval under Title 17 of KMC subject to conditions of approval.

#### DECISION

**THEREFORE**, the Ketchum Planning and Zoning Commission **approves** this Design Review application this Monday, February 24<sup>th</sup>, 2020 subject to the following conditions:

#### CONDITIONS OF APPROVAL

1. This Design Review approval is subject to all comments and conditions as described in the City Department Comments (Table 2), the Zoning and Dimensional Standards Analysis including the off-street parking requirements subject to existing agreements between the City and the YMCA (Table 3), and the Design Review Standards Analysis (Table 4).
2. This Design Review approval is based on the plans and information presented and approved at the meeting on the date noted herein. Building Permit plans for all on-site improvements must conform to the approved Design Review plans unless otherwise approved in writing by the Planning and Zoning Commission or Administrator. Any building or site discrepancies which do not conform to the approved plans will be subject to removal.
3. All governing ordinances, requirements, and regulations of the Fire Department (2012 International Fire Code and local Fire Protection Ordinance No.1125), Building Department (2012 International Building Code, the 2012 International Residential Code, and Title 15 of Ketchum Municipal Code), Utilities Department, Street Department (Title 12 of Ketchum Municipal Code), and the City Engineer shall be met prior to Certificate of Occupancy.
4. The applicant shall submit final civil drawings prepared by an engineer registered in the State of Idaho to include specification for the ROW, utilities, and drainage improvements to be reviewed and approved by the City Engineer, Streets, and Utilities departments prior to issuance of a Building Permit for the project.
5. The term of Design Review approval shall be twelve (12) months from the date that the Findings of Fact, Conclusions of Law, and Decision are adopted by the Commission or upon appeal, the date the approval is granted by the Council subject to changes in zoning regulations (KMC §17.96.090).
6. All Design Review elements shall be completed prior to issuance of a Certificate of Occupancy for the building.
7. All exterior lighting on the property shall be in compliance with Ketchum Municipal Code, Chapter 17.132, Dark Skies, and shall be inspected by Planning Staff and approved prior the issuance of a Certificate of Occupancy for the building.
8. The project shall comply with the requirements of §17.124.040 Development Standards as adopted on the date a Building Permit is submitted for the project.
9. Prior to issuance of a Building Permit for the project, the applicant shall submit a construction management plans, which addresses each of the standards as set forth in Ketchum Municipal Code, Chapter 15.06 Construction Activity Standards.
10. In addition to the requirements set forth in this Design Review approval, this project shall comply with all applicable local, state, and federal laws.
11. OPTIONAL Planning & Zoning Commission recommendations to Ketchum City Council regarding the traffic and circulation study and safety treatments.

Findings of Fact **adopted** this 24<sup>th</sup> day of February, 2020.

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Neil Morrow, Chair  
City of Ketchum  
Planning and Zoning Commission

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