



**City of Ketchum
Planning & Building**

**STAFF REPORT
KETCHUM PLANNING AND ZONING COMMISSION
SPECIAL MEETING OF AUGUST 13, 2019**

PROJECT: Sun Valley & 1st

FILE NUMBER: P19-083

APPLICANT: David Wilson (Geneva Plaza LLC per Blaine County Assessor's Office Data)

REPRESENTATIVE: Buffalo Rixon, Architect, Ruscitto Latham Blanton Architectura PA

REQUEST: Design Review of a remodel and addition to the existing building located at the corner of Sun Valley Road and 1st Avenue to accommodate 3 new retail areas, 2 community housing units, and 2 upper level condominium units

LOCATION: 311 N 1st Avenue (Ketchum Townsite: Block 57: Lot 4)

NOTICE: Public hearing notice for the subject application was mailed to adjacent property owners on July 31st, 2019.

ZONING: Mixed-Use Sub-District of the Community Core (CC-2)

REVIEWER: Abby Rivin, Associate Planner

BACKGROUND

The subject Design Review request is for a remodel and 6,550 sq ft addition to the existing building located at 311 N 1st Avenue, which is commonly referred to as the Old Post Office building. A second floor is proposed to be added to the existing single-story building in order to accommodate two new residential condominium units. The existing first floor will be remodeled to accommodate three new retail spaces and to community housing units. The subject site is and 8,250 sq ft Ketchum Townsite lot located within the Mixed Use Sub-district of the Community Core (CC-2). The total proposed gross floor area of the mixed-use building will be 14,305 gross sq ft with a Floor Area Ratio (FAR) of 1.74, which is 4,257 gross sq ft less than maximum 2.25 FAR permitted with the inclusionary housing incentive in the CC-2 Zone (Ketchum Municipal Code §17.124.040.A). Pursuant to KMC §17.96.010.C, the Administrator has waived Pre-Application Design Review as the project scope is a remodel and addition to an existing building.

ANALYSIS

Staff recommends the Planning & Zoning Commission consider the analysis contained in the Tables 1-5 of the Staff Report, the applicant submittal package included as Exhibit A, and any public comment received, deliberate, and move to approve the Design Review for the Sun Valley & First mixed-use building project. A full analysis and explanation of this recommendation is detailed within the Staff Report.

COMPREHENSIVE PLAN ANALYSIS:

The proposed mixed-use building project demonstrates three of the Core Community Values contained in the 2014 Comprehensive Plan—A Strong and Diverse Economy, Vibrant Downtown, and a Variety of Housing

Options. The project is consistent with the uses, goals, and policies listed below as specified within the 2014 Comprehensive Plan.

Table 1. Comprehensive Plan Analysis

SUPPORTING SECTION	SUMMARY OF COMPLIANCE WITH THE 2014 COMPREHENSIVE PLAN
Future Land Use	
Mixed-Use Commercial	This Mixed-Use Commercial category is intended to promote a wide range of land uses, including offices, medical facilities, health/wellness-related services, recreation, government, residential, and services. General retail is limited to the downtown core. (Areas in the ACI with this designation will require evaluation with regard to the provision of access, utility service, safety, and environmental impacts.)
Community Design and Neighborhoods	
Policy CD-1.1 Unique Design Elements for Identifiable Neighborhoods	Each neighborhood or district should include a mix of design elements that will reinforce its unique design quality.
Policy CD-1.3 Compatible Infill and Redevelopment Projects	Infill and redevelopment projects should be contextually appropriate to the neighborhood and development in which they will occur. Context refers to the natural and manmade features adjoining a development site; it does not imply a certain style.
Housing	
Goal H-1	Ketchum will increase its supply of homes, including rental and special-needs housing for low-, moderate-, and median income households.
Policy H-1.2 Local Solutions to Attainable Housing	The City of Ketchum will place greater emphasis on locally-developed solutions to meet the housing needs of low-, moderate-, and median-income households. The City further recognizes that such needs likely will not be met solely through private development. To facilitate affordable housing opportunities, the City will look to new funding mechanisms, and encourage a broad range of regulatory incentives and options for community housing. These may include unit buy-downs, unit reuse, density increases, and height bonuses.
Policy H-1.4 Integrated Housing in Business and Mixed-Use Areas	Housing should be integrated into the downtown core and light industrial areas, and close to the ski base. The resulting mix of land use will help promote a greater diversity of housing opportunities as well as social interactions.
Goal H-3	Ketchum will have a mix of housing types and styles.
Policy H-3.1 Mixture of Housing Types in New Development	The City should encourage the private sector, through land-use regulations and incentive programs, to provide a mixture of housing types with varied price ranges and densities that meet a variety of needs. The City will evaluate the use of incentives, such as flexibility in height, density and parking requirements to achieve greater housing diversity. Additionally, the City will promote the siting of higher density housing near public transportation, the ski base areas, shopping, and designated neighborhoods and districts.
A Strong and Diverse Economy	
Policy E-1(b) Downtown as a Major Community Asset and Tourism Attraction	The community will strive to maintain a single concentrated commercial and retail core. The City will reinforce the downtown core’s role as a major asset and visitor attraction by encouraging businesses that fit the downtown character and by developing policies, programs, investment strategies, and organizations that help retain downtown business.
Mobility	
Policy M-1.3 Compact Development and Housing Downtown and in Activity Centers	Encourage compact development, mixed uses, and additional housing density in the downtown and in high-activity areas. This will increase opportunities for walking, bicycling and transit ridership and reduce vehicle traps.

Future Land Use	
Goal LU-2	Support infill and redevelopment in the downtown, major activity areas and specific areas that can take advantage of proximity to services and transportation.
Policy LU-2.1 Infill and Redevelopment	Support intensification of land uses on appropriate infill and redevelopment sites in the following areas: downtown, industrial areas, St Luke’s Hospital/McHanville/Cold Springs Canyon, Warm Springs area, and existing neighborhoods with significant vacant parcels.
Policy LU-2.2 Compatible Residential Infill	Appropriate types of infill include the new residential units on vacant lots/areas, additions to existing units, accessory dwelling units, and residential units with businesses. Ensure that residential infill is compatible in character and scale within the surrounding neighborhood.
LU-2.3 Land Use Patterns for a Dynamic and Thriving Downtown	Commercial strip development along arterial streets and high intensity retail and office uses outside the downtown core will be discouraged, except to provide neighborhood-scale retail and service uses.

Table 2. City Department Comments

City Department Comments
<p><i>Note: City Department comments are preliminary and based on the project concept as proposed with the subject Design Review application. All City Departments shall review and approve the project through the Building Permit application process. All comments pertaining to the Design Review drawings are subject to change. All right-of-way improvements shall be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project. All City Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project.</i></p>
<p>Fire Department:</p> <ul style="list-style-type: none"> • It is the General Contractor’s responsibility to understand and adhere to all Fire Protection Ordinance #1125 requirements in addition to any and all other City of Ketchum requirements in effect at the time of Building Permit issuance. Failure to comply with all local ordinances and codes may result in project work stoppage as well as criminal penalties. • The above project shall meet all 2012 International Fire Code requirements in addition to specific City Building and Fire Ordinances. • Approved address and unit numbers shall be placed in such a position to be plainly visible and legible from the road fronting the property. Numbers and letters shall be a minimum of four (4) inches tall, contrast with their background and be positioned a minimum of forty-eight (48) inches above final grade. • Vehicle parking and material storage during construction shall not restrict or obstruct public streets or access to any building. A minimum twenty-foot travel lane for emergency vehicle access shall be maintained clear and unobstructed at all times. All required Fire Lanes, including within 15 feet of fire hydrants, shall be maintained clear and unobstructed at all times. • An approved automatic fire sprinkler system shall be installed throughout the building per City of Ketchum Ordinance #1125 (www.ketchumfire.org) and the National Fire Protection Association Standard 13. An approved fire sprinkler flow bell, Knox box and Fire Department Connection shall be installed in an approved location visible to approaching firefighters. Water service lines to structures shall be hydraulically calculated for size to meet fire sprinkler flow requirements. Fire sprinkler systems shall be annually tested and maintained per NFPA 25. An approved fire department connection and flow bell shall be installed in a location approved by the fire department and the system shall be supervised by an approved alarm system. NOTE: One electronic set of fire sprinkler system plans must be submitted to the Ketchum Fire Department as well as the State Fire Marshal’s office and a Ketchum Fire Department Permit must be obtained prior to installation of fire sprinkler systems. Inspections of fire sprinkler systems by the Fire Chief or an appointee are required. Inspections must be scheduled at least 48 hours in advance.

- An approved monitored fire sprinkler alarm system shall be installed per City of Ketchum Ordinance #1125 (www.ketchumfire.org) and the requirements of NFPA 72. Two (2) sets of alarm system plans shall be submitted to the Ketchum Fire Department for approval and a permit is required prior to installation of alarm systems. Inspections of fire detection systems by the Fire Chief or an appointee are required and shall be scheduled at least 48 hours in advance.
- Fire extinguishers shall be installed and maintained per 2012 IFC Section 906 both during construction and upon occupancy of the building.
- Spark arresters are required on all solid fuel burning appliance chimneys to reduce potential fires from burning embers.
- An approved key box shall be installed, with the appropriate keys, for emergency fire department access in a location approved by the fire department. The key box shall be a Knox box brand and sized to accommodate keys to every door of the project.
- Inspections of fire department permit required installations shall be scheduled at least 48 hours in advance.
- An 8 ½ by 11 color coded site map of this project shall be provided on paper and electronically to the fire department. This site map shall show the locations of gas shut-offs, power shut-offs, fire sprinkler riser rooms, fire department connections, alarm panels, Knox boxes, access doors, egress windows, stairways and any additional fire department requirements. Exact details for color coded “On-Sites” can be found at www.ketchumfire.org.
- Final inspections of all fire department permit required installations by the Fire Chief or an appointee are required and shall be scheduled at least 48 hours in advance. A Final Inspection Checklist can be found at www.ketchumfire.org. Fire Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project.

City Engineer & Streets Department:

- All drainage shall be retained on site including water from any roof drains (KMC §17.96.060.C.1).
- All construction for the project must comply with the standards set forth in Ketchum Municipal Code, Chapter 15.06 Construction Activity Standards. The applicant shall submit a Construction Activity Plan addressing all applicable activities (KMC §15.06.030), including how materials will be off-loaded at the site, plan for coordinating with neighbors on temporary closures, temporary traffic control, and construction fencing with appropriate screening, to be reviewed and approved prior to issuance of a Building Permit for the project. Pursuant to KMC §15.06.030.A.2, the applicant shall provide notice of the project, construction schedule, and general contractor’s contact information to all neighbors with properties adjacent to the project site.
- The Building Permit plans and construction drawings shall meet all applicable sections of Chapter 12 of Ketchum Municipal Code.
- Sidewalk snow removal is the responsibility of the property owner. If a snowmelt system is proposed, an encroachment permit approved by the City will be required. Property owner will be responsible for all associated maintenance and repair.
- All lighting within the ROW will need to meet city ROW standards. (see Right-of-Way Standards, Commercial Category) on both 1st Ave. and Sun Valley Road. Per City ROW standards a lighting study will need to be provided to ensure proper lighting of sidewalks. Consistent with the standards of the Dark Sky Society, the footcandles illuminating the sidewalk shall be an average of 0.2 fc and shall not exceed 5 fc.
- The existing light on the corner of 1st Avenue and Sun Valley Road shall be removed.
- Per ADA Standards sidewalks cross slopes are 1.75 +/- 0.25 percent. Sidewalk along 1st Avenue indicates a cross slope of 2%, which shall be corrected on the plan submitted with the Building Permit application to be reviewed and approved by the City Engineer and Streets Department.
- 8’ sidewalks are required as measured from the back of walkway to the back of curb. Civil Plans currently show 7.7’. Civil Plans shall be amended to reflect sidewalk width compliant with City Right-of-Way Standards to be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.

- Sign locations and bases will need to be shown on the plans to be reviewed and approved by the City Streets department. The Streets Department will provide sign bases.
- Street trees will require electrical outlets and irrigation.
- The corner layout of 1st and SV Road and entrance bulb out shown the Design Review plans submitted on 7-18-2019 does not meet City Standards and is not approved as proposed. Bulb outs are not a City Standard, but may be considered on a case by case basis.
- Applicant will need to fill out a design exception request and provide a sketch or drawing of the proposed bulb out design prior to Building Permit application submittal. Design exception requests will be distributed internally for to City Departments for review and consideration.
- Parallel parking stalls are 8 ft x 20 ft.
- The alley improvements as indicated in the Design Review submittal are adequate. Final civil drawings for all associated ROW improvements shall be submitted with the Building Permit application to be verified, reviewed, and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.
- The applicant shall submit a Street and Alley Digging, Excavation, and Trenching (“DIG”) Permit application with an associated traffic control plan for all construction work within the City right-of-way to be reviewed and approved by the Streets Department. The use of City right-of-way for construction including the closure of adjacent streets or sidewalks requires a Temporary Use of Right-of-Way Permit (“TURP”).
- City Engineer & Streets Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project.

Utilities:

- The existing 1” water service line shall be abandoned at the main. A new fire line shall be installed for the fire sprinkler system and all metering must be taken off of the new fire line.
- Requirements and specifications for the water and sewer connections will be verified, reviewed, and approved by the Utilities Department prior to issuance of a Building Permit for the project.

Building:

- The building must meet the 2012 International Building Code and Title 15 Buildings and Construction of Ketchum Municipal Code.
- Building Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project.

Planning and Zoning:

Comments are denoted throughout Tables 1, 3, 4, and 5.

Table 3: Zoning Standard Analysis

Compliance with Zoning Standards				
Compliant			Standards and Staff Comments	
Yes	No	N/A	Guideline	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.040	Minimum Lot Area
			<i>Staff Comments</i>	Required: 5,500 square feet minimum Existing: 8,250 square feet existing
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.124.040	Floor Area Ratios and Community Housing
			<i>Staff Comments</i>	Permitted in Community Core Mixed-Use Sub-district (CC-2) Permitted Gross FAR: 1.0 Permitted Gross FAR with Inclusionary Housing Incentive: 2.25 <i>Proposed Mixed-Use Building Gross Floor Area: 14,305 gross sq ft</i> <i>Lot 4 Area: 8,250 sq ft</i> <i>FAR Proposed: 1,74 (14,305 gross sq ft/8,250 sq ft lot area)</i> <i>Increase Above Permitted FAR: 6,055 sq ft</i> <i>20% of Increase: 1,211 sq ft</i>

			<p><i>Net Livable (15% Reduction): 1,029 sq ft</i> <i>Community Housing In-Lieu Fee: \$244,902</i></p> <p><i>The Project Information on Sheet A0.0 included as Exhibit A to the Staff Report indicates that the applicant will provide 1,220 sq ft of community housing within two units on the first floor of the mixed-use building. Prior to issuance of a Building Permit, an Exceedance Agreement addressing the square footage above the permitted 1.0 Floor Area Ratio and the associated community housing contribution is required (KMC §17.124.040.B).</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>17.12.040 Minimum Building Setbacks</p> <p><i>Staff Comments</i></p> <p>Required: <i>Front (N 1st Avenue): 5' average</i> <i>Street Side (Sun Valley Road): 5' average</i> <i>Adjacent to alleyway (Rear): 3'</i> <i>Interior Side: 0'</i> <i>Cantilevered decks and overhangs: 0'</i> <i>Non-habitable structures, fixed amenities, solar and mechanical equipment affixed to a roof from all building facades: 10'</i></p> <p>Proposed: <i>Front (N 1st Avenue): 6.9' average</i> <i>Street Side (Sun Valley Road): 5.7' average</i> <i>Adjacent to alleyway (Rear): 3'</i> <i>Interior Side: 0'</i> <i>Cantilevered decks overhangs: 0'</i> <i>Non-habitable structures, fixed amenities, solar and mechanical equipment affixed to a roof from all building facades: As indicated on Sheet A2.1 of the Design Review submittal, the applicant has proposed maintaining the mechanical equipment in its existing location. The applicant has proposed planters for the associated roof decks. The applicant shall submit a roof deck plan with the Building Permit application indicating the setback of all proposed non-habitable structures, fixed-amenities, solar and mechanical equipment from all building facades for review and approval by the Planning & Building Department. All fixed amenities and equipment shall be setback 10 ft from the building façades and all mechanical, electrical, and plumbing equipment shall be fully screened from public view.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p>17.12.030 Building Height</p> <p><i>Staff Comments</i></p> <p>Maximum Permitted: 42' Non-habitable structures located on building roof tops: 10' Roof top solar and mechanical equipment above roof surface: 5' HEIGHT OF BUILDING/CC DISTRICT: The greatest vertical distance of a building in the community core district measured by determining the average elevation of the front property line and rear property line. Draw a line from the average front or rear elevation up to the maximum building height allowed, and then draw a line at that height parallel to the front or rear property line. The resulting line establishes the highest elevation of the front or rear facade. The front or rear facade shall not extend above this line. Side facades may be stepped up or down to transition from the highest elevation of the front facade height to the highest elevation of the rear facade. One or multiple steps along the side facades are allowed, except no step shall occur within forty feet (40') of the front elevation or within thirty five feet (35') of the rear facade.</p> <p>Proposed: Building Height: 42' (Sheet A3.0)'</p>

				Non-habitable structures located on building rooftops: <i>All non-habitable structures, including the roof deck components, are within the 42 ft maximum building threshold height except for a portion of the chimney chase.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.125.030.H	Curb Cut
			<i>Staff Comments</i>	Required: <i>A total of 35% of the linear footage of any street frontage can be devoted to access to off street parking.</i> Proposed: <i>No curb cut is proposed as the parking is accessed from the adjacent alley.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.125.40	Parking Spaces
			<i>Staff Comments</i>	<i>The mix of uses in the proposed mixed-use development require 4 total parking spaces</i> 17.125.040 Off Street Parking and Loading Calculations: A.1. Multiple Uses: <i>Lots containing more than one use shall provide parking and loading in an amount equal to the total of the requirements for all uses, unless a use is exempted by this chapter or a reduction is approved through a shared parking plan or Parking Demand Analysis in compliance with this chapter.</i> Non-residential, in zoning districts other than LI-1, LI-2, and LI-3: <i>1 parking space per 1,000 gross square feet.</i> Residential multiple-family dwelling within the Community Core (CC) District and the Tourist (T) District, Tourist 3000 (T-3000), and Tourist 4000 (T-4000): <i>Units 750 square feet or less: 0 parking spaces</i> <i>Units 751 square feet to 2,000 square feet: 1 space</i> <i>Units 2,001 square feet and above: 2 parking spaces</i> 17.125.040(C) Exemptions: In the Community Core (CC) and Tourist (T) zoning districts the following uses meeting the definitions found in 17.08.020 are exempt from providing off street parking: <i>a. Community Housing</i> <i>c. The first 5,500 gross sq ft of retail trade</i> Required: <i>3 Retail Spaces (4,250 gross sq ft): Exempt</i> <i>2 Community Housing Units & Associated Storage: Exempt</i> <i>Unit 1 Residence (2,700 sq ft): 2 parking spaces</i> <i>Unit 2 Residence (2,895 sq ft): 2 parking space</i> <i>Total Parking Spaces Required On-Site: 4 parking spaces</i> Proposed: <i>The applicant has proposed 4 parking within an enclosed garage accessed off of the Block 57 alley.</i>

Table 4: Design Review Standards for all projects

Design Review Requirements				
IMPROVEMENTS AND STANDARDS: 17.96.060				
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(A)(1) Streets	The applicant shall be responsible for all costs associated with providing a connection from an existing city street to their development.
			<i>Staff Comments</i>	<i>The subject property is a corner lot with street frontage along 1st Avenue and Sun Valley Road. The applicant has proposed vehicular access to the enclosed parking garage from the Block 57 alley.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(A)(2) Streets	All street designs shall be approved by the City Engineer.
			<i>Staff Comments</i>	<i>The street design does not change with this proposal. All ROW improvements associated with the project shall be indicated on Civil Drawings submitted with the Building Permit</i>

				<p>application for review and approval by the City Engineer and Streets Department. See Table 2 for City Engineer and Streets Department comments.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(B)(1)	<p>All projects under 17.96.010(A) that qualify as a “Substantial Improvement” shall install sidewalks as required by the Public Works Department.</p> <p><i>Staff Comments</i></p> <p>Sidewalks are existing along both Sun Valley Road and 1st Avenue equal to the length of the property lines. As the project qualifies as a Substantial Improvements, all sidewalks adjacent to the property shall be improved to the City’s Right-of-Way Standards.</p> <p>Per ADA Standards sidewalks cross slopes are 1.75 +- 0.25 percent. Sidewalk along 1st Avenue indicates a cross slope of 2%, which shall be corrected on the plan submitted with the Building Permit application to be reviewed and approved by the City Engineer and Streets Department.</p> <p>8’ sidewalks are required as measured from the back of walkway to the back of curb. Civil Plans currently show 7.7’. Civil Plans shall be amended to reflect sidewalk width compliant with City Right-of-Way Standards to be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.</p> <p>The corner layout of 1st and SV Road and entrance bulb out shown on Sheet C0.1 and C1.0 of the Design Review submittal does not meet City Standards and is not approved as proposed. Bulb outs are not a City Standard, but may be considered on a case by case basis. Applicant will need to fill out a design exception request and provide a sketch or drawing of the proposed bulb out design prior to Building Permit application submittal. Design exception requests will be distributed internally for to City Departments for review and consideration.</p> <p>The sidewalks must provide a minimum 5 ft clearance around all obstacles (street trees and grates, lights, etc.). All sidewalks shall be constructed to City standards contained in KMC §12.04.030.M as well as all applicable City right-of-way standards. City Engineer & Streets Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project.</p> <p>See Table 2 for comment from the City Engineer & Streets Department.</p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	17.96.060 (B)(2)c	<p>Sidewalk width shall conform to the City’s right-of-way standards, however the City Engineer may reduce or increase the sidewalk width and design standard requirements at their discretion.</p> <p><i>Staff Comments</i></p> <p>8’ sidewalks are required as measured from the back of walkway to the back of curb. Civil Plans currently show 7.7’. Civil Plans shall be amended to reflect sidewalk width compliant with City Right-of-Way Standards to be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.</p> <p>The corner layout of 1st and SV Road and entrance bulb out shown on Sheet C0.1 and C1.0 of the Design Review submittal does not meet City Standards and is not approved as proposed. Bulb outs are not a City Standard, but may be considered on a case by case basis. Applicant will need to fill out a design exception request and provide a sketch or drawing of the proposed bulb out design prior to Building Permit application submittal. Design exception requests will be distributed internally for to City Departments for review and consideration.</p> <p>The sidewalks must provide a minimum 5 ft clearance around all obstacles (street trees and grates, lights, etc.). All sidewalks shall be constructed to City standards contained in KMC §12.04.030.M as well as all applicable City right-of-way standards.</p>

				<i>City Engineer & Streets Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project. See Table 2 for comment from the City Engineer and Streets Department.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060 (B)(3)	<p>Sidewalks may be waived if one of the following criteria is met:</p> <ul style="list-style-type: none"> a. The project comprises an addition of less than 250 square feet of conditioned space. b. The City Engineer finds that sidewalks are not necessary because of existing geographic limitations, pedestrian traffic on the street does not warrant a sidewalk, or if a sidewalk would not be beneficial to the general welfare and safety of the public.
			<i>Staff Comments</i>	<i>N/A. The project qualifies as a substantial improvement.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060 (B)(4)	<p>The length of sidewalk improvements constructed shall be equal to the length of the subject property line(s) adjacent to any public street or private street.</p>
			<i>Staff Comments</i>	<p><i>The applicant shall improve the sidewalk to City ROW standards equal to the length of the subject property lines adjacent to Sun Valley Road and 1st Avenue.</i></p> <p><i>City Engineer & Streets Department requirements and associated specifications for the required improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project. See Table 2 for comment from the City Engineer and Streets Department.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060 (B)(5)	<p>New sidewalks shall be planned to provide pedestrian connections to any existing or future sidewalks adjacent to the site. In addition, sidewalks shall be constructed to provide safe pedestrian access to and around a building.</p>
			<i>Staff Comments</i>	<i>The proposed sidewalk shall provide a pedestrian connection to the existing sidewalks along Sun Valley Road and 1st Avenue. The sidewalk will provide pedestrian access to and around the building by connecting to the retail entrances and the residential entryways.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060 (B)(6)	<p>The City may approve and accept voluntary cash contributions in-lieu of the above described improvements, which contributions must be segregated by the City and not used for any purpose other than the provision of these improvements. The contribution amount shall be one hundred ten percent (110%) of the estimated costs of concrete sidewalk and drainage improvements provided by a qualified contractor, plus associated engineering costs, as approved by the City Engineer. Any approved in-lieu contribution shall be paid before the City issues a certificate of occupancy.</p>
			<i>Staff Comments</i>	<i>N/A. Staff does not recommend a voluntary cash contribution in-lieu of improvements for this project.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(C)(1)	<p>All storm water shall be retained on site.</p>
			<i>Staff Comments</i>	<i>All storm water shall be retained on site including water from roof drains. The grading and drainage plan is indicated on Sheet C1.0 and C2.0 of the submittal. The proposed drainage plan is comprised of a system of drywells and catch basins. A final drainage plan shall be submitted with the Building Permit application for verification, review, and approval by the City Engineer and Streets Department.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(C)(2)	<p>Drainage improvements constructed shall be equal to the length of the subject property lines adjacent to any public street or private street.</p>
			<i>Staff Comments</i>	<i>See above Staff comment for Ketchum Municipal Code §17.96.060(C)(1). All drainage improvements shall require approval from the City Engineer and Streets Department prior to issuance of a Building Permit for the project.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(C)(3)	<p>The City Engineer may require additional drainage improvements as necessary, depending on the unique characteristics of a site.</p>
			<i>Staff Comments</i>	<i>See above Staff comment for Ketchum Municipal Code §17.96.060(C)(1). A final drainage plan shall be submitted with the Building Permit application for verification, review, and approval by the City Engineer and Streets Department.</i>
			17.96.060(C)(4)	Drainage facilities shall be constructed per City standards.

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<i>Staff Comments</i>	<i>The applicant must submit final civil drawings for the required Drainage Plan with the Building Permit application. Drainage facilities shall be constructed per City standards and require approval from the City Engineer and Streets Department prior to issuance of a Building Permit for the project.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(D)(1)	All utilities necessary for the development shall be improved and installed at the sole expense of the applicant.
			<i>Staff Comments</i>	<i>All utilities necessary for the development shall be improved and installed at the sole expense of the applicant. Prior to issuance of a Building Permit for the project, the applicant shall submit a will serve letter from Idaho Power. The applicant must submit a Utilities Plan at Design Review. See Staff comment from the Utilities Department in Table 2. Requirements and specifications for the water and sewer connections will be verified, reviewed, and approved by the Utilities Department prior to issuance of a Building Permit for the project.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(D)(2)	Utilities shall be located underground and utility, power, and communication lines within the development site shall be concealed from public view.
			<i>Staff Comments</i>	<i>Utilities shall be located underground and utility, power, and communication lines within the development site shall be concealed from public view. See Staff comment from the Utilities Department in Table 2.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(D)(3)	When extension of utilities is necessary all developers will be required to pay for and install two (2") inch SDR11 fiber optical conduit. The placement and construction of the fiber optical conduit shall be done in accordance with city of Ketchum standards and at the discretion of the City Engineer.
			<i>Staff Comments</i>	<i>When extension of utilities is necessary all developers will be required to pay for and install two (2") inch SDR11 fiber optical conduit. The placement and construction of the fiber optical conduit shall be done in accordance with city of Ketchum standards and at the discretion of the City Engineer.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(E)(1)	The project's materials, colors and signing shall be complementary with the townscape, surrounding neighborhoods and adjoining structures.
			<i>Staff Comments</i>	<i>The proposed changes include a complete overhaul to the existing façade materials and colors. Proposed exterior materials include vertical wood siding, aluminum windows, metal awnings, structural steel framing, plaster soffit, stone veneer, and metal panels. Proposed colors include light gray, bronze, and black. The proposed materials and colors. The natural materials and color palate complement adjacent structures within Block 57 as well as Ketchum's mountain backdrop and natural surroundings. The modern design references Ketchum's mining history through the use of bronzed metal panels and steel components. The signage plan has not been finalized, but the elevations on Sheet A4.0 indicate their placement along the front and street side facades. Prior to sign installation, the applicant shall submit a Sign Permit application for review and approval by the Planning & Building Department (KMC §17.127.020).</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060(E)(2)	Preservation of significant landmarks shall be encouraged and protected, where applicable. A significant landmark is one which gives historical and/or cultural importance to the neighborhood and/or community.
			<i>Staff Comments</i>	<i>N/A. There are no identified landmarks on the property.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060(E)(3)	Additions to existing buildings, built prior to 1940, shall be complementary in design and use similar material and finishes of the building being added to.
			<i>Staff Comments</i>	<i>N/A as the existing building was built in 1967.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(1)	Building(s) shall provide unobstructed pedestrian access to the nearest sidewalk and the entryway shall be clearly defined.
			<i>Staff Comments</i>	<i>The building design provides unobstructed pedestrian access from the residential entryways and retail storefront to the sidewalks along Sun Valley Road and 1st Avenue. The entryways are defined by steel canopies and metal awnings. Awnings and canopy elements that extend over the property line into the City ROW may require a ROW Encroachment Permit to be reviewed by the City Engineer and Streets Department.</i>
			17.96.060(F)(2)	The building character shall be clearly defined by use of architectural features.

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<i>Staff Comments</i>	<i>The building design is defined by vertical elements, such as the wood siding, and horizontal elements, such as the terrace guardrails and canopy elements, that provide undulation and relief to the front and street facing facades, The chimney chases clad in stone veneer echoes the residential entryway. The applicant has proposed both storefront windows and fenestration at the upper level, which invites pedestrians into the retail spaces and celebrates Ketchum's natural surroundings. The varying flat roof height further distinguish the building.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(3)	There shall be continuity of materials, colors and signing within the project.
			<i>Staff Comments</i>	<i>The wood, steel, metal, and stone materials and natural colors are consistent along the front, street side, and rear façade, The interior façade incorporates the vertical wood siding, metal wall cladding, and windows as relief from the painted CMU wall. The condominium penthouses at the upper level are distinguished from the lower level by the lighter gray finish associated with the wood siding. The lower level façade design is consistent across the residential and retail areas through the use of similar materials, such as the metal panels.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(4)	Accessory structures, fences, walls and landscape features within the project shall match or complement the principal building.
			<i>Staff Comments</i>	<i>No accessory structures, fences, or walls have been proposed with the mixed-use building project. The landscape features including the proposed street trees and rooftop planters soften the mass of the building while fostering a pedestrian oriented streetscape.</i> <i>The sidewalks must provide a minimum 5 ft clearance around all obstacles (street trees and grates, lights, etc.). City Engineer & Streets Department requirements and associated specifications for the required ROW improvements must be verified, reviewed, and approved prior to issuance of a Building Permit for the project.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(5)	Building walls shall provide undulation/relief, thus reducing the appearance of bulk and flatness.
			<i>Staff Comments</i>	<i>The proposed elevation views provided by the applicant demonstrate that all building walls provide undulation and relief, serving to reduce the appearance of bulk and flatness at all façades. The material differentiation in conjunction with the horizontal and vertical elements as well as the canopy projections and significant fenestration provide undulation and relief and reduce the appearance of bulk and flatness.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(6)	Building(s) shall orient towards their primary street frontage.
			<i>Staff Comments</i>	<i>The proposed building has two street-fronting facades adjacent to Sun Valley Road and N 1st Avenue. The building orients towards the primary street frontages.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(7)	Garbage storage areas and satellite receivers shall be screened from public view and located off alleys.
			<i>Staff Comments</i>	<i>The garbage storage area, indicated on Sheet A0.1, is located off the alley and includes a pivot system. The applicant shall submit a will-serve letter from Clear Creek Disposal prior to issuance of a Building Permit for the project.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(8)	Building design shall include weather protection which prevents water to drip or snow to slide on areas where pedestrians gather and circulate or onto adjacent properties.
			<i>Staff Comments</i>	<i>The project design incorporates sloped flat roofs as well as canopy and overhang elements at the residential and retail entrances. These architectural features not only define the building entryways, but also serve as weather protection to prevent water to drip and snow to slide on areas where pedestrians may gather and circulate.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(G)(1)	Pedestrian, equestrian and bicycle access shall be located to connect with existing and anticipated easements and pathways.
			<i>Staff Comments</i>	<i>The proposed mixed-use building provides unobstructed pedestrian access to the proposed improved sidewalks adjacent to 1st Avenue and Sun Valley Road. As noted on Sheet A0.1 and C1.0, the sidewalk design will connect with existing sidewalks along 1st Avenue and Sun valley Road.</i>

				<p>The applicant is required to install one (1) bicycle rack, able to accommodate at least two (2) bicycles, for every four (4) parking spaces as required by the proposed use (KMC §17.96.070.F.1). The applicant has proposed the installation of two bike racks within the public ROW. The final siting of the bicycle rack shall be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.</p> <p>No equestrian access is proposed or required.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(G)(2)	<p>Awnings extending over public sidewalks shall extend five (5') feet or more across the public sidewalk but shall not extend within two (2') feet of parking or travel lanes within the right of way.</p> <p><i>Staff Comments</i> The Architectural Site Plan on Sheet A0.1 indicates the line of the proposed awning that extends across the property line and over the public ROW. All overhang and canopy elements extending over the property line shall be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project. A ROW encroachment permit may be required for all encroachments overhanging into the public ROW.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(G)(3)	<p>Traffic shall flow safely within the project and onto adjacent streets. Traffic includes vehicle, bicycle, pedestrian and equestrian use. Consideration shall be given to adequate sight distances and proper signage.</p> <p><i>Staff Comments</i> This standard has been met. Pedestrian access is provided via sidewalk and vehicle access is taken from the alley only.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060(G)(4)	<p>Curb cuts and driveway entrances shall be no closer than twenty (20') feet to the nearest intersection of two or more streets, as measured along the property line adjacent to the right of way. Due to site conditions or current/projected traffic levels or speed, the City Engineer may increase the minimum distance requirements.</p> <p><i>Staff Comments</i> N/A as access to the parking garage is provided from the alley.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(G)(5)	<p>Unobstructed access shall be provided for emergency vehicles, snowplows, garbage trucks and similar service vehicles to all necessary locations within the proposed project.</p> <p><i>Staff Comments</i> The building may be accessed from Sun Valley Rd, 1st Avenue, and the alleyway. Unobstructed access is provided for emergency vehicles, snowplows, and garbage trucks.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060(H)(1)	<p>Snow storage areas shall not be less than thirty percent (30%) of the improved parking and pedestrian circulation areas.</p> <p><i>Staff Comments</i> Snow removal on site as well as from the sidewalk within the public ROW is the responsibility of the property owner. As permitted by KMC §17.96.060(H)(4), the applicant has proposed a snowmelt system in lieu of providing a snow storage area on the site. The proposed system will require a ROW encroachment permit to be reviewed and approved by the City.</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060(H)(2)	<p>Snow storage areas shall be provided on-site.</p> <p><i>Staff Comments</i> See above Staff comment for Ketchum Municipal Code §17.96.060(H)(1).</p>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060(H)(3)	<p>A designated snow storage area shall not have any dimension less than five (5') feet and shall be a minimum of twenty five (25) square feet.</p> <p><i>Staff Comments</i> See above Staff comment for Ketchum Municipal Code §17.96.060(H)(1).</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(H)(4)	<p>In lieu of providing snow storage areas, snow melt and hauling of snow may be allowed.</p> <p><i>Staff Comments</i> Snow removal on site as well as from the sidewalk within the public ROW is the responsibility of the property owner. The applicant has proposed a snowmelt system in lieu of providing a snow storage area on the site. The proposed system will require a ROW encroachment permit to be reviewed and approved by the City. See Table 2 for comment from the City Engineer and Streets Department.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(I)(1)	<p>Landscaping is required for all projects.</p>

			<i>Staff Comments</i>	<i>The landscape plan is included on Sheet A0.1. The applicant has proposed 5 street trees, which are specified as 3" caliper autumn blaze maples. All street trees will require electrical outlets and irrigation. The applicant has also proposed planters at the rooftop terrace.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(I)(2)	Landscape materials and vegetation types specified shall be readily adaptable to a site's microclimate, soil conditions, orientation and aspect, and shall serve to enhance and complement the neighborhood and townscape.
			<i>Staff Comments</i>	<i>The applicant shall specify the materials and vegetation for the rooftop planters with the Building Permit application. The landscape materials and vegetation types shall be reviewed and approved by the City Arborist prior to issuance of a Building Permit for the project. The landscape materials and vegetation types specified shall be readily adaptable to the site's microclimate, soil conditions, orientation and aspect, and shall serve to enhance and complement the neighborhood and townscape.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(I)(3)	All trees, shrubs, grasses and perennials shall be drought tolerant. Native species are recommended but not required.
			<i>Staff Comments</i>	<i>All proposed landscape materials and vegetation types shall be drought tolerant. The applicant is encouraged to select native species.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(I)(4)	Landscaping shall provide a substantial buffer between land uses, including, but not limited to, structures, streets and parking lots. The development of landscaped public courtyards, including trees and shrubs where appropriate, shall be encouraged.
			<i>Staff Comments</i>	<i>The subject site is within the Community Core surrounded by commercial development. The proposed street trees provide a buffer from 1st Avenue and Sun Valley Road and enhance the pedestrian experience.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(J)(1)	Where sidewalks are required, pedestrian amenities shall be installed. Amenities may include, but are not limited to, benches and other seating, kiosks, bus shelters, trash receptacles, restrooms, fountains, art, etc. All public amenities shall receive approval from the Public Works Department prior to design review approval from the Commission.
			<i>Staff Comments</i>	<i>As indicated on Sheet A2.0, the applicant has proposed new bike racks, a bench, and trash receptacle within a proposed bulb out. The corner layout of 1st and SV Road and entrance bulb out shown the Design Review plans does not meet City Standards and is not approved as proposed. Bulb outs are not a City Standard, but may be considered on a case by case basis. Applicant will need to fill out a design exception request and provide a sketch or drawing of the proposed bulb out design prior to Building Permit application submittal. Design exception requests will be distributed internally for to City Departments for review and consideration. The siting of public amenities within the public ROW require review and approval by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.</i>

Table 5: Design Review Standards for Community Core Projects

IMPROVEMENTS AND STANDARDS: 17.96.070 - Community Core (CC) Projects				
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.070 A(1)	Street trees, street lights, street furnishings, and all other street improvements shall be installed or constructed as determined by the Public Works Department.
			<i>Staff Comments</i>	<i>The applicant proposes to install street trees (5), a trash receptacle, and a bench within the public ROW). Per the City Engineer and Streets Department comments in Table 2, the applicant will be required to install street lights. Street light locations are subject to a lighting study provided by the manufacturer of the city's standard streetlight.</i> <i>Final location and details for the right-of-way items will occur through Building Permit review.</i>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.070(A)(2) Streets	Street trees with a minimum caliper size of three (3") inches, shall be placed in tree grates.
			<i>Staff Comments</i>	<i>The applicant has proposed the installation of 3" caliper Autumn Blaze Maple trees and has indicated the installation will meet all applicable City standards, which include tree grates, irrigation, and associated lighting.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.070(A)(3)	Due to site constraints, the requirements if this subsection 17.96.070. (A) may be modified by the Public Works Department.
			<i>Staff Comments</i>	<i>N/A as the requirements of subsection 17. 96.070.A. have not been waived for the subject project. Final specifications will be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.070 (B)(1)	Facades facing a street or alley or located more than five (5') feet from an interior side property line shall be designed with both solid surfaces and window openings to avoid the creation of blank walls and employ similar architectural elements, materials, and colors as the front façade.
			<i>Staff Comments</i>	<i>The street facing and alley facing facades have been designed to include both solid surfaces and window/door openings intended to avoid the creation of blank walls. While built to the interior property line, the side façade includes a limited degree of material differentiation through the incorporation of windows at the upper level and metal panels to provide relief to the painted CMU wall. Similar architectural elements used to create uniformity include a continuity of materials as well as canopy and overhang elements.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	17.96.070 (B)(2)	For nonresidential portions of buildings, front building facades and facades fronting a pedestrian walkway shall be designed with ground floor storefront windows and doors with clear transparent glass. Landscaping planters shall be incorporated into facades fronting pedestrian walkways.
			<i>Staff Comments</i>	<i>The applicant has proposed storefront windows for the three (3) commercial spaces adjacent to Sun Valley Rd and 1st Avenue. The applicant has incorporated landscape planters at the roof top terraces, which in conjunction with the street trees, softens the mass of the building façade and enhances the pedestrian environment.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.070 (B)(3)	For nonresidential portions of buildings, front facades shall be designed to not obscure views into windows.
			<i>Staff Comments</i>	<i>The design of the front and street facing façades (N 1st Avenue and Sun Valley Road) do not obscure views into windows. The design of the front and street facing façades incorporate generous window openings providing views into the commercial space.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.070 (B)(4)	Roofing forms and materials shall be compatible with the overall style and character of the structure. Reflective materials are prohibited.
			<i>Staff Comments</i>	<i>The proposed design of the mixed-use building incorporates a flat roof. As indicated on Sheet A2.1 the roofing material is proposed to be a Class "A" ballasted EPDM membrane. Reflective materials are prohibited.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.070 (B)(5)	All pitched roofs shall be designed to sufficiently hold all snow with snow clips, gutters, and downspouts.
			<i>Staff Comments</i>	<i>N/A as the project design does not incorporate any pitched roofs.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.070 (B)(6)	Roof overhangs shall not extend more than three (3') feet over a public sidewalk. Roof overhangs that extend over the public sidewalk shall be approved by the Public Works Department.
			<i>Staff Comments</i>	<i>The applicant has proposed canopy and overhang elements over the public sidewalk. Awnings are permitted to extend 5 ft over a public sidewalk. All overhangs and canopy elements shall be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.070 (B)(7)	Front porches and stoops shall not be enclosed on the ground floor by permanent or temporary walls, windows, window screens, or plastic or fabric materials.
			<i>Staff Comments</i>	<i>N/A. Front porches or stoops are not incorporated in the project design.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.070(C)(1)	Trash disposal areas and shipping and receiving areas shall be located within parking garages or to the rear of buildings. Trash disposal areas shall not be located within the public right of way and shall be screened from public views.

			<i>Staff Comments</i>	<i>The trash disposal area is located at the rear of the building adjacent to the alley and will be screened from public view.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.070(C)(2)	Roof and ground mounted mechanical and electrical equipment shall be fully screened from public view. Screening shall be compatible with the overall building design.
			<i>Staff Comments</i>	<i>The applicant is aware of this requirement. At this time, plans do not indicate how roof-mounted or ground mounted equipment will be screened. Screening will be verified prior to issuance of Building Permit for the project. The applicant shall note the setback of all mechanical equipment and fixed/unconditioned structures on the roof plan at Design Review.</i> <i>The final screening, location, and height of all ground and roof mounted mechanical equipment shall be reviewed prior to the issuance of a Building Permit and approved at Final Inspection prior to issuance of a Certificate of Occupancy for the project.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.070(D)(1)	When a healthy and mature tree is removed from a site, it shall be replaced with a new tree. Replacement trees may occur on or off site.
			<i>Staff Comments</i>	<i>No mature trees are being removed from the site.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.070(D)(2)	Trees that are placed within a courtyard, plaza, or pedestrian walkway shall be placed within tree wells that are covered by tree grates.
			<i>Staff Comments</i>	<i>Sidewalk trees are proposed to be installed per City standards, which includes wells, grates, irrigation, and lighting. Trees to be located on the upper story roof deck are proposed to be located in planters.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.070(D)(3)	The city arborist shall approve all parking lot and replacement trees.
			<i>Staff Comments</i>	<i>N/a as no parking lot or replacement trees are proposed or required.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.070(E)(1)	Surface parking lots shall be accessed from off the alley and shall be fully screened from the street.
			<i>Staff Comments</i>	<i>N/A. No surface parking lots are proposed with the subject submittal.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.070(E)(2)	Surface parking lots shall incorporate at least one (1) tree and one (1) additional tree per ten (10) onsite parking spaces. Trees shall be planted in landscaped planters, tree wells and/or diamond shaped planter boxes located between parking rows. Planter boxes shall be designed so as not to impair vision or site distance of the traveling public.
			<i>Staff Comments</i>	<i>N/A. No surface parking lots are proposed with the subject submittal.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.070(E)(3)	Ground cover, low lying shrubs, and trees shall be planted within the planters and planter boxes. Tree grates or landscaping may be used in tree wells located within pedestrian walkways.
			<i>Staff Comments</i>	<i>Sidewalk trees are proposed to be installed per City standards, which includes wells, grates, irrigation, and lighting. Landscaping sited at the upper story roof deck are proposed to be located in planters.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.070(F)(1)	One (1) bicycle rack, able to accommodate at least two (2) bicycles, shall be provided for every four (4) parking spaces as required by the proposed use. At a minimum, one (1) bicycle rack shall be required per development.
			<i>Staff Comments</i>	<i>The applicant has proposed two (2) bike racks each able to accommodate two (2) bicycles. Only one (1) bicycle rack is required.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.070(F)(2)	When the calculation of the required number of bicycle racks called for in this section results in a fractional number, a fraction equal to or greater than one-half (1/2) shall be adjusted to the next highest whole number.
			<i>Staff Comments</i>	<i>See Staff comment above. The fraction of the calculation is not equal to or greater than one-half.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.070(F)(3)	Bicycle racks shall be clearly visible from the building entrance they serve and not mounted less than fifty (50') feet from said entrance or as close as the nearest non-ADA parking space, whichever is closest. Bicycle racks shall be located to achieve unobstructed access from the public right-of-way and not in areas requiring access via stairways or other major obstacles.

			<i>Staff Comments</i>	<i>The bicycle rack is located adjacent to the front entry at 1st Avenue but does not obstruct the front entry or access to the door. Final siting of the proposed bike racks shall be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit for the project.</i>
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STAFF RECOMMENDATION:

Staff recommends the Planning & Zoning Commission consider the analysis contained in the Tables 1-5 of the Staff Report, the applicant submittal package included as Exhibit A, and any public comment received, deliberate, and move to approve the Design Review for the Sun Valley & First mixed-use building project.

RECOMMENDED MOTION

“I move to approve the Sun Valley & First Design Review application subject to conditions and authorize the chair to sign the Findings of Fact and Conclusions of Law”

RECOMMENDED CONDITIONS

1. This Design Review approval is subject to all comments and conditions as described in Tables 2, 3, 4, and 5.
2. All roof mounted and ground mounted mechanical equipment, including plumbing and ventilation stacks, shall be screened.
3. This Design Review approval is based on the plans and information presented and approved at the meeting on the date noted herein, August 13, 2019. Building Permit plans for all on-site improvements must conform to the approved Design Review plans unless otherwise approved in writing by the Planning and Zoning Commission or Administrator. Any building or site discrepancies which do not conform to the approved plans will be subject to removal.
4. All governing ordinances, requirements, and regulations of the Fire Department (2012 International Fire Code and local Fire Protection Ordinance No.1125), Building Department (2012 International Building Code, the 2012 International Residential Code, and Title 15 of Ketchum Municipal Code), Utilities Department, Street Department (Title 12 of Ketchum Municipal Code), and the City Engineer shall be met prior to Certificate of Occupancy.
5. The term of Design Review approval shall be twelve (12) months from the date that the Findings of Fact, Conclusions of Law, and Decision are adopted by the Commission or upon appeal, the date the approval is granted by the Council subject to changes in zoning regulations (KMC §17.96.090).
6. All Design Review elements shall be completed prior to issuance of a Certificate of Occupancy for the building.
7. All exterior lighting on the property shall be in compliance with Ketchum Municipal Code, Chapter 17.132, Dark Skies, and shall be inspected by Planning Staff and approved prior the issuance of a Certificate of Occupancy for the building.
8. As a voluntary contribution, in exchange for an increase in FAR, a total community housing contribution of 1,029 sq ft is required. An exceedance agreement between the applicant and the City regarding the community housing contribution shall be signed prior to issuance of a Building Permit for the project.
9. In addition to the requirements set forth in this Design Review approval, this project shall comply with all applicable local, state, and federal laws.

Exhibits:

- A. Sun Valley and First Design Review Applicant Submittal Package

Exhibit A:
Sun Valley and First
Design Review
Applicant Submittal Package



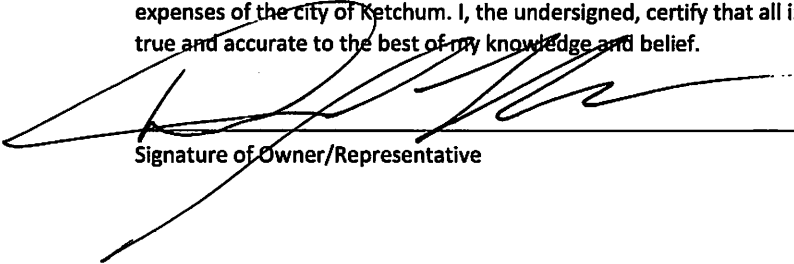
City of Ketchum
Planning & Building

OFFICIAL USE ONLY	
File Number:	P19-083
Date Received:	7-18-19
By:	mp
Pre Application Fee Paid:	
Design Review Fee Paid:	2944.50
Approved Date:	
Denied Date:	
By:	
ADRC: Yes <input type="checkbox"/>	

Design Review Application

APPLICANT INFORMATION			
Project Name: SUN VALLEY & 1ST		Phone: 208 726 9776	
Owner: DAVID I. WILSON		Mailing Address: P.O. BOX 6770, KETCHUM, ID 83340	
Email: dwilson@wilsonconstructions.com			
Architect/Representative: RUSCITTO LATHAM BLANTON ARCHITECTURA P.A.		Phone: 208 726 5608	
Email: buffalo@rlb-sv.com		Mailing Address: P.O. BOX 419, SUN VALLEY, ID 83353	
Architect License Number: AR-2529			
Engineer of Record: RUSCITTO LATHAM BLANTON ARCHITECTURA P.A.		Phone: 208 726 5608	
Email: scott@rlb-sv.com		Mailing Address: P.O. BOX 419, SUN VALLEY, ID 83353	
Engineer License Number: 59431-2203			
All design review plans and drawings for public commercial projects, residential buildings containing more than four (4) dwelling units and development projects containing more than four (4) dwelling units shall be prepared by an Idaho licensed architect or an Idaho licensed engineer.			
PROJECT INFORMATION			
Legal Land Description: LOT 4, E 50' OF LOTS 7 & 8, BLOCK 57, CITY OF KETCHUM			
Street Address: 311 FIRST AVENUE N			
Lot Area (Square Feet): 8,250			
Zoning District: (CC) COMMUNITY CORE - MIXED US SUBDISTRICT			
Overlay District: <input type="checkbox"/> Floodplain <input type="checkbox"/> Avalanche <input type="checkbox"/> Mountain			
Type of Construction: <input type="checkbox"/> New <input checked="" type="checkbox"/> Addition <input checked="" type="checkbox"/> Remodel <input type="checkbox"/> Other			
Anticipated Use: RETAIL/RESIDENTIAL		Number of Residential Units: (4)	
TOTAL FLOOR AREA			
	Proposed	Existing	
Basements		Sq. Ft.	Sq. Ft.
1 st Floor	7,940	Sq. Ft.	7,755
2 nd Floor	6,140	Sq. Ft.	N/A
3 rd Floor	225	Sq. Ft.	Sq. Ft.
Mezzanine		Sq. Ft.	Sq. Ft.
Total	14,305	Sq. Ft.	7,755
FLOOR AREA RATIO			
Community Core: 1.74		Tourist: _____	
		General Residential-High: _____	
BUILDING COVERAGE/OPEN SPACE			
Percent of Building Coverage: 96%			
DIMENSIONAL STANDARDS/PROPOSED SETBACKS			
Front: 0		Side: 0	
		Side: 0	
		Rear: 3'-0"	
Building Height: _____			
OFF STREET PARKING			
Parking Spaces Provided: 4			
Curb Cut: _____ Sq. Ft.		%	
WATER SYSTEM			
<input checked="" type="checkbox"/> Municipal Service		<input type="checkbox"/> Ketchum Spring Water	

The Applicant agrees in the event of a dispute concerning the interpretation or enforcement of the Design Review Application in which the city of Ketchum is the prevailing party, to pay the reasonable attorney fees, including attorney fees on appeal and expenses of the city of Ketchum. I, the undersigned, certify that all information submitted with and upon this application form is true and accurate to the best of my knowledge and belief.

A handwritten signature in black ink, consisting of several loops and a long horizontal stroke, positioned above a horizontal line.

Signature of Owner/Representative

7/18/19
Date

July 18, 2019

To: City of Ketchum
Planning Department

From: Geneva Plaza LLC

Re: 311 First Ave north
Ketchum, ID 83340

To Whom It May Concern,

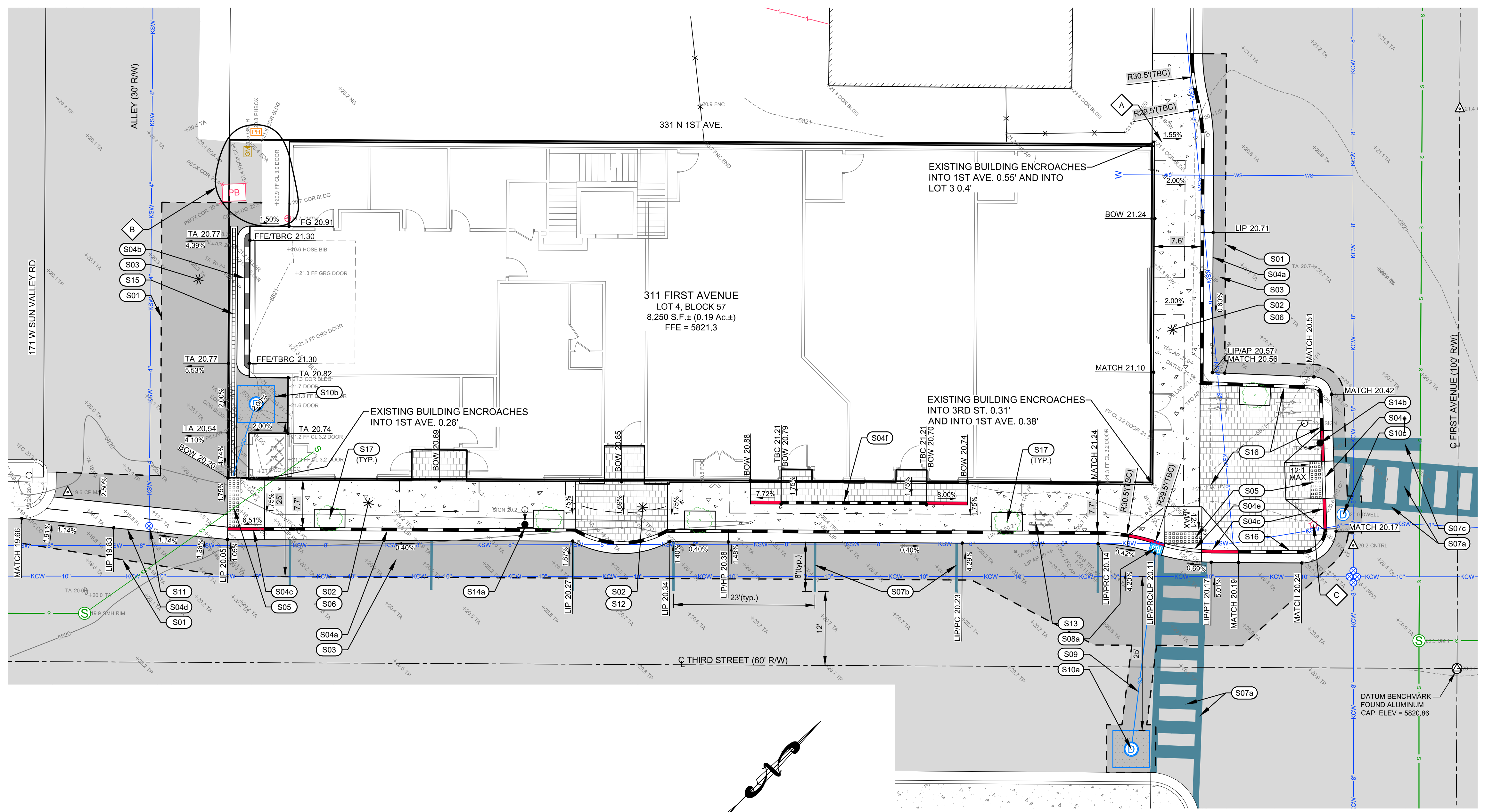
As the managing member of Geneva Plaza LLC, the owners of the above referenced property, this letter will serve as written confirmation that the architecture firm of Ruscitto, Latham and Blantam has our permission to submit plans for the redevelopment of the above referenced property for the benefit of the soon to be new owner, Dave Wilson. Please feel free to contact me if you have any questions. Thank you for your cooperation in this matter.

Sincerely,



Steve Giacobbi
Managing Member
Geneva Plaza LLC

NO.	DATE	BY	REVISIONS

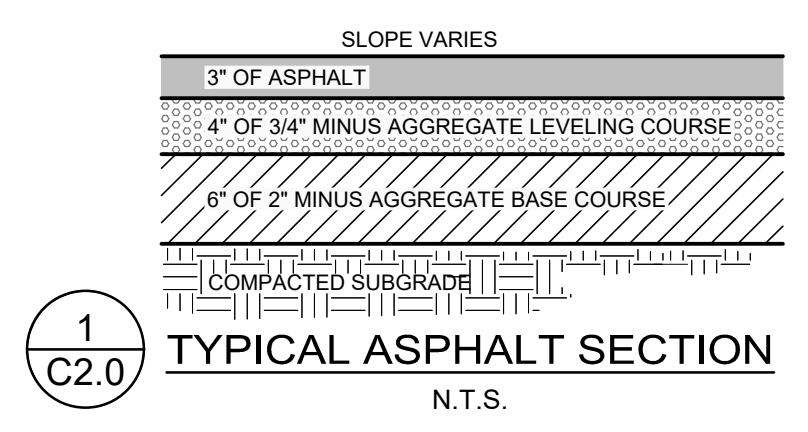


LEGEND		PROPOSED ITEMS	
	Property Line		Asphalt
	Adjoiner's Lot Line		Pavers
	Centerline		Concrete Sidewalk
	FDAC = Found Aluminum Cap		Concrete 6" Vertical Curb
	CHTR = Survey Control		Concrete Rolled Curb
	S Contour Interval		Curb Transition
	1' Contour Interval		ADA Access Truncated Dome
	Curb & Gutter		Sign
	FNC = Fence Line		Drywell
	BLDG = Building		Storm Drain
	Asphalt		Catch Basin
	Concrete Sidewalk		Sawed Line
	Road Overhang		Road Paint
	SGN = Sign		Grada
	GMTR = Gas Meter		Spot Elevation
	PHBOX = Telephone Riser		Tree and Tree Grate
	Overhead Power Line		Trench Drain
	Light		
	PBOX = Power Box		
	PMTR = Power Meter		
	PP = Power Pole		
	GLY = Claycrete		
	Sewer Main		
	SMH = Sewer Manhole		
	SS = Sewer Service		
	DWELL = Dry Well		
	WV = Water Valve		
	Ketchum City Water Line (10")		
	Ketchum City Water Line (8")		
	Ketchum Spring Line (8")		
	Ketchum Spring Line (4")		
	Water Service		
	Hose Bib		

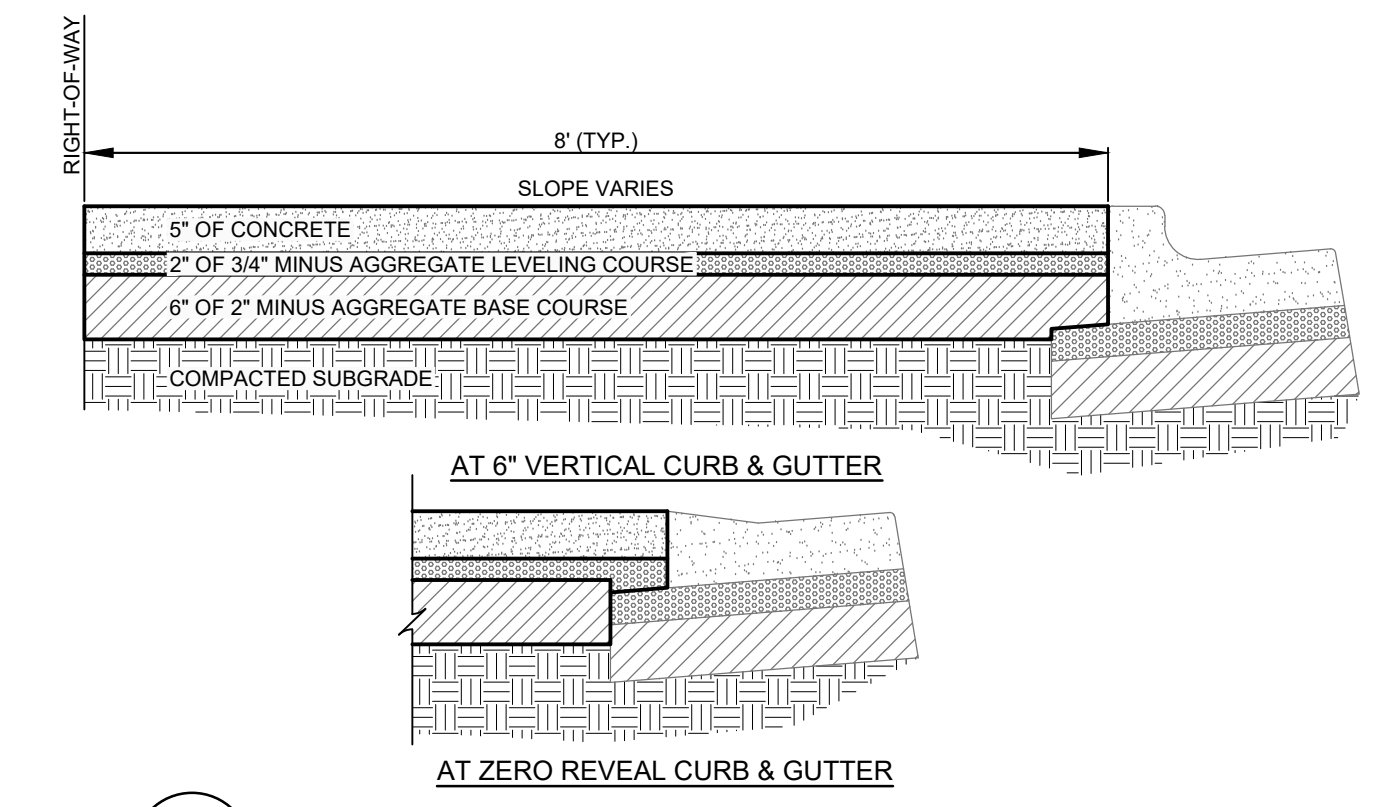
- ### CONSTRUCTION NOTES
- ALL CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE MOST CURRENT EDITION OF THE "IDAHO REGULATIONS FOR PUBLIC DRINKING WATER SYSTEMS," THE CURRENT EDITION OF THE "IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION" (ISPMC), AND CITY OF KETCHUM STANDARDS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING AND KEEPING A COPY OF THE ISPMC ON SITE DURING CONSTRUCTION.
 - THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE SHOWN ON THE PLANS IN AN APPROXIMATE WAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING EXISTING UTILITIES PRIOR TO COMMENCING AND DURING THE CONSTRUCTION. THE CONTRACTOR AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH RESULT FROM HIS FAILURE TO ACCURATELY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. CONTRACTOR SHALL CALL DIGLINE (1-800-342-1585) TO LOCATE ALL EXISTING UNDERGROUND UTILITIES.
 - THE CONTRACTOR SHALL CLEAN UP THE SITE AFTER CONSTRUCTION SO THAT IT IS IN A CONDITION EQUAL TO OR BETTER THAN THAT WHICH EXISTED PRIOR TO CONSTRUCTION, INCLUDING BUT NOT LIMITED TO, EPA'S NPDES CONSTRUCTION GENERAL PERMIT.
 - THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION.
 - CONSTRUCTION OF WATER MAINS AND ALL OTHER RELATED APPURTENANCES SHALL BE IN ACCORDANCE WITH THE IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION (ISPMC), IDAPA 58.01.08, IDAHO RULES FOR PUBLIC DRINKING WATER SYSTEMS AND THE CITY OF KETCHUM UTILITIES DEPARTMENT STANDARDS.
 - CONTRACTOR SHALL PRESSURE TEST, DISINFECT, AND CONDUCT BIOLOGICAL TESTING IN ACCORDANCE WITH THE IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION (ISPMC), AMERICAN WATER WORKS ASSOCIATION (AWWA) STANDARDS, AND THE PRESSURE TESTING, DISINFECTION, AND MICROBIOLOGICAL TESTING PROCEDURES.
 - ALL WATER SUPPLY FIXTURES, FITTINGS, PIPING, AND ALL RELATED APPURTENANCES SHALL BE ANSISF STD. 61 COMPLIANT.
 - ALL WATER SUPPLY FIXTURES, FITTINGS, PIPING, AND ALL RELATED APPURTENANCES SHALL COMPLY WITH THE LOW LEAD ACT REQUIRING ALL MATERIALS TO HAVE A LEAD CONTENT EQUAL TO OR LESS THAN 0.25%.
 - THE CONTRACTOR SHALL USE ANSISF STANDARD 60 CHEMICALS AND COMPOUNDS DURING INSTALLATION & DISINFECTION OF POTABLE WATER MAIN.
 - CONTRACTOR SHALL COORDINATE LOCATIONS OF DRY UTILITY FACILITIES (POWER, CABLE, PHONE, TV) NOT SHOWN ON THE DRAWING WITH IDAHO POWER.
 - ALL CLEARING & GRUBBING SHALL CONFORM TO ISPMC SECTION 201.
 - ALL EXCAVATION & EMBANKMENT SHALL CONFORM TO ISPMC SECTION 202. EXCAVATED SUBGRADE SHALL BE COMPACTED AND ALL UNSUITABLE SECTIONS REMOVED AND REPLACED WITH STRUCTURAL FILL AS DETERMINED BY THE ENGINEER. MINIMUM COMPACTION OF PLACED MATERIAL SHALL BE 95% OF MAXIMUM LABORATORY DENSITY AS DETERMINED BY AASHTO T-99 OR ITD T-91.
 - ALL 2" MINUS GRAVEL SHALL CONFORM TO ISPMC 802, TYPE II (ITD STANDARD 703.04, 2"). SHALL BE PLACED IN CONFORMANCE WITH ISPMC SECTION 801 AND COMPACTED PER SECTION 202. MINIMUM COMPACTION OF PLACED MATERIAL SHALL BE 90% OF MAXIMUM LABORATORY DENSITY AS DETERMINED BY AASHTO T-99.
 - ALL 3/4" MINUS CRUSHED GRAVEL SHALL CONFORM TO ISPMC 802, TYPE I (ITD STANDARD 703.04, 3/4" B). SHALL BE PLACED IN CONFORMANCE WITH ISPMC SECTION 802 AND COMPACTED PER SECTION 202. MINIMUM COMPACTION OF PLACED MATERIAL SHALL BE 95% OF MAXIMUM LABORATORY DENSITY AS DETERMINED BY AASHTO T-99 OR ITD T-91.
 - ALL ASPHALTIC CONCRETE PAVEMENT WORK SHALL CONFORM TO ISPMC SECTION(S) 805, 810, AND 811 FOR CLASS II PAVEMENT. ASPHALT AGGREGATE SHALL BE 1/2" (13MM) NOMINAL SIZE CONFORMING TO TABLE 803B IN ISPMC SECTION 803. ASPHALT BINDER SHALL BE PG 58-28 CONFORMING TO TABLE A-1 IN ISPMC SECTION 805.
 - ALL EDGES OF EXISTING ASPHALT PAVING SHALL BE SAW CUT 24" TO PROVIDE A CLEAN PAVEMENT EDGE FOR MATCHING. NO WHEEL CUTTING SHALL BE ALLOWED.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING TRAFFIC CONTROL PER THE CURRENT EDITION OF THE US DEPARTMENT OF TRANSPORTATION MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
 - ALL CONCRETE FORM WORK SHALL CONFORM TO ISPMC SECTION 701 AND 703. ALL CONCRETE SHALL BE 3,000 PSI MINIMUM, 28 DAY, AS DEFINED IN ISPMC SECTION 703, TABLE 1.C.
 - ALL TRENCHING SHALL CONFORM TO ISPMC STANDARD DRAWING SD-301. TRENCHES SHALL BE BACKFILLED AND COMPACTED TO A MINIMUM OF 95% OF MAXIMUM DENSITY AS DETERMINED BY AASHTO T-99.
 - TOPOGRAPHIC, SITE, AND BOUNDARY SURVEYS SHOWN HEREON WERE CONDUCTED BY GALENA ENGINEERING, 02/12/07. LOCATIONS OF WATER AND SEWER MAINS AND SERVICES SHOWN HEREON ARE PER THE OFFICIAL WATER AND SEWER SYSTEM MAPS PROVIDED BY THE CITY OF KETCHUM.
 - PER IDAHO CODE § 55-1913, THE CONTRACTOR SHALL RETAIN AND PROTECT ALL MONUMENTS, ACCESSORIES TO CORNERS, BENCHMARKS AND POINTS SET IN CONTROL SURVEYS. ALL MONUMENTS, ACCESSORIES TO CORNERS, BENCHMARKS AND POINTS SET IN CONTROL SURVEYS THAT ARE LOST OR DISTURBED BY CONSTRUCTION SHALL BE REESTABLISHED AND RE-MONUMENTED. AT THE EXPENSE OF THE AGENCY OR PERSON CAUSING THEIR LOSS OR DISTURBANCE AT THEIR ORIGINAL LOCATION OR BY SETTING OF A WITNESS CORNER OR REFERENCE POINT OR A REPLACEMENT BENCHMARK OR CONTROL POINT, BY OR UNDER THE DIRECTION OF A PROFESSIONAL LAND SURVEYOR.

- ### CONSTRUCTION KEYNOTES
- SAWCUT EXISTING ASPHALT TO PROVIDE FOR A CLEAN VERTICAL EDGE.
 - REMOVE AND DISPOSED OF EXISTING CURB, GUTTER, & SIDEWALK.
 - CONSTRUCT/REPAIR ASPHALT. SEE DETAIL 1 / C2.0.
 - CONSTRUCT CONCRETE CURB
 - 6" CONCRETE VERTICAL CURB AND GUTTER PER DETAIL 4 / C2.0.
 - 6" CONCRETE ROLLED CURB AND GUTTER PER DETAIL 5 / C2.0.
 - CURB TRANSITION PER DETAIL 3 / C2.0.
 - 4" WIDE CONCRETE VALLEY GUTTER PER IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION STANDARD DETAIL SD-708.
 - ZERO REVEAL CURB AND GUTTER PER DETAIL 3 / C2.0.
 - CONCRETE VERTICAL CURB PER DETAIL 12 / C2.0.
 - CONSTRUCT ADA COMPLIANT PEDESTRIAN RAMP. INSTALL TRUNCATED DOME DETECTABLE WARNING INSERT.
 - CONSTRUCT CONCRETE SIDEWALK, WIDTH AS SHOWN HEREON. SEE DETAIL 2 / C2.0.
 - INSTALL ROAD STRIPING / PAINT. OBLITERATE PREVIOUS PAVEMENT MARKINGS AS NECESSARY.
 - WHITE CROSSWALK STRIPING (12"x24" WIDE).
 - WHITE ASPHALT PARKING STRIPING (4" WIDE) MATCH CITY PATTERNS.
 - WHITE CROSSWALK / STOP BAR STRIPING (24" WIDE).
 - INSTALL CATCH BASIN PER DETAIL 8 / C2.0.
 - RIM ELEV = 5819.91
 - IE (OUT) = 5819.91
 - INSTALL 12" ADS N-12 STORM DRAIN PIPE WITH A MINIMUM SLOPE OF 2.0%. SEE DETAIL 10 / C2.0 FOR POTABLE AND NON-POTABLE WATER LINE SEPARATION AND DETAIL 7 / C2.0 FOR TRENCHING.
 - CONSTRUCT DRYWELL. SEE DETAIL 9 / C2.0.
 - RIM = MATCH EXISTING ASPHALT ELEVATION. INV. IN = 5816.24
 - RIM = 5820.69
 - CONNECT ROOF DRAINS AND TRENCH DRAIN TO DRYWELL.
 - RIM = 5820.0 CLEAN AND/OR RECONSTRUCT EXISTING DRYWELL.
 - RELOCATE GAND/OR VALVE OUT OF VALLEY GUTTER. COORDINATE WORK WITH CITY OF KETCHUM WATER DEPARTMENT.
 - CONSTRUCT PAVEMENT SIDEWALK, WIDTH AS SHOWN HEREON. SEE DETAIL 6 / C2.0.
 - INSTALL BIKE RACK / BOLLARD. SEE ARCHITECTURAL PLANS FOR DETAILS AND FINAL LOCATION.
 - RELOCATE SIGN.
 - TIME RESTRICTIVE SIGN.
 - STOP / STREET SIGN.
 - INSTALL TRENCH DRAIN. SEE DETAIL 11 / C2.0. CONNECT TO DRYWELL.
 - RECONSTRUCT SIDEWALK WITH PAVERS. RECONSTRUCT CURB AND GUTTER MATCHING EXISTING LINES AND GRADES.
 - INSTALL NEW TREES AND TREE GRATES. SEE ARCHITECTURAL PLANS FOR DETAILS AND FINAL LOCATIONS.
 - MATCH EXISTING LINES AND GRADES
 - RETAIN AND PROTECT DRY UTILITIES (POWER, GAS, COMMUNICATIONS). INSTALL BOLLARDS IF NECESSARY.
 - RETAIN AND PROTECT STREET LIGHT.

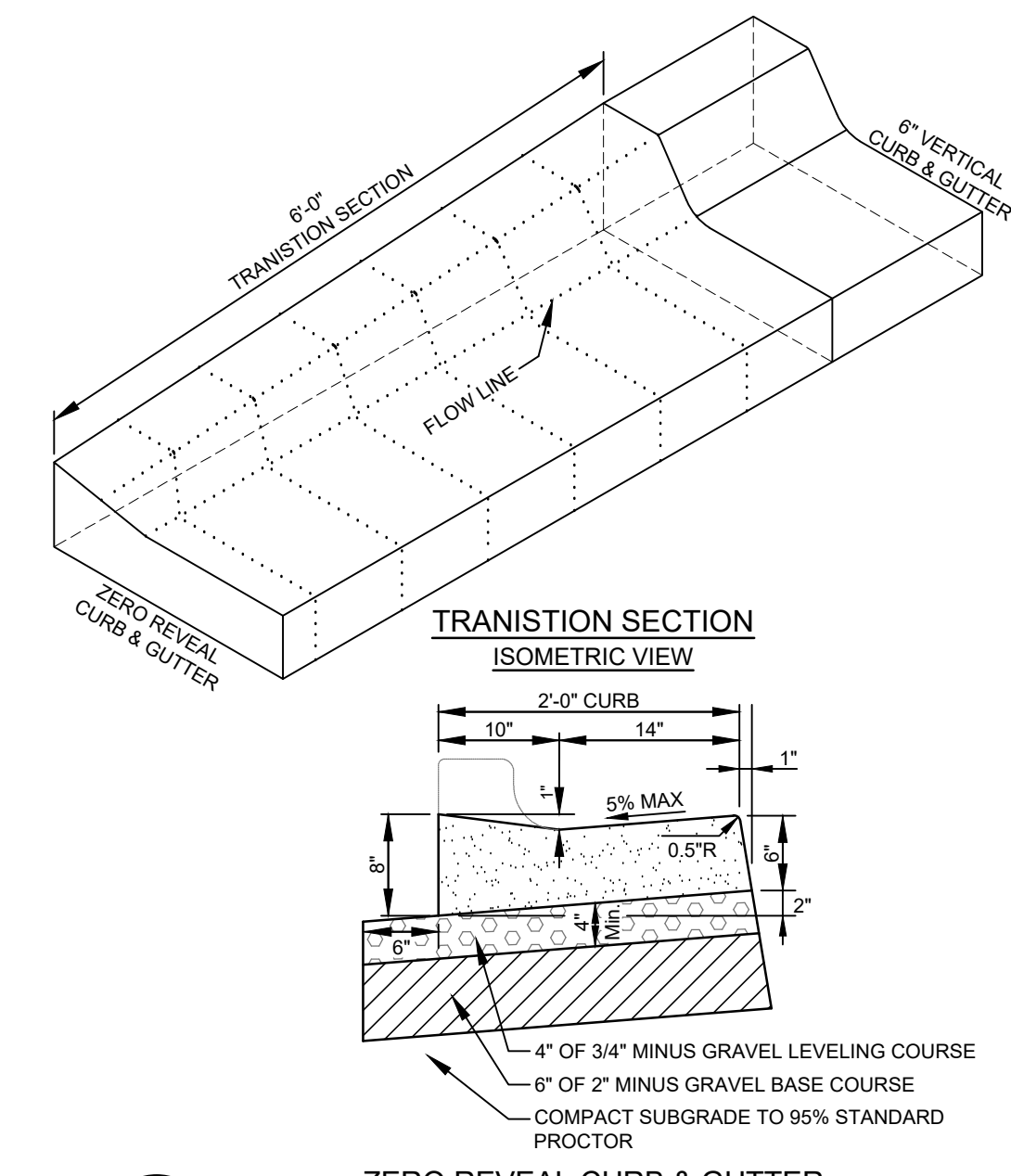




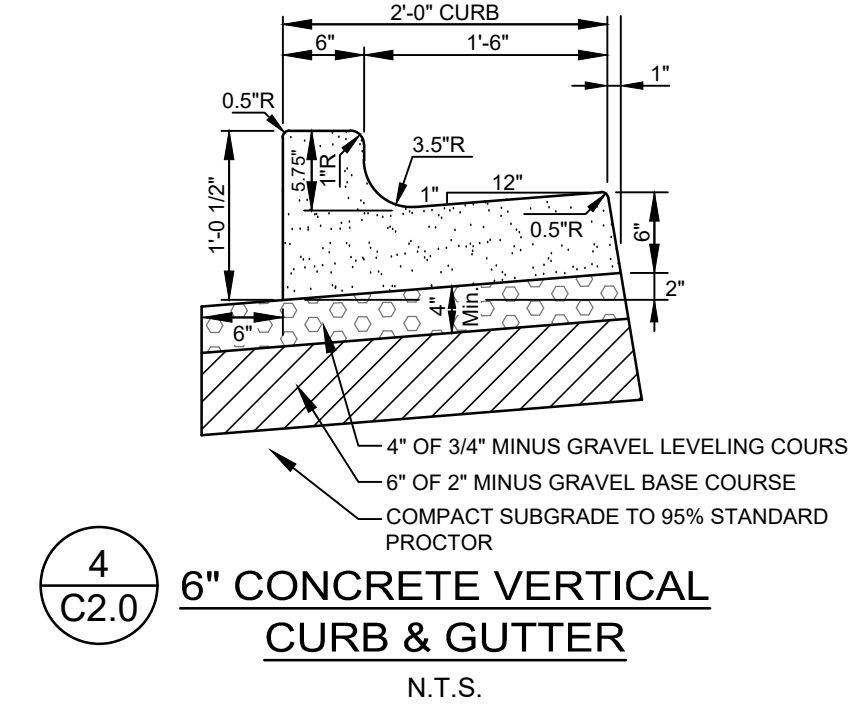
1
C2.0
TYPICAL ASPHALT SECTION
N.T.S.



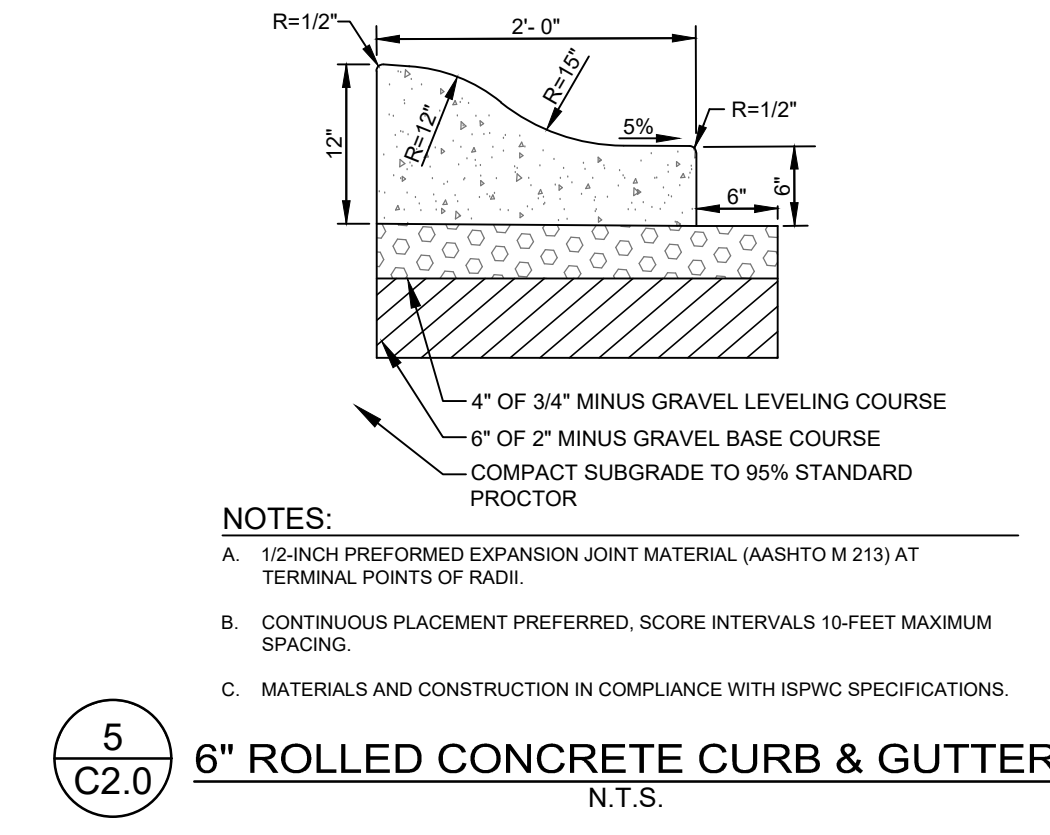
2
C2.0
TYPICAL CONCRETE SIDEWALK SECTION
N.T.S.



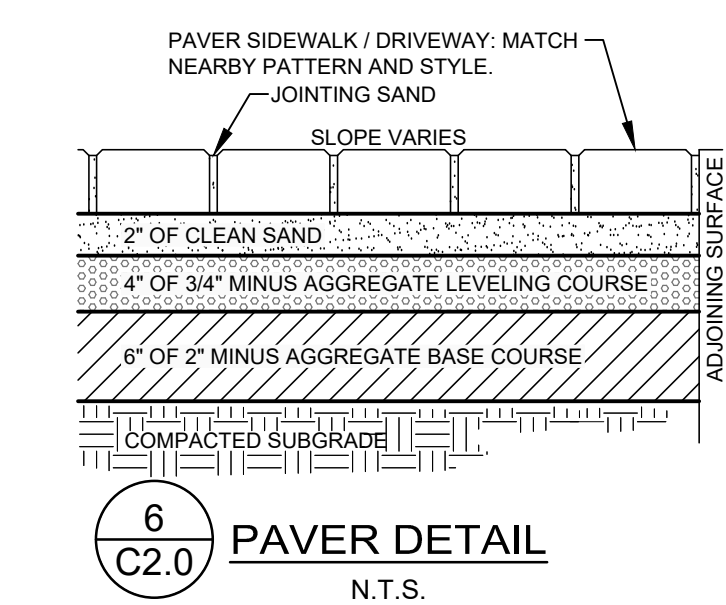
3
C2.0
TYPICAL CURB TRANSITION DETAIL
N.T.S.



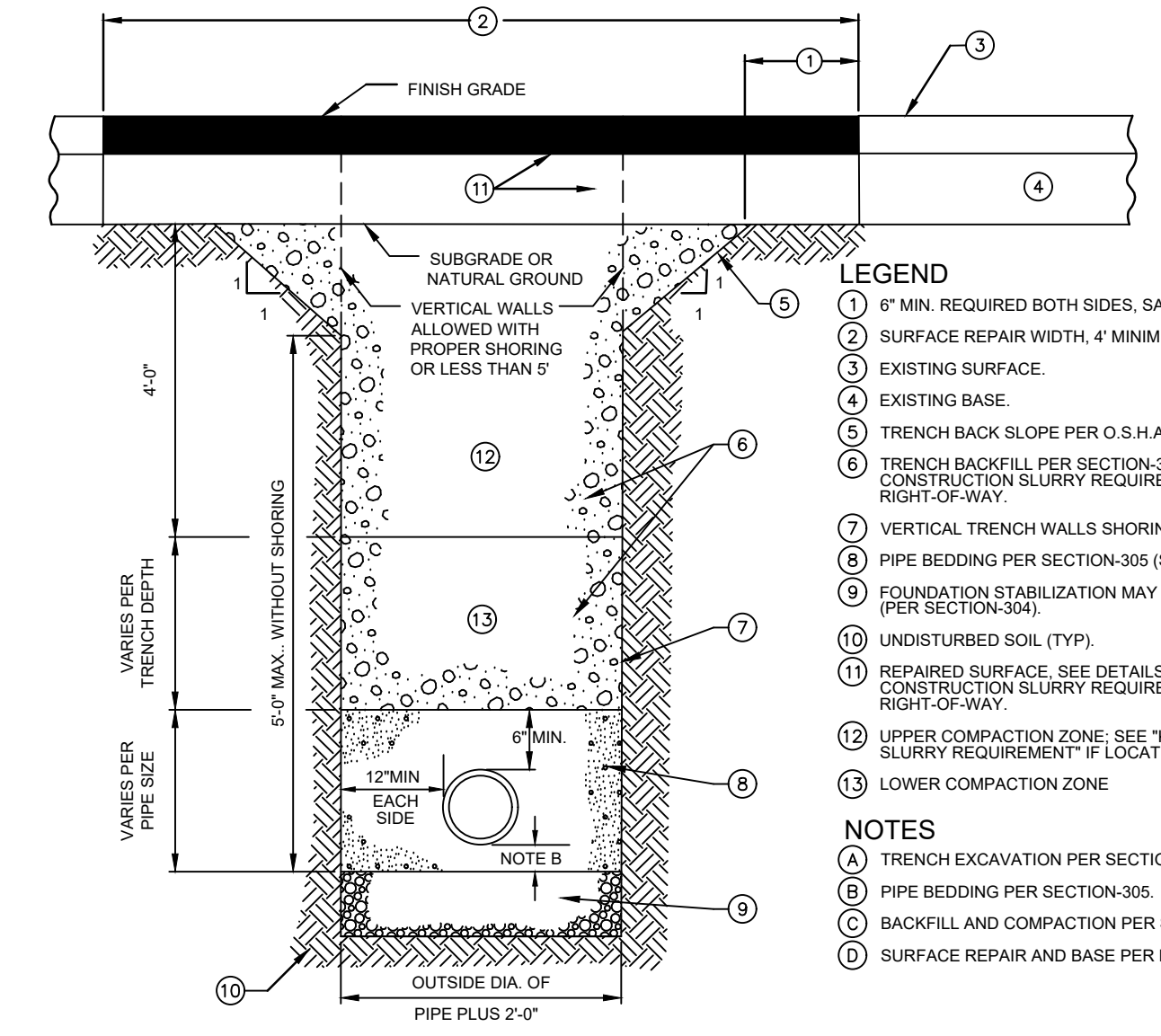
4
C2.0
6" CONCRETE VERTICAL CURB & GUTTER
N.T.S.



5
C2.0
6" ROLLED CONCRETE CURB & GUTTER
N.T.S.



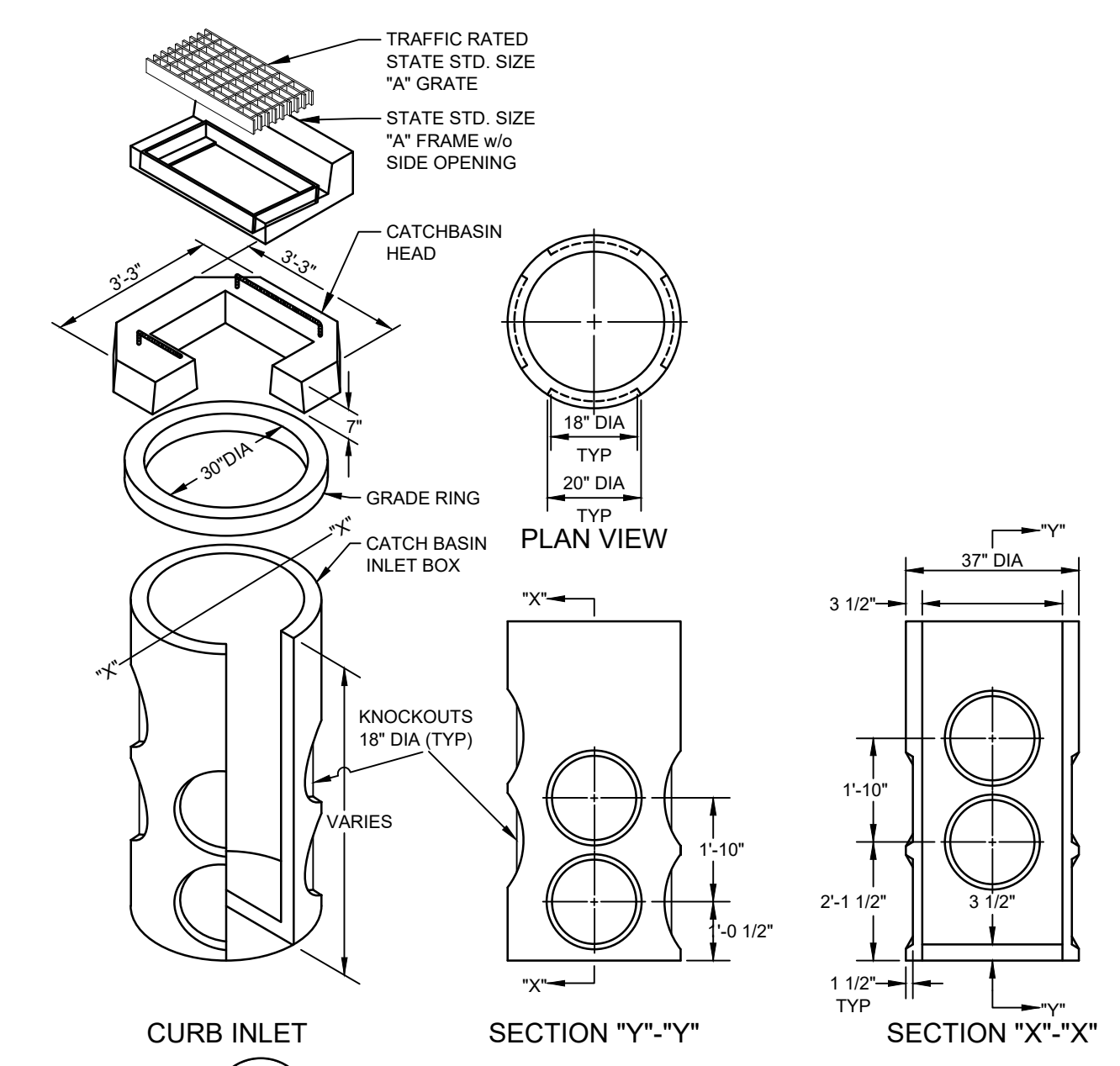
6
C2.0
PAVER DETAIL
N.T.S.



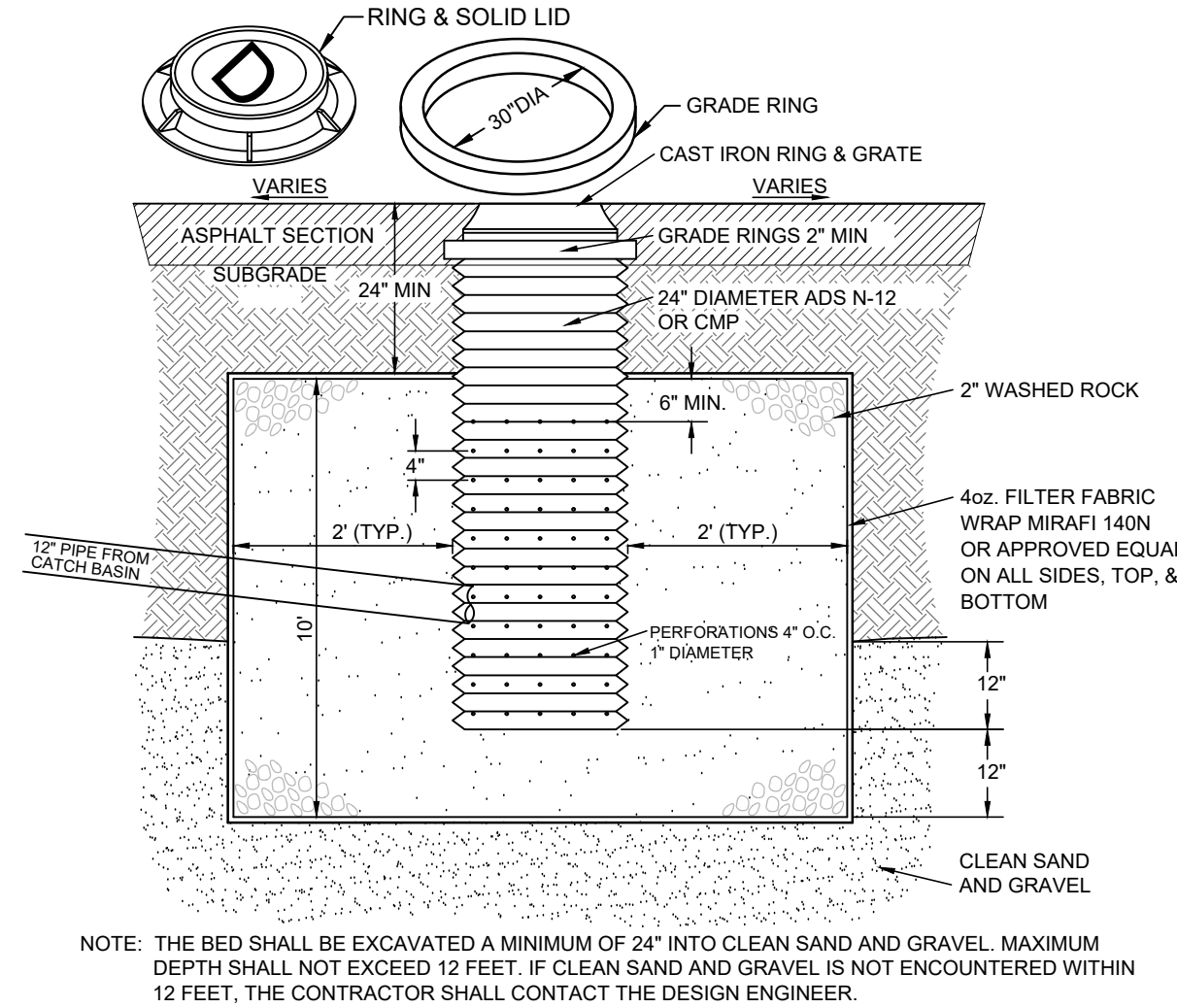
7
C2.0
TYPICAL TRENCH SECTION
N.T.S.

- LEGEND**
- 6" MIN. REQUIRED BOTH SIDES, SAWCUT REQUIRED.
 - SURFACE REPAIR WIDTH, 4" MINIMUM.
 - EXISTING SURFACE.
 - EXISTING BASE.
 - TRENCH BACK SLOPE PER O.S.H.A. OR SUITABLE SHORING.
 - TRENCH BACKFILL PER SECTION 306 OR SEE "KETCHUM PUBLIC CONSTRUCTION SLURRY REQUIREMENT" IF LOCATED WITHIN PUBLIC RIGHT-OF-WAY.
 - VERTICAL TRENCH WALLS SHORING PER O.S.H.A.
 - PIPE BEDDING PER SECTION 305 (SEE SD-302).
 - FOUNDATION STABILIZATION MAY VARY PER SOIL TYPE AND STABILITY (PER SECTION 304).
 - UNDISTURBED SOIL (TYP).
 - PREPARED SURFACE: SEE DETAILS 1 AND 2. SEE "KETCHUM PUBLIC CONSTRUCTION SLURRY REQUIREMENT" IF LOCATED WITHIN PUBLIC RIGHT-OF-WAY.
 - UPPER COMPACTION ZONE: SEE "KETCHUM PUBLIC CONSTRUCTION SLURRY REQUIREMENT" IF LOCATED WITHIN PUBLIC RIGHT-OF-WAY.
 - LOWER COMPACTION ZONE.
- NOTES**
- TRENCH EXCAVATION PER SECTION 301.
 - PIPE BEDDING PER SECTION 305.
 - BACKFILL AND COMPACTION PER SECTION 306.
 - SURFACE REPAIR AND BASE PER DETAIL 3C20.

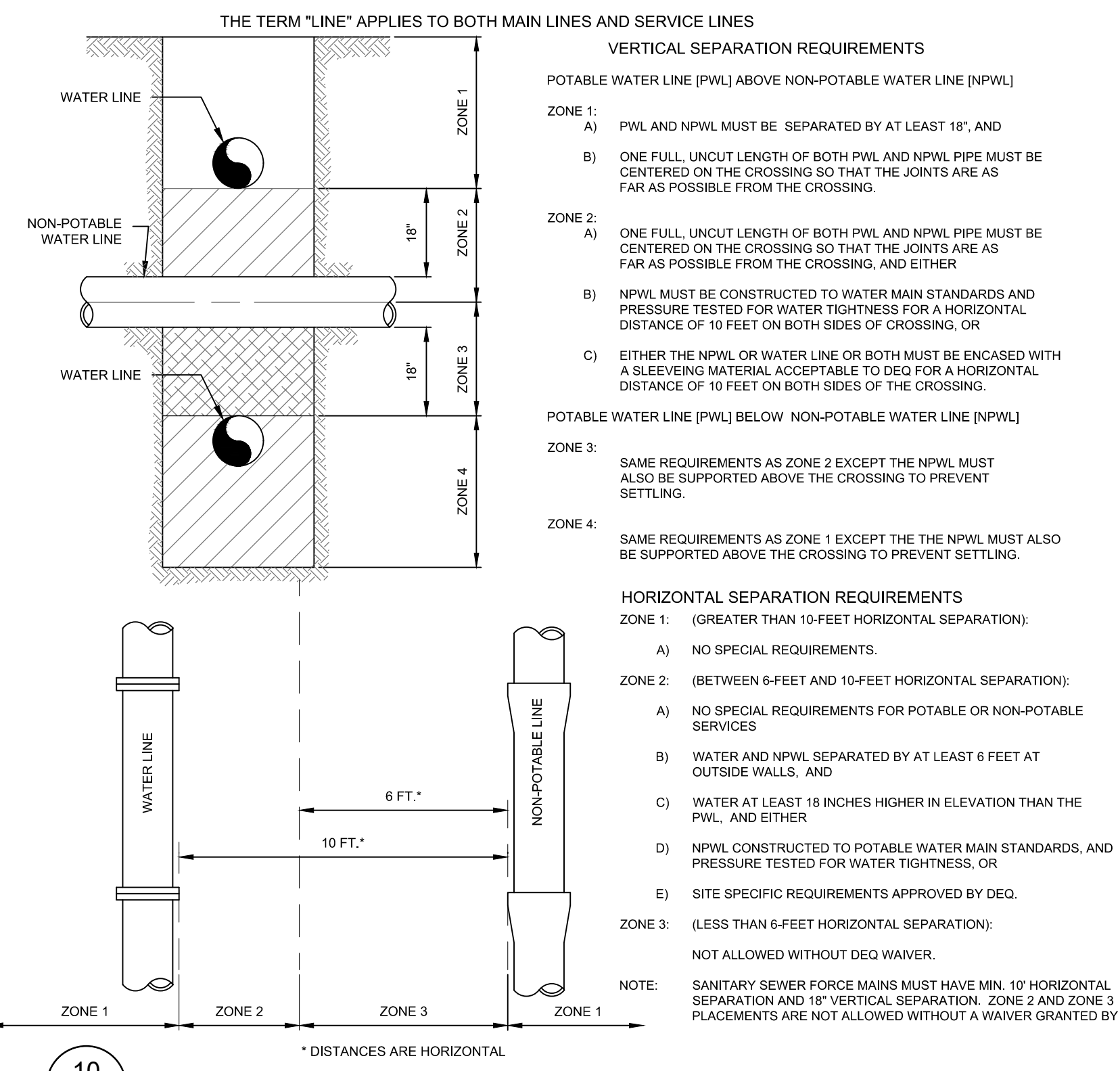
KETCHUM PUBLIC CONSTRUCTION SLURRY REQUIREMENT
 IN AREAS WHERE IT IS NECESSARY TO CUT THE ASPHALT PAVEMENT AND DIG A TRENCH FOR BURIAL OF CONDUIT CABLE OR OTHER CITY UTILITY, THE TRENCH SHALL BE BACKFILLED WITH A LEAN CONCRETE MIX TO THE BOTTOM OF FINISH SURFACE MATERIAL WITH THE FOLLOWING PROPORTIONS OF MATERIALS:
 COARSE AGGREGATE (6" MINUS)
 SAND
 PORTLAND CEMENT
 WATER
 WATER CONTENT IS MAXIMUM AND MAY BE REDUCING DOWNWARD. CARES SHALL BE TAKEN TO ASSURE THAT EXCESS WATER IS NOT PRESENT IN THE MIXING DRUM PRIOR TO CHARGING THE MIXER WITH MATERIALS. THOROUGH MIXING WILL BE REQUIRED PRIOR TO DISCHARGE.
 NO COMPACTION, VIBRATION OR FINISHING IS REQUIRED. THE LEAN CONCRETE MIX SHALL BE STRUCK OFF AT OR BELOW THE ELEVATION OF THE PLANT MIX SURFACING WITH A SQUARE-NOSE SHOVEL OR SIMILAR HAND TOOL. THE BACKFILL MIX SHALL BE ALLOWED TO SET FOR A MINIMUM OF 2 HOURS BEFORE THE PERMANENT PLANT MIX SURFACING IS PLACED TO COMPLETE THE TRENCH REPAIR. TEMPORARY PLACEMENT OF ASPHALT COLD MIX SURFACING MAY BE NECESSARY TO ACCOMMODATE TRAFFIC WITHIN THE FIRST 2 HOURS OF BACKFILL PLACEMENT PRIOR TO COMPLETING THE PERMANENT REPAIR.



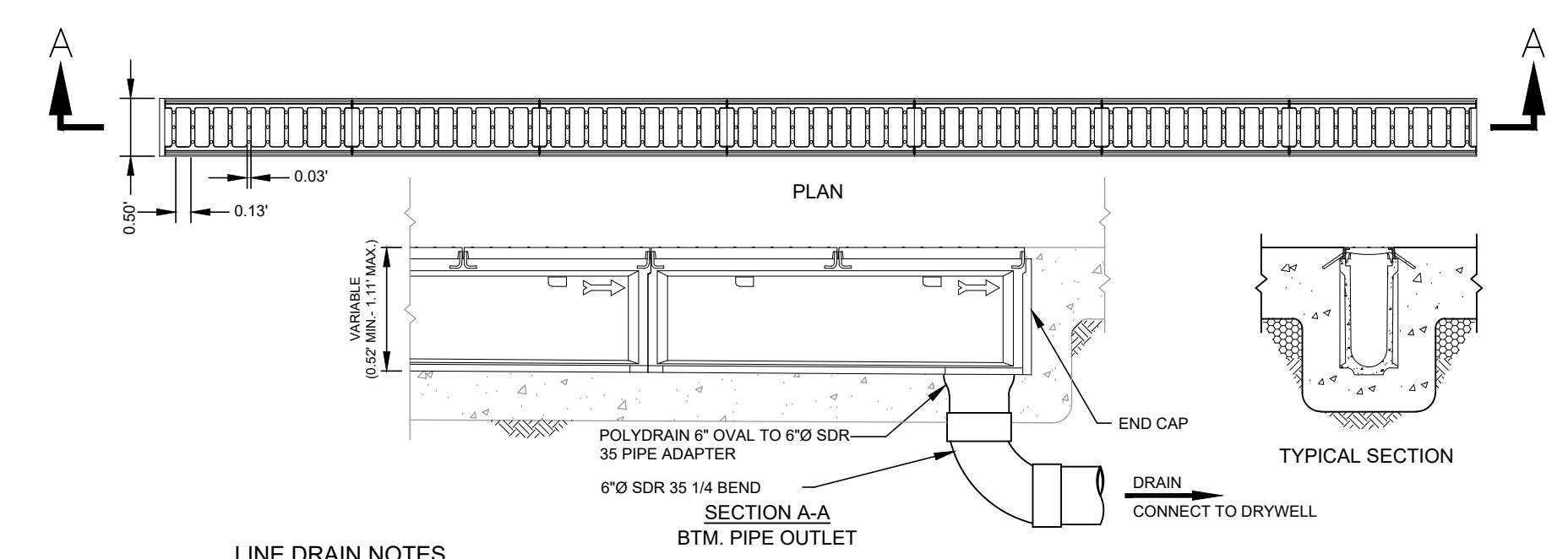
8
C0.2
30" DIAMETER CATCH BASIN
N.T.S.



9
C2.0
DRYWELL DETAIL (6'X6')
N.T.S.

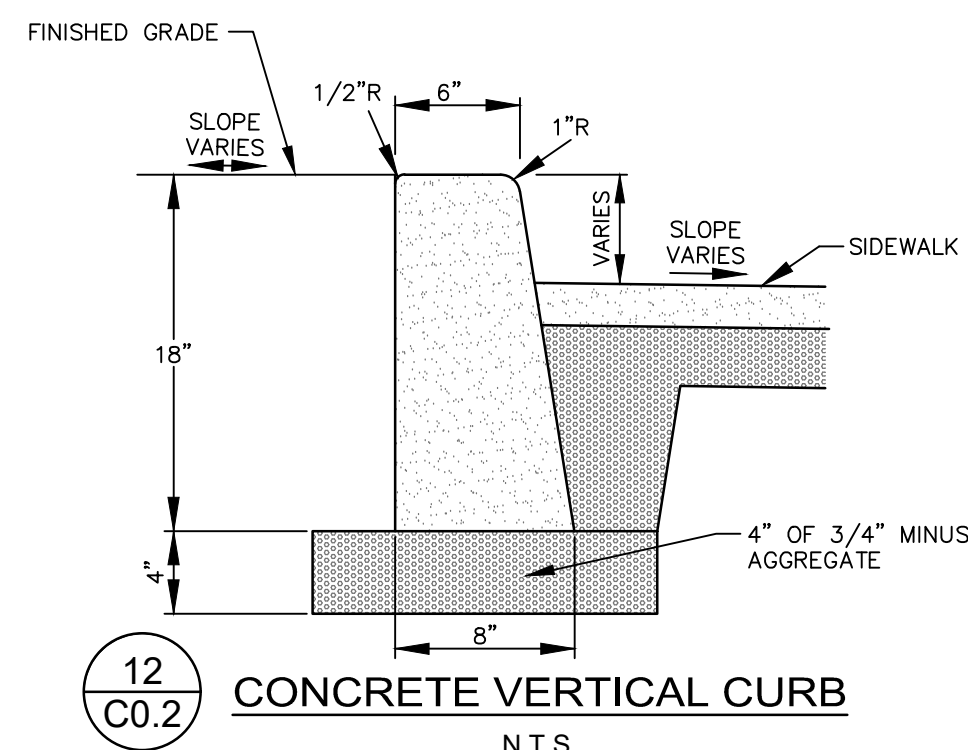


10
C2.0
POTABLE AND NON-POTABLE WATER LINE (NPWL) SEPARATION
N.T.S.



11
C0.2
TYPICAL TRENCH DRAIN DETAIL
(ABT INTERCEPTOR LINE DRAIN OR APPROVED EQUAL)
N.T.S.

- LINE DRAIN NOTES**
- Line drain is suitable for applications for controlling spread in gutter flow conditions or to intercept sheet flow. Typical application is at the street curb or barrier.
 - The frame and grate is suitable for pedestrian and bicycle traffic and rated for H-25 and HS-25 loads.
 - Concrete thickness, type, and amount of reinforcement to be same as adjacent pavement or greater. Perform structural analysis to determine requirements for application.
 - Top of grate to be installed flush to 1/8" below finished grade. Bevel concrete to top of grate if below flush.



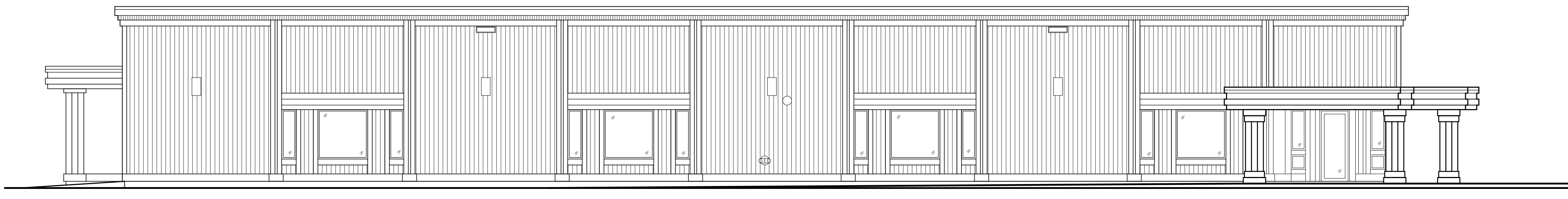
12
C0.2
CONCRETE VERTICAL CURB
N.T.S.



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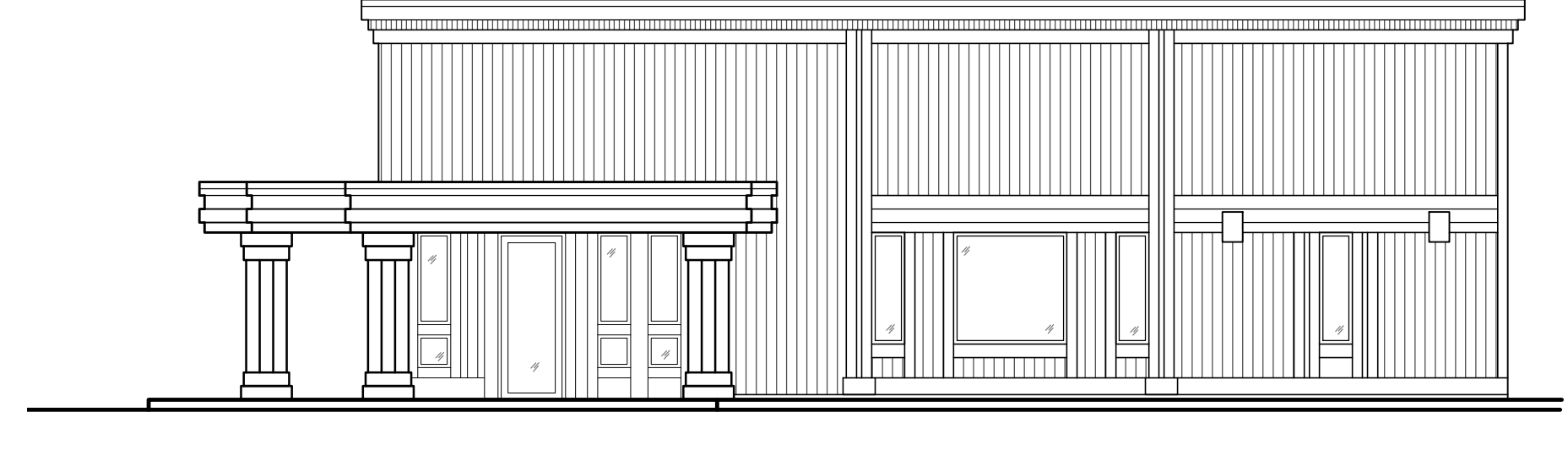
ISSUED	
2019.07.18	DESIGN REVIEW ISSUE

WALL LEGEND	
	WALL TO BE REMOVED
	EXISTING 2X4 STUD WALL
	EXISTING 8" MASONRY WALL
	NEW 8" MASONRY WALL
	NEW 2X4 WOOD STUD WALL
	NEW 2X6 WOOD STUD WALL
	NEW 2X6 FIRE RATED (1-HOUR) WALL



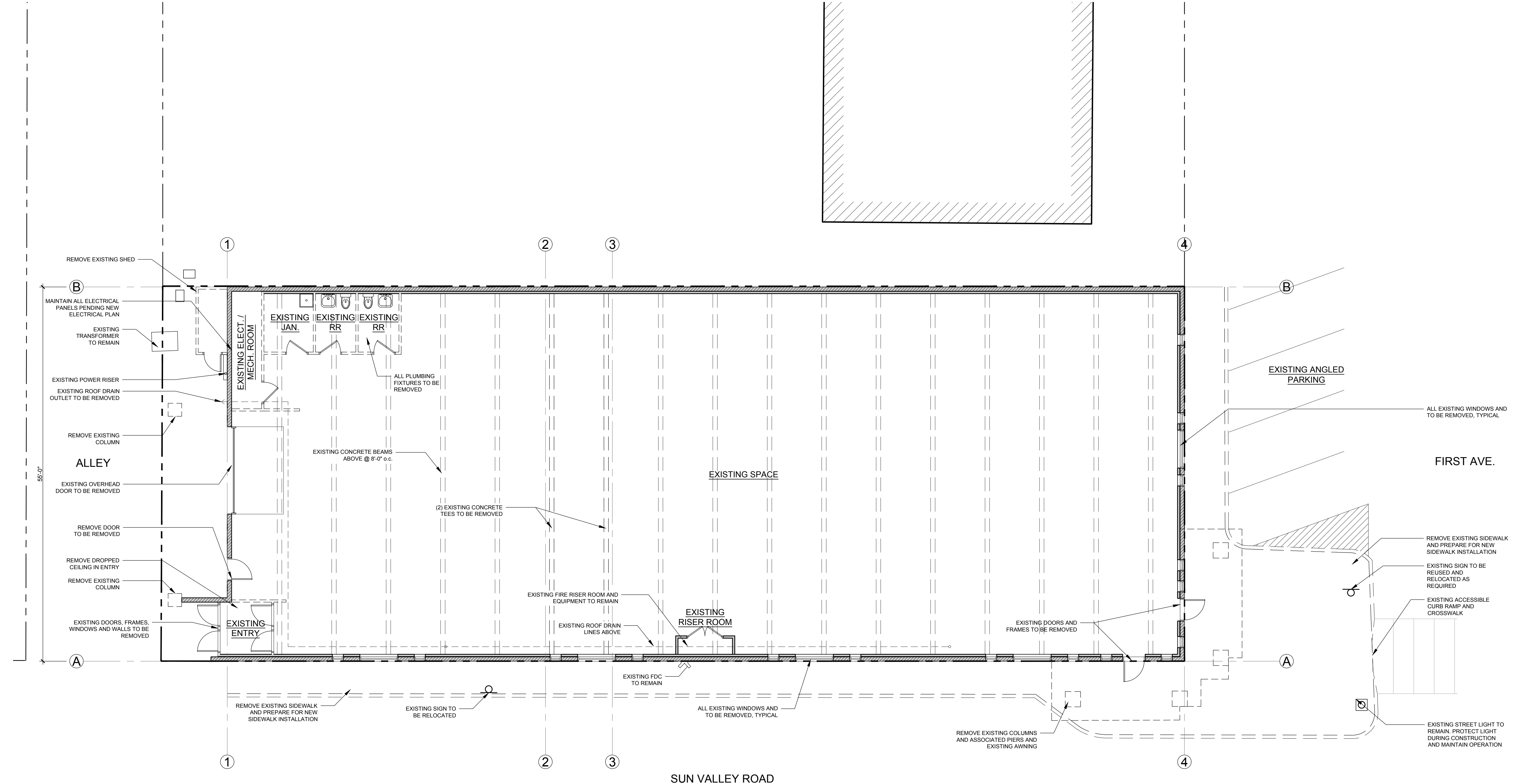
EAST ELEVATION - EXISTING FROM SUN VALLEY ROAD

SCALE: 1/8" = 1'-0"



NORTH ELEVATION - EXISTING FROM FIRST AVENUE

SCALE: 1/8" = 1'-0"



SUN VALLEY ROAD

EXISTING MAIN LEVEL FLOOR PLAN

SCALE: 1/8" = 1'-0"

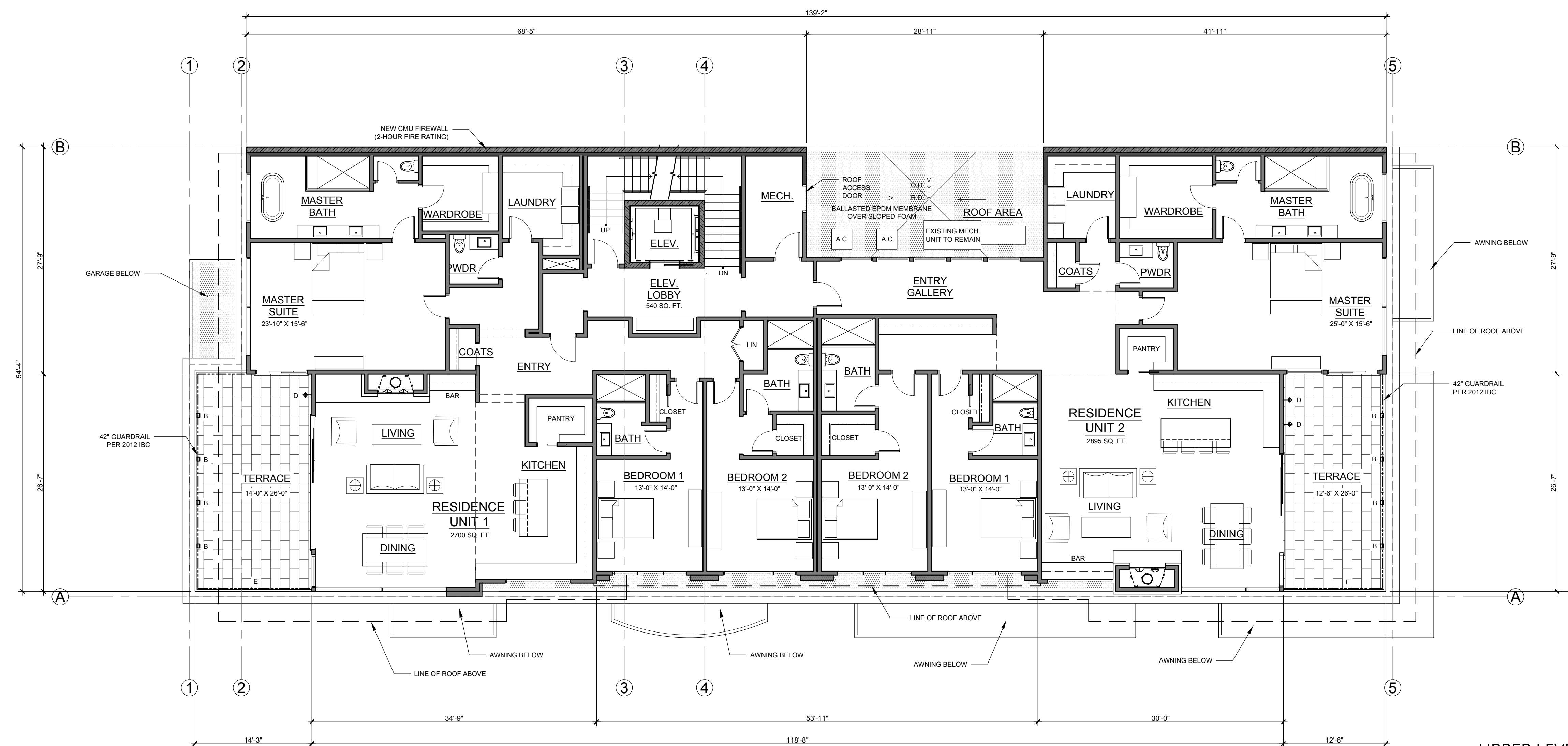
RUSCITTO LATHAM BLANTON
 ARCHITECTURE P.A.
 208.726.5688
 www.rlb-sv.com

SUN VALLEY & FIRST
 311 FIRST AVE. KETCHUM, IDAHO

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 DRAWING
 MAIN LEVEL FLOOR
 PLAN AND ELEVATIONS
 - EXISTING

DWG. #
A1.0

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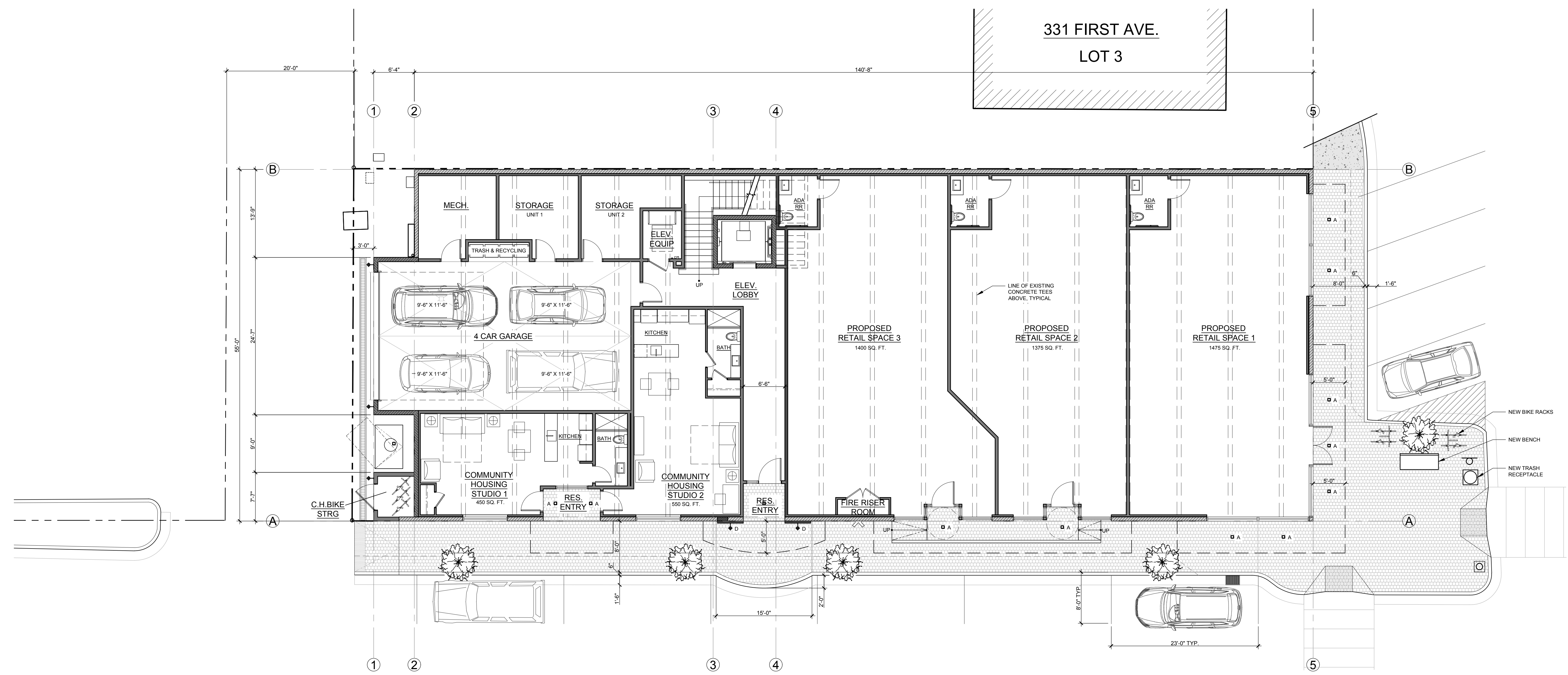
UPPER LEVEL FLOOR PLAN - PROPOSED
SCALE: 1/8" = 1'-0"

WALL LEGEND	
	WALL TO BE REMOVED
	EXISTING 2X4 STUD WALL
	EXISTING 8" MASONRY WALL
	NEW 8" MASONRY WALL
	NEW 2X4 WOOD STUD WALL
	NEW 2X6 WOOD STUD WALL
	NEW 2X6 FIRE RATED (1-HOUR) WALL

GROSS SQUARE FOOTAGES:	
MAIN LEVEL	7,940 SQ. FT.
UPPER LEVEL	6,140 SQ. FT.
THIRD LEVEL	160 SQ. FT.
TOTAL	14,240 SQ. FT.

EXTERIOR LIGHTING LEGEND	
	TYPE 'D1' DECORATIVE RECTANGULAR WALL SCORNER 7'-6" HEIGHT HINKLEY DARK SKY 1668 (4 FIXTURES)
	TYPE 'A' 4" RECESSED SOFFIT DOWNLIGHT 9W LED COOPER LIGHTING HALO LED H4 SERIES SOFFIT MOUNT (15 FIXTURE TOTAL)
	TYPE 'B' WEDGE DECK RAIL LIGHT 0.3W LED TREX OUTDOOR LIGHTING MOUNTED TO GUARDRAIL POSTS 3'-0" HEIGHT (9 FIXTURE TOTAL)
	TYPE 'C' WALL MOUNTED DOWNLIGHT 6W LED CIVIC DARK SKY LED MOUNTED ABOVE DOORS (2) FIXTURES
	TYPE 'E' LINEAR LED DOWNLIGHTING AT TERRACE PARAPET WALLS LINEA THIN 66 6W LED AT PERIMETERS OF UPPER TERRACES

NOTE:
ALL EXTERIOR LIGHTING TO COMPLY WITH CITY OF KETCHUM LIGHTING ORDINANCE FOR COMMUNITY CORE (CC) ZONE, CHAPTER 17-132



MAIN LEVEL FLOOR PLAN - PROPOSED
SCALE: 1/8" = 1'-0"

ISSUED	
2019.07.18	DESIGN REVIEW ISSUE

R L B
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ARCHITECTURE P.A.
www.rlb-va.com
208.726.5688
po box 419 sun valley, id 83353

SUN VALLEY & FIRST
311 FIRST AVE. KETCHUM, IDAHO

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DRAWING
FLOOR PLANS - PROPOSED
DWG. #

A 2.0

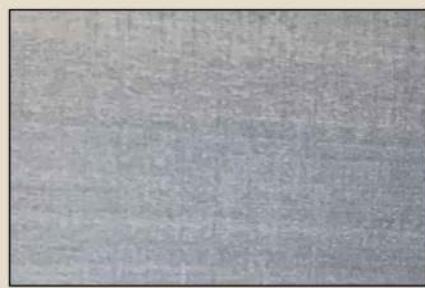




VERTICAL WOOD SIDING
LIGHT GRAY FINISH



**ALUMINUM WINDOWS, METAL
AWNINGS, FASCIAS AND STRUCTURAL
STEEL FRAMING**
BLACK



PLASTER SOFFIT
LIGHT GRAY FINISH



METAL PANELS
WALL CLADDING - KYNAR COATED
PINE BRONZE



STONE VENEER
QUARTZITE LEDGE



ELEVATION LOOKING WEST

SCALE: NTS



ELEVATION LOOKING NORTH

SCALE: NTS

SUN VALLEY AND FIRST
311 FIRST AVE. KETCHUM, IDAHO