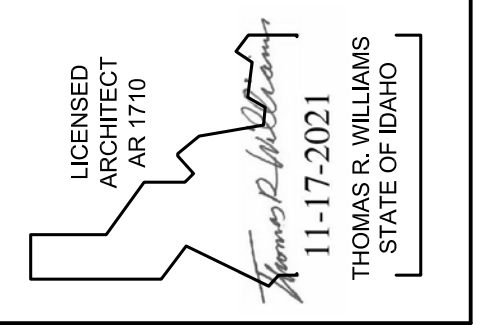
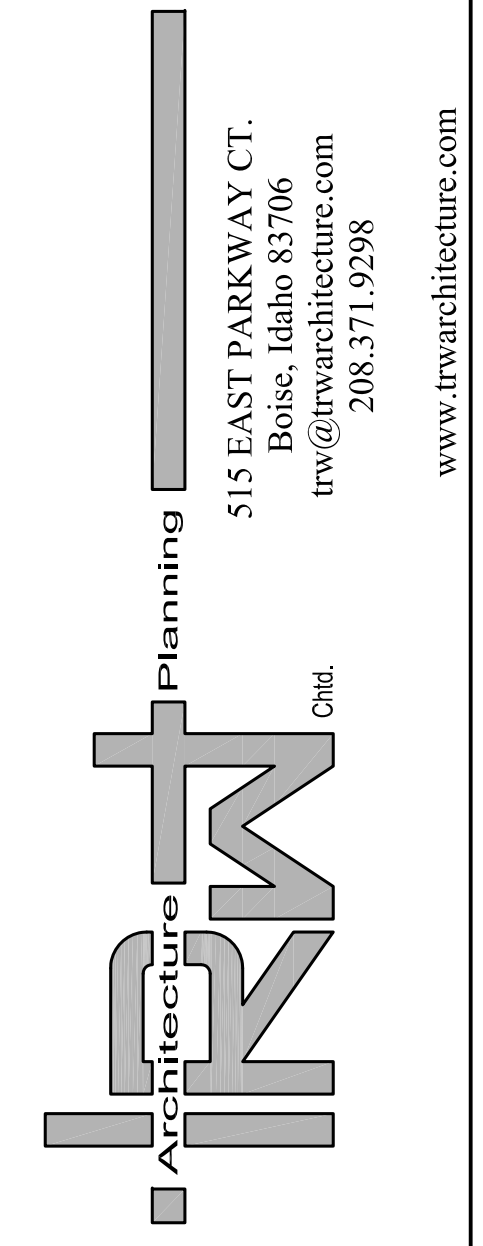


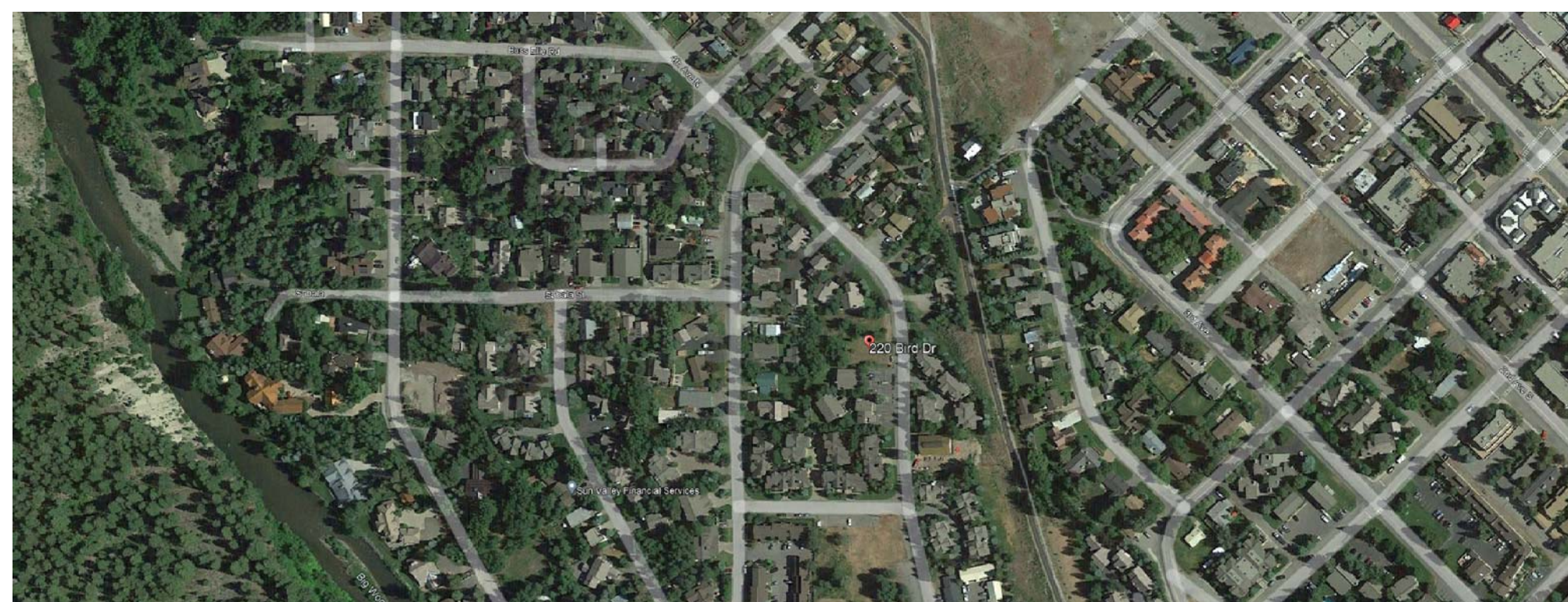


# SNOWBIRD TOWNHOMES

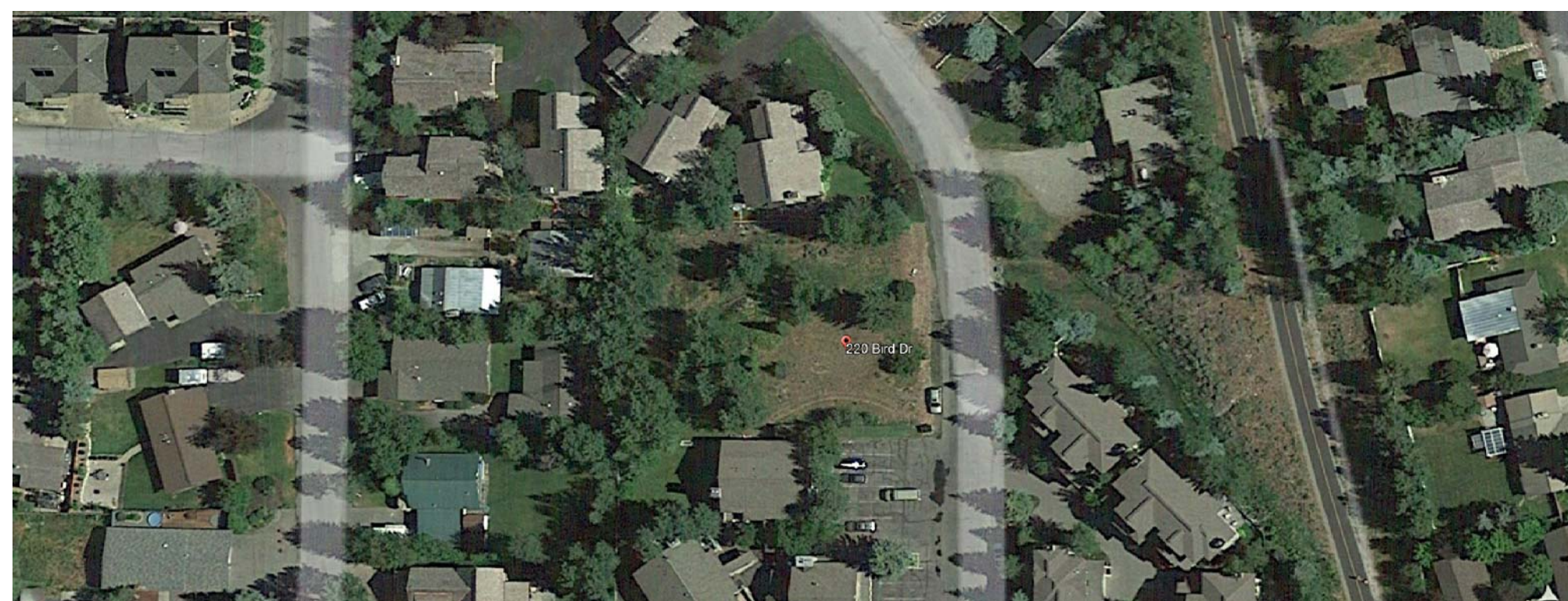
Ketchum, Idaho



## PROJECT LOCATION



AREA MAP



VICINITY MAP

## BUILDING CODE

ALL WORK SHALL MEET OR EXCEED THE CURRENT ADOPTED CODES:

- KETCHUM MUNICIPAL CODE
- THE INTERNATIONAL RESIDENTIAL CODE (IRC), 2018 EDITION, AS AMENDED BY THE IDAHO BUILDING CODE BOARD, PARTS I-III AND IX, INCLUDING APPENDIX F, RADON CONTROL METHODS
- THE INTERNATIONAL ENERGY CONSERVATION CODE (IECC), 2018 EDITION, AS MANDATED BY THE IDAHO BUILDING CODE BOARD
- THE INTERNATIONAL FIRE CODE, 2018 EDITION, INCLUDING APPENDIX CHAPTERS B-F PUBLISHED BY THE INTERNATIONAL CODE COUNCIL
- PROVIDE COMPLIANCE WITH THE KETCHUM MUNICIPAL CODE, CHAPTER 15.08 REQ'D
- ALL OUTDOOR LIGHT FIXTURES TO COMPLY WITH THE CITY OF KETCHUM DARK SKY ORDINANCE
- PROVIDE APPROVED AUTOMATIC SPRINKLER SYSTEM MEETING APPLICABLE CODES

## PROJECT DATA

**PROJECT LOCATION:** 220 BIRD DRIVE  
KETCHUM, IDAHO 83340

**PROJECT DESCRIPTION:**

**LEGAL DESCRIPTION:** SNOWBIRD SUB LOT 2 BLK 1

**SIZE:** LOT 2A: .23 ACRES = 10,022 SF +/-  
(10,022 SF - 1,873 SF OF DRIVEWAY = 8,149 SF)

**ZONE:** GR-1

**OCCUPANCY GROUP:** 2 TOWNHOMES

**TYPE OF CONSTRUCTION:** TYPE V-B

**STORIES:** 3 STORIES

**MAXIMUM BUILDING HEIGHT:** 34'-4" (unit 1, lot 2a)

**ZONING:** CITY

**NATIONAL WETLANDS INVENTORY:** NO

**MOUNTAIN OVERLAY DISTRICT:** NO

**PARCEL DETERMINATION:** NO

**LOT 2A SQUARE FOOTAGE SUMMARY**

1st LEVEL	1,385 SF
2nd LEVEL	1,375 SF
3rd LEVEL	941 SF
<b>TOTAL</b>	<b>3,701 SF</b>

**BUILDING COVERAGE / OPEN SPACE**  
1,420 SF / 8,149 SF = 17.42%  
X2 BUILDINGS = 34.84% TOTAL

## CONTACTS DIRECTORY

**OWNERS:**  
SCOTT EDWARDS  
13019 NAOMILAWN DR SW  
LAKEWOOD, WA 98498

**ARCHITECT:**  
TRW ARCHITECTURE CHTD.  
TOM WILLIAMS  
515 EAST PARKWAY CT.  
BOISE, ID 83706  
208.371.9298  
trw@trwarchitecture.com

**CONTRACTOR:**  
CHOCOLATE GULCH BUILDERS, LLC  
ROGER GODFREY  
214 CHOCOLATE GULCH ROAD  
KETCHUM, ID 83340  
208.720.0936  
200birdreg@gmail.com

**LANDSCAPE ARCHITECT:**  
TTKLA KING  
TERRY KING  
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BOISE, ID 83701  
208.721.3730  
terry@ttkla.com

**STRUCTURAL ENGINEER:**  
FROST STRUCTURAL ENGINEERING  
MARKELL BATEMAN, P.E.  
1020 E LINCOLN ROAD  
IDAHO FALLS, ID 83401  
208.227.8406  
contact@frost-structural.com

## SHEET INDEX

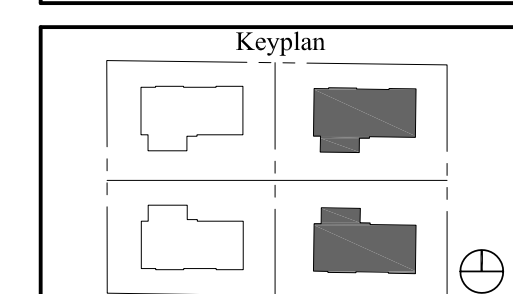
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A0.0	TITLE SHEET	L-1.1	GRADING PLAN
C1	GRADING & DRAINAGE PLAN	L-1.2	LANDSCAPE PLAN
C2		L-1.3	DETAILS
C3		20 TOTAL SHEETS	
ARCHITECTURAL			
CM1.0	CONSTRUCTION MANAGEMENT PLAN		
A1.0	ARCHITECTURAL SITE PLAN		
A2.0	FLOOR PLANS		
A2.1	FLOOR PLANS		
A3.0	BUILDING SECTIONS		
A3.1	BUILDING SECTIONS		
A3.2	COMPOSITE BUILDING SECTIONS		
A4.0	EXTERIOR ELEVATIONS		
A4.1	EXTERIOR ELEVATIONS		
A4.2	COMPOSITE EXTERIOR ELEVATIONS		
A4.3	COMPOSITE EXTERIOR ELEVATIONS		
A4.4	PHOTOMETRIC PLAN		
A5.0	PROJECT MATERIALS COLOR BOARD		

**SNOWBIRD TOWNHOMES**  
**LOT 2A**  
 220 BIRD DRIVE  
 LOTS 1 & 2, SNOWBIRD SUBDIVISION  
 KETCHUM, IDAHO 83340

2021 17

Date: 03/15/2022  
 Drawn By: RJS  
 Checked By: TW

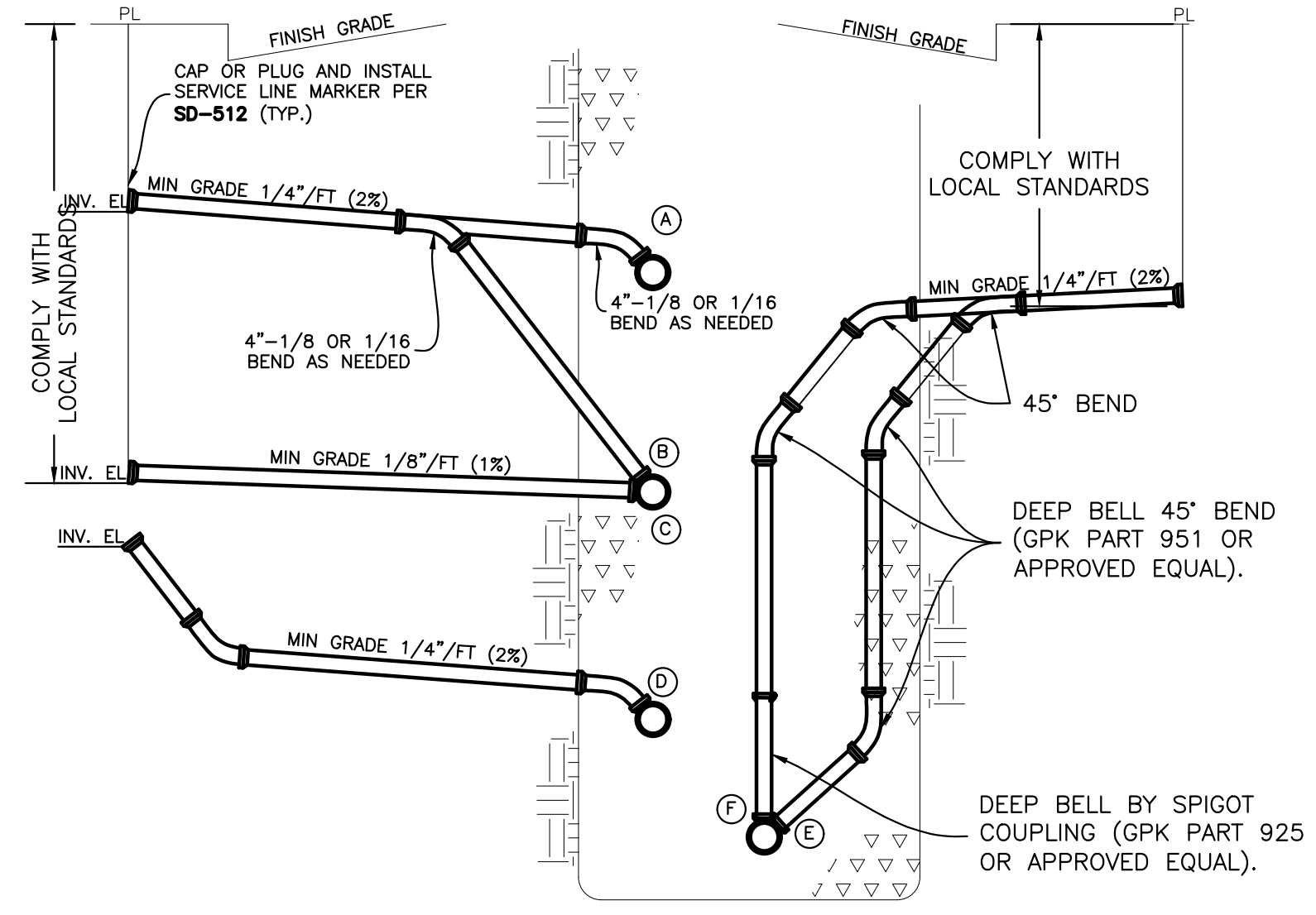
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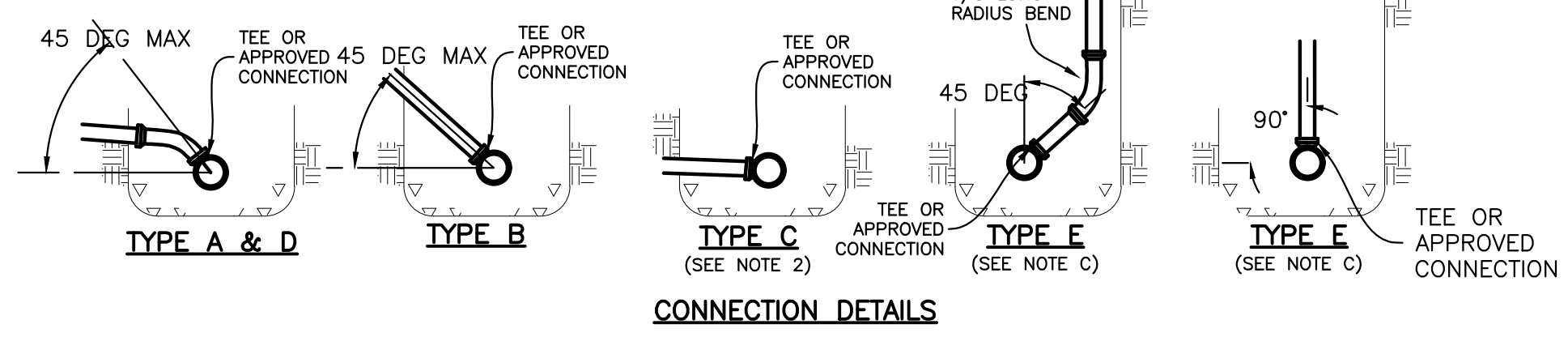
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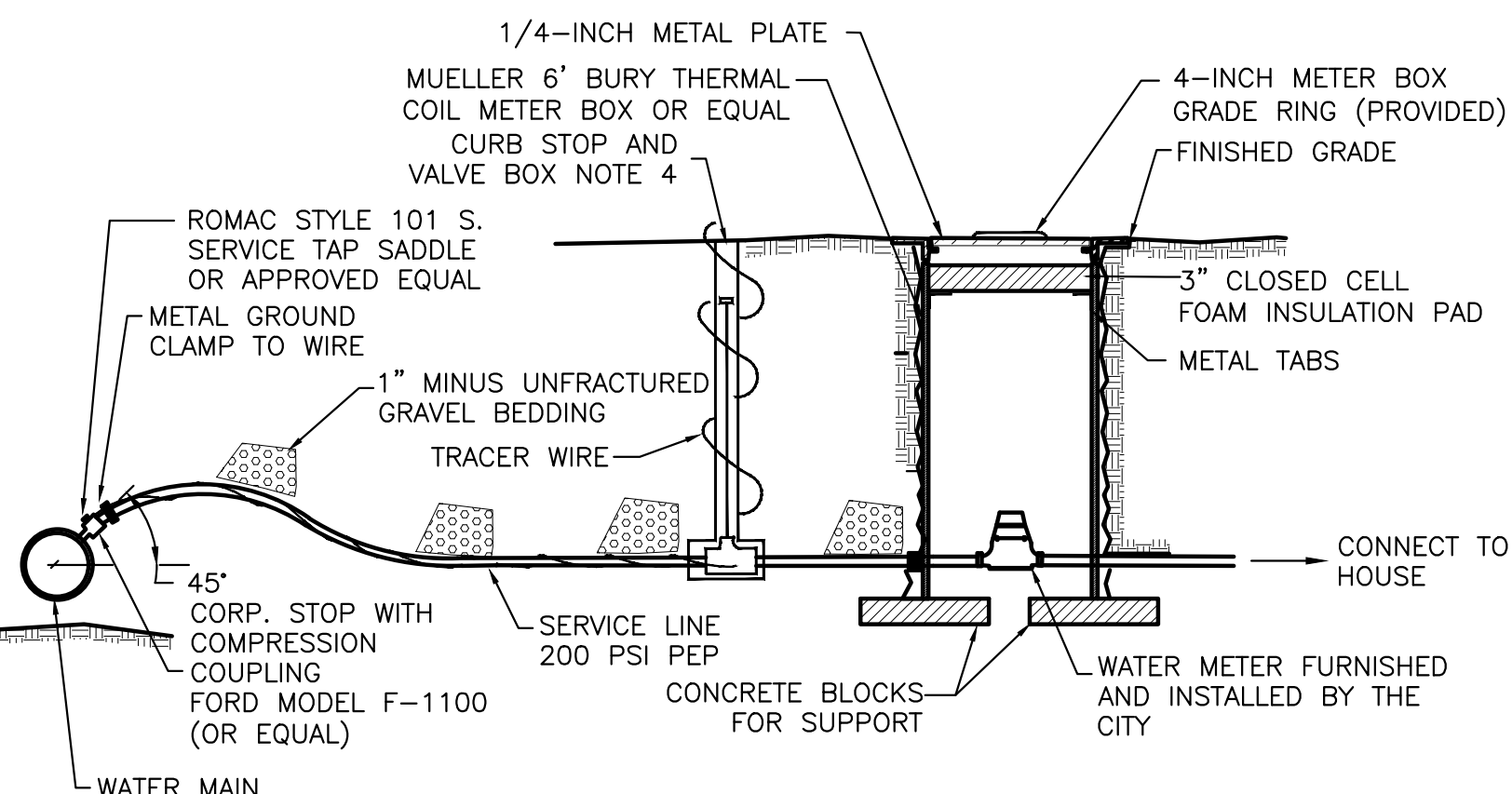
PROFESSIONAL ENGINEER  
 SCOTT J. EDWARDS  
 STATE OF IDAHO  
 PRICED JOHANNESSEN  
 17661  
 6/25/21



- NOTES**
- ALL SERVICE LINES TO BE 4" INSIDE DIAMETER UNLESS OTHERWISE NOTED.
  - TYPE "C" CONNECTION WILL ONLY BE ALLOWED IF SHOWN ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER.
  - TYPE "E" OR "F" ALLOWABLE FOR TRENCHES 15" DEEP OR GREATER WITH PRE-APPROVAL BY ENGINEER.

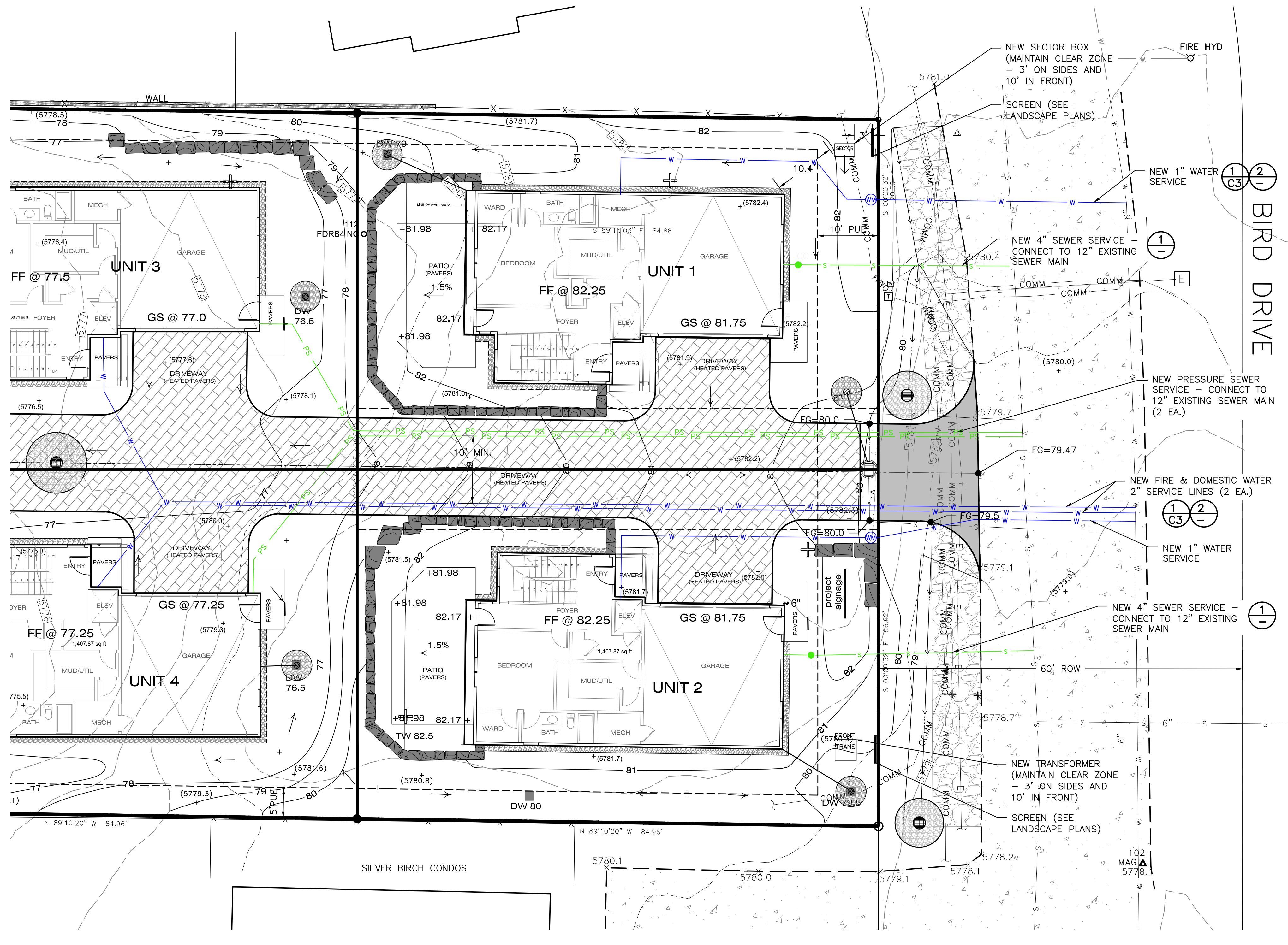


1 STANDARD SEWER SERVICE LINES  
 SCALE: NONE



- NOTES**
- WATER SERVICE LINE SHALL HAVE A 6" MIN. BURY DEPTH
  - SERVICE LINE SHALL BE 1" DIAMETER POLYETHYLENE PIPE UNLESS OTHERWISE SPECIFIED.
  - WATER SERVICE LINES SHALL BE BEDDED WITH 1" MINUS UNFRACTURED GRAVEL. BEDDING SHALL BE INSTALLED 4" UNDER THE PIPE AND 6" OVER THE PIPE.
  - FORD MODEL B-111 RESILIENT SEAT, CURB BALL VALVE (OR EQUAL), FORD EXTENSION CURB BOX WITH ARCHED BASE, 1-INCH UPPER SECTION, AND 2 HOLE "ERIE" PATTERN LID.

2 WATER SERVICE AND METER CONNECTION  
 SCALE: N.T.S.



REVISIONS	DESCRIPTION	DATE	BY
No. 1			



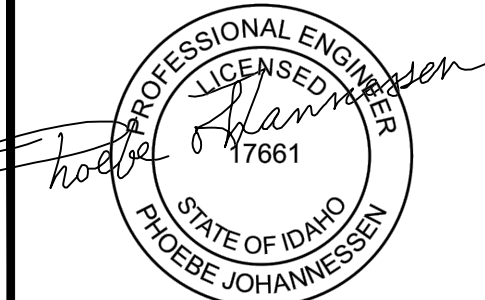
PREPARED BY:  
 BENCHMARK ASSOCIATES, P.A.  
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 KETCHUM, IDAHO 83340  
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 FAX 726-9514  
 WEB: WWW.BMASB.COM  
 MAIL: WWW.BMASB.COM

**UTILITY PLAN**  
 SNOWBIRD TOWNHOMES  
 T4N, R17E, SEC 13, B.M., CITY OF KETCHUM, IDAHO  
 PREPARED FOR: SCOTT J. EDWARDS

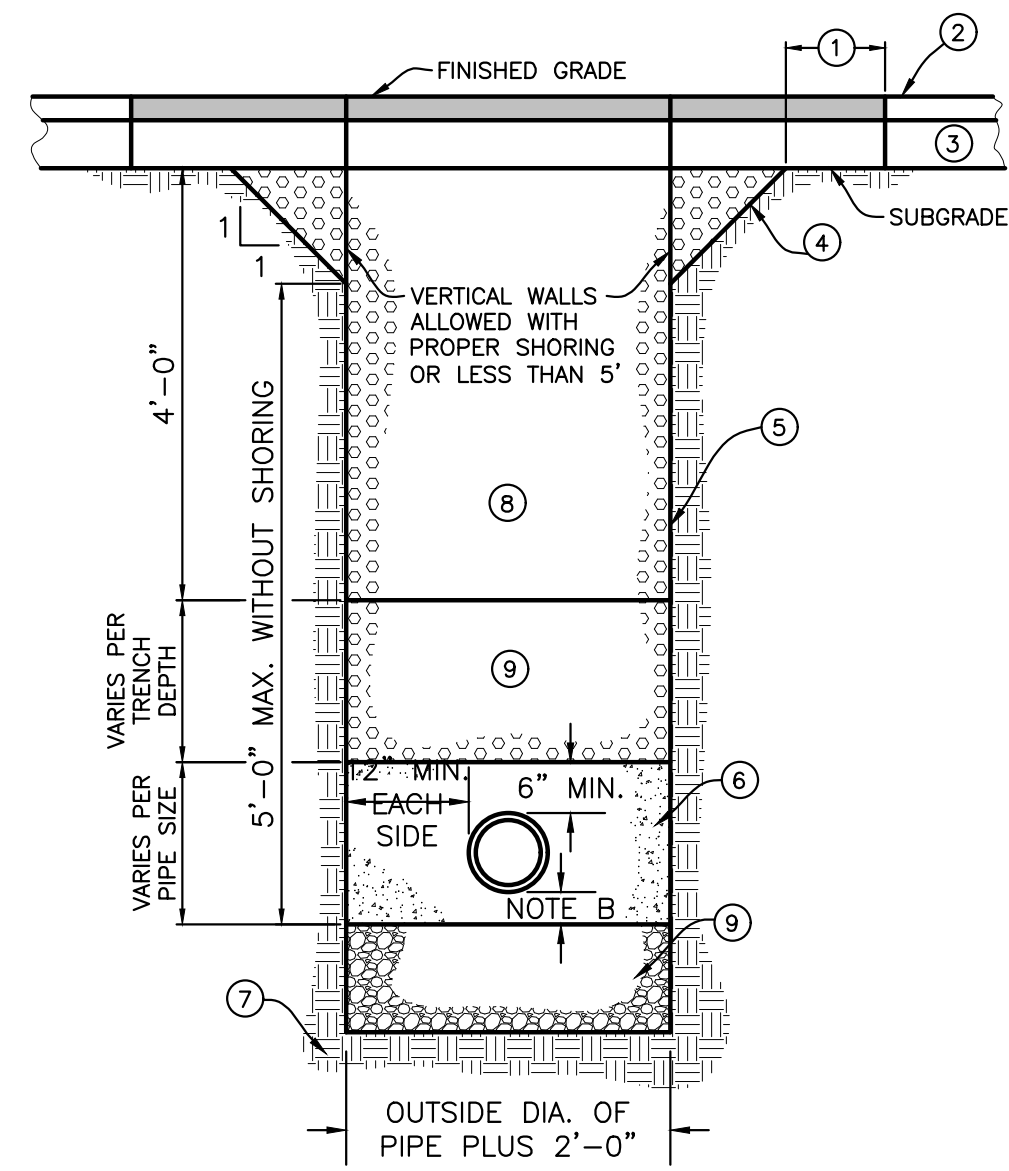
DRAWN BY: PLJ  
 DESIGNED BY: PLJ  
 CHECKED BY: GLM  
 DATE: 6/25/21  
 PROJECT NO.: 21070

DRAWING NO.

C2



6/25/21



**LEGEND**

① 6" MIN. REQUIRED BOTH SIDES & SHALL BE SAWCUT.  
② EXISTING SURFACE. (REPAIR ASPHALT TO MATCH EXISTING. (3" MINIMUM COMPACTED)  
③ EXISTING BASE.  
④ TRENCH BACK SLOPE  
⑤ VERTICAL TRENCH WALLS, SHORING PER O.S.H.A.  
⑥ PIPE BEDDING PER ISPMC SECTION-305 (SEE SD-302).  
⑦ UNDISTURBED SOIL  
⑧ LEAN CONCRETE  
⑨ LOWER COMPACTION ZONE

**DESCRIPTION AND CONSTRUCTION REQUIREMENTS:**

IN AREAS WHERE IT IS NECESSARY TO CUT THE ASPHALT PAVEMENT AND DIG A TRENCH FOR BURIAL OF CONDUIT CABLE OR OTHER CITY UTILITY, THE TRENCH SHALL BE BACKFILLED WITH A LEAN CONCRETE MIX WITH THE FOLLOWING PROPORTIONS OF MATERIALS -

COARSE AGGREGATE (3/4" MINUS)	2,600 LBS
SAND	800 LBS
PORTLAND CEMENT	94 LBS
WATER	11 GAL. (MAX.)

WATER CONTENT IS MAXIMUM AND MAY BE REDUCED. CARE SHALL BE TAKEN TO ASSURE THAT EXCESS WATER IS NOT PRESENT IN THE MIXING DRUM PRIOR TO CHARGING THE MIXER WITH MATERIALS. THOROUGH MIXING WILL BE REQUIRED PRIOR TO DISCHARGE.

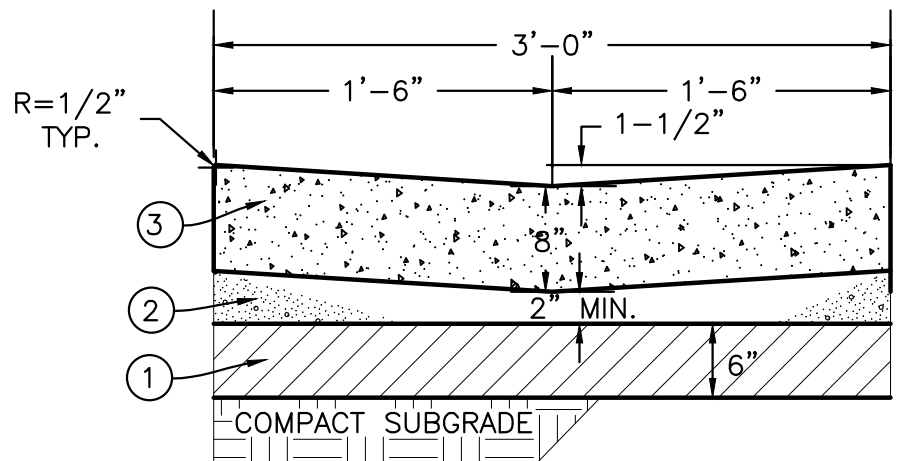
NO COMPACTION, VIBRATION OR FINISHING IS REQUIRED. THE LEAN CONCRETE MIX SHALL BE STRUCK OFF AT OR BELOW THE ELEVATION OF THE PLANT MIX SURFACING WITH A SQUARE-NOSE SHOVEL OR SIMILAR HAND TOOL. THE BACKFILL MIX SHALL BE ALLOWED TO SET FOR A MINIMUM OF 2 HOURS BEFORE THE PERMANENT PLANT MIX SURFACING IS PLACED TO COMPLETE THE TRENCH REPAIR.

TEMPORARY PLACEMENT OF ASPHALT COLD MIX SURFACING MAY BE NECESSARY TO ACCOMMODATE TRAFFIC WITHIN THE FIRST 2 HOURS OF BACKFILL PLACEMENT PRIOR TO COMPLETING THE PERMANENT REPAIR.

**NOTES**

A TRENCH EXCAVATION PER ISPMC SECTION-301.  
B BACKFILL AND COMPACTION PER ISPMC SECTION-306.

1 TRENCH REPAIR IN CITY STREET  
C2 SCALE: NONE



**TYPICAL SECTION**

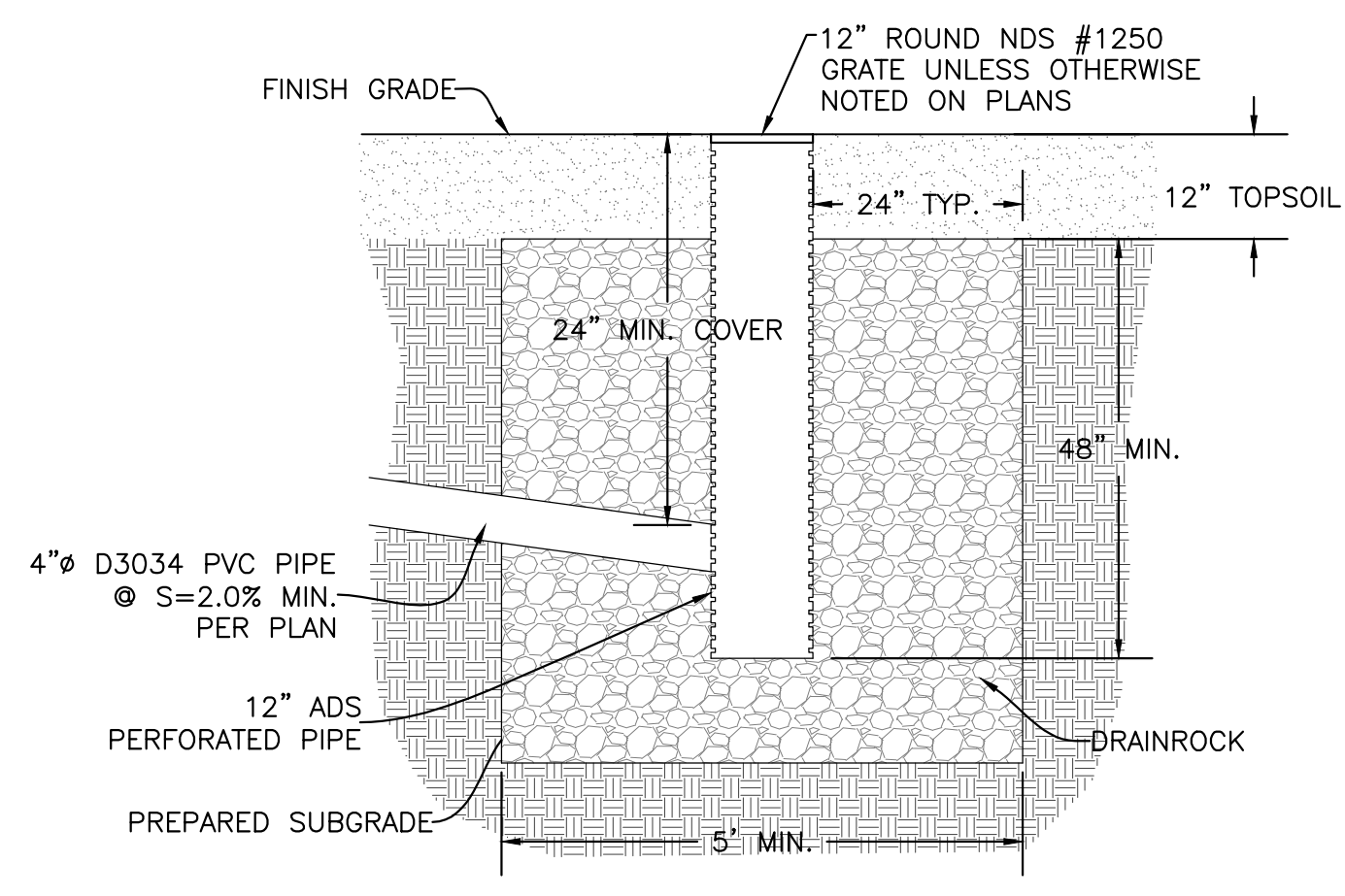
**LEGEND**

① 2" TYPE II SUBBASE  
② 3/4" TYPE I AGGREGATE BASE  
③ CONCRETE

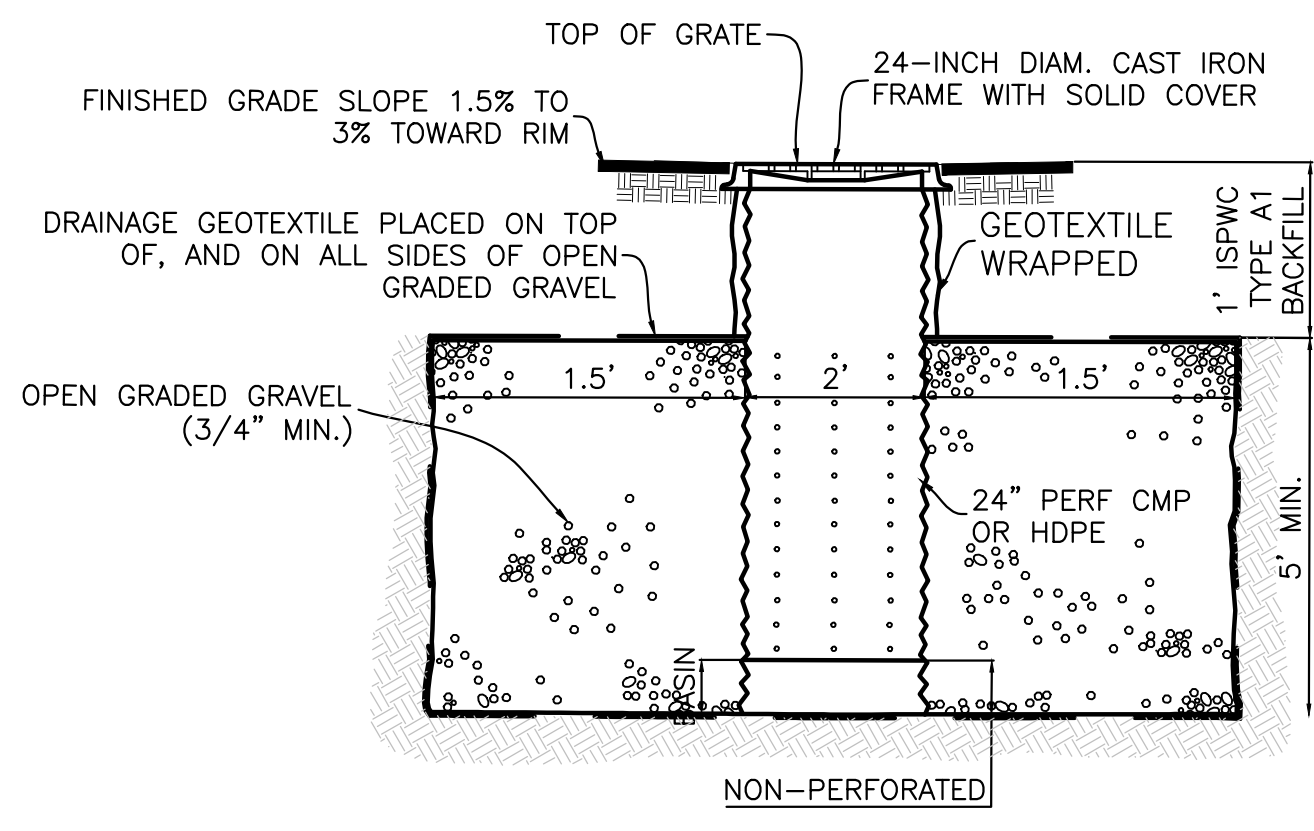
**NOTES:**

A GRADE OF GUTTER MINIMUM 0.5%.  
B 1/2-INCH PREFORMED EXPANSION JOINT MATERIAL (AASHTO M 213. AT TERMINAL POINTS OF RADII.  
C SCORE INTERVALS - 10" MAXIMUM SPACING.  
D MATERIALS SHALL CONFORM WITH CURRENT ISPMC STANDARDS, DIVISION 800 AGGREGATES AND ASPHALT.

2 36" CONCRETE VALLEY GUTTER  
C1 SCALE: N.T.S.



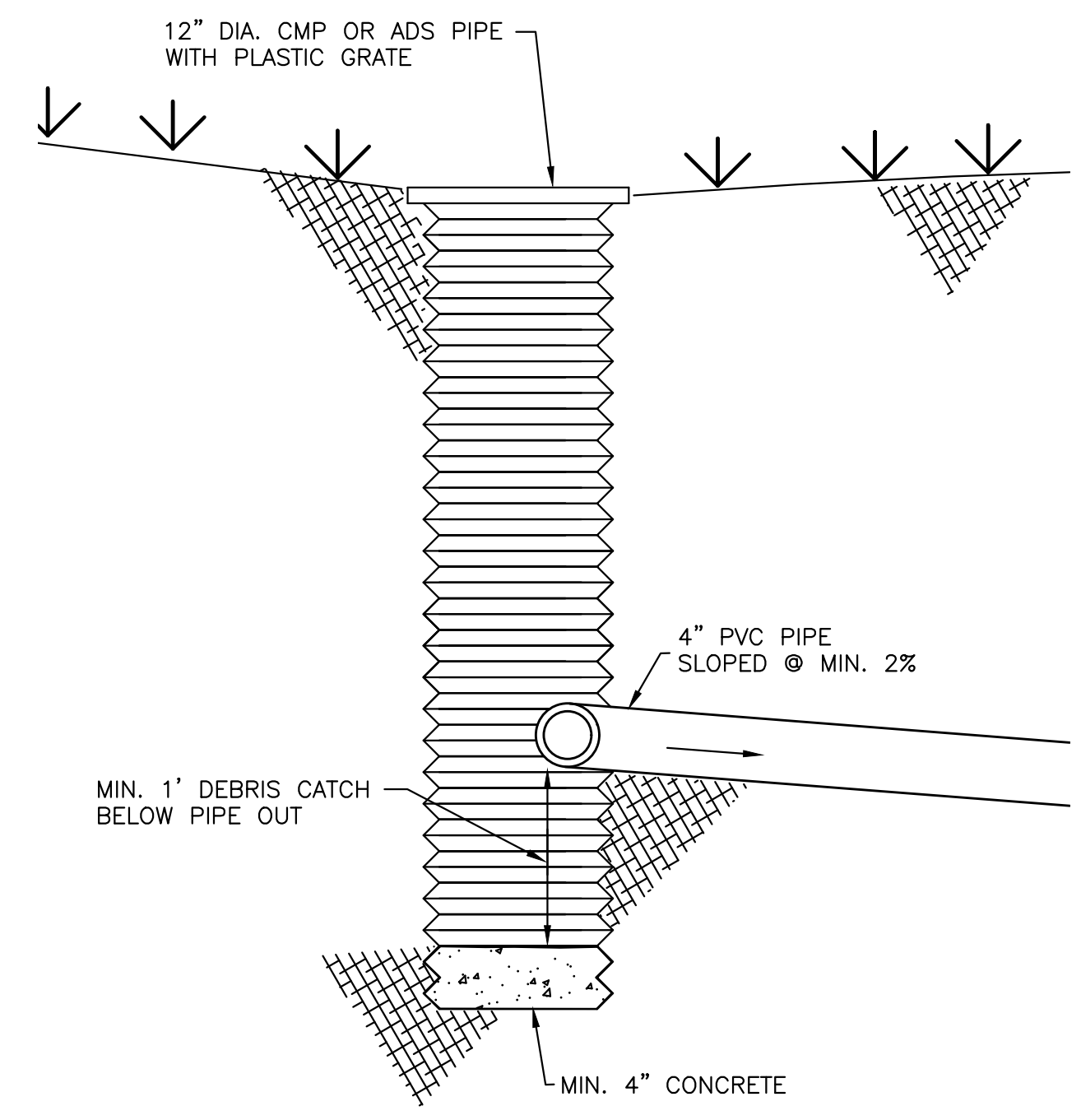
3 LANDSCAPE DRYWELL  
C1 SCALE: N.T.S.



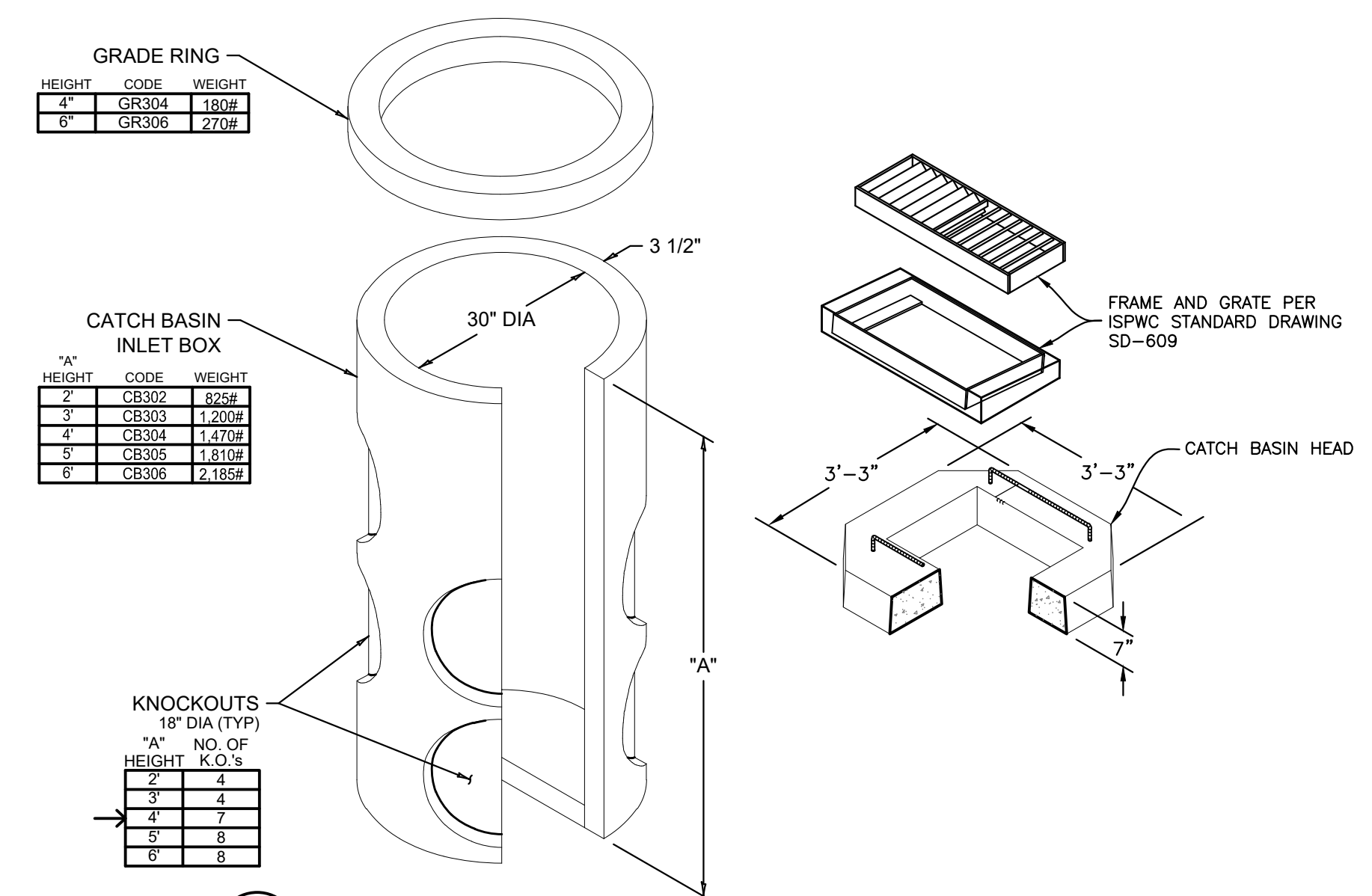
**NOTES:**

① ALL PRODUCTS AS NOTED OR APPROVED SUBSTITUTION.

4 DRIVEWAY DRYWELL DETAIL  
C1 SCALE: NONE



5 12" CATCH BASIN  
C1 SCALE: NONE



6 30" CATCH BASIN  
C1 SCALE: N.T.S.

REVISIONS	DATE	BY
No. 1		

**BENCHMARK ASSOCIATES**

PREPARED BY:  
BENCHMARK ASSOCIATES, P.A.  
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**DETAILS**


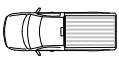
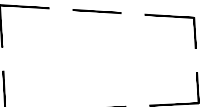
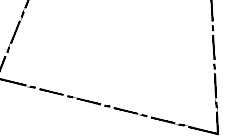
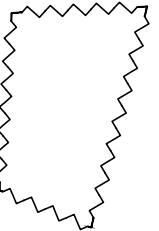
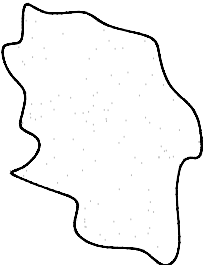


SNOWBIRD TOWNHOMES  
T4N, R17E, SEC 13, B.M., CITY OF KETCHUM, IDAHO  
PREPARED FOR: SCOTT J. EDWARDS

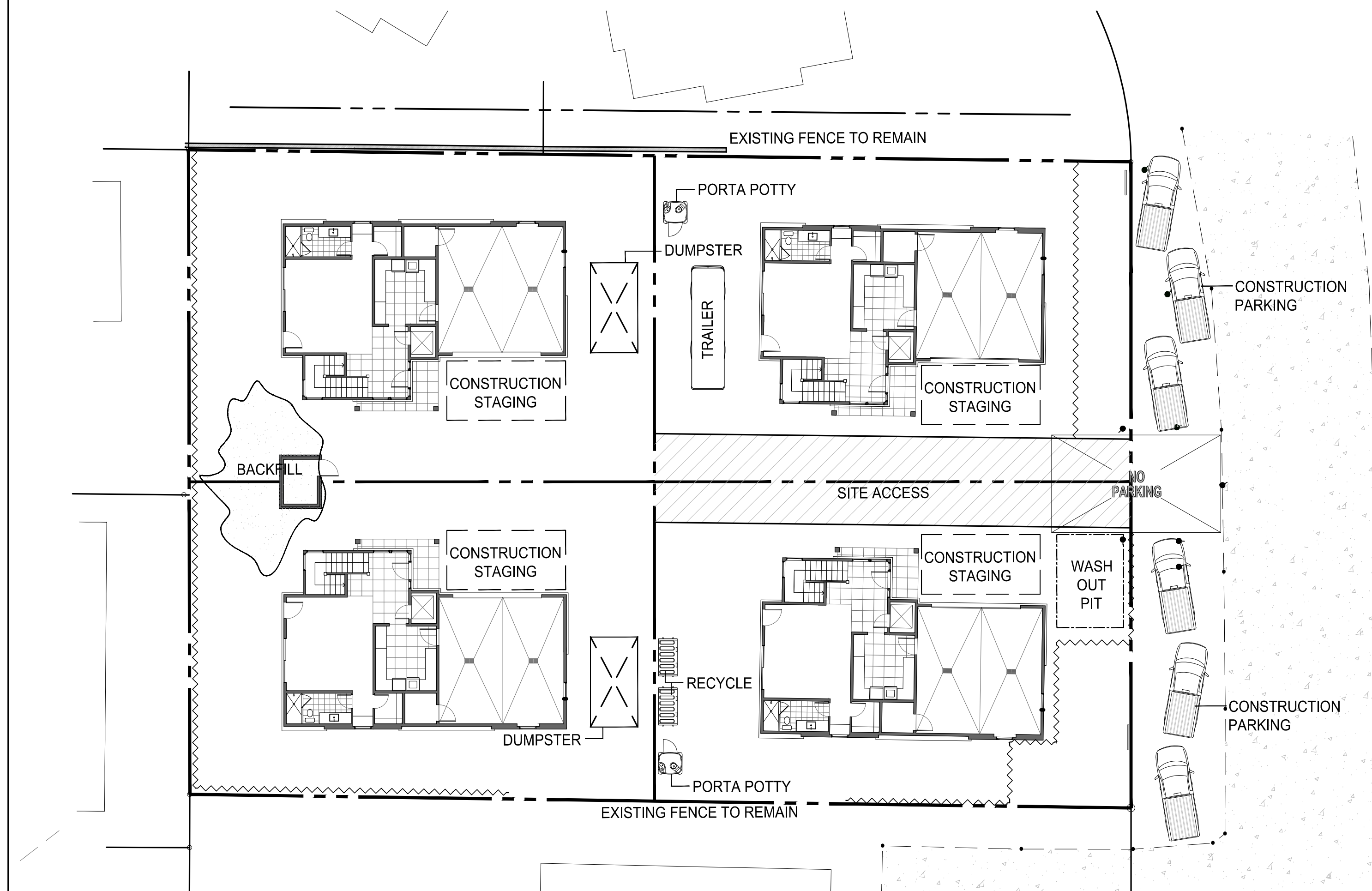
DRAWN BY: PLJ  
DESIGNED BY: PLJ  
CHECKED BY: GLM  
DATE: 6/25/2021  
PROJECT NO.: 21070

DRAWING NO.

C3

# LEGEND

-  TREE PROTECTION FENCING
-  CONSTRUCTION PARKING
-  CONSTRUCTION STAGING AREA
-  WASH OUT PIT
-  CONSTRUCTION FENCING
-  BACKFILL
-  RECYCLE BIN
-  CONSTRUCTION TRAILER



## CONSTRUCTION STAGING PLAN

SCALE: 1/16" = 1'-0"

### CONSTRUCTION STAGING NOTES

1. CONSTRUCTION ACCESS AS NOTED IN CONSTRUCTION STAGING PLAN.
2. PROPOSED ON-SITE CONSTRUCTION VEHICLE PARKING ALONG BIRD DRIVE.
3. ALL CONSTRUCTION ACTIVITIES SHALL BE WITHIN THE CONSTRUCTION LIMIT AREA SHOWN ON PLAN. OBTAIN EXPRESS WRITTEN APPROVAL FROM ARCHITECT FOR ANY WORK OUTSIDE THE LIMITS SHOWN. ALL MATERIALS, EQUIPMENT AND STOCKPILING SHALL BE WITHIN THE CONSTRUCTION LIMIT LINE. NO EXCEPTIONS.
4. STORAGE OF MATERIALS OR EQUIPMENT ON THE STREET IS STRICTLY PROHIBITED. STORE MATERIALS AND EQUIPMENT AWAY FROM TREE PROTECTION AREAS OR EROSION CONTROL MEASURES.
5. LOCATION OF PORTA-POTTY AND DUMPSTER PER PLANS.
6. PROVIDE AND INSTALL FENCING FOR TREE PROTECTION AS SHOWN ABOVE AND AS FURTHER REQUIRED BY THE TOWN OR ARCHITECT.
7. ADHERE TO KETCHUM CONSTRUCTION HOURS. FAILURE TO COMPLY WITH TOWN CONSTRUCTION HOURS MAY RESULT IN A STOP WORK NOTICE.
8. AT NO TIME SHALL THE STREET BE CLOSED OR BLOCKED. USE FLAG MEN WHEN DELIVERIES OR CONSTRUCTION RESTRICT ANY PORTION OF THE RIGHT OF WAY.
9. LIMIT ON-STREET VEHICLE PARKING TO ONE SIDE OF THE STREET ONLY. VEHICLES MAY NOT PARK ON BOTH SIDES OF THE STREET.
10. ALL PROTECTIVE FENCING SHALL BE MAINTAINED IN PLACE THROUGHOUT THE DURATION OF CONSTRUCTION.
11. NO DEMOLITION OR CONSTRUCTION MAY COMMENCE UNTIL ALL PROTECTIVE FENCING HAS BEEN INSTALLED.
12. MAINTAIN A CLEAN SITE AND CLEAN & CLEAR RIGHT-OF-WAY. CLEAN ANY DEBRIS OR DIRT FROM THE STREET IMMEDIATELY. MAINTAIN THE SITE IN AN ORDERLY FASHION PER THE APPROVAL OF THE ARCHITECT.
13. ESTABLISH TEMPORARY POWER FOR CONSTRUCTION FROM THE EXISTING SERVICE. LOCATE THE TEMPORARY PANEL IN A CONVENIENT LOCATION AS ACCEPTABLE TO THE ARCHITECT.

### POLLUTION PREVENTION NOTES

1. PROTECT DOWN SLOPE DRAINAGE COURSE, STREAMS AND STORM DRAINS WITH ROCK BAGS, HAY BALES, TEMPORARY DRAINAGE SWALES, SILT FENCES, BERMS OR STORM DRAIN INLET FILTERS.
2. STRAW ROLLS SHALL BE INSTALLED PRIOR TO THE INCEPTION OF ANY WORK ON-SITE, AND SHALL REMAIN IN PLACE UNTIL THE LANDSCAPING GROUND COVER IS INSTALLED.
3. DRY SWEEPING METHODS SHALL BE USED TO REMOVE ANY DEBRIS AND/OR SOIL TRACKED ONTO BIRD DRIVE. DRY SWEEPING SHALL BE DONE AT THE END OF EACH WORK DAY AS NECESSARY.
4. THE CONTRACTOR SHALL FOLLOW AND USE BEST MANAGEMENT PRACTICES (BMP) FOR DISCHARGE INTO THE TOWN'S STORM WATER SYSTEM DURING SITE STRIPPING, HAULING, EARTH MOVING ACTIVITIES, HEAVY EQUIPMENT OPERATIONS AND GENERAL CONSTRUCTION. THOROUGHLY SUPERVISE THE WORK AT ALL TIMES TO INSURE NO PAINTING MATERIALS, SOLVENTS, ADHESIVES, LANDSCAPING MATERIALS OR OTHER SUCH MATERIALS WASH AWAY FROM THE SITE OR OTHERWISE ENTER THE FLOW OF STORM WATER RUNOFF.
5. STOCKPILED MATERIAL SHALL BE COVERED WITH POLY-EHTALENE SHEETING OR A TARPULIN UNTIL THE MATERIAL IS REMOVED FROM THE SITE. ANY REMAINING BARE SOIL THAT EXISTS AFTER THE STOCKPILE HAS BEEN REMOVED SHALL BE COVERED UNTIL A NATURAL GROUND COVER IS ESTABLISHED, OR IT MAY BE SEEDED OR PLANTED TO PROVIDE GROUND COVER PRIOR TO THE FALL RAINY SEASON.
6. DUST CONTROL SHALL BE DONE BY WATERING.
7. IF EROSION DEVELOPS IN A TEMPORARY EROSION PROTECTED AREA OR ANY ESTABLISHED VEGETATED AREA, THE CONTRACTOR SHALL IMMEDIATELY ALLEVIATE AND REMEDY THE PROBLEM AND TAKE PREVENTATIVE MEASURES TO MINIMIZE THE POSSIBILITY OF ITS RECURRENCE, AND ALSO TO PREVENT THE RESULTING FLOW OF SOILS OR WATER WITH SUSPENDED SOILS FROM GETTING INTO THE TOWN'S DRAINAGE SYSTEM OR ANY NATURAL DRAINAGE CHANNEL OR DITCH.
8. ALL DISTURBED SOILS SHALL BE "MATTED" AND SEEDED WITHIN TWO (2) WEEKS OF "FINAL DISTURBANCE".
9. STOCKPILE SHALL BE COVERED, PROTECTED W/ SANDBAGS & PROVIDED WITH ADDITIONAL PROTECTION AT THE BASE UNTIL AFTER APRIL 15. NO FILLING OR SPREADING OF MATERIAL CAN OCCUR UNTIL AFTER APRIL 15.

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LICENSED ARCHITECT  
 AR 1710  
 11-17-2021  
 THOMAS R. WILLIAMS  
 STATE OF IDAHO

**SNOWBIRD TOWNHOMES**  
**LOT 2A**  
 220 BIRD DRIVE  
 LOTS 1 & 2, SNOWBIRD SUBDIVISION  
 KETCHUM, IDAHO 83340

2021 17  
 Date: 03/15/2022  
 Drawn By: RJS  
 Checked By: TW

Revisions

**CM1.0**  
 Construction Management Plan

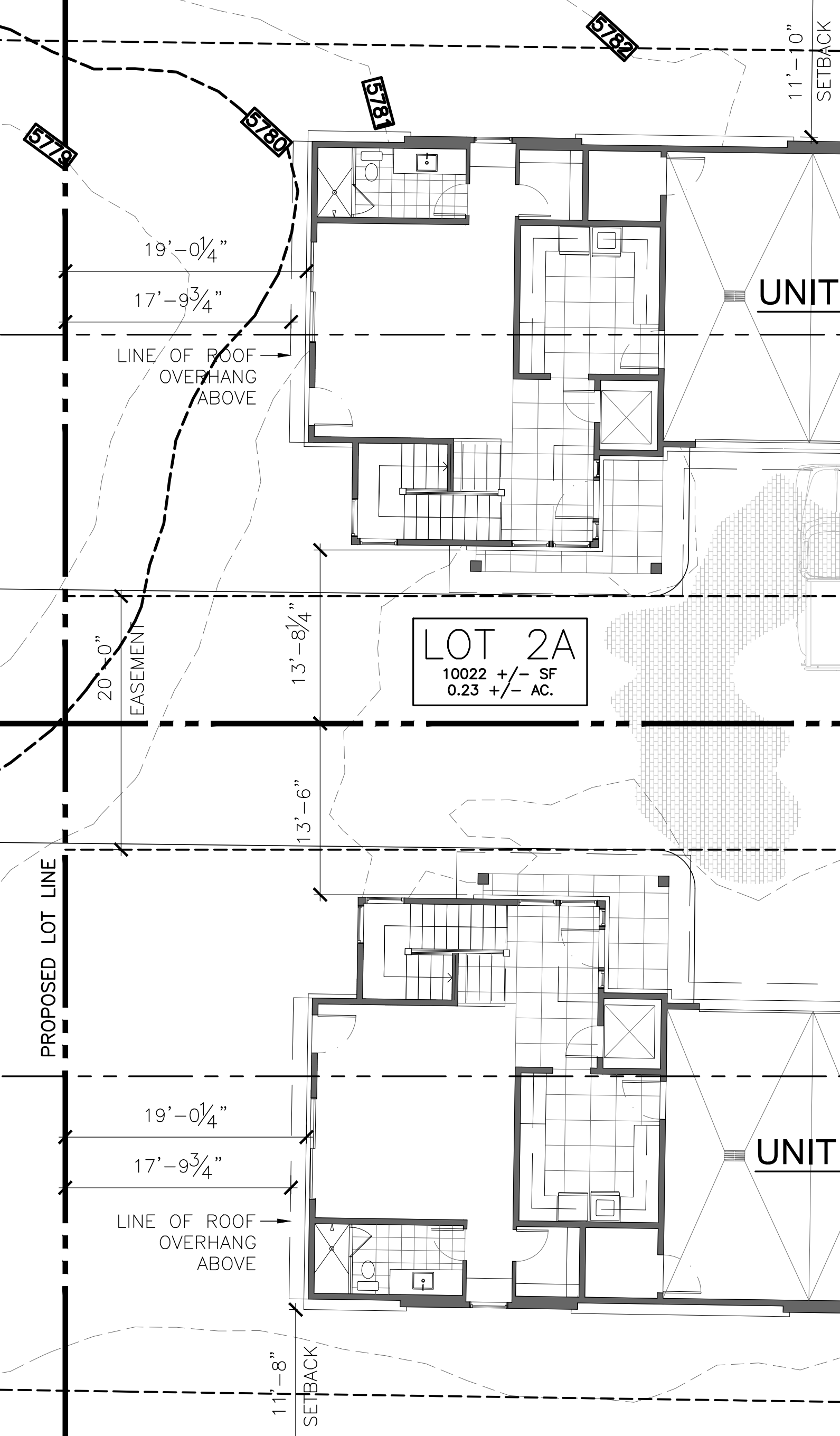
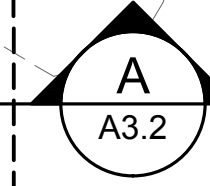
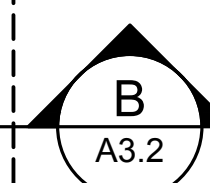
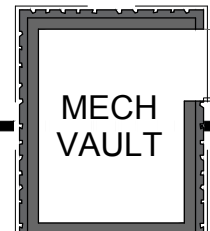
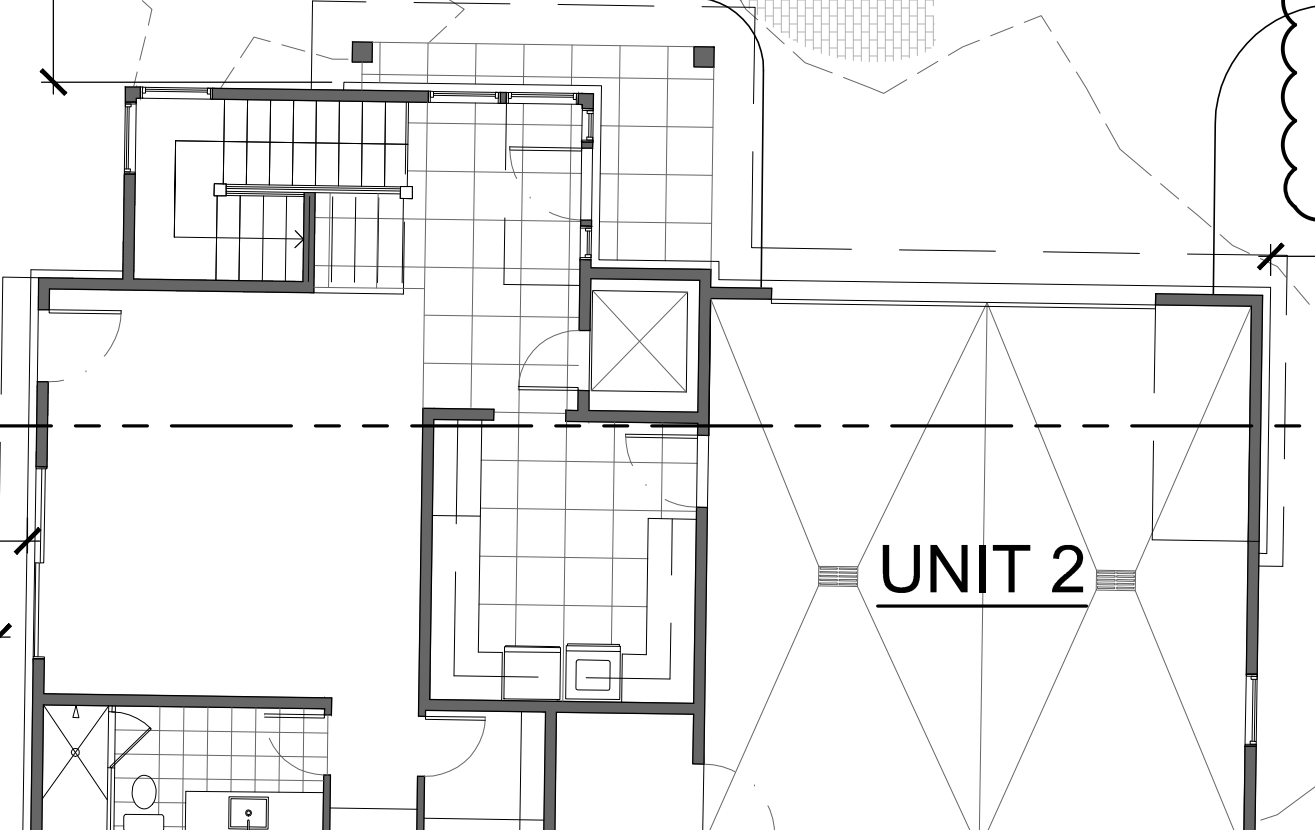
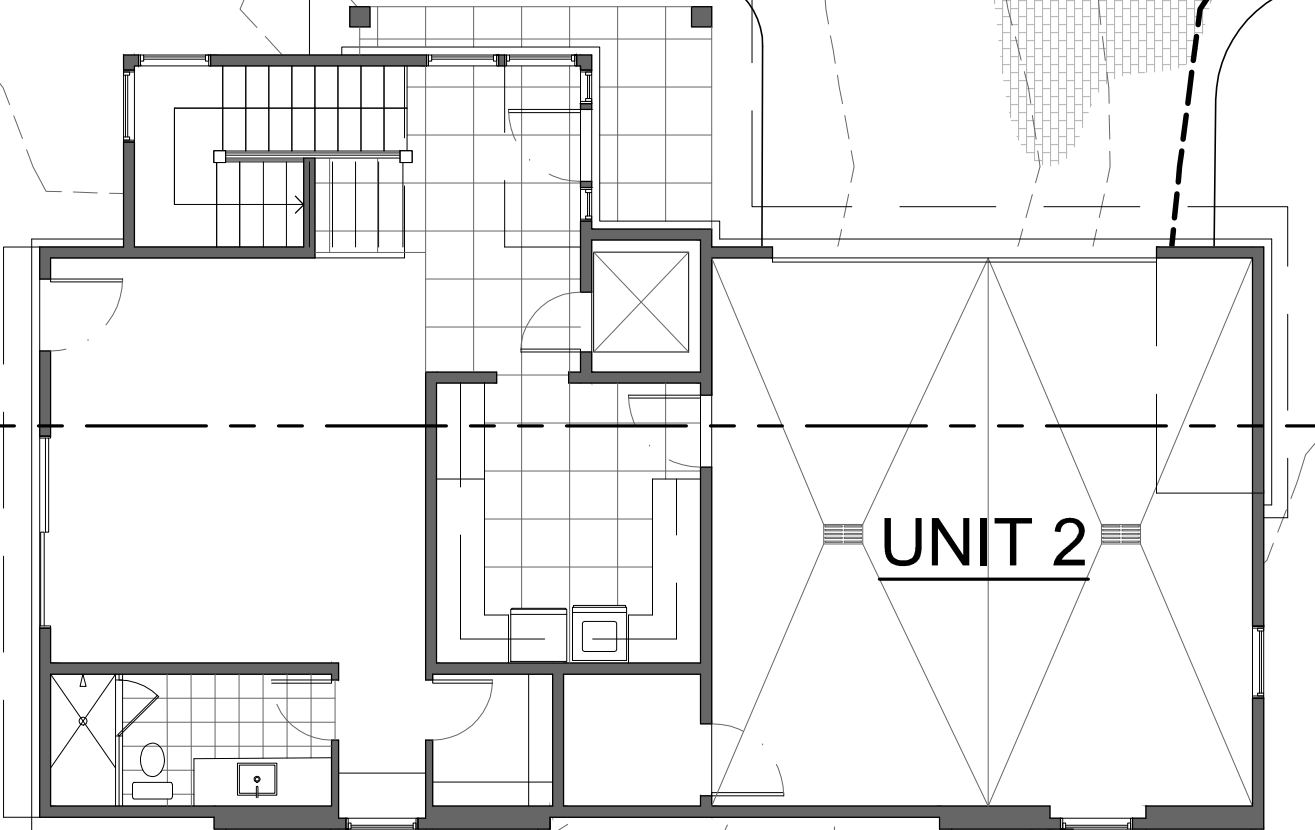
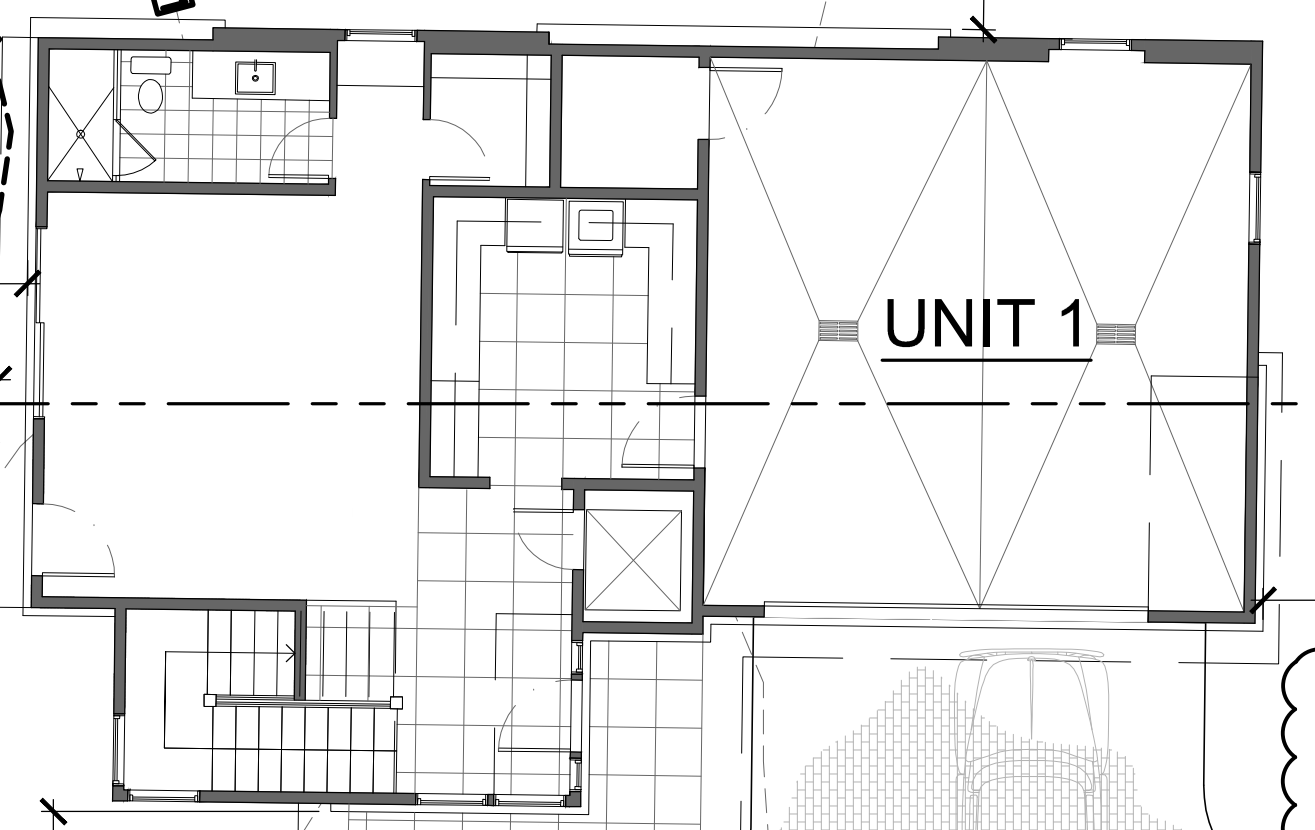
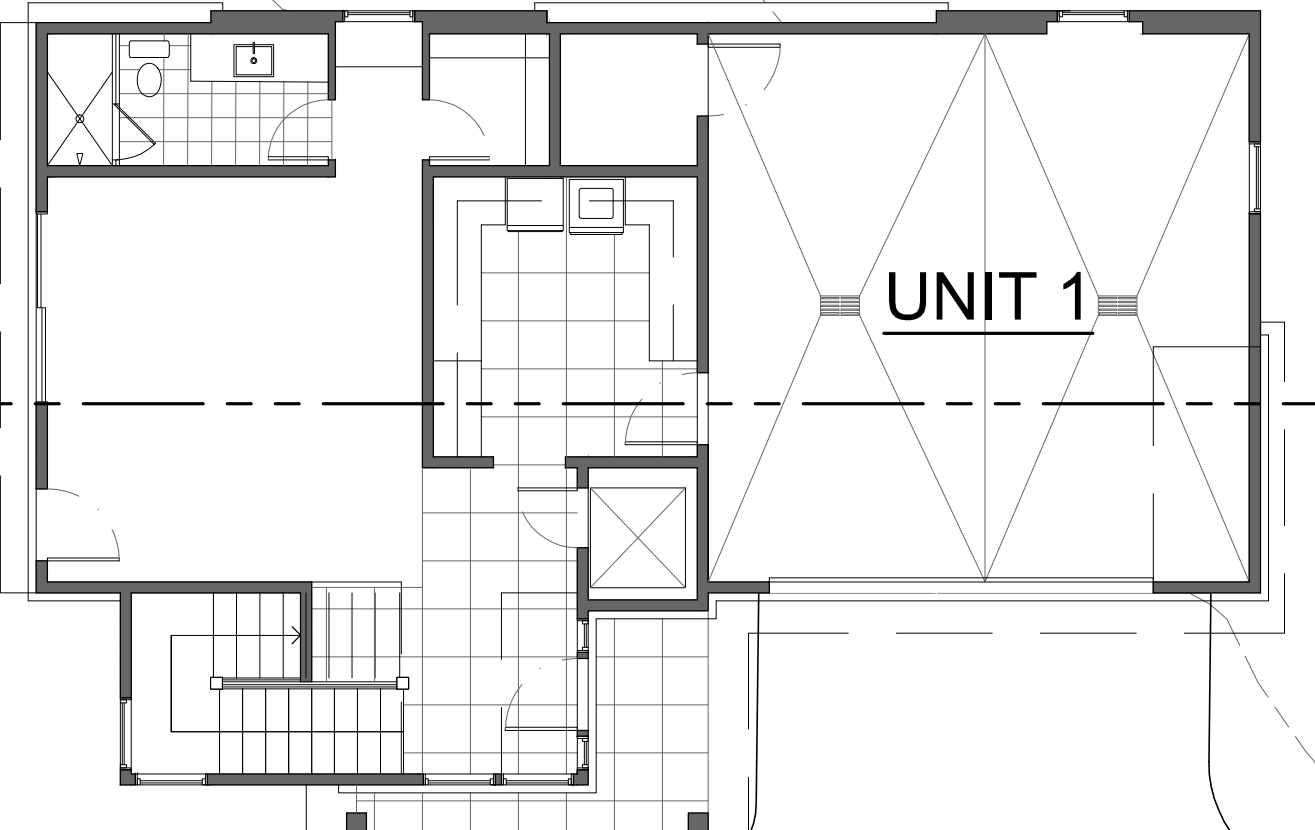
FRENCHMANS TOWNHOMES  
SUBLOT 7

FRENCHMANS TOWNHOMES  
SUBLOT 8

LOT 1A TOWNHOUSES SHOWN  
FOR REFERENCE ONLY

LOT 1A  
9779 +/- SF  
0.22 +/- AC.

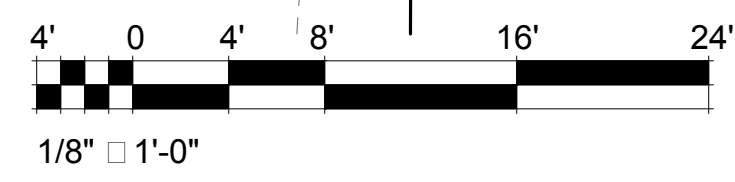
LOT 2A  
10022 +/- SF  
0.23 +/- AC.



BIRD DRIVE

TAX LOT 4426

SILVER BIRCH CONDOS



1/8" = 1'-0"

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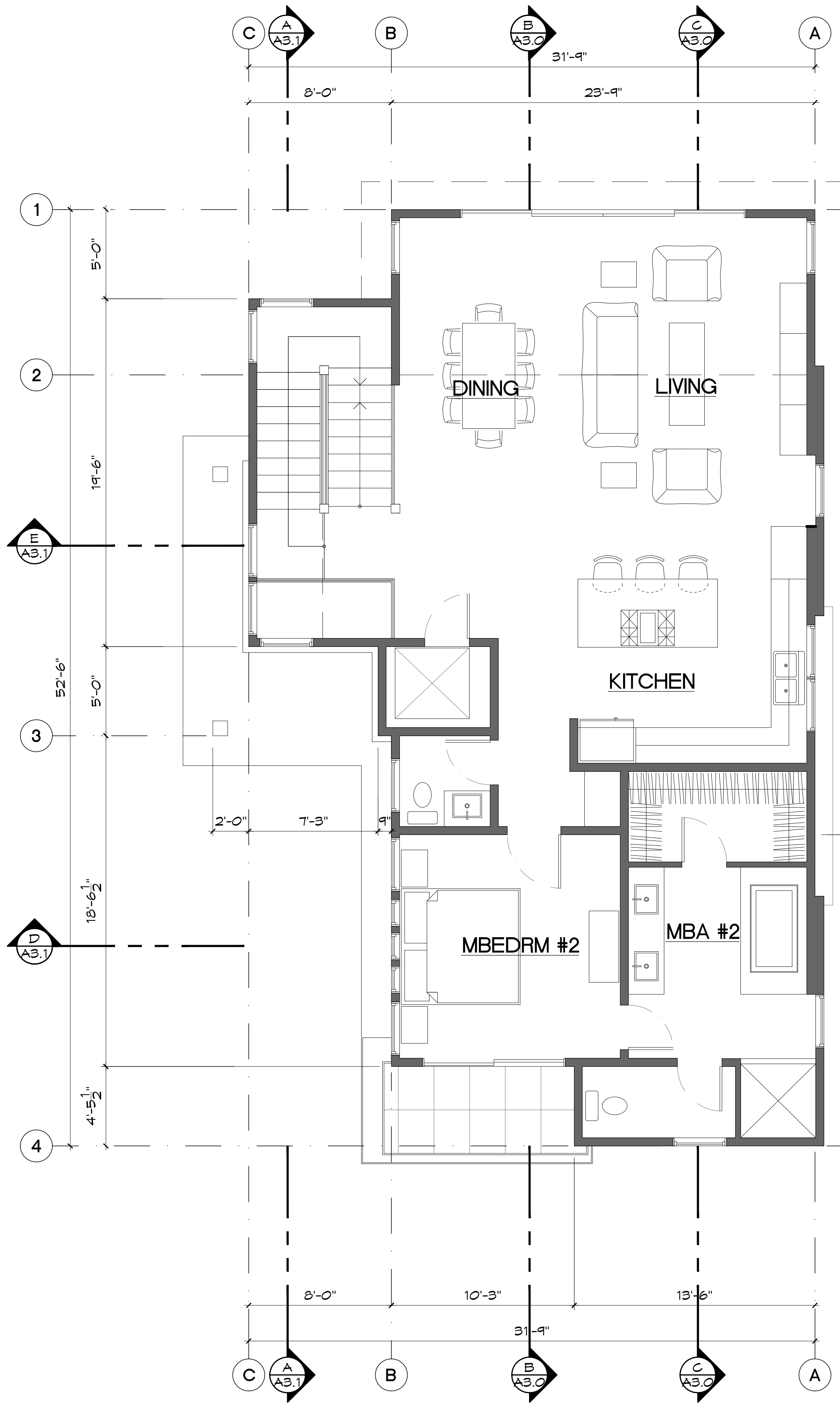
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Date: 03/15/2022
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Revisions

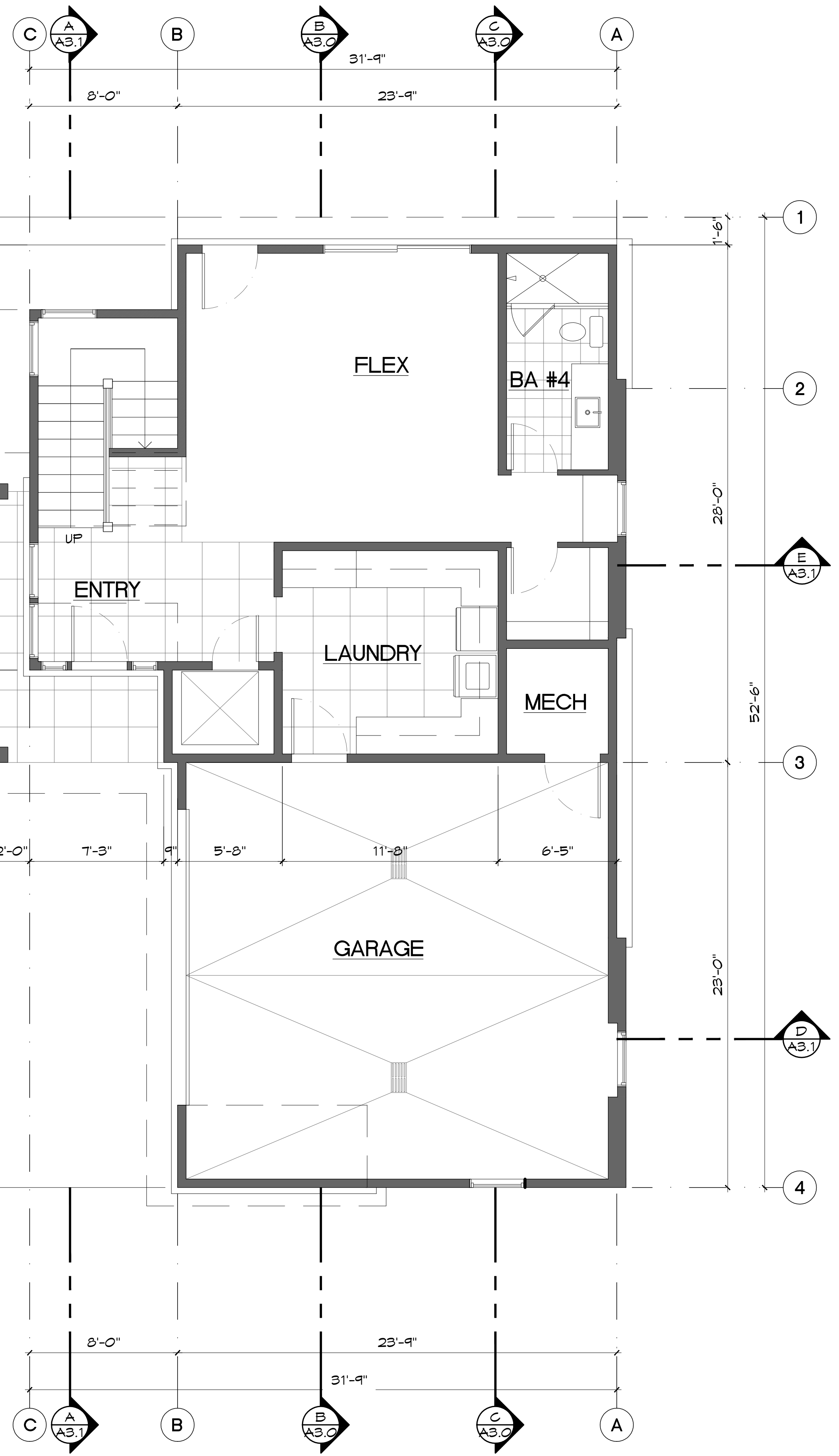
**A1.0**  
 Architectural Site Plan

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**MAIN/SECOND FLOOR PLAN**

SCALE 1/4" = 1'-0"



**GROUND/FIRST FLOOR PLAN**

SCALE 1/4" = 1'-0"

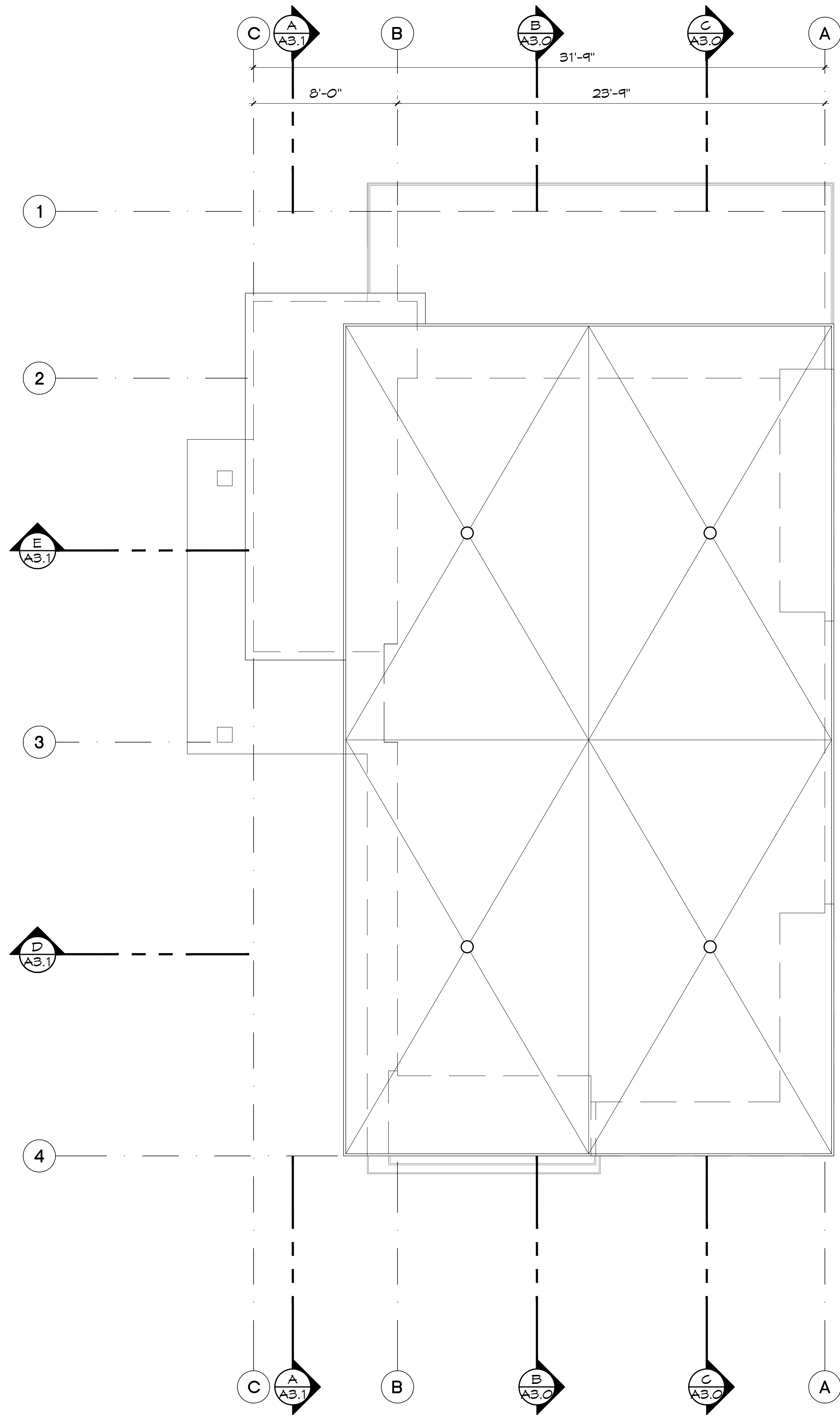
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 Boise, Idaho 83706  
 trw@trwarchitecture.com  
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 www.trwarchitecture.com

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 ARCHITECT AR 1710  
 11-17-2021  
 THOMAS R. WILLIAMS  
 STATE OF IDAHO

**SNOWBIRD TOWNHOMES**  
**LOT 2A**  
 220 BIRD DRIVE  
 LOTS 1 & 2, SNOWBIRD SUBDIVISION  
 KETCHUM, IDAHO 83340

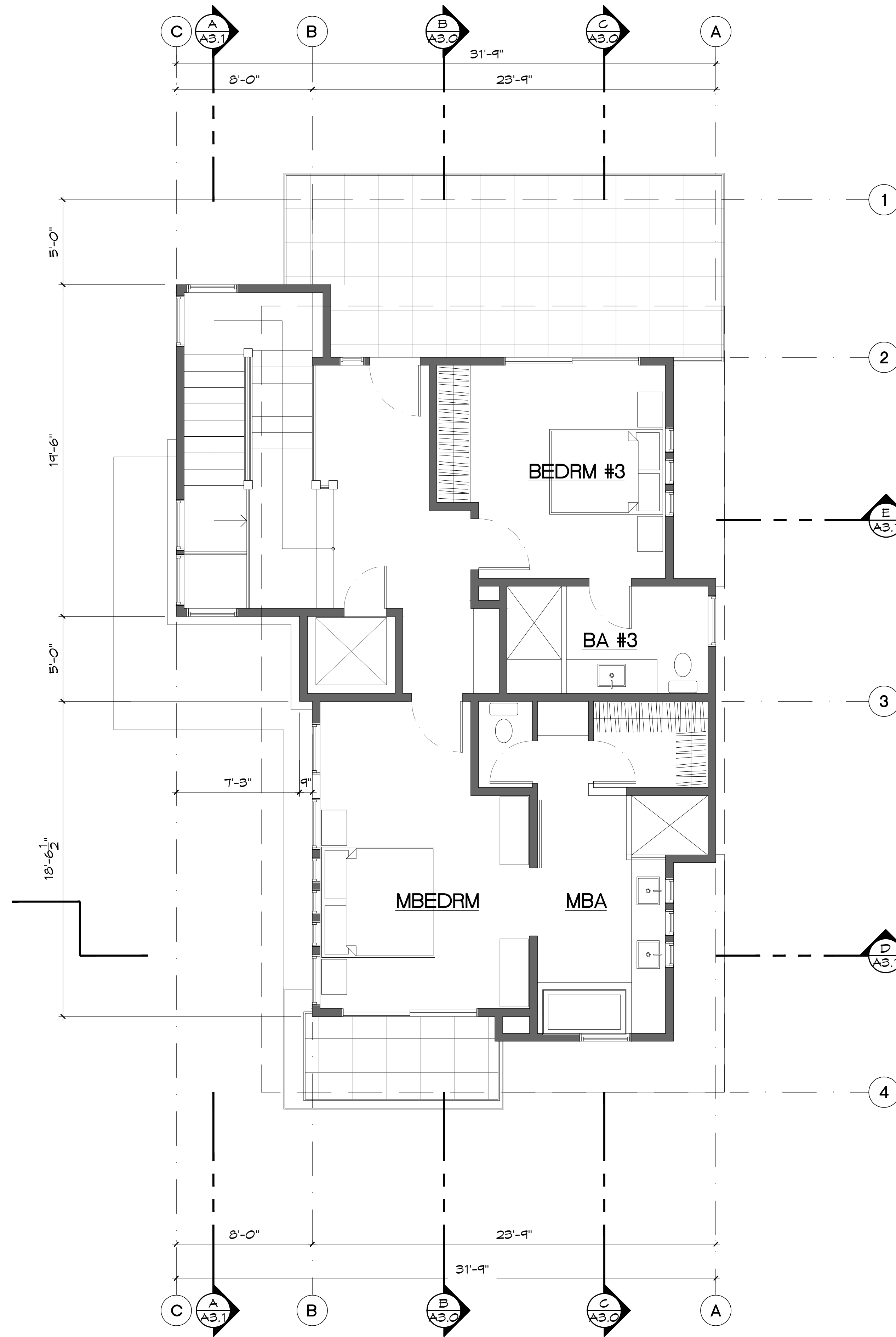
2021 17
Date: 03/15/2022
Drawn By: RJS
Checked By: TW
Revisions

**A2.0**  
 Floor Plans



**ROOF PLAN**

SCALE  $\frac{1}{4}'' = 1'-0''$

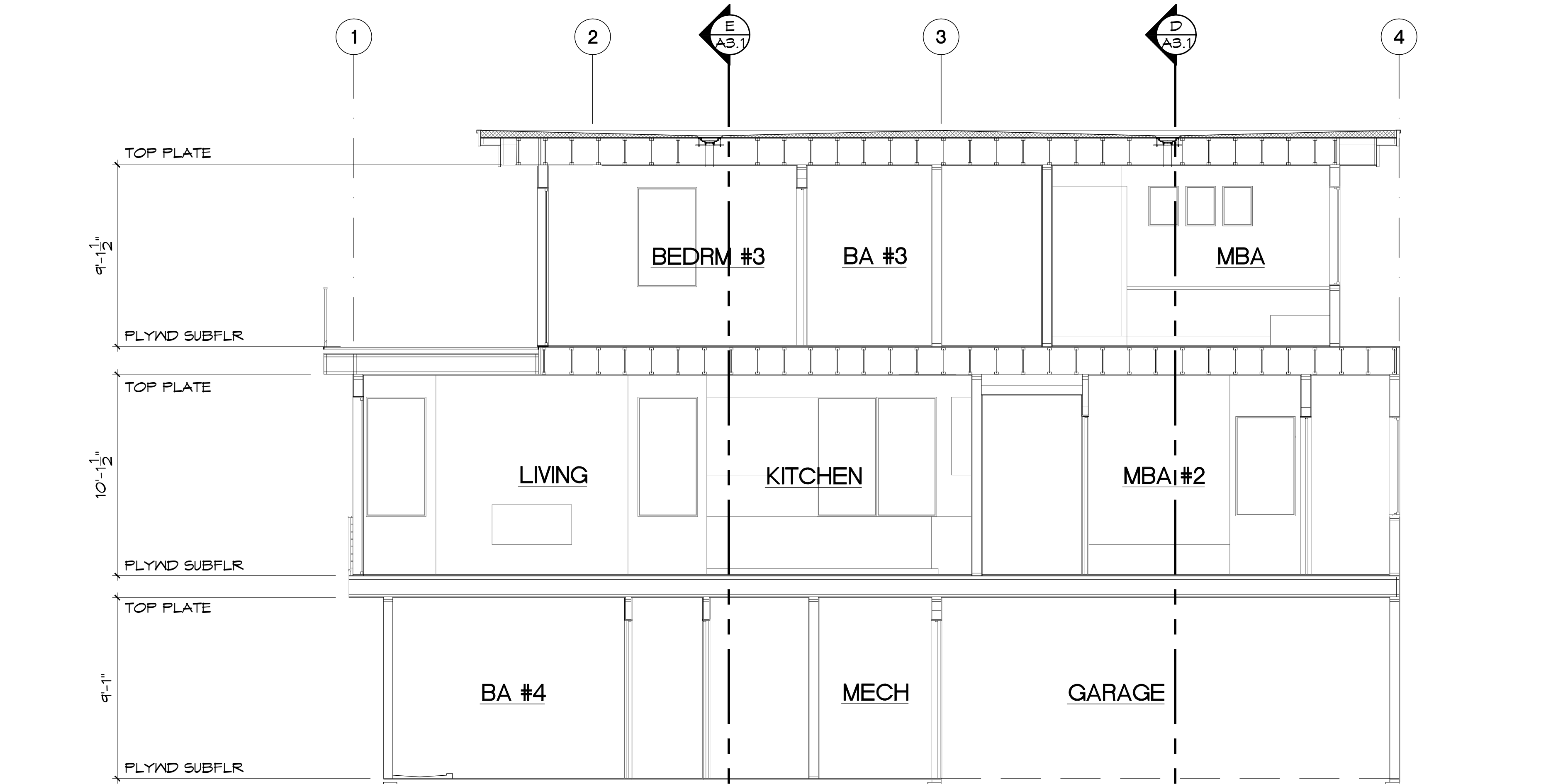


**UPPER/THIRD FLOOR PLAN**

SCALE  $\frac{1}{4}'' = 1'-0''$

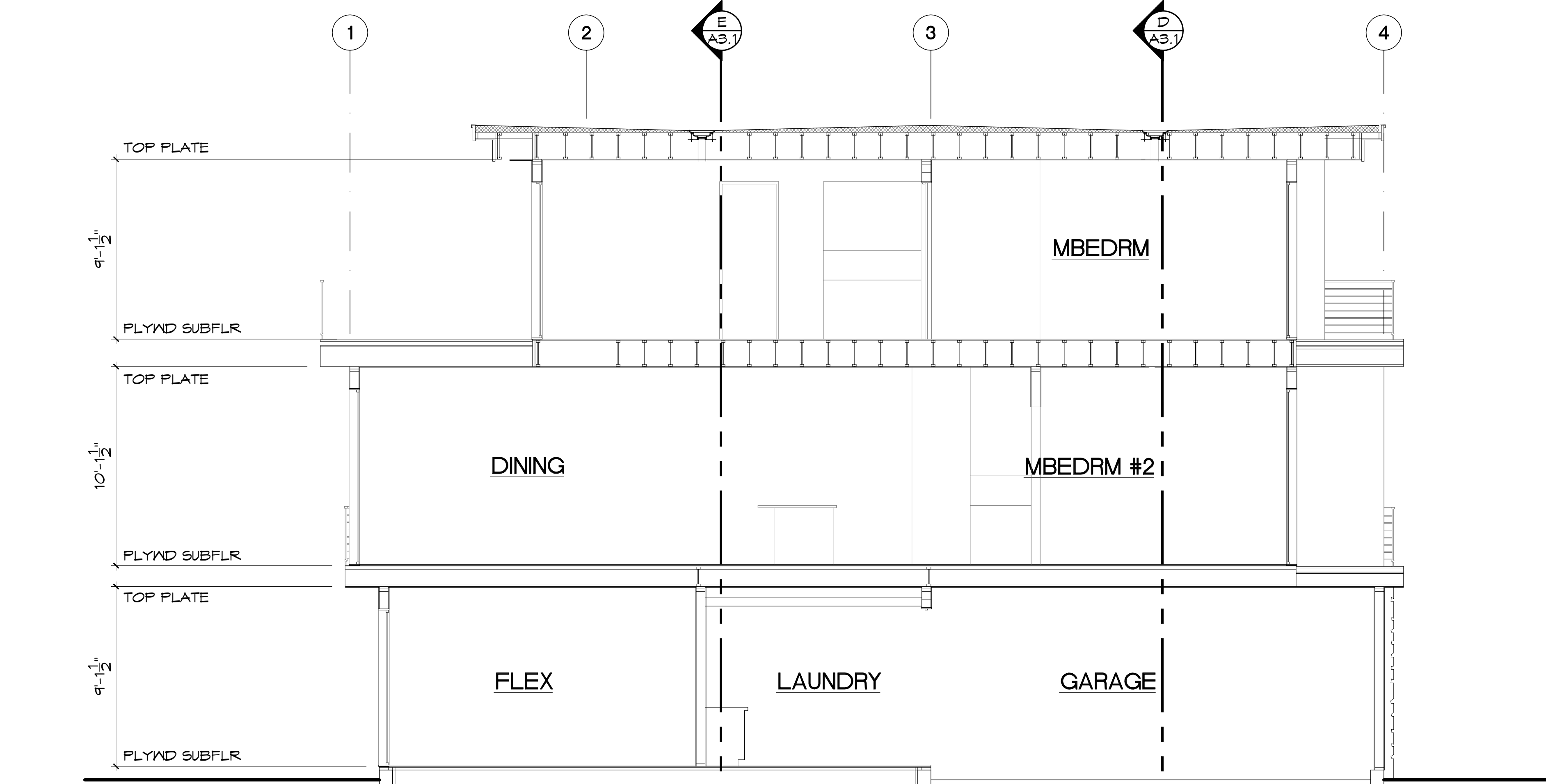
2021 17
Date: 03/15/2022
Drawn By: RJS
Checked By: TW
Revisions





**C**  
**A3.0**

**SECTION**



**B**  
**A3.0**

**SECTION**

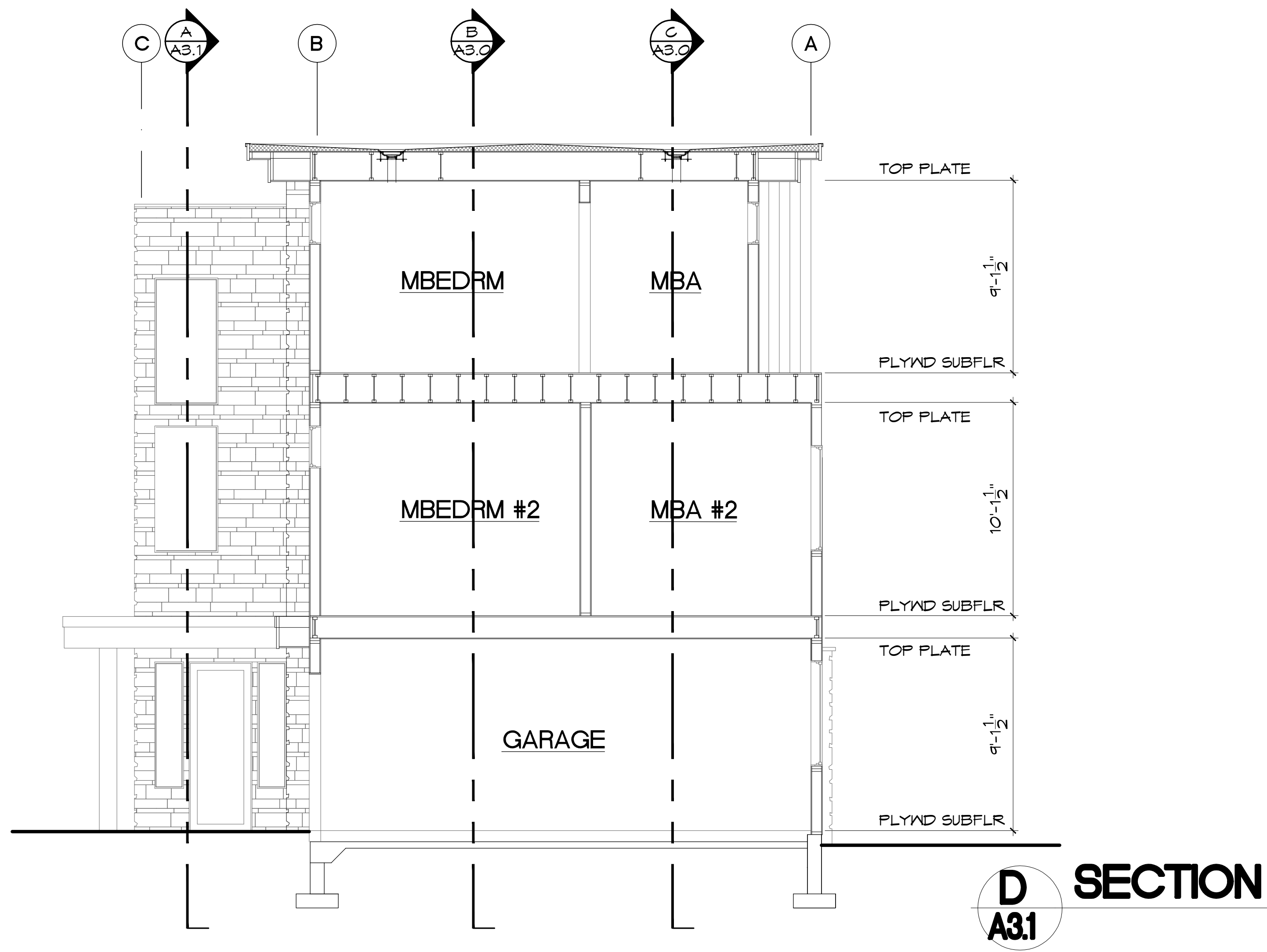
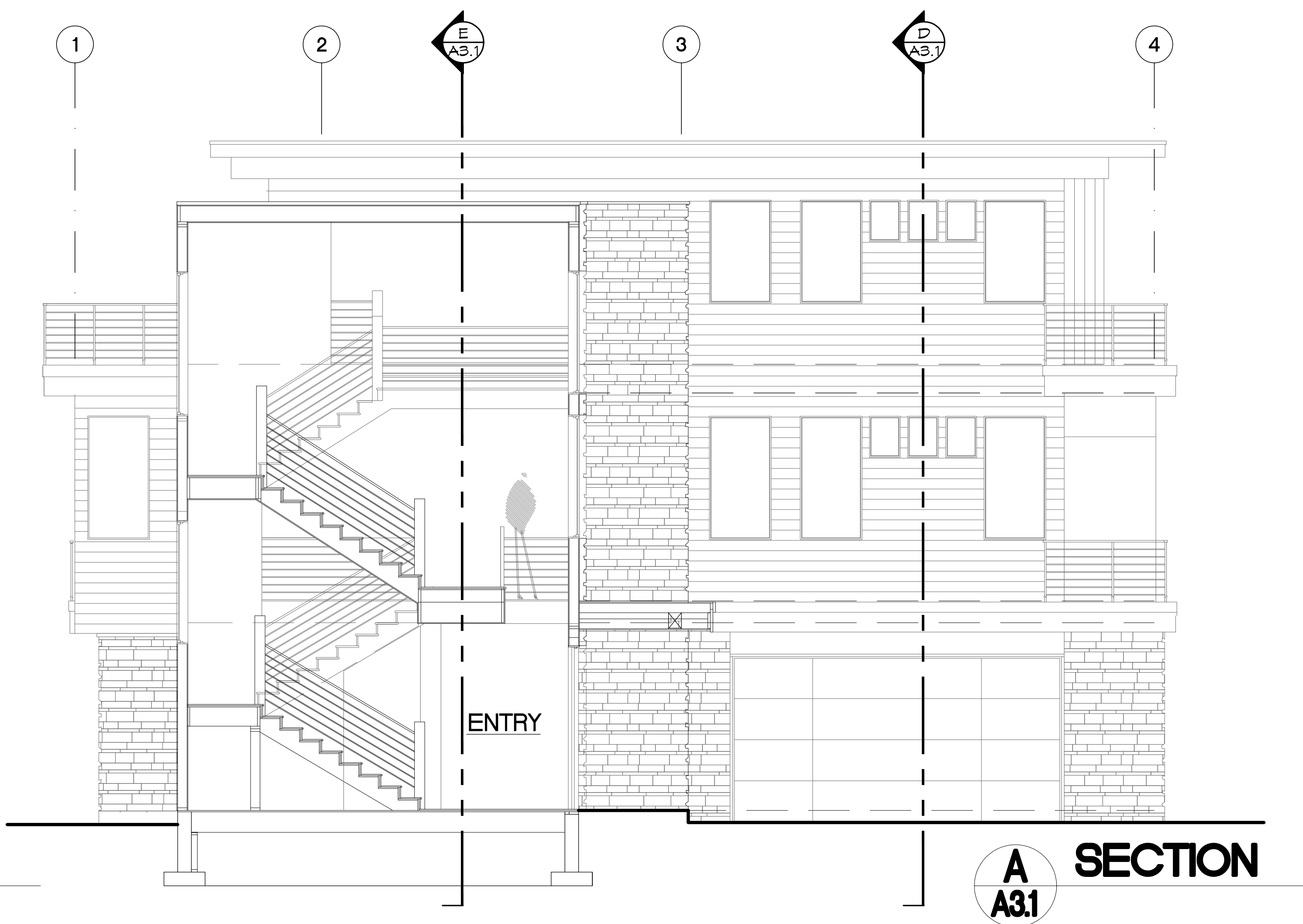
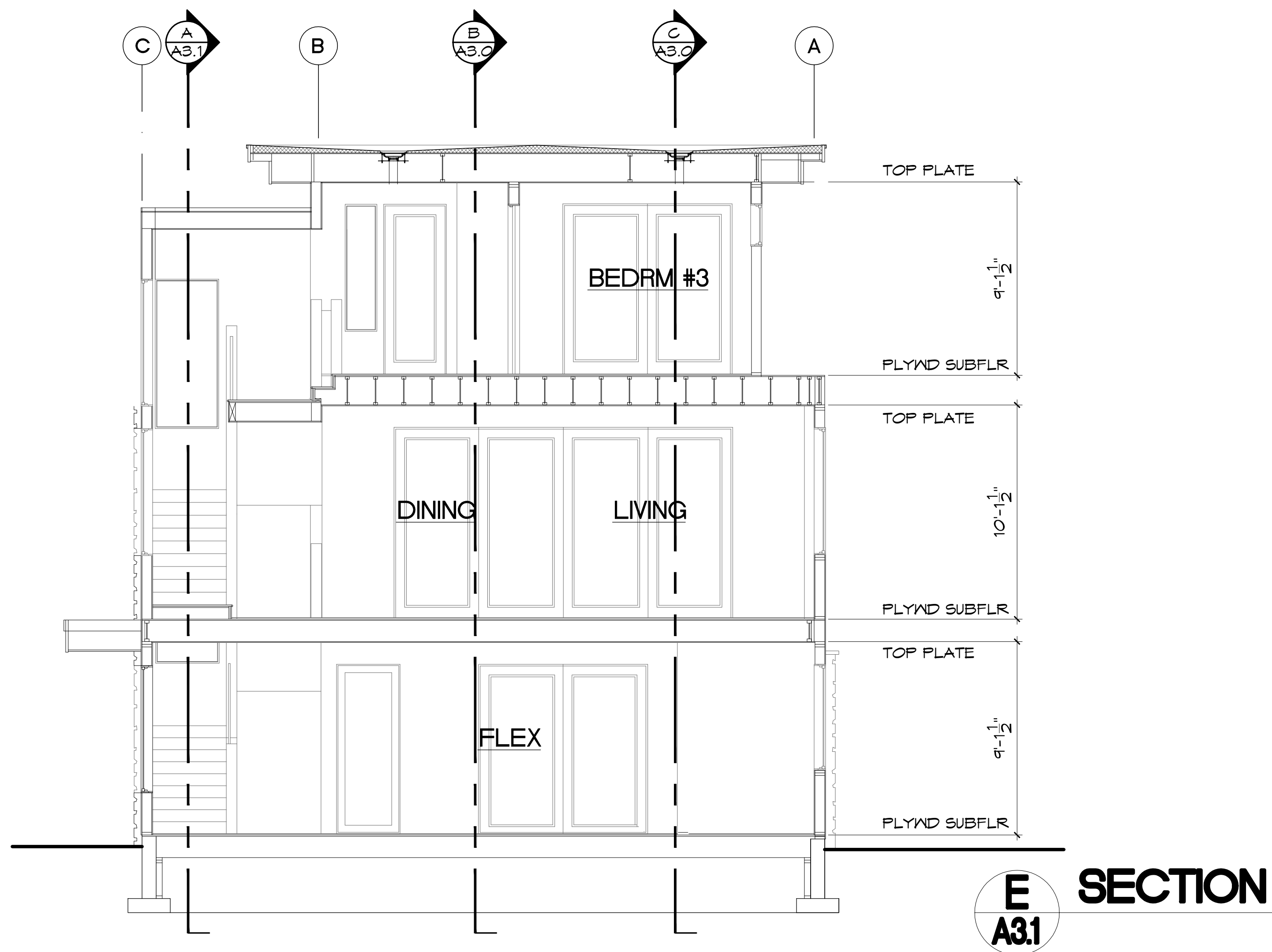
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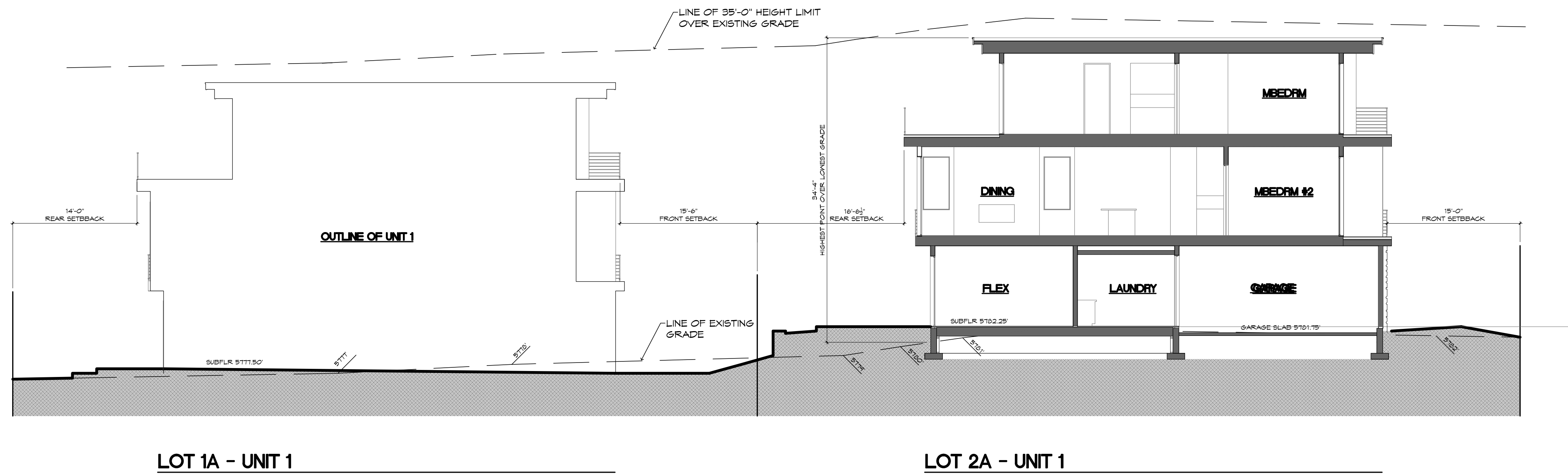
**SNOWBIRD TOWNHOMES**  
**LOT 2A**  
 220 BIRD DRIVE  
 LOTS 1 & 2, SNOWBIRD SUBDIVISION  
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Date:	2021 17
Drawn By:	03/15/2022
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**A3.0**  
 Building Sections

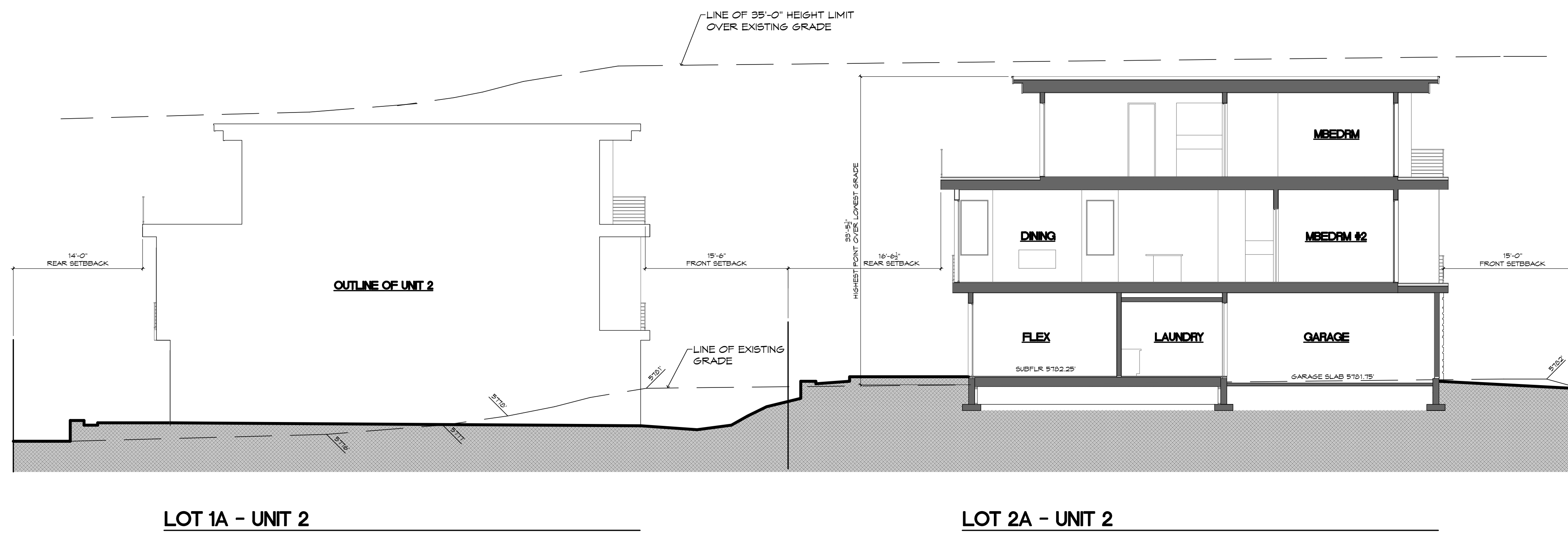


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Revisions	
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**B** SITE SECTION  
A32

SCALE: 1/4" = 1'-0"



**A** SITE SECTION  
A32

SCALE: 1/4" = 1'-0"

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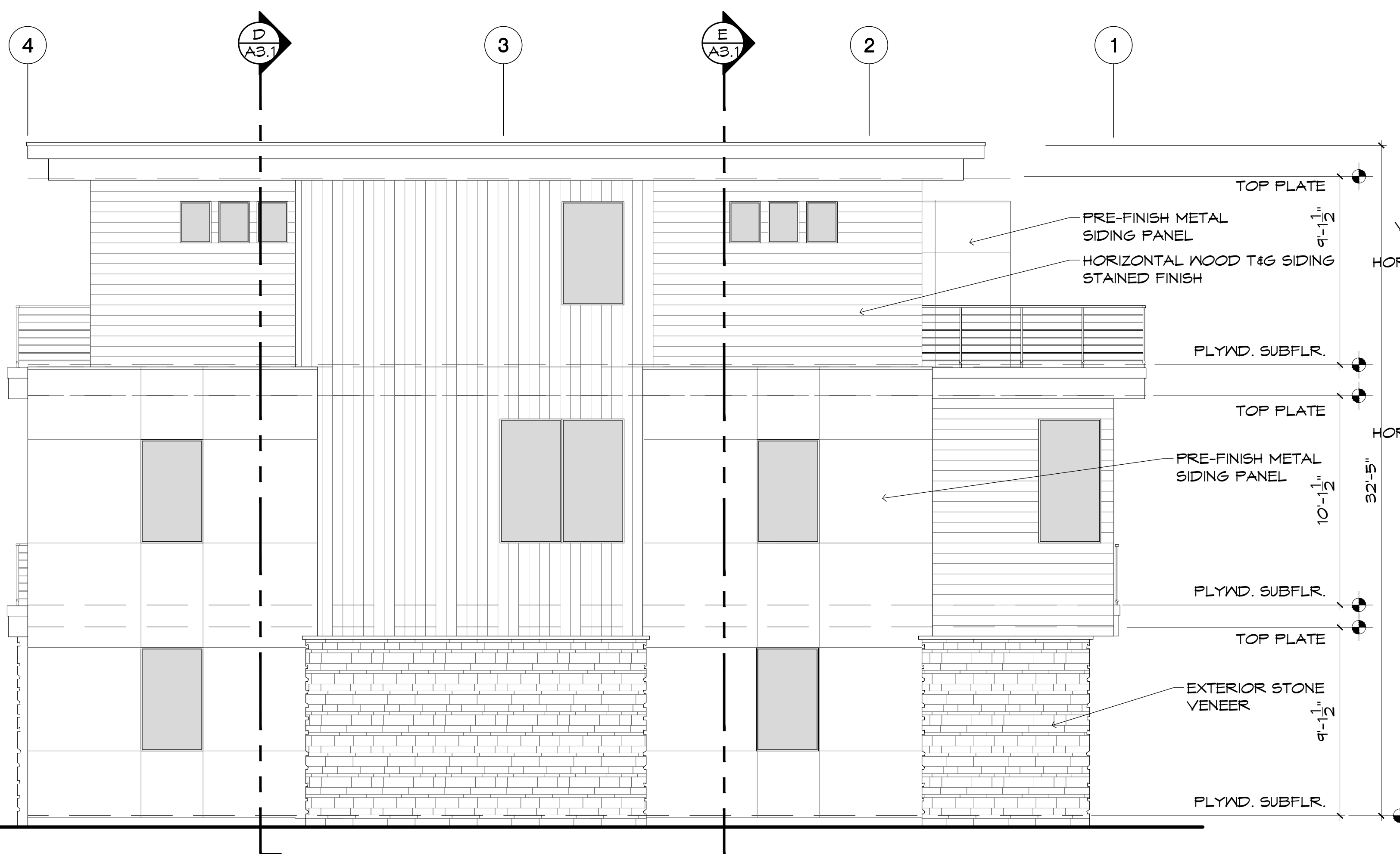
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**SNOWBIRD TOWNHOMES**  
**LOT 2A**  
 220 BIRD DRIVE  
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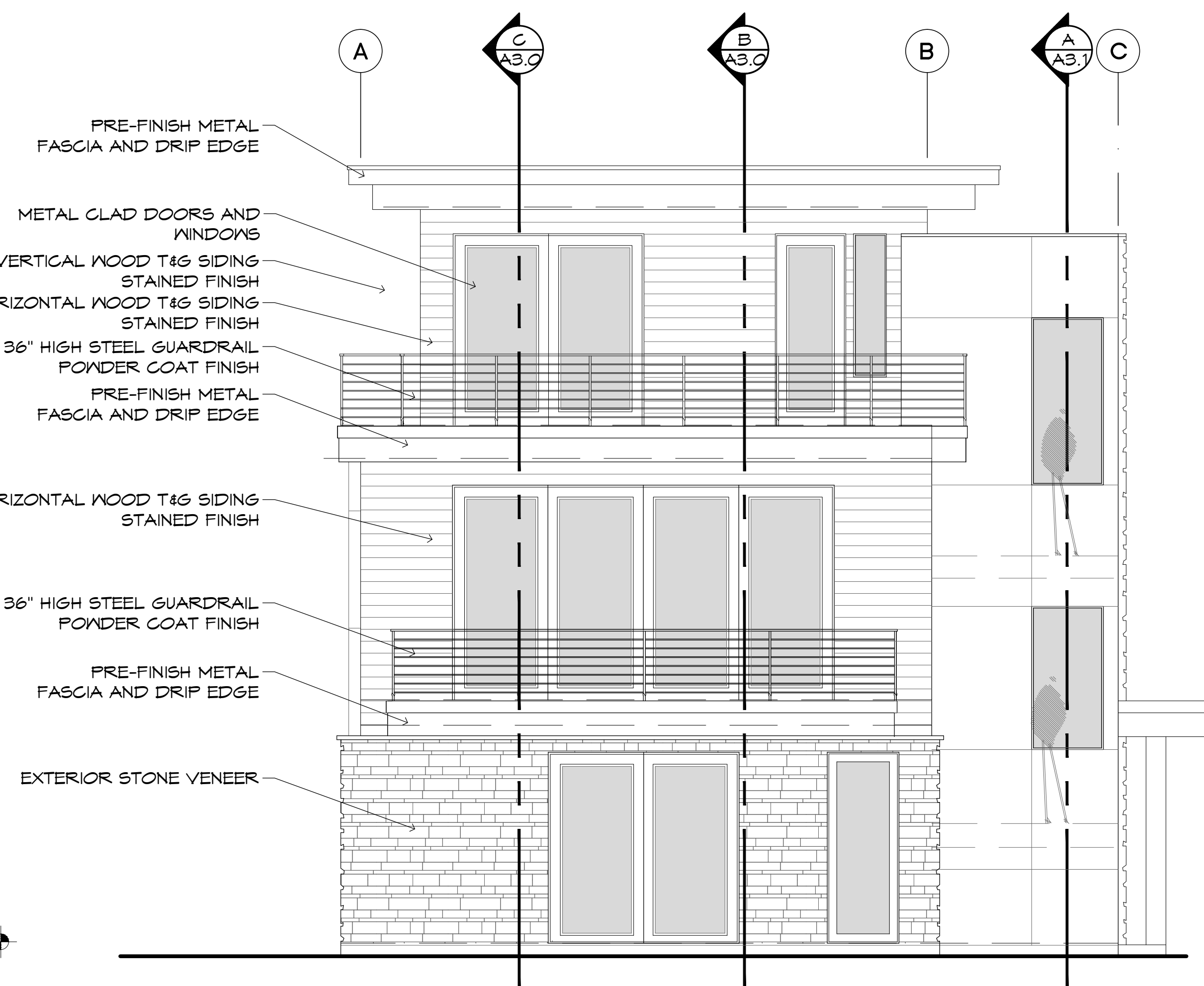
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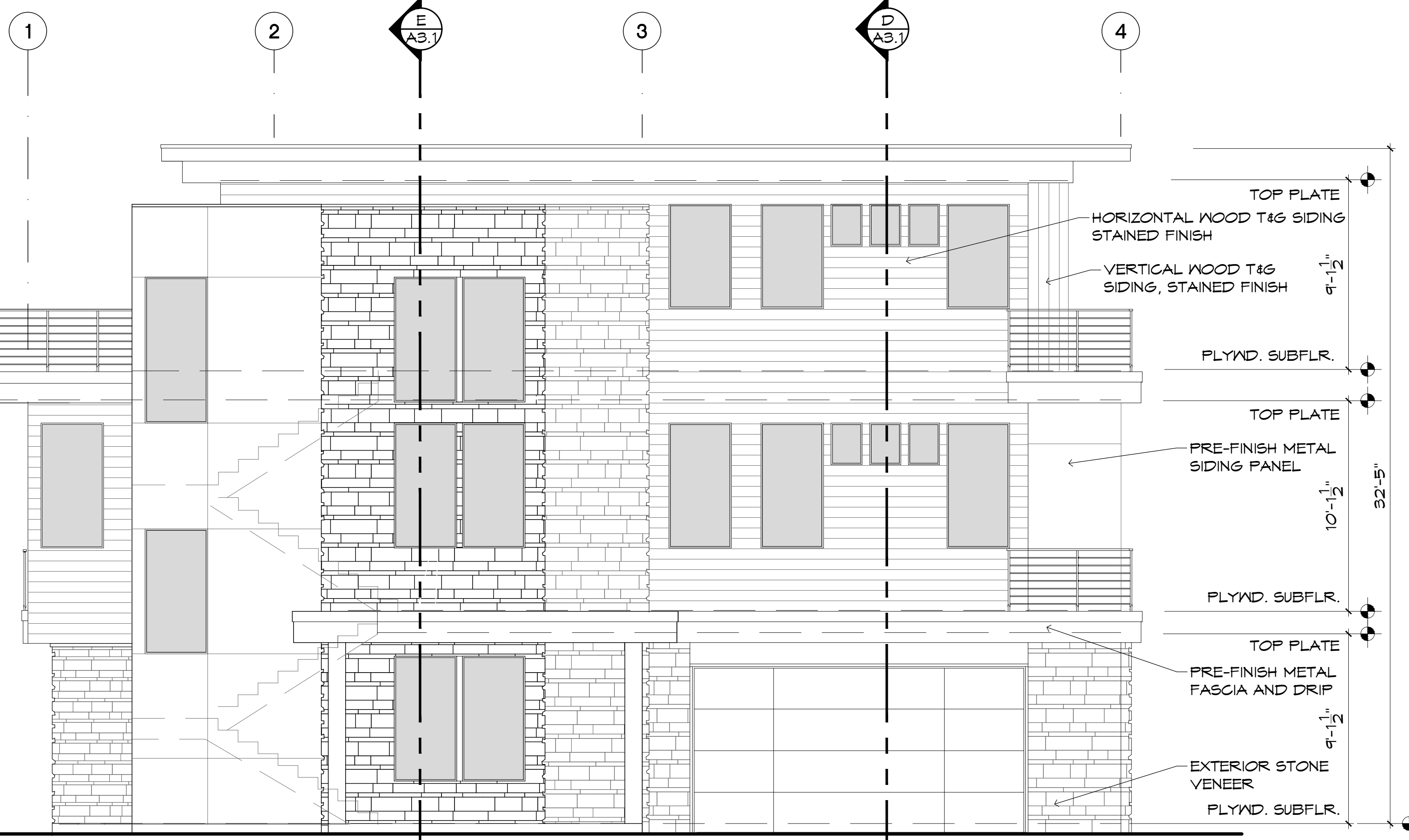
**A3.2**  
 Composite Building Sections



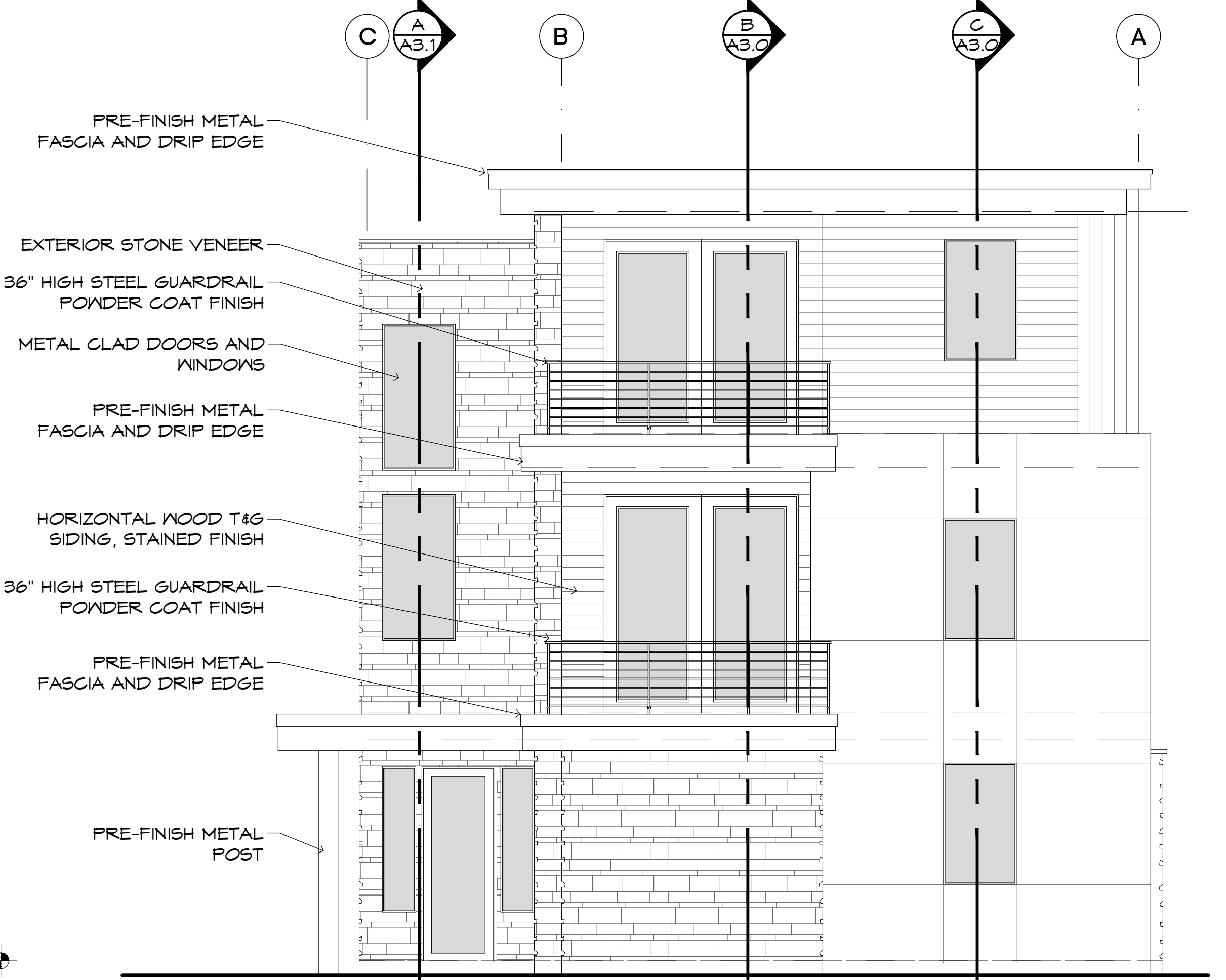
**RIGHT SIDE ELEVATION**



**REAR ELEVATION**



**LEFT SIDE ELEVATION  
LOTS 1A + 2A, UNITS 1**



**FRONT ELEVATION**

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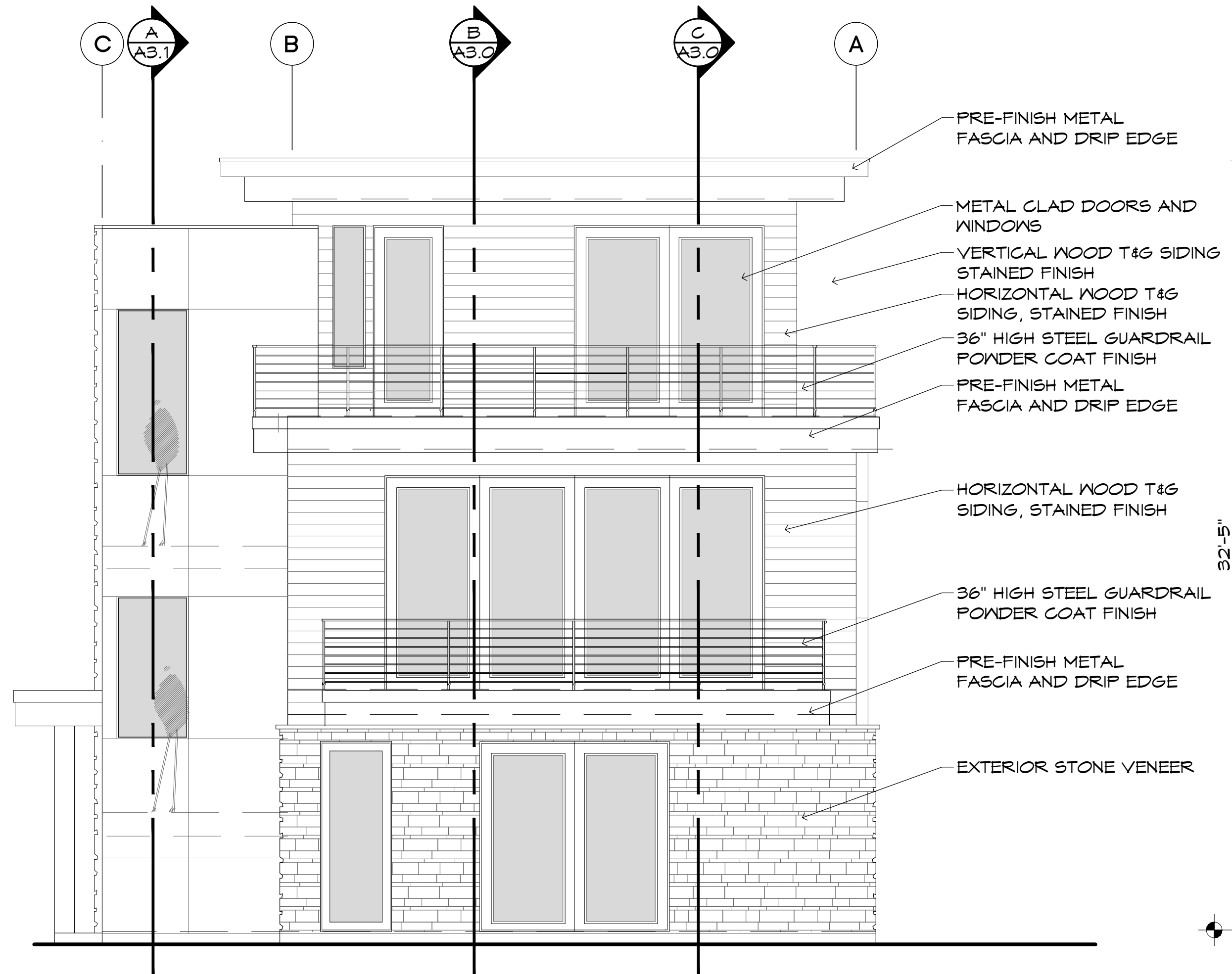
LICENSED ARCHITECT  
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**SNOWBIRD TOWNHOMES**  
**LOT 2A**  
 220 BIRD DRIVE  
 LOTS 1 & 2, SNOWBIRD SUBDIVISION  
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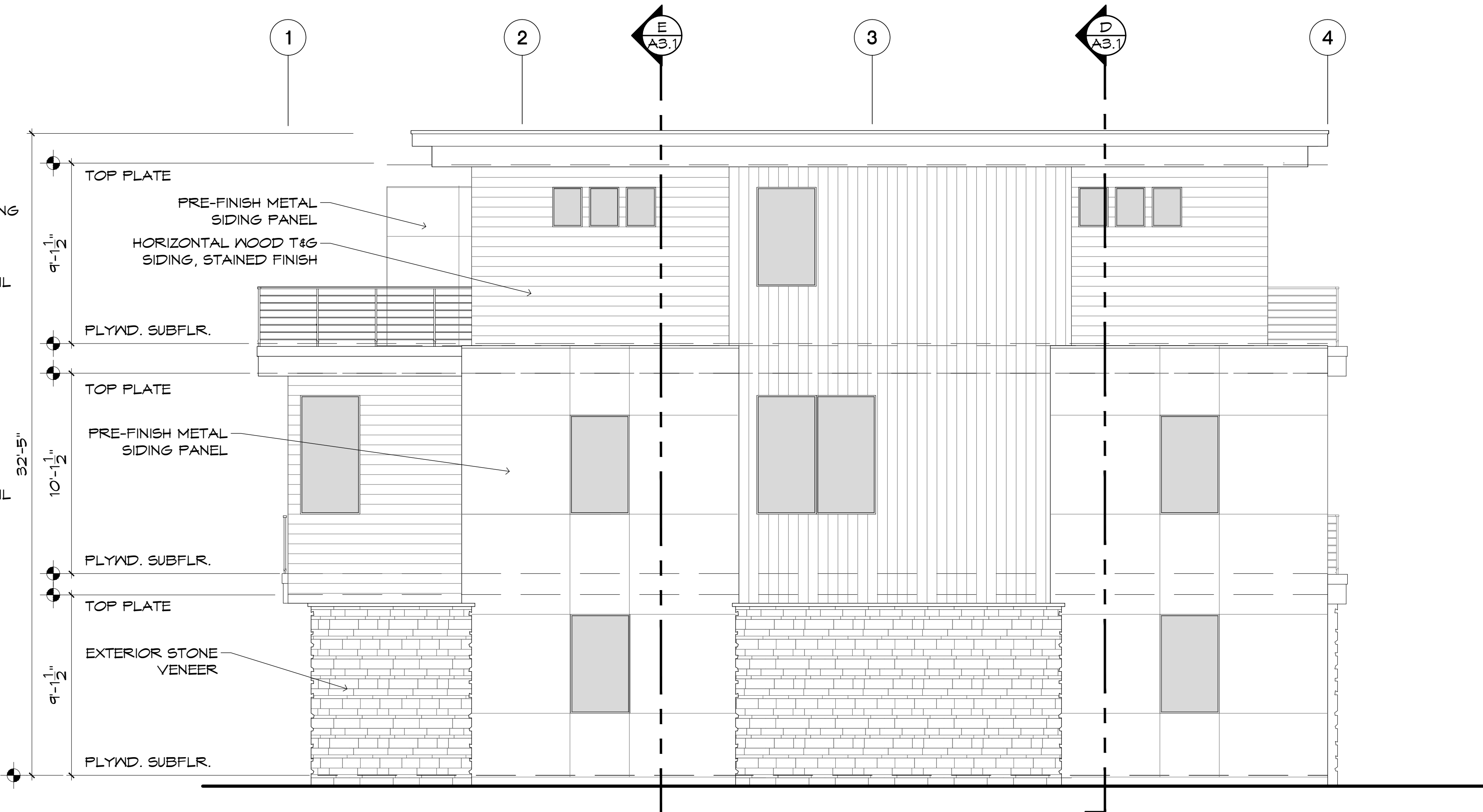
2021 17
Date: 03/15/2022
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Checked By: TW
Revisions

**A4.0**  
 Exterior Elevations

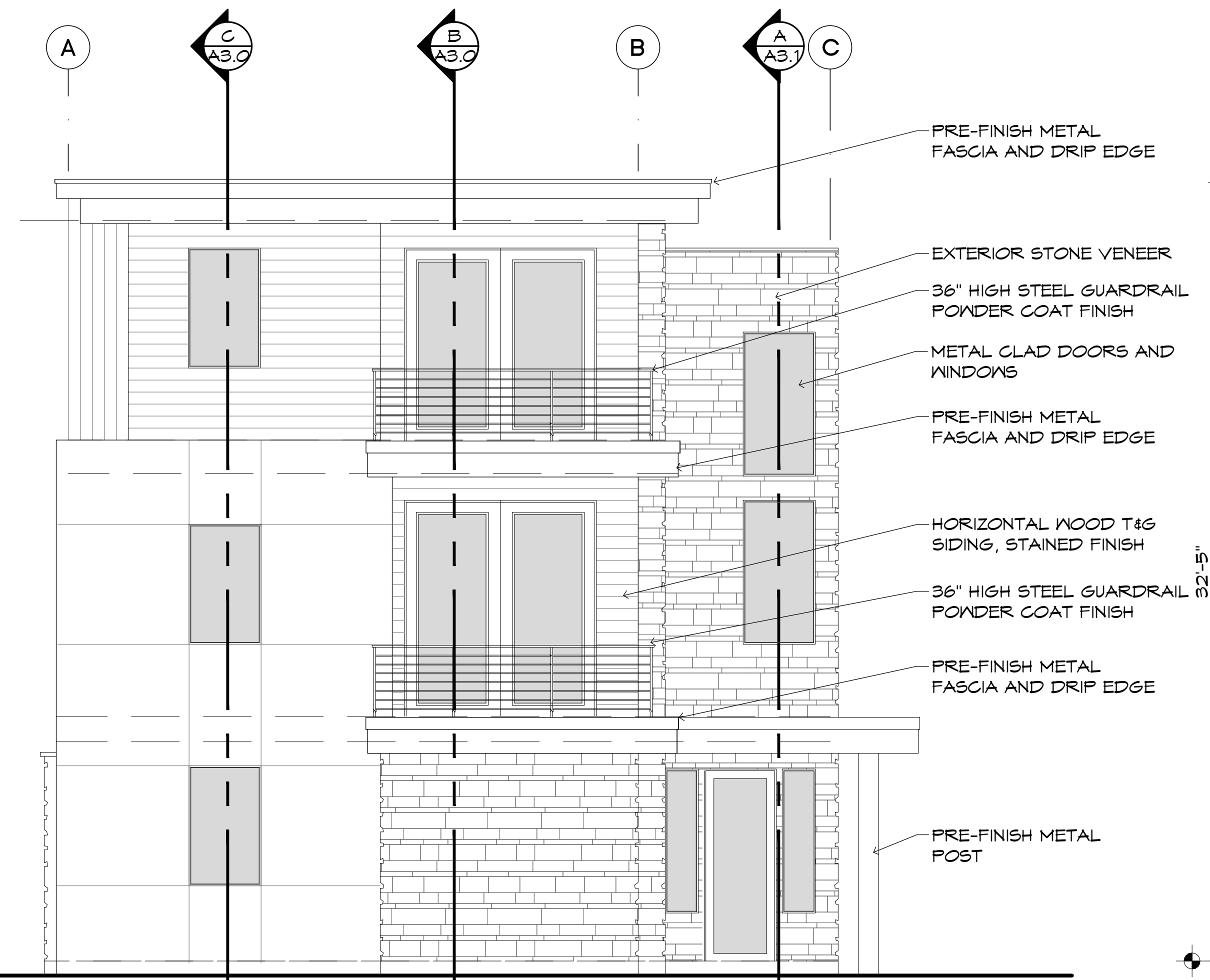
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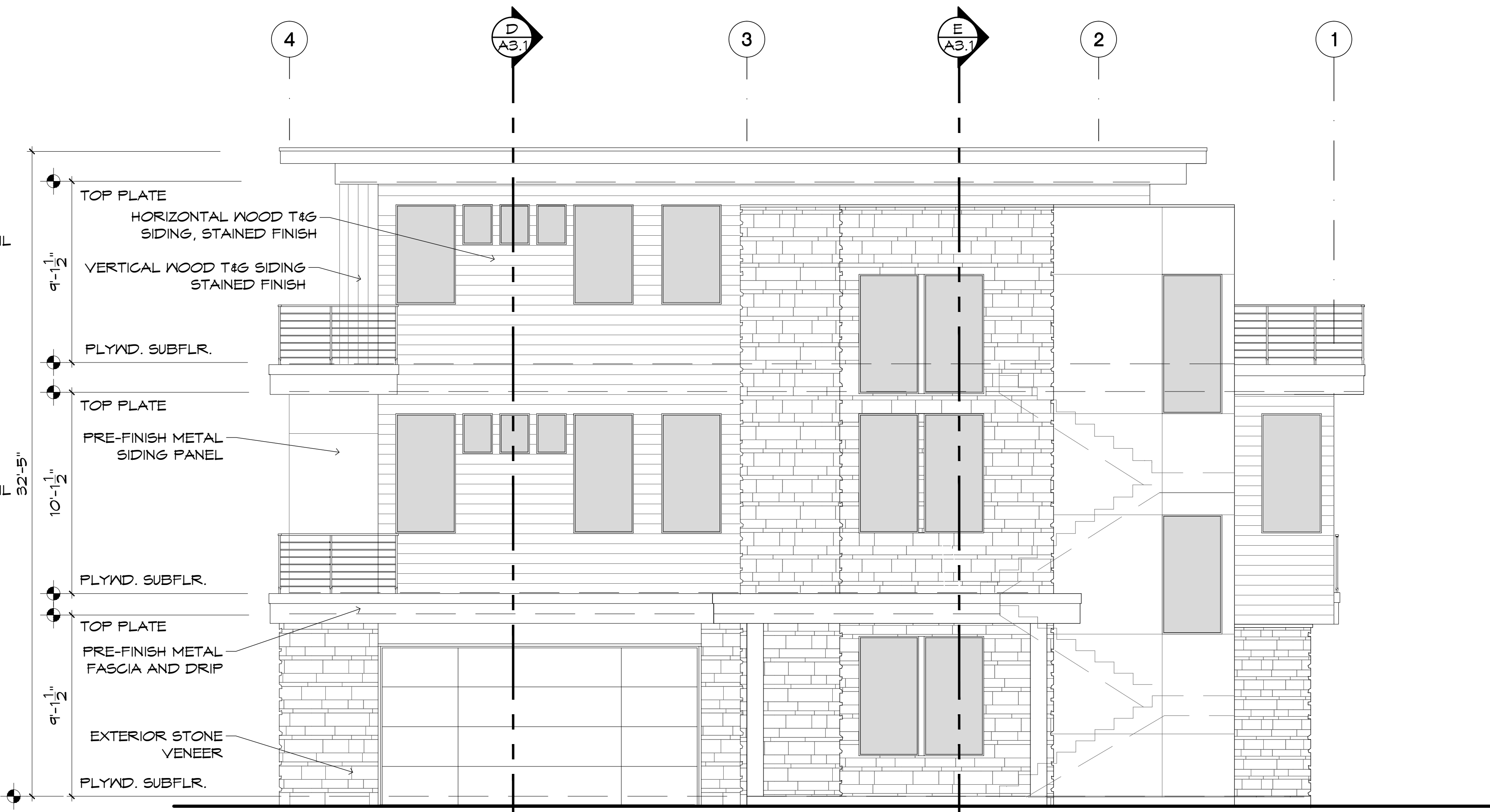
**REAR ELEVATION**



**LEFT SIDE ELEVATION**



**FRONT ELEVATION**  
**LOTS 1A + 2A, UNITS 2**



**RIGHT SIDE ELEVATION**

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**SNOWBIRD TOWNHOMES**  
**LOT 2A**  
 220 BIRD DRIVE  
 LOTS 1 & 2, SNOWBIRD SUBDIVISION  
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2021 17
Date: 03/15/2022
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Checked By: TW
Revisions

**A4.1**  
 Exterior Elevations



**RIGHT (NORTH) ELEVATION**  
**LOT 2A - UNIT 1**

**RIGHT (NORTH) ELEVATION**  
**LOT 1A - UNIT 1 (FOR REFERENCE ONLY)**

**B SITE ELEVATION** SCALE: 1/8" = 1'-0"



**LEFT (SOUTH) ELEVATION**  
**LOT 1A - UNIT 2 (FOR REFERENCE ONLY)**

**LEFT (SOUTH) ELEVATION**  
**LOT 2A - UNIT 2**

**A SITE ELEVATION** SCALE: 1/8" = 1'-0"

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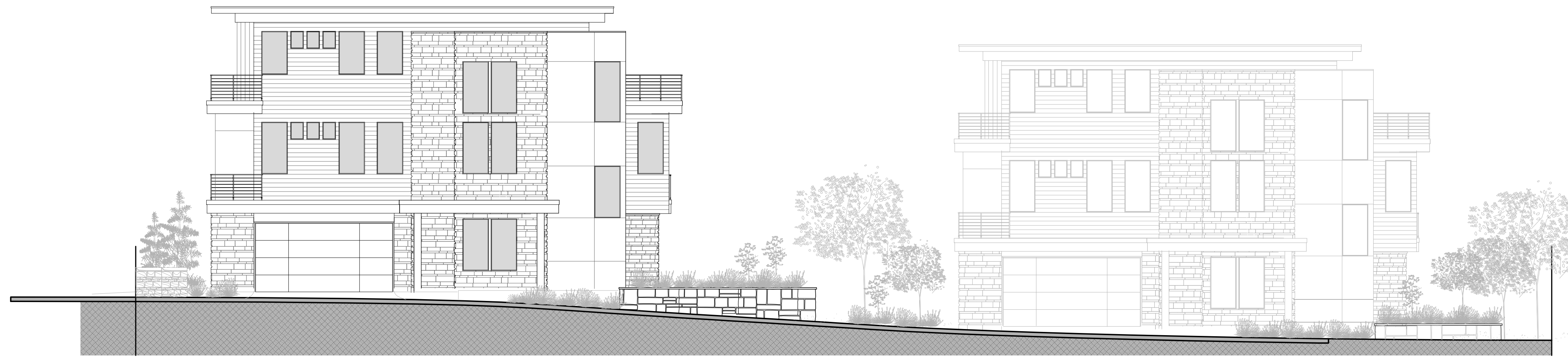
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**SNOWBIRD TOWNHOMES**  
**LOT 2A**  
 220 BIRD DRIVE  
 LOTS 1 & 2, SNOWBIRD SUBDIVISION  
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Date:	03/15/2022
Drawn By:	RJS
Checked By:	TW
Revisions	
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▲	
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**A4.2**

Composite Exterior Elevations



**RIGHT (NORTH) ELEVATION**  
LOT 2A - UNIT 2

**RIGHT (NORTH) ELEVATION**  
LOT 1A - UNIT 2 (FOR REFERENCE ONLY)

**C SITE ELEVATION**

SCALE: 1/8" = 1'-0"

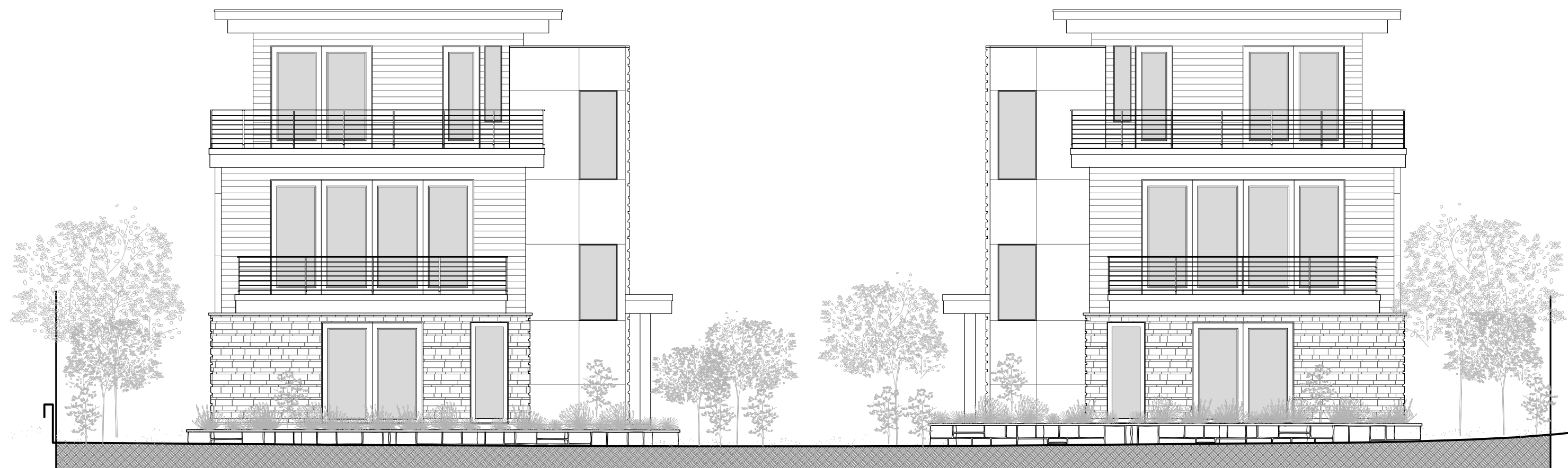


**LEFT (SOUTH) ELEVATION**  
LOT 1A - UNIT 1 (FOR REFERENCE ONLY)

**LEFT (SOUTH) ELEVATION**  
LOT 2A - UNIT 1

**B SITE ELEVATION**

SCALE: 1/8" = 1'-0"

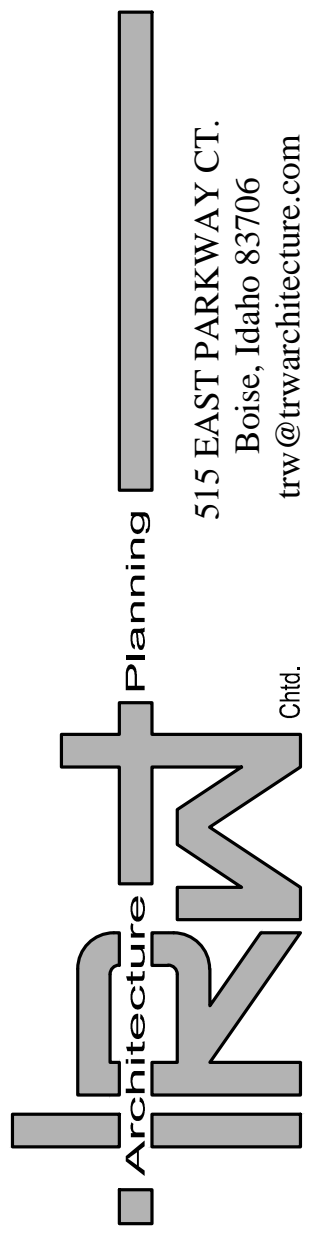


**REAR (WEST) ELEVATION**  
LOT 2A - UNIT 1

**REAR (WEST) ELEVATION**  
LOT 2A - UNIT 2

**A SITE ELEVATION**

SCALE: 1/8" = 1'-0"


  
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**SNOWBIRD TOWNHOMES**  
**LOT 2A**  
 220 BIRD DRIVE  
 LOTS 1 & 2, SNOWBIRD SUBDIVISION  
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Date:	03/15/2022
Drawn By:	RJS
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Revisions	
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**A4.3**  
 Composite Exterior Elevations



**BIRD DRIVE PERSPECTIVE**



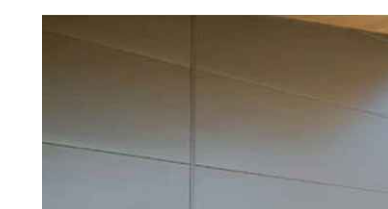
**BIRD DRIVE PERSPECTIVE**



**LEFT SIDE ELEVATION  
LOTS 1A + 2A, UNITS 1**



**Wood Siding**  
Vertical T&G Hem-Fir



**Metal Siding**  
Flat Lock Panel, Quartz-Zinc



**Wood Siding**  
Horizontal T&G Hem-Fir



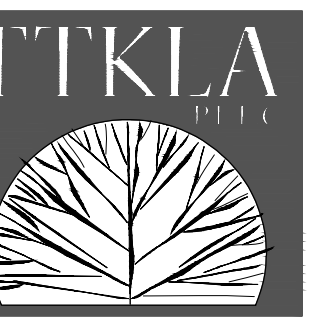
**Stone Veneer**  
Limestone



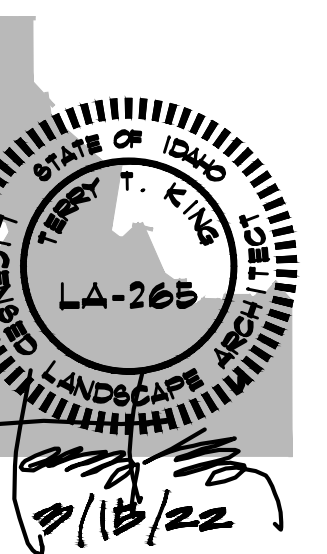
**FRONT ELEVATION**







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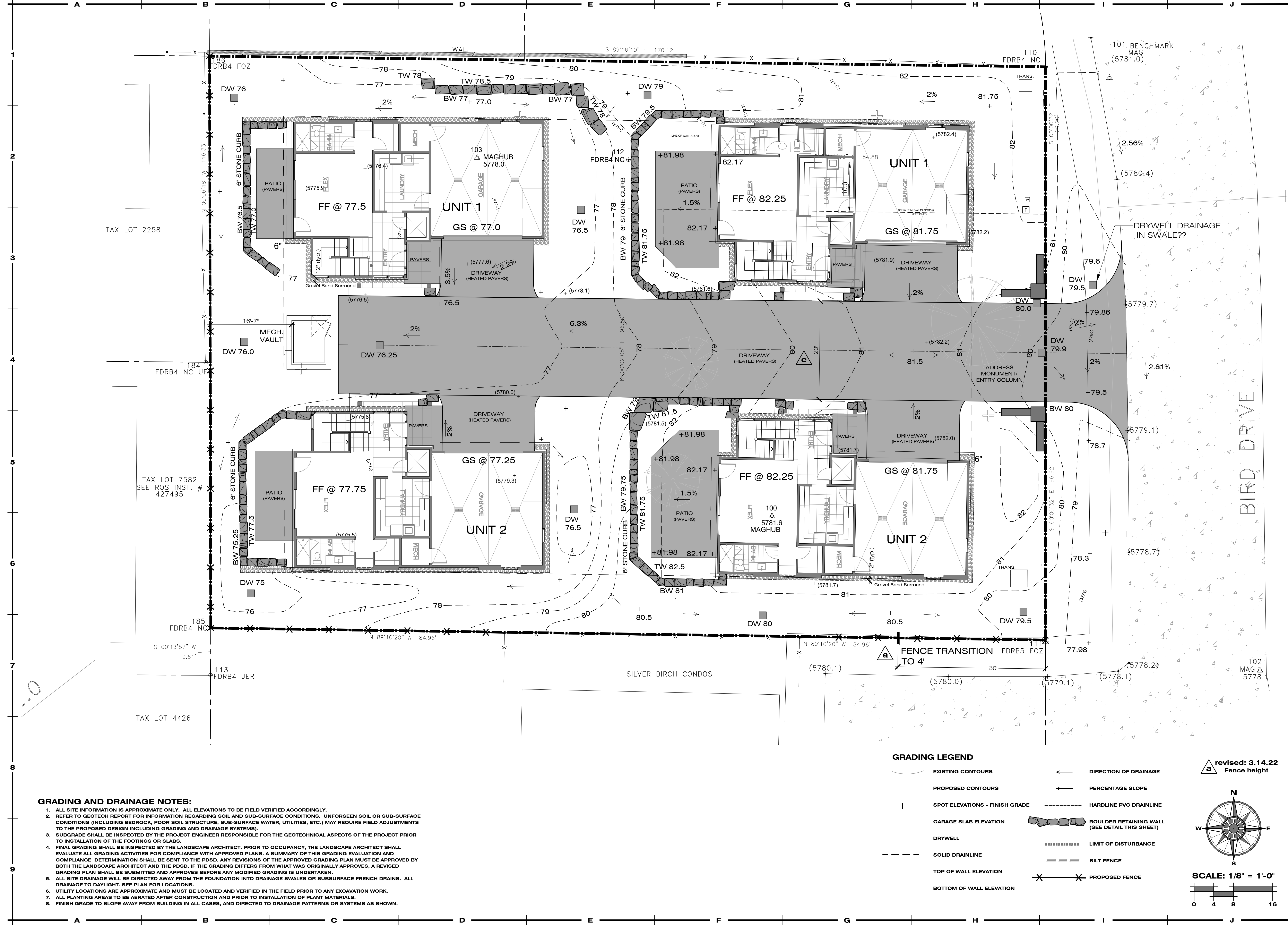
**Grading Plan**

**Snowbird Townhomes**

220 Bird Dr.  
Lots 1 & 2, Snowbird Sub.  
Ketchum • Idaho • 83340

drawn by: mo  
checked by: tk  
date: March 15, 2022  
revision:  
March 14, 2022

**L-1.1**  
**SHEET**



- GRADING AND DRAINAGE NOTES:**
1. ALL SITE INFORMATION IS APPROXIMATE ONLY. ALL ELEVATIONS TO BE FIELD VERIFIED ACCORDINGLY.
  2. REFER TO GEOTECH REPORT FOR INFORMATION REGARDING SOIL AND SUB-SURFACE CONDITIONS. UNFORSEEN SOIL OR SUB-SURFACE CONDITIONS (INCLUDING BEDROCK, POOR SOIL STRUCTURE, SUB-SURFACE WATER, UTILITIES, ETC.) MAY REQUIRE FIELD ADJUSTMENTS TO THE PROPOSED DESIGN INCLUDING GRADING AND DRAINAGE SYSTEMS).
  3. SUBGRADE SHALL BE INSPECTED BY THE PROJECT ENGINEER RESPONSIBLE FOR THE GEOTECHNICAL ASPECTS OF THE PROJECT PRIOR TO INSTALLATION OF THE FOOTINGS OR SLABS.
  4. FINAL GRADING SHALL BE INSPECTED BY THE LANDSCAPE ARCHITECT. PRIOR TO OCCUPANCY, THE LANDSCAPE ARCHITECT SHALL EVALUATE ALL GRADING ACTIVITIES FOR COMPLIANCE WITH APPROVED PLANS. A SUMMARY OF THIS GRADING EVALUATION AND COMPLIANCE DETERMINATION SHALL BE SENT TO THE PSDS. ANY REVISIONS OF THE APPROVED GRADING PLAN MUST BE APPROVED BY BOTH THE LANDSCAPE ARCHITECT AND THE PSDS. IF THE GRADING DIFFERS FROM WHAT WAS ORIGINALLY APPROVES, A REVISED GRADING PLAN SHALL BE SUBMITTED AND APPROVES BEFORE ANY MODIFIED GRADING IS UNDERTAKEN.
  5. ALL SITE DRAINAGE WILL BE DIRECTED AWAY FROM THE FOUNDATION INTO DRAINAGE SWALES OR SUBSURFACE FRENCH DRAINS. ALL DRAINAGE TO DAYLIGHT. SEE PLAN FOR LOCATIONS.
  6. UTILITY LOCATIONS ARE APPROXIMATE AND MUST BE LOCATED AND VERIFIED IN THE FIELD PRIOR TO ANY EXCAVATION WORK.
  7. ALL PLANTING AREAS TO BE AERATED AFTER CONSTRUCTION AND PRIOR TO INSTALLATION OF PLANT MATERIALS.
  8. FINISH GRADE TO SLOPE AWAY FROM BUILDING IN ALL CASES, AND DIRECTED TO DRAINAGE PATTERNS OR SYSTEMS AS SHOWN.

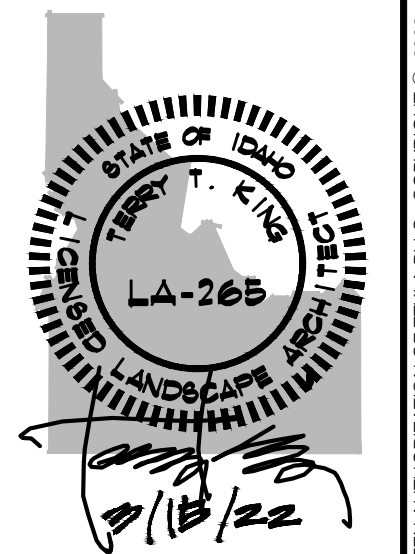
**GRADING LEGEND**

	EXISTING CONTOURS		DIRECTION OF DRAINAGE
	PROPOSED CONTOURS		PERCENTAGE SLOPE
	SPOT ELEVATIONS - FINISH GRADE		HARDLINE PVC DRAINLINE
	GARAGE SLAB ELEVATION		BOULDER RETAINING WALL (SEE DETAIL THIS SHEET)
	DRYWELL		LIMIT OF DISTURBANCE
	SOLID DRAINLINE		SILT FENCE
	TOP OF WALL ELEVATION		PROPOSED FENCE
	BOTTOM OF WALL ELEVATION		

**SCALE: 1/8" = 1'-0"**

**revised: 3.14.22**  
a Fence height

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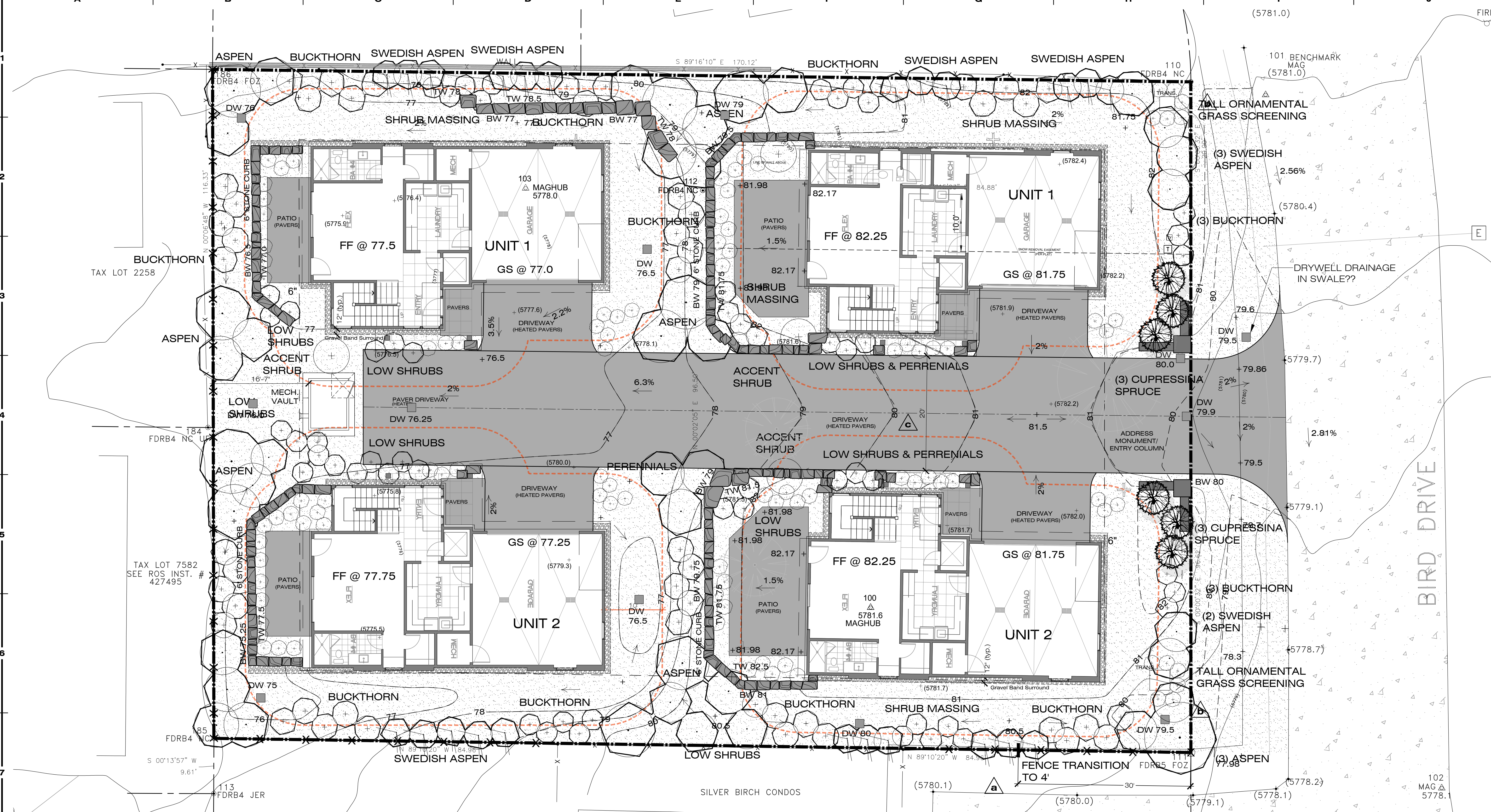
**LANDSCAPE PLAN**

**Snowbird Townhomes**

220 Bird Dr.  
Lots 1 & 2, Snowbird Sub.  
Ketchum • Blaine County • Idaho

drawn by: mo  
checked by: tk  
date: March 14, 2022  
revisions:  
March 15, 2022

**L-1.2**  
SHEET

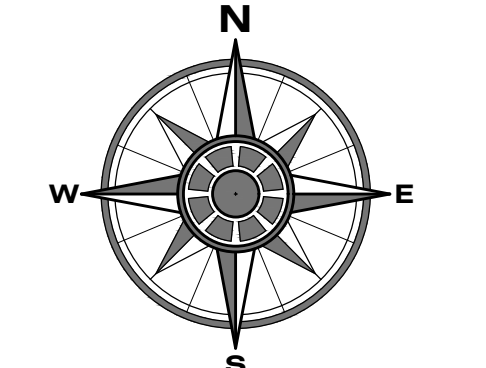


**PLANT LEGEND**

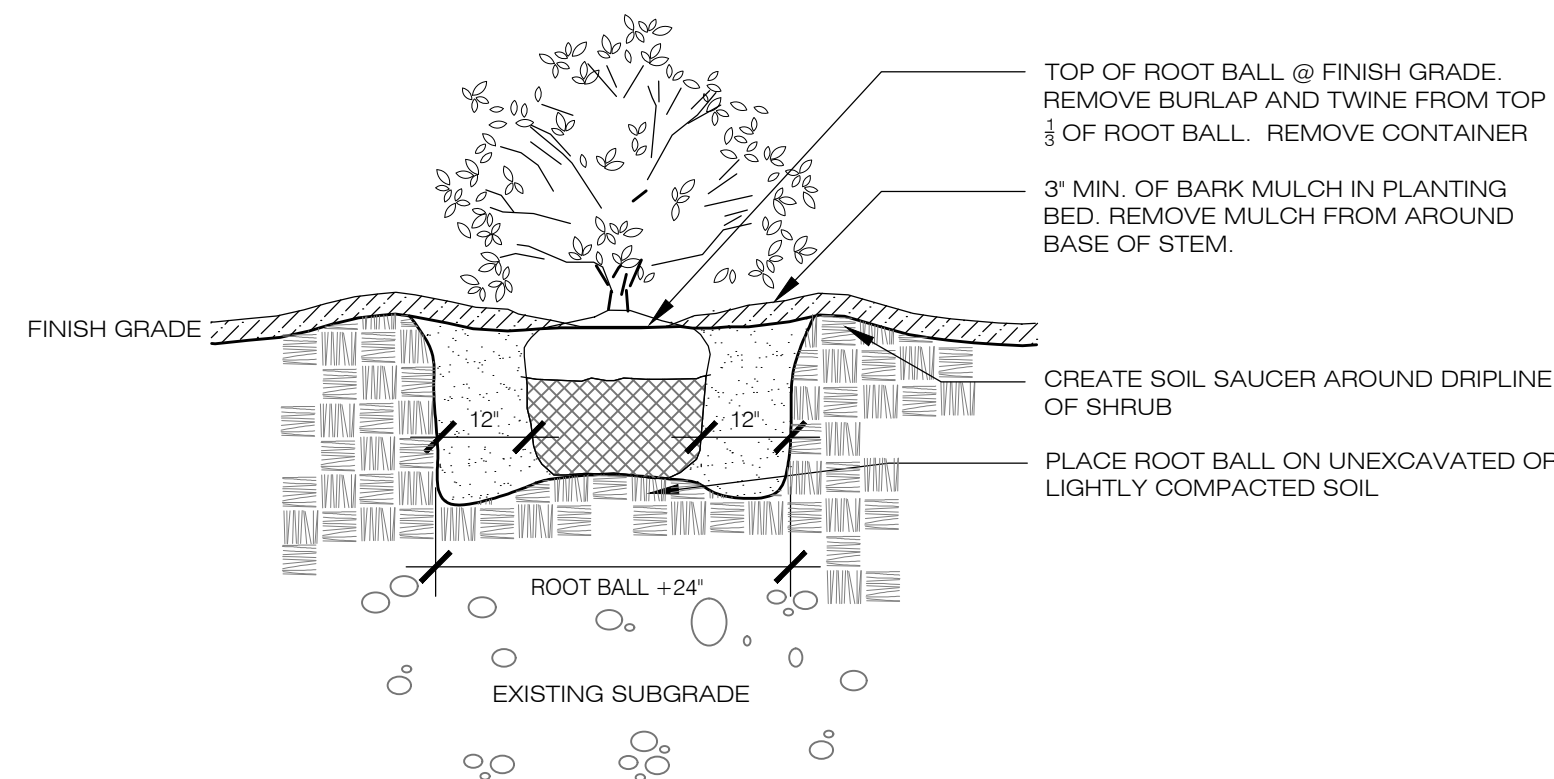
	<b>2</b> EVERGREEN TREES		<b>112</b> SHRUBS		<b>10,230 sf</b> PLANTER BEDS		<b>645 lf</b> EDGING
6 - 10-12'	Cupressina Spruce, Picea abies 'Cupressina'		Barberry - Helmond Pillar, Berberis thunb. f. atro. 'Helmond Pillar' Barberry - Orange Rocket, Berberis thunbergii 'Orange Rocket' Buckthorn - Tallhedge, Rhamnus frangula 'Tallhedge' Dogwood - Kelsey, Cornus sericea 'Kelsey' Peking Cotoneaster, Cotoneaster acutifolius Snowberry, Symphoricarpos albus Spirea - Tor Birchleaf, Spirea fritschiana Spirea - Tor Birchleaf, Spirea betulifolia 'Tor' Sumac - Gro-Low, Rhus aromatica 'Gro-Low'		3" MIN. BARK MULCH 10,230 s.f. - 95 yds		STEEL EDGING 645 l.f.
	<b>51</b> DECIDUOUS TREES		<b>90</b> PERENNIALS		<b>460 sf</b> 12" GRAVEL FOUNDATION BORDER		<b>5,360 lf</b> PAVERS
10 - 6-8' 16 - 8-10' 15 - 10-12' 10 - 12'+	Quaking Aspen, Populus Tremuloides		SALVIA PENSTEMON COLUMBINE COREOPSIS KINNIKINICK SWEET WOODRUFF CREEPING MAHONIA CORAL BELLS SEDUM		460 s.f.		5,360 s.f.
	<b>28</b>				<b>228 lf</b> 6 1/4" TALL WOOD FENCE		
6 - 6-8' 8 - 8-10' 8 - 10-12' 6 - 12'+	Swedish Aspen, Populus Tremula 'Erecta'				158 l.f. - 6' tall fence 30 l.f. - 4' tall fence		

**LANDSCAPE & PLANTING NOTES:**

- LOCATIONS OF PROPOSED PLANT MATERIALS ARE APPROXIMATE AND MUST BE FIELD VERIFIED PRIOR TO INSTALLATION. LOCATIONS SHOWN ON THE PLAN MAY VARY BASED ON EXISTING FIELD CONDITIONS. CONSULT LANDSCAPE ARCHITECT PRIOR TO INSTALL.
- ANY UTILITY LOCATIONS ARE APPROXIMATE AND MUST BE LOCATED AND VERIFIED IN THE FIELD PRIOR TO ANY EXCAVATION WORK INCLUDING INSTALLATION OF PLANT MATERIALS.
- ALL PLANTING AREAS TO BE AERATED AFTER CONSTRUCTION AND PRIOR TO INSTALLATION OF PLANT MATERIALS.
- ALL PLANT MATERIALS TO COMPLY WITH THE AMERICAN ASSOCIATION OF NURSERYMEN STANDARDS.
- ALL PLANT MATERIAL SUBJECT TO AVAILABILITY.
- REFER TO ADDITIONAL MATERIALS AND SPECS INCLUDING IRRIGATION PLAN AND PERFORMANCE SPECIFICATIONS FOR ADDITIONAL INFORMATION.
- ALL LANDSCAPING TO BE IRRIGATED BY AN AUTOMATIC SPRINKLER SYSTEM.
- THIS PROJECT SHALL COMPLY WITH THE CITY OF KETCHUM FIRE PROTECTION AND DEFENSIBLE SPACE CHARACTERISTICS. TREE CROWNS EXTENDING TO WITHIN 10 FEET OF ANY STRUCTURE SHALL BE PRUNED TO MAINTAIN A MINIMUM HORIZONTAL CLEARANCE OF 10 FEET. TREE CROWNS WITHIN 30 FEET OF ANY STRUCTURE SHALL BE PRUNED TO REMOVE LIMBS LOCATED LESS THAN 6 FEET ABOVE THE GROUND SURFACE ADJACENT TO THE TREES. NON FIRE-RESISTIVE VEGETATION OR GROWTH SHALL BE KEPT CLEAR OF BUILDINGS AND STRUCTURES. IN SUCH A MANNER AS TO PROVIDE A CLEAR AREA FOR FIRE SUPPRESSION OPERATIONS. THE AREA 12" HORIZONTAL FROM THE BASE OF A WALL SHALL BE FINISHED IN A WAY TO PREVENT ANY VEGETATION GROWING, AND FOR VEGETATIVE DEBRIS TO BE EASILY REMOVED.

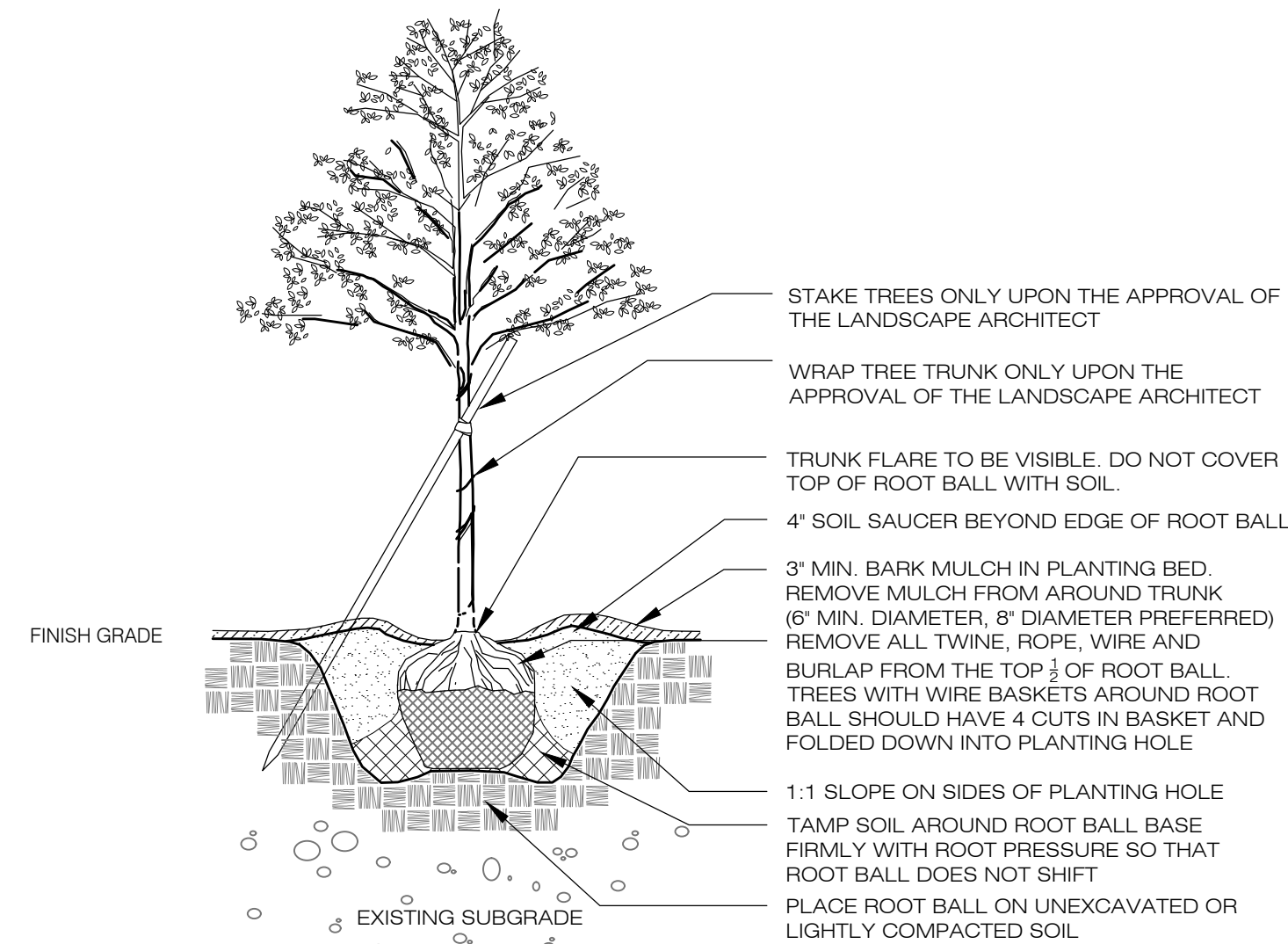


- a** revised: 3.14.22 Fence height
- b** revised: 3.14.22 Add screening plants
- c** revised: 3.14.22 width of driveway



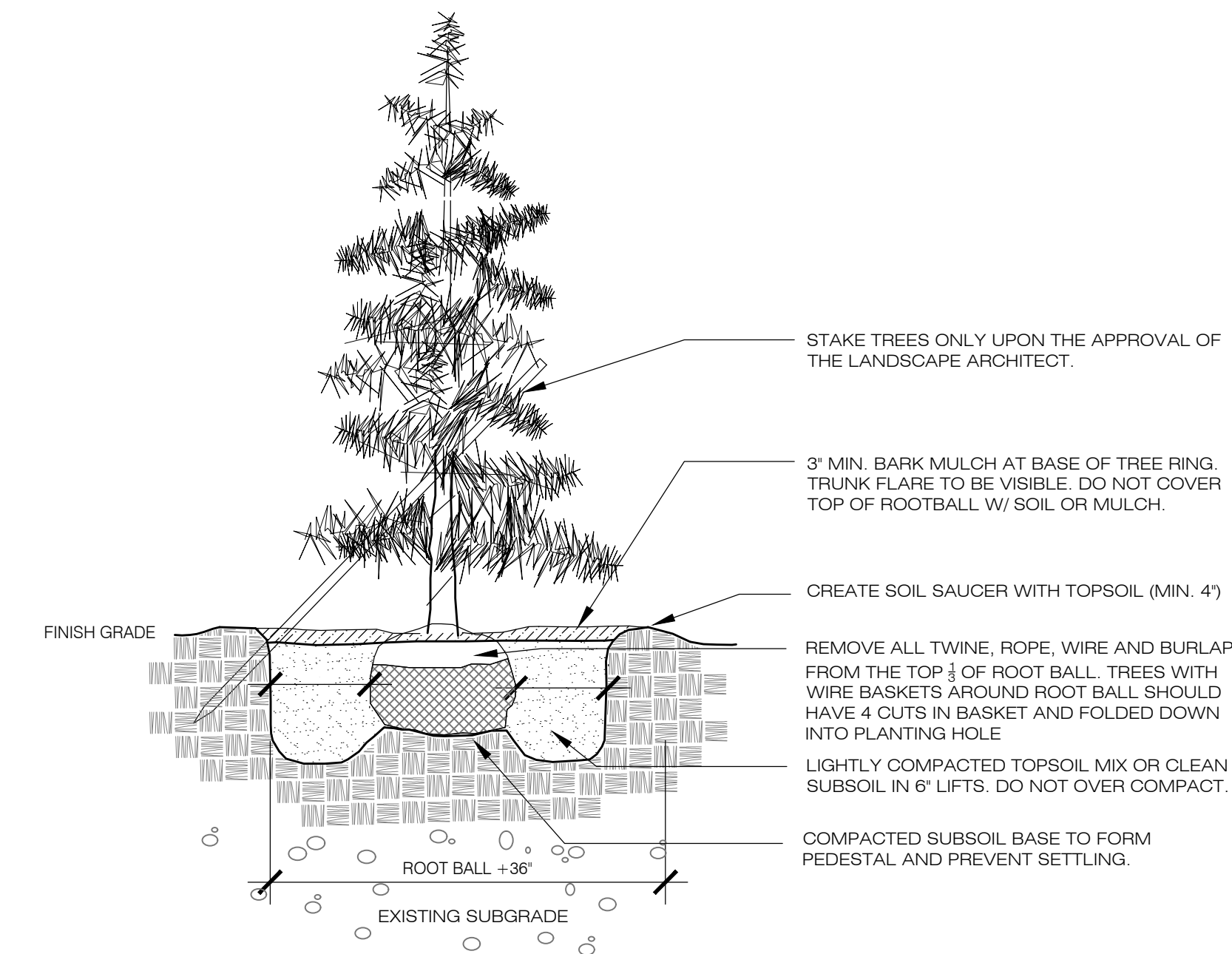
- NOTES:
1. COMPLETELY REMOVE ALL STRINGS, RIBBONS, TAGS AND OTHER FOREIGN OBJECTS FROM THE PLANT.
  2. ALL SHRUBS TO BE PLANTED IN BARK MULCH BEDS.
  3. PRUNE ALL BROKEN AND WEAK BRANCHES. PRUNE WHEN FIRST INSTALLED, EXCEPT IF PLANT IS IN BLOOM. ALL PRUNING MUST OCCUR WHEN PLANT IS NOT IN BLOOM.
  4. ALL SHRUBS TO BE COMPLETELY EXCAVATED OF ALL EXISTING SOIL TO REQUIRED DEPTH AND BACKFILLED WITH REQUIRED SOIL MIX.

**SHRUB PLANTING DETAIL**  
SCALE: NTS



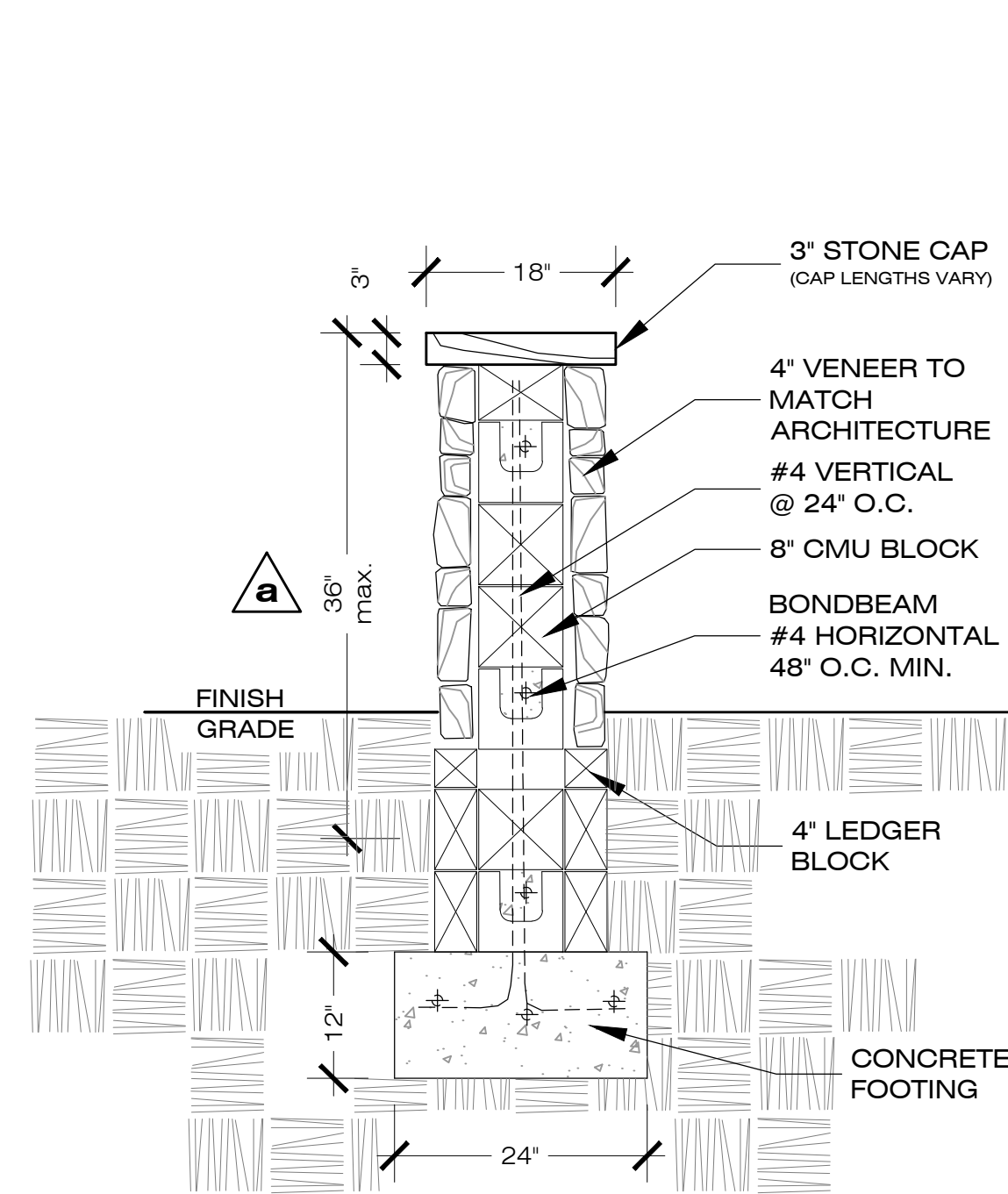
- NOTES:
1. SET TOP OF ROOT BALL FLUSH WITH FINISHED GRADE OR 1'-3" HIGHER IN SLOWLY DRAINING SOILS.
  2. DO NOT HEAVILY PRUNE TREE AT TIME OF PLANTING. PRUNE ONLY CROSSOVER LIMBS, CO-DOMINANT LEADERS, BROKEN OR DEAD BRANCHES. DO NOT REMOVE TERMINAL BUDS THAT EXTEND TO THE EDGE OF THE CROWN. DO NOT PRUNE MAIN LEADER.

**DECIDUOUS TREE PLANTING DETAIL**  
SCALE: NTS

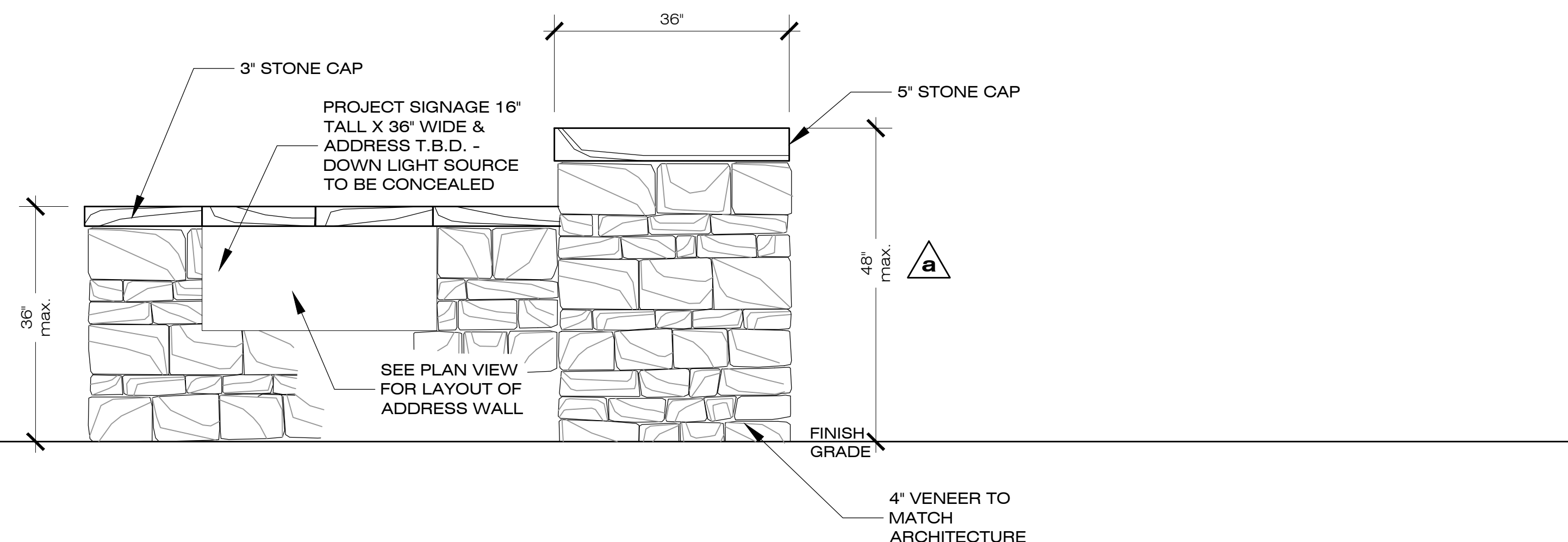


- NOTES:
1. DIG HOLE TWICE THE SIZE OF ROOT BALL.
  2. WHEN PLANTING PINE (PINUS) SPECIES IN SLOWLY DRAINING SOILS, SET TOP OF ROOT BALL 1'-3" HIGHER THAN THE EXISTING GRADE OR WITH OTHER EVERGREEN SPECIES.
  3. PRUNE ALL DEAD AND DAMAGED BRANCHES. DO NOT PRUNE MAIN LEADER. DO NOT PLANT IN EVENT MAIN LEADER IS DAMAGED UNLESS DIRECTED BY LANDSCAPE ARCHITECT.

**EVERGREEN TREE PLANTING DETAIL**  
SCALE: NTS



**Wall Section and Elevation Views**  
SCALE:  $\frac{3}{4}$ " = 1'-0"



**Entry Column and Wall Elevation**  
SCALE:  $\frac{3}{4}$ " = 1'-0"

revised: 3.14.22  
column and wall heights

# SNOWBIRD TOWNHOMES II

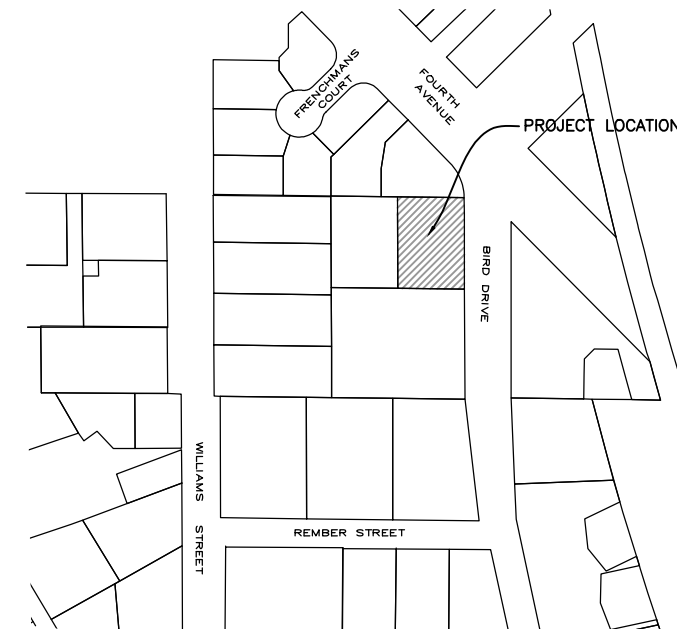
LOCATED WITHIN: SECTION 13, TOWNSHIP 4 NORTH, RANGE 17 EAST, B.M., CITY OF KETCHUM, BLAINE COUNTY, IDAHO

A TOWNHOUSE SUBDIVISION OF SNOWBIRD SUBDIVISION, LOT 2A, CREATING TWO SUBLOTS.

## MARCH 2022 PRELIMINARY PLAT

### OWNER OF RECORD

SCOTT J. EDWARDS  
13019 NAOMILAWN DRIVE SOUTHWEST  
LAKEWOOD, WA 98498



VICINITY MAP  
NOT TO SCALE

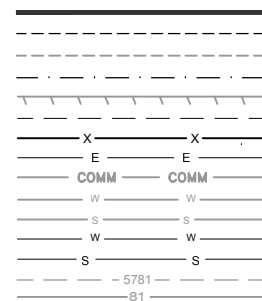
### SURVEYOR'S NARRATIVE:

1. THE PURPOSE OF THIS PLAT IS TO CREATE TWO TOWNHOUSE SUBLOTS. FOUND MONUMENTS WERE ACCEPTED AS EITHER ORIGINAL OR REPLACEMENTS OF ORIGINAL CORNERS.
2. DIMENSIONS SHOWN HEREON ARE MEASURED. FOR RECORD DIMENSIONS, SEE REFERENCED SURVEYS.
3. DOCUMENTS USED IN THE COURSE OF THIS SURVEY:
  - A. ORIGINAL PLAT OF "SNOWBIRD SUBDIVISION", INST. NO. 321440.
  - B. REPLAT OF "SNOWBIRD SUBDIVISION: LOTS 1A & 2A", INST. NO. \_\_\_\_\_.
  - C. SNOWBIRD TOWNHOMES I, INST. NO. \_\_\_\_\_.

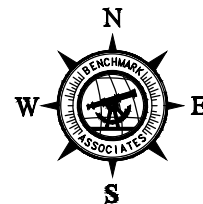
### NOTES:

1. THE TOWNHOME DECLARATION FOR SNOWBIRD TOWNHOMES WAS RECORDED AS INST. NO. \_\_\_\_\_, RECORDS OF BLAINE COUNTY, IDAHO.
2. THE PHASED DEVELOPMENT AGREEMENT FOR SNOWBIRD TOWNHOMES WAS RECORDED AS INST. NO. \_\_\_\_\_, RECORDS OF BLAINE COUNTY, IDAHO.
3. ALL TOWNHOUSE OWNERS SHALL HAVE MUTUAL RECIPROCAL EASEMENTS FOR ACCESS AND EXISTING/FUTURE PUBLIC AND PRIVATE UTILITIES INCLUDING, BUT NOT LIMITED TO, MECHANICAL VAULTS, WATER, CABLE TV, SEWER, NATURAL GAS, TELEPHONE, AND ELECTRIC LINES OVER, UNDER AND ACROSS THEIR TOWNHOUSE SUBLOTS FOR THE REPAIR, MAINTENANCE AND REPLACEMENT THEREOF.
4. A 10 FOOT WIDE PUBLIC UTILITY EASEMENT EXISTS CENTERED UPON ALL SIDE AND REAR LOT LINES PER ORIGINAL PLAT. A 10-FOOT WIDE PUBLIC UTILITY EASEMENT EXISTS ALONG FRONT PROPERTY LINE PER PREVIOUS PLAT.
5. A 20 FOOT WIDE MUTUAL RECIPROCAL DRIVEWAY & UTILITY EASEMENT TO BENEFIT SNOWBIRD TOWNHOMES I, SUBLOTS 1 & 2, AND SNOWBIRD TOWNHOMES II, SUBLOTS 1 & 2 EXISTS PER PREVIOUS PLAT.
6. CURRENT ZONING IS GR-L, GENERAL RESIDENTIAL LOW DENSITY.
7. A POWERLINE EASEMENT WAS RECORDED JULY 8, 1963 AS INST. NO. 118840, RECORDS OF BLAINE COUNTY, IDAHO.
8. GARAGE(S) MAY NOT BE CONDOMINIUMIZED, SOLD AND/OR OWNED SEPARATE FROM THE CORRESPONDING DWELLING UNIT WITHIN THE TOWNHOUSE DEVELOPMENT.
9. EXISTING UTILITIES ARE PER SURFACE EVIDENCE & CITY OF KETCHUM RECORDS & ARE APPROXIMATE. OTHER UNDERGROUND UTILITIES MAY EXIST.
10. PROPOSED UTILITIES, CONTOURS & DRAINAGE PER CIVIL & LANDSCAPE PLANS.
11. CONTOUR INTERVAL: 1' - CONTOURS IN AREAS OF DENSE VEGETATION MAY DEVIATE FROM TRUE ELEVATION BY ONE HALF THE HEIGHT OF THE VEGETATION. DATE OF LIDAR FLIGHT FOR CONTOURS: 2017.

### LEGEND:



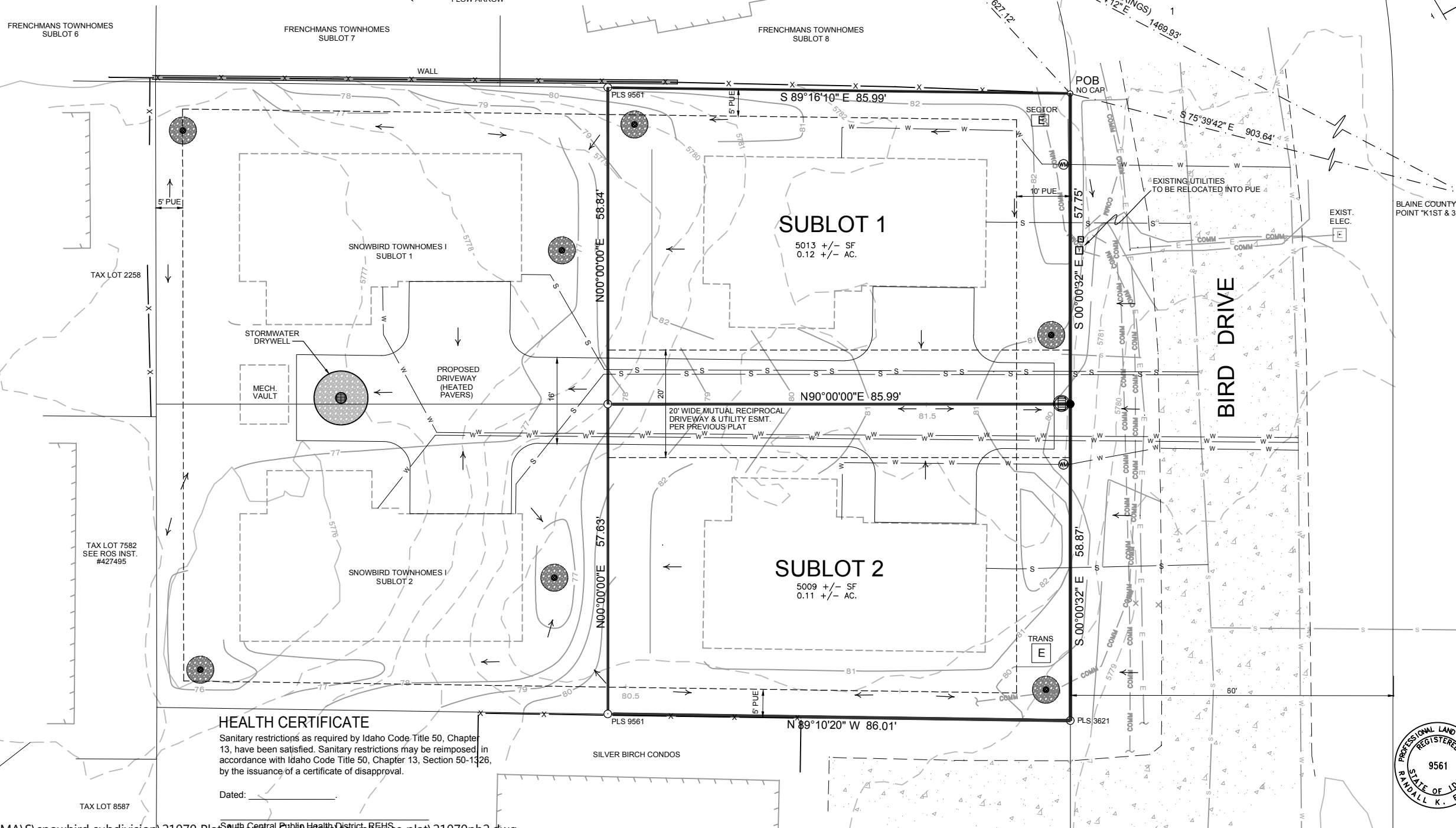
- PROPERTY LINE
- EASEMENT LINE
- APPROXIMATE BUILDING FOOTPRINT
- BLAINE COUNTY GIS TIE
- APPROXIMATE EXISTING BUILDING
- EDGE PAVEMENT
- EXISTING WOOD FENCE
- EXISTING ELECTRIC LINE
- EXISTING COMMUNICATION LINE
- EXISTING WATER LINE
- EXISTING SEWER LINE
- NEW WATER SERVICE
- NEW SEWER SERVICE
- EXISTING CONTOUR
- PROPOSED CONTOUR
- EXISTING TELEPHONE BOX
- EXISTING CABLE TV BOX
- FOUND 1/2" REBAR
- FOUND 5/8" REBAR
- SET 5/8" REBAR, PLS #9561
- FOUND ALUMINUM CAP
- NEW ELECTRICAL BOX
- NEW DRYWELL
- NEW CATCH BASIN
- FLOW ARROW



FRENCHMANS TOWNHOMES  
SUBLOT 6

FRENCHMANS TOWNHOMES  
SUBLOT 7

FRENCHMANS TOWNHOMES  
SUBLOT 8



### HEALTH CERTIFICATE

Sanitary restrictions as required by Idaho Code Title 50, Chapter 13, have been satisfied. Sanitary restrictions may be reimposed in accordance with Idaho Code Title 50, Chapter 13, Section 50-1326, by the issuance of a certificate of disapproval.

Dated: \_\_\_\_\_



### SNOWBIRD TOWNHOMES II

LOCATED WITHIN  
SECTION 13, TOWNSHIP 4 NORTH, RANGE 17 EAST, B.M.,  
CITY OF KETCHUM, BLAINE COUNTY, IDAHO

PREPARED FOR: SCOTT J. EDWARDS

PROJECT NO. 21070	DWG BY: DWS/CPL	21070ph2.DWG
PRELIMINARY PLAT	DATE: 03/21/2022	SHEET: 1 OF 1