



City of Ketchum
Planning & Building

IN RE:)
)
 Warm Springs Ranch Residences Lot 35) **KETCHUM PLANNING & ZONING COMMISSION**
 Design Review) **FINDINGS OF FACT, CONCLUSIONS OF LAW, AND**
 Date: September 26, 2023) **DECISION**
)
 File Number: P23-020)

PROJECT: Warm Springs Ranch Residences Lot 35
 FILE NUMBER: P23-020
 APPLICATION TYPE: Design Review
 REPRESENTATIVE: John Shirley – Think Architecture (architect)
 PROPERTY OWNER: Brennan Holdings No. 300 LLC
 LOCATION: 190 Bald Mountain Road (Lot 35, Block 4, Warm Springs Ranch Residences)
 ZONING: General Residential – Low Density (GR-L)
 OVERLAY: None

RECORD OF PROCEEDINGS

The Planning and Building Department received the Design Review applications on March 28, 2023. Following the receipt of the applications, staff routed the application materials to all City departments for review. The applications were reviewed concurrently, and the applications were deemed complete on July 28, 2023, after two rounds of review. As of the date of this letter, all department comments have been resolved or addressed through conditions of approval recommended below.

A public hearing notice for the project was mailed to all owners of property within 300 feet of the project site and all political subdivisions on August 23, 2023. The public hearing notice was published in the Idaho Mountain Express on August 23, 2023. A notice was posted on the project site and the city’s website on September 5, 2023. Story poles were documented on the project site as of September 6, 2023.

The Planning and Zoning Commission considered the Warm Springs Ranch Residences Lots 32-35 Design Review applications (File No. P23-017 – P23-020) concurrently during their regular meeting on September 12, 2023. After considering Staff’s analysis, the applicant’s presentation, and public

comment, the Planning & Zoning Commission unanimously approved the Design Review application for Lot 35 and continued the Design Review applications for Lots 32-34 to October 10, 2023.

FINDINGS OF FACT

The Planning & Zoning Commission, having reviewed the entire project record, provided notice, and conducted the required public hearing, does hereby make and set forth these Findings of Fact, Conclusions of Law, and Decision as follows:

BACKGROUND

The applicant is proposing to construct a new 3,791 square foot single-family residence (the “project”), located at Lot 35, Block 4, Warm Springs Ranch Residences (the “subject property”). The subject property is zoned General Residential – Low Density (GR-L) and the lot is currently vacant. The Warm Springs Ranch Subdivision, which was platted in 2021, includes plat note which requires development on Lots 32-35 be subject to Design Review standards of the Ketchum Municipal Code to ensure development on the lots have a minimal visual impact to the view of Bald Mountain from Warm Springs Road.

The project will construct improvements to the right-of-way per the City of Ketchum improvement standards. The project proposes a driveway snowmelt system located entirely within the property boundary and not within the Bald Mountain Road right-of-way. All improvements to the right-of-way have been preliminarily reviewed by the Streets Department and the City Engineer. Final review of the proposed improvements will be conducted by the City Engineer and Streets Department prior to issuance of building permit.

FINDINGS REGARDING COMPLIANCE WITH ZONING CODE AND DIMENSIONAL STANDARDS

Compliance with Zoning and Dimensional Standards				
Compliant			Standards and Findings	
Yes	No	N/A	Ketchum Municipal Code	City Standards and Findings
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	Minimum Lot Area
			Finding	Required: 8,000 square feet Existing: 9,907 square feet (.23 acres)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	Building Coverage
			Finding	Permitted: 35% Proposed: 25% (2,499 square feet building coverage / 9,907 square feet lot area)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	Minimum Building Setbacks
			Finding	Minimum Required Setbacks: Front: 15' Side: > of 1' for every 3' in building height, or 5' Rear: > of 1' for every 3' in building height, or 15' Proposed: Front (south): 15' Side (east): 11'-8"

				Side (west): N/A (pie-shaped lot) Rear (north): 15'
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	Building Height
			Finding	Maximum Permitted: 35' Proposed: 30' - 3 3/8"
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.125.030.H	Curb Cut
			Finding	Permitted: A total of 35% of the linear footage of any street frontage can be devoted to access off street parking. Proposed: 9% (20-foot-wide driveway/213 feet of property frontage along Bald Mountain Road Road).
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.125.020.A.2 & 17.125.050	Parking Spaces
			Finding	Off-street parking standards of this chapter apply to any new development and to any new established uses. Required: Residential (one family dwelling), in all applicable zoning districts require two parking spaces. Proposed: The project plans indicate 2 parking spaces within the enclosed garage.

FINDINGS REGARDING COMPLIANCE WITH DESIGN REVIEW STANDARDS

17.96.060.A.1 - Streets	Conformance
<i>The applicant shall be responsible for all costs associated with providing a connection from an existing City street to their development.</i>	YES
Finding: The project proposes to construct a new asphalt driveway to access the subject property from Bald Mountain Road. All project costs associated with the development, including the City street connection, are the responsibility of the applicant.	

17.96.060.A.2 - Streets	Conformance
<i>All street designs shall be approved by the City Engineer.</i>	YES
Finding: The City Engineer has reviewed the proposed driveway design for the property and finds it to be sufficient for the project.	
All street designs shall be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit.	

17.96.060.B.1 - Sidewalks	Conformance
<i>All projects under subsection 17.96.010.A of this chapter that qualify as a "substantial improvement" shall install sidewalks as required by the Public Works Department.</i>	YES

Finding: A sidewalk has already been installed along Bald Mountain Road adjacent to the subject property as part of the Warm Springs Ranch Residences Subdivision Development Agreement. Per Phase One Block 1 Infrastructure Improvements listed in the Warm Springs Ranch Subdivision Development Agreement, sidewalks were installed adjacent to Bald Mountain Road to Warm Springs Road to Lot 3 in Block 1. Any damage to the right-of-way, including sidewalks, must be reconstructed to City standards at the owner’s expense prior to issuance of Certificate of Occupancy.

17.96.060.B.2 - Sidewalks	Conformance
<i>Sidewalk width shall conform to the City's right-of-way standards, however the City Engineer may reduce or increase the sidewalk width and design standard requirements at their discretion.</i>	YES

Finding: A sidewalk has already been installed along Bald Mountain Road adjacent to the subject property as part of the Warm Springs Ranch Residences Subdivision Development Agreement. Per Phase One Block 1 Infrastructure Improvements listed in the Warm Springs Ranch Subdivision Development Agreement, sidewalks were installed adjacent to Bald Mountain Road to Warm Springs Road to Lot 3 in Block 1. Any damage to the right-of-way, including sidewalks, must be reconstructed to City standards at the owner’s expense prior to issuance of Certificate of Occupancy.

17.96.060.B.3 - Sidewalks	Conformance
<p><i>Sidewalks may be waived if one of the following criteria is met:</i></p> <ul style="list-style-type: none"> <i>a) The project comprises an addition of less than 250 square feet of conditioned space.</i> <i>b) The City Engineer finds that sidewalks are not necessary because of existing geographic limitations, pedestrian traffic on the street does not warrant a sidewalk, or if a sidewalk would not be beneficial to the general welfare and safety of the public.</i> 	YES

Finding: A sidewalk has already been installed along Bald Mountain Road adjacent to the subject property as part of the Warm Springs Ranch Residences Subdivision Development Agreement. Per Phase One Block 1 Infrastructure Improvements listed in the Warm Springs Ranch Subdivision Development Agreement, sidewalks were installed adjacent to Bald Mountain Road to Warm Springs Road to Lot 3 in Block 1. Any damage to the right-of-way, including sidewalks, must be reconstructed to City standards at the owner’s expense prior to issuance of Certificate of Occupancy.

17.96.060.B.4 - Sidewalks	Conformance
<i>The length of sidewalk improvements constructed shall be equal to the length of the subject property line(s) adjacent to any public street or private street.</i>	YES

Finding: A sidewalk has already been installed along Bald Mountain Road adjacent to the subject property as part of the Warm Springs Ranch Residences Subdivision Development Agreement. Per Phase One Block 1 Infrastructure Improvements listed in the Warm Springs

Ranch Subdivision Development Agreement, sidewalks were installed adjacent to Bald Mountain Road to Warm Springs Road to Lot 3 in Block 1. Any damage to the right-of-way, including sidewalks, must be reconstructed to City standards at the owner's expense prior to issuance of Certificate of Occupancy.

17.96.060.B.5 – Sidewalks	Conformance
<i>New sidewalks shall be planned to provide pedestrian connections to any existing or future sidewalks adjacent to the site. In addition, sidewalks shall be constructed to provide safe pedestrian access to and around a building.</i>	YES
<p>Finding: A sidewalk has already been installed along Bald Mountain Road adjacent to the subject property as part of the Warm Springs Ranch Residences Subdivision Development Agreement. Per Phase One Block 1 Infrastructure Improvements listed in the Warm Springs Ranch Subdivision Development Agreement, sidewalks were installed adjacent to Bald Mountain Road to Warm Springs Road to Lot 3 in Block 1. Any damage to the right-of-way, including sidewalks, must be reconstructed to City standards at the owner's expense prior to issuance of Certificate of Occupancy.</p>	

17.96.060.B.6 - Sidewalks	Conformance
<i>The City may approve and accept voluntary cash contributions in lieu of the above described improvements, which contributions must be segregated by the City and not used for any purpose other than the provision of these improvements. The contribution amount shall be 110 percent of the estimated costs of concrete sidewalk and drainage improvements provided by a qualified contractor, plus associated engineering costs, as approved by the City Engineer. Any approved in lieu contribution shall be paid before the City issues a certificate of occupancy.</i>	YES
<p>Finding: A sidewalk has already been installed along Bald Mountain Road adjacent to the subject property as part of the Warm Springs Ranch Residences Subdivision Development Agreement. Per Phase One Block 1 Infrastructure Improvements listed in the Warm Springs Ranch Subdivision Development Agreement, sidewalks were installed adjacent to Bald Mountain Road to Warm Springs Road to Lot 3 in Block 1. Any damage to the right-of-way, including sidewalks, must be reconstructed to City standards at the owner's expense prior to issuance of Certificate of Occupancy.</p>	

17.96.060.C.1 - Drainage	Conformance
<i>All stormwater shall be retained on site.</i>	YES
<p>Finding: Pursuant to KMC §17.96.060.C.1, all storm water drainage shall be retained on site. Drainage improvements are specified on Sheet C-1 of the project plans. The drainage improvements include the installation of a trench drain bordering the width of the driveway. A combination of drywells and catch basins will be installed to collect stormwater from the rest of the property. The City Engineer has reviewed the proposed drainage plan and believes the trench drain and drywell improvements are sufficient to maintain all storm water drainage on the subject property.</p>	

All drainage plans and specifications shall be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit.

17.96.060.C.2 - Drainage	Conformance
<i>Drainage improvements constructed shall be equal to the length of the subject property lines adjacent to any public street or private street.</i>	YES

Finding: Drainage improvements are specified on Sheet C-1 of the project plans. The drainage improvements include the installation of a trench drain bordering the width of the driveway. A combination of drywells and catch basins will be installed to collect stormwater from the rest of the property. The City Engineer has reviewed the proposed drainage plan and believes the trench drain and drywell improvements are sufficient to maintain all storm water drainage on the subject property.

All drainage plans and specifications shall be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit.

17.96.060.C.3 - Drainage	Conformance
<i>The City Engineer may require additional drainage improvements as necessary, depending on the unique characteristics of a site.</i>	YES

Finding: The City Engineer has reviewed the proposed drainage plan and believes the trench drain and drywell/catch basin improvements are sufficient to maintain storm water drainage on the subject property. The City Engineer may require additional drainage improvements if necessary. If approved, the applicant shall submit final civil drawings for all drainage improvements with the building permit application to be verified, reviewed, and approved by the City Engineer and Streets Department.

17.96.060.C.4 - Drainage	Conformance
<i>Drainage facilities shall be constructed per City standards.</i>	YES

Finding: The drainage improvements include the installation of a trench drain bordering the width of the driveway along Bald Mountain Road. A combination of drywells and catch basins will be installed to collect stormwater from the rest of the property. The City Engineer has reviewed the proposed drainage plan and believes the proposed trench drain and drywell improvements meet city standards.

All drainage plans and specifications shall be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit.

17.96.060.D.1 - Utilities	Conformance
<i>All utilities necessary for the development shall be improved and installed at the sole expense of the applicant.</i>	YES

Finding: All project costs associated with the development, including the installation of utilities, are the responsibility of the applicant. The applicant has not made requests for funding to the City for utility improvements. No funds have been provided by the City for the project.

17.96.060.D.2 - Utilities	Conformance
<i>Utilities shall be located underground and utility, power, and communication lines within the development site shall be concealed from public view.</i>	YES
Finding: As shown on Sheet C-1 of the project plans, the project will connect to the municipal water and sewer systems from existing lines on Bald Mountain Road. Requirements and specification for the water and sewer connections will be verified, reviewed, and approved by the Utilities Department prior to issuance of a Building Permit.	

17.96.060.D.3 - Utilities	Conformance
<i>When extension of utilities is necessary all developers will be required to pay for and install two-inch SDR11 fiber optical conduit. The placement and construction of the fiber optical conduit shall be done in accordance with City of Ketchum standards and at the discretion of the City Engineer.</i>	N/A
Finding: N/A. Extension of utilities is not necessary to service the residence.	

17.96.060.E.1 – Compatibility of Design	Conformance
<i>The project's materials, colors and signing shall be complementary with the townscape, surrounding neighborhoods and adjoining structures.</i>	YES
Finding: Pursuant to KMC §17.96.060.E.1, “The project’s materials, colors and signing shall be complementary with the townscape, surrounding neighborhoods and adjoining structures.” The Warm Springs Ranch Subdivision was platted in 2021 and is in the process of being developed. Multiple single-family residences are currently being constructed on Bald Mountain Road and Mountain Creek Drive. All of which have similar, yet unique architectural styles that utilize both modern (flat and shed roofs with cold materials such as concrete and metal) and traditional (gabled roofs with warmer materials such as wood and stone) designs. The proposed development features gabled roofs with large windows and a mix of cold and warm materials including wood, metal, and stone.	

17.96.060.E.2 – Compatibility of Design	Conformance
<i>Preservation of significant landmarks shall be encouraged and protected, where applicable. A significant landmark is one which gives historical and/or cultural importance to the neighborhood and/or community.</i>	N/A
Finding: N/A. The subject property does not contain any significant landmarks.	

17.96.060.E.3 – Compatibility of Design	Conformance
<i>Additions to existing buildings, built prior to 1940, shall be complementary in design and use similar material and finishes of the building being added to.</i>	N/A
Finding: N/A. The subject property is vacant.	

17.96.060.F.1 – Architectural	Conformance
<i>Building(s) shall provide unobstructed pedestrian access to the nearest sidewalk and the entryway shall be clearly defined.</i>	YES
<p>Finding: A sidewalk has already been installed along Bald Mountain Road adjacent to the subject property as part of the Warm Springs Ranch Residences Subdivision Development Agreement. Per Phase One Block 1 Infrastructure Improvements listed in the Warm Springs Ranch Subdivision Development Agreement, sidewalks were installed adjacent to Bald Mountain Road to Warm Springs Road to Lot 3 in Block 1. Any damage to the right-of-way, including sidewalks, must be reconstructed to City standards at the owner’s expense prior to issuance of Certificate of Occupancy.</p>	

17.96.060.F.2 – Architectural	Conformance
<i>The building character shall be clearly defined by use of architectural features.</i>	YES
<p>Finding: As stated previously, the proposed structure features both modern and traditional designs with gabled roofs, large windows, and a mix of wood, metal, and stone materials.</p>	

17.96.060.F.3 – Architectural	Conformance
<i>There shall be continuity of materials, colors and signing within the project.</i>	YES
<p>Finding: The proposed structure features dark bronze trimmed windows and a mix of wood, metal, and stone materials.</p>	

17.96.060.F.4 – Architectural	Conformance
<i>Accessory structures, fences, walls and landscape features within the project shall match or complement the principal building.</i>	YES
<p>Finding: A 4’ wooden fence is proposed along the rear property line. No accessory structures are proposed. The project proposes landscaping improvements that complement and soften the visual appearance of the structure from Warm Springs Road and neighboring properties. The landscaping includes trees, shrubs, and grasses. Utilities on the lot are screened with shrubs. New pine trees in the western property corner will provide screening from the adjacent Bald Mountain Townhomes.</p>	

17.96.060.F.5 – Architectural	Conformance
<i>Building walls shall provide undulation/relief, thus reducing the appearance of bulk and flatness.</i>	YES
<p>Finding: The project features a two-story structure with multiple wall and deck pop-outs to provide undulation and reduce the appearance of flatness.</p>	

17.96.060.F.6 – Architectural	Conformance
<i>Building(s) shall orient toward their primary street frontage.</i>	YES
<p>Finding: The structure is oriented towards the primary street frontage along Bald Mountain Road.</p>	

17.96.060.F.7 – Architectural	Conformance
<i>Garbage storage areas and satellite receivers shall be screened from public view and located off alleys.</i>	YES
Finding: No satellite receivers are proposed for the project. Sheet A101 of the project plans indicates that garbage bins will be stored within the garage and screened from public view.	

17.96.060.F.8 – Architectural	Conformance
<i>Building design shall include weather protection which prevents water to drip or snow to slide on areas where pedestrians gather and circulate or onto adjacent properties.</i>	YES
Finding: The roof plan indicates that sections of the roofs will have snow clips installed, and that other areas will have gutters installed and be sloped to downspouts.	

17.96.060.G.1 – Circulation Design	Conformance
<i>Pedestrian, equestrian and bicycle access shall be located to connect with existing and anticipated easements and pathways.</i>	YES
Finding: A sidewalk has already been installed along Bald Mountain Road adjacent to the subject property as part of the Warm Springs Ranch Residences Subdivision Development Agreement. Per Phase One Block 1 Infrastructure Improvements listed in the Warm Springs Ranch Subdivision Development Agreement, sidewalks were installed adjacent to Bald Mountain Road to Warm Springs Road to Lot 3 in Block 1. Any damage to the right-of-way, including sidewalks, must be reconstructed to City standards at the owner’s expense prior to issuance of Certificate of Occupancy.	

17.96.060.G.2 – Circulation Design	Conformance
<i>Awnings extending over public sidewalks shall extend five feet or more across the public sidewalk but shall not extend within two feet of parking or travel lanes within the right-of-way.</i>	YES
Finding: A sidewalk has already been installed along Bald Mountain Road adjacent to the subject property as part of the Warm Springs Ranch Residences Subdivision Development Agreement. Per Phase One Block 1 Infrastructure Improvements listed in the Warm Springs Ranch Subdivision Development Agreement, sidewalks were installed adjacent to Bald Mountain Road to Warm Springs Road to Lot 3 in Block 1. Any damage to the right-of-way, including sidewalks, must be reconstructed to City standards at the owner’s expense prior to issuance of Certificate of Occupancy.	

17.96.060.G.3 – Circulation Design	Conformance
<i>Traffic shall flow safely within the project and onto adjacent streets. Traffic includes vehicle, bicycle, pedestrian and equestrian use. Consideration shall be given to adequate sight distances and proper signage.</i>	YES

Finding: A sidewalk has already been installed along Bald Mountain Road adjacent to the subject property as part of the Warm Springs Ranch Residences Subdivision Development Agreement. Per Phase One Block 1 Infrastructure Improvements listed in the Warm Springs Ranch Subdivision Development Agreement, sidewalks were installed adjacent to Bald Mountain Road to Warm Springs Road to Lot 3 in Block 1. Any damage to the right-of-way, including sidewalks, must be reconstructed to City standards at the owner's expense prior to issuance of Certificate of Occupancy.

The City Engineer has reviewed the proposed driveway and finds the circulation design to meet city standards. Final circulation design shall be reviewed and approved by the City Engineer and Streets Department prior to issuance of a Building Permit.

17.96.060.G.4 – Circulation Design	Conformance
<i>Curb cuts and driveway entrances shall be no closer than 20 feet to the nearest intersection of two or more streets, as measured along the property line adjacent to the right-of-way. Due to site conditions or current/projected traffic levels or speed, the City Engineer may increase the minimum distance requirements.</i>	YES
Finding: The proposed driveway is located further than 20 feet away from the nearest intersection of Bald Mountain Road and Lopey Lane.	

17.96.060.G.5 – Circulation Design	Conformance
<i>Unobstructed access shall be provided for emergency vehicles, snowplows, garbage trucks and similar service vehicles to all necessary locations within the proposed project.</i>	YES
Finding: Access for emergency vehicles, snowplows, and garbage trucks is provided along Bald Mountain Road.	

17.96.060.H.1 – Snow Storage	Conformance
<i>Snow storage areas shall not be less than 30 percent of the improved parking and pedestrian circulation areas.</i>	YES
Finding: Sheet L3 of the project plans indicate the proposed snow storage areas to include a total of 400 square feet, greater than the required 30% (1,050 * .30 = 315 square feet).	

17.96.060.H.2 – Snow Storage	Conformance
<i>Snow storage areas shall be provided on site.</i>	YES
Finding: Locations of snow storage areas are indicated on Sheet L3 of the project plans.	

17.96.060.H.3 – Snow Storage	Conformance
<i>A designated snow storage area shall not have any dimension less than five feet and shall be a minimum of 25 square feet.</i>	YES
Finding: None of the snow storage areas has dimensions less than five feet.	

17.96.060.H.4 – Snow Storage	Conformance
<i>In lieu of providing snow storage areas, snowmelt and hauling of snow may be allowed.</i>	YES
Finding: A driveway snowmelt system within the property boundary, and not within the Bald Mountain Road right-of-way, will be installed. In addition to the snowmelt system, snow storage areas are also provided on-site.	

17.96.060.I.1 – Landscaping	Conformance
<i>Landscaping is required for all projects.</i>	YES
Finding: Landscaping has been provided for the project as indicated on Sheets L3 of the project plans.	

17.96.060.I.2 – Landscaping	Conformance
<i>Landscape materials and vegetation types specified shall be readily adaptable to a site's microclimate, soil conditions, orientation and aspect, and shall serve to enhance and complement the neighborhood and townscape.</i>	YES
Finding: The front, side, and rear yards will be setback will be vegetated with native grasses. The project proposes landscaping improvements that complement and soften the visual appearance of the structure from Warm Springs Road and neighboring properties. The landscaping includes trees, shrubs, and grasses. Utilities on the lot are screened with shrubs. New pine trees in the western property corner will provide screening from the adjacent Bald Mountain Townhomes. Ornamental grasses and wildflowers are also proposed around the structure.	

17.96.060.I.3 – Landscaping	Conformance
<i>All trees, shrubs, grasses and perennials shall be drought tolerant. Native species are recommended but not required.</i>	YES
Finding: The landscape plan proposes drought-tolerant and native species, including pine trees, native shrubs, and drought tolerant grasses.	

17.96.060.I.4 – Landscaping	Conformance
<i>Landscaping shall provide a substantial buffer between land uses, including, but not limited to, structures, streets and parking lots. The development of landscaped public courtyards, including trees and shrubs where appropriate, shall be encouraged.</i>	YES
Finding: Landscaping along the rear property boundary provides a buffer from Warm Springs Road and the Bald Mountain Townhomes. Landscaping on the front and eastern side yard provides privacy from adjacent properties.	

17.96.060.J.1 – Public Amenities	Conformance
<i>Where sidewalks are required, pedestrian amenities shall be installed. Amenities may include, but are not limited to, benches and other seating, kiosks, bus shelters, trash receptacles, restrooms, fountains, art, etc. All public</i>	YES

<i>amenities shall receive approval from the Public Works Department prior to design review approval from the Commission.</i>	
Finding: A sidewalk has already been installed along Bald Mountain Road adjacent to the subject property as part of the Warm Springs Ranch Residences Subdivision Development Agreement. Per Phase One Block 1 Infrastructure Improvements listed in the Warm Springs Ranch Subdivision Development Agreement, sidewalks were installed adjacent to Bald Mountain Road to Warm Springs Road to Lot 3 in Block 1. Any damage to the right-of-way, including sidewalks, must be reconstructed to City standards at the owner’s expense prior to issuance of Certificate of Occupancy.	

17.96.060.K.1 – Underground Encroachments	Conformance
<i>Encroachments of below grade structures into required setbacks are subject to subsection 17.128.020.K of this title and shall not conflict with any applicable easements, existing underground structures, sensitive ecological areas, soil stability, drainage, other sections of this Code or other regulating codes such as adopted International Code Council Codes, or other site features concerning health, safety, and welfare.</i>	N/A
Finding: N/A. No encroachments of below grade structures are proposed.	

17.96.060.K.2 – Underground Encroachments	Conformance
<i>No below grade structure shall be permitted to encroach into the riparian setback.</i>	N/A
Finding: N/A. No encroachments of below grade structures into the riparian setback are proposed.	

CONCLUSIONS OF LAW

1. The City of Ketchum is a municipal corporation established in accordance with Article XII of the Constitution of the State of Idaho and Title 50 Idaho Code and is required and has exercised its authority pursuant to the Local Land Use Planning Act codified at Chapter 65 of Title 67 Idaho Code and pursuant to Chapters 3, 9 and 13 of Title 50 Idaho Code to enact the ordinances and regulations, which ordinances are codified in the Ketchum Municipal Code (“KMC”) and are identified in the Findings of Fact and which are herein restated as Conclusions of Law by this reference and which City Ordinances govern the applicant’s Design Review application for the development and use of the project site.
2. The Commission has authority to hear the applicant’s Design Review Applications pursuant to Chapter 17.96 of Ketchum Municipal Code Title 17.
3. The City of Ketchum Planning Department provided notice for the review of the applications in accordance with Ketchum Municipal Code §17.96.080.
4. The Design Review application is governed under Ketchum Municipal Code Chapters 17.96, 17.124, 17.08, 17.12, 17.18, and 17.128.

5. The Warm Springs Ranch Residences Lot 35 Design Review application meets all applicable standards specified in Title 17 of Ketchum Municipal Code.

DECISION

THEREFORE, the Planning and Zoning Commission **approves** the Design Review Application File No. P23-020 this Tuesday, September 26, 2023, subject to the following conditions of approval.

CONDITIONS OF APPROVAL

1. This Design Review approval is based on the project plans for Lot 35 presented at the September 12, 2023, Planning and Zoning Commission meeting. The project plans for all on-site improvements submitted for the building permit must conform to the approved design review plans unless otherwise approved in writing by the Planning and Zoning Commission or Administrator. Any building or site discrepancies which do not conform to the approved plans will be subject to review by the Commission and/or removal.
2. The applicant shall submit final civil drawings prepared by an engineer registered in the State of Idaho which include specifications for right-of-way, circulation design, utilities, and drainage improvements for review and approval by the City Engineer, Streets, and Utilities departments prior to issuance of a building permit for the project.
3. The term of Design Review approval shall be twelve (12) months from the date that the Findings of Fact, Conclusions of Law, and Decision are adopted by the Commission or upon appeal, the date the approval is granted by the Council subject to changes in zoning regulations.
4. In addition to the requirements set forth in this Design Review approval, this project shall comply with all applicable local, state, and federal laws.
5. Any damage to the right-of-way, including sidewalks, must be reconstructed to City standards at the owner's expense prior to issuance of Certificate of Occupancy.

Findings of Fact **adopted** this 26th day of September 2023.

Neil Morrow, Chair
City of Ketchum
Planning and Zoning Commission



WARM SPRINGS #35

PROJECT ADDRESS
190 BALD MOUNTAIN ROAD KETCHUM, ID 83340
PROPERTY I.D. NUMBER:

DRAWING INDEX

GENERAL				STRUCTURAL			
SHEET #	SHEET NAME	#	DATE	SHEET #	SHEET NAME	#	DATE
COVER	COVER SHEET	1	04-27-2023	S101	STRUCTURAL		
G002	GENERAL NOTES			MECHANICAL SHEET # SHEET NAME # DATE M101 MECHANICAL GENERAL NOTES M102 MECHANICAL PLAN			
G003	BUILDING AREA ANALYSIS						
G005	SPECIFICATIONS						
G006	SPECIFICATIONS						
G007	SPECIFICATIONS	1	04-27-2023				
G008	SPECIFICATIONS						
G009	SPECIFICATIONS						
G010	SPECIFICATIONS			ELECTRICAL SHEET # SHEET NAME # DATE E101 ELECTRICAL GENERAL NOTES E102 ELECTRICAL PLAN			
CIVIL SHEET # SHEET NAME # DATE C101 CIVIL				LANDSCAPE SHEET # SHEET NAME # DATE L101 Landscape			
ARCHITECTURAL SHEET # SHEET NAME # DATE A101 SITE PLAN A103 LEVEL 1 SLAB PLAN A104 LEVEL 1 FLOOR PLAN A105 LEVEL 2 FLOOR PLAN A107 ROOF PLAN A109 LEVEL 1 CEILING PLAN A110 LEVEL 2 CEILING PLAN A201 EXTERIOR ELEVATIONS A202 EXTERIOR ELEVATIONS A203 EXTERIOR ELEVATIONS A301 BUILDING SECTIONS A302 BUILDING SECTIONS A401 FIREPLACE ELEVATIONS A501 ARCHITECTURAL DETAILS A502 ARCHITECTURAL DETAILS A503 STAIR RAIL DETAILS A601 DOOR SCHEDULE & ELEVATIONS A602 WINDOW SCHEDULE & ELEVATIONS							

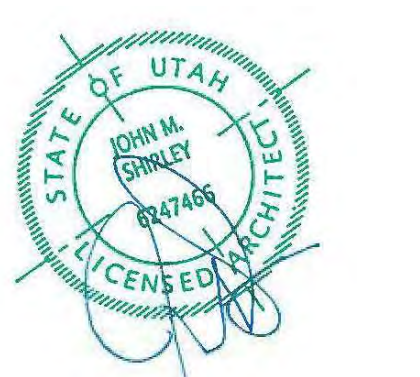


Architecture
Interior Design
Landscape Architecture
Land Planning
Construction Management

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PROJECT TEAM	SEAL	GOVERNING BUILDING CODES & INFORMATION	ABBREVIATIONS	GRAPHIC SYMBOLS/ MATERIAL LEGENDS	VICINITY MAP																																																																																																																																																																																																																																																																																																												
ARCHITECT: THINK ARCHITECTURE: 7927 SOUTH HIGH POINT WAY, SUITE 300 SANDVY, UT 84094 801.269.0055 STRUCTURAL ENGINEER: VECTOR ENGINEERS 1500 S. CLOVERDALE ROAD, SUITE 315 KETCHUM, ID 83340 208.996.0003 MECHANICAL ENGINEER: DESIGN BUILD ELECTRICAL ENGINEER: DESIGN BUILD CIVIL ENGINEER: BENCHMARK ASSOCIATES PA 1102 BELL DRIVE KETCHUM, ID 83340 208.726.9512 LANDSCAPE ARCHITECT: EGGERS ASSOCIATES, PA 340 NORTH 2ND AVE KETCHUM, ID 83340 208.726.0988 GENERAL CONTRACTOR: MAGLEBY CONSTRUCTION SUN VALLEY 511 EAST AVENUE NORTH SUITE 201 KETCHUM, IDAHO 83340 208.726.3923 OWNER: VP PROPERTIES 240 LEADVILLE KETCHUM, IDAHO 83340 208.726.1875		BUILDING CODE: 2018 INTERNATIONAL RESIDENTIAL CODE (I.R.C.) WITH IDAHO STATE AMENDMENTS PLUMBING CODE: 2017 IDAHO STATE PLUMBING CODE (I.S.P.C.) ELECTRICAL CODE: 2017 IDAHO ELECTRICAL CODE (INFA 70) ACCESSIBILITY: 2009 ANSI 117.1 & 2018 I.B.C. Mechanical Code: 2018 INTERNATIONAL MECHANICAL CODE (I.M.C.) AMENDED Fire Code: 2018 INTERNATIONAL FIRE CODE (I.F.C.) AMENDED Energy Conservation: 2018 INTERNATIONAL ENERGY CONSERVATION CODE (I.E.C.C.) OCCUPANCY GROUP: R2 FIRE SPRINKLER: YES MONITORED SYSTEM: YES BUILDING TYPE: TYPE V-8 FIRE SPRINKLER TYPE: NFPA 72 FIRE DETECTION SYSTEM PER KETCHUM ORDINANCE #1217: BUILDING TYPE: TYPE V-8 ADDRESSABLE FIRE ALARMS: YES RISK CATEGORY (I.C. 104.5): II SITE EXPOSURE: B BASIC WIND SPEED: 90 MPH ULTIMATE WIND SPEED: 115 MPH MATERIALS: CONCRETE FOUNDATIONS, WOOD FRAME W/ BRICK MASONRY & FIBER CEMENT PANELS, SINGLE PLY ROOFING MEMBRANE, & METAL ROOFING.	<table border="0"> <tr><td>#</td><td>NUMBER</td><td>HDW.</td><td>HARDWARE</td></tr> <tr><td>A.B.</td><td>ANCHOR BOLT</td><td>H.M.</td><td>HOLLOW METAL</td></tr> <tr><td>ADJ.</td><td>ADJUSTABLE</td><td>HOBZ</td><td>HORIZONTAL</td></tr> <tr><td>A.F.F.</td><td>ABOVE FINISHED FLOOR</td><td>HT</td><td>HEIGHT</td></tr> <tr><td>ALUM.</td><td>ALUMINUM</td><td>HVAC</td><td>HEATING/VENTILATION/AIR CONDITIONING</td></tr> <tr><td>BD</td><td>BOARD</td><td>HYD</td><td>HYDRANT</td></tr> <tr><td>BDO</td><td>BUILDING</td><td>ID</td><td>INSIDE DIAMETER</td></tr> <tr><td>B.M.</td><td>BENCHMARK</td><td>INFO.</td><td>INFORMATION</td></tr> <tr><td>B.O.</td><td>BOTTOM OF</td><td>INSUL.</td><td>INSULATION</td></tr> <tr><td>BOF.</td><td>BOTTOM</td><td>LAV</td><td>LAVATORY</td></tr> <tr><td>B.P.</td><td>BASE PLATE</td><td>LT</td><td>LIGHT</td></tr> <tr><td>BRG.</td><td>BEARING</td><td>LT WF</td><td>LIGHT WEIGHT</td></tr> <tr><td>BTWN.</td><td>BETWEEN</td><td>MAINT.</td><td>MAINTENANCE</td></tr> <tr><td>C.J.</td><td>CONSTRUCTION JOINT</td><td>MANUF.</td><td>MANUFACTURER</td></tr> <tr><td>CLG.</td><td>CEILING</td><td>MAX.</td><td>MAXIMUM</td></tr> <tr><td>CLR.</td><td>CLEAR</td><td>MAT</td><td>MATERIAL</td></tr> <tr><td>C.M.U.</td><td>CONCRETE MASONRY UNIT</td><td>M.C.J.</td><td>MASONRY CONTROL JOINT</td></tr> <tr><td>COL.</td><td>COLUMN</td><td>MECH.</td><td>MECHANICAL</td></tr> <tr><td>CONC.</td><td>CONCRETE</td><td>MIN.</td><td>MINIMUM</td></tr> <tr><td>CONT.</td><td>CONTINUOUS</td><td>MISC.</td><td>MISCELLANEOUS</td></tr> <tr><td>CORNL.</td><td>CORNER</td><td>M.O.</td><td>MASONRY OPENING</td></tr> <tr><td>C.T.J.</td><td>CONTRACTION JOINT</td><td>MTL.</td><td>METAL</td></tr> <tr><td>DBL.</td><td>DOUBLE</td><td>N.I.C.</td><td>NOT IN CONTRACT</td></tr> <tr><td>DFT./DTL.</td><td>DETAIL</td><td>N.I.S.</td><td>NOT TO SCALE</td></tr> <tr><td>DIA.</td><td>DIAMETER</td><td>O.C.</td><td>ON CENTER</td></tr> <tr><td>DWL.</td><td>DRAWINGS</td><td>O.D.</td><td>OUTSIDE DIAMETER</td></tr> <tr><td>DWCS.</td><td>EACH FACE</td><td>O.F.</td><td>OUTSIDE FACE</td></tr> <tr><td>E.F.</td><td>EXPANSION JOINT</td><td>PERP.</td><td>PERPENDICULAR</td></tr> <tr><td>E.J.</td><td>ELEVATION</td><td>PL</td><td>PLATE</td></tr> <tr><td>EQ.</td><td>SQUARE</td><td>PTD.</td><td>PAINTED</td></tr> <tr><td>ES.</td><td>EACH SIDE</td><td>QTY.</td><td>QUANTITY</td></tr> <tr><td>E.W.</td><td>EACH WAY</td><td>R.D.</td><td>ROOF DRAIN</td></tr> <tr><td>EXIST.</td><td>EXISTING</td><td>RAD.</td><td>RADIUS</td></tr> <tr><td>EXPAN.</td><td>EXPANSION</td><td>RNF.</td><td>REINFORCED</td></tr> <tr><td>EXT.</td><td>EXTERIOR</td><td>REQ.D.</td><td>REQUIRED</td></tr> <tr><td>F.D.</td><td>FLOOR DRAIN</td><td>RM</td><td>ROOM</td></tr> <tr><td>FDN./FDN.</td><td>FOUNDATION</td><td>R.O.</td><td>ROUGH OPENING</td></tr> <tr><td>F.F.</td><td>FIRE EXTINGUISHER</td><td>SCHED</td><td>SCHEDULE</td></tr> <tr><td>F.F.C.</td><td>FIRE EXTINGUISHER CABINET</td><td>SHT.</td><td>SHEET</td></tr> <tr><td>FIN.</td><td>FINISH</td><td>SHR</td><td>SHRIMP</td></tr> <tr><td>FIN.</td><td>FINISH</td><td>SPEC.</td><td>SPECIFICATION</td></tr> <tr><td>FT</td><td>FEET</td><td>STC</td><td>SOUND TRANSMISSION COEFFICIENT</td></tr> <tr><td>FIG.</td><td>FOOTING</td><td>STRUC.T.</td><td>STRUCTURAL</td></tr> <tr><td>GA.</td><td>GAGE/GAUGE</td><td>SUSP.</td><td>SUSPENDED</td></tr> <tr><td>GALL.</td><td>GALLONS PER MINUTE</td><td>T.O.F.</td><td>TOP OF FOOTING</td></tr> <tr><td>GPM</td><td>GALLONS PER MINUTE</td><td>T.O.S.</td><td>TOP OF SLAB OR SIDEWALK</td></tr> <tr><td>GND.</td><td>GROUND</td><td>T.O.C.</td><td>TOP OF CURB</td></tr> <tr><td>GYP.W.</td><td>GYP.SUM WALL BOARD</td><td>T.O.F.</td><td>TOP OF FOOTING</td></tr> <tr><td>HC</td><td>HANDICAPPED</td><td>T.O.S.</td><td>TOP OF SLAB OR SIDEWALK</td></tr> <tr><td></td><td></td><td>T.O.W.</td><td>TOP OF WALL</td></tr> <tr><td></td><td></td><td>TYP.</td><td>TYPICAL</td></tr> <tr><td></td><td></td><td>U.N.O.</td><td>UNLESS NOTED OTHERWISE</td></tr> <tr><td></td><td></td><td>VERT.</td><td>VERTICLE</td></tr> <tr><td></td><td></td><td>W/</td><td>WITH</td></tr> <tr><td></td><td></td><td>WO.</td><td>WOOD</td></tr> <tr><td></td><td></td><td>W.W.F.</td><td>WELDED WIRE FABRIC</td></tr> </table>	#	NUMBER	HDW.	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XXX	WINDOW NUMBER	A	BRICK VENEER																																																																																																																																																																																																																																																																																																														
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SPECIAL INSPECTIONS REQUIREMENTS SPECIAL INSPECTIONS ARE REQUIRED IN ACCORDANCE WITH IBC 2015 CHAPTER 17, SECTION 1704. SPECIAL INSPECTIONS SHALL KEEP RECORDS OF INSPECTIONS. THE SPECIAL INSPECTOR SHALL FURNISH INSPECTION REPORTS TO THE BUILDING OFFICIAL, AND TO THE REGISTERED DESIGN PROFESSIONALS IN RESPONSIBLE CHARGE. (2015 IBC SECTION 1704.2.4). SEE PROJECT MANUAL, SPECIFICATIONS, STRUCTURAL, MECHANICAL, PLUMBING, AND ELECTRICAL GENERAL NOTES FOR ADDITIONAL SPECIAL INSPECTION REQUIREMENTS.		BUILDING AREAS SEE SHEET G003 FOR AREA PLANS	OWNER & MUNICIPAL DRAWING APPROVALS OWNER: _____ DATE: _____ CITY ENGINEER: _____ DATE: _____ CITY PLANNING & ZONING DEPARTMENT: _____ DATE: _____ CITY FIRE DEPARTMENT: _____ DATE: _____ CITY BUILDING DEPARTMENT: _____ DATE: _____ CITY POLICE DEPARTMENT: _____ DATE: _____																																																																																																																																																																																																																																																																																																														

WARM SPRINGS RESIDENCE #35

190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

PROJECT NO. 22023
DATE: 2023.06.30

REVISIONS:
1 04-27-2023 PER CITY COMMENTS

SHEET TITLE:
COVER SHEET

SHEET NUMBER:
COVER

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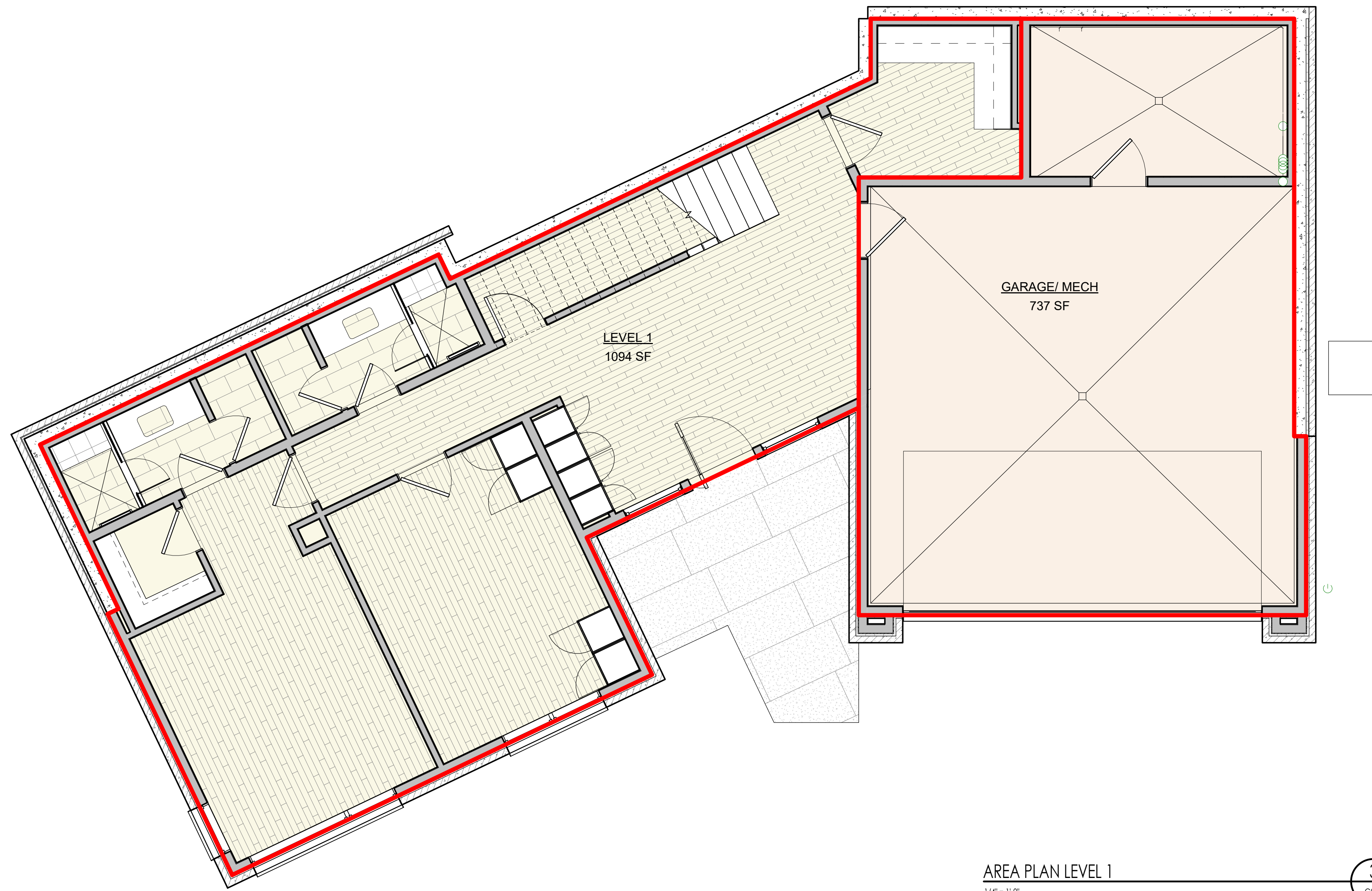
Architecture

Interior Design
Landscape Architecture
Land Planning
Construction Management

7927 So. Highpoint Parkway, Suite 300
Sandwich, Utah 84094
ph. 801.269.0555
fax 801.269.1425
www.thinkaoc.com

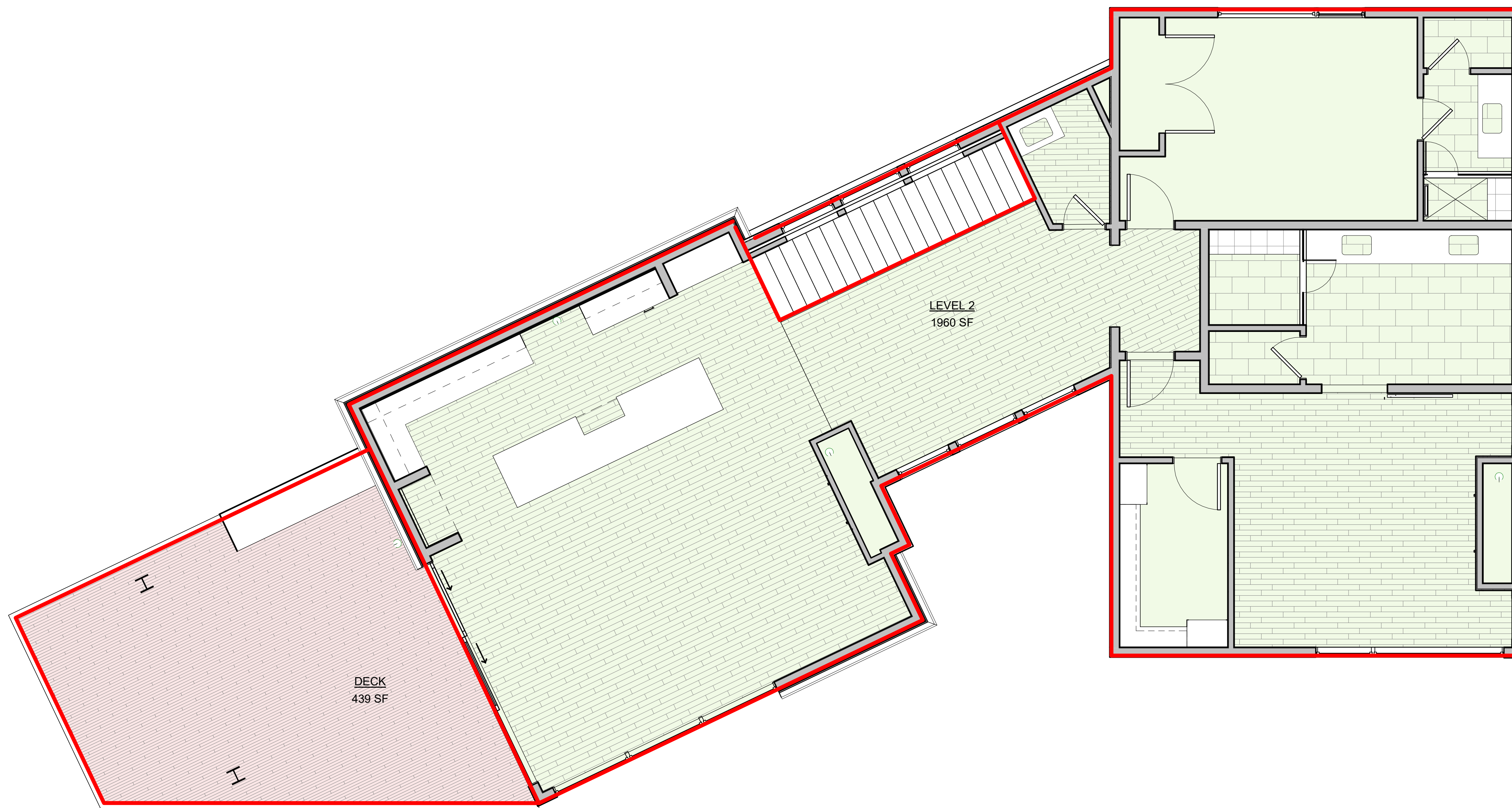
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AREA PLAN LEVEL 1
1/4" = 1'-0"

3
G003



AREA PLAN LEVEL 2
1/4" = 1'-0"

2
G003

BUILDING AREA - FINISHED	
AREA	FINISHED
LEVEL 1	1094 SF
LEVEL 2	1960 SF
	3054 SF

BUILDING AREA - UNFINISHED	
AREA	UNFINISHED
GARAGE/MECH	737 SF
	737 SF

BUILDING AREA - TOTAL	
TOTAL	
	3791 SF

EXTERIOR AREA - DECK	
AREA	EXTERIOR
DECK	439 SF
	439 SF

WARM SPRINGS RESIDENCE #35

190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

PROJECT NO. 22023
DATE: 2023.06.30

REVISIONS:

SHEET TITLE:
BUILDING AREA
ANALYSIS

SHEET NUMBER:

G003

PERMIT SET

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BUILDING KEYNOTES AND SPECIFICATIONS

DIVISION 32- EXTERIOR IMPROVEMENTS/LANDSCAPING

32-04 UNIT PAVERS/ RETAINING WALLS/ STAIRS

GENERAL PRODUCTS
PAVERS SHALL BE THE FOLLOWING: AS PER LANDSCAPE DRAWINGS
PAVERS SHALL BE INSTALLED IN FOLLOWING PATTERN: AS PER LANDSCAPE DRAWINGS
PAVER COLOR SHALL BE SELECTED BY ARCHITECT. AS PER LANDSCAPE DRAWINGS

SUBMITTALS
SAMPLES FOR UNIT PAVERS, JOINT MATERIALS, AND EDGE RESTRAINTS

EXECUTION
DO NOT USE FROZEN MATERIALS OR BUILD ON FROZEN SUBGRADE OR SETTING BEDS. PROTECT UNIT PAVEMENT WORK AGAINST FREEZING FOR 24 HOURS AFTER INSTALLATION.

MIX PAVERS FROM SEVERAL PALLETS OR CUBES, AS THEY ARE PLACED, TO PRODUCE UNIFORM BLEND OF COLORS AND TEXTURES.

CUT UNIT PAVERS WITH MOTOR-DRIVEN MASONRY SAW EQUIPMENT TO PROVIDE PATTERN INDICATED AND TO FIT ADJOINING WORK NEATLY. USE FULL UNITS WITHOUT CUTTING WHERE POSSIBLE. INSTALL EDGE RESTRAINTS BEFORE PLACING UNIT PAVERS.

TOLERANCES: DO NOT EXCEED 1/16-INCH UNIT-TO-UNIT OFFSET FROM FLUSH (UPPAGE) NOR 1/8 INCH IN 24 INCHES AND 1/4 INCH IN 10 FEET FROM LEVEL OR INDICATED SLOPE, FOR FINISHED SURFACE OF PAVING.

COMPACT SOIL SUBGRADE UNIFORMLY AND PLACE AGGREGATE BASE. COMPACT BY TAMPING WITH PLATE VIBRATOR, AND SCREED TO DEPTH AS INDICATED.

PLACE LEVELING COURSE AND SCREED TO A THICKNESS OF 1 TO 1-1/2 INCHES, TAKING CARE THAT MOISTURE CONTENT REMAINS CONSTANT AND DENSITY IS LOOSE AND CONSTANT UNTIL PAVERS ARE SET AND COMPACTED. TREAT LEVELING COURSE WITH HERBICIDE TO INHIBIT GROWTH OF GRASS AND WEEDS.

SET PAVERS WITH A MINIMUM JOINT WIDTH OF 1/16-INCH AND A MAXIMUM OF 1/8 INCH. BEING CAREFUL NOT TO DISTURB LEVELING BASE. IF PAVERS HAVE SPACER BARS, PLACE PAVERS HAND TIGHT AGAINST SPACER BARS.

VIBRATE PAVERS INTO LEVELING COURSE AND SPREAD DRY SAND AND FILL JOINTS IMMEDIATELY AFTER VIBRATING PAVERS INTO LEVELING COURSE. VIBRATE PAVERS AND ADD SAND UNTIL JOINTS ARE COMPLETELY FILLED, THEN REMOVE EXCESS SAND. LEAVE A SLIGHT SURPLUS OF SAND ON THE SURFACE FOR JOINT FILLING.

32-10 IRRIGATION SYSTEMS
GENERAL PRODUCTS
SEE LANDSCAPE DRAWINGS

ALL IRRIGATION SHALL MEET ALL CITY LANDSCAPE REQUIREMENTS.

32-11 PLANTING
GENERAL PRODUCTS
SEE LANDSCAPE DRAWINGS.

ALL PLANTING SHALL MEET ALL CITY LANDSCAPE REQUIREMENTS.



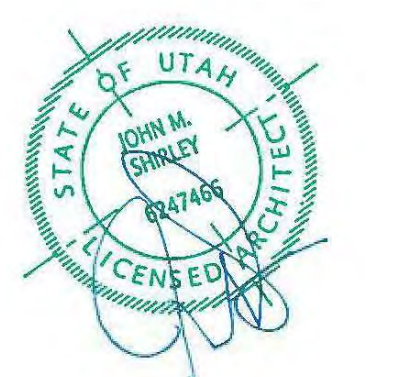
Architecture

Architecture
Interior Design
Landscape Architecture
Land Planning
Construction Management

7927 So. Highpoint Parkway, Suite 300
Sandwich, Utah 84094
ph. 801.269.0055
fax 801.269.1425
www.thinkaec.com

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WARM SPRINGS RESIDENCE #35

190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

PROJECT NO. 22023
DATE: 2023.06.30

REVISIONS:

SHEET TITLE:
SPECIFICATIONS

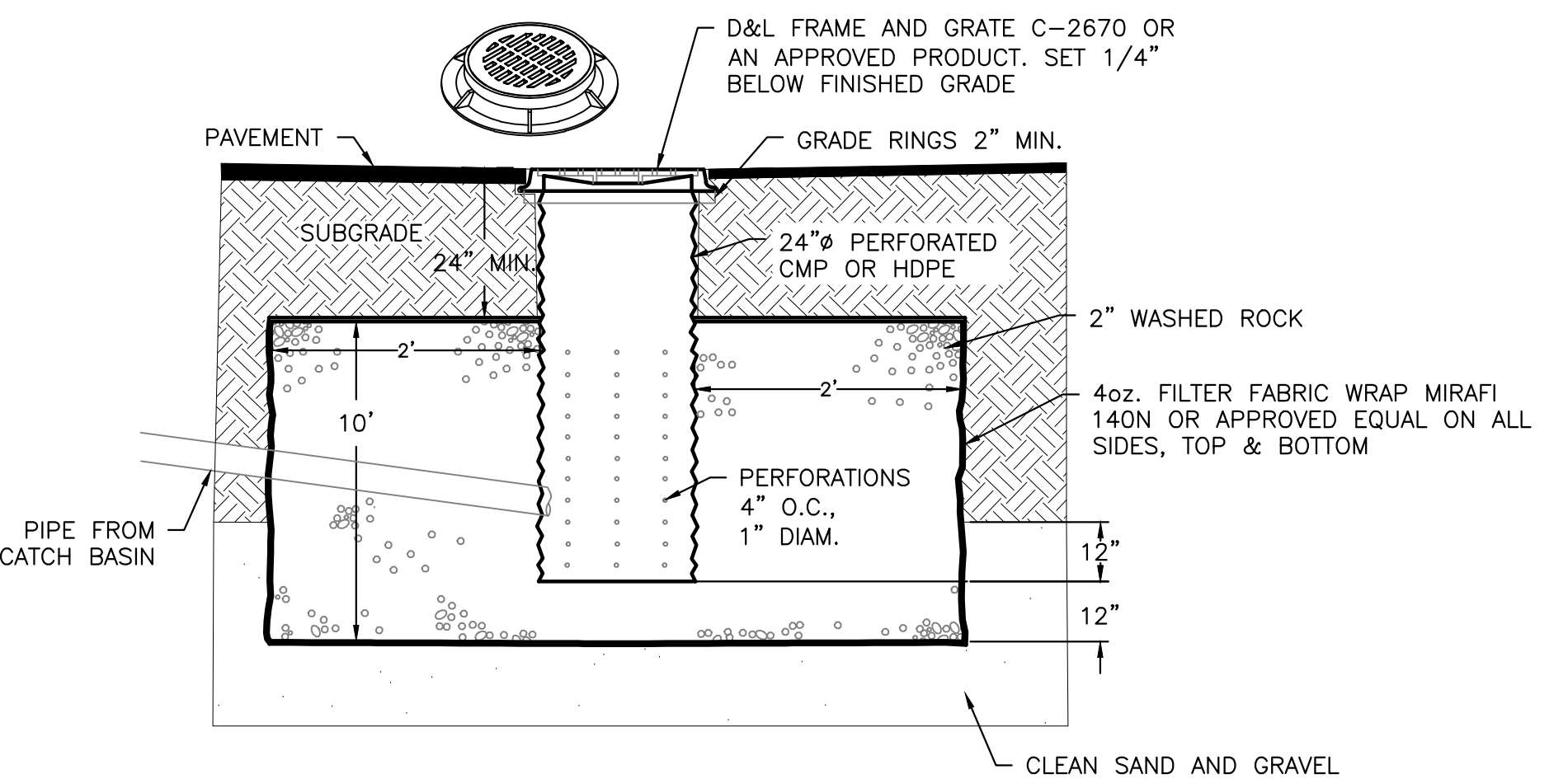
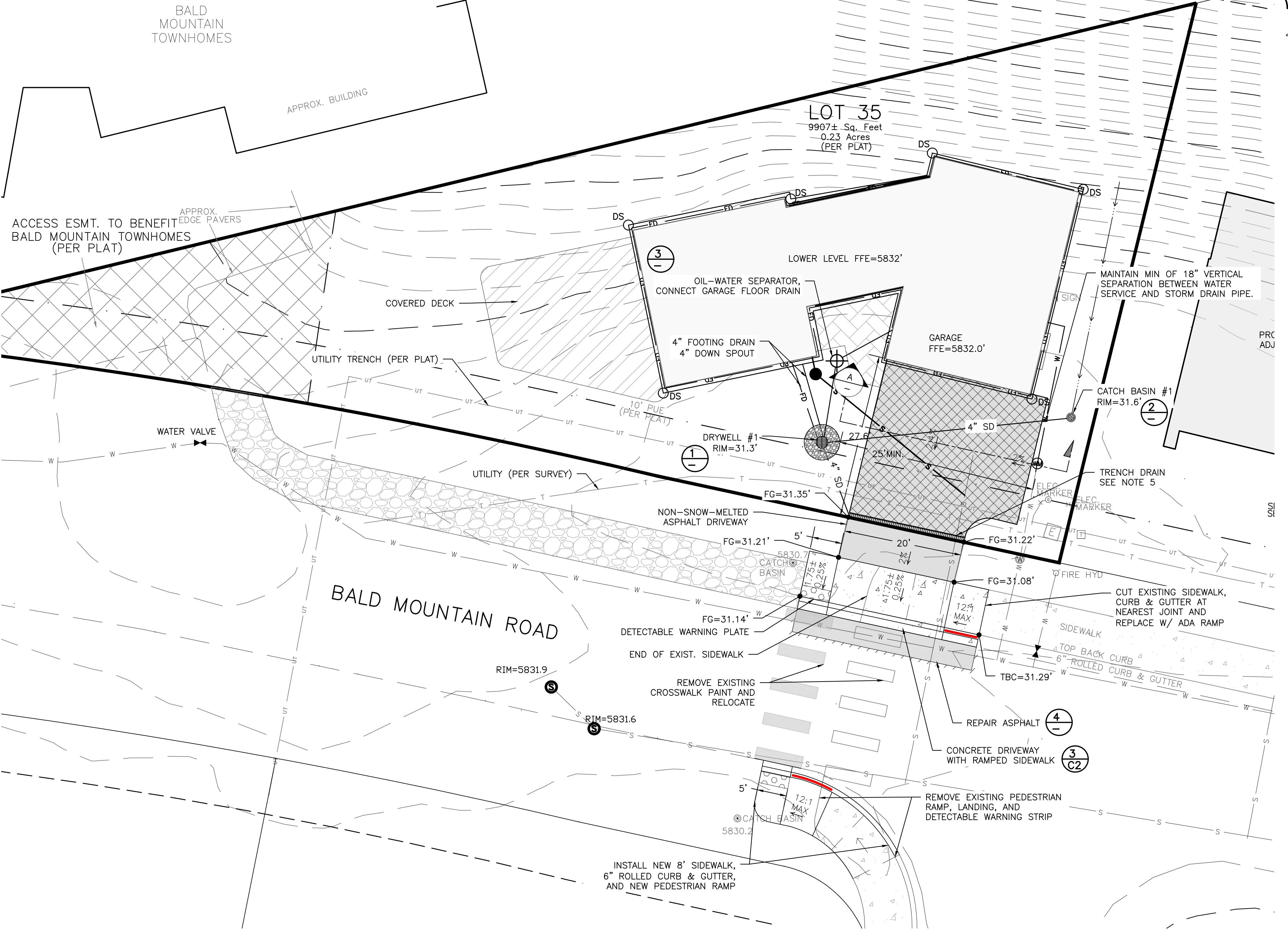
SHEET NUMBER:
G010

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PERMIT SET

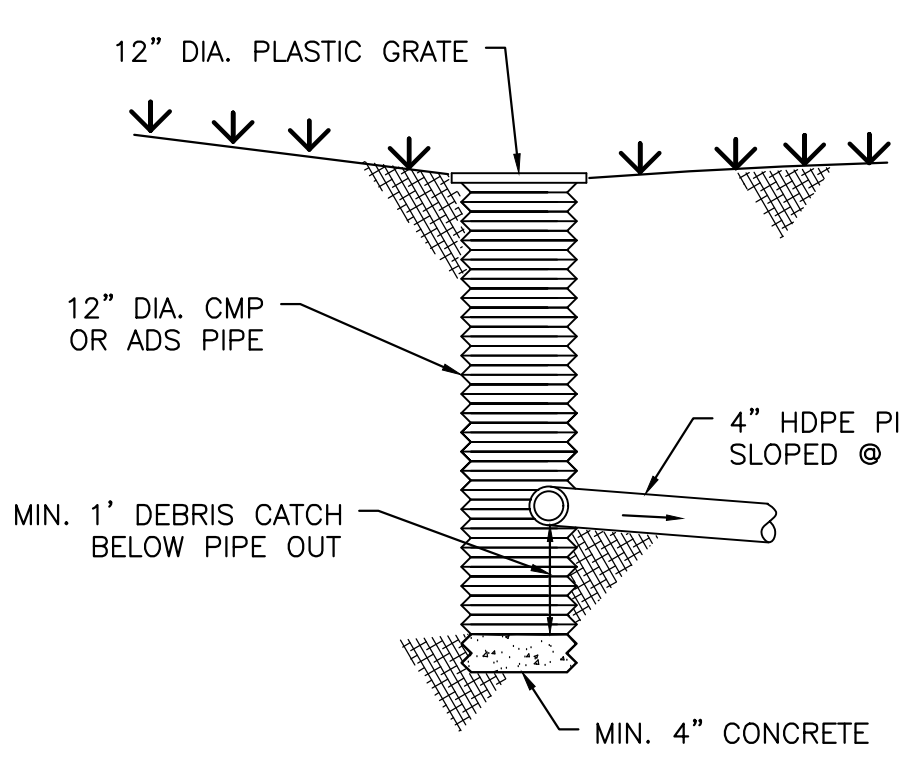
LEGEND	
PROPERTY LINE	---
ADJOINING PROPERTY LINE	---
EASEMENT	---
FENCE	X
EDGE OF PAVEMENT	---
SEWER	S
SEWER MANHOLE (MH)	⊙
WATER	W
WATER GATE VALVE	⊕
HYDRANT	⊙
CURB STOP	⊙
TELEPHONE	T
UTILITY TRENCH	UT
ELEVATION CONTOUR	5775
PROPOSED ELEV CONTOUR	59
SAWCUT LINE	---
CURB TRANSITION	---
FLOW LINE	---
FOOTING DRAIN	FD
STORM DRAIN PIPE	---
DOWN SPOUT	DS
CATCH BASIN-EXIST	⊙
CATCH BASIN	⊙
OIL-WATER SEPARATOR	⊕
DRYWELL	⊕
ASPHALT PAVEMENT	---
PAVERS	---
GRAVEL	---
CONCRETE	---
FG	---
EG	---
FINISHED GRADE	---
EXISTING GROUND	---

- GENERAL NOTES**
- CONTRACTOR SHALL FIELD VERIFY LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING CONSTRUCTION. ANY CONFLICT SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER.
 - CONTRACTOR SHALL NOTIFY DIGLINE (1-800-342-1585) AT LEAST 48 HOURS PRIOR TO BEGINNING CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING UTILITIES ENCOUNTERED DURING CONSTRUCTION.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR DUST CONTROL DURING THE CONSTRUCTION OF ALL ITEMS HEREON. DUST CONTROL SHALL BE CONTINUOUS DURING CONSTRUCTION, 24 HOURS PER DAY 7 DAYS PER WEEK.
 - CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM THE HOUSE.
 - TRENCH DRAIN SHALL BE A 6" WIDE HDPE CHANNEL WITH A 0.75 BUILT IN CHANNEL SLOPE (ZURN FLO-THRU MODEL Z886 OR EQUIVALENT). GRATE SHALL BE DUCTILE IRON WITH A SLOTTED PATTERN. CATCH BASIN SHALL BE 6" WIDE X 20" LONG X 20" DEEP AND SHALL BE MADE OF HDPE. OUTLET PIPE SHALL BE 4" DIAMETER. (FLO-THRU MODEL Z887 OR EQUIVALENT). ALL COMPONENTS SHALL BE RATED FOR H2O LOADING.
 - ALL WORK WITHIN THE CITY RIGHT OF WAY SHALL CONFORM TO CITY OF KETCHUM STANDARDS.
 - AN OIL-WATER SEPARATOR IS REQUIRED FOR THE GARAGE DRAIN PRIOR TO ENTERING THE DRYWELL. OIL-WATER SEPARATOR MAY BE INSTALLED IN GARAGE IF DESIRED.
 - CONCRETE WITHIN CITY RIGHT-OF-WAY SHALL BE TITAN MIX OR EQUAL. ALTERNATE COLD WEATHER MIX WILL NEED TO BE SUBMITTED TO THE CITY FOR REVIEW AND APPROVAL PRIOR TO PLACEMENT.
 - CONCRETE SHALL BE SEALED WITH AN OPAQUE SEALER.
 - 6" ROLLED CURB & GUTTER SHALL BE PER CITY OF KETCHUM STANDARD DETAIL #4.
 - CONCRETE SIDEWALK SHALL BE PER CITY OF KETCHUM STANDARD DETAIL #7.
 - ADA RAMP SHALL BE PER CITY OF KETCHUM STANDARD DETAILS #8 AND #9 AND MODIFIED FOR MID-BLOCK CROSSING.

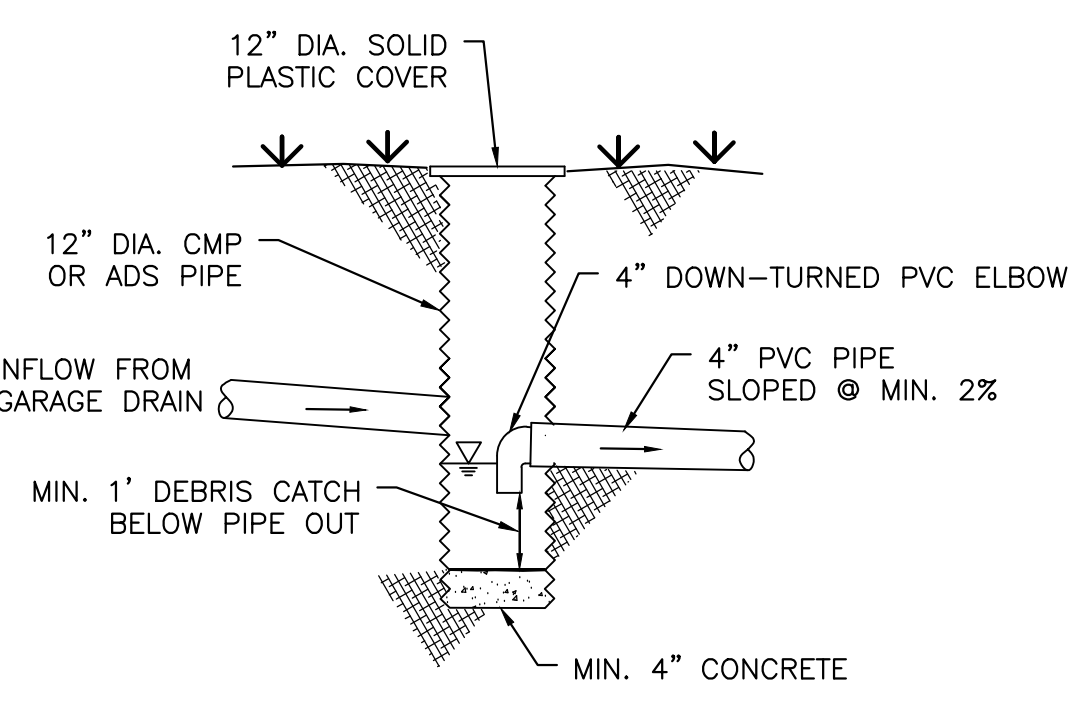


- NOTES:**
- THE BED SHALL BE EXCAVATED A MINIMUM OF 24" INTO CLEAN SAND AND GRAVEL.
 - MAXIMUM DEPTH SHALL NOT EXCEED 12 FEET.
 - IF CLEAN SAND AND GRAVEL IS NOT ENCOUNTERED WITHIN 12 FEET, THE CONTRACTOR SHALL CONTACT THE DESIGN ENGINEER.
 - GRATE OR SOLID LID AS APPROVED BY CITY OF KETCHUM.

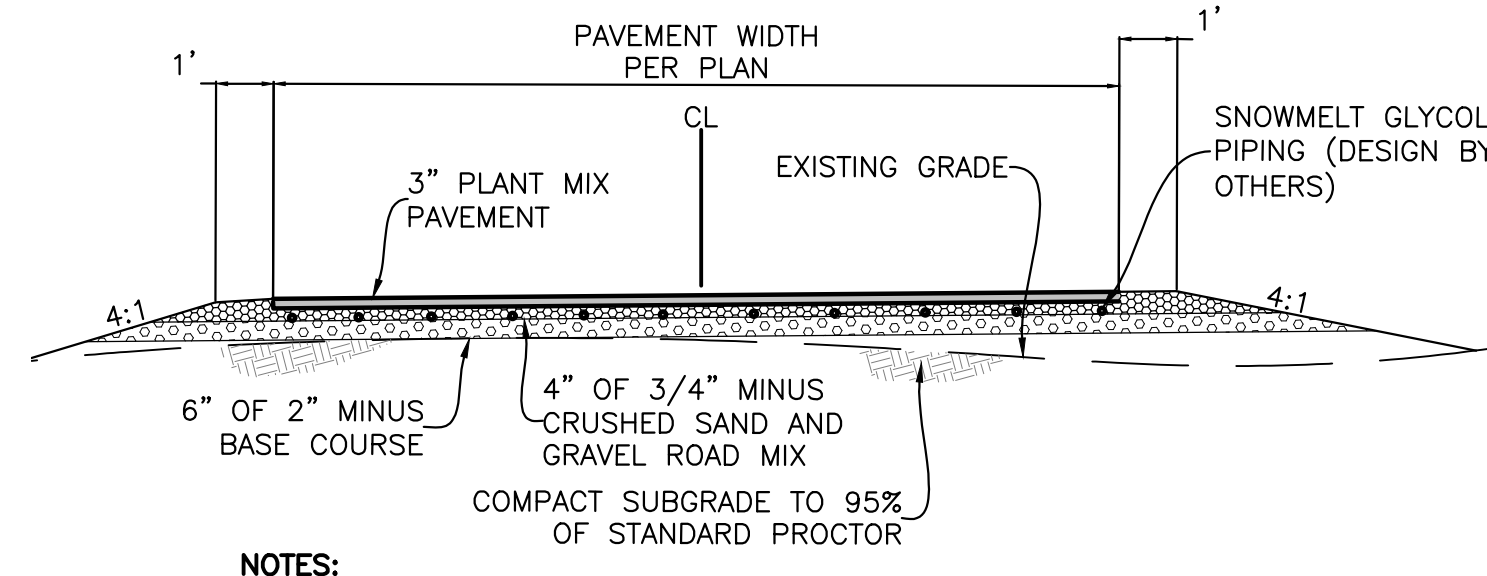
1 TYPICAL DRYWELL DETAIL
SCALE: NONE



2 12" CATCH BASIN PROFILE
NOT TO SCALE

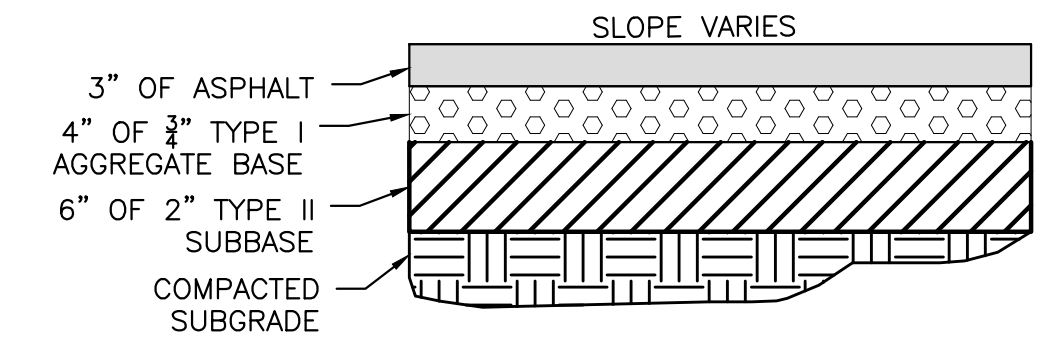
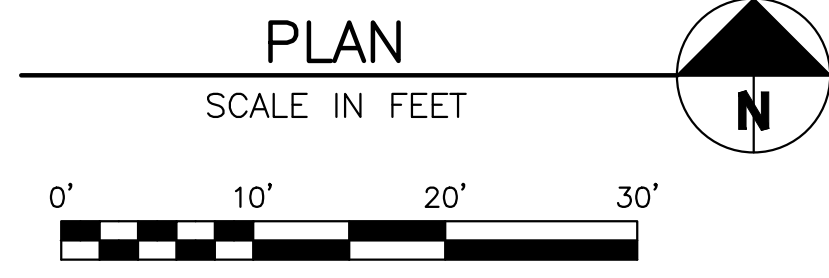


3 OIL-WATER SEPARATOR
NOT TO SCALE

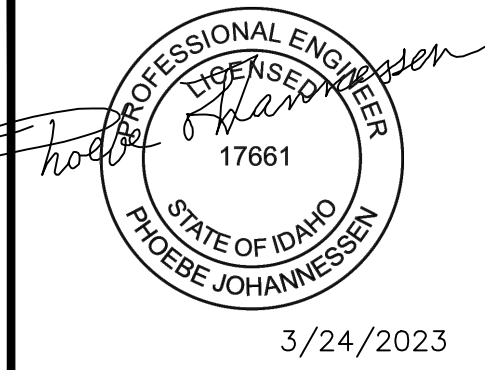


- NOTES:**
- COMPACT DRIVEWAY SUBGRADE AND ALL STRUCTURAL FILL MATERIAL TO AT LEAST 95% OF THE MAXIMUM DENSITY OF EACH MATERIAL ACCORDING TO STANDARD PROCTOR ASTM D-698.

A DRIVEWAY SECTION WITH SNOW-MELTED PAVEMENT
SCALE: NTS



4 TYPICAL STREET ASPHALT SECTION
SCALE: NONE



REVISIONS	DESCRIPTION	DATE	BY
No. 1			



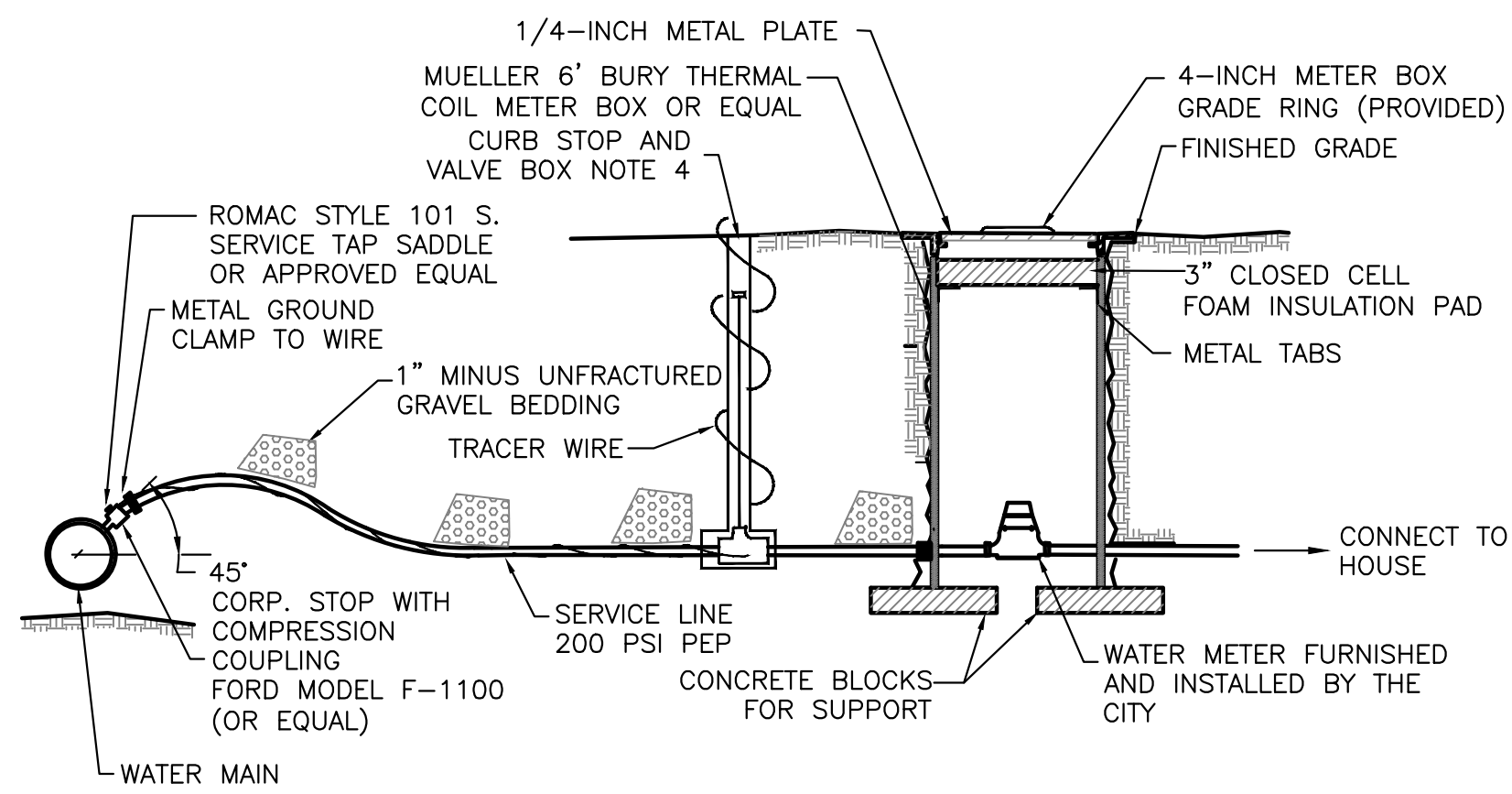
PREPARED BY:
BENCHMARK ASSOCIATES, P.A.
P.O. BOX 733 100 BELL DRIVE
KETCHUM, IDAHO 83340
(208) 726-9512
FAX 726-9514
WEB: WWW.BMASB.COM
MAIL: WWW.BMASB.COM

GRADING & DRAINAGE PLAN
WARM SPRINGS RANCH LOT 35
T4N, R17E, SEC 12, B.M., BLAINE COUNTY, IDAHO
PREPARED FOR: MAGLEBY CONSTRUCTION

DRAWN BY: SLS
DESIGNED BY: SLS
CHECKED BY: PLJ
DATE: 03/24/2023
PROJECT NO.: 22076

DRAWING NO.

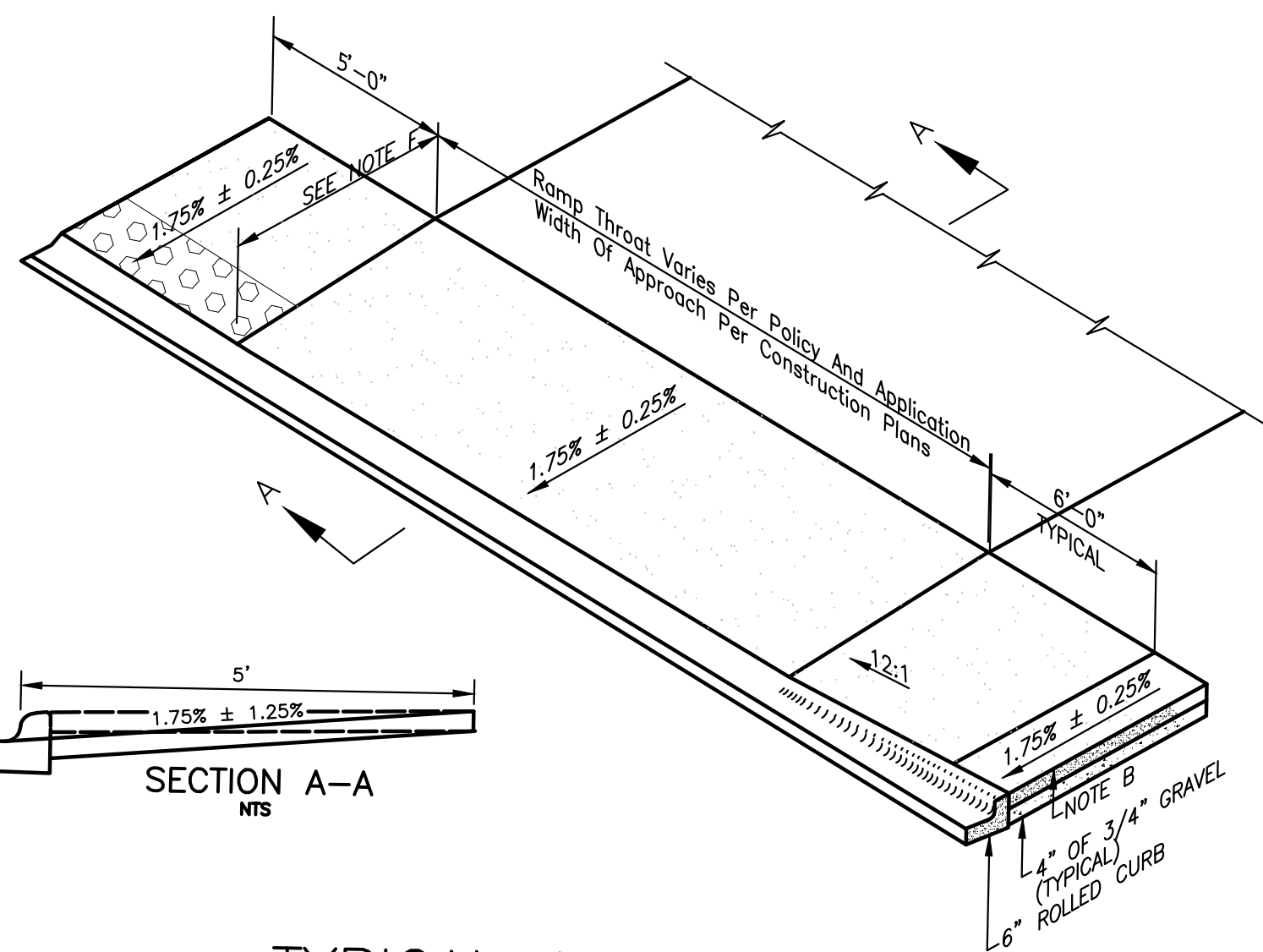
C-1



NOTES

1. WATER SERVICE LINE SHALL HAVE A 6" MIN. BURY DEPTH
2. SERVICE LINE SHALL BE 1" DIAMETER POLYETHYLENE PIPE UNLESS OTHERWISE SPECIFIED.
3. WATER SERVICE LINES SHALL BE BEDDED WITH 1" MINUS UNFRACTURED GRAVEL. BEDDING SHALL BE INSTALLED 4" UNDER THE PIPE AND 6" OVER THE PIPE.
4. FORD MODEL B-111 RESILIENT SEAT, CURB BALL VALVE (OR EQUAL), FORD EXTENSION CURB BOX WITH ARCHED BASE, 1-INCH UPPER SECTION, AND 2 HOLE "ERIE" PATTERN LID.

1 WATER SERVICE AND METER CONNECTION
SCALE: N.T.S.

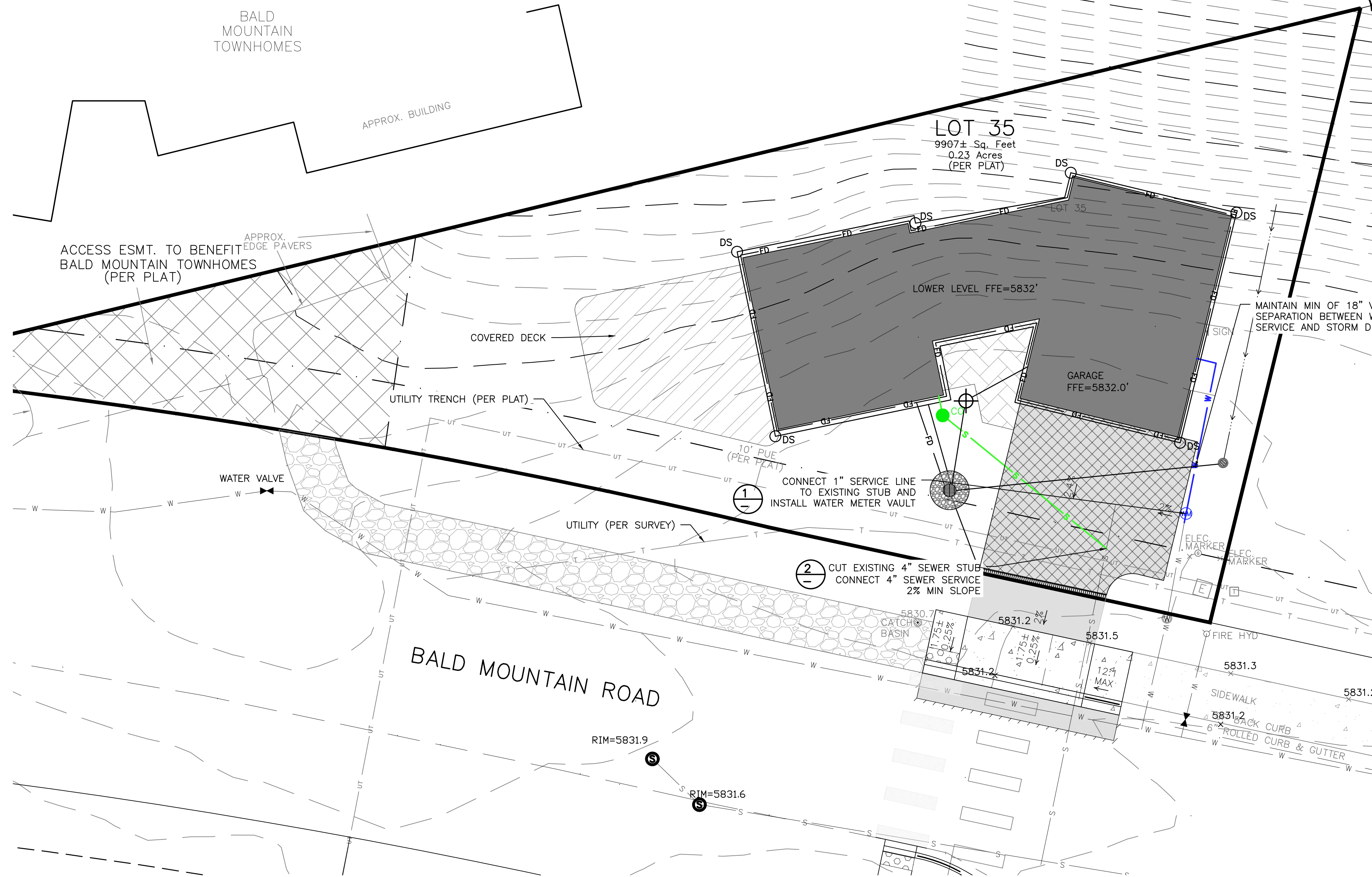


TYPICAL APPROACH

NOTES:

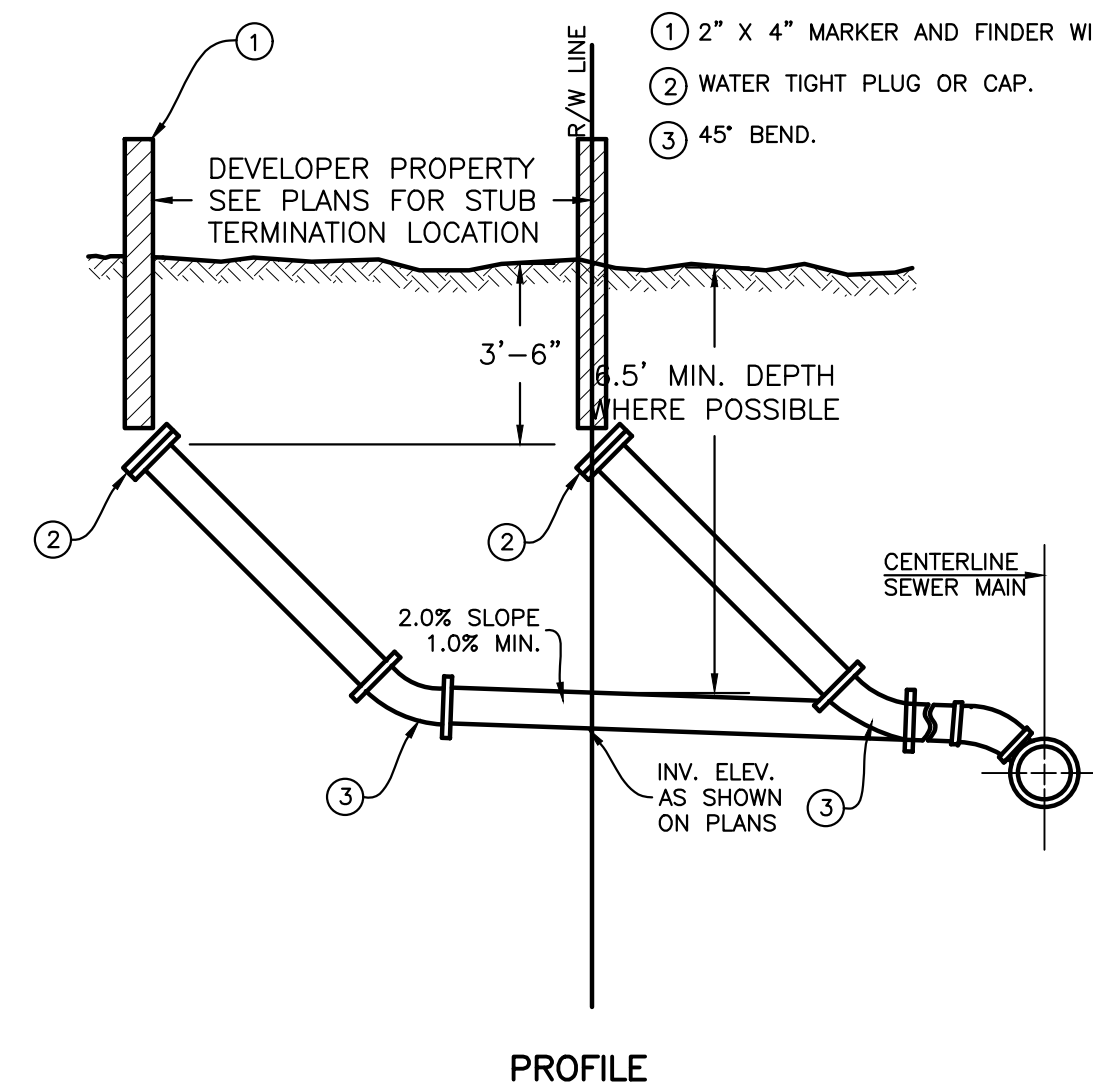
- APPROACH TO CONFORM TO THE LATEST ADA STANDARDS.
- INSTALL EXPANSION JOINT AT TIP OF APPROACH WINGS WHERE SIDEWALK CHANGES THICKNESS.
- BASE TO BE A 4" THICKNESS OF 3/4" MINUS CRUSHED AGGREGATE PER SECTION-802.
- APPROACH THROAT WIDTHS SET BY POLICY AND APPLICATION. ALL CONCRETE TO BE 6" THICK FROM TIP OF WING TO TIP OF WING UP TO THE EXPANSION JOINT. WHEN SIDEWALK IS SEPARATE FROM CURB THE SIDEWALK IMMEDIATELY BEHIND THE APPROACH THROAT SHALL BE 6" THICK ALSO.
- ALL CONCRETE SHALL BE CLASS 3000 PER SECTION-703.
- SIDEWALK WIDTH IS 8 FEET.

3 CONCRETE DRIVEWAY WITH RAMPED SIDEWALK
SCALE: NONE



LEGEND

- 2" x 4" MARKER AND FINDER WIRE PER SD-512.
- WATER TIGHT PLUG OR CAP.
- 45° BEND.



NOTES

1. INSULATION REQUIRED WHERE SEWER LINE BURY DEPTH IS LESS THAN 5'.

2 STANDARD SEWER SERVICE CONNECTION DETAIL
SCALE: NONE

PROFESSIONAL ENGINEER
STATE OF IDAHO
PHILIP JOHANNESSEN
17661
3/24/2023

REVISIONS	NO.	DESCRIPTION	DATE	BY
	1			



PREPARED BY:
BENCHMARK ASSOCIATES, P.A.
P.O. BOX 733 100 BELL DRIVE
KETCHUM, IDAHO 83340
(208) 726-9512
FAX 726-9514
WEB: WWW.BMA5B.COM
MAIL: WWW.BMA5B.COM

UTILITY PLAN
WARM SPRINGS RANCH LOT 35
T4N, R17E, SEC 12, B.M., BLAINE COUNTY, IDAHO
PREPARED FOR: MAGLEBY CONSTRUCTION

DRAWN BY: SLS
DESIGNED BY: SLS
CHECKED BY: PLJ
DATE: 03/24/2023
PROJECT NO.: 22076

DRAWING NO.

C-2

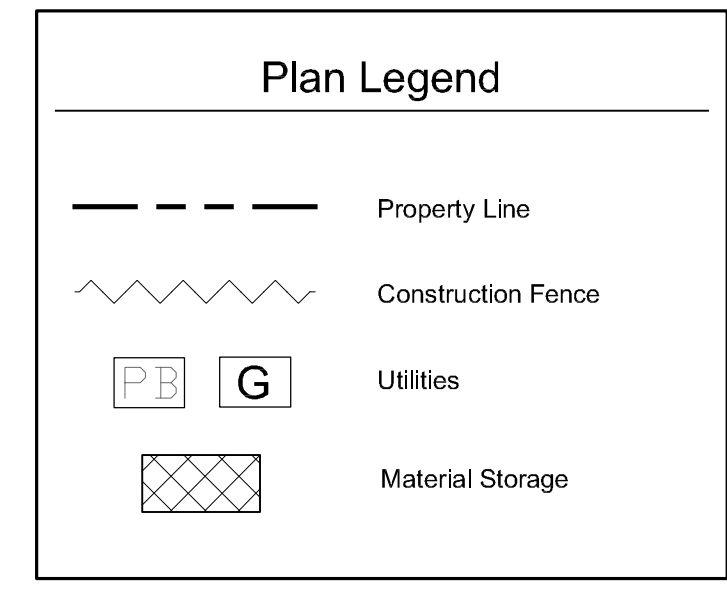
Cut And Fill:

Total Cut = 450 Cubic Yards

Total Fill = 100 Cubic Yards

Total Export = 350 Cubic Yards

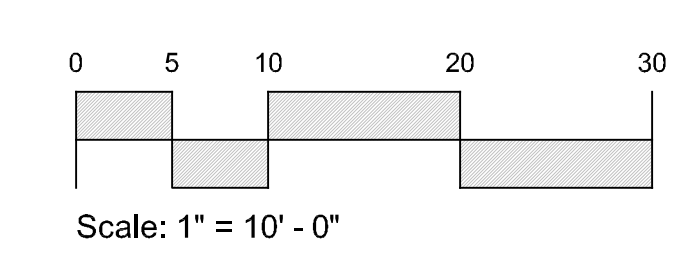
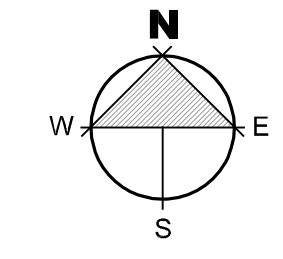
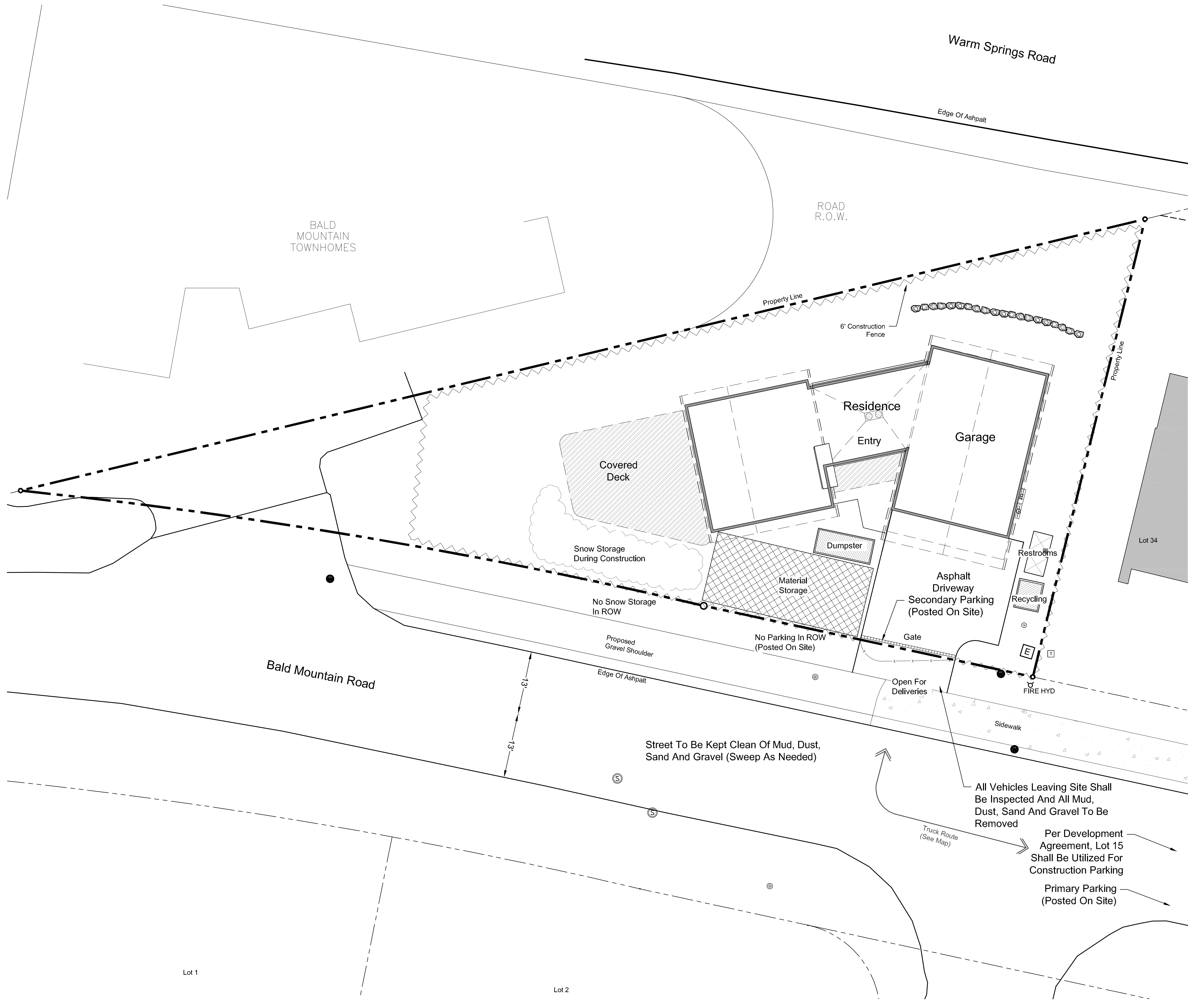
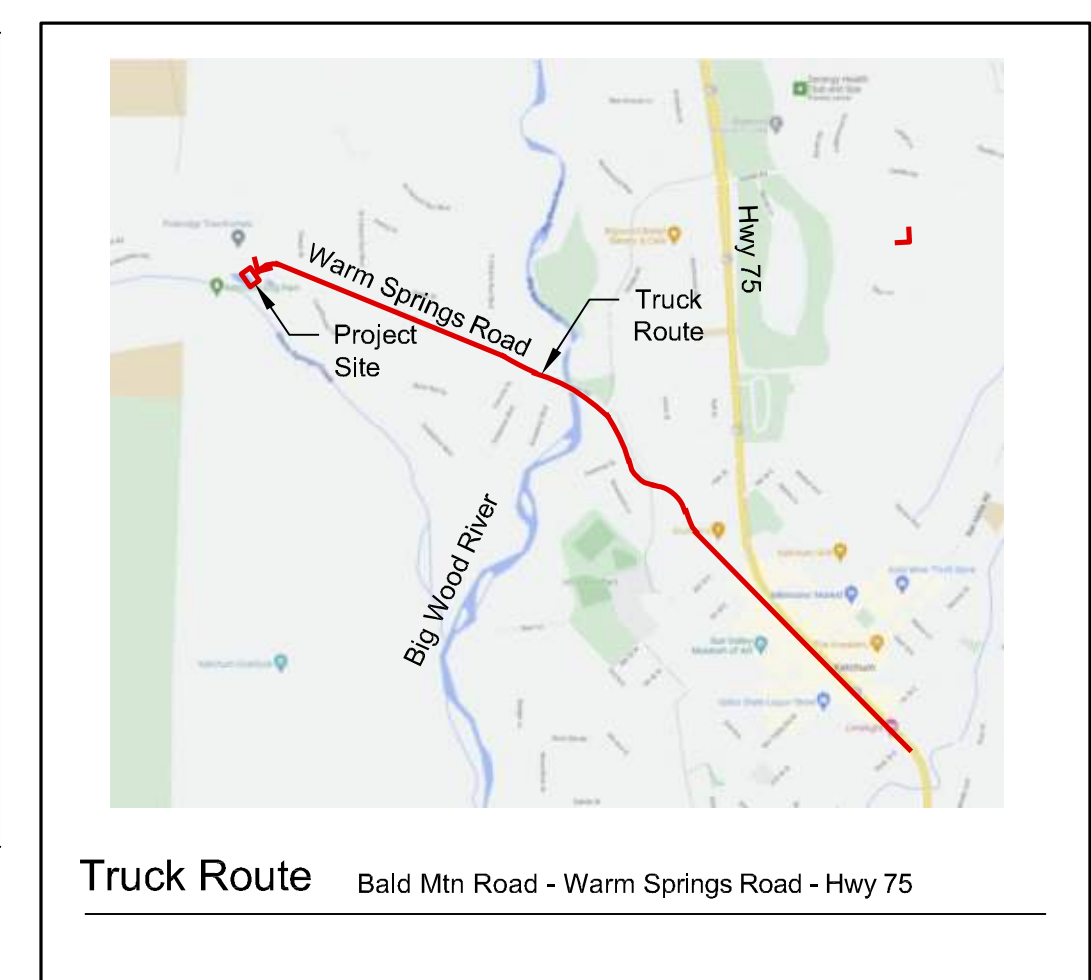
- General Notes**
1. Base map information taken from survey by Benchmark Associates dated 11/24/21 and from on-site information. Architectural information provided by Think Architecture dated 02/22/22. Contractor shall verify conditions in the field prior to construction.
 2. Landscape architect is not responsible for any deviation from these plans, unless such changes are authorized by the landscape architect in writing.
 3. All existing utilities are underground. All new utilities shall be underground.
 4. Site serviced by City of Ketchum.



NOTES:

(1) See Civil Plans for All Work in Right of Way.

- CONSTRUCTION ACTIVITY NOTES:**
- (1) Dust Control Using Water Truck as Needed.
 - (2) All Construction Traffic Shall Have Tires Inspected for Mud, Sand, and Gravel Prior to Leaving Site. Any Material Shall be Removed Prior to Entering City Streets.
 - (3) Contractor is Responsible for Snow Removal of Site and Construction Parking.
 - (4) Any Temporary Use Of The Public Right-Of-Way Will Require A Temporary Use Right-Of-Way (TURP) Permit From The City.
 - (5) All Neighbors In The Project Vicinity Shall Be Provided Notice Of The Project, Schedule And The General Contractor's Contact Information In Advance Of Construction.
 - (6) The Job Site Shall be Kept In A Clean And Orderly Condition, Trash Shall be Picked Up On The Site And Surrounding Areas On A Daily Basis, And Materials Shall be Stored In Neat, Tidy Piles.
 - (7) The Condition Of The Right-Of-Way Shall be Documented With Photographs And A Site Visit With Street Department Personnel. Repair Of Damage To The Right-Of-Way Shall be The Responsibility Of The General Contractor.
 - (8) Manholes May Not be Obstructed At Any Time. In Addition, Minimum Three Feet Clear Shall be Maintained On Back And Sides Of Fire Hydrants, And Minimum 15 Feet Clear Shall be Maintained On The Front, Street Side Of Fire Hydrants.
 - (9) Speed Limits For Construction Vehicles Shall be Limited To 15 MPH Within One Block Of The Construction Site, Unless Otherwise Determined Be The Ketchum Police Department.



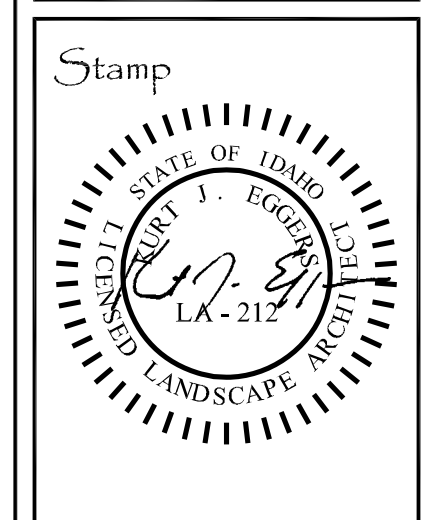
Warm Springs Ranch Residences
Block 1, Lot 35
Ketchum, Idaho

Job No: 22.28

Scale: 1" = 10'-0"

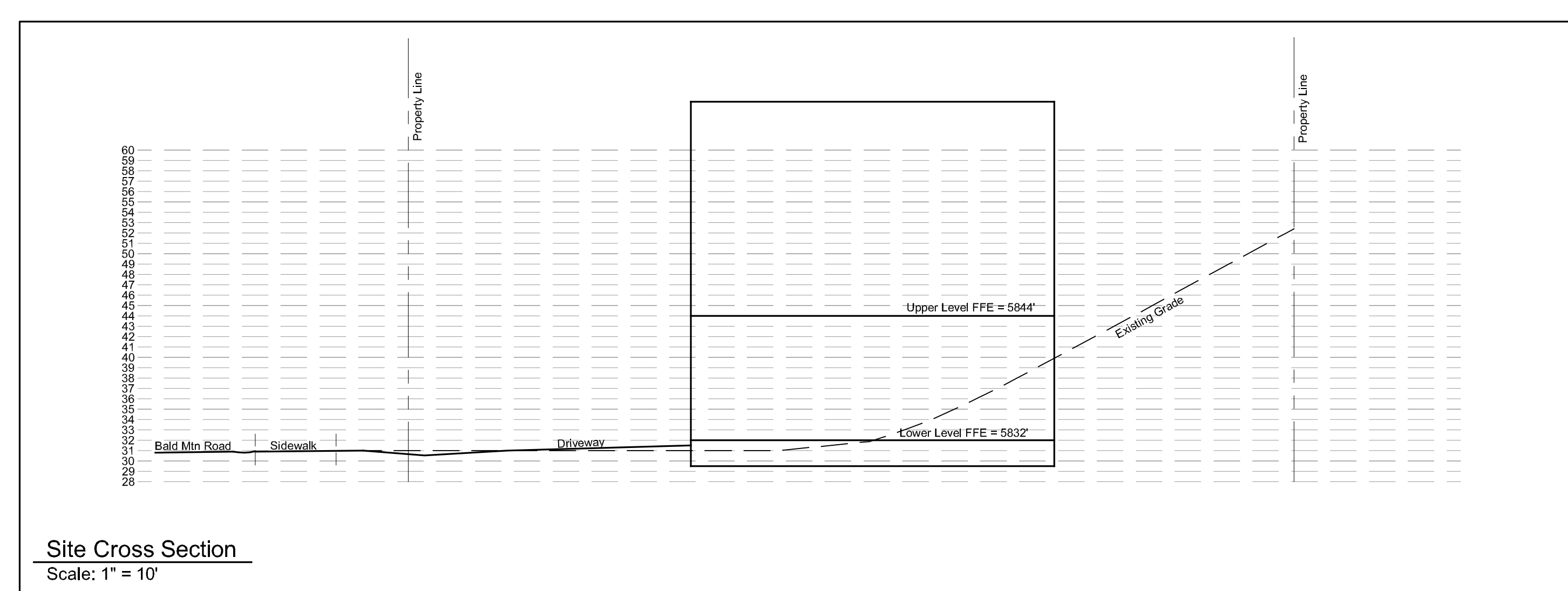
Issue/Revisions	Date
Design Review	05/22/23
RVSD	05/08/23

All information appearing herein shall not be construed, interpreted or otherwise used without the written consent of Eggers Associates P.A.



Sheet Title:
Construction Management

Sheet No:
L1



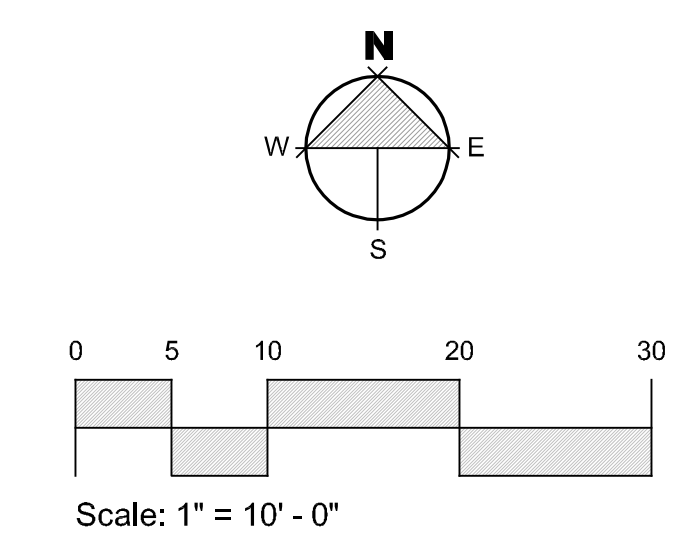
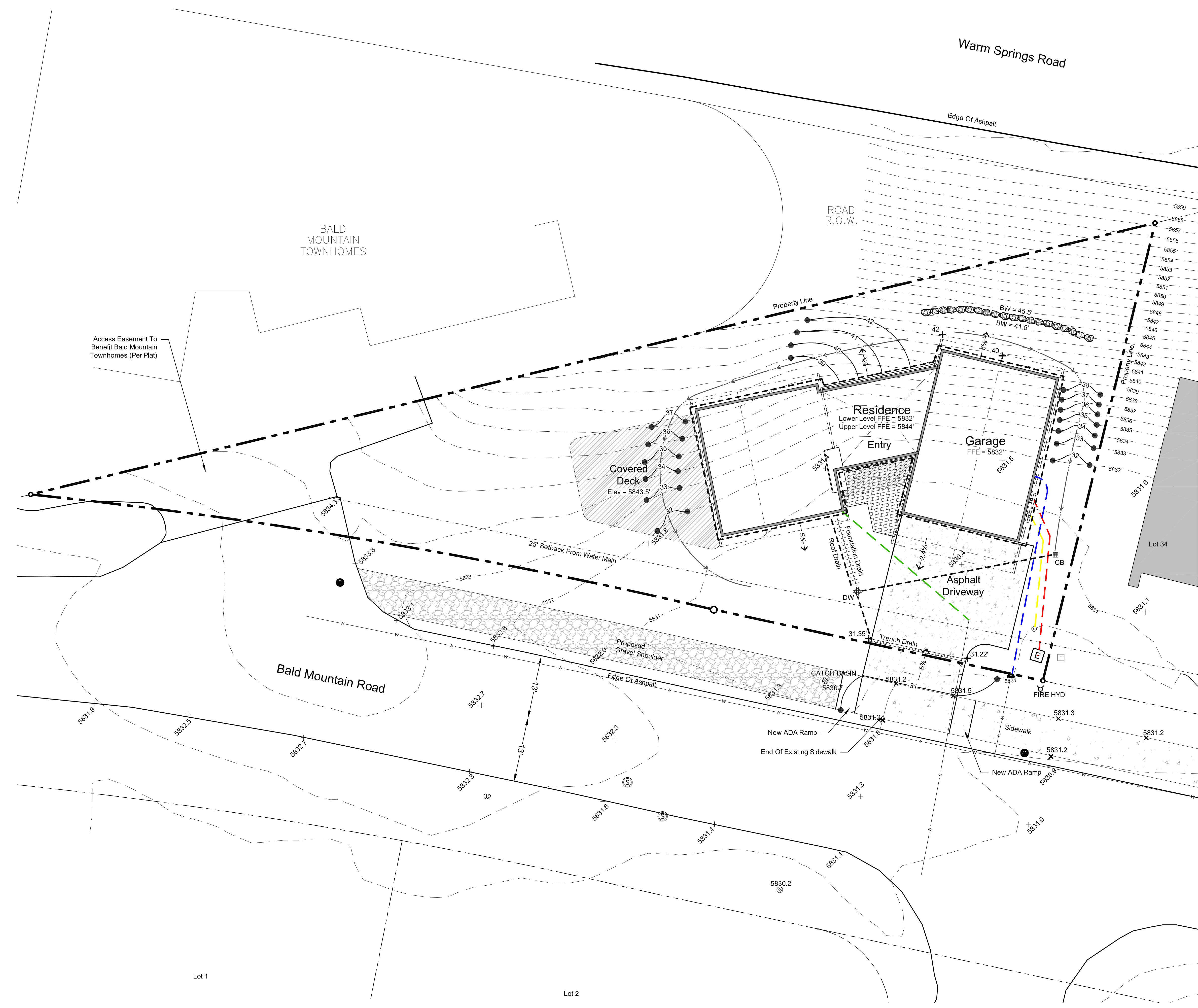
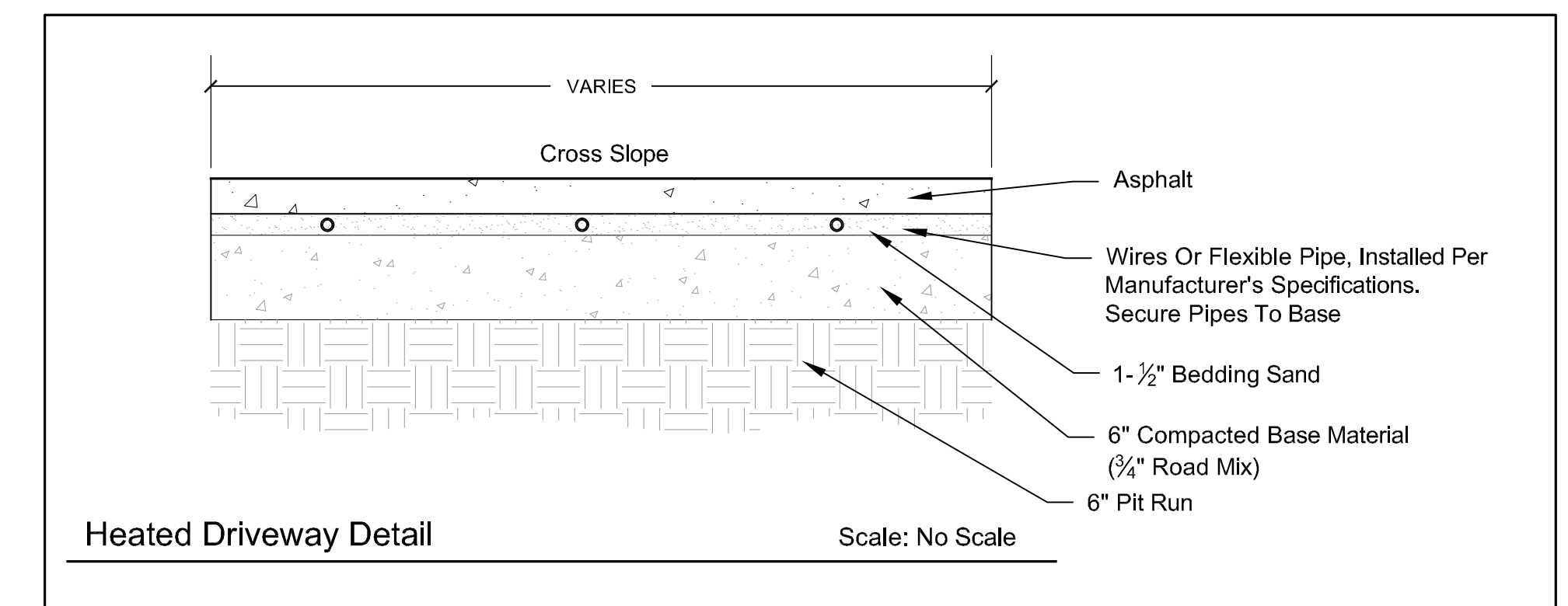
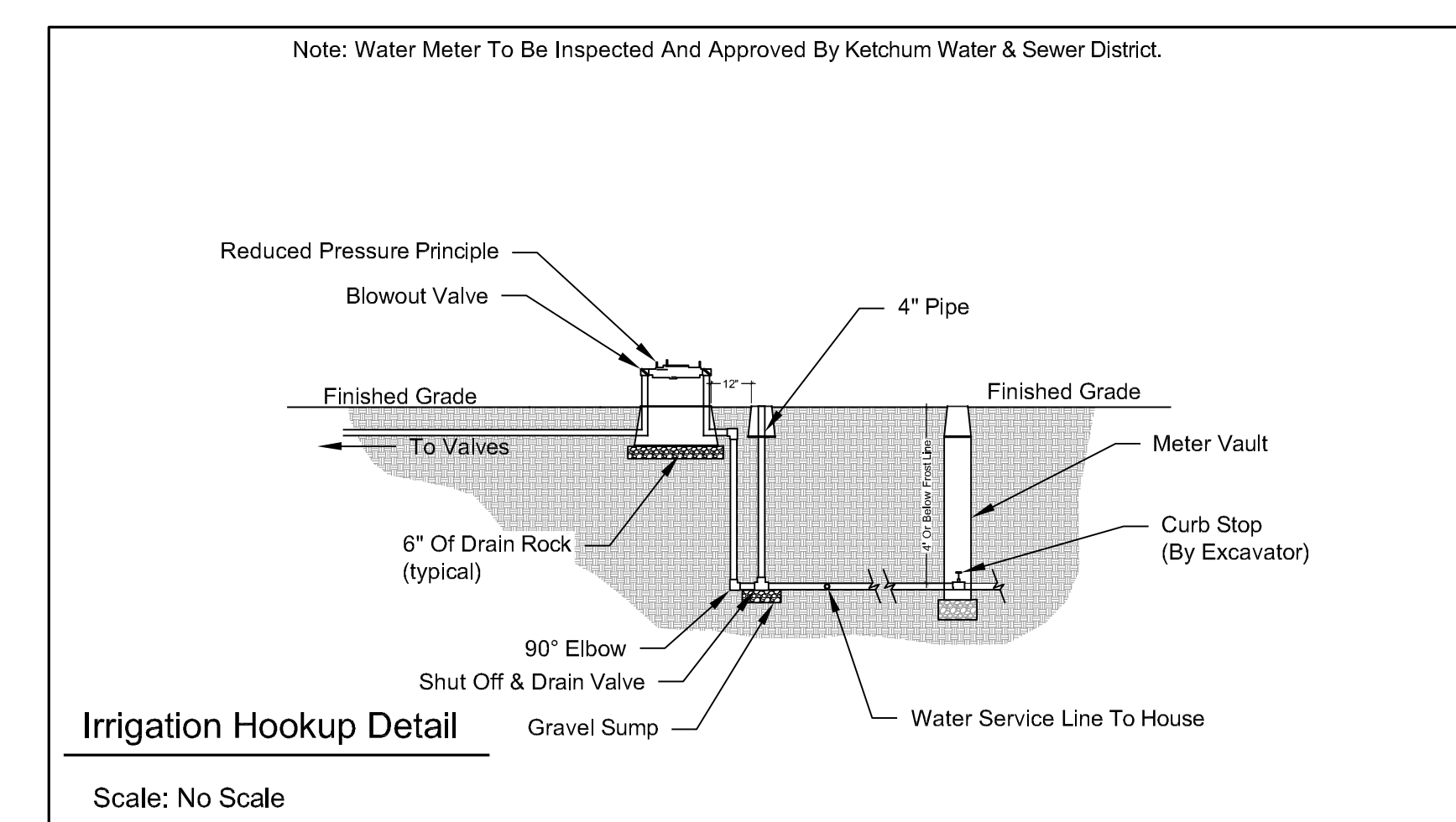
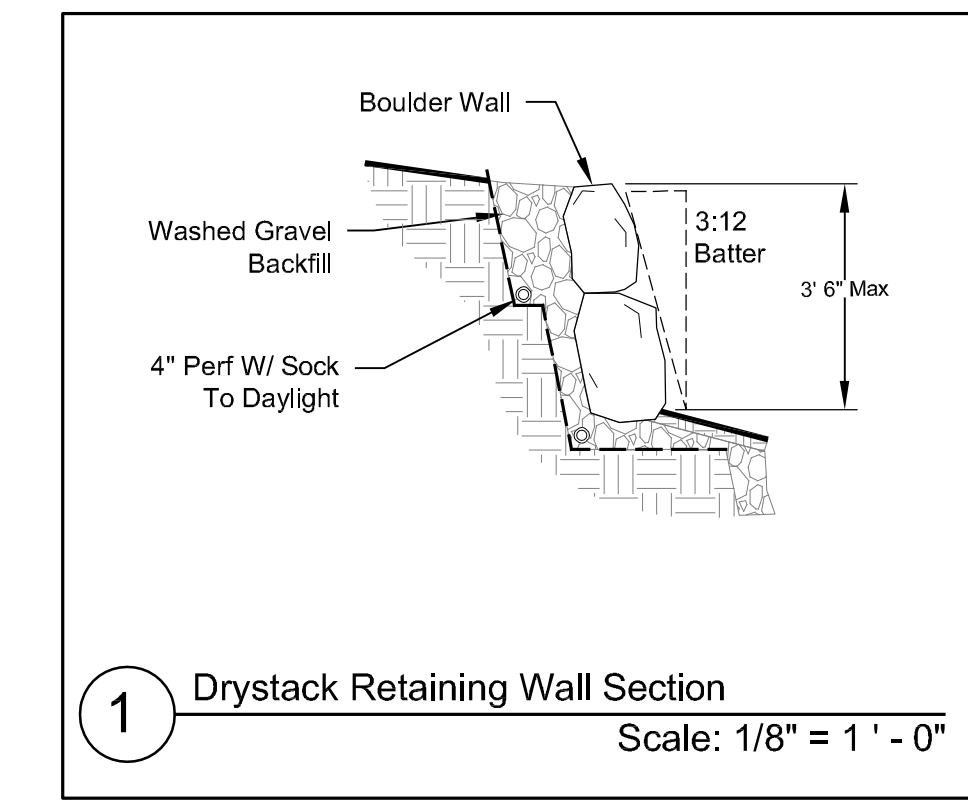
Cut & Fill	
Excavation:	
Building/Garage:	450 Cu/Yds
Site:	0 Cu/Yds
Total Cut: 450 Cu/Yds	
Fill:	
Site Landscape Fill:	0 Cu/Yds
Driveway:	100 Cu/Yds
Total Fill: 100 Cu/Yds	
Total Export: 350 Cu/Yds	

- General Notes**
1. Base map information taken from survey by Benchmark Associates dated 11/24/21 and from on-site information. Architectural information provided by Think Architecture dated 02/22/22. Contractor shall verify conditions in the field prior to construction.
 2. Landscape architect is not responsible for any deviation from these plans, unless such changes are authorized by the landscape architect in writing.
 3. All existing utilities are underground. All new utilities shall be underground.
 4. Site serviced by City of Ketchum.

***See Civil Plan (C1) For Site Grading And Drainage**

Plan Legend

---	Property Line
- - - - -	Existing Contour
x 92.7 (92.5)	Existing Spot Elevations
● 95	Proposed Contour
⊕	Landscape Drywell
■	Catch Basin
2% Slope	Proposed Drainage Direction With Slope Percentage
+ 92.5	Proposed Spot Elevations
	4" Perforated Footing Drain
----	4" Solid Drain Pipe (Roof & Downspouts)
⊖	Drystack Retaining Wall
—w—	Water Line
---	Electrical Service
---	Gas Service
---	Water Service
---	Sewer Service



Irrigation Calculation	
(Lot = ±.23 Acres)	
Description	Square Footage
Natural Grass	4,900 sq.ft.
Planter Beds	100 sq.ft.
Total Irrigated Area	5,000 sq.ft.
	+/- .12 Acres

Snow Storage	
Driveway Area:	900 sq ft
Walkway Area:	150 sq ft
	x .30%
Required Area:	315 sq ft
Snow Storage Provided:	400 sq ft

- Per Development Agreement:**
- 1) Landscaping Shall Be Drought Tolerant
 - 2) Irrigation System Shall Be Equipped With Shut Off Valve Not Impacting Water Service To Residence
 - 3) Irrigation System Shall Be Water Efficient In Ground Components, Controller With Rain/Freeze Sensor.
 - 4) Isolate Zones Per Plant Type And Exposure.

- Landscape Notes:**
- 1) The Area 12" Horizontal From The Base Of A Wall Shall Be Finished In A Way To Prevent Any Vegetation Growing, And For Vegetative Debris To Be Easily Removed.
 - 2) Any Trees With Crowns Closer Than 30 Feet To Any Structure Shall Be Limbed Up A Minimum Of 6' From Ground Level.
 - 3) Any Tree Crowns Shall Be Pruned To Have A Minimum 10' Horizontal Clearance From Any Structure.

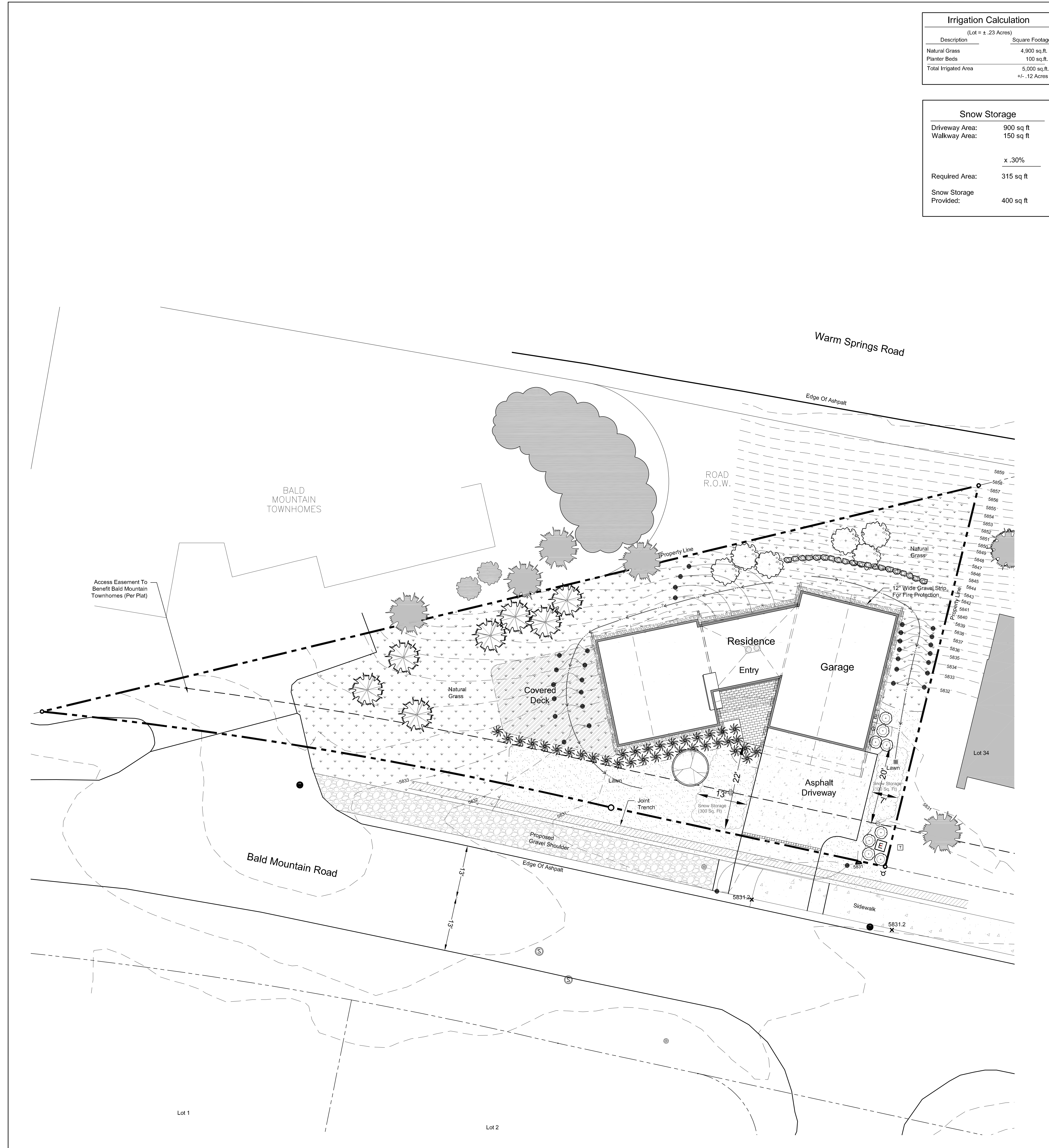
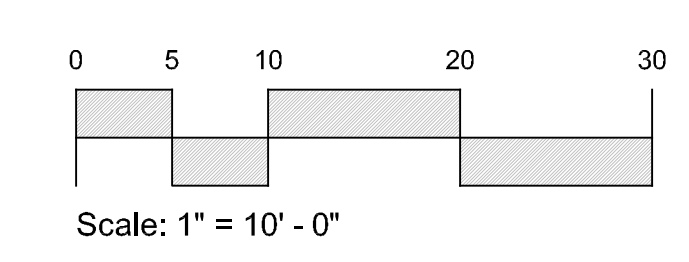
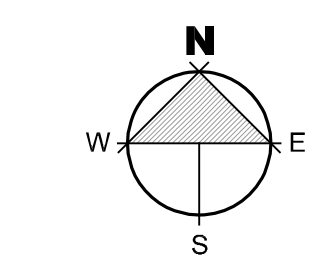
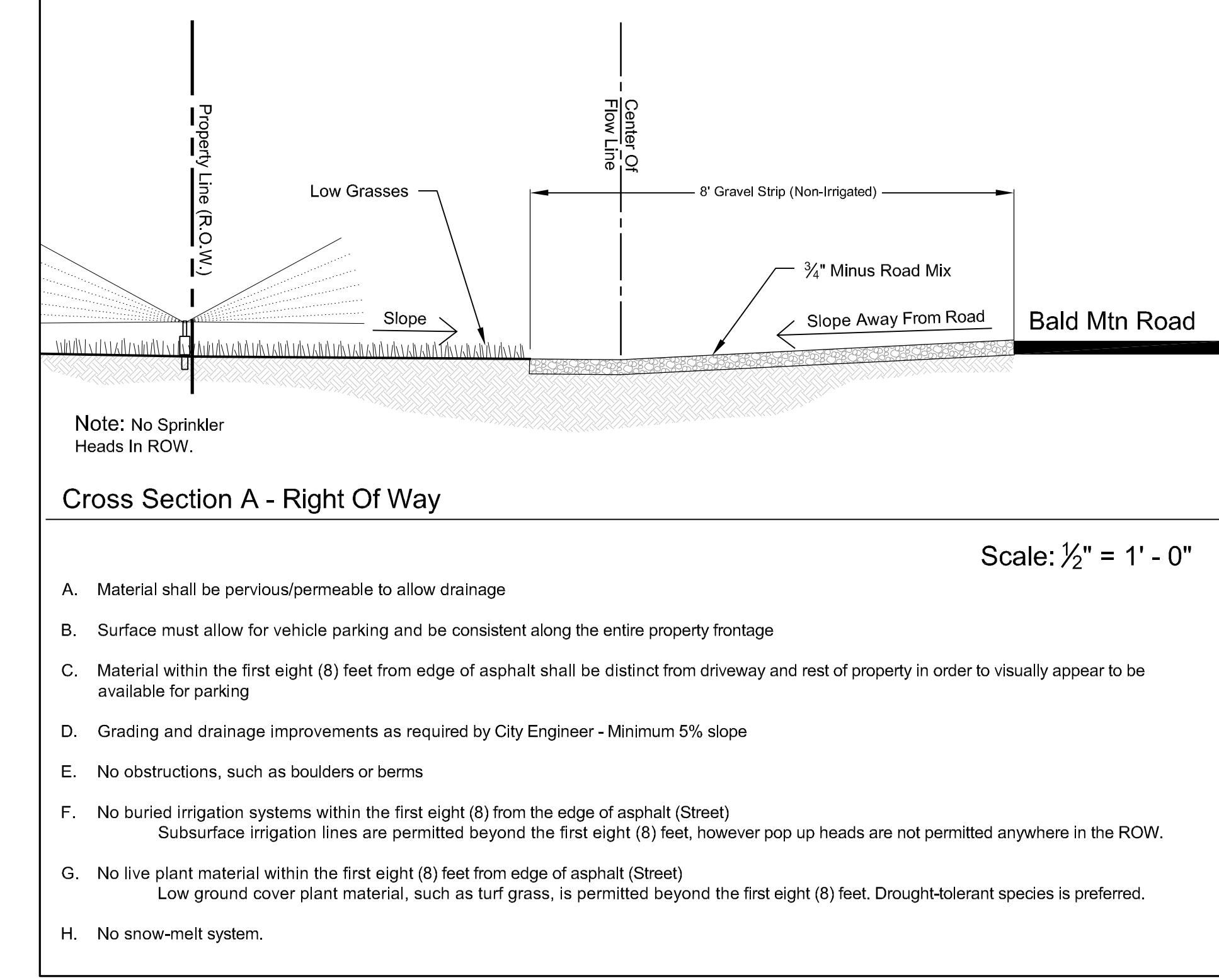
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 2. Landscape architect is not responsible for any deviation from these plans, unless such changes are authorized by the landscape architect in writing.
 3. All existing utilities are underground. All new utilities shall be underground.
 4. Site serviced by City of Ketchum.

Plan Legend

	Property Line
	PUE And Snow Storage Easement
	Proposed Contour
	Landscape Drywell
	Concrete Pavers
	Asphalt
	Gravel

Plant Legend

Qty.	Common Name	Botanical Name	Size
7	Conifer Trees		12' -16'
	Pine	<i>Pinus spp.</i>	
6	Deciduous Trees		12' -16'
	Aspen	<i>Populus tremuloides</i>	
1	Crabapple	<i>Malus spp.</i>	
8	Deciduous Shrubs		5 gal.
	Alpine Currant	<i>Ribes alpinum</i>	
	Burning Bush	<i>Euonymus alatus</i>	
	Cotoneaster	<i>Cotoneaster spp.</i>	
	Dogwood	<i>Cornus spp.</i>	
	Lilac	<i>Syringa spp.</i>	
	Maple	<i>Acer spp.</i>	
	Mockorange	<i>Philadelphus spp.</i>	
	Ninebark	<i>Physocarpus spp.</i>	
	Snowberry	<i>Symphoricarpos spp.</i>	
	Spiraea	<i>Spiraea spp.</i>	
35	Ornamental Grasses		Flats
	Blue Fescue	<i>Festuca ovina gluca</i>	
	Ribbon Grass	<i>Phalaris arundinacea 'Picata'</i>	
	Karl Foerster Feather Reed	<i>C. arundinacea 'Karl Foerster'</i>	
4,900 Sq.Ft.	Grasses & Wildflowers		Sod or Seed
	(20%) Hard Fescue	<i>Festuca trachyphylla</i>	
	(20%) Chewing Fescue	<i>Festuca rubra var. commutata</i>	
	(20%) Sheep Fescue	<i>Festuca ovina</i>	
	(20%) Creeping Red Fescue	<i>Festuca rubra</i>	
	(20%) Wildflowers	Various	
1,600 Sq.Ft.	Grasses - Lawn Mix		Sod or Seed
	(33%) Tall Fescue	<i>Festuca arundinacea</i>	
	(33%) Hard Fescue	<i>Festuca trachyphylla</i>	
	(33%) Chewing Fescue	<i>Festuca rubra var. commutata</i>	



WSR Residences Lot 35

EGGERS ASSOCIATES, P.A.
landscape architecture

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P.O. Box 973
Ketchum, ID 83340

Warm Springs Ranch Residences
Block 1, Lot 35
Ketchum, Idaho

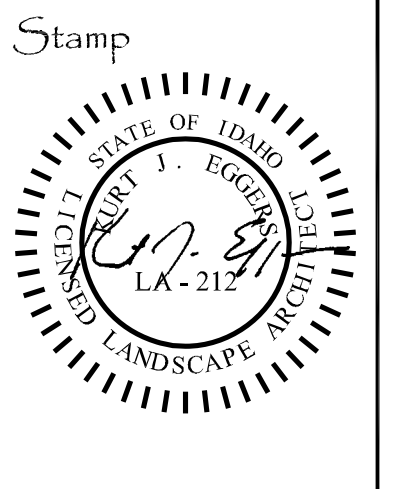
Job No: 22.28

Scale: 1" = 10'-0"

Issue/Revisions: Date:

Design Review 05/22/23

RVSD 05/08/23

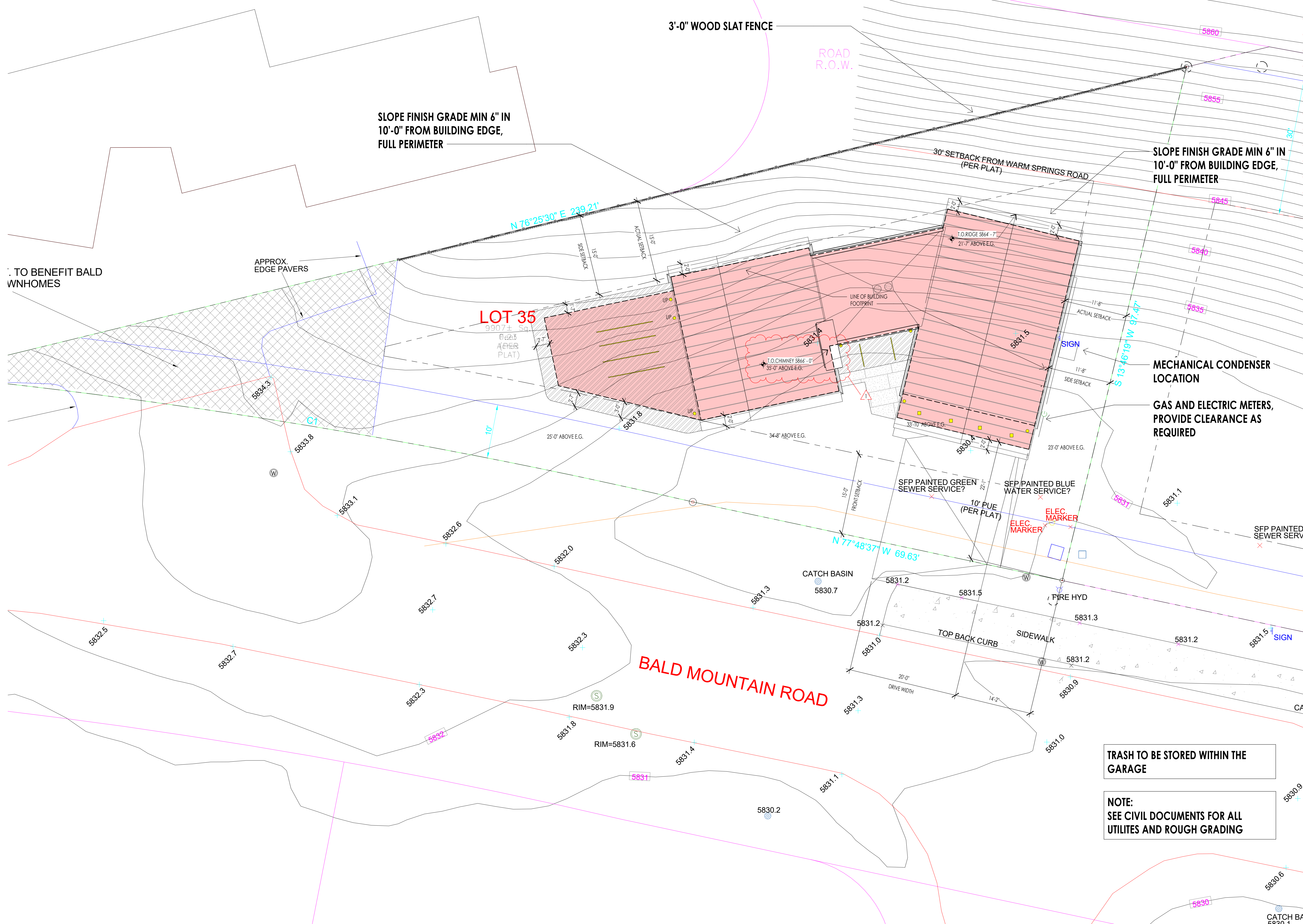
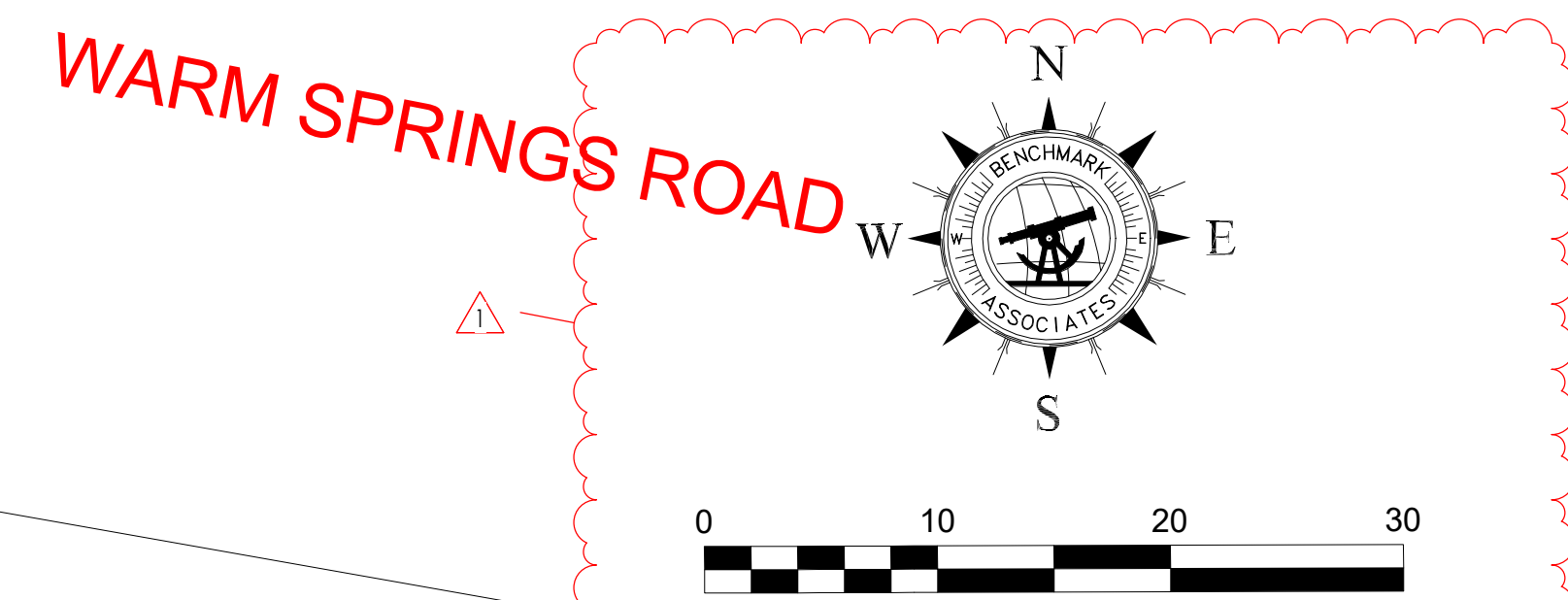


Sheet Title:
Landscape Plan

Sheet No:
L3



REAR FENCE EXAMPLE



SITE PLAN GENERAL NOTES

1. CONTRACTOR TO VERIFY ALL EXISTING UTILITIES AND LOCATIONS. PROTECT AS REQUIRED.
 2. CONTRACTOR TO PROTECT EXISTING TREES TO BE PRESERVED.
 3. UTILITY TRENCHING TO BE CONDUCTED IN A WAY TO NOT DISTURB EXISTING TREES.
 4. ANY TREES LOCATED WITHIN 10'-0" HORIZONTAL OF THE STRUCTURE MUST BE MIN. 10'-0" HIGHER THAN THE ADJACENT STRUCTURE OR WILL NEED TO BE REMOVED IN ACCORDANCE TO THE FIRE CODE AS DEFENSIBLE SPACE.
 5. ALL DEAD WOOD AND COMBUSTIBLE MATERIALS WITHIN 30'-0" OF THE STRUCTURE TO BE REMOVED BY CONTRACTOR AS PER THE FIRE CODE DEFENSIBLE SPACES.
 6. INSTALL EROSION CONTROL MAT ON ALL SLOPES 3:1 AND GREATER.
- DATUM:
LEVEL 1 100'-0" = 5,832.00 USGS
LEVEL 2 112'-0" = 5,844.00 USGS
- VERIFY FINISHED FLOOR HEIGHT WITH FINAL CIVIL ENGINEER'S DRAWINGS.

SITE PLAN KEY NOTES

- LOT SIZE:**
AREA = .23 ACRES OR 9,907 SQ. FT.
ZONE: GR-1
HEIGHT LIMIT: 35' MAX
- BUILDING PAD:**
AREA = 4,197 SQ. FT.
- PROPOSED BUILDING FOOTPRINT:**
AREA = 2,499 SQ. FT.
LOT COVERAGE ALLOWED: 35%, 3,467 SQ. FT.

- RECESSED LIGHT LOCATION
- WALL SCONCE LOCATION

LEGAL DESCRIPTION:
LOT 35 BALD MOUNTAIN ROAD

- SITE NOTES:**
1. ALL CONSTRUCTION TO BE DONE ACCORDING TO TOWN STANDARDS AND SPECIFICATIONS.
 2. CONTRACTOR TO VERIFY ALL EXISTING CONDITIONS AND LOCATION OF EXISTING UTILITIES PRIOR TO CONSTRUCTION.
 3. CONTRACTOR TO VERIFY PROPER DRAINAGE AWAY FROM HOUSE.
 4. PROVIDE DRAINAGE SWALE FROM HIGH TO LOW SIDE OF HOUSE.
 5. ZONING: GR-1
 6. CONTRACTOR TO VERIFY EXISTING GRADES/TOPOGRAPHY PRIOR TO EXCAVATION AND/OR CONSTRUCTION. VERIFY DRIVEWAY DOES NOT EXCEED 10% GRADE.
 7. EXISTING TOPOGRAPHY SHOWN IS BASED ON ROAD DESIGN GRADES & SITE OBSERVATIONS. CONTRACTORS/OWNER TO VERIFY.
 8. HEIGHT LIMIT OF 35'-0" OVER EXISTING GRADE. 5'-0" ALLOWANCE FOR CHIMNEYS AND MECHANICAL.

SHEET TITLE:
SITE PLAN

SHEET NUMBER:
A101

PROJECT NO. 22023
 DATE: 2023.06.30
 REVISIONS:
 1 04-27-2023 PER CITY COMMENTS

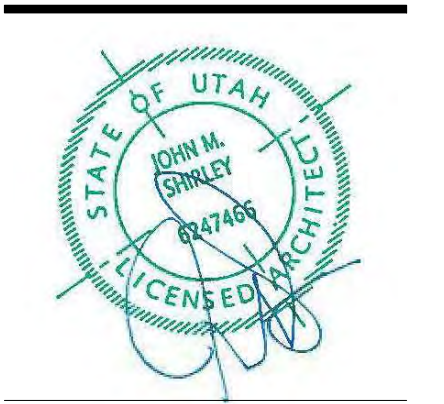


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 www.thinkaoc.com

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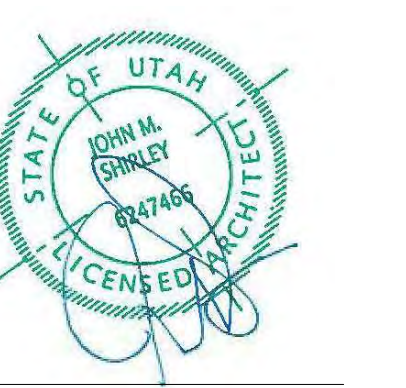
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WARM SPRINGS RESIDENCE #35
 190 BALD MOUNTAIN ROAD
 KETCHUM, IDAHO 83340

PROJECT NO. 22023
 DATE: 2023.06.30
 REVISIONS:
 1 04-27-2023 PER CITY COMMENTS

SHEET NUMBER:
A101



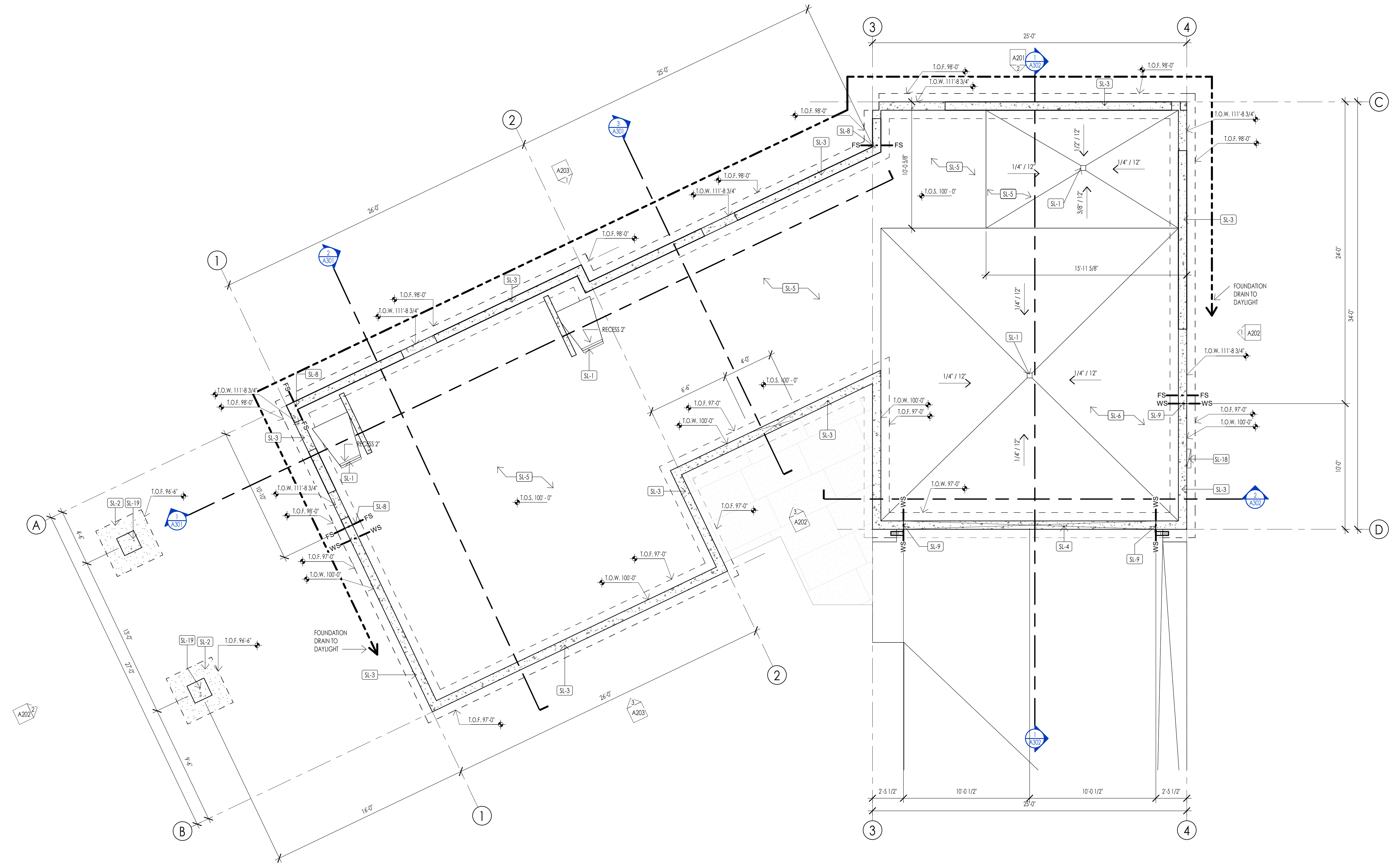
FOUNDATION PLAN LEGEND	
HATCH PATTERN	DESCRIPTION
	POURED IN PLACE CONCRETE
	2" RIGID FOAM INSULATION TO EXTEND FROM BOTTOM OF SLAB DOWN TO FOOTING AND HORIZONTALLY UNDER SLAB 4" MIN. AT PERIMETER OF FOUNDATION.

FOUNDATION PLAN SYMBOLS LEGEND	
SYMBOL	DESCRIPTION
FS - - - FS	FOOTING STEP
WS - - - WS	WALL STEP
T.O.F.	TOP OF FOOTING ELEVATION
T.O.W.	TOP OF WALL ELEVATION
T.O.S.	TOP OF SLAB ELEVATION
T.O. PIER	TOP OF PIER ELEVATION

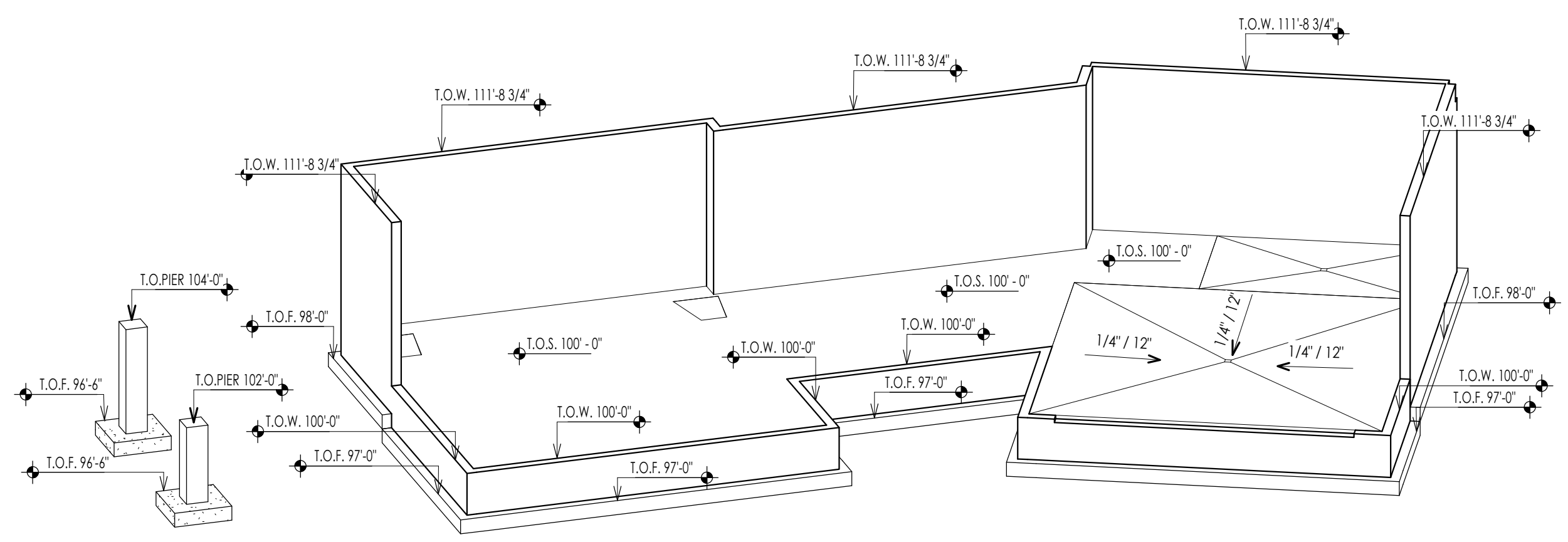
- FOUNDATION GENERAL NOTES**
- COORDINATE ARCHITECTURAL FOUNDATION PLAN WITH STRUCTURAL FOUNDATION PLAN. CONTRACTOR SHALL REPORT ANY DISCREPANCIES IN THE PLANS TO THE ARCHITECT PRIOR TO COMMENCING RELATED WORK.
 - COORDINATE MECHANICAL, ELECTRICAL, & PLUMBING PRIOR TO CONSTRUCTION OF FOOTINGS & FOUNDATION.
 - VERIFY ELEVATIONS OF FOUNDATION WALLS & FOOTINGS. COORDINATE WITH SITE PLAN & PROPOSED CONTOURS.
 - CONCRETE FLOOR SLABS, EXCEPT THOSE IN UNHEATED ACCESSORY STRUCTURES, SHALL HAVE A VAPOR RETARDER CONSISTING OF 6 MIL. POLYETHYLENE (OR APPROVED EQUAL) VAPOR RETARDER WITH JOINTS LAPPED NOT LESS THAN 6 INCHES PLACED BETWEEN THE CONCRETE FLOOR SLAB & THE BASE COURSE OF THE PREPARED SUB-GRADE WHERE NO BASE COURSE EXISTS.
 - FOUNDATION REBAR INSPECTIONS ARE REQUIRED FOR FOUNDATION WALLS OVER 8 FEET HIGH. FORMS ARE NOT TO BE INSTALLED ON ONE SIDE UNTIL AFTER THE REBAR HAS BEEN INSPECTED.

DATUM ELEVATIONS		
ARCHITECTURE	CIVIL	LEVEL
87'-6"	-	LEVEL 00 - TOP OF SLAB
88'-6"	-	LEVEL 0 - TOP OF SLAB
99'-0"	-	TOP OF SLAB AT FRONT OF GARAGE
100'-0"	-	LEVEL 1 - TOP OF PLYWOOD

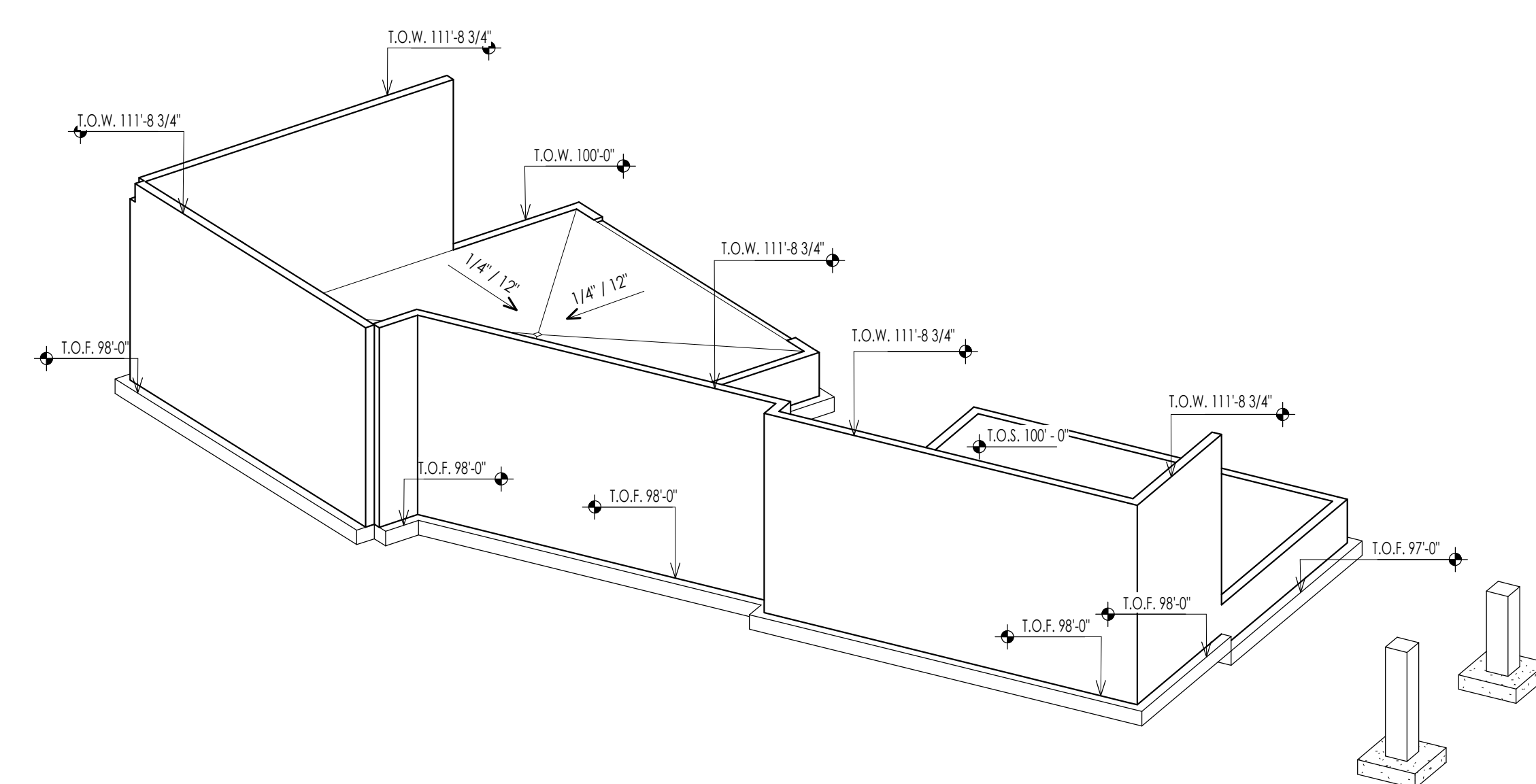
FOUNDATION PLAN KEYNOTES	
KEYNOTES	
SL-1	CONTRACTOR TO COORDINATE LOCATION OF FLOOR DRAIN - SLOPE SLAB TOWARDS DRAIN AS REQUIRED
SL-2	CAST IN PLACE FOOTINGS TO BEAR ON UNDISTURBED SOIL OR ENG COMPACTED FILL - SEE STRUCTURAL GENERAL NOTES & PROJECT MANUAL
SL-3	CAST IN PLACE FOUNDATION WALLS W/WATER PROOFING AS NOTED - SEE STRUCTURAL NOTES AND DETAILS
SL-4	PROVIDE BLOCKOUT AT FOUNDATION WALL AT DOOR OPENINGS AND POUR SLAB OVER TOP OF WALL - SEE DETAILS
SL-5	CAST IN PLACE INTERIOR CONCRETE SLABS TO BE 4" CONCRETE SLAB REINFORCED WITH FIBER MESH OVER 4" GRAVEL BASE - SEE STRUCTURAL NOTES
SL-6	CAST IN PLACE GARAGE CONCRETE SLABS TO BE 5" CONCRETE SLAB OVER 4" GRAVEL BASE AND FINISH AS NOTED - SEE STRUCTURAL NOTES
SL-8	CONTRACTOR TO COORDINATE FOOTING STEPS TO ASSURE REQUIRED FROST PROTECTION AT EACH FOOTING - NOTIFY ARCHITECT IF FOOTING ELEVATIONS NEED TO CHANGE
SL-9	CONTRACTOR TO COORDINATE FOUNDATION WALL STEPS WITH FINAL GRADING SPECIFIED AND NOTIFY ARCHITECT OF CHANGES PRIOR TO POURING CONCRETE FOUNDATION
SL-18	PROVIDE A U-FER GROUND. AN ELECTRODE ENCASED BY A LEAST 2" OF CONCRETE SHALL BE LOCATED NEAR THE BOTTOM OF THE CONCRETE FOUNDATION SYSTEM AND SHALL BE IN DIRECT CONTACT WITH THE EARTH, CONSISTING OF AT LEAST 20 FEET OF BARE ELECTRICALLY CONDUCTIVE #00-#1 EARTH 1/2 INCH IN DIAMETER OR BARE COPPER CONDUCTOR NOT SMALLER THAN 4 AWG. (I.R.C. E3308.1.2 AND N.E.C. 250.53)
SL-19	CONCRETE COLUMN PER STRUCTURAL



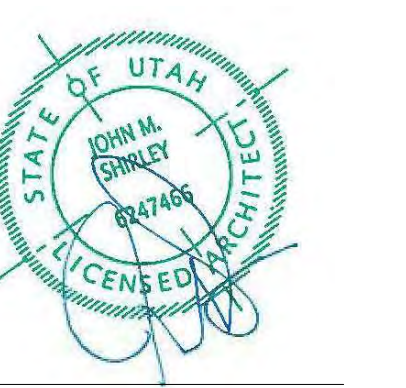
LEVEL 1 - SLAB PLAN
1/4" = 1'-0"



ISOMETRIC A
2
A103



ISOMETRIC B
3
A103

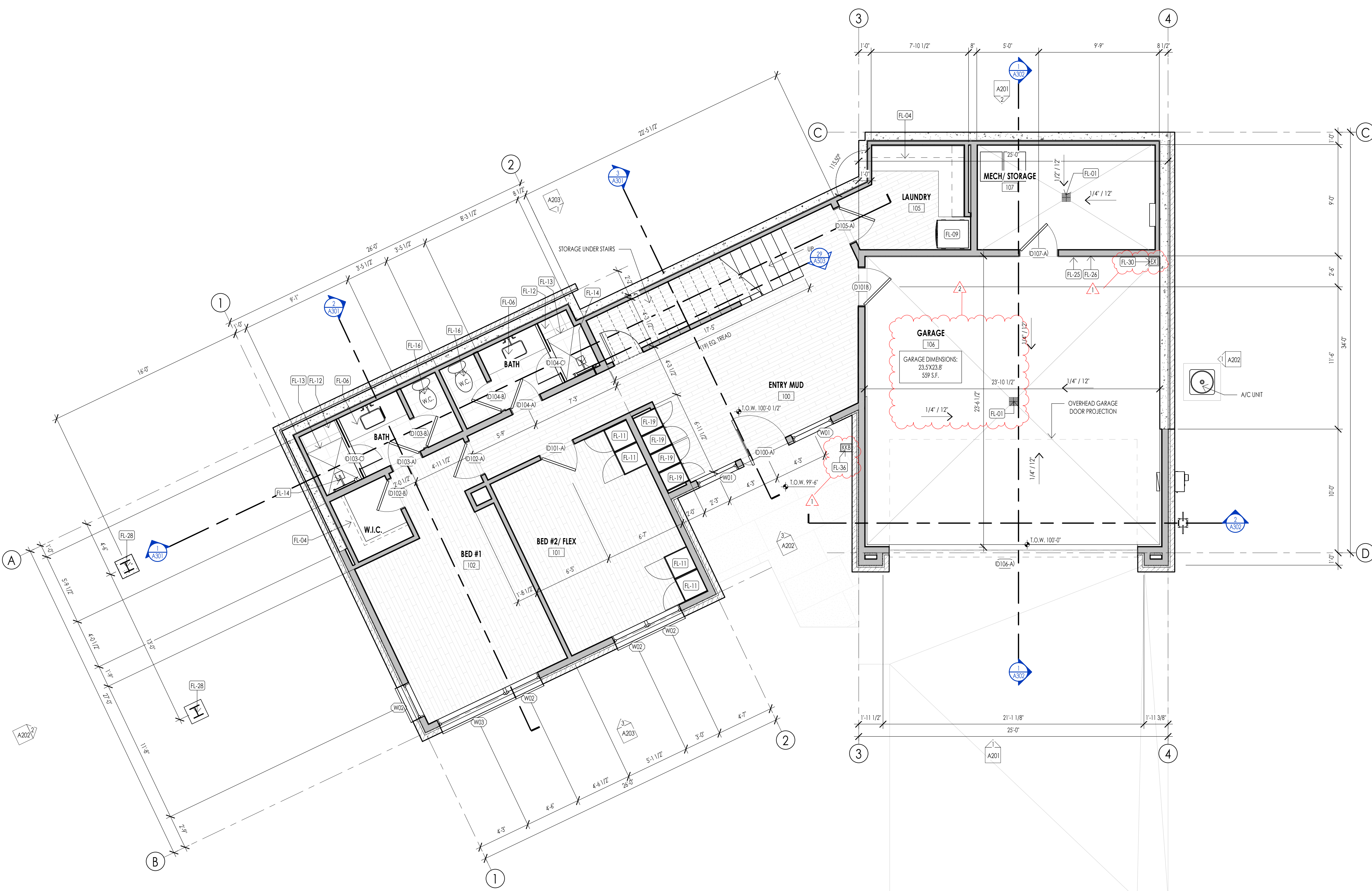


FLOOR PLAN LEGEND			
HATCH PATTERN	DESCRIPTION	HATCH PATTERN	DESCRIPTION
	POURED IN PLACE CONCRETE WALL		CARPET FINISH
	CMU BLOCK WALL		TILE FINISH
	STONE VENEER		EXTERIOR CONCRETE SLABS
	METAL STUD WALL		
	WOOD STUD WALL		

- ### FLOOR PLAN GENERAL NOTES
- ALL DIMENSIONS ARE TO INTERIOR FACE-OF-STUD (F.O.S.) UNLESS NOTED OTHERWISE.
 - CEILING HEIGHTS MEASURED FROM FLYWOOD OR CONCRETE - SEE SECTIONS
 - REFER TO ENLARGED PLANS FOR ALL UNIT DIMENSIONS, WINDOW TYPES, DOORS AND WALLS.
 - REFER TO ENLARGED PLANS FOR ALL DECK/PATIOS.
 - COORDINATE WITH ALL ENLARGED PLANS FOR ADDITIONAL INFORMATION AND DETAILS.
 - ALL TOPPING SLABS MUST BE POURED AFTER ROOF IS COMPLETE AND BUILDING IS DRIED IN.
 - SEE SHEET A202 FOR PROJECT GENERAL NOTES AND SHEET A203 FOR PROJECT KEYNOTES. REVIEW ALL NOTES PRIOR TO CONSTRUCTION.
 - COORDINATE WITH STRUCTURAL FRAMING PLANS AND SHEAR WALL PLANS FOR LOCATIONS OF COLUMNS, BEAMS, SHEAR WALLS, ETC.
 - COORDINATE WITH BUILDER/OWNER FOR ALL INTERIOR FINISHES
 - COORDINATE WITH ELECTRICAL DRAWINGS FOR ALL LIGHTING, POWER AND DATA REQUIREMENTS.
 - ALL EXTERIOR WALLS ARE ASSUMED TO BE 2X4 STUD WALLS UNLESS SHOWN/NOTED OTHERWISE.
 - ALL INTERIOR WALLS ARE ASSUMED TO BE 2X4 STUD WALLS UNLESS SHOWN/NOTED OTHERWISE.
 - ALL ROOF TRUSSES TO HAVE RAISED ENERGY HEEL CONSTRUCTION TO ALLOW FOR FULL DEPTH INSULATION OVER EXTERIOR WALLS (COORDINATE INSULATION REQUIREMENTS WITH RESCHECKS).

FLOOR PLAN KEYNOTES

KEYNOTES
FL-01 CONTRACTOR TO COORDINATE FLOOR PENETRATIONS WITH MECHANICAL, ELECTRICAL AND PLUMBING DRAWINGS.
FL-04 CLOSET SHELVING/ ROD - PER INTERIOR DESIGNER
FL-06 BATHROOM SINK - VANITY PER INTERIOR DESIGNER
FL-09 STACKED WASHER AND DRYER. CONTRACTOR TO COORDINATE WITH MECHANICAL AND ELECTRICAL
FL-11 CABINET SYSTEM SHELVING PER INTERIOR DESIGNER
FL-12 PROVIDE SHOWER BENCH AS PER OWNER INTERIOR DESIGN
FL-13 PROVIDE "SCHLITZ" EROSION LINEAR TRENCH DRAIN AGAINST BENCH. INTERIOR DESIGNER TO PROVIDE DRAIN COVER SPEC.
FL-14 SHOWER HEAD PER INTERIOR DESIGN
FL-16 W.C. TO BE "KOHLEP" PERIODE CURRY COMFORT
FL-19 BUILT IN INDUCTION CABINETS AS PER INTERIOR DESIGNER
FL-25 PROVIDE HOT/COLD HOOK UP
FL-26 PROVIDE 50 AMP EV CONNECTION POINT
FL-28 STRUCTURAL WIDE FLANGE COLUMN AS PER STRUCT.
FL-30 FIRE EXTINGUISHERS SHALL BE INSTALLED AND MAINTAINED PER 2018 IFC SECTION 906 BOTH DURING CONSTRUCTION AND UPON OCCUPANCY OF THE BUILDING. DURING CONSTRUCTION FIRE EXTINGUISHERS SHALL BE PLACED IN A CONSPICUOUS, EASY TO ACCESS, UNOBSTRUCTED LOCATION THAT IS LESS THAN 75' TRAVEL DISTANCE TO ANY COMBUSTIBLES ON SITE. 30" TO ANY HOT WORK. EXTINGUISHERS SHALL BE MOUNTED IN A CONSPICUOUS, EASY TO ACCESS, UNOBSTRUCTED LOCATION UPON COMPLETION OF PROJECT EVERY SINGLE-FAMILY RESIDENCE SHALL HAVE A MINIMUM OF ONE EXTINGUISHER PER GARAGE AND ONE EXTINGUISHER PER KITCHEN AREA.
FL-36 AN APPROVED KEY BOX SHALL BE INSTALLED, WITH THE APPROPRIATE KEYS FOR EMERGENCY FIRE DEPARTMENT ACCESS IN A LOCATION APPROVED BY THE FIRE DEPARTMENT. THE KEY BOX SHALL BE A KNIX BOX BRAND AND SIZED TO ACCOMMODATE KEYS TO EVERY DOOR OF THE PROJECT. LOCATION NO MORE THAN 3FT HOR. FROM MAIN DOOR AND NO MORE THAN 5.5FT VERTICAL - VERIFY LOCAL CODE.



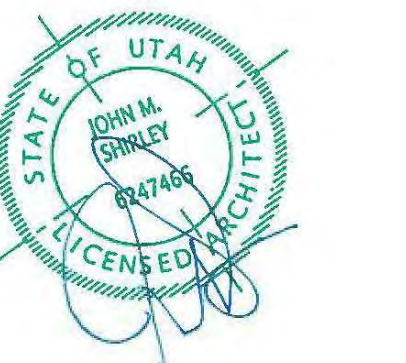
LEVEL 1 - FLOOR PLAN
1/4" = 1'-0"



Architecture
Interior Design
Landscape Architecture
Land Planning
Construction Management

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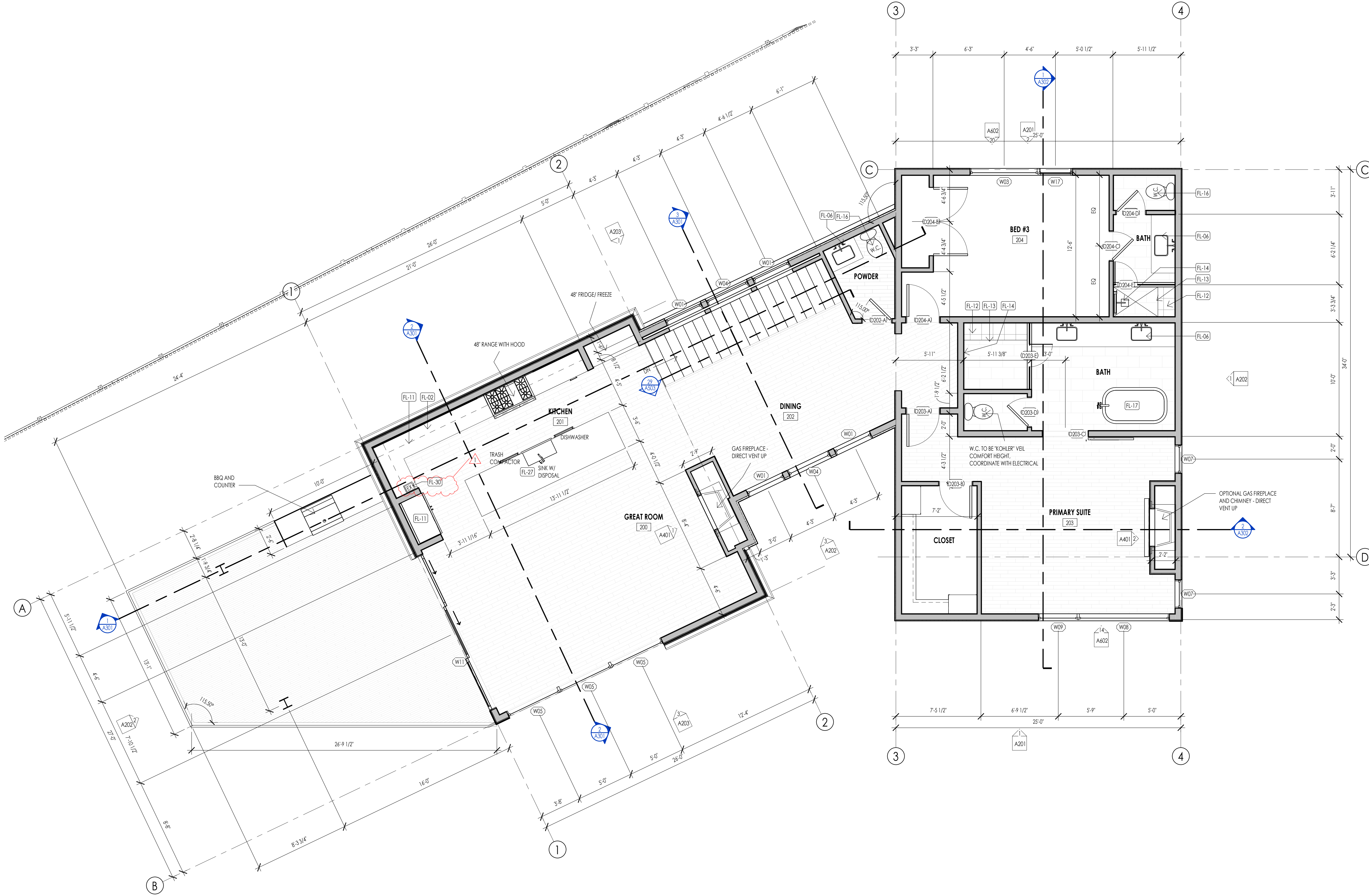


FLOOR PLAN LEGEND			
HATCH PATTERN	DESCRIPTION	HATCH PATTERN	DESCRIPTION
	POURED IN PLACE CONCRETE WALL		CARPET FINISH
	CMU BLOCK WALL		TILE FINISH
	STONE VENEER		EXTERIOR CONCRETE SLABS
	METAL STUD WALL		
	WOOD STUD WALL		

- FLOOR PLAN GENERAL NOTES**
- ALL DIMENSIONS ARE TO INTERIOR FACE-OF-STUD (F.O.S.) UNLESS NOTED OTHERWISE.
 - CEILING HEIGHTS MEASURED FROM FLYWOOD OR CONCRETE - SEE SECTIONS
 - REFER TO ENLARGED PLANS FOR ALL UNIT DIMENSIONS, WINDOW TYPES, DOORS AND WALLS.
 - REFER TO ENLARGED PLANS FOR ALL DECK/PATIOS.
 - COORDINATE WITH ALL ENLARGED PLANS FOR ADDITIONAL INFORMATION AND DETAILS.
 - ALL TOPPING SLABS MUST BE POURED AFTER ROOF IS COMPLETE AND BUILDING IS DRIED IN.
 - SEE SHEET A202 FOR PROJECT GENERAL NOTES AND SHEET A203 FOR PROJECT KEYNOTES. REVIEW ALL NOTES PRIOR TO CONSTRUCTION.
 - COORDINATE WITH STRUCTURAL FRAMING PLANS AND SHEAR WALL PLANS FOR LOCATIONS OF COLLUMNS, BEAMS, SHEAR WALLS, ETC.
 - COORDINATE WITH BUILDER/OWNER FOR ALL INTERIOR FINISHES
 - COORDINATE WITH ELECTRICAL DRAWINGS FOR ALL LIGHTING, POWER AND DATA REQUIREMENTS.
 - ALL EXTERIOR WALLS ARE ASSUMED TO BE 2X4 STUD WALLS UNLESS SHOWN/NOTED OTHERWISE.
 - ALL INTERIOR WALLS ARE ASSUMED TO BE 2X4 STUD WALLS UNLESS SHOWN/NOTED OTHERWISE.
 - ALL ROOF TRUSSES TO HAVE RAISED ENERGY HEEL CONSTRUCTION TO ALLOW FOR FULL DEPTH INSULATION OVER EXTERIOR WALLS (COORDINATE INSULATION REQUIREMENTS WITH RESCHECKS).

FLOOR PLAN KEYNOTES

KEYNOTES	
FL-02	OVERHEAD CABINETS PROJECTION
FL-06	BATHROOM SINK - VANITY PER INTERIOR DESIGNER
FL-11	CABINET SYSTEMS/SHELVING PER INTERIOR DESIGNER
FL-12	PROVIDE SHOWER BENCH AS PER OWNER/ INTERIOR DESIGNER
FL-13	PROVIDE "SCHLITZ" KEROLINE LINEAR TRENCH DRAIN AGAINST BENCH, INTERIOR DESIGNER TO PROVIDE DRAIN COVER SPEC.
FL-14	SHOWER HEAD PER INTERIOR DESIGNER
FL-16	W.C. TO BE "KOHLER" PERIASE CURBY COMFORT
FL-17	TUB/ SOAKER TUB AS PER INTERIOR DESIGNER
FL-27	PROVIDE COUPLER BUTTIGN SWITCH FOR DISPOSAL UNIT
FL-30	FIRE EXTINGUISHERS SHALL BE INSTALLED AND MAINTAINED PER 2018 IFC SECTION 906 BOTH DURING CONSTRUCTION AND UPON OCCUPANCY OF THE BUILDING. DURING CONSTRUCTION FIRE EXTINGUISHERS SHALL BE PLACED IN A CONSPICUOUS, EASY TO ACCESS, UNOBSTRUCTED LOCATION THAT IS LESS THAN 75' TRAVEL DISTANCE TO ANY COMBUSTIBLES ON SITE. 30' TO ANY HOT WORK. EXTINGUISHERS SHALL BE MOUNTED IN A CONSPICUOUS, EASY TO ACCESS, UNOBSTRUCTED LOCATION. UPON COMPLETION OF PROJECT EVERY SINGLE-FAMILY RESIDENCE SHALL HAVE A MINIMUM OF ONE EXTINGUISHER PER GARAGE AND ONE EXTINGUISHER PER KITCHEN AREA.



LEVEL 2 - FLOOR PLAN
1/4" = 1'-0"

WARM SPRINGS RESIDENCE #35

190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

PROJECT NO. 22023
DATE: 2023.06.30

REVISIONS:
1 04-27-2023 PER CITY COMMENTS

SHEET TITLE:
LEVEL 2 FLOOR PLAN

SHEET NUMBER:
A105

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PERMIT SET



ROOF PLAN LEGEND			
HATCH PATTERN	DESCRIPTION	HATCH PATTERN	DESCRIPTION
	SINGLE PLY ROOFING MEMBRANE		
	STANDING SEAM METAL ROOFING SYSTEM		
	LOCATION OF SNOW RETENTION SYSTEM - TO BE DESIGNED AND ENGINEERED BY OTHERS - SEE DETAILS BELOW		
	3/8" STEEL PLATE AWNING		
	RAIN GUTTER WITH DOWN SPOUT		

TRA SNOW & SUN
1887 South 980 East
American Fork, UT 84003
801-656-8981 | www.tra-snow.com

07.72.53

SNOW DIAMOND CLAMP-ON -1 RAIL

DESCRIPTION:

- Designed for standing seam metal roofs.
- Available for 16 types including Double Lock, Single Lock, Snap Lock, Skyline, etc.
- Designed for clamping square tubular snow clamp-on rails around seam areas.
- Allows for full insulation without penetration of the roof surface.
- Powder coated to match roofing material color.
- Overall dimensions: 2 1/2" x 5/8" x 1/2".
- Height (HT) of clamp varies based on its type and height.
- Carriage hole (CH) 5/8" Dia. x 1/4" x 1/2".

MATERIALS:

- Zinc Coated Steel: 316" (ASTM A653)
- Stainless Steel: 16G (ASTM A666-03)

COLORS/FINISHES:

- Color Powder Coat
- Hot Dipped Galvanized Steel
- Mill Finish Stainless Steel
- Powder coating available in many colors visit www.tra-snow.com/color-chart

NOTE: Due to specific job conditions, TRA Snow and Sun will only warranty a snow retention system that has been designed by TRA Snow and Sun.

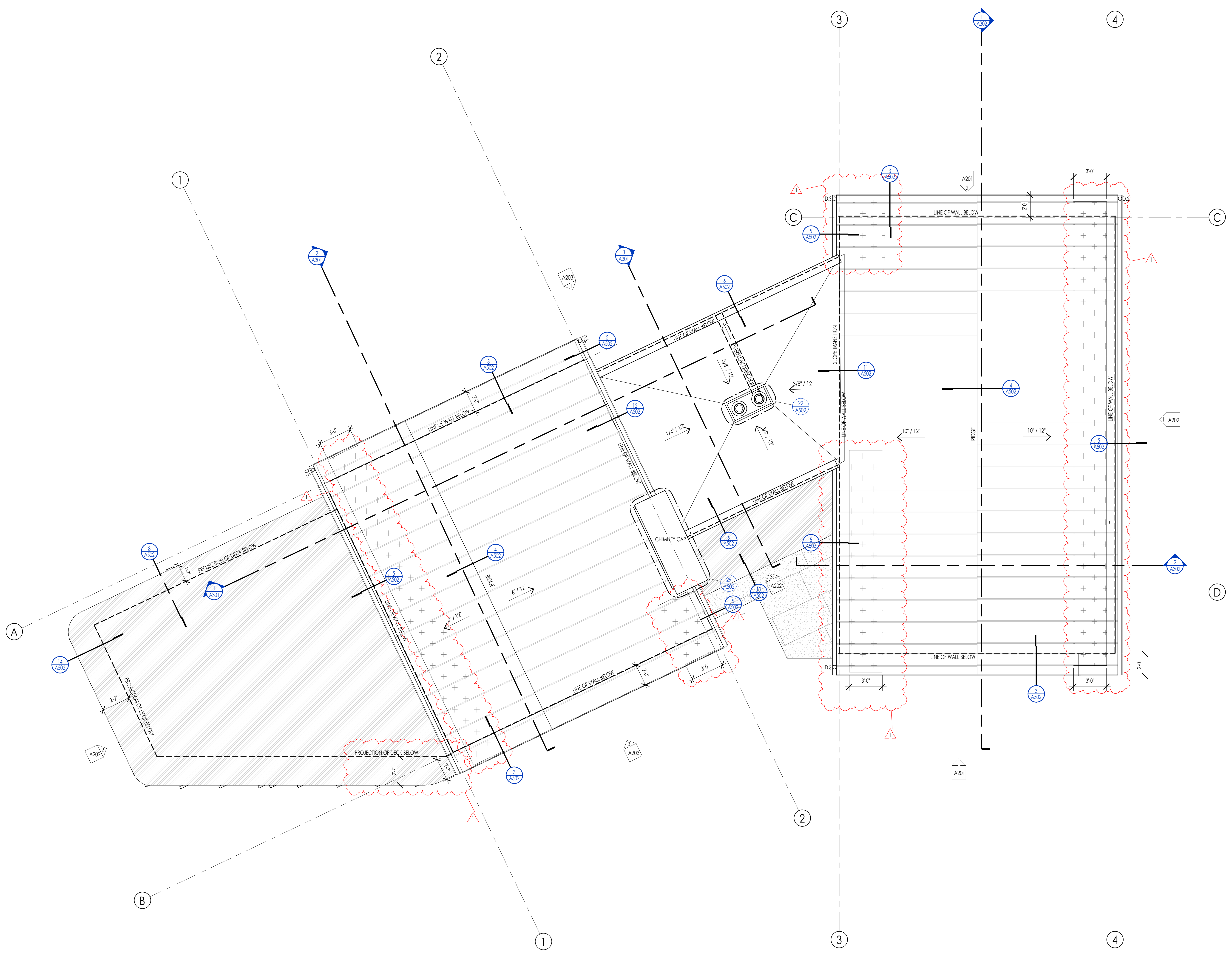
1 RAIL CLAMP-ON SNOW FENCE SYSTEM

USED ON: STANDING SEAM METAL

INSTALLATION:
CLAMP THE SNOW FENCE TO THE RIB BY USING A TORQUE WRENCH WITH 20 FT.-LBS. OF PRESSURE APPLIED TO EACH RAIL. FOR SPECIFIC ENGINEERING & PLACEMENT CALL TRA SNOW & SUN AT 801-656-8981.

DIMENSIONS

SNOW RETENTION SYSTEMS



- ROOF PLAN GENERAL NOTES**
- SEE SHEET 0002 FOR PROJECT GENERAL NOTES. REVIEW ALL NOTES PRIOR TO CONSTRUCTION.
 - FLASH ALL ROOF PENETRATIONS WHETHER SHOWN OR NOT.
 - COORDINATE WITH MECHANICAL, PLUMBING, AND ELECTRICAL DRAWINGS FOR ALL ROOF PENETRATIONS.
 - PROVIDE HEAT TRACE IN ALL RAIN GUTTERS, DOWN SPOUTS AND RAIN CHAINS.
 - ROOFING CONTRACTOR SHALL REVIEW ALL SUBSTRATES PRIOR TO BEGINNING WORK.
 - ALL ROOFING SHALL BE REVIEWED PRIOR TO INSTALLATION.
 - CONTRACTOR IS RESPONSIBLE TO ASSUME THAT NO ROOF SLOPES CREATE DEAD SPOTS OR LOW SPOTS THAT WILL PREVENT DRAINAGE.
 - ALL ROOF TRUSSES TO HAVE RAISED ENERGY HEE CONSTRUCTION TO ALLOW FOR FULL DEPTH INSULATION OVER EXTERIOR WALLS (COORDINATE INSULATION REQUIREMENTS WITH RESCHECKS).
 - DIMENSIONS SHOWN ON THE ROOF PLAN ARE FROM THE EXTERIOR SIDE OF THE STUD FRAMING BELOW.

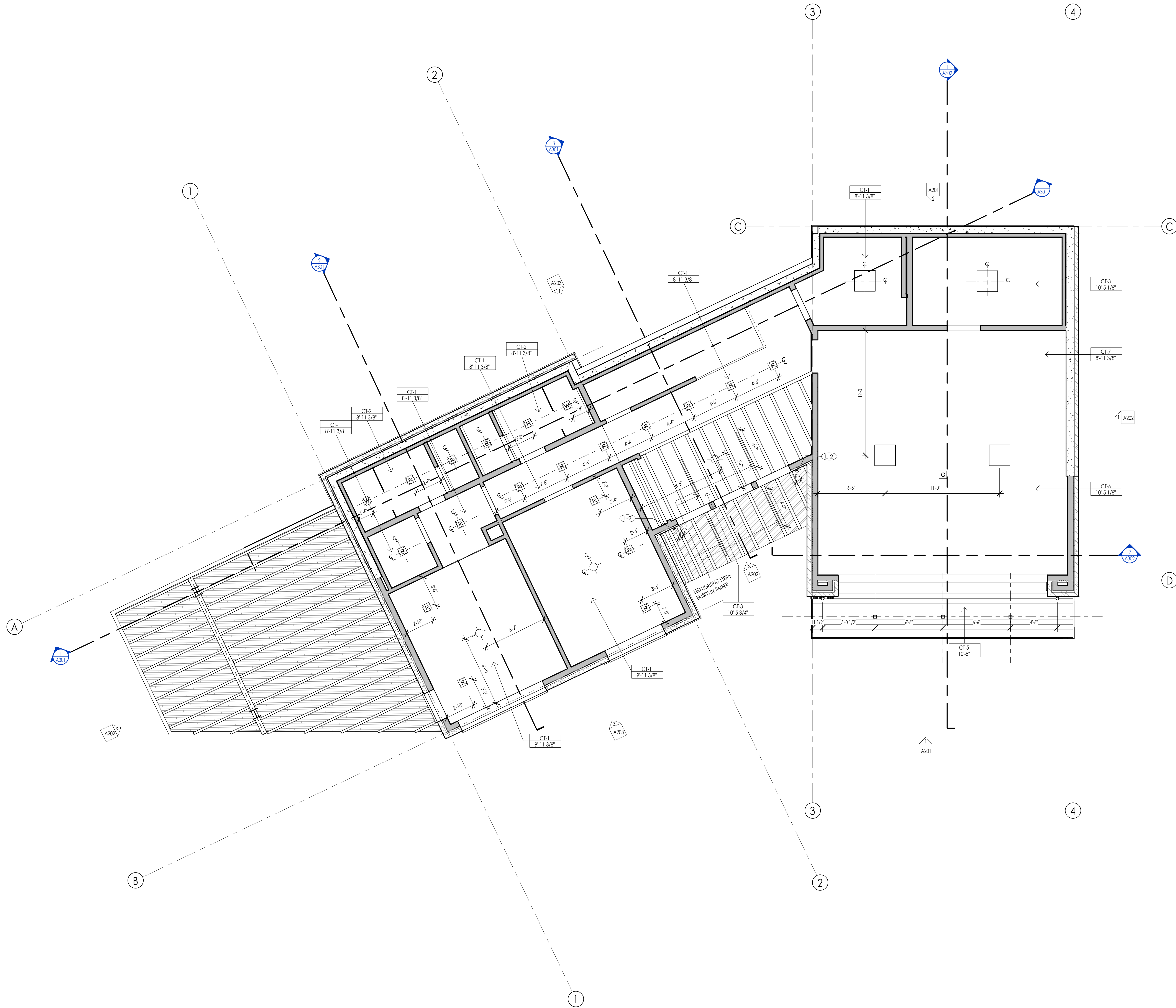
ROOF PLAN KEYNOTES	
KEYNOTES	

WARM SPRINGS RESIDENCE #35
 190 BALD MOUNTAIN ROAD
 KETCHUM, IDAHO 83340

PROJECT NO. 22023
DATE: 2023.06.30
REVISIONS:
1 04-27-2023 PER CITY COMMENTS

SHEET TITLE:
ROOF PLAN

SHEET NUMBER:
A107



REFLECTED CEILING PLAN MATERIAL LEGEND					
HATCH PATTERN	TYPE	DESCRIPTION	HATCH PATTERN	TYPE	DESCRIPTION
[Hatch Pattern]	CT-1	5/8" GYPSUM BOARD 2x4	[Hatch Pattern]	CT-7	5/8" GYPSUM BOARD 2x4 TYPE X FIRE RESISTANT
[Hatch Pattern]	CT-2	5/8" WATER RESISTANT GYPSUM BOARD 2x4	[Hatch Pattern]	CT-8	EXPOSED STRUCTURE
[Hatch Pattern]	CT-3	5/8" GYPSUM BOARD @ FLOOR OR ROOF			
[Hatch Pattern]	CT-4	3/4" 1-G INTERIOR SOFFIT @ ROOF			
[Hatch Pattern]	CT-5	3/4" 1-G EXTERIOR SOFFIT 2x4			
[Hatch Pattern]	CT-6	5/8" GYPSUM BOARD TYPE X FIRE RESISTANT			

- REFLECTED CEILING PLAN GENERAL NOTES**
- ALL DIMENSIONS ARE TO INTERIOR FACE-OF-STUD (F.O.S.) UNLESS NOTED OTHERWISE.
 - ALL CEILING HEIGHTS MEASURED FROM TOP OF PLYWOOD OR CONCRETE SLAB TO BOTTOM OF CEILING FRAMING, U.N.O. - SEE SECTIONS.
 - REFER TO ENLARGED PLANS FOR ALL UNIT DIMENSIONS, WINDOW TYPES, DOORS AND WALLS.
 - REFER TO ENLARGED PLANS FOR ALL DECKS.
 - COORDINATE WITH ALL ENLARGED PLANS FOR ADDITIONAL INFORMATION AND DETAILS.
 - SEE SHEET G002 FOR PROJECT SPECIFICATION LIST. REVIEW ALL NOTES PRIOR TO CONSTRUCTION.
 - COORDINATE WITH ELECTRICAL DRAWINGS FOR ALL LIGHTING, POWER AND DATA REQUIREMENTS.
 - ALL INTERIOR FINISHES ARE NOTED FOR CONCEPT ONLY. SEE INTERIOR DRAWINGS FOR MATERIAL SPECIFICATIONS, COLORS, PATTERNS, AND OTHER REQUIREMENTS PRIOR TO INSTALLATION.

CEILING TAG SYMBOL	DESCRIPTION
[Symbol]	CEILING TYPE
[Symbol]	HEIGHT

REFLECTED CEILING PLAN KEYNOTES

KEYNOTES

PROJECT NO. 22023
 DATE: 2023.06.30
 REVISIONS:

SHEET TITLE:
 LEVEL 1 CEILING PLAN

SHEET NUMBER:
 A109

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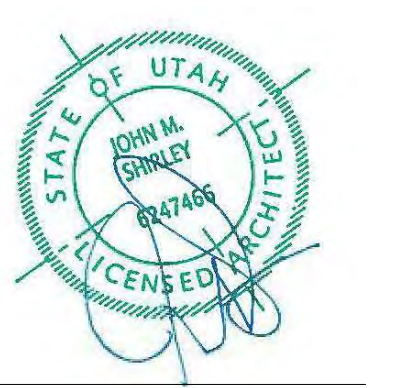


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7927 So. Highpoint Parkway, Suite 300
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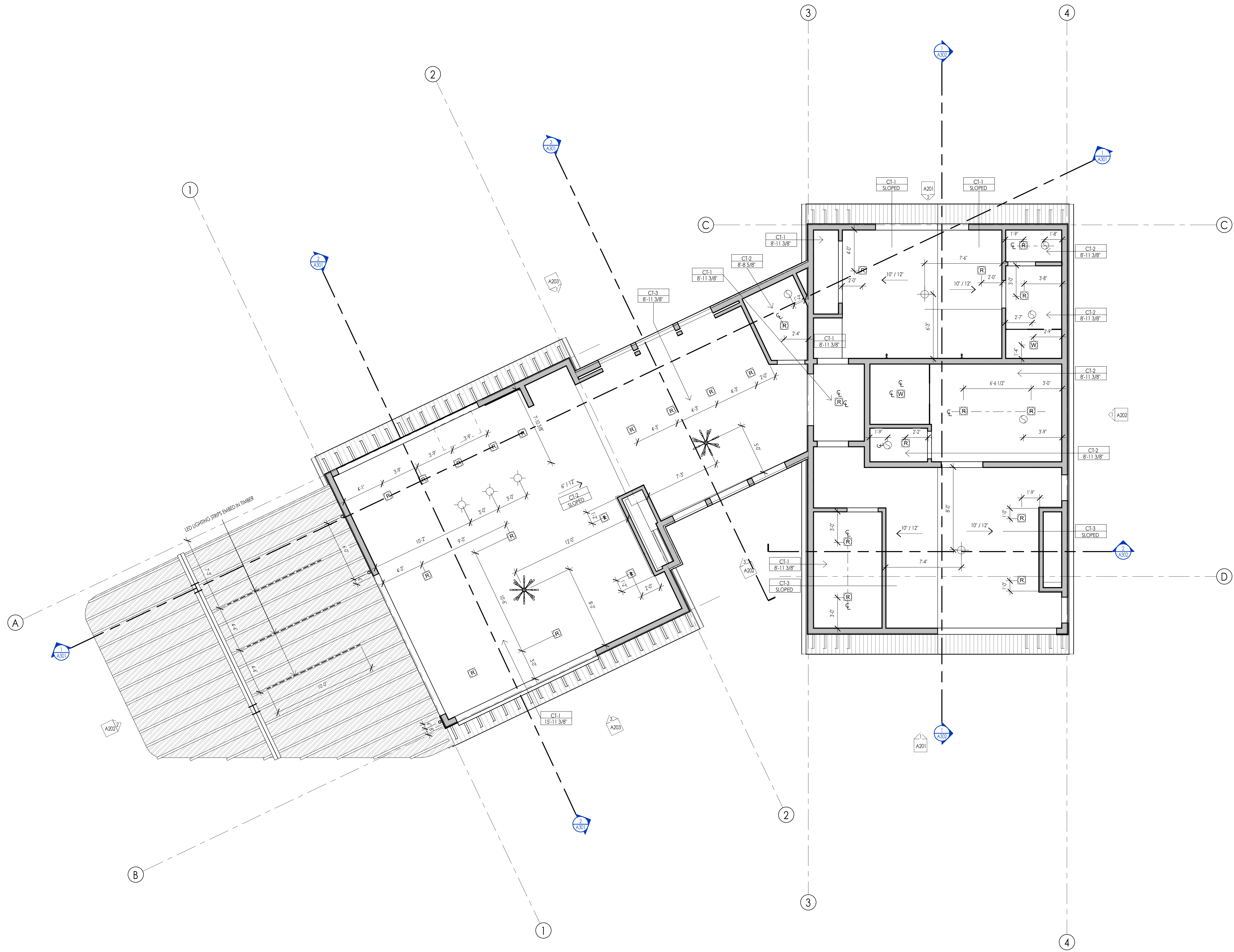
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WARM SPRINGS RESIDENCE #35

190 BALD MOUNTAIN ROAD
 KETCHUM, IDAHO 83340



REFLECTED CEILING PLAN MATERIAL LEGEND					
HATCH PATTERN	TYPE	DESCRIPTION	HATCH PATTERN	TYPE	DESCRIPTION
[Hatch Pattern]	CT-1	5/8" GYPSUM BOARD 24	[Hatch Pattern]	CT-7	5/8" GYPSUM BOARD 24 TYPE X FIRE RESISTANT
[Hatch Pattern]	CT-2	5/8" WATER RESISTANT GYPSUM BOARD 24	[Hatch Pattern]	CT-8	EXPOSED STRUCTURE
[Hatch Pattern]	CT-3	5/8" GYPSUM BOARD @ FLOOR OR ROOF			
[Hatch Pattern]	CT-4	3/4" 1-G INTERIOR SOFFIT @ ROOF			
[Hatch Pattern]	CT-5	3/4" 1-G EXTERIOR SOFFIT 24			
[Hatch Pattern]	CT-6	5/8" GYPSUM BOARD TYPE X FIRE RESISTANT			

- REFLECTED CEILING PLAN GENERAL NOTES**
1. ALL DIMENSIONS ARE TO INTERIOR FACE-OF-STUD (F.O.S.) UNLESS NOTED OTHERWISE.
 2. ALL CEILING HEIGHTS MEASURED FROM TOP OF PLYWOOD OR CONCRETE SLAB TO BOTTOM OF CEILING FRAMING, U.N.O. - SEE SECTIONS.
 3. REFER TO ENLARGED PLANS FOR ALL UNIT DIMENSIONS, WINDOW TYPES, DOORS AND WALLS.
 4. REFER TO ENLARGED PLANS FOR ALL DECKS.
 5. COORDINATE WITH ALL ENLARGED PLANS FOR ADDITIONAL INFORMATION AND DETAILS.
 6. SEE SHEET G002 FOR PROJECT SPECIFICATION LIST. REVIEW ALL NOTES PRIOR TO CONSTRUCTION.
 7. COORDINATE WITH ELECTRICAL DRAWINGS FOR ALL LIGHTING, POWER AND DATA REQUIREMENTS.
 8. ALL INTERIOR FINISHES ARE NOTED FOR CONCEPT ONLY. SEE INTERIOR DRAWINGS FOR MATERIAL SPECIFICATIONS, COLORS, PATTERNS, AND OTHER REQUIREMENTS PRIOR TO INSTALLATION.

CEILING TAG SYMBOL	DESCRIPTION
CT-1	CEILING TYPE
1'-0"	HEIGHT

REFLECTED CEILING PLAN KEYNOTES

PERMIT SET

PROJECT NO. 22023
DATE: 2023.06.30
REVISIONS:

SHEET TITLE:
LEVEL 2 CEILING PLAN

SHEET NUMBER:
A110

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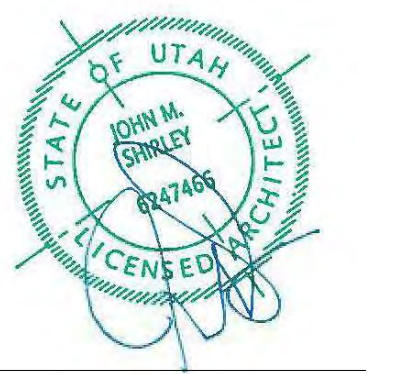


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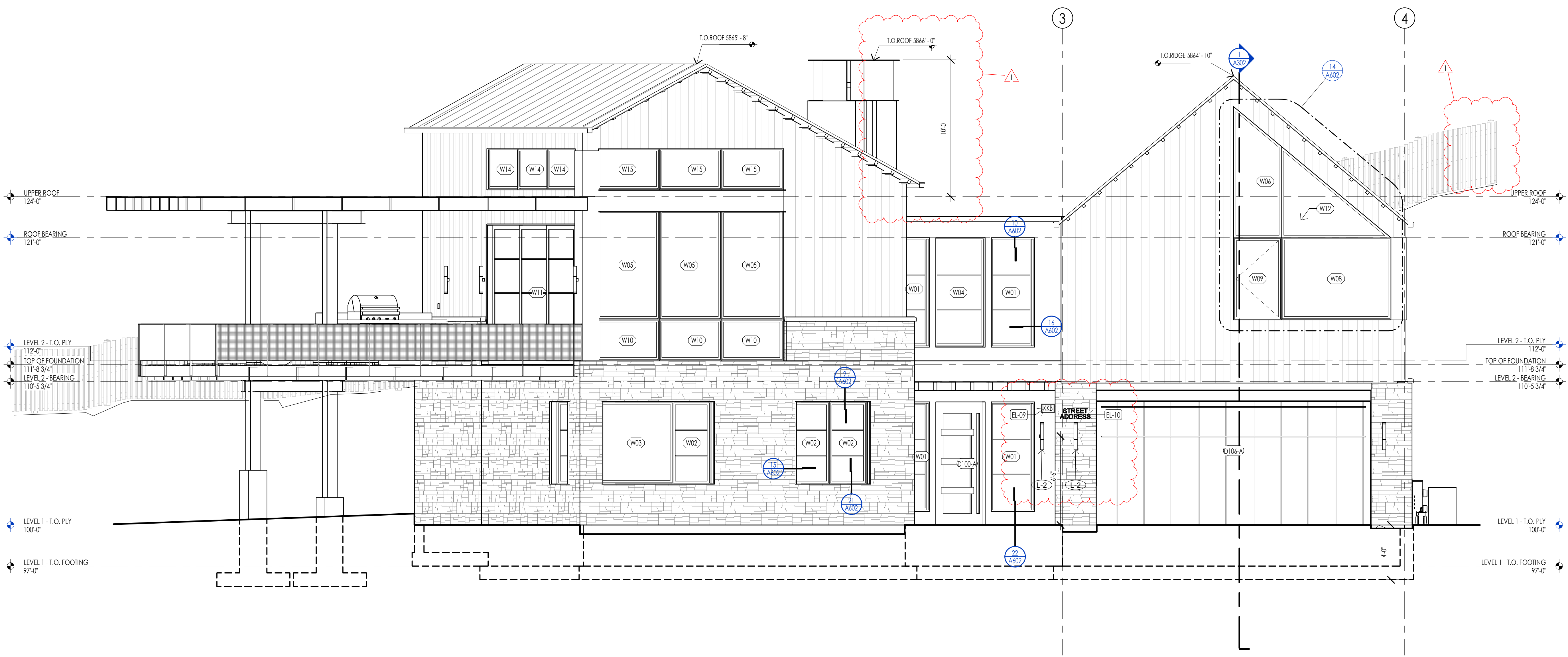
7927 So. Highpoint Parkway, Suite 300
Sandwich, Utah 84094
ph. 801.269.0555
fax. 801.269.1425
www.thinkaec.com

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WARM SPRINGS RESIDENCE #35
190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340



SOUTH ELEVATION
1/4" = 1'-0"

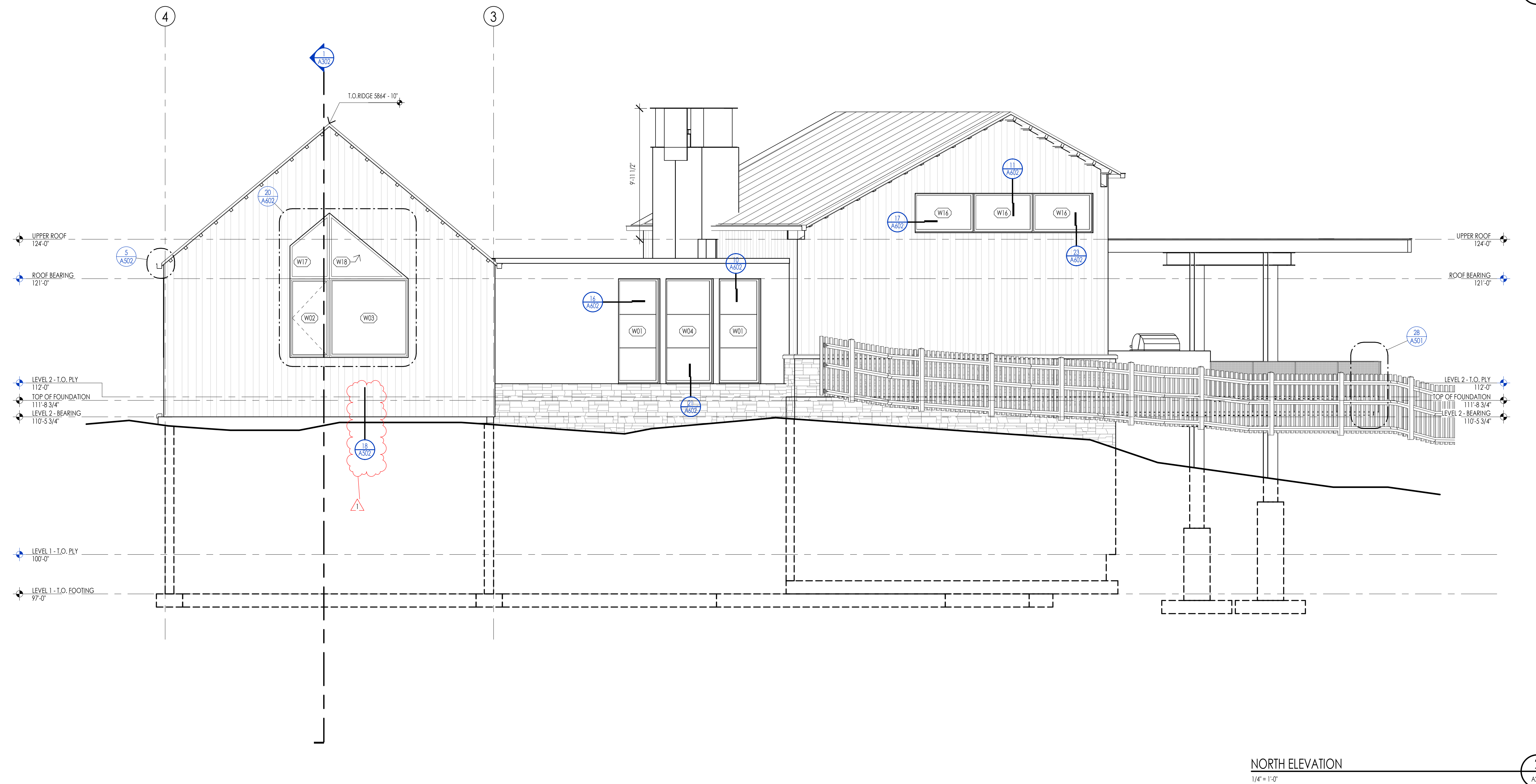
HATCH PATTERN	DESCRIPTION
	STANDING SEAM ROOFING SYSTEM
	SINGLE PLY ROOFING MEMBRANE SYSTEM
	2X10 CEDAR CHANNEL LAP VERTICAL SIDING SYSTEM WITH SQUARE EDGES
	EXTERIOR STONE VENEER
	EXTERIOR METAL PANEL WALL SYSTEM

NOTE: REFER TO MATERIAL SPECIFICATIONS DOCUMENT FOR DETAILED INFORMATION REGARDING EACH FINISH MATERIAL.

ELEVATION/ SECTION KEYNOTES

KEYNOTES	
EL-09	AN APPROVED KEY BOX SHALL BE INSTALLED, WITH THE APPROPRIATE KEYS FOR EMERGENCY FIRE DEPARTMENT ACCESS IN A LOCATION APPROVED BY THE FIRE DEPARTMENT. THE KEY BOX SHALL BE A KNIX BOX BRAND AND SIZED TO ACCOMMODATE KEYS TO EVERY DOOR OF THE PROJECT. LOCATION NO MORE THAN 3FT HOR. FROM MAIN DOOR AND NO MORE THAN 5.3FT VERTICAL. VERIFY LOCAL CODE.
EL-10	APPROVED ADDRESS NUMBERS SHALL BE PLACED IN SUCH A POSITION TO BE PLAINLY VISIBLE AND LEGIBLE FROM THE ROAD FRONTING THE PROPERTY. NUMBERS AND LETTERS SHALL BE A MINIMUM OF FOUR (4) INCHES TALL, HAVE A MINIMUM 1/8" BRUSH STROKE, CONTRAST WITH THEIR BACKGROUND, AND BE POSITIONED A MINIMUM OF FORTY-EIGHT (48) INCHES ABOVE FINIAL GRADE.

ELEVATION/ SECTION KEYNOTES	
KEYNOTES	
EL-09	AN APPROVED KEY BOX SHALL BE INSTALLED, WITH THE APPROPRIATE KEYS FOR EMERGENCY FIRE DEPARTMENT ACCESS IN A LOCATION APPROVED BY THE FIRE DEPARTMENT. THE KEY BOX SHALL BE A KNIX BOX BRAND AND SIZED TO ACCOMMODATE KEYS TO EVERY DOOR OF THE PROJECT. LOCATION NO MORE THAN 3FT HOR. FROM MAIN DOOR AND NO MORE THAN 5.3FT VERTICAL. VERIFY LOCAL CODE.
EL-10	APPROVED ADDRESS NUMBERS SHALL BE PLACED IN SUCH A POSITION TO BE PLAINLY VISIBLE AND LEGIBLE FROM THE ROAD FRONTING THE PROPERTY. NUMBERS AND LETTERS SHALL BE A MINIMUM OF FOUR (4) INCHES TALL, HAVE A MINIMUM 1/8" BRUSH STROKE, CONTRAST WITH THEIR BACKGROUND, AND BE POSITIONED A MINIMUM OF FORTY-EIGHT (48) INCHES ABOVE FINIAL GRADE.



NORTH ELEVATION
1/4" = 1'-0"

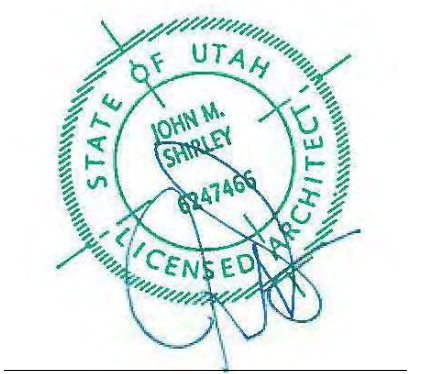


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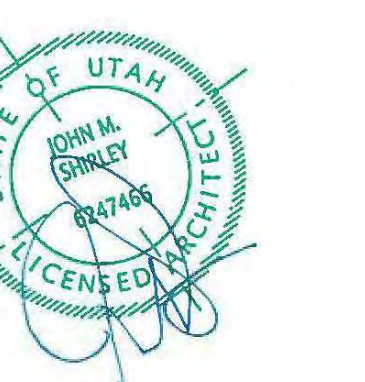
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WARM SPRINGS RESIDENCE #35
190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

PROJECT NO. 22023
DATE: 2023.06.30
REVISIONS:
1 04-27-2023 PER CITY COMMENTS

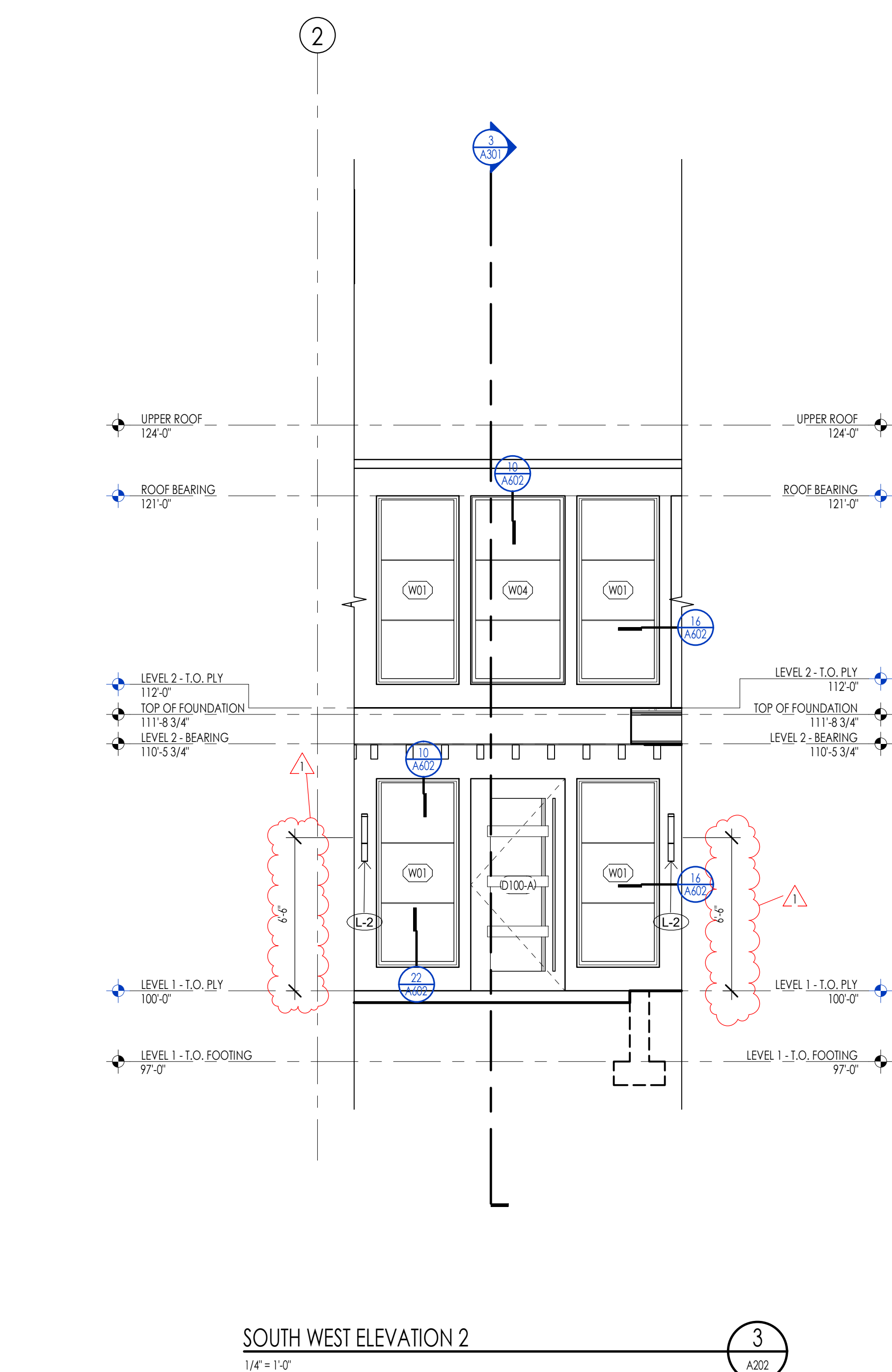
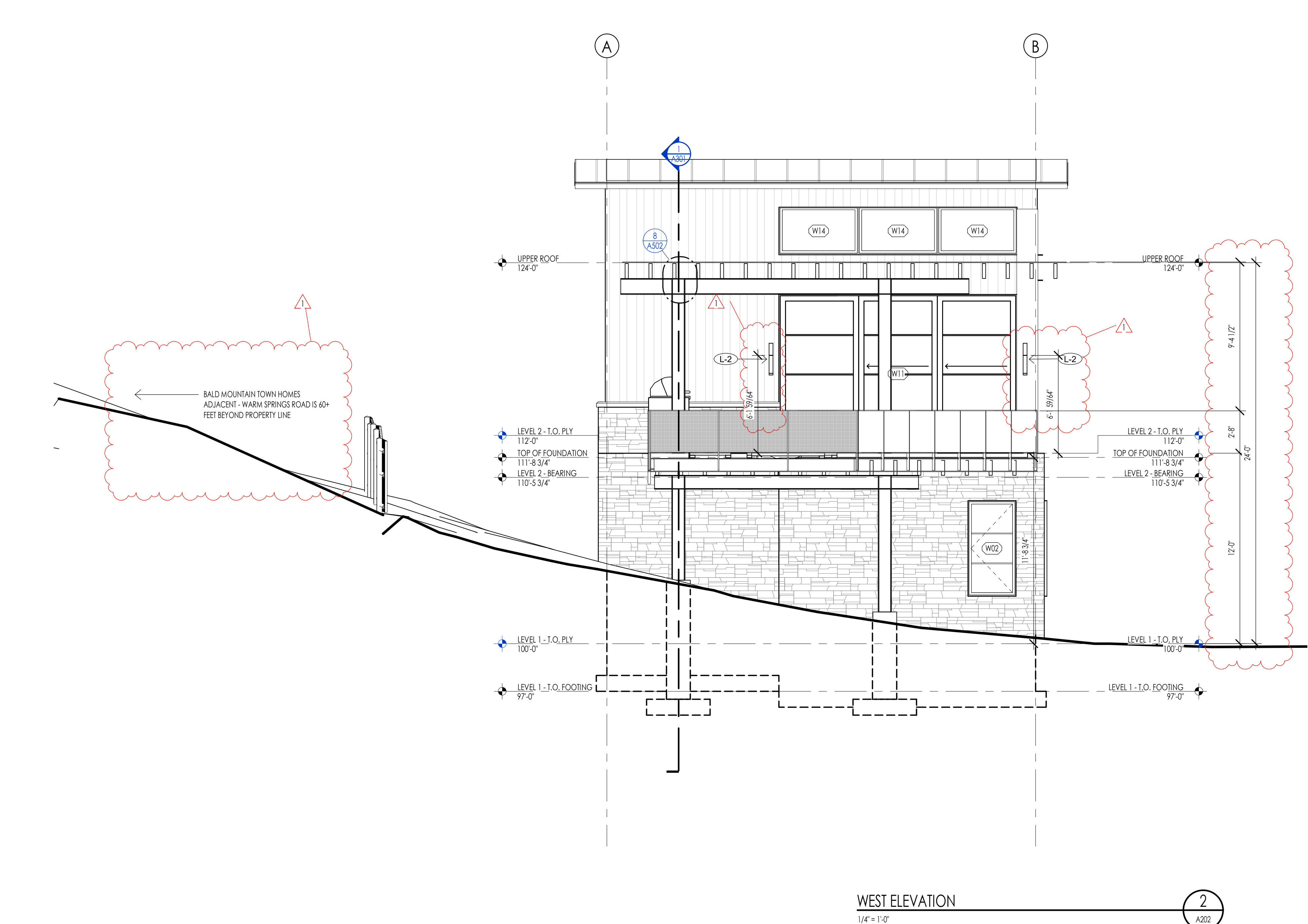
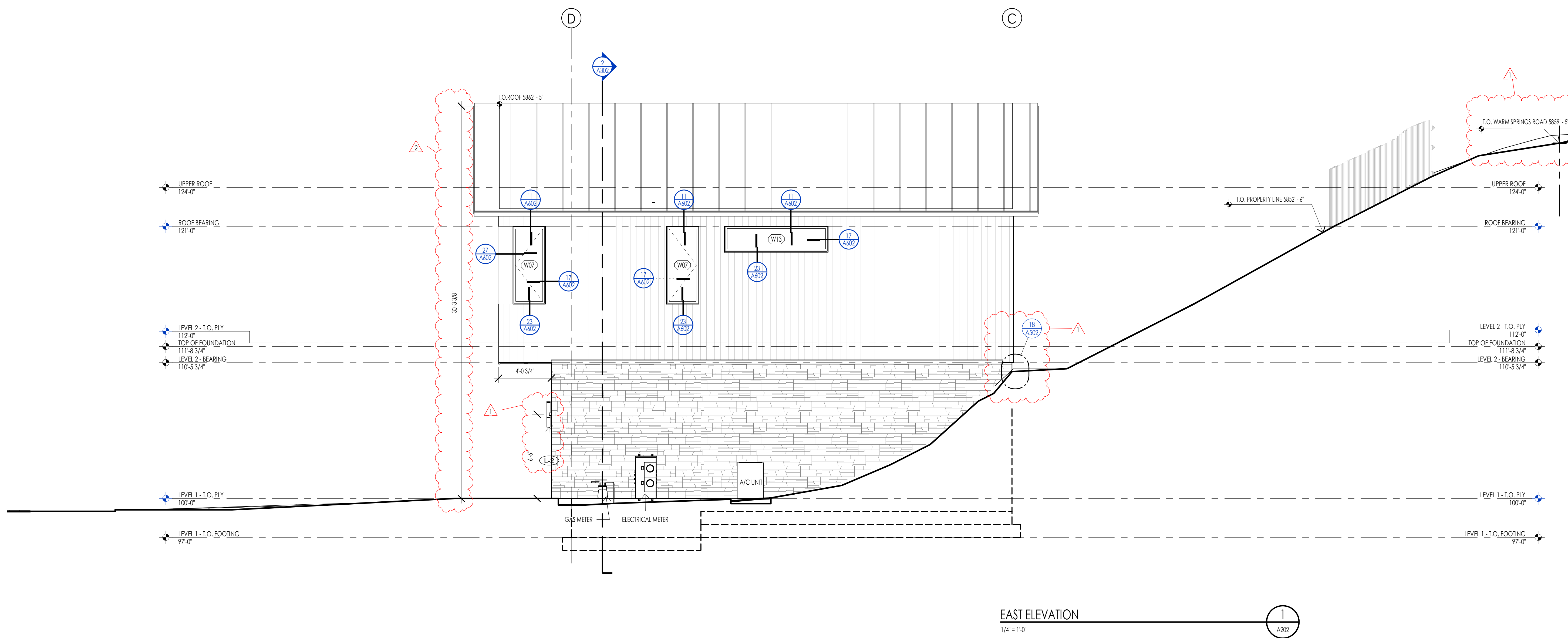
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EXTERIOR ELEVATIONS
SHEET NUMBER:
A201
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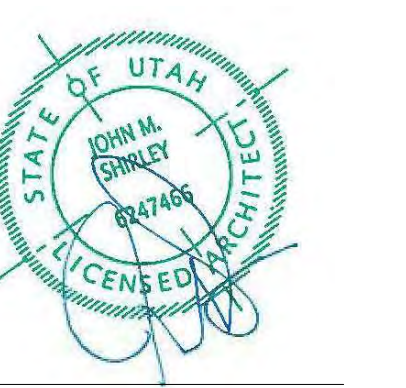


ELEVATION/ SECTION MATERIAL LEGEND	
HATCH PATTERN	DESCRIPTION
	STANDING SEAM ROOFING SYSTEM
	SINGLE PLY ROOFING MEMBRANE SYSTEM
	2X10 CEDAR CHANNEL LAP VERTICAL SIDING SYSTEM WITH SQUARE EDGES
	EXTERIOR STONE VENEER
	EXTERIOR METAL PANEL WALL SYSTEM

NOTE: REFER TO MATERIAL SPECIFICATIONS DOCUMENT FOR DETAILED INFORMATION REGARDING EACH FINISH MATERIAL.

ELEVATION/ SECTION KEYNOTES

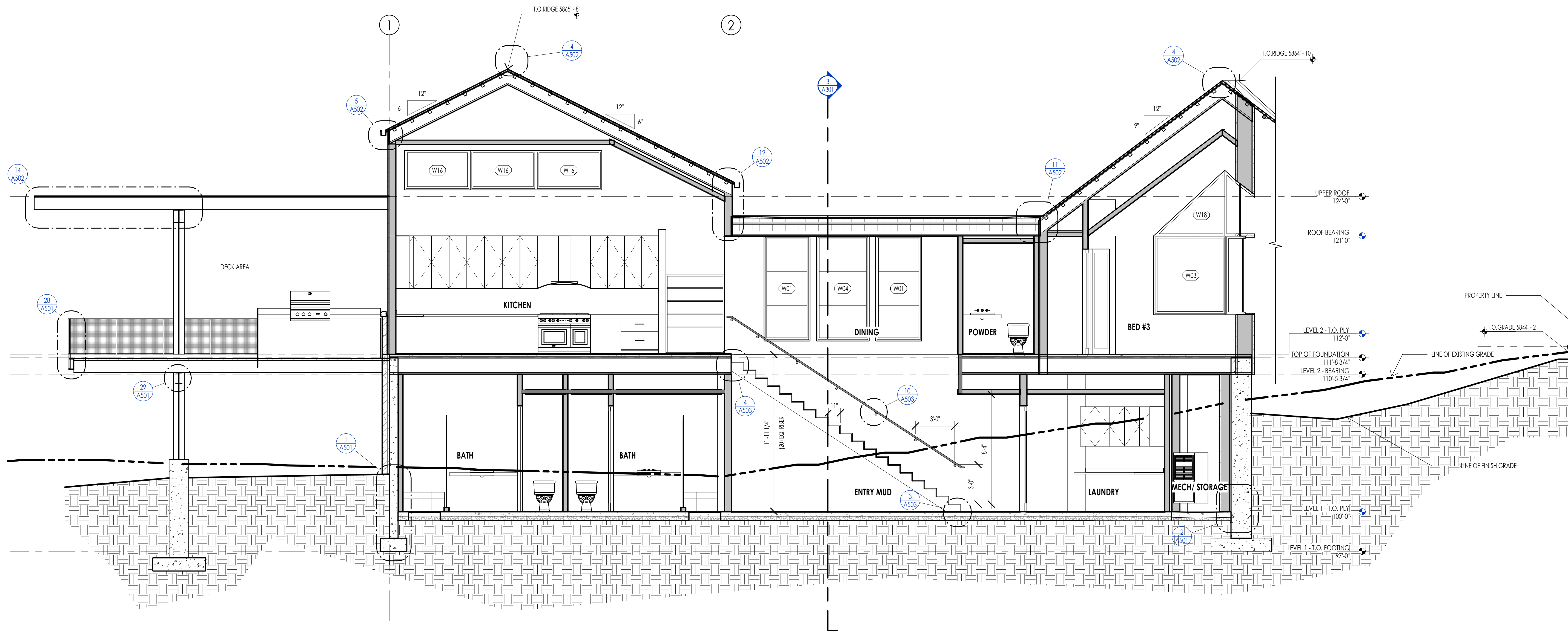




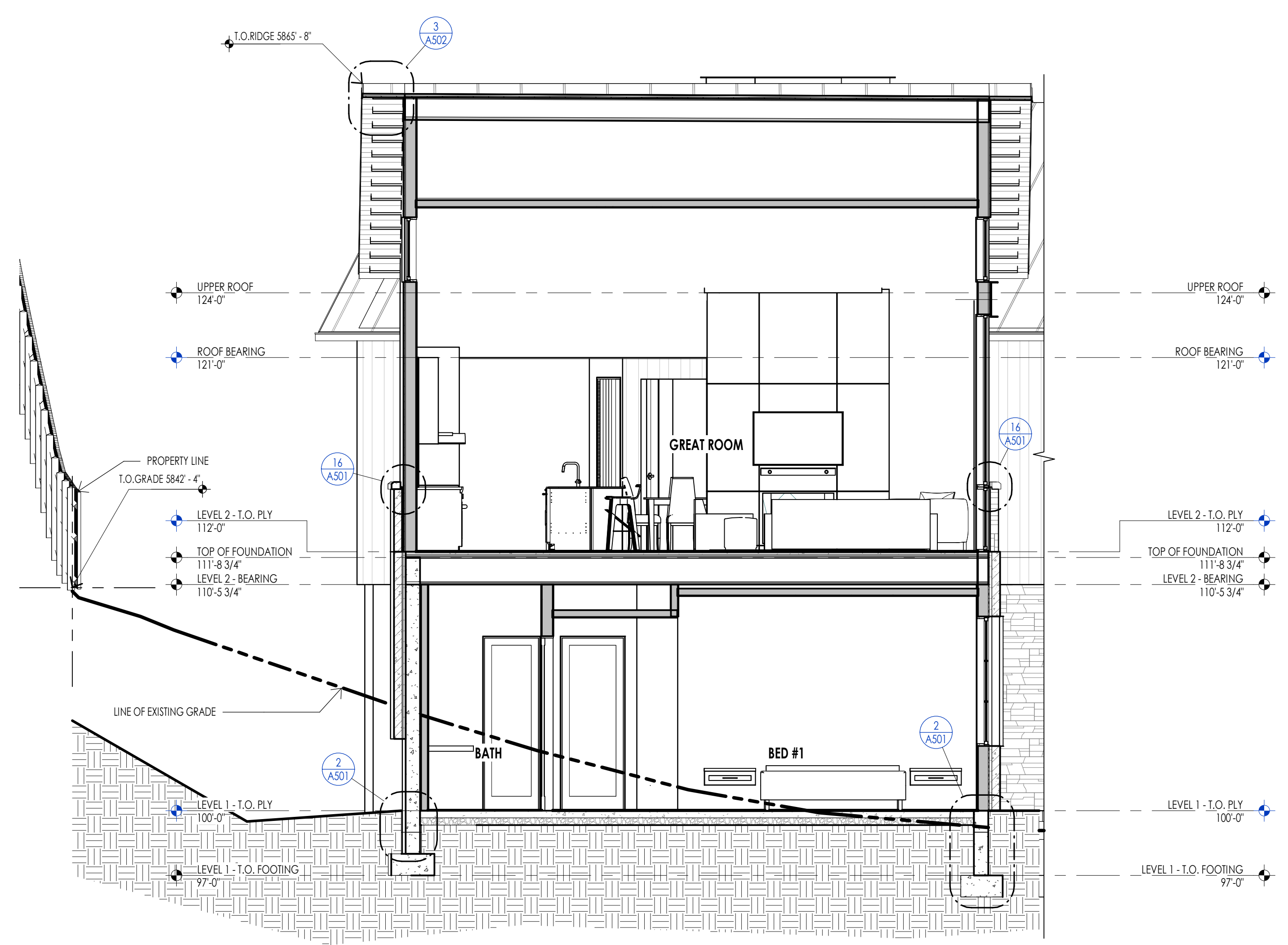
ELEVATION/ SECTION MATERIAL LEGEND	
HATCH PATTERN	DESCRIPTION
	STANDING SEAM ROOFING SYSTEM
	SINGLE PLY ROOFING MEMBRANE SYSTEM
	2X10 CEDAR CHANNEL LAP VERTICAL SIDING SYSTEM WITH SQUARE EDGES
	EXTERIOR STONE VENEER
	EXTERIOR METAL PANEL WALL SYSTEM

NOTE: REFER TO MATERIAL SPECIFICATIONS DOCUMENT FOR DETAILED INFORMATION REGARDING EACH FINISH MATERIAL.

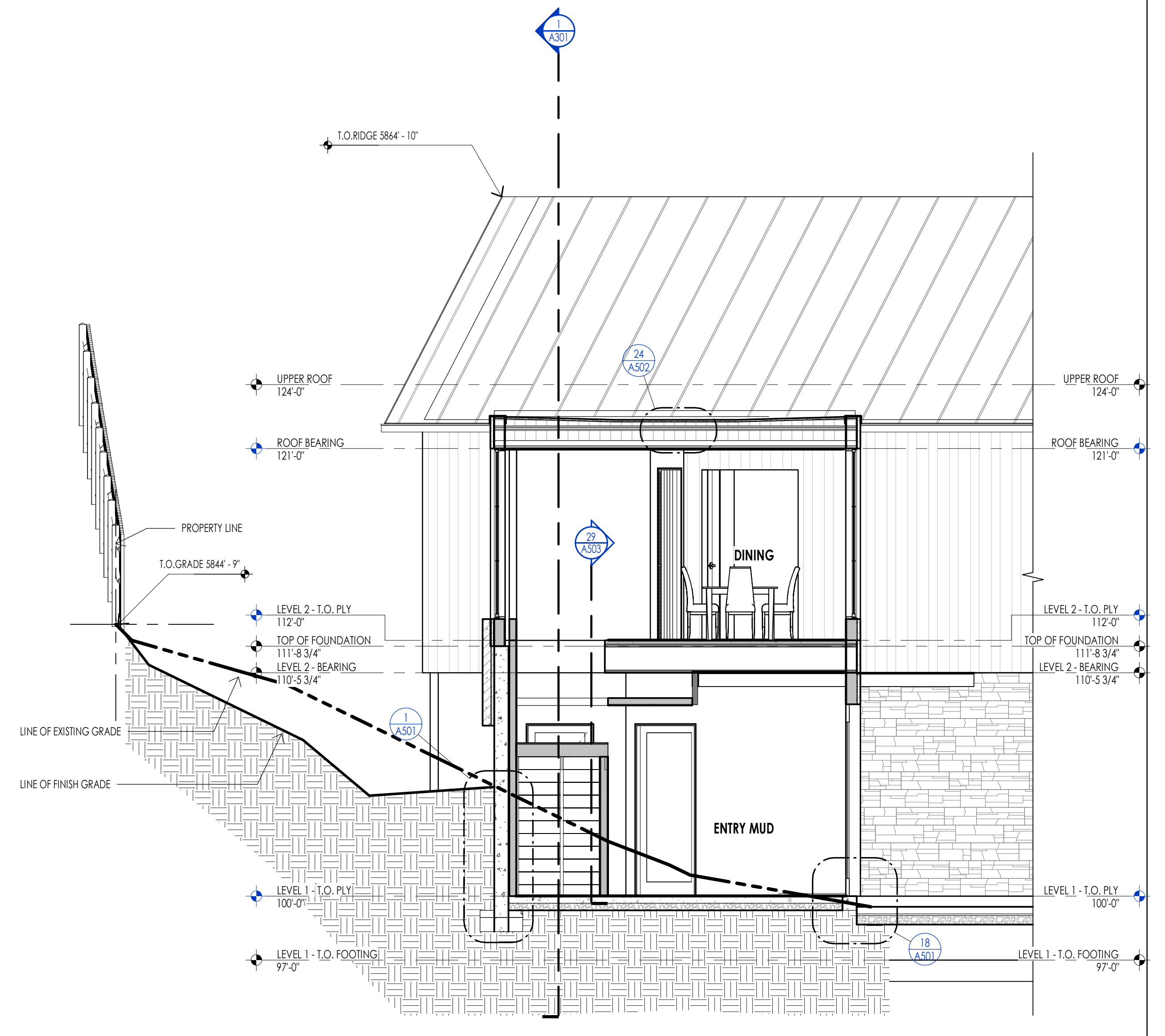
ELEVATION/ SECTION KEYNOTES



Section 1
1/4" = 1'-0" (A301)



Section 2
1/4" = 1'-0" (A301)



Section 3
1/4" = 1'-0" (A301)

WARM SPRINGS RESIDENCE #35

190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

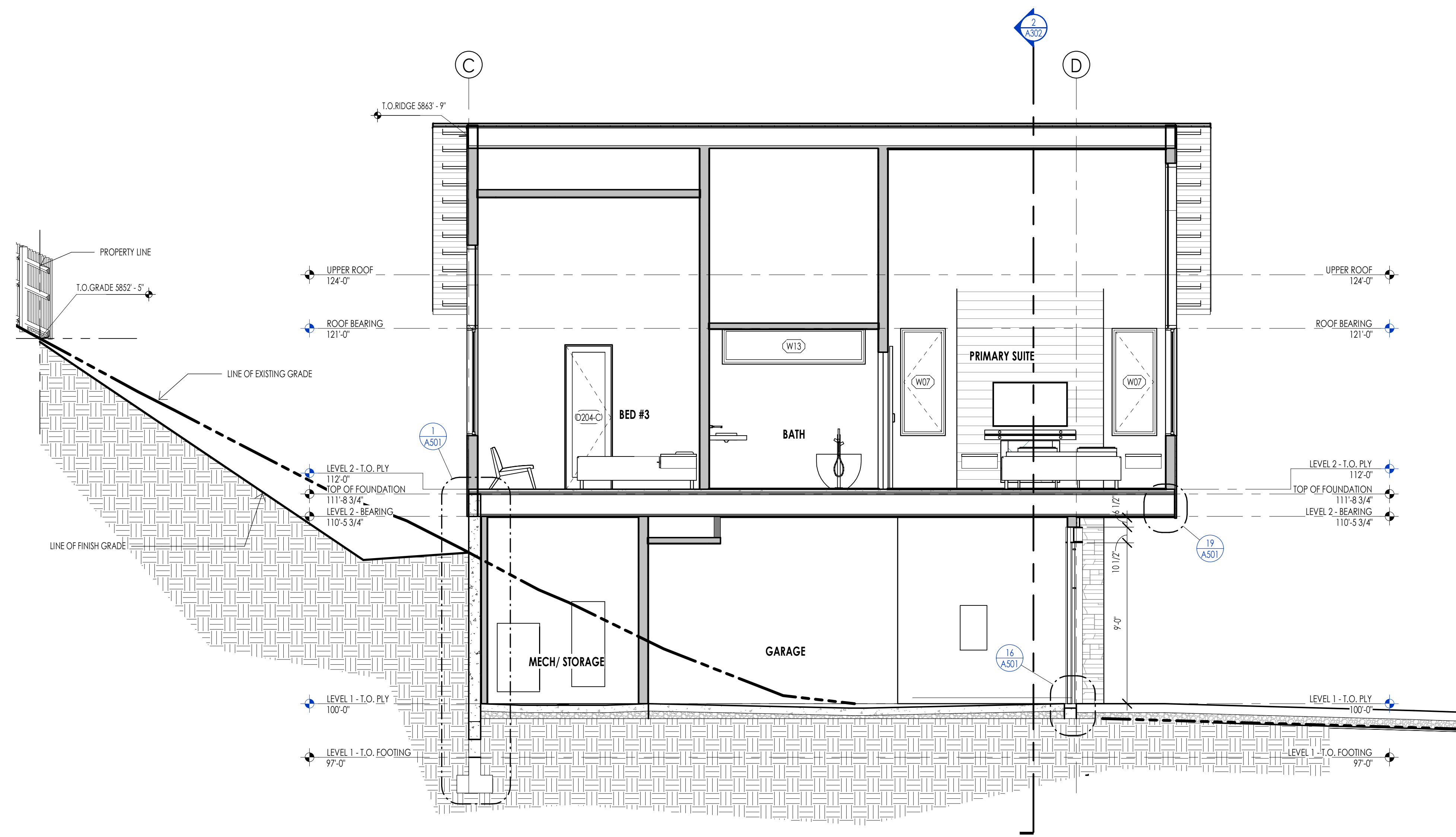
PROJECT NO. 22023
DATE: 2023.06.30

REVISIONS:
1 04-27-2023 PER CITY COMMENTS

SHEET TITLE:
BUILDING SECTIONS

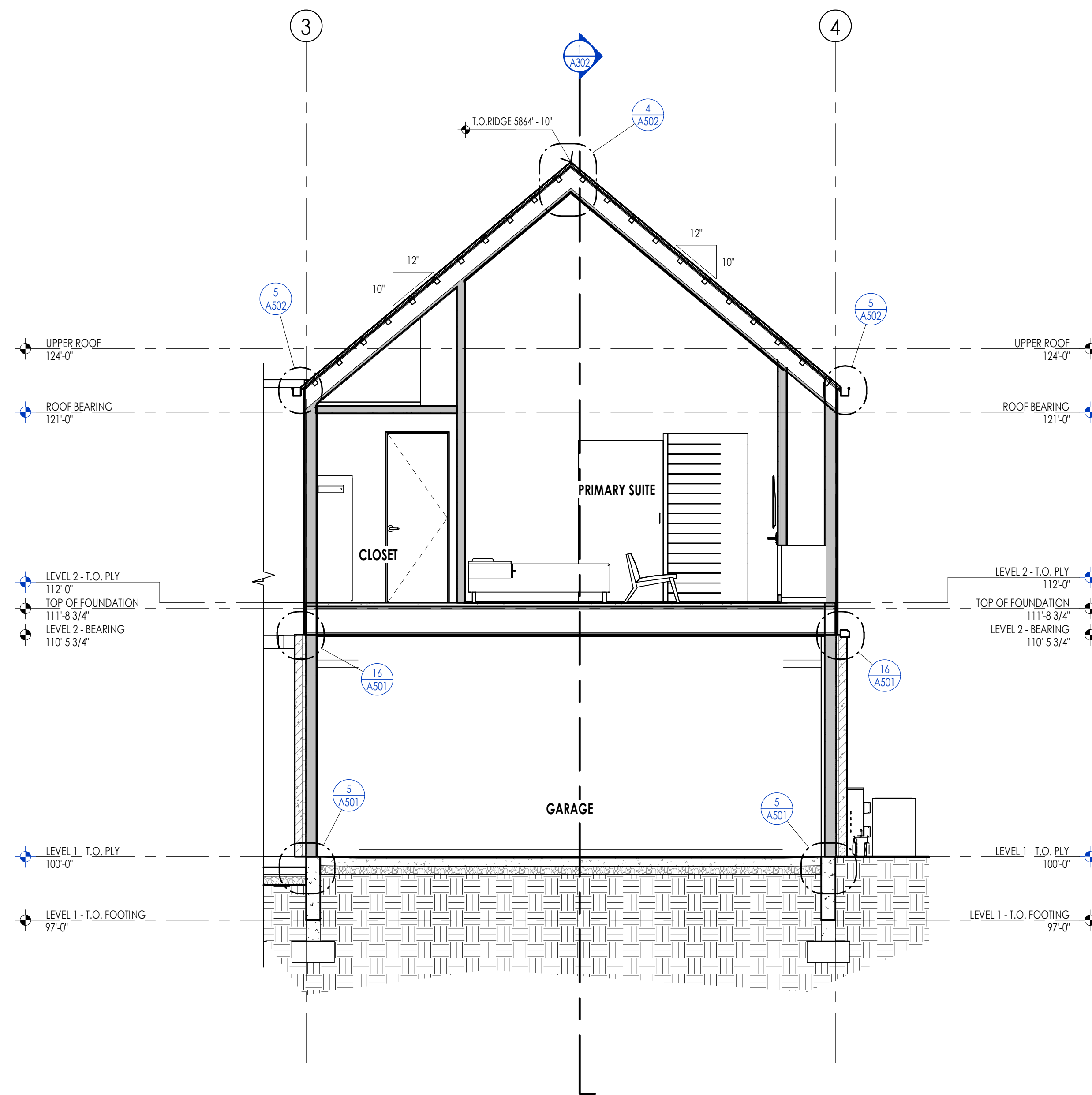
SHEET NUMBER:
A301

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Section 4
1/4" = 1'-0"

1
A302



Section 5
1/4" = 1'-0"

2
A302

ELEVATION/ SECTION MATERIAL LEGEND	
HATCH PATTERN	DESCRIPTION
	STANDING SEAM ROOFING SYSTEM
	SINGLE PLY ROOFING MEMBRANE SYSTEM
	2X10 CEDAR CHANNEL LAP VERTICAL SIDING SYSTEM WITH SQUARE EDGES
	EXTERIOR STONE VENEER
	EXTERIOR METAL PANEL WALL SYSTEM

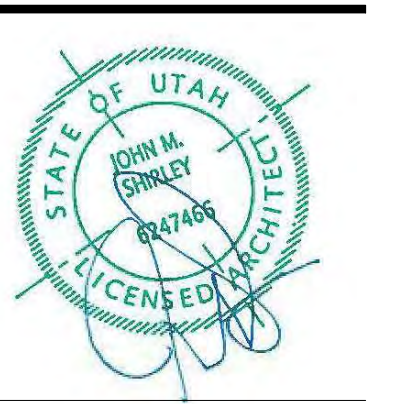
NOTE: REFER TO MATERIAL SPECIFICATIONS DOCUMENT FOR DETAILED INFORMATION REGARDING EACH FINISH MATERIAL.

ELEVATION/ SECTION KEYNOTES



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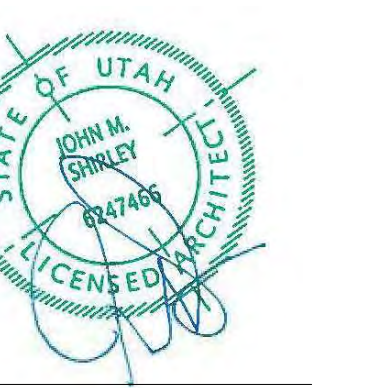
WARM SPRINGS RESIDENCE #35
190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

PROJECT NO. 22023
DATE: 2023.06.30
REVISIONS:
1 04-27-2023 PER CITY COMMENTS

SHEET TITLE:
BUILDING SECTIONS

SHEET NUMBER:
A302

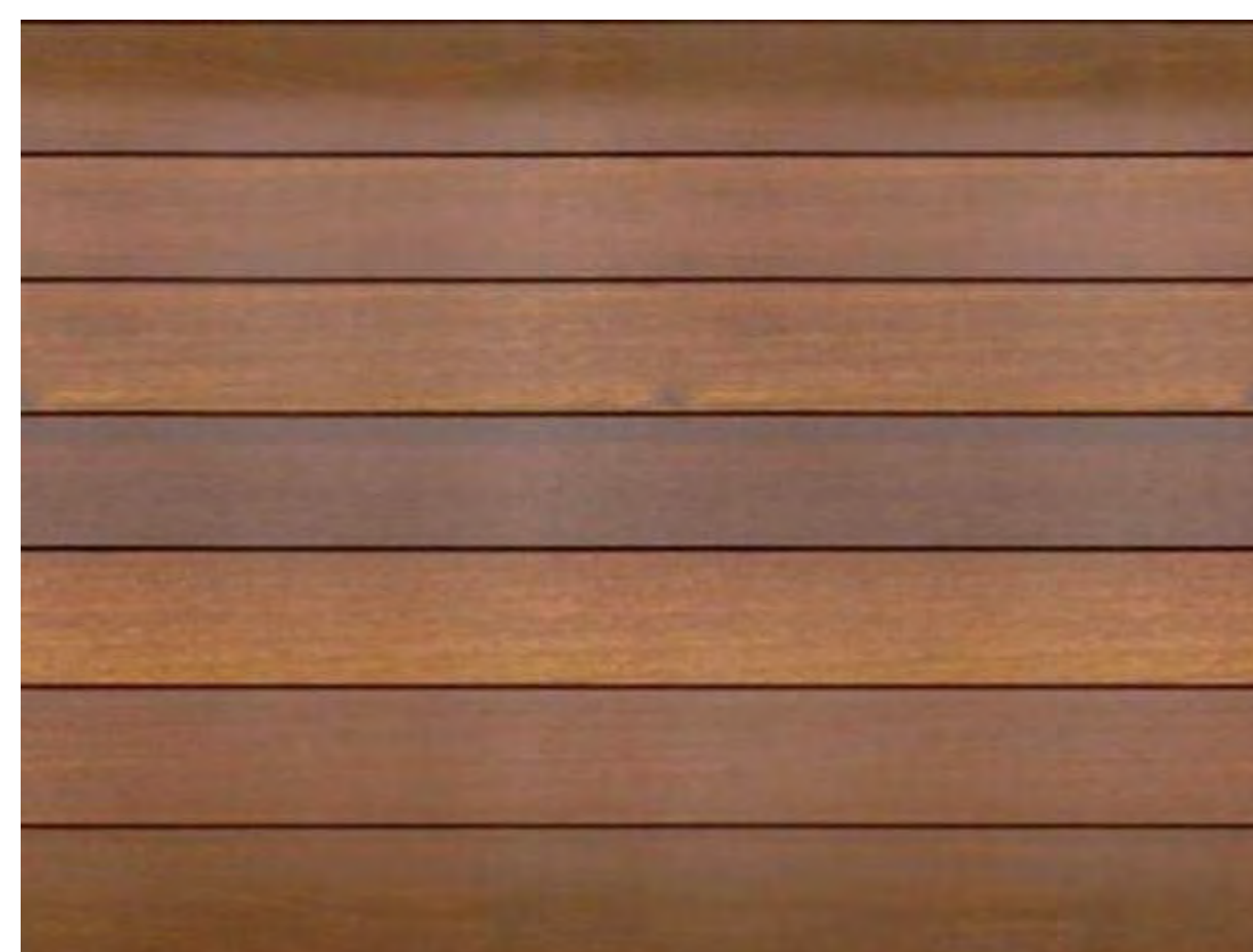
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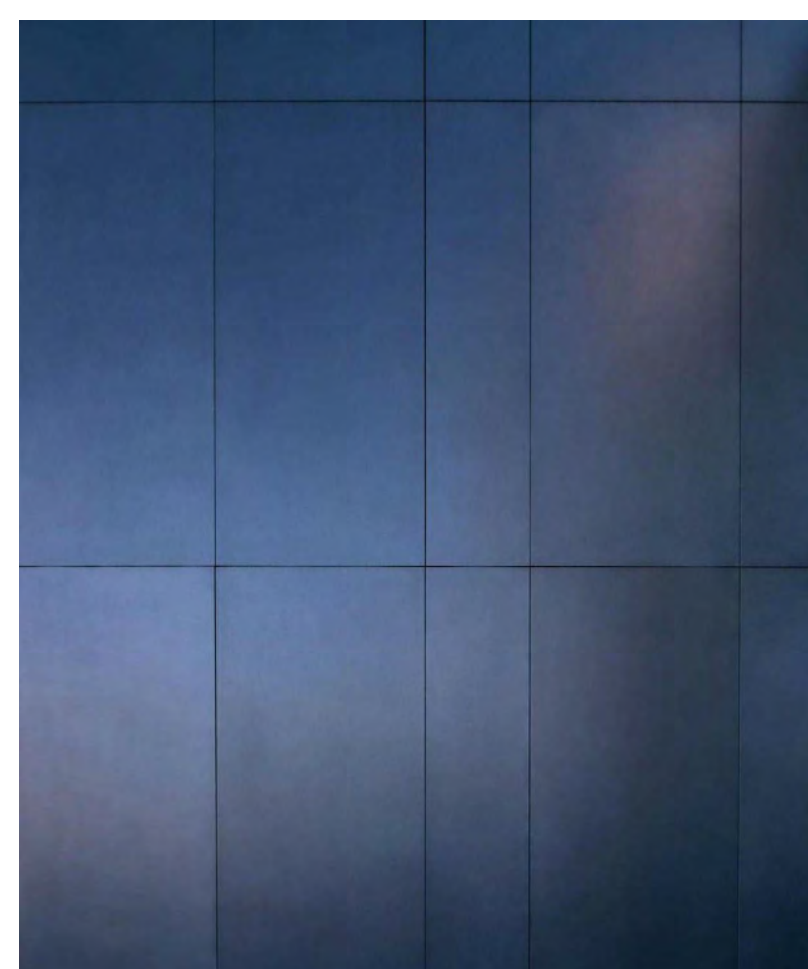
GREAT ROOM INTERIOR VIEW



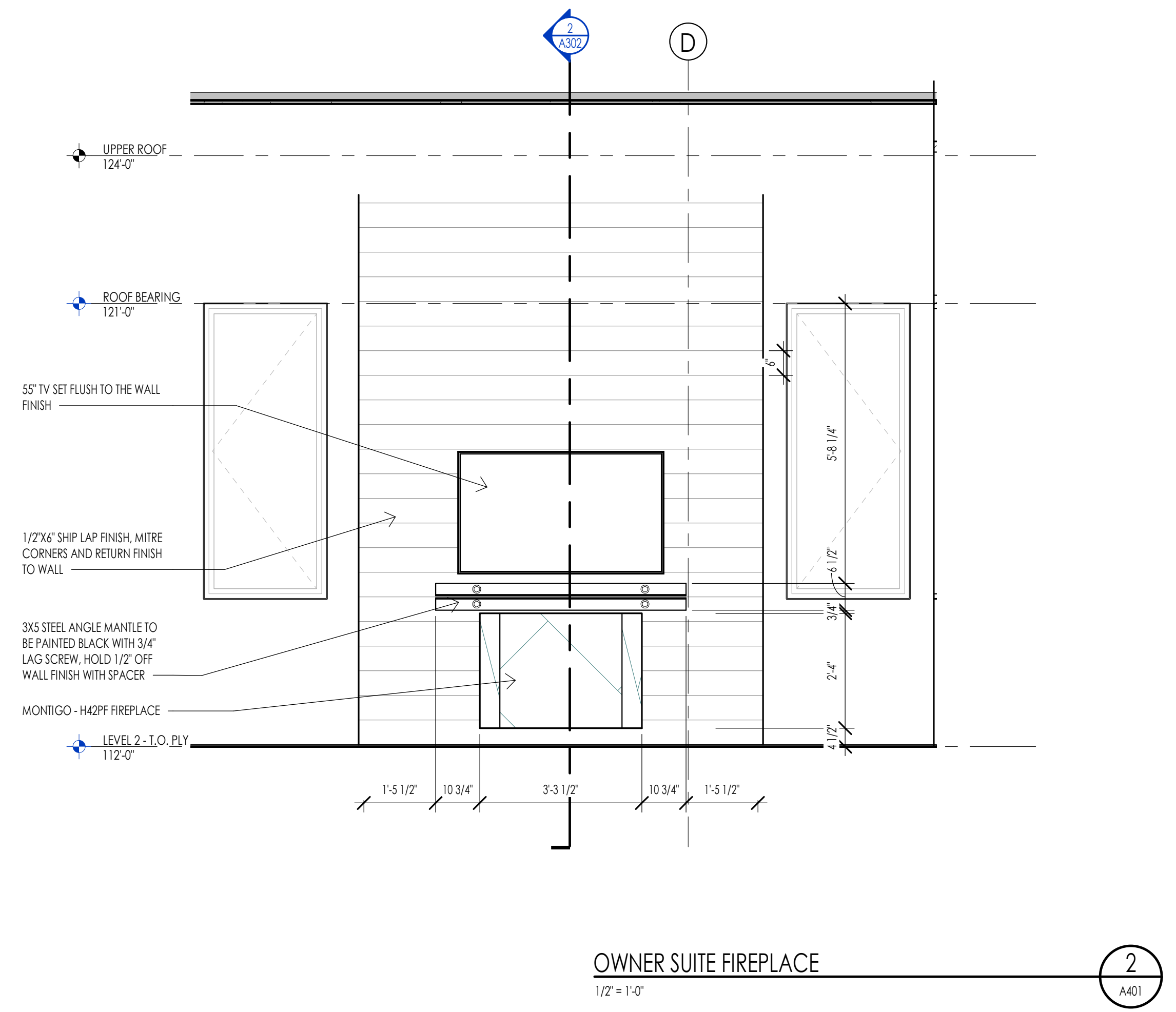
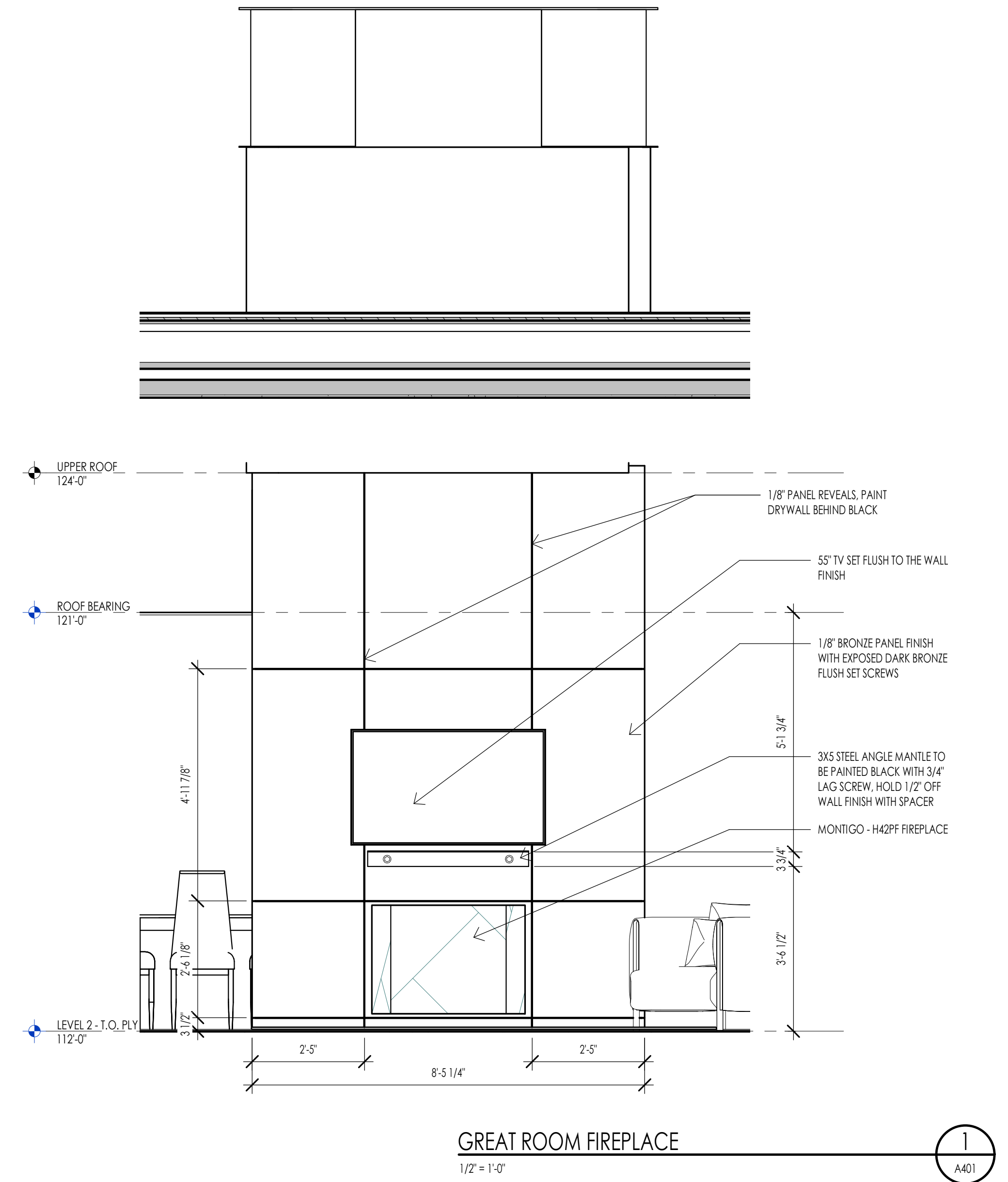
OUTDOOR LIVING VIEW



INTERIOR WOOD SLAT FINISH



STEEL FIREPLACE SURROUND



WARM SPRINGS RESIDENCE #35

190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

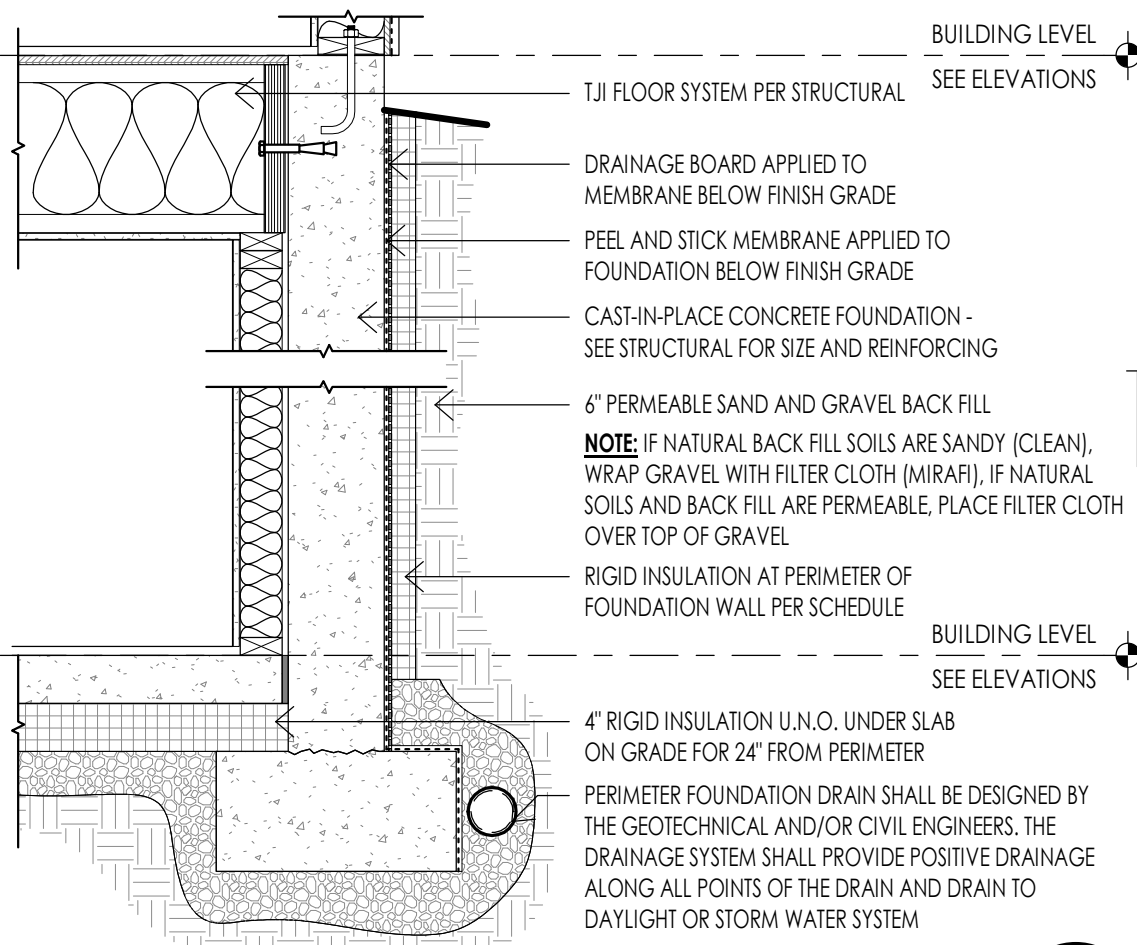
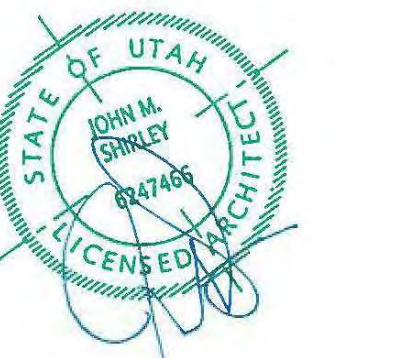
PROJECT NO. 22023
DATE: 2023.06.30
REVISIONS:

SHEET TITLE:
FIREPLACE ELEVATIONS

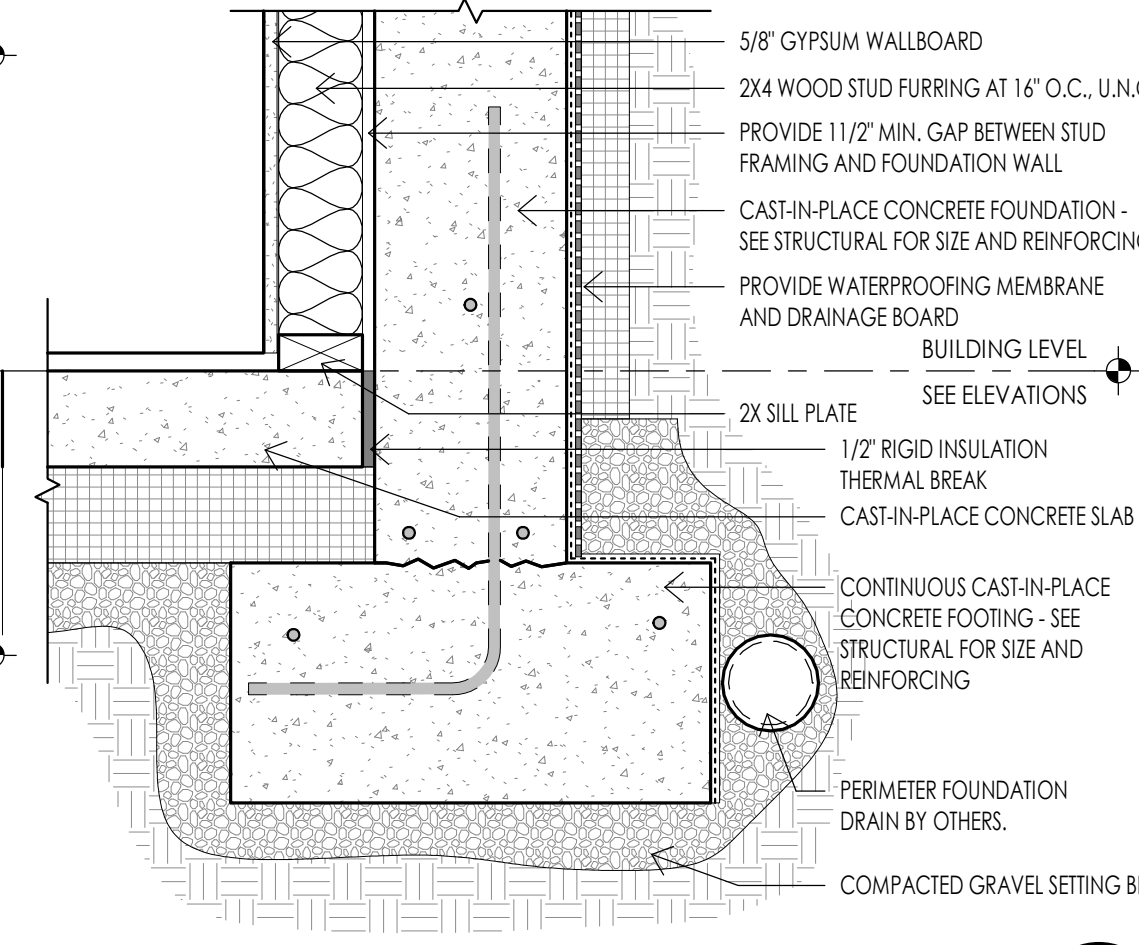
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PERMIT SET

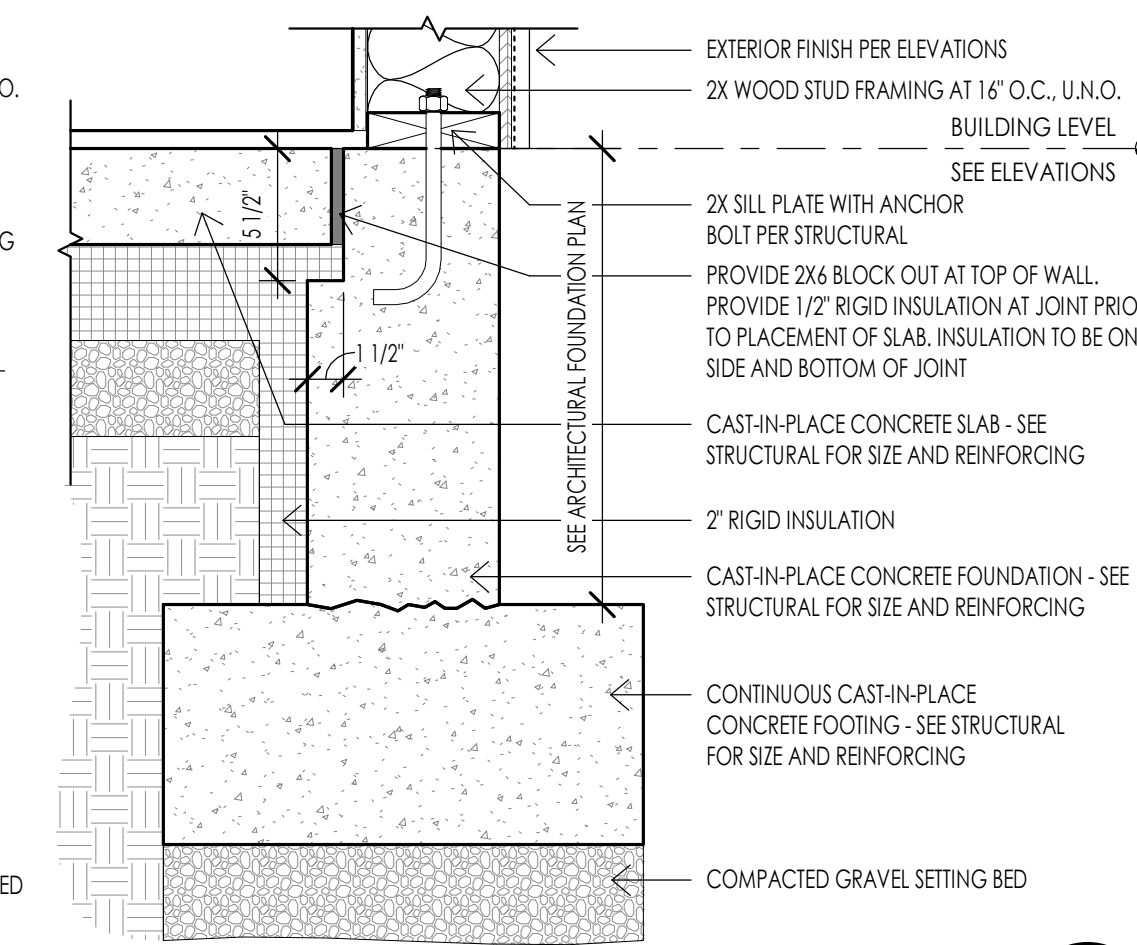
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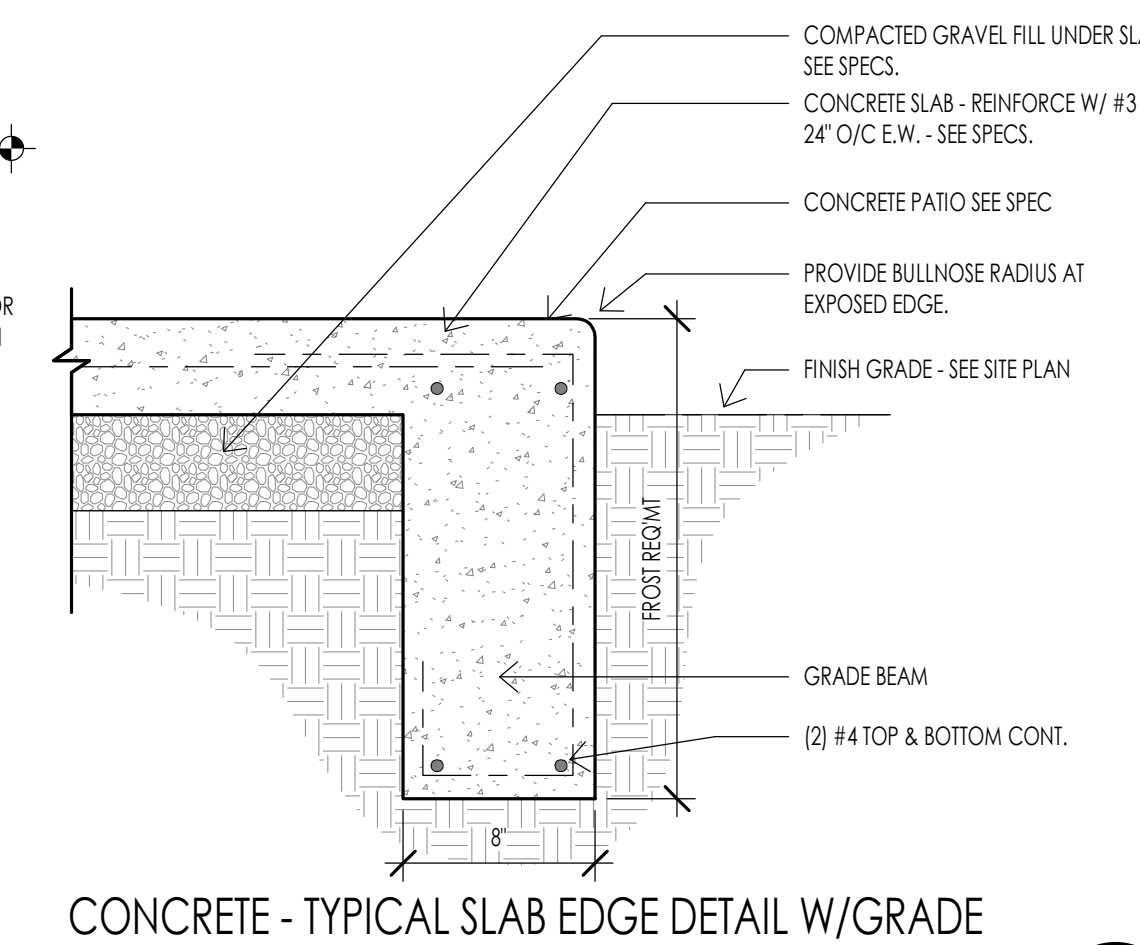
CONCRETE - TYPICAL FOUNDATION WATERPROOFING
3/4" = 1'-0"
A501



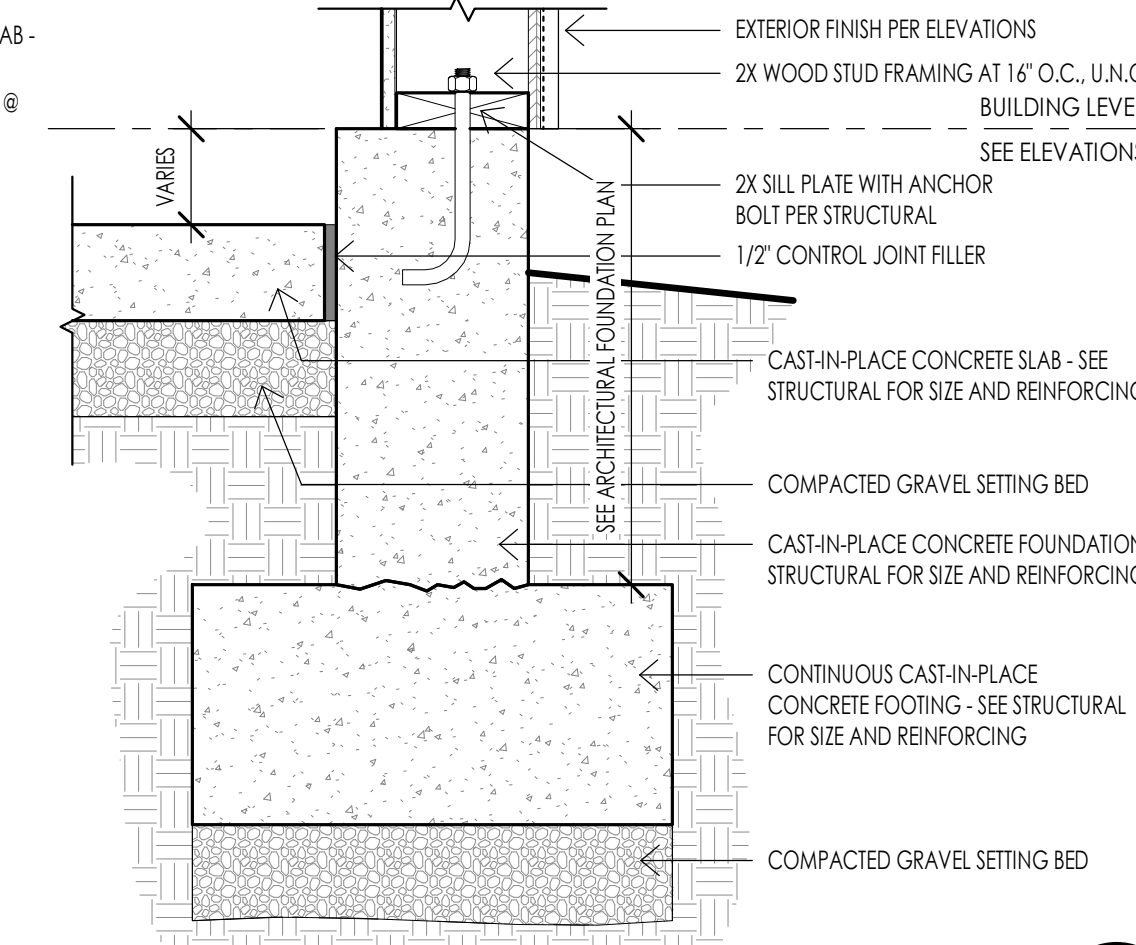
CONCRETE - TYPICAL FOUNDATION FURR OUT WALL
1 1/2" = 1'-0"
A501



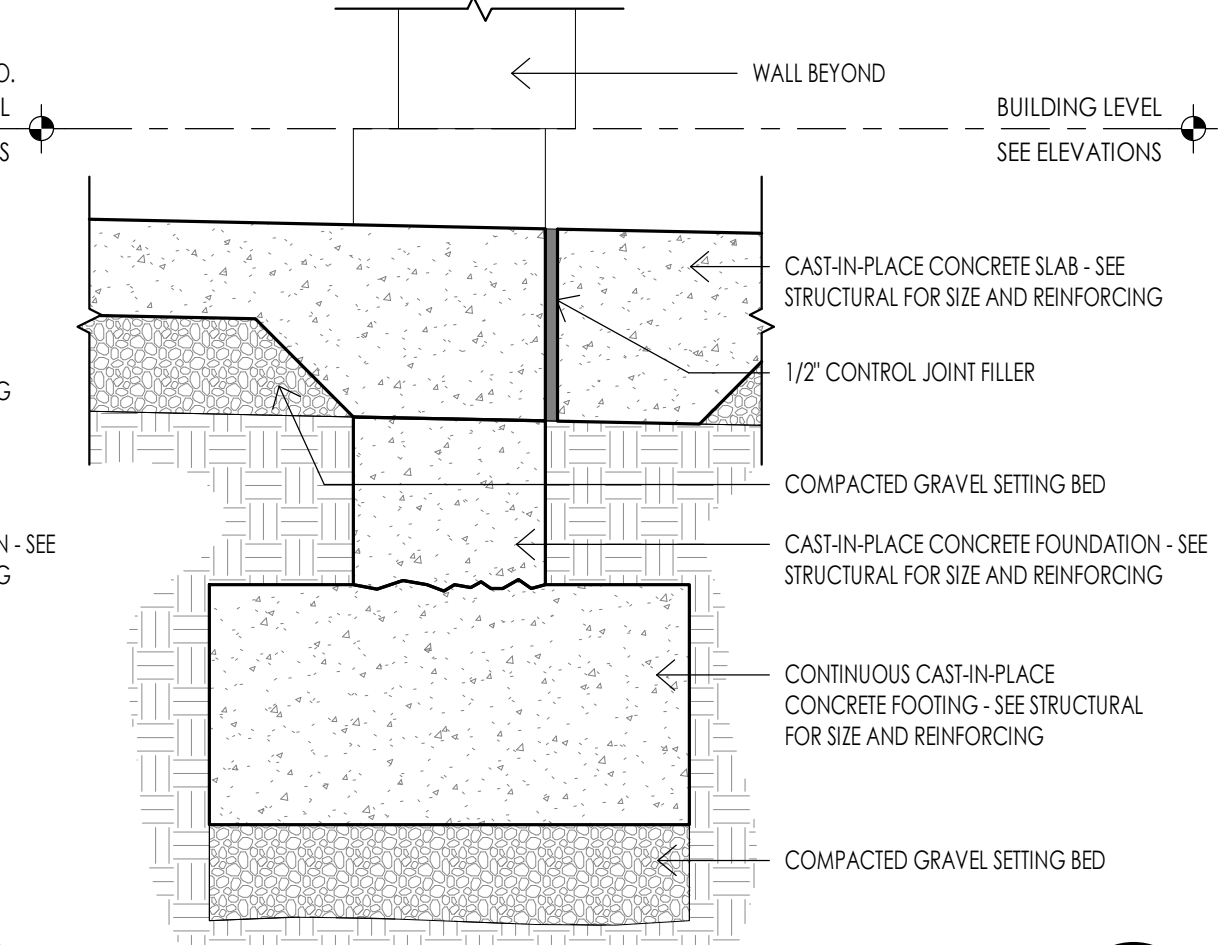
CONCRETE - FOUNDATION DETAIL AT WALKOUT
1 1/2" = 1'-0"
A501



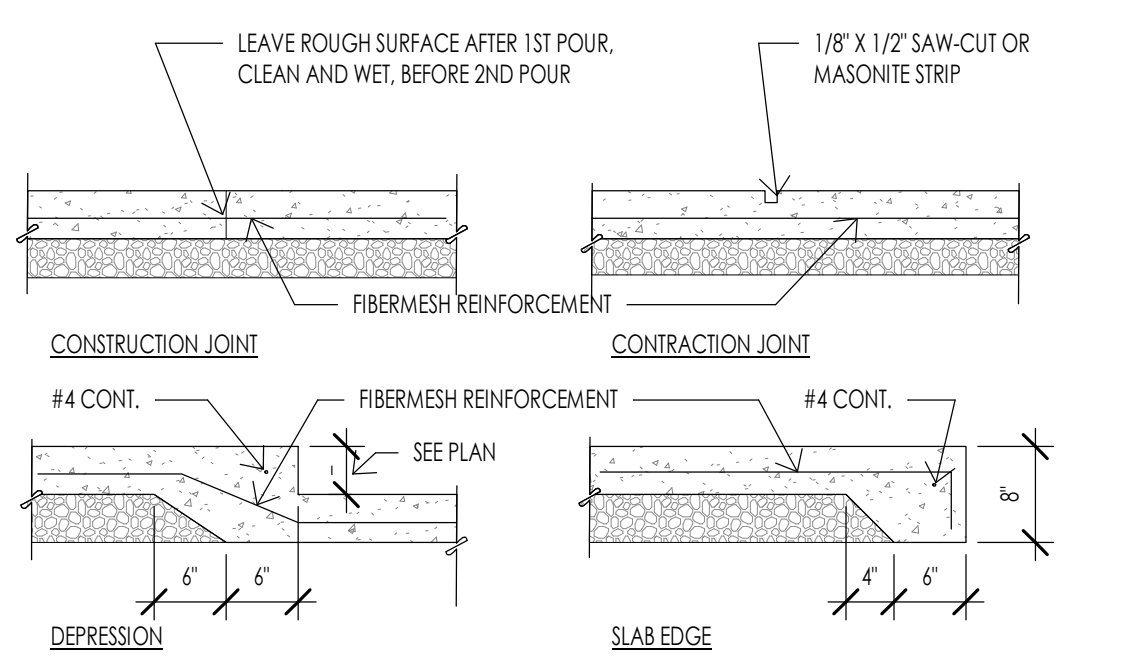
CONCRETE - TYPICAL SLAB EDGE DETAIL W/GRADE BEAM
1 1/2" = 1'-0"
A501



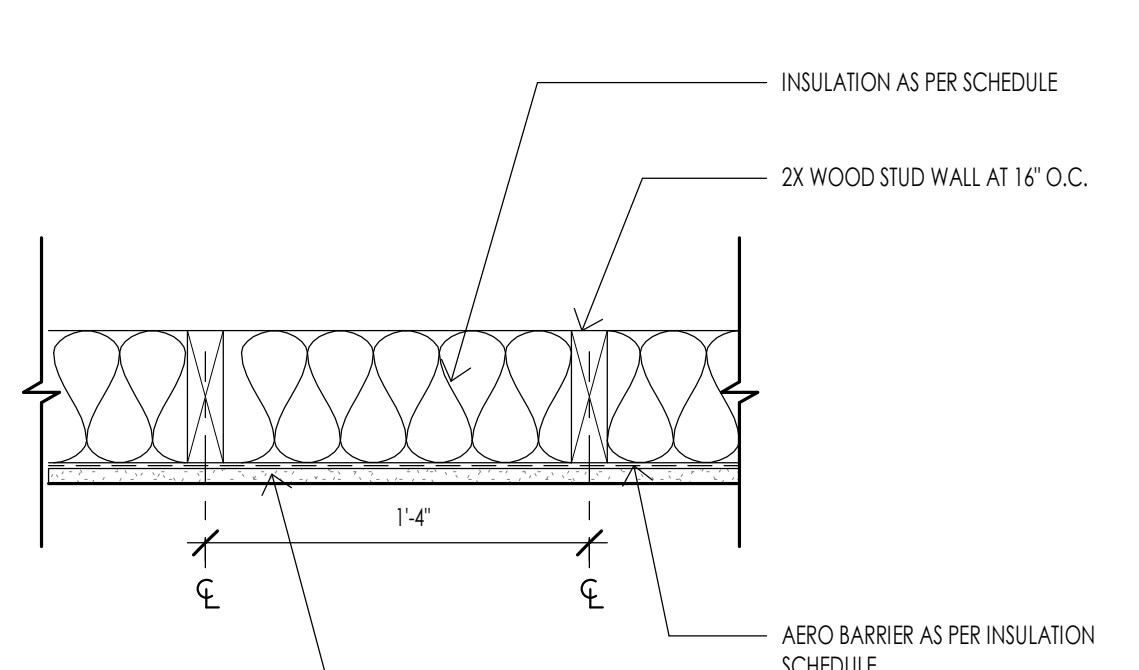
CONCRETE - FOUNDATION DETAIL AT GARAGE
1 1/2" = 1'-0"
A501



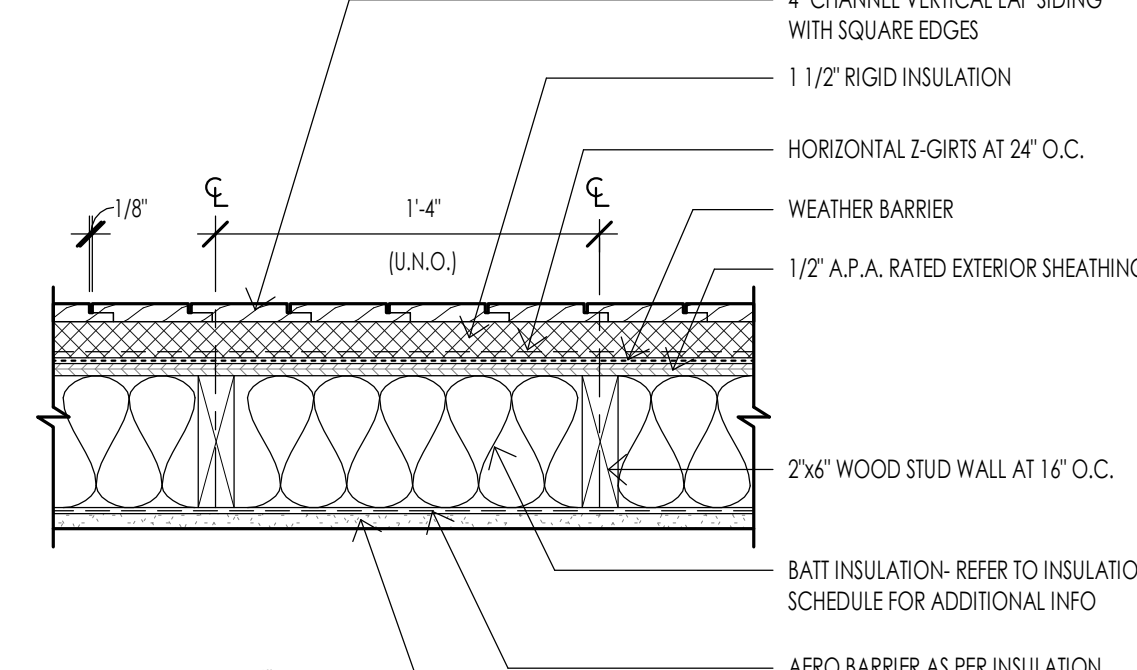
CONCRETE - FOUNDATION DETAIL AT GARAGE DOOR
1 1/2" = 1'-0"
A501



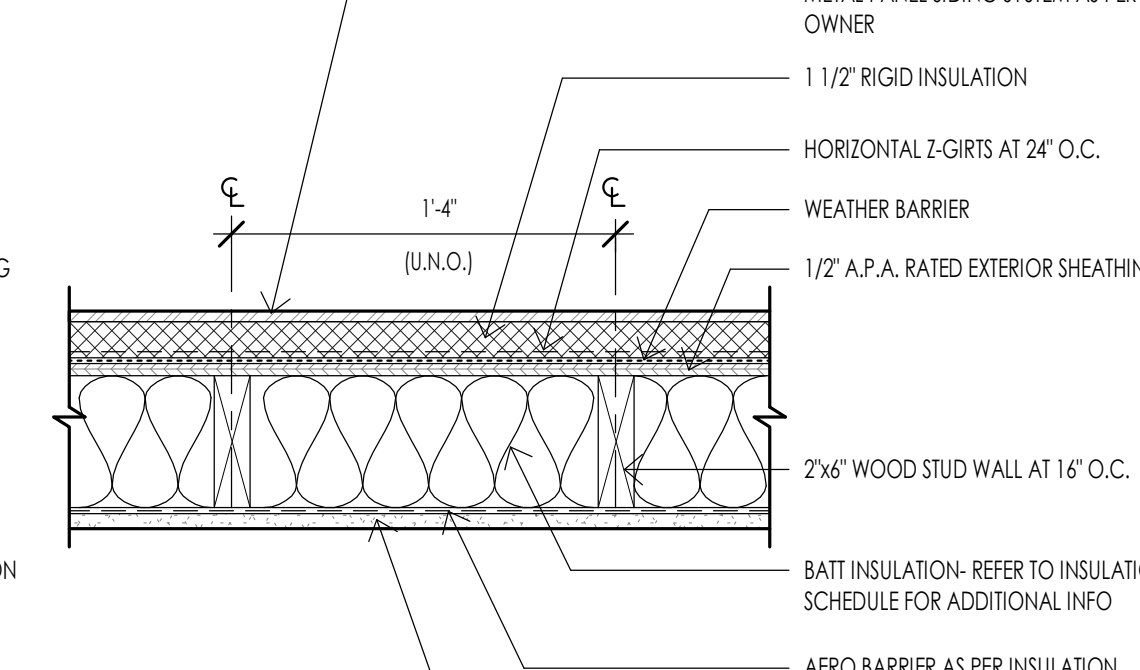
CONCRETE - TYPICAL SLAB DETAILS
3/4" = 1'-0"
A501



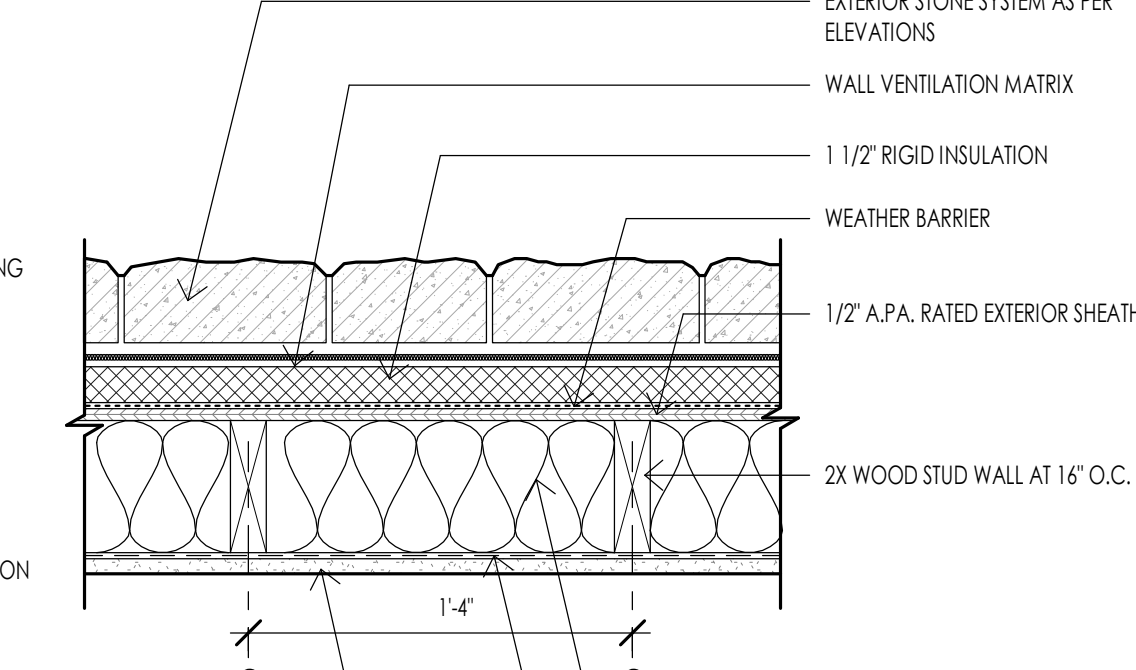
FRAMING - FURR OUT WALL ASSEMBLY
1 1/2" = 1'-0"
A501



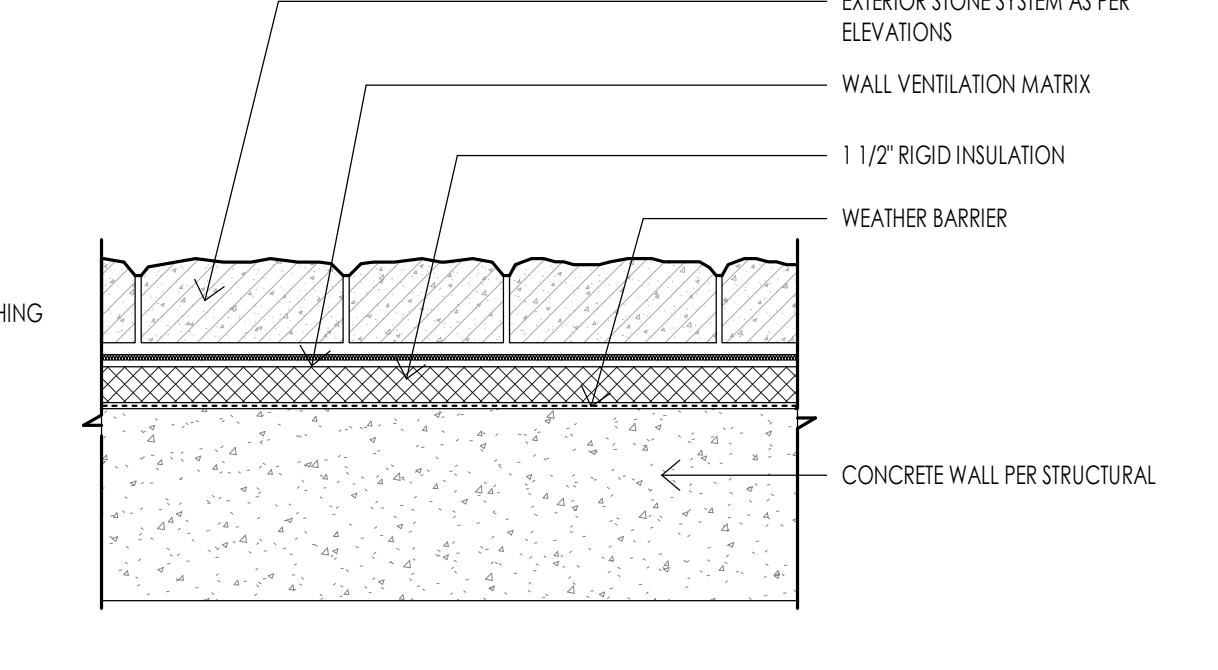
FRAMING - WOOD SIDING WALL ASSEMBLY
1 1/2" = 1'-0"
A501



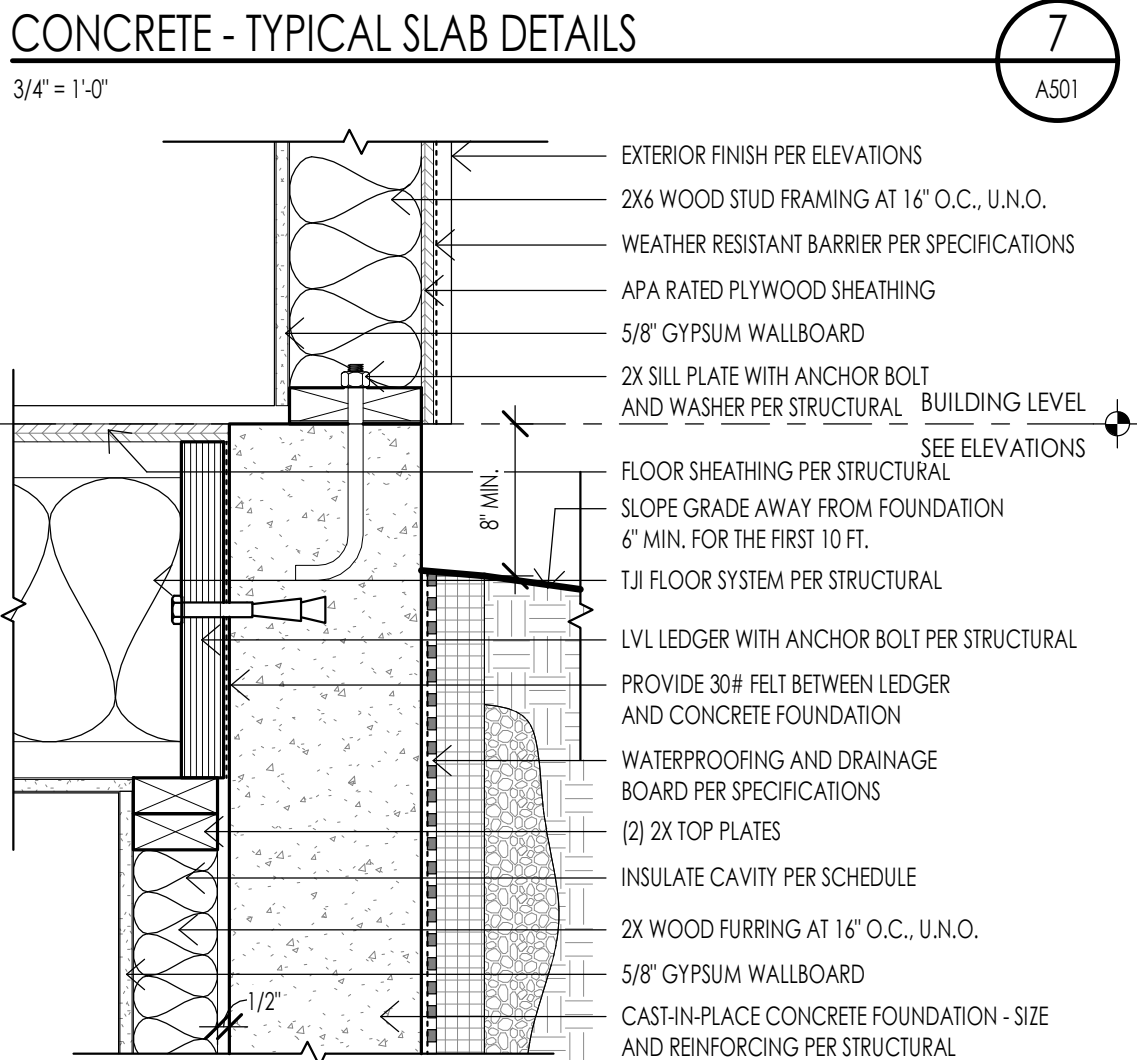
FRAMING - METAL PANEL WALL ASSEMBLY
1 1/2" = 1'-0"
A501



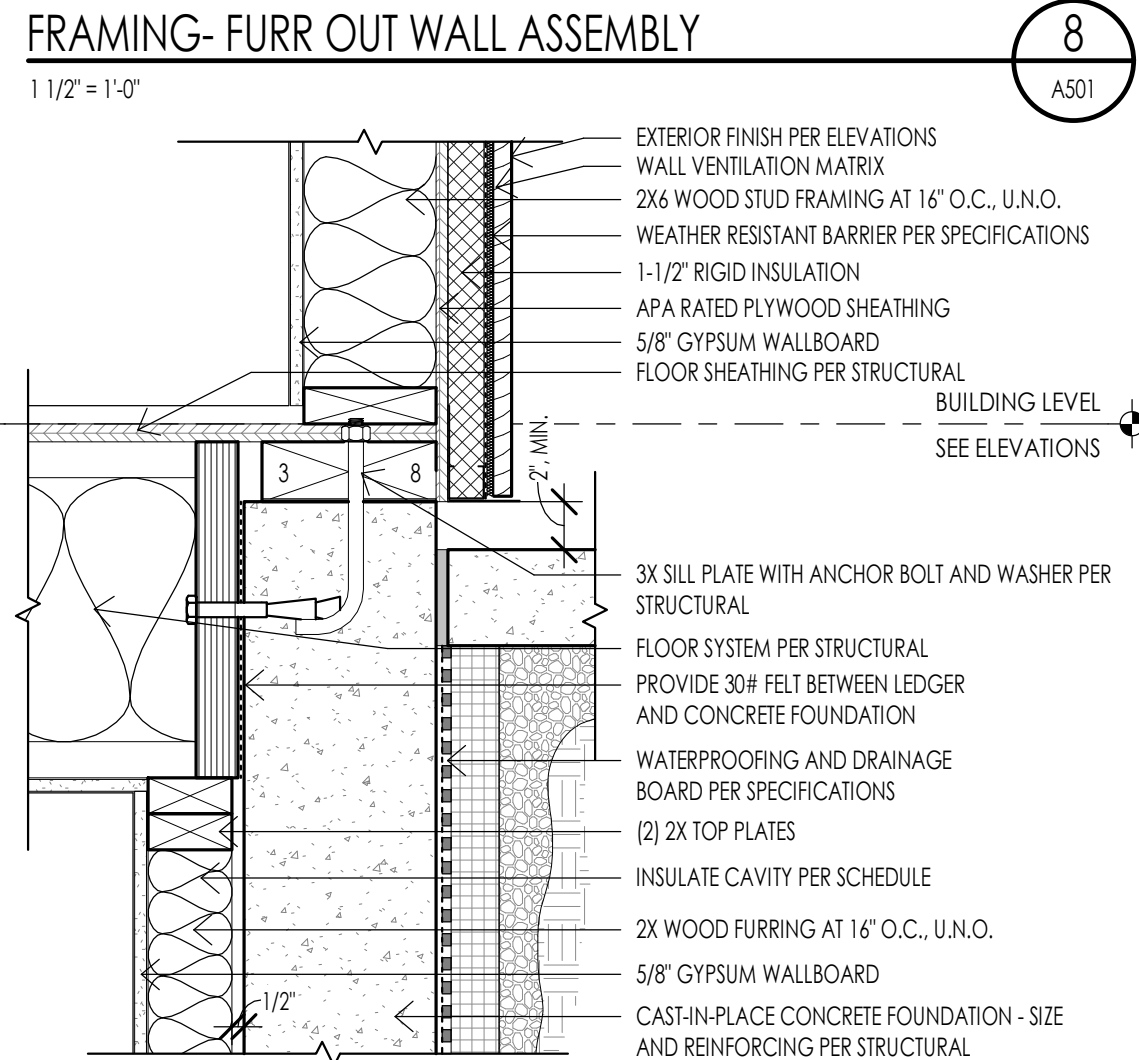
FRAMING - STONE MASONRY WALL ASSEMBLY
1 1/2" = 1'-0"
A501



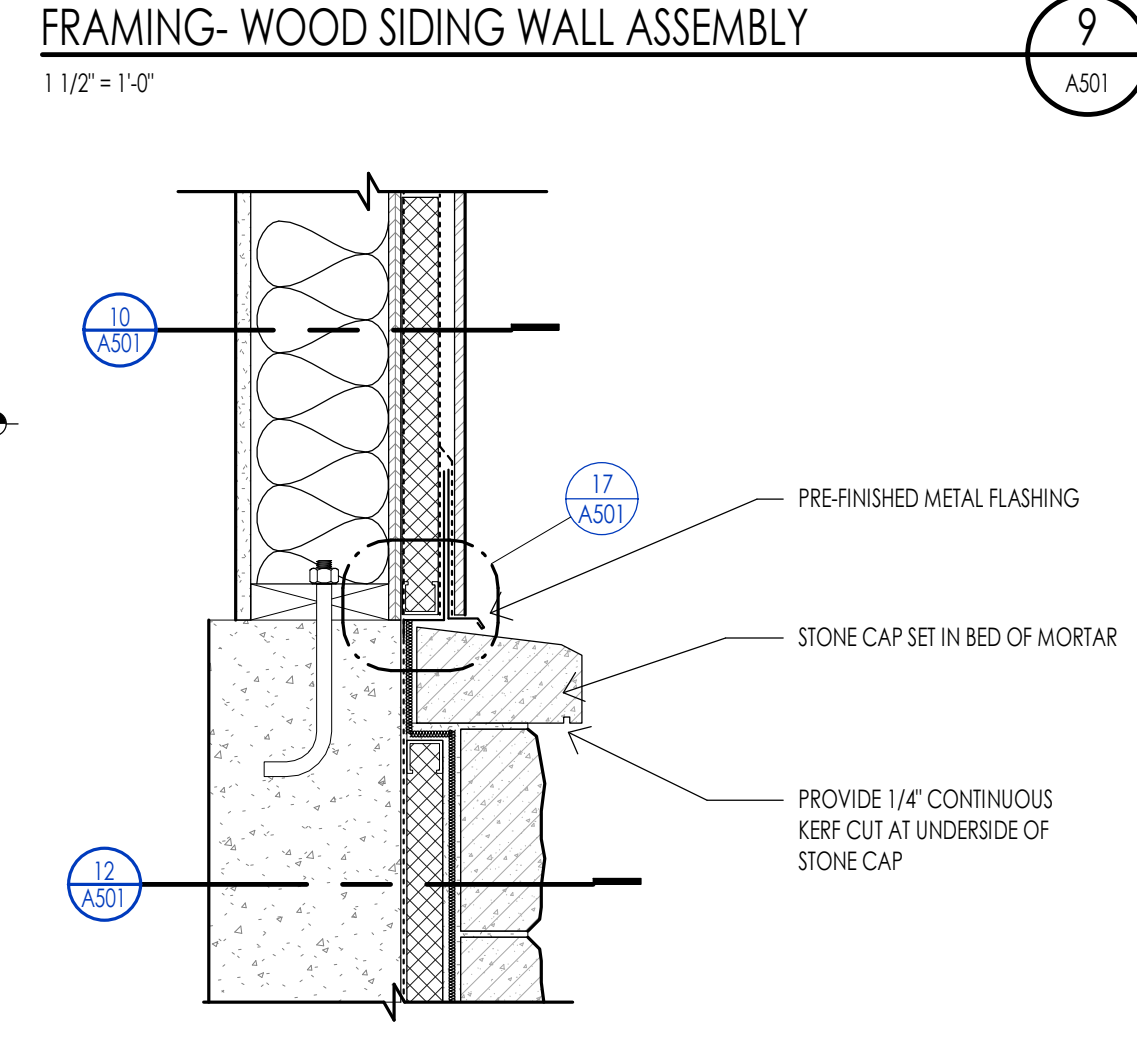
FRAMING - CONCRETE WALL W STONE ASSEMBLY
1 1/2" = 1'-0"
A501



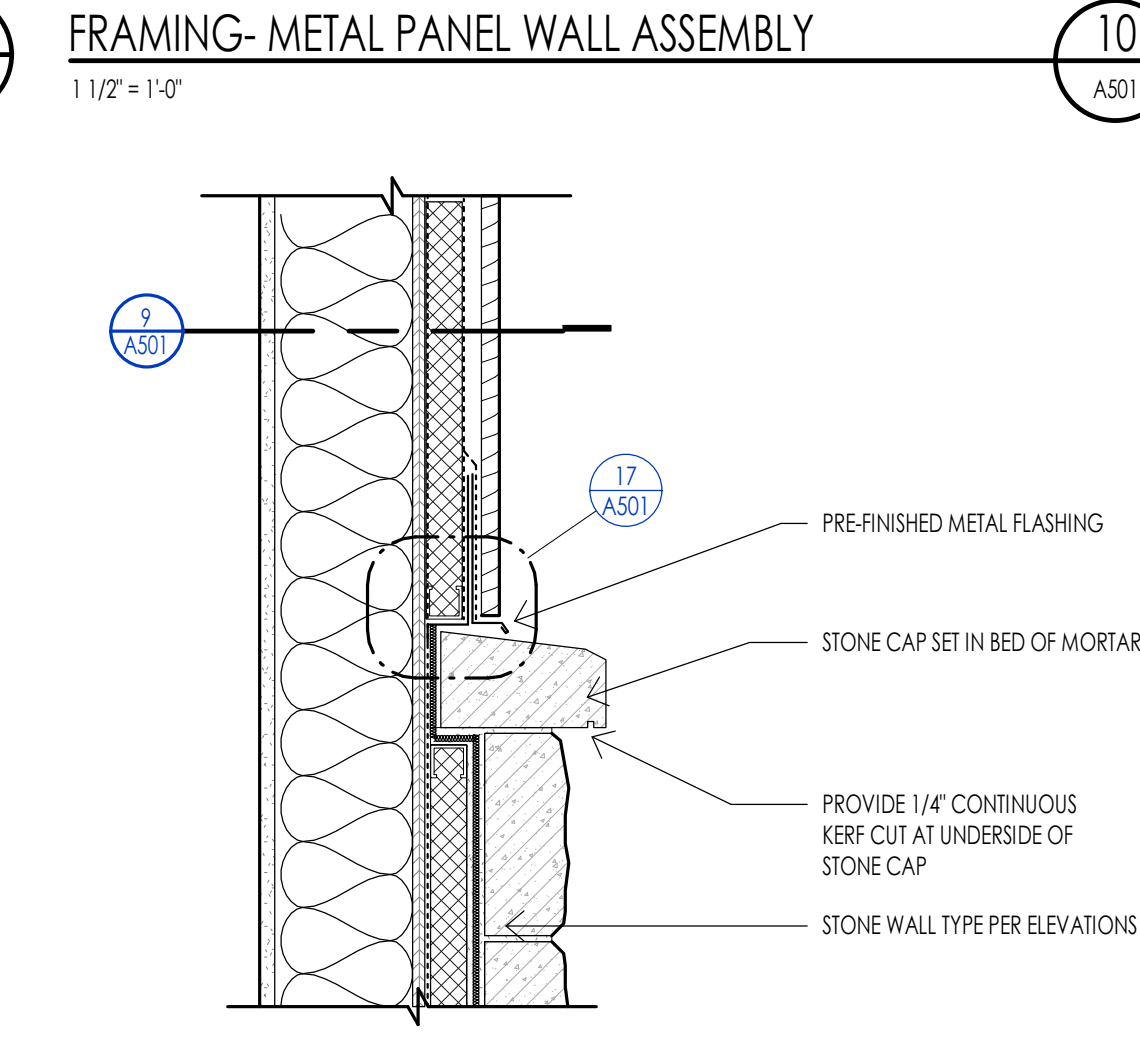
TYPICAL FLOOR DETAIL AT FOUNDATION
1 1/2" = 1'-0"
A501



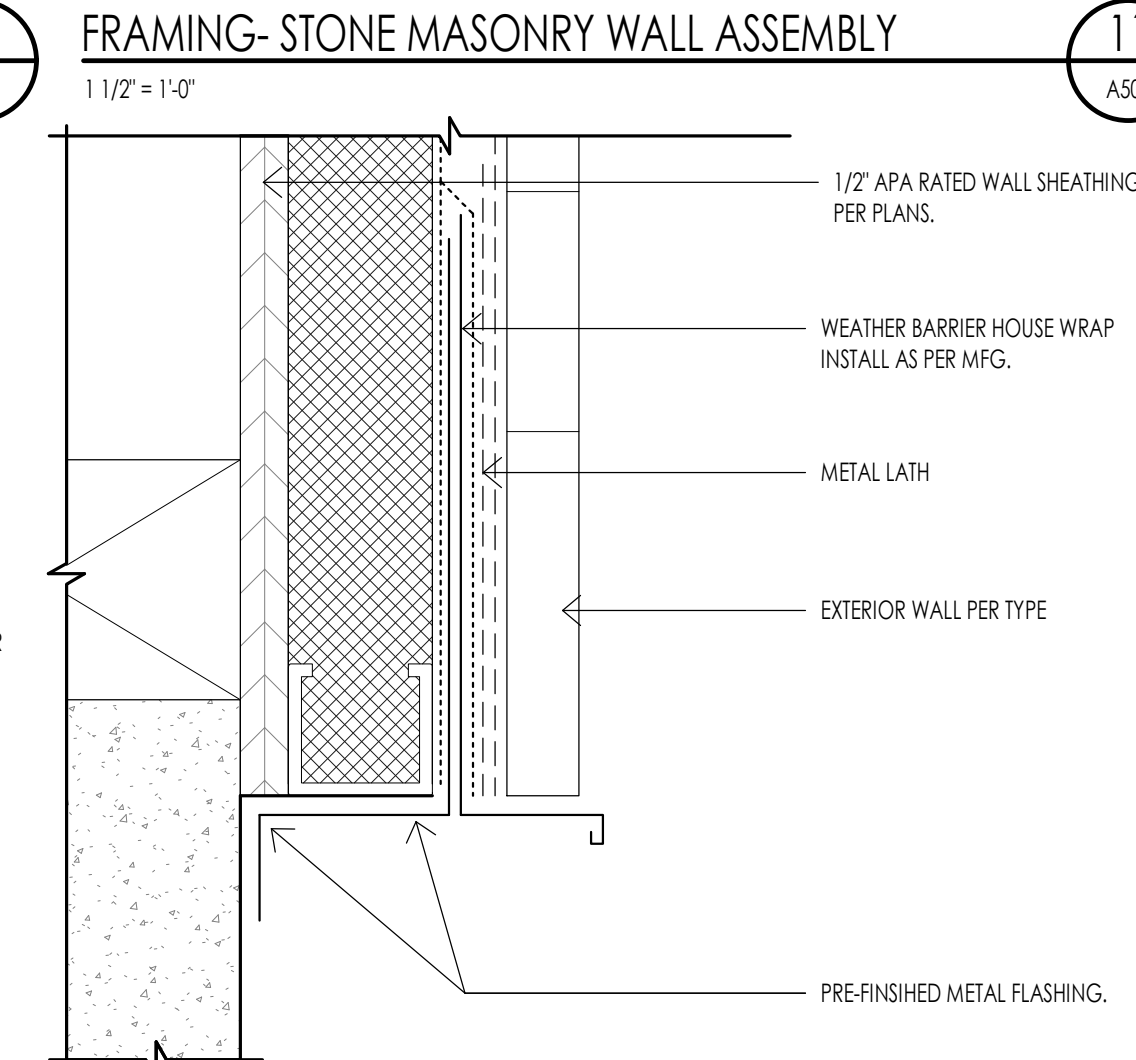
FLOOR DETAIL AT FOUNDATION/PATIO
1 1/2" = 1'-0"
A501



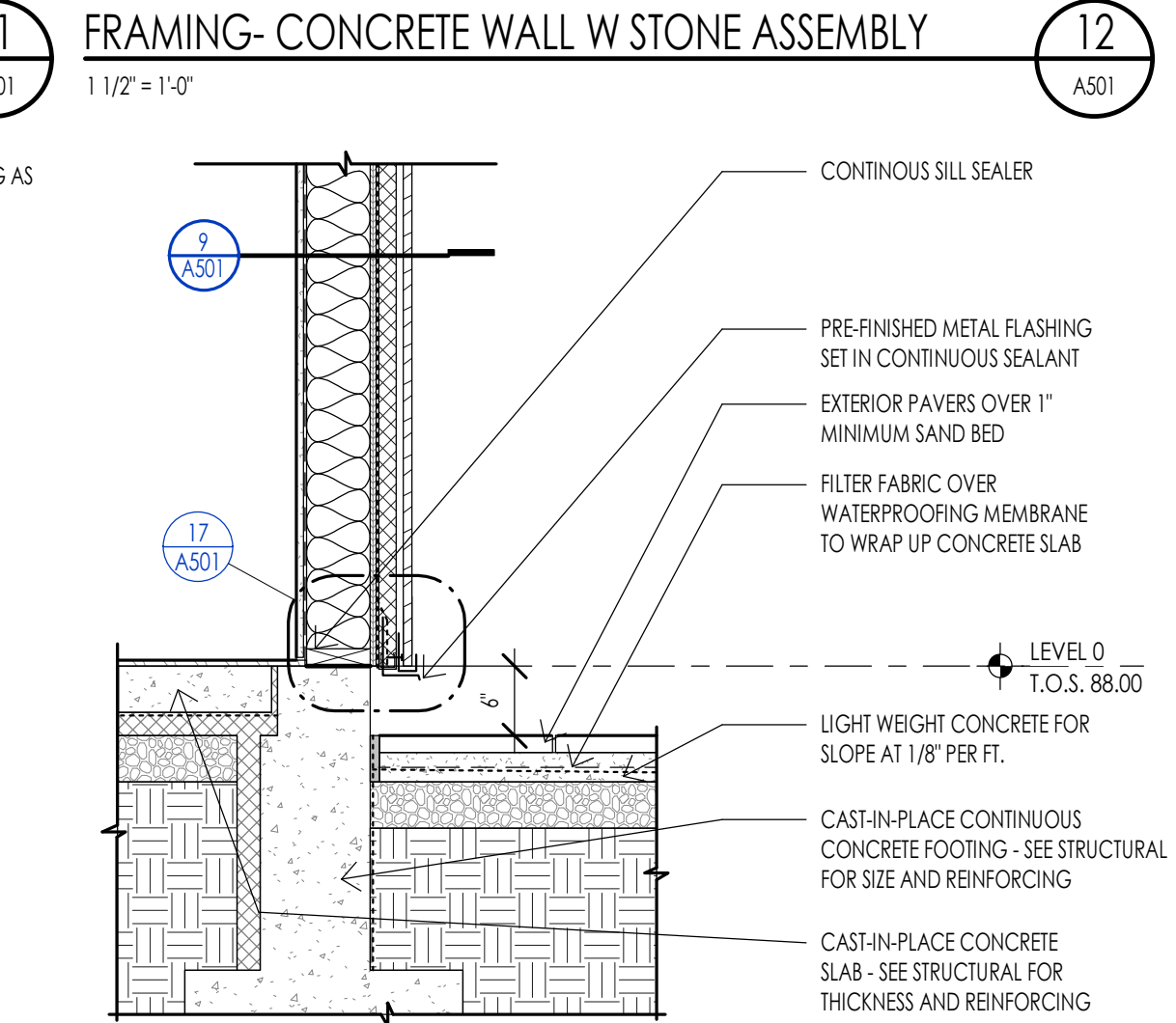
MASONRY - STONE WALL TO METAL PANEL WALL
1 1/2" = 1'-0"
A501



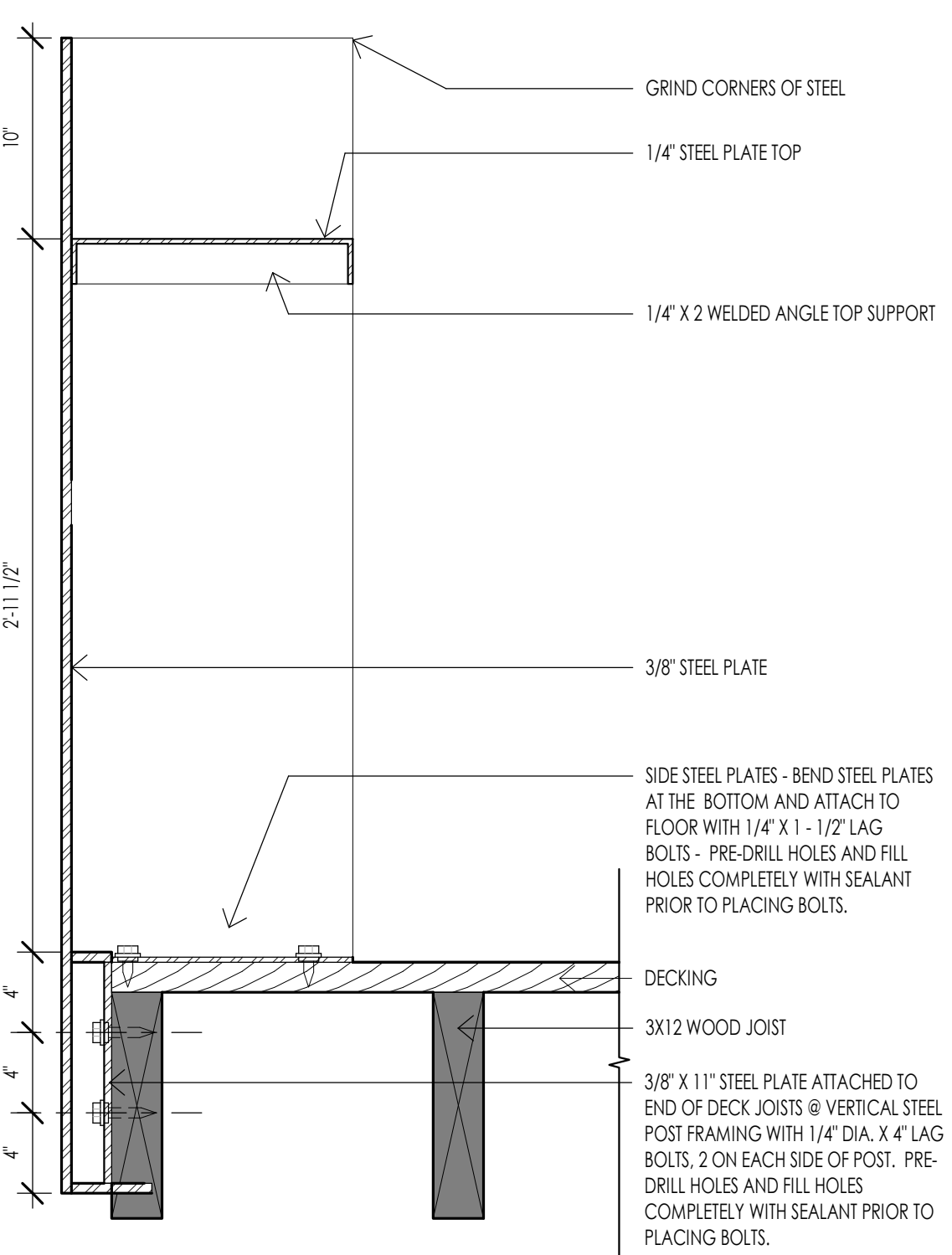
MASONRY - TRANSITION STONE WALL TO SIDING
1 1/2" = 1'-0"
A501



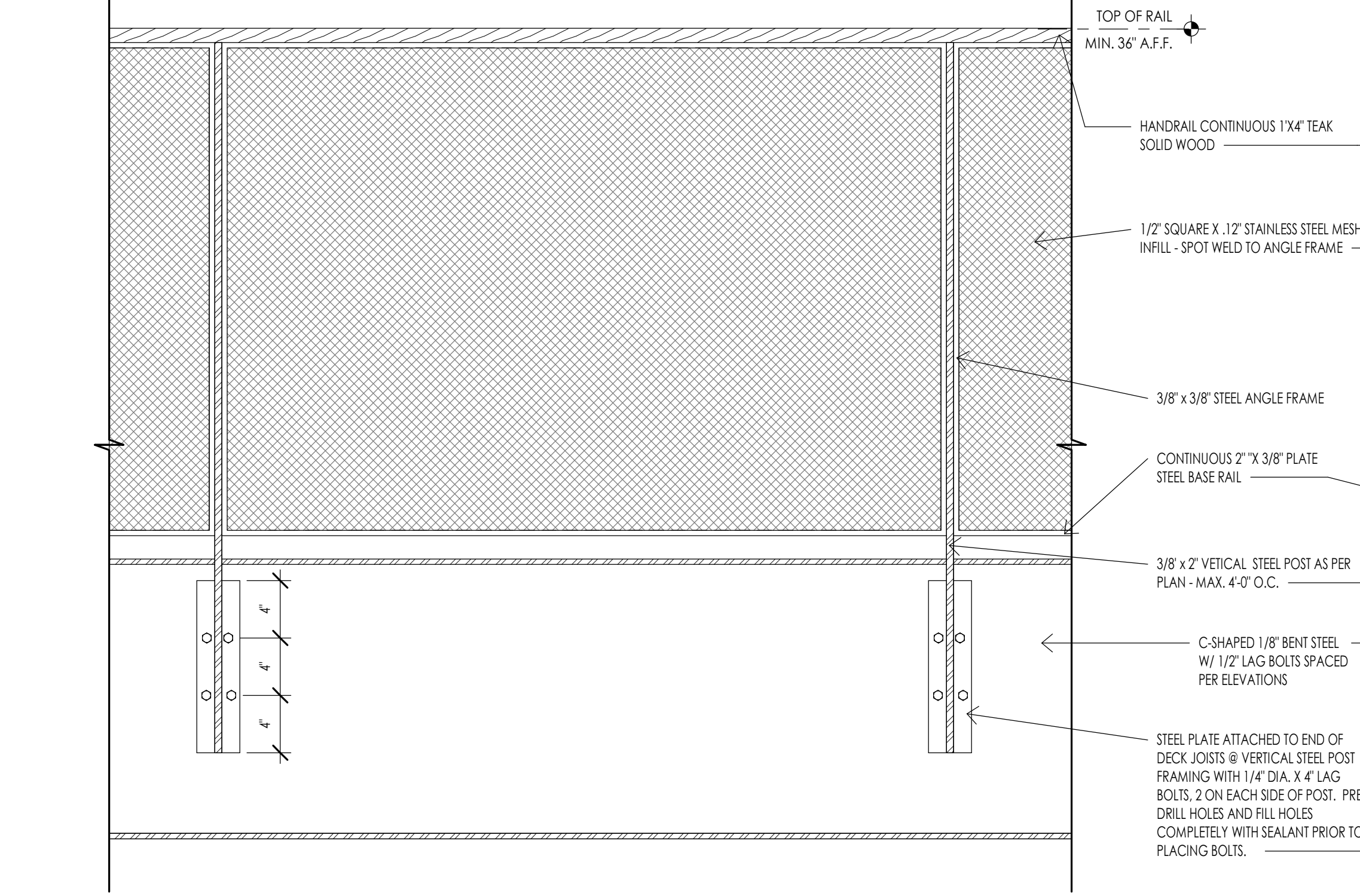
MASONRY - WOOD SIDING BASE FLASHING
6" = 1'-0"
A501



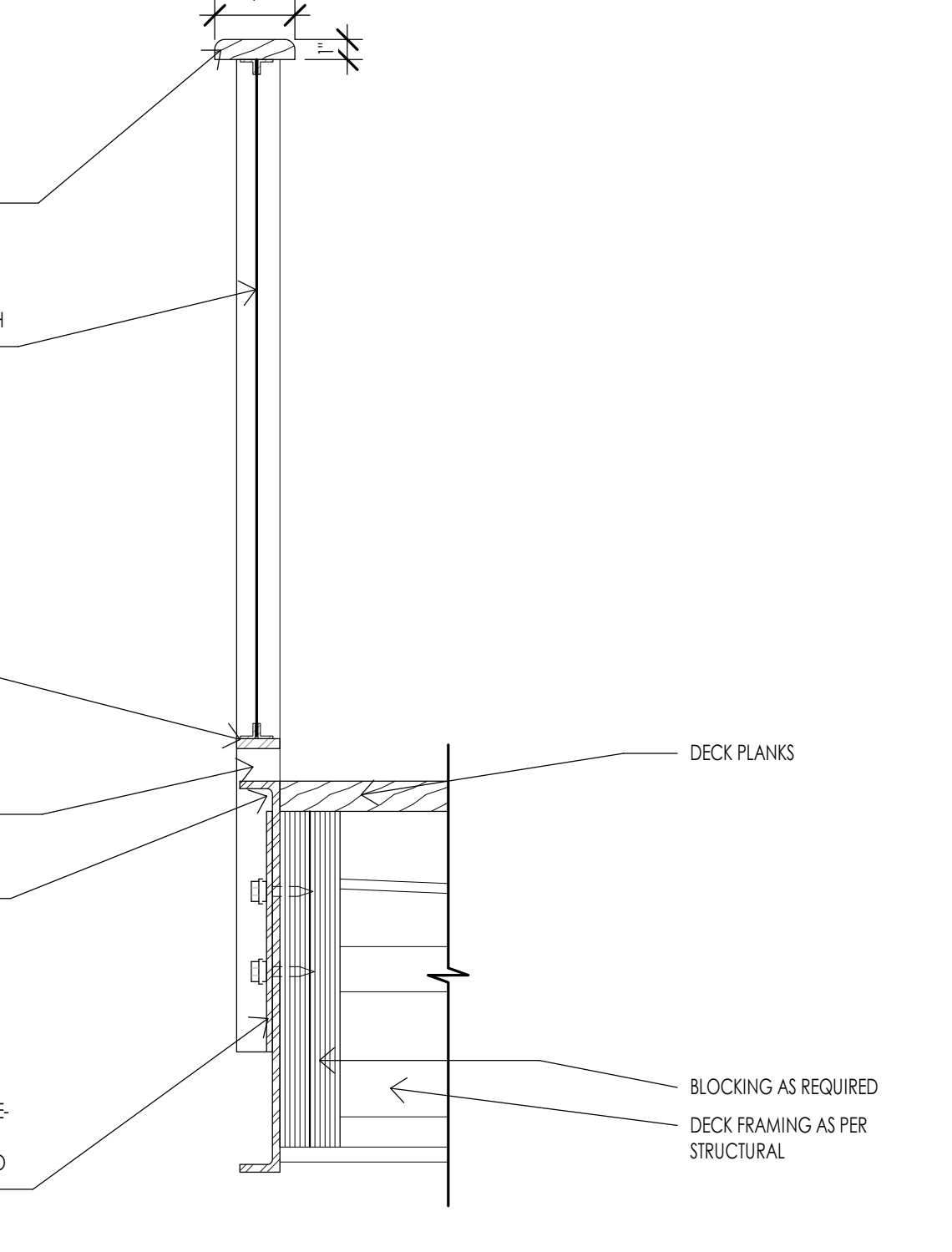
CONCRETE - FOUNDATION AT PAVERS
3/4" = 1'-0"
A501



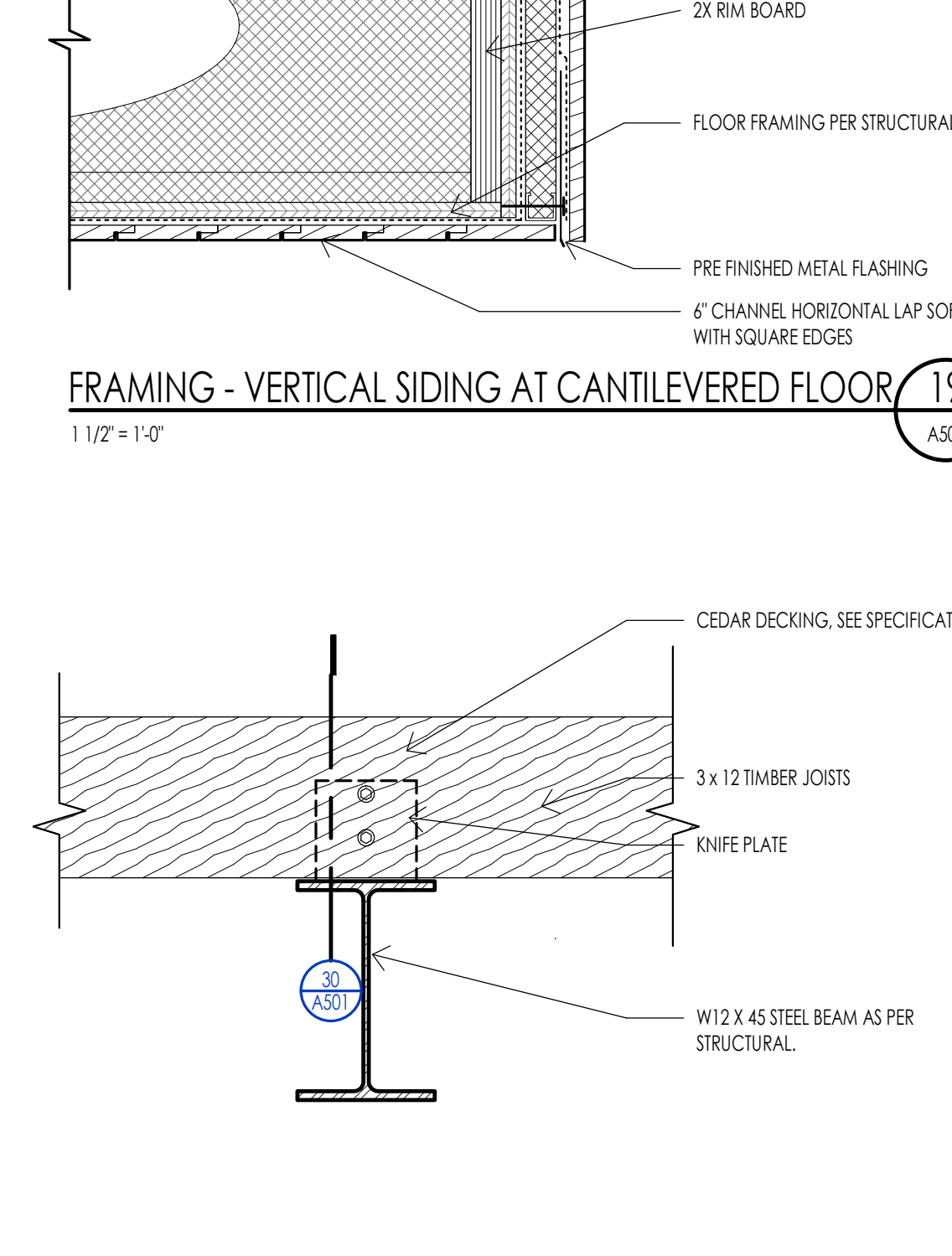
DECK - BBQ SECTION DETAIL
1 1/2" = 1'-0"
A501



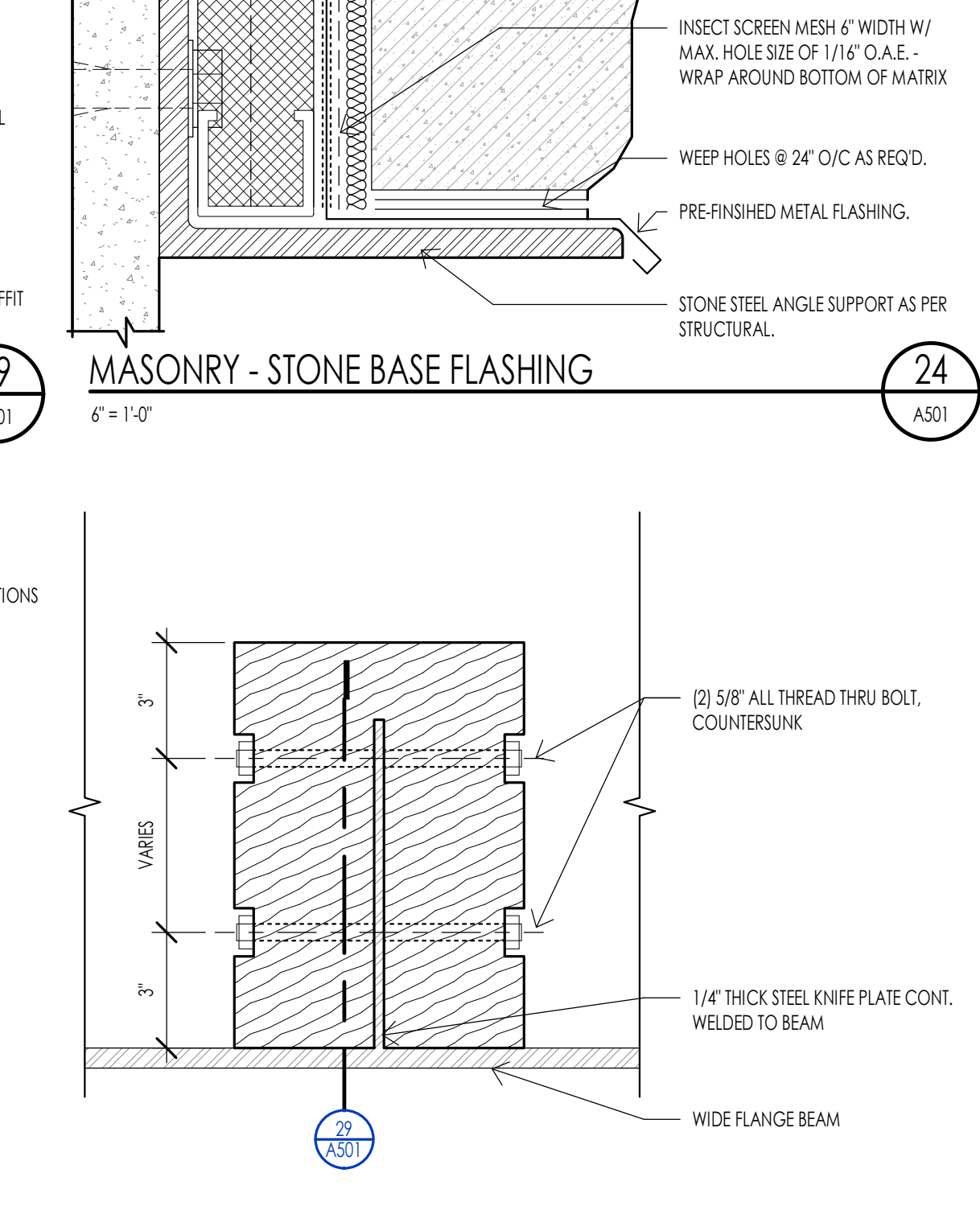
DECK - GUARDRAIL DETAIL
1 1/2" = 1'-0"
A501



DECK - STEEL BEAM TO JOIST DETAIL
6" = 1'-0"
A501



DECK - TYPICAL TIMBER CONNECTION
3" = 1'-0"
A501



MASONRY - STONE BASE FLASHING
6" = 1'-0"
A501

WARM SPRINGS RESIDENCE #35

190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

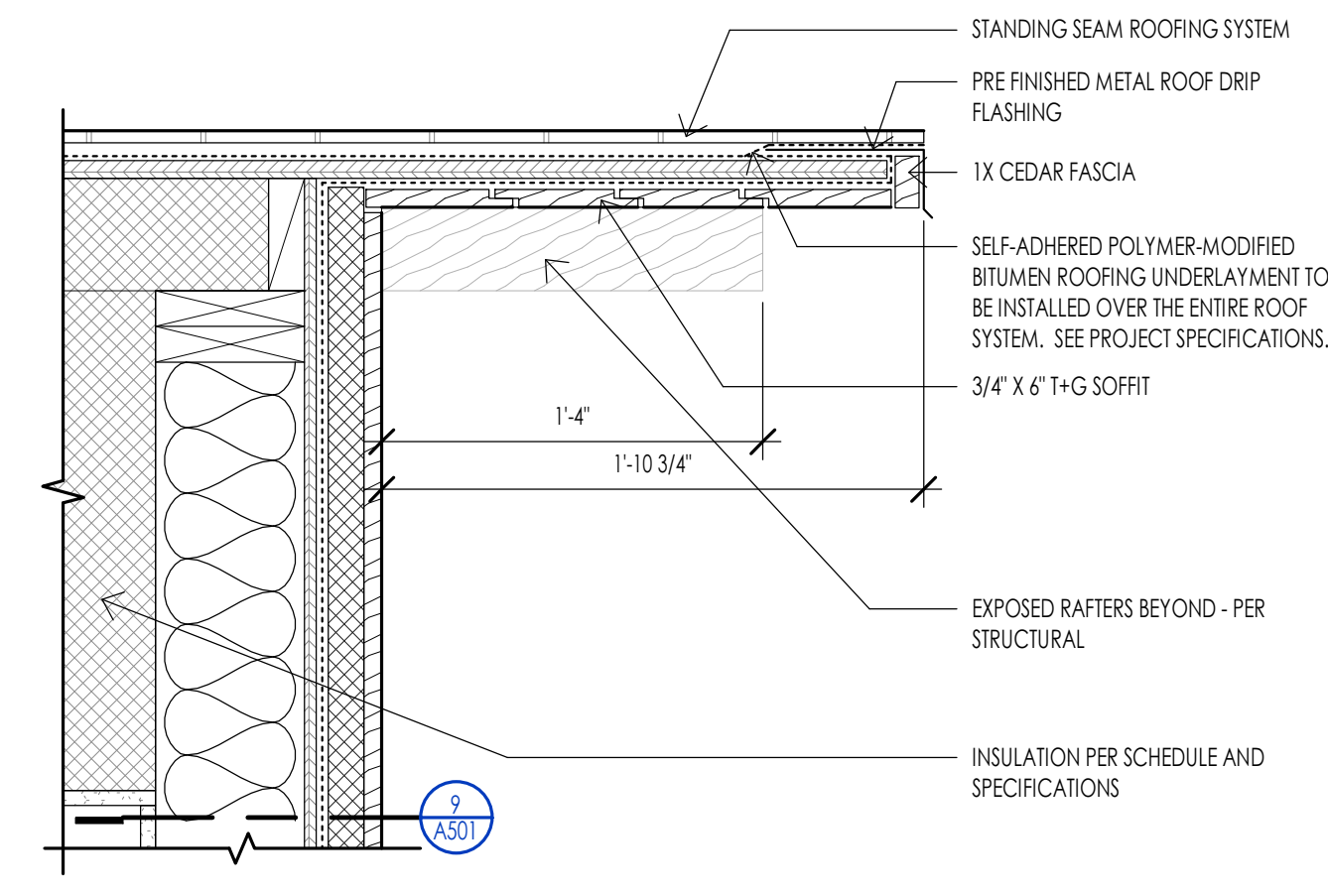
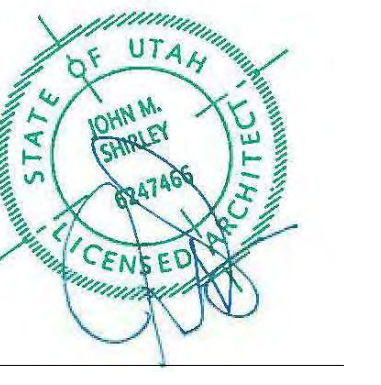
PROJECT NO. 22023
DATE: 2023.06.30
REVISIONS:

SHEET TITLE:
**ARCHITECTURAL
DETAILS**

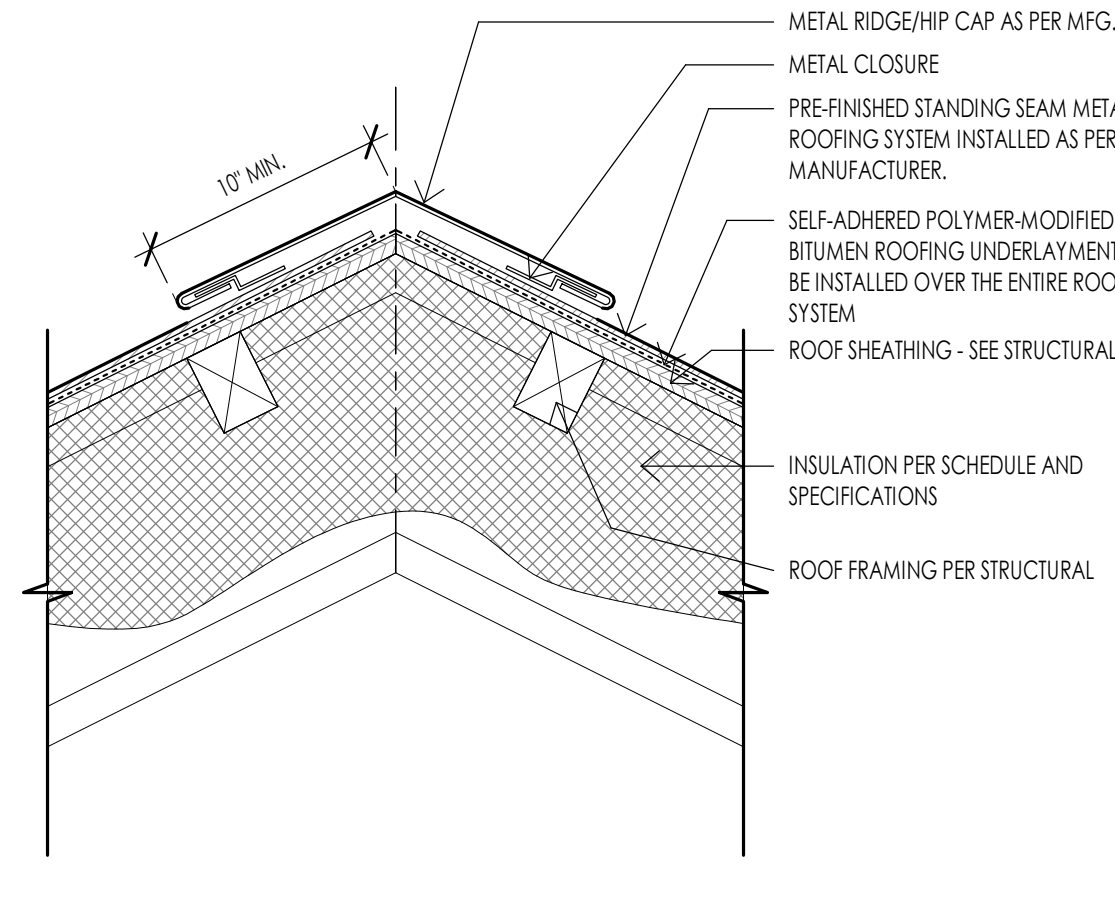
SHEET NUMBER:
A501

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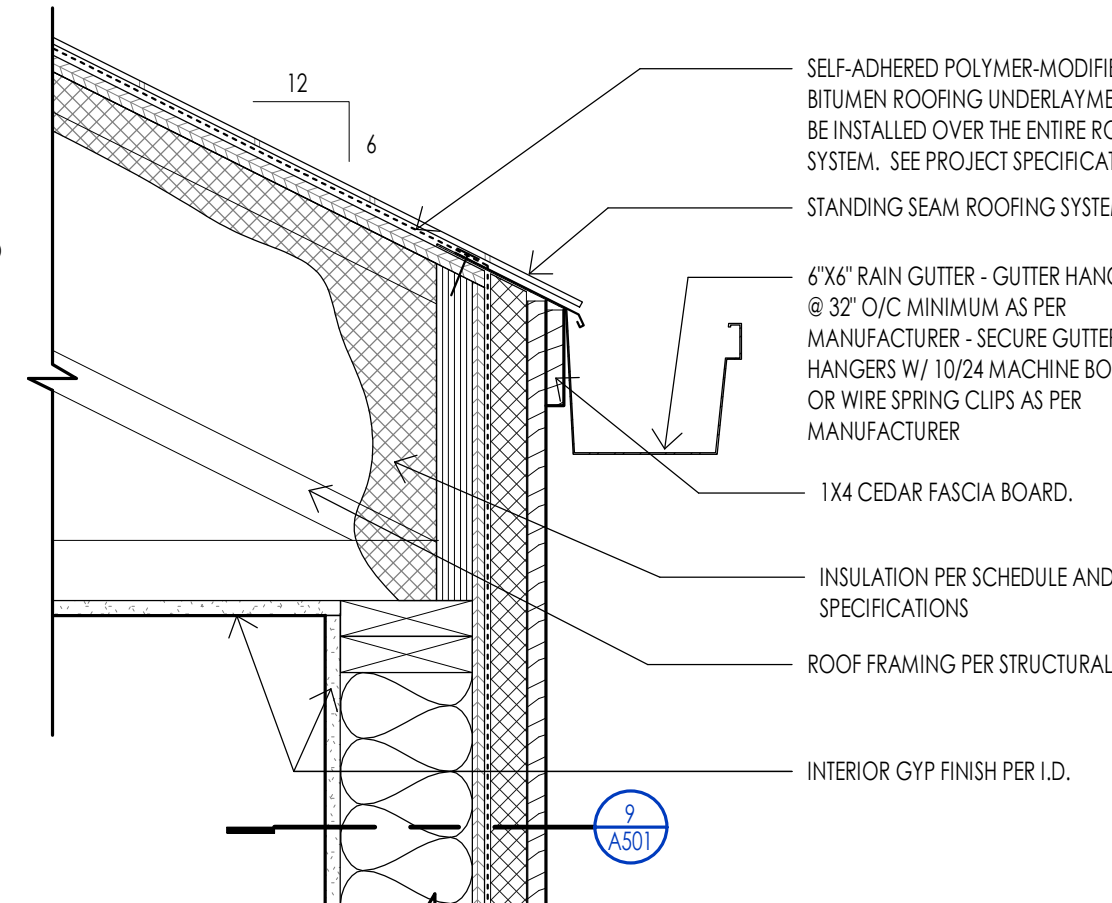
PERMIT SET



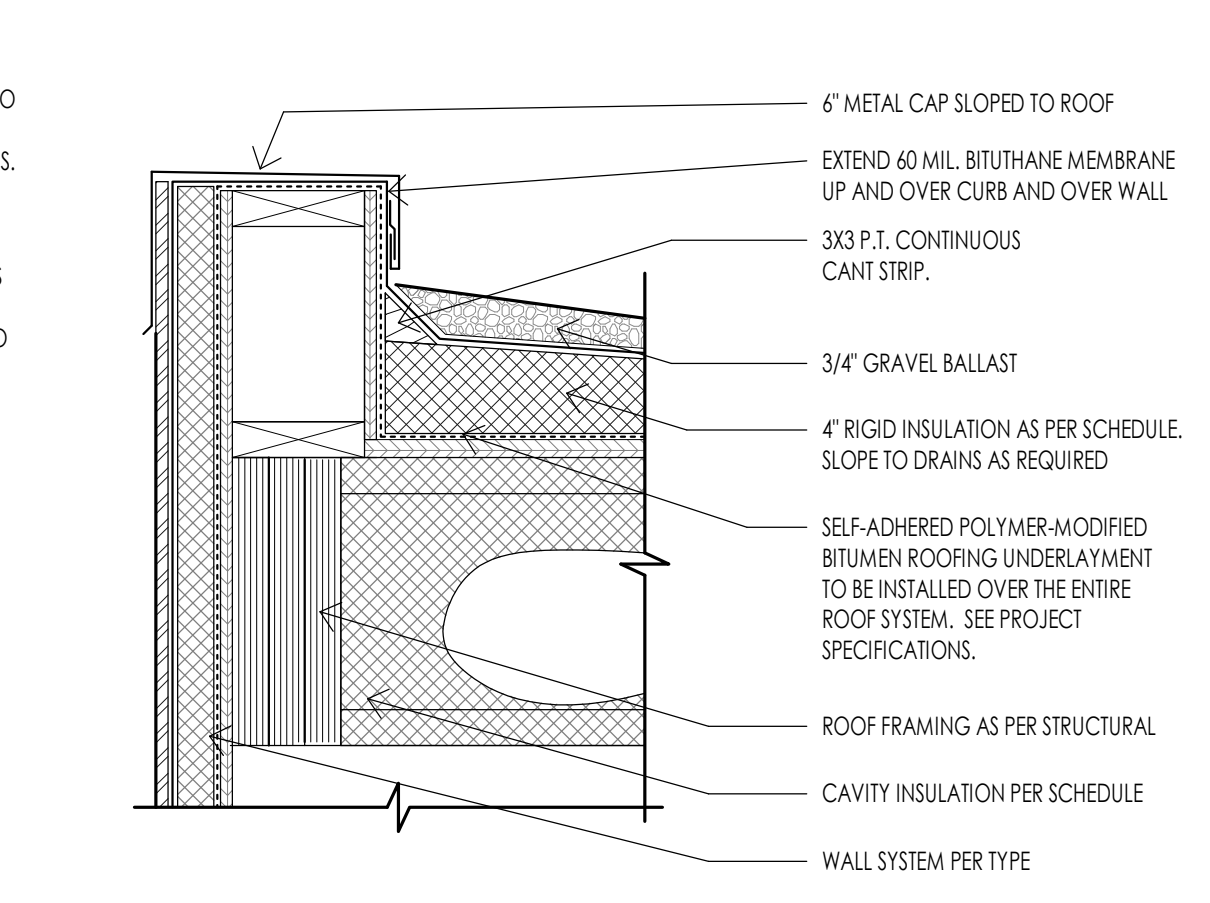
ROOF - RAKE DETAIL
1 1/2" = 1'-0"



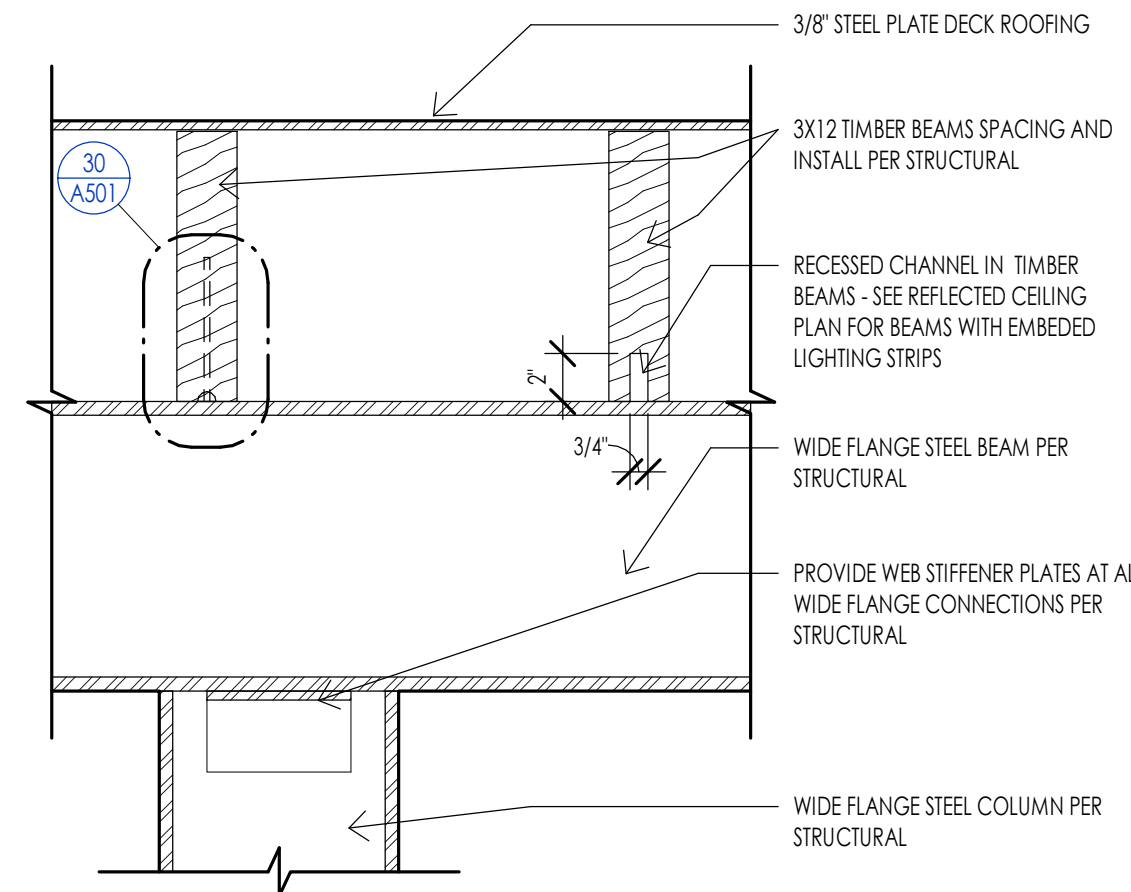
ROOF - STANDING SEAM RIDGE
1 1/2" = 1'-0"



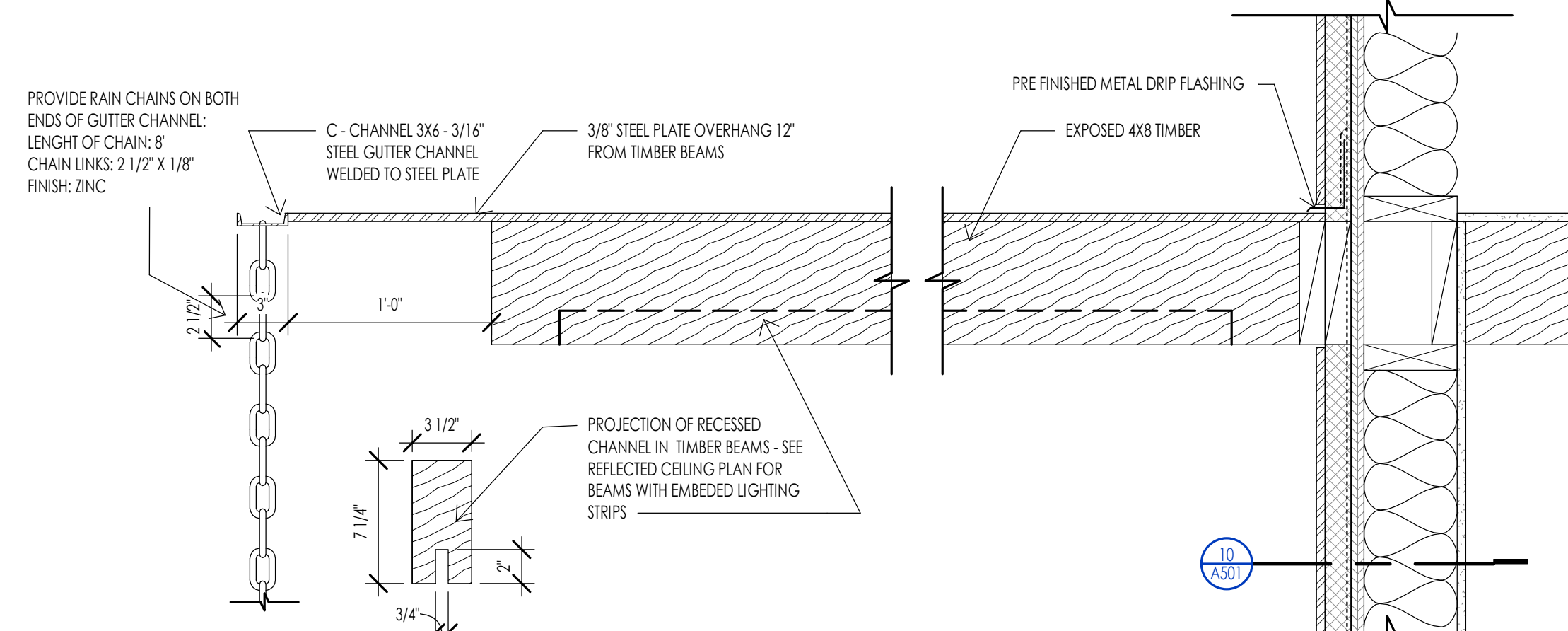
ROOF - 6/12 EAVE DETAIL AT SIDING
1 1/2" = 1'-0"



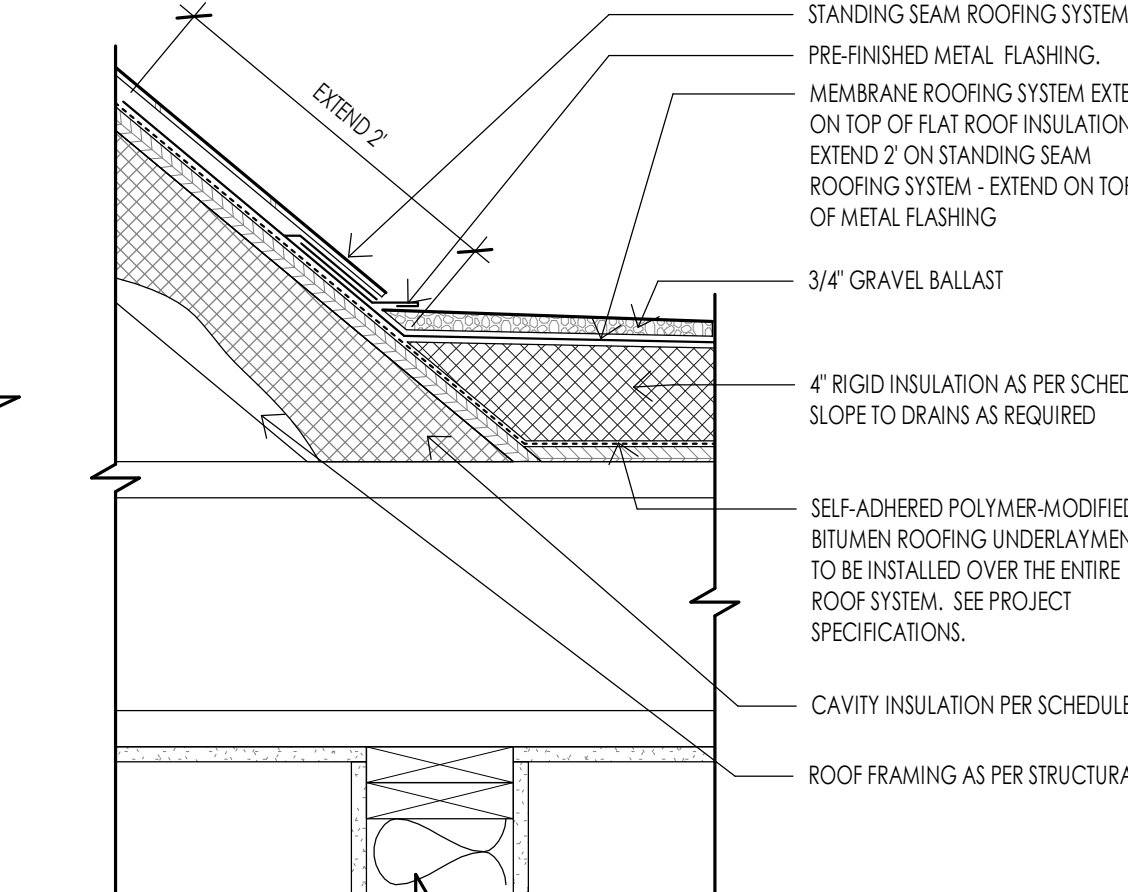
ROOF - METAL SIDING PARAPET TO ROOF
1 1/2" = 1'-0"



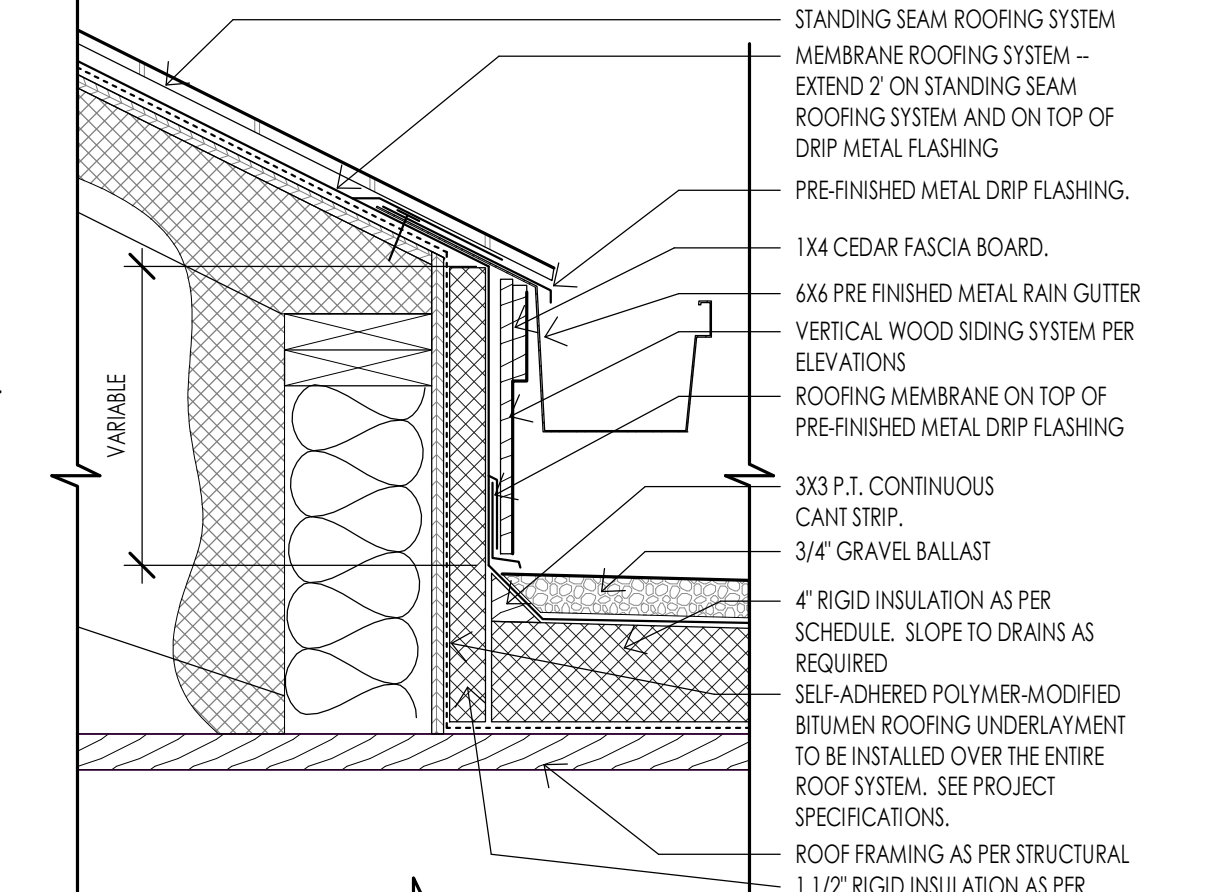
ROOF - DECK AWNING DETAIL
1 1/2" = 1'-0"



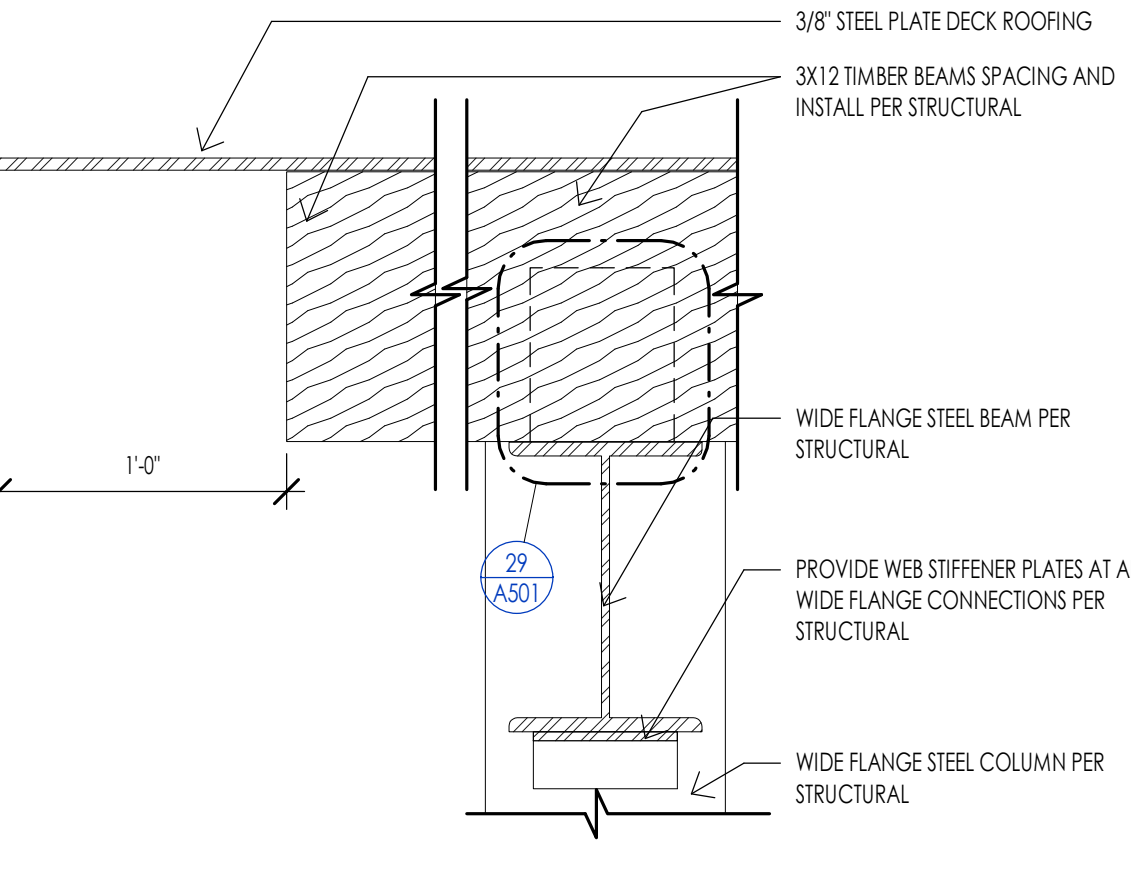
ROOF - ENTRY CANOPY DETAIL
1 1/2" = 1'-0"



ROOF - STANDING SEAM TRANSITION TO FLAT ROOF
1 1/2" = 1'-0"



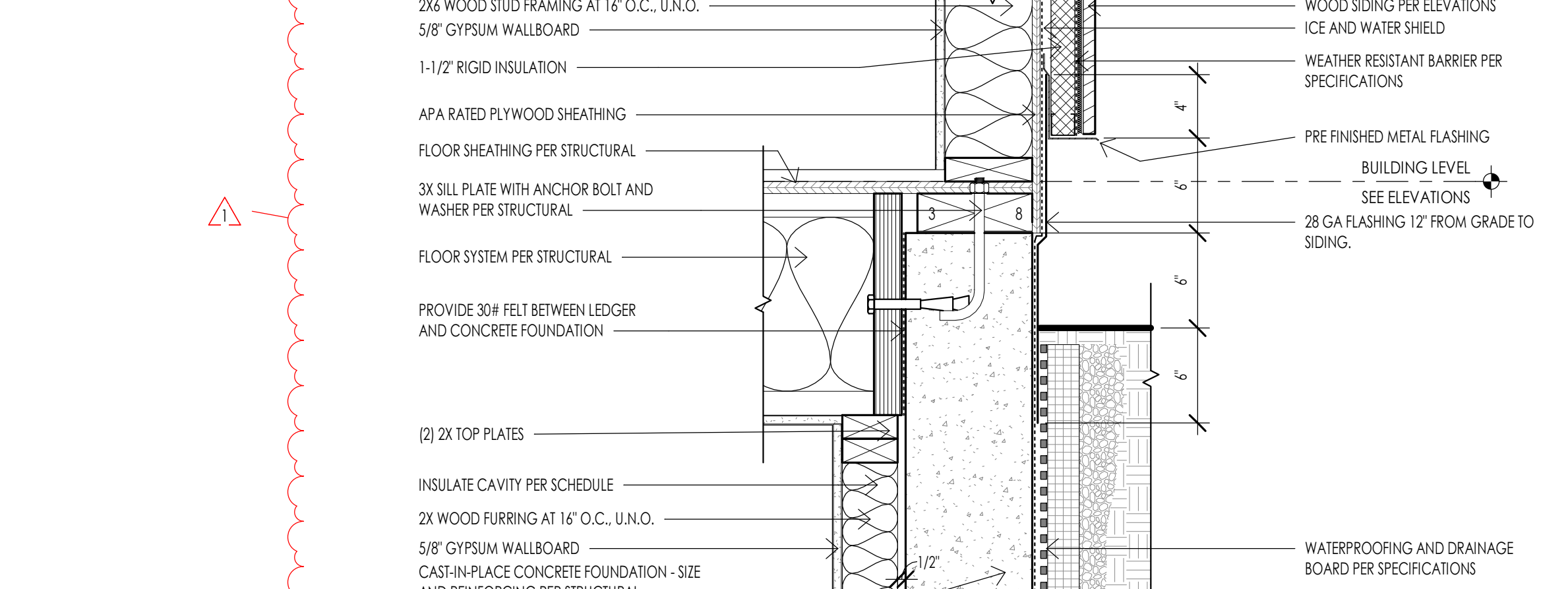
ROOF - STANDING SEAM TRANSITION
1 1/2" = 1'-0"



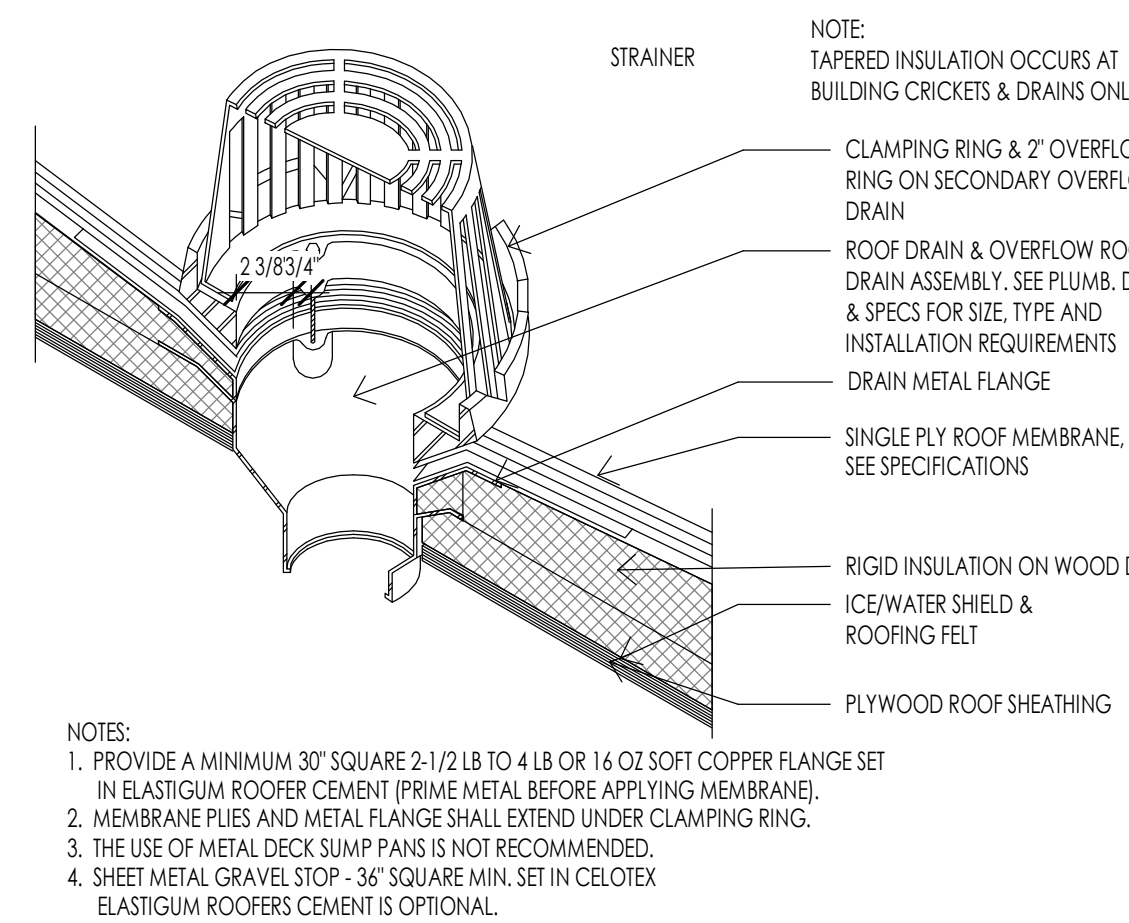
ROOF - DECK AWNING SECTION
1 1/2" = 1'-0"



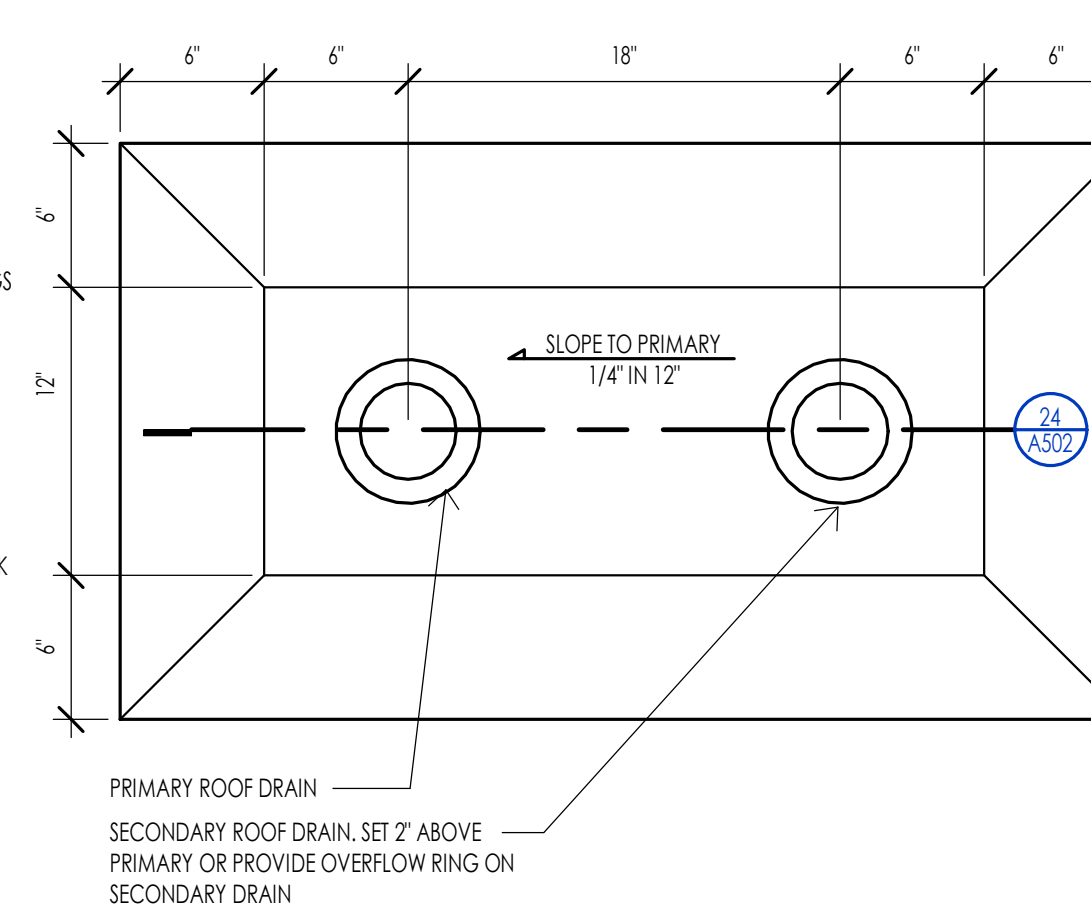
ROOF - ENLARGED DRAIN DETAIL
1 1/2" = 1'-0"



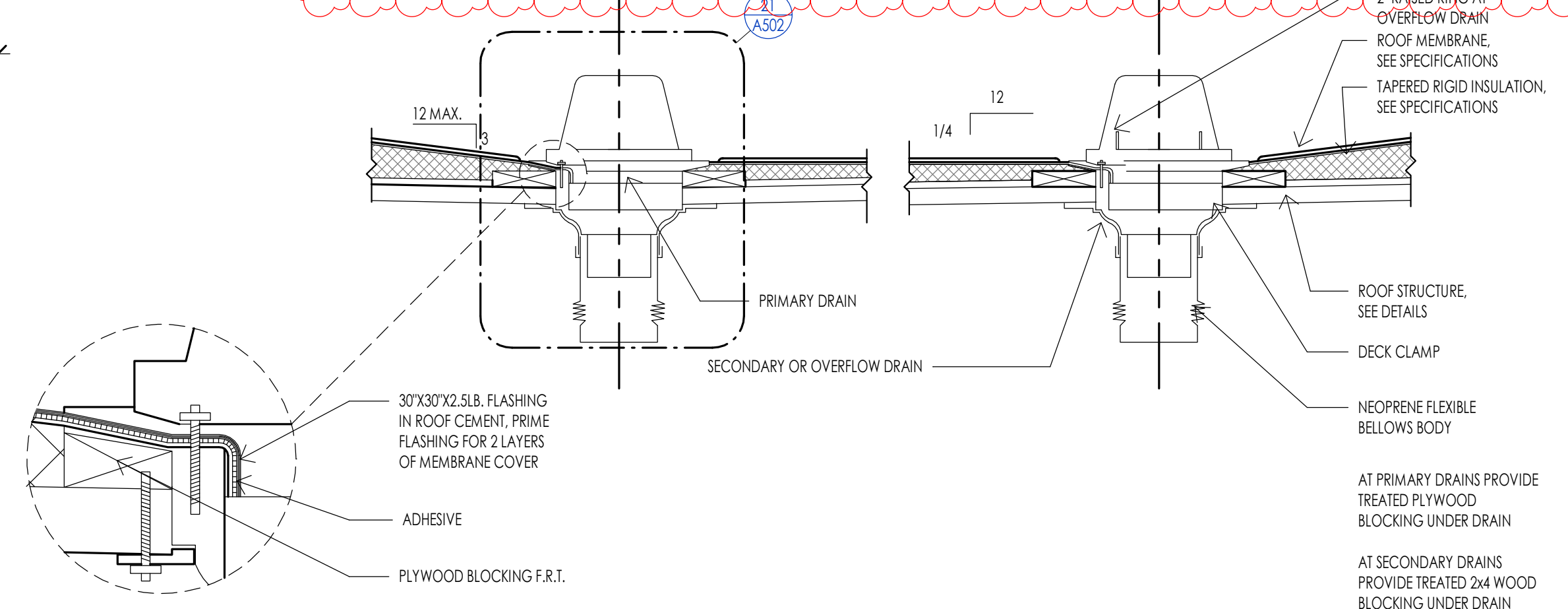
FRAMING - FLASHING DETAIL - SIDING AT GRADE
1 1/2" = 1'-0"



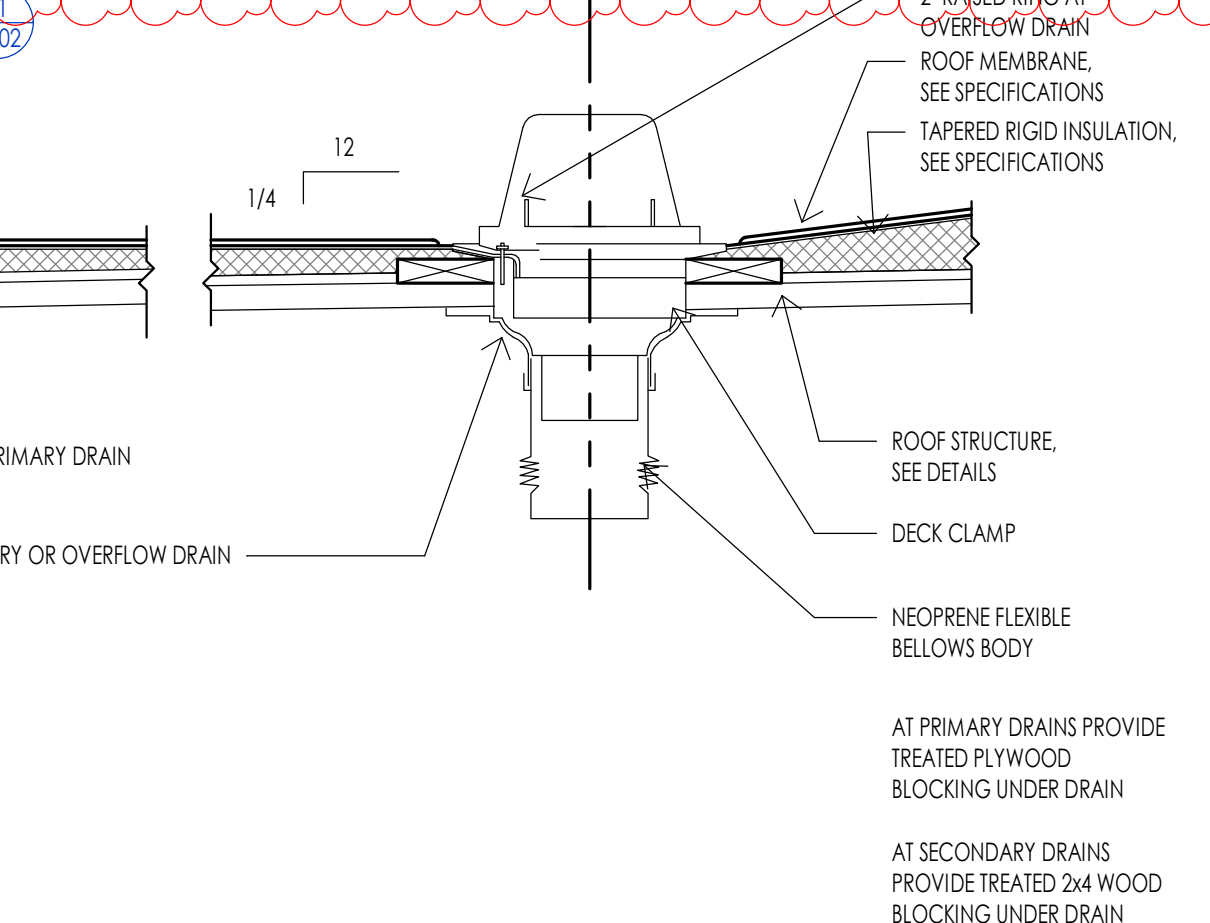
ROOF - DRAIN DETAIL
1 1/2" = 1'-0"



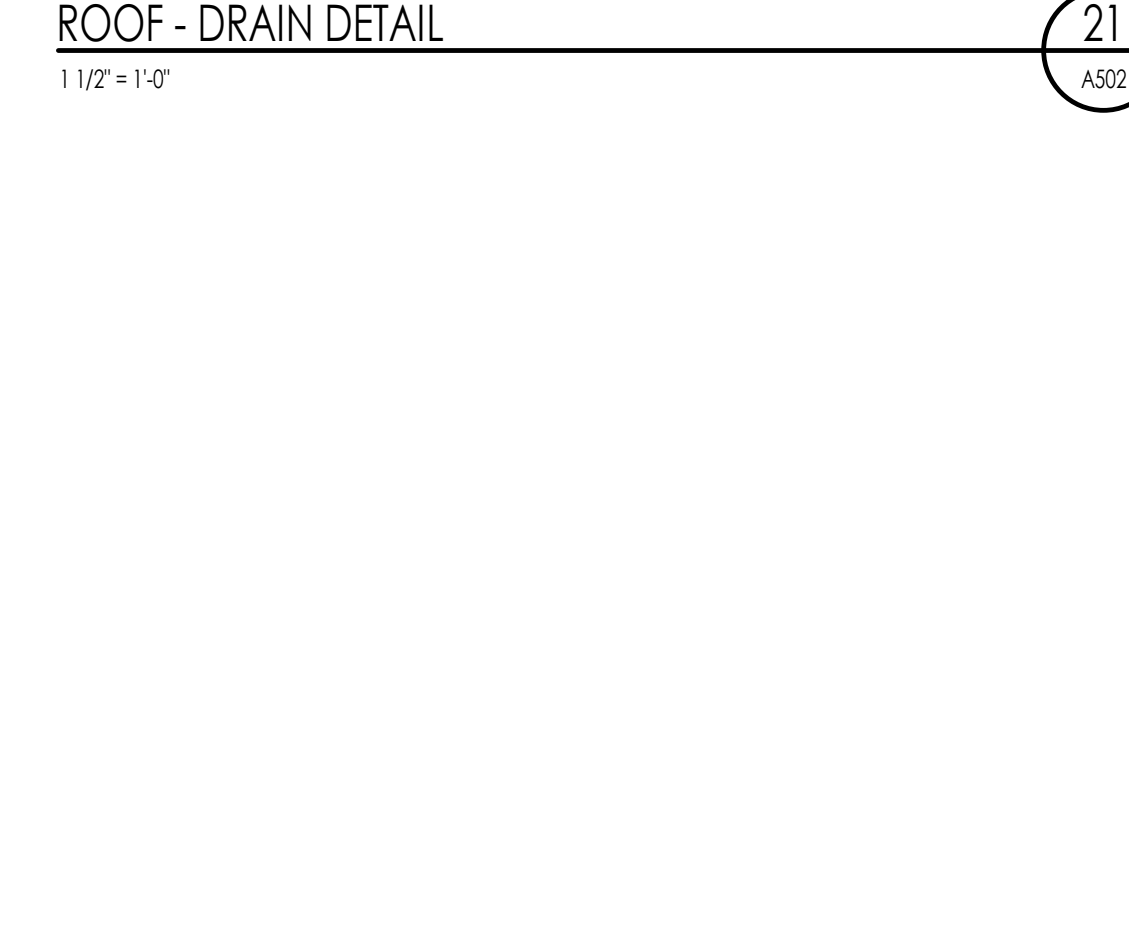
ROOF - DRAIN SECTION
1" = 1'-0"



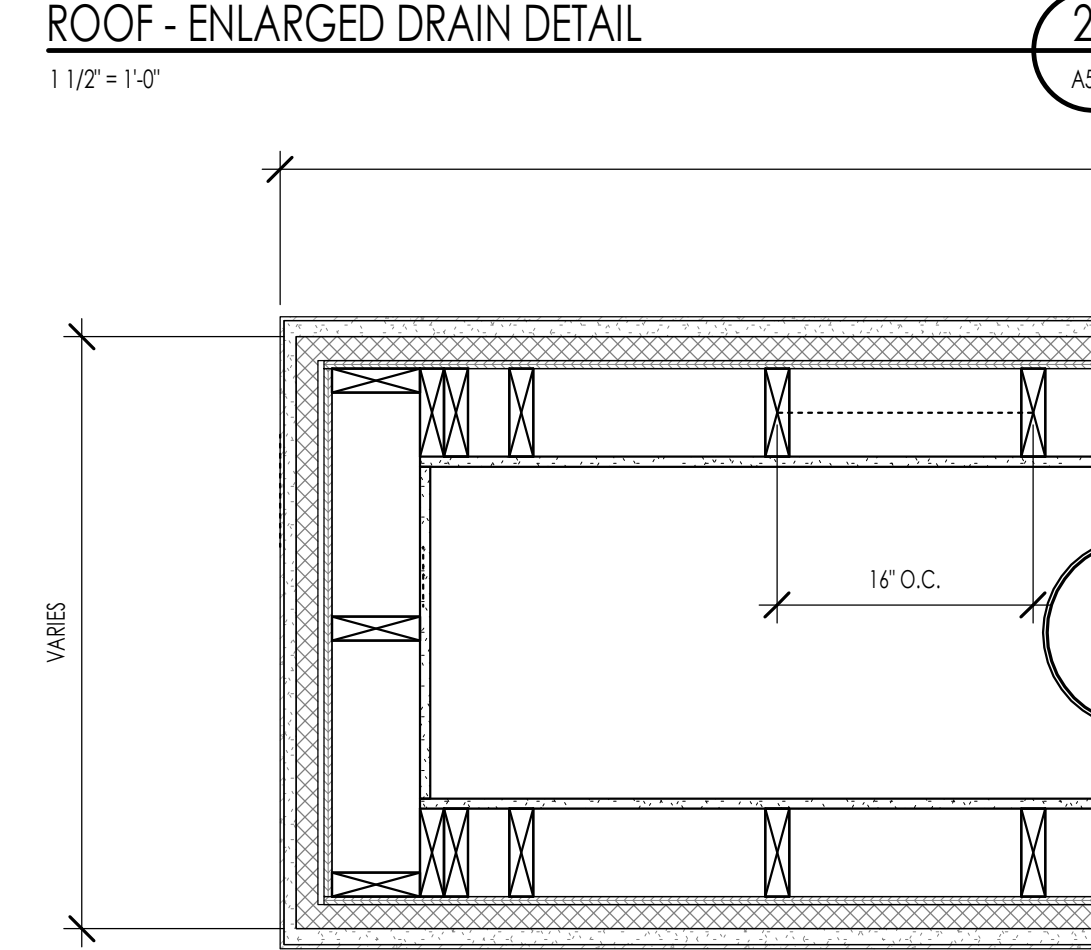
ROOF - CHIMNEY TYPICAL FRAMING
1" = 1'-0"



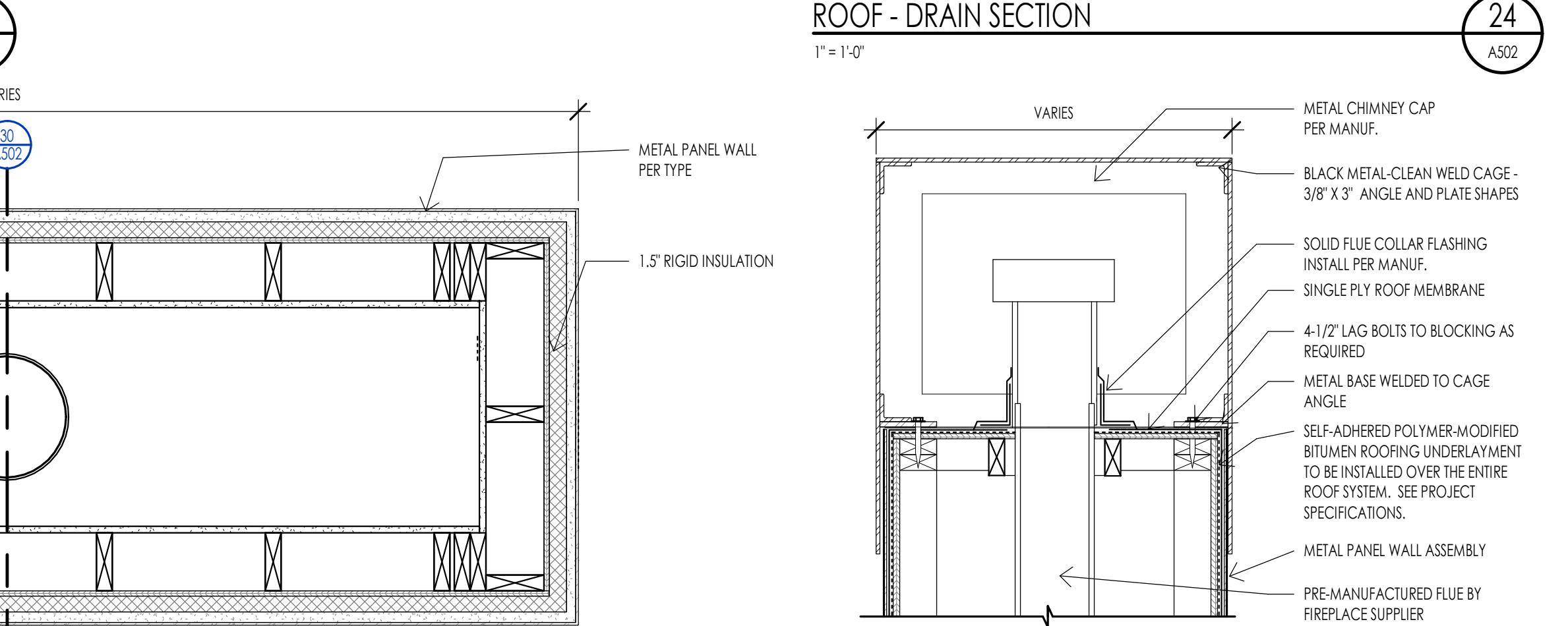
ROOF - OPEN SIDE CHIMNEY CAP DETAIL
1" = 1'-0"



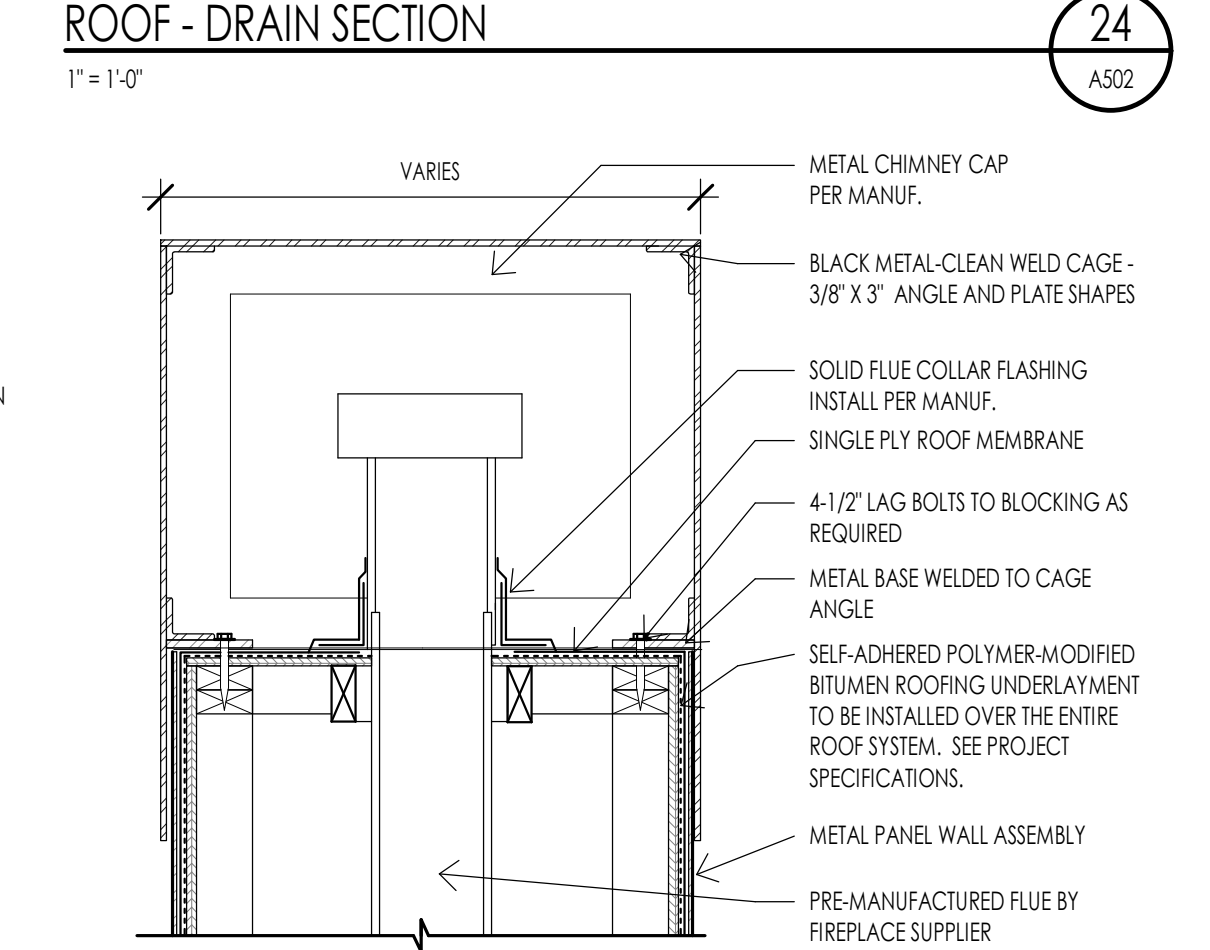
ROOF - DRAIN DETAIL
1 1/2" = 1'-0"



ROOF - ENLARGED DRAIN DETAIL
1 1/2" = 1'-0"



ROOF - CHIMNEY TYPICAL FRAMING
1" = 1'-0"



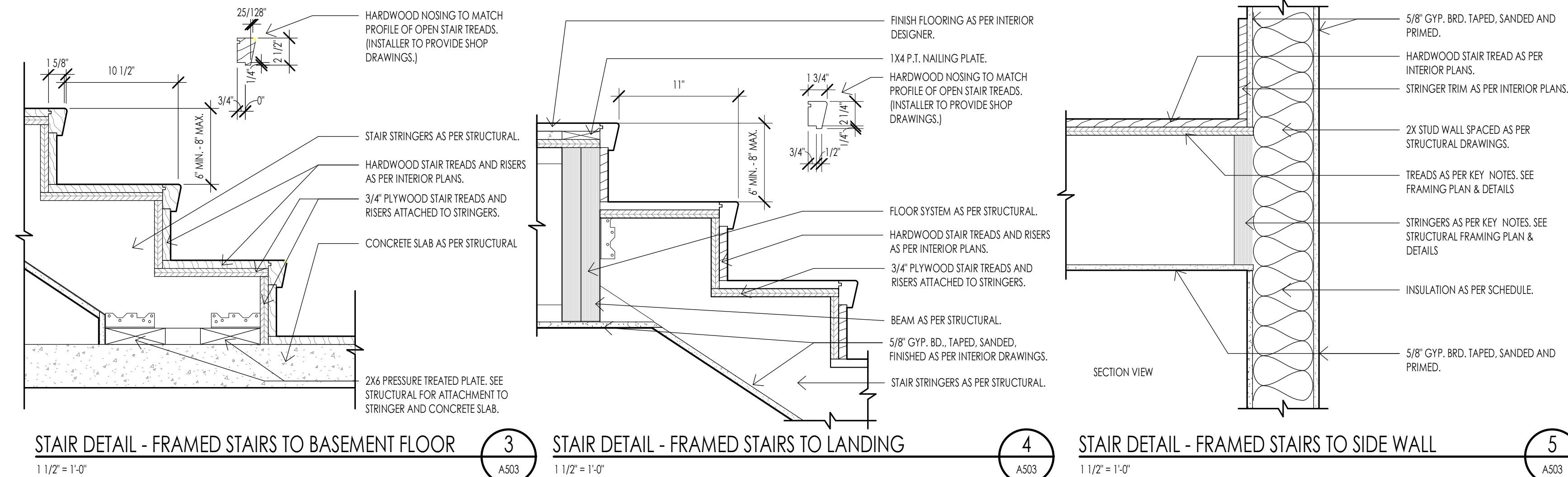
ROOF - OPEN SIDE CHIMNEY CAP DETAIL
1" = 1'-0"



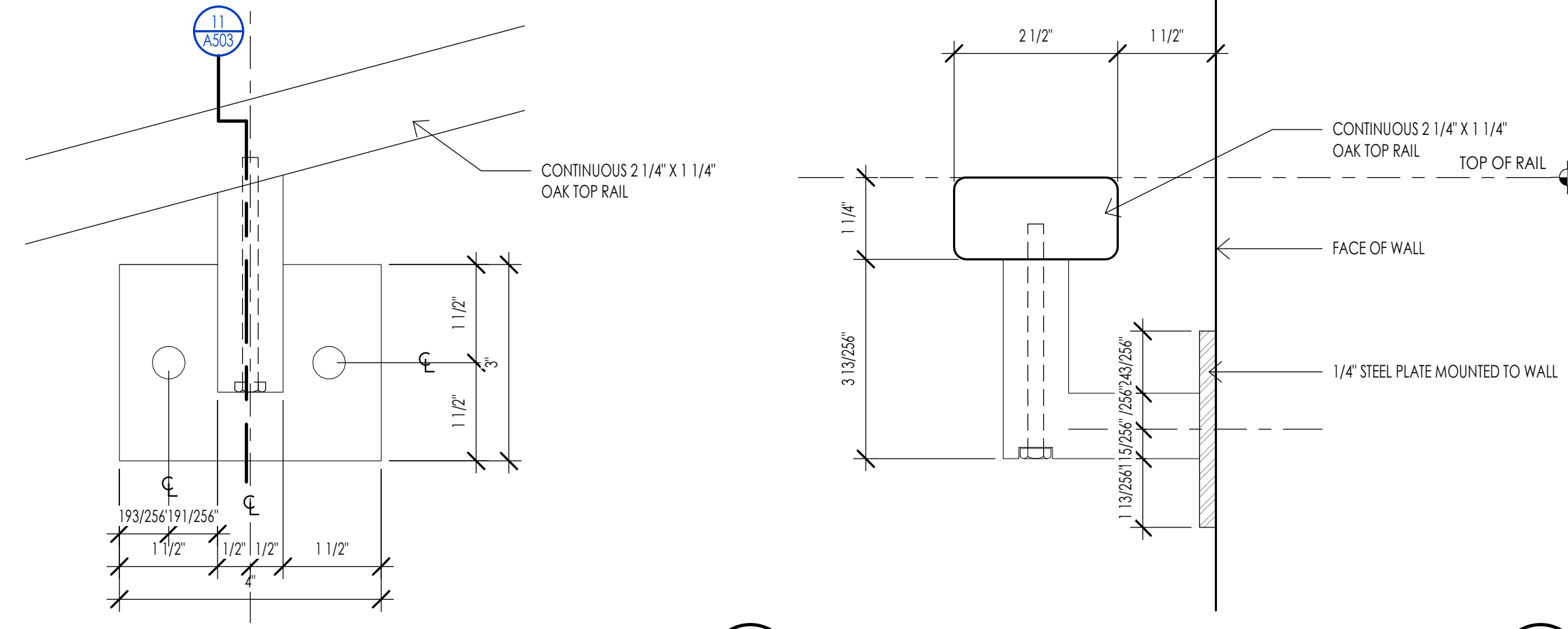
STAIR PLAN GENERAL NOTES

- STAIR CONSTRUCTION SHALL MEET THE FOLLOWING REQUIREMENTS. SEE I.R.C. SECTION R311.5.
 - THE MINIMUM STAIRWAY WIDTH SHALL NOT BE LESS THAN 36 INCHES CLEAR WIDTH. HANDRAILS MAY PROJECT INTO THE REQUIRED WIDTH A DISTANCE OF 4 1/2 INCHES FROM EACH SIDE OF A STAIRWAY. (I.R.C. R311.7.1) FOR ADDITIONAL WIDTH REQUIREMENTS OR FOR SPIRAL, CURVED, WINDING STAIRS, ETC. REQUIREMENTS SEE I.R.C. SECTION R311.7.10.1
 - THE MAXIMUM STAIR RISER HEIGHT SHALL NOT EXCEED 7 3/4 INCHES AND THE MINIMUM STAIR TREAD DEPTH SHALL BE 10 INCHES. THE TREAD DEPTH SHALL BE MEASURED HORIZONTALLY BETWEEN THE VERTICAL PLANES OF THE FOREMOST PROJECTION OF ADJACENT TREADS. THE GREATEST RISER HEIGHT OR TREAD DEPTH SHALL NOT EXCEED THE SMALLEST BY MORE THAN 3/8 INCH. (UTAH STATE AMENDMENT (REPLACES R311.7.5 AND ALLOWS FOR 8" MAX RISERS AND 9" MIN. TREADS))
 - LANDINGS: EVERY LANDING SHALL HAVE A WIDTH DIMENSION OF NOT LESS THAN THE STAIRWAY SERVED. EVERY LANDING SHALL HAVE A MINIMUM DEPTH DIMENSION OF 36 INCHES MEASURED IN THE DIRECTION OF TRAVEL. FOR LANDINGS WITH ADJOINING DOORS SEE I.R.C. SECTION R311.7.6 FOR SOME EXCEPTIONS/OPTIONS.
 - ENCLOSED ACCESSIBLE SPACE UNDER STAIRS SHALL HAVE WALLS, UNDER STAIR SURFACE AND ANY SOFFITS PROTECTED ON THE ENCLOSED SIDE WITH 1/2 INCH GYPSUM BOARD. (I.R.C. R302.7)
 - HEADROOM: EVERY STAIRWAY SHALL HAVE A MINIMUM HEADROOM CLEARANCE IN ALL PARTS OF THE STAIR OF NOT LESS THAN 6 FEET 8 INCHES. SUCH CLEARANCES SHALL BE MEASURED VERTICALLY FROM THE SLOPED PLANE ADJOINING THE TREAD NOSING OR FROM THE FLOOR SURFACE OF THE LANDING. (I.R.C. R311.7.2)
- HEADROOM: EVERY STAIRWAY SHALL HAVE A MINIMUM HEADROOM CLEARANCE IN ALL PARTS OF THE STAIR OF NOT LESS THAN 6 FEET 8 INCHES. SUCH CLEARANCES SHALL BE MEASURED VERTICALLY FROM THE SLOPED PLANE ADJOINING THE TREAD NOSING OR FROM THE FLOOR SURFACE OF THE LANDING. (I.R.C. R311.7.2)
 - HANDRAILS SHALL BE MOUNTED A MINIMUM OF 34 INCHES AND A MAXIMUM OF 38 INCHES ABOVE THE NOSING OF THE TREAD AND SHALL BE PROVIDED ON AT LEAST ONE SIDE OF STAIRWAYS. ALL REQUIRED HANDRAILS SHALL BE CONTINUOUS THE FULL LENGTH OF THE STAIRS WITH FOUR OR MORE RISERS FROM A POINT DIRECTLY ABOVE THE TOP RISER OF THE FLIGHT TO A POINT DIRECTLY ABOVE THE LOWEST RISER. BARS SHALL BE RETURNED OR SHALL TERMINATE IN NEVEL FLOODS, VOLUTES, TURNOUT OR STARTING EASING SHALL BE ALLOWED OVER THE LOWEST TREAD.
 - THE HAND GRIP PORTION OF HANDRAILS SHALL HAVE A CIRCULAR CROSS SECTION OF 1 1/4 INCHES MINIMUM TO 2 5/8 INCHES MAXIMUM. OTHER HANDRAIL SHAPES THAT HAVE AN EQUIVALENT GRASPING SURFACE ARE PERMISSIBLE. SEE BUILDING CODE. EDGES SHALL HAVE A MINIMUM RADIUS OF 0.031 INCH. R311.7.8.3.
 - HANDRAILS ADJACENT TO A WALL SHALL HAVE A SPACE OF NOT LESS THAN 1 1/2 INCHES BETWEEN THE WALL AND THE HANDRAIL.
- GUARD RAILS SHALL MEET THE FOLLOWING REQUIREMENTS. SEE I.R.C. SECTION R312.
 - GUARDRAILS ARE REQUIRED AT ALL PORCHES, BALCONIES OR RAISED FLOOR SURFACES LOCATED MORE THAN 30 INCHES ABOVE THE FLOOR OR GRADE BELOW AND SHALL BE NOT LESS THAN 36 INCHES IN HEIGHT. OPEN SIDES OF STAIRS WITH A TOTAL RISE OF MORE THAN 30 INCHES ABOVE THE FLOOR OR GRADE BELOW SHALL HAVE GUARDS NOT LESS THAN 34 INCHES IN HEIGHT MEASURED VERTICALLY FROM THE NOSING OF THE TREAD.
 - REQUIRED GUARDS ON OPEN SIDES OF STAIRWAYS, RAISED FLOOR AREAS, BALCONIES, ETC. SHALL HAVE INTERMEDIATE RAILS OR ORNAMENTAL CLOSURES THAT DO NOT ALLOW PASSAGE OF A SPHERE 4 INCHES IN DIAMETER.
 - THE TRIANGLE OPENINGS FORMED BY THE RISER, TREAD AND BOTTOM RAIL OF A GUARD AT THE OPEN SIDE OF A STAIRWAY ARE PERMITTED TO BE OF SUCH A SIZE THAT A SPHERE 6 INCHES IN DIAMETER CANNOT PASS THROUGH.
 - THE GUARDS AT SIDES OF STAIRS SHALL HAVE GUARD SUCH THAT A 4-3/8" SPHERE CANNOT PASS THROUGH IT.

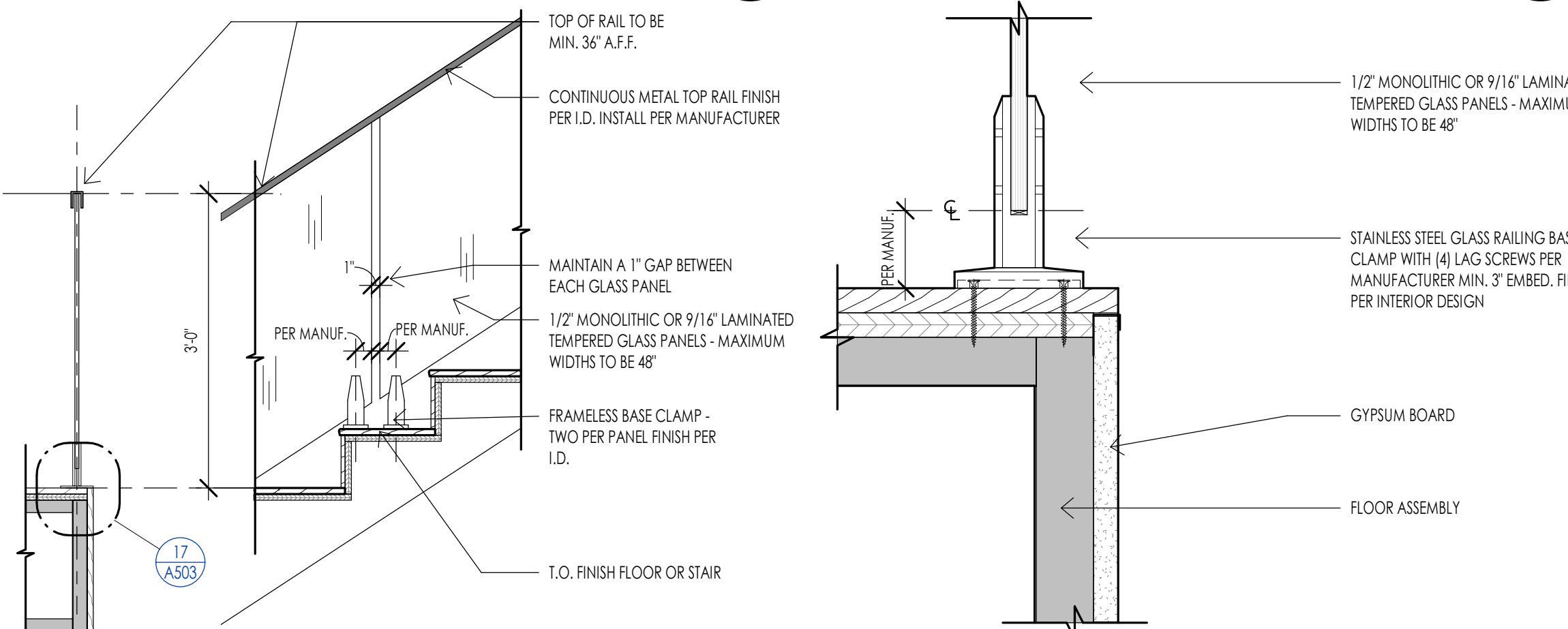
STAIR PLAN KEYNOTES



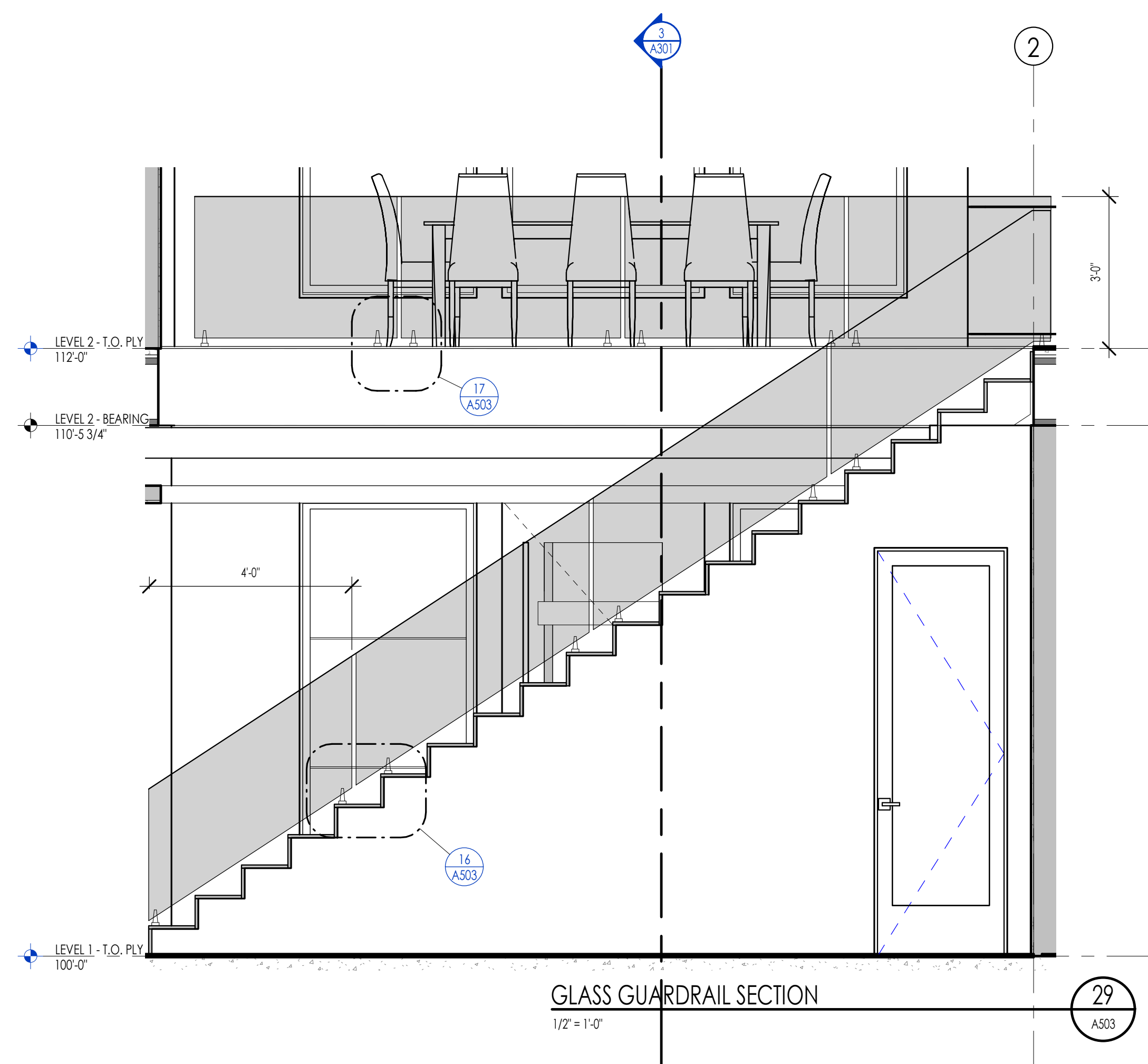
3 STAIR DETAIL - FRAMED STAIRS TO BASEMENT FLOOR 1 1/2" = 1'-0" A503
4 STAIR DETAIL - FRAMED STAIRS TO LANDING 1 1/2" = 1'-0" A503
5 STAIR DETAIL - FRAMED STAIRS TO SIDE WALL 1 1/2" = 1'-0" A503



10 HANDRAIL DETAIL AT WALL 6" = 1'-0" A503
11 HANDRAIL SECTION AT WALL 6" = 1'-0" A503



16 GLASS GUARDRAIL AT STAIR 3/4" = 1'-0" A503
17 GLASS GUARDRAIL - FLOOR CONNECTION 3" = 1'-0" A503



29 GLASS GUARDRAIL SECTION 1/2" = 1'-0" A503

DOOR SCHEDULE

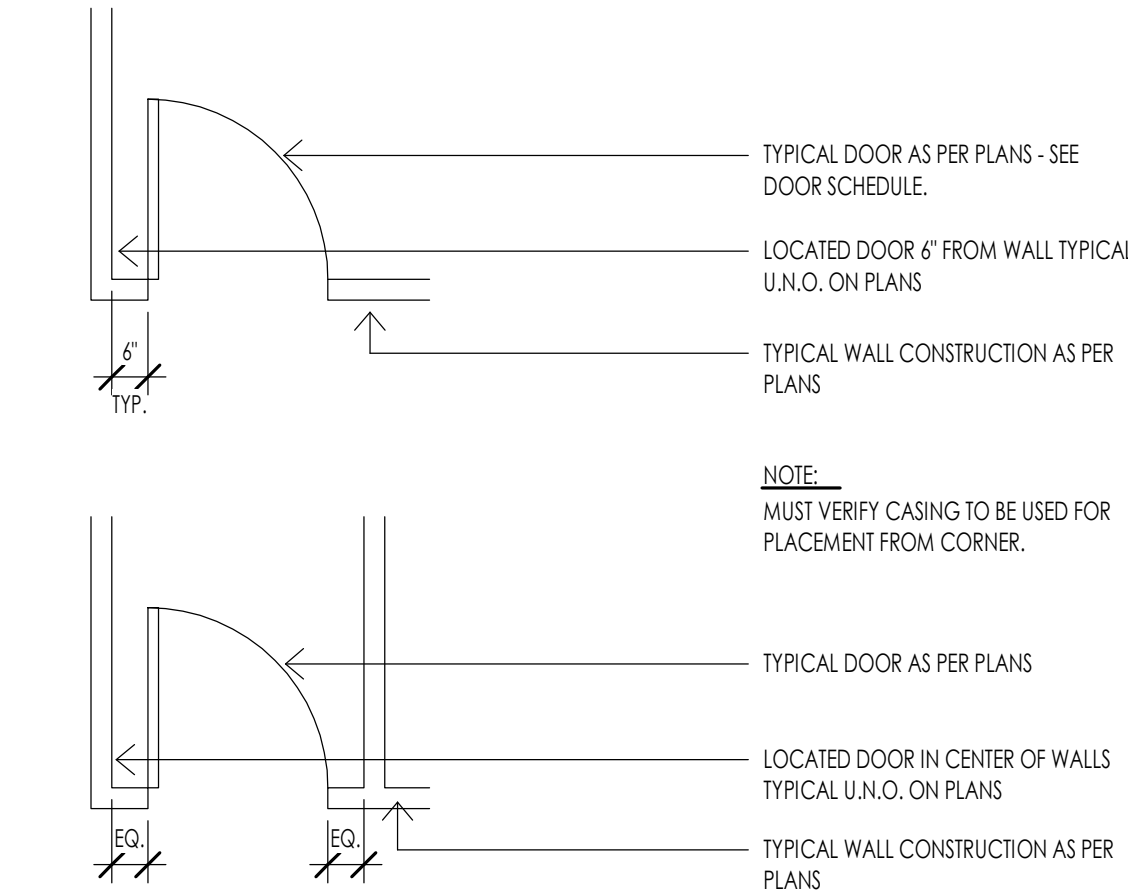
MARK	DOOR					FRAME					FIRE RATING	HARDWARE	REMARKS	
	SIZE	MATERIAL	TYPE	FINISH	DETAILS	MATERIAL	TYPE	FINISH	FIRE RATING	HARDWARE				
														WIDTH
D103A	4'-0"	9'-0"	2"		D4	AS PER I.D.	22/A601	23/A601	-	F2	AS PER I.D.	NON-RATED	H10	
D101A	2'-10"	8'-0"	2"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H8	
D101B	3'-0"	8'-0"	2"		D3	AS PER I.D.	22/A601	23/A601	24/A601	METAL	F3	AS PER I.D.	20 MIN	H4
D101C	2'-4"	8'-0"	1 3/4"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H5	
D102A	2'-10"	8'-0"	2"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H8	
D102B	2'-4"	8'-0"	2"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H5	
D103A	2'-4"	8'-0"	2"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H8	
D103B	2'-4"	8'-0"	2"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H8	
D103C	2'-0"	7'-0"	1/4"	GLASS	D4					FRAMELESS	F1	AS PER I.D.	NON-RATED	H8
D104A	2'-4"	8'-0"	2"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H8	
D104B	2'-4"	8'-0"	2"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H8	
D104C	2'-0"	7'-0"	1/4"	GLASS	D4					FRAMELESS	F1	AS PER I.D.	NON-RATED	H8
D105A	3'-0"	8'-0"	2"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H5	
D106A	28'-0"	9'-0"	2"	METAL	D7									GARAGE DOOR
D107A	3'-0"	8'-0"	2"		D1	AS PER I.D.	22/A601	23/A601	24/A601	F1	AS PER I.D.	NON-RATED	H1	
D202A	2'-4"	8'-0"	2 1/4"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H8	
D203A	2'-10"	8'-0"	2 1/4"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H8	
D203B	2'-10"	8'-0"	2 1/4"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H5	
D203C	4'-0"	8'-0"	1 3/4"		D5	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H4	
D203D	2'-4"	8'-0"	2"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H8	
D203E	2'-0"	7'-0"	1/4"	GLASS	D4					FRAMELESS	F1	AS PER I.D.	NON-RATED	H8
D204A	2'-10"	8'-0"	2 1/4"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H8	
D204B	6'-0"	8'-0"	2"		D2	AS PER I.D.	22/A601	23/A601	-	F2	AS PER I.D.	NON-RATED	H5	
D204C	2'-4"	8'-0"	2"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H8	
D204D	2'-4"	8'-0"	2"		D1	AS PER I.D.	22/A601	23/A601	-	F1	AS PER I.D.	NON-RATED	H8	
D204E	2'-0"	7'-0"	1/4"	GLASS	D4					FRAMELESS	F1	AS PER I.D.	NON-RATED	H8

DOOR SCHEDULE GENERAL NOTES

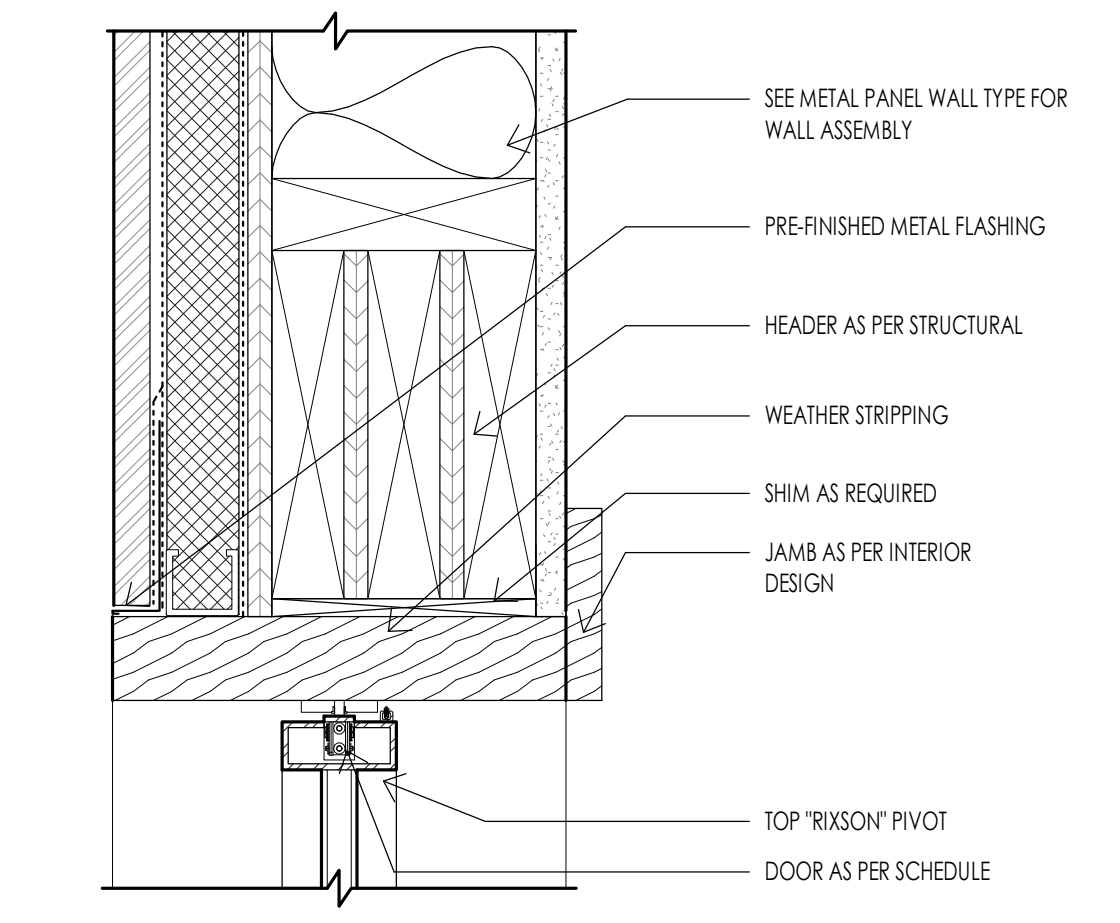
- SEE SHEET A601 FOR DOOR AND FRAME TYPES.
- CONTRACTOR SHALL FIELD VERIFY ALL DOOR OPENINGS PRIOR TO ORDERING ALL DOORS.
- CONTRACTOR SHALL SUBMIT COMPLETE DOOR AND HARDWARE SHOP DRAWINGS AND SUBMITTALS FOR APPROVAL FOR EACH BUILDING PRIOR TO ORDERING AND TAKING RECEIPT OF DOOR ORDER. ARCHITECT SHALL REVIEW ALL DOORS FOR COMPLIANCE SPECIFICATIONS AND BUILDING CODE.
- ALL DOORS REQUIRED TO BE RATED SHALL HAVE APPROPRIATE U.L. RATINGS AS INDICATED IN DOOR SCHEDULE AND SPECIFICATION. ALL DOORS SHALL HAVE LABEL ON DOOR AND FRAME FOR INSPECTION ON SITE, AND SHALL NOT BE REMOVED.
- ALL DOORS SHALL BE INSTALLED SO AS NOT TO HAVE MORE THAN 1/2" THRESHOLD AT EACH DOOR.
- SEE SPECS FOR HARDWARE SCHEDULE.
- REFER TO INTERIOR DESIGNER FOR CORRECT DOOR STYLES, SPECIES, AND FINISHES.
- OPENINGS BETWEEN GARAGE AND RESIDENCE SHALL BE EQUIPPED WITH SOLID WOOD DOOR NOT LESS THAN 1 3/8 INCHES THICK, SOLID HONEYCOMB CORE STEEL DOORS NOT LESS THAN 1 3/8 INCHES THICK, OR 20 MINUTE FIRE RATED DOORS. SEE I.R.C. SECTION R202.5.1.

HARDWARE GROUPS

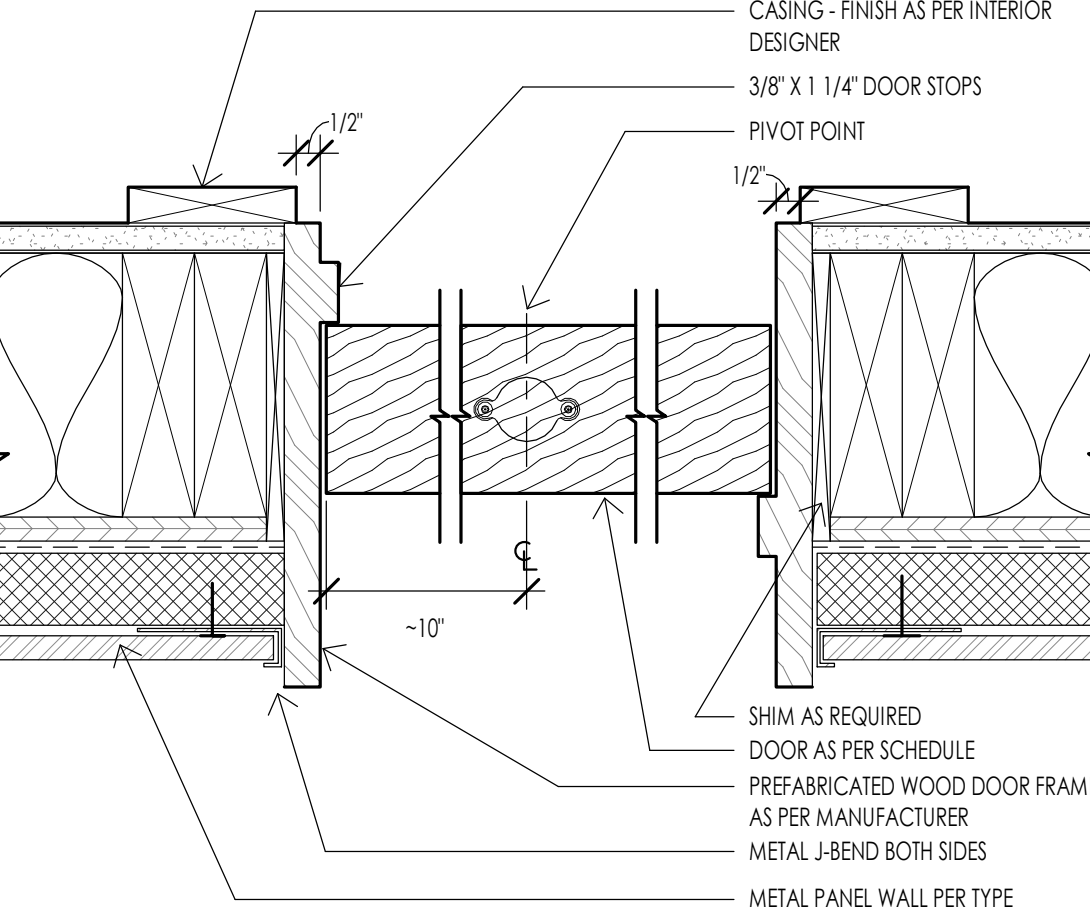
- H1 MECHANICAL ROOM**
 - 2 PAIR SPRING HINGES
 - 1 SMOKE SEAL
 - 1 PASSAGE SET
- H2 OVERHEAD GARAGE DOORS**
 - GARAGE ENTRY PROVIDED BY OVERHEAD DOOR MANUFACTURER
- H3 SECONDARY ENTRY DOOR**
 - 3 PAIR SPRING HINGES
 - 1 SMOKE SEAL
 - 1 LOCKSET
 - 1 DEADBOLT
 - 1 THRESHOLD
- H4 GARAGE HOUSE**
 - 3 PAIR HINGES
 - 1 WEATHER STRIP
 - 1 LOCKSET
 - 1 DEADBOLT
- H5 INTERIOR DOOR**
 - 3 PAIR HINGES
 - 1 PASSAGE SET
- H6 INTERIOR BARN DOOR**
- H7 INTERIOR DOUBLE BARN DOOR**
- H8 INTERIOR DOOR**
 - 3 PAIR HINGES
 - 1 PRIVACY SET
- H9 INTERIOR DOUBLE DOOR**
 - 6 PAIR HINGES
 - 2 KNOBS (SINGLE SIDE ONLY)
- H10 ENTRY DOOR**
 - 1 WEATHER STRIP
 - 1 THRESHOLD
 - 1 LOCKSET
 - 1 DEADBOLT
 - 1 PIVOT HINGE (TOP AND BOTTOM)
- H11 INTERIOR DOUBLE DOOR**
 - 6 PAIR HINGES
 - 2 PASSAGE SET
- H12 POCKET DOOR**



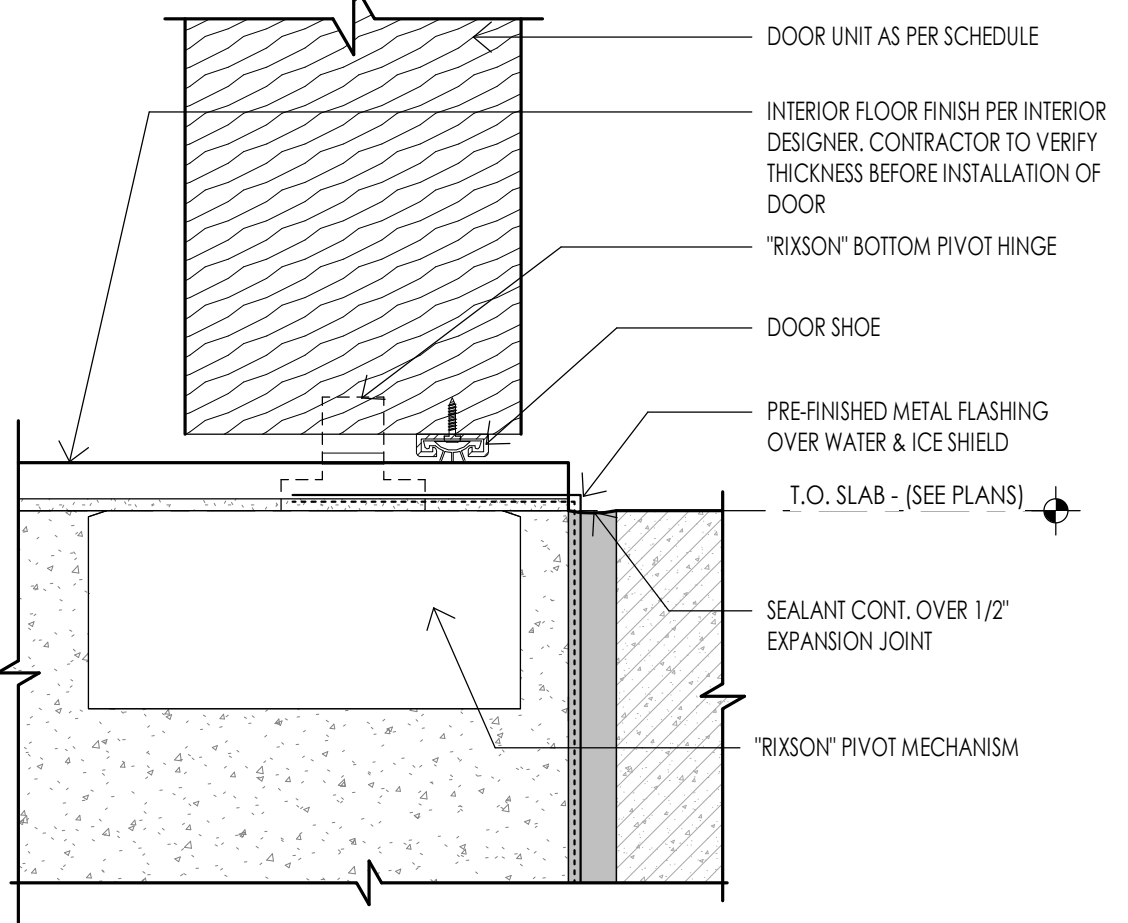
DOOR - TYPICAL PLACEMENT DETAIL
3/8" = 1'-0"



DOOR - PIVOT HEAD DETAIL
3" = 1'-0"

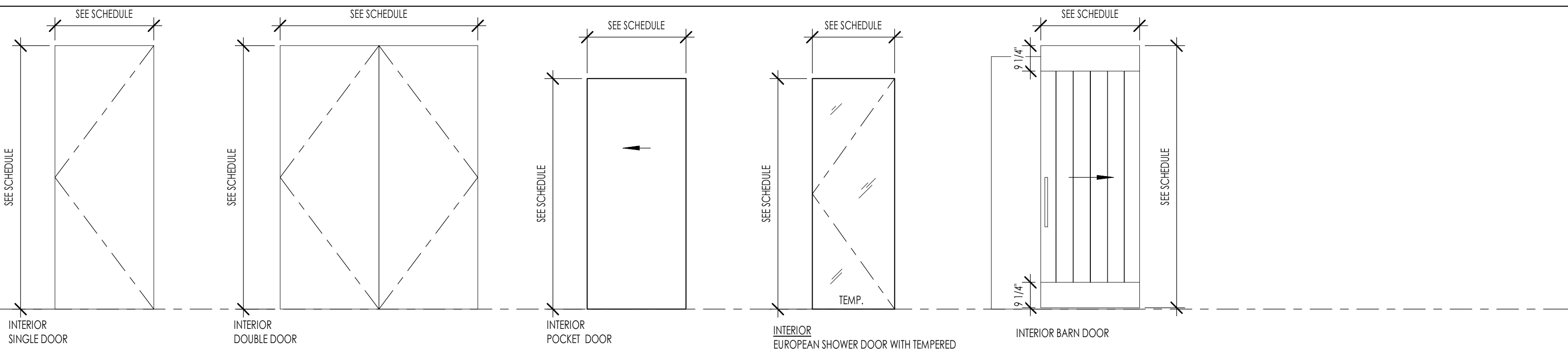


DOOR - PIVOT JAMB DETAIL WOOD DOOR
3" = 1'-0"

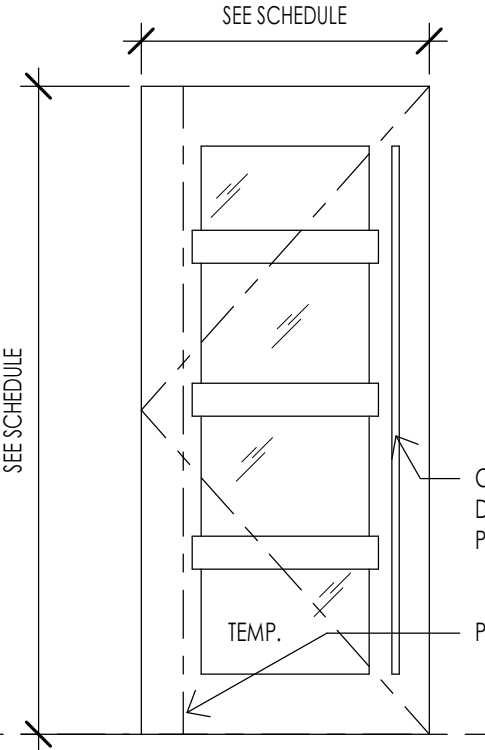


DOOR - PIVOT SILL DETAIL WOOD DOOR
6" = 1'-0"

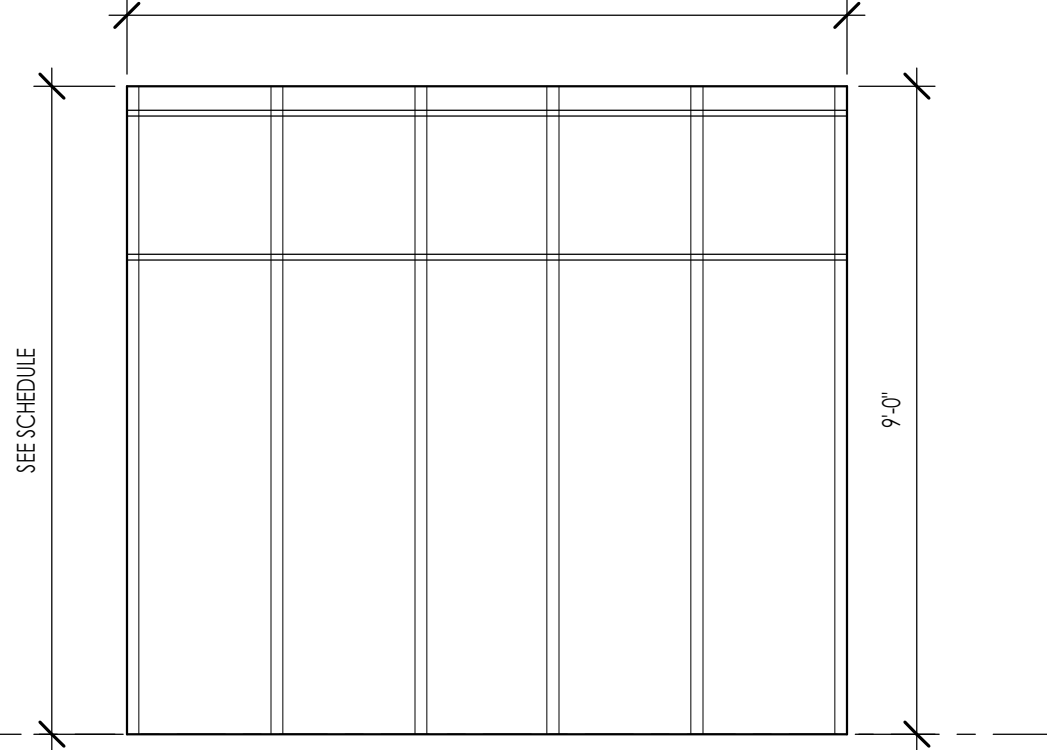
DOOR TYPES



TYPE D1 **TYPE D2** **TYPE D3** **TYPE D4** **TYPE D5**

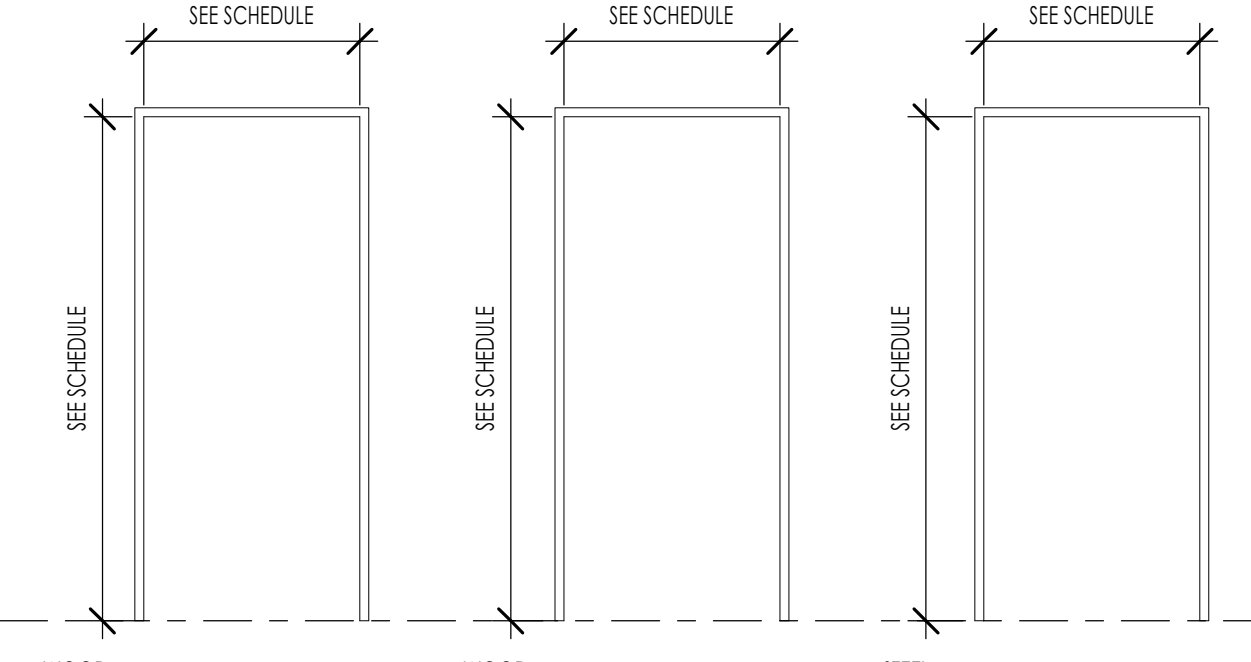


TYPE D6

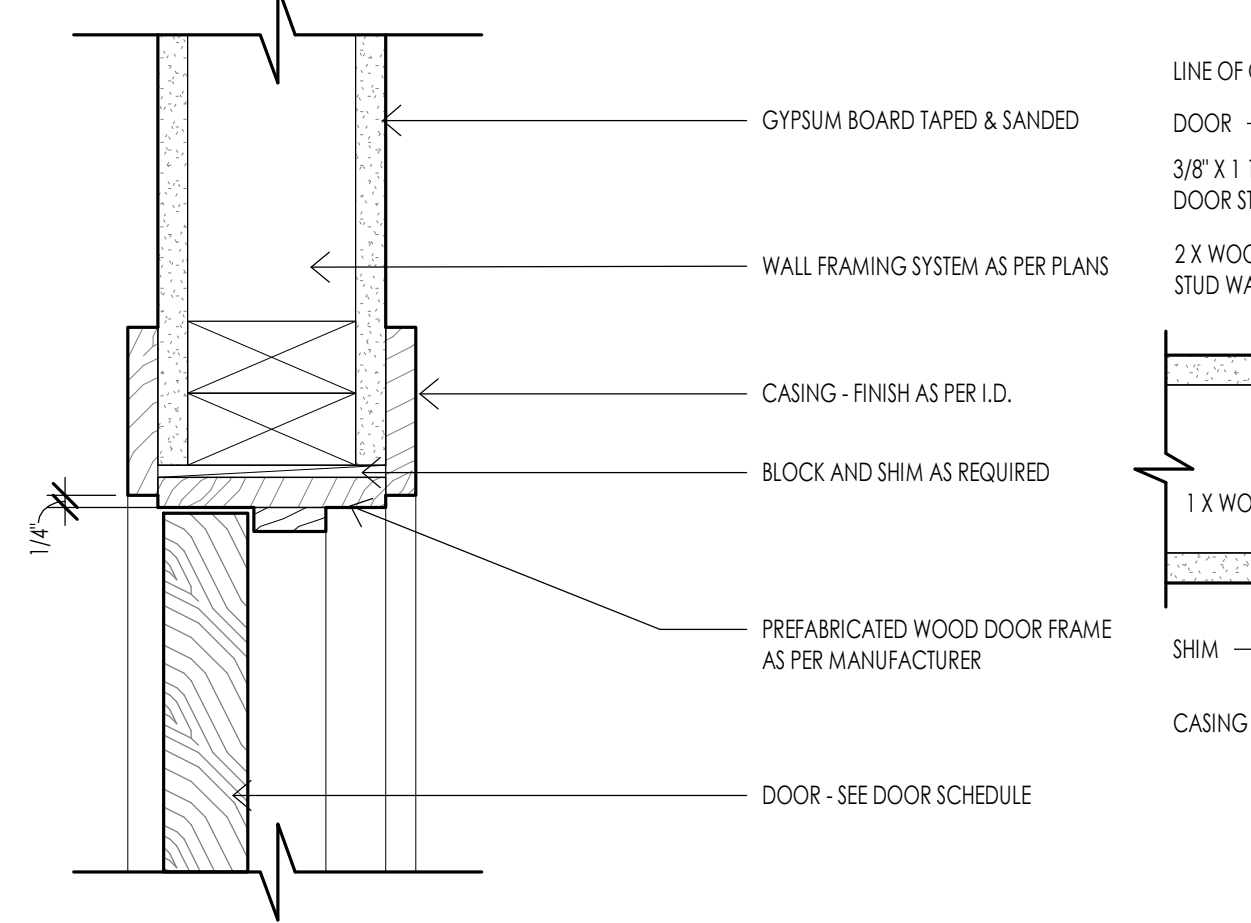


TYPE D7

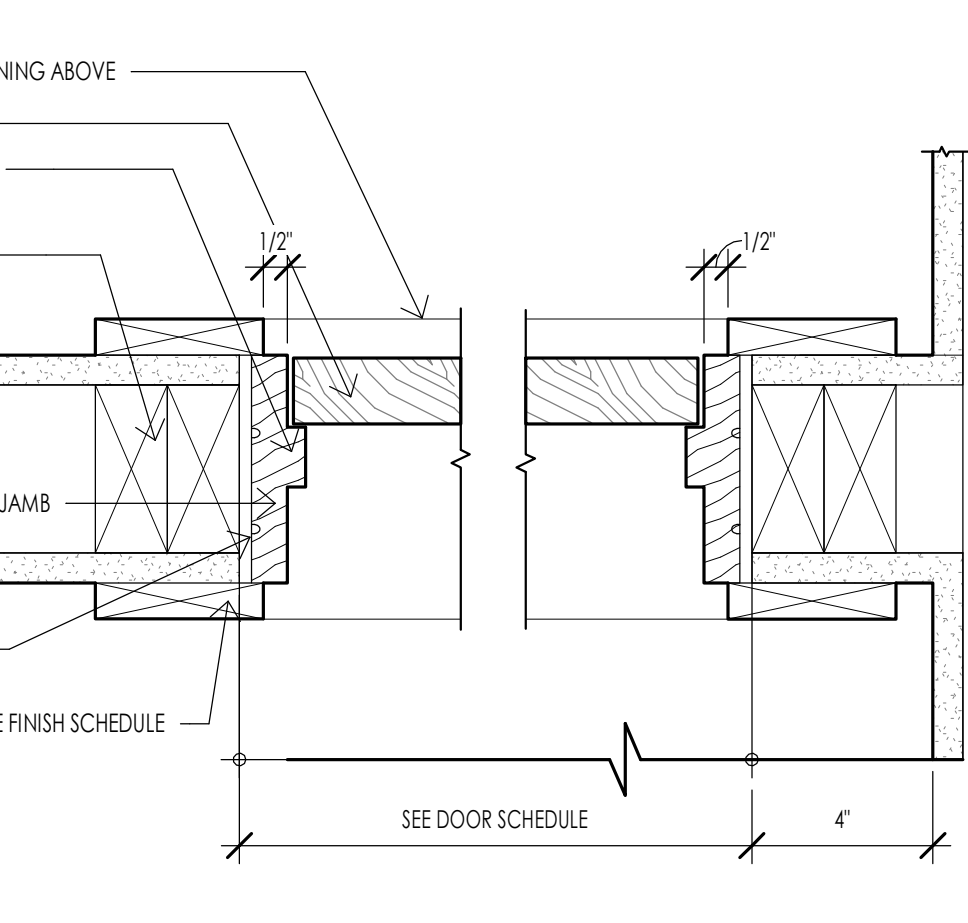
FRAME TYPES



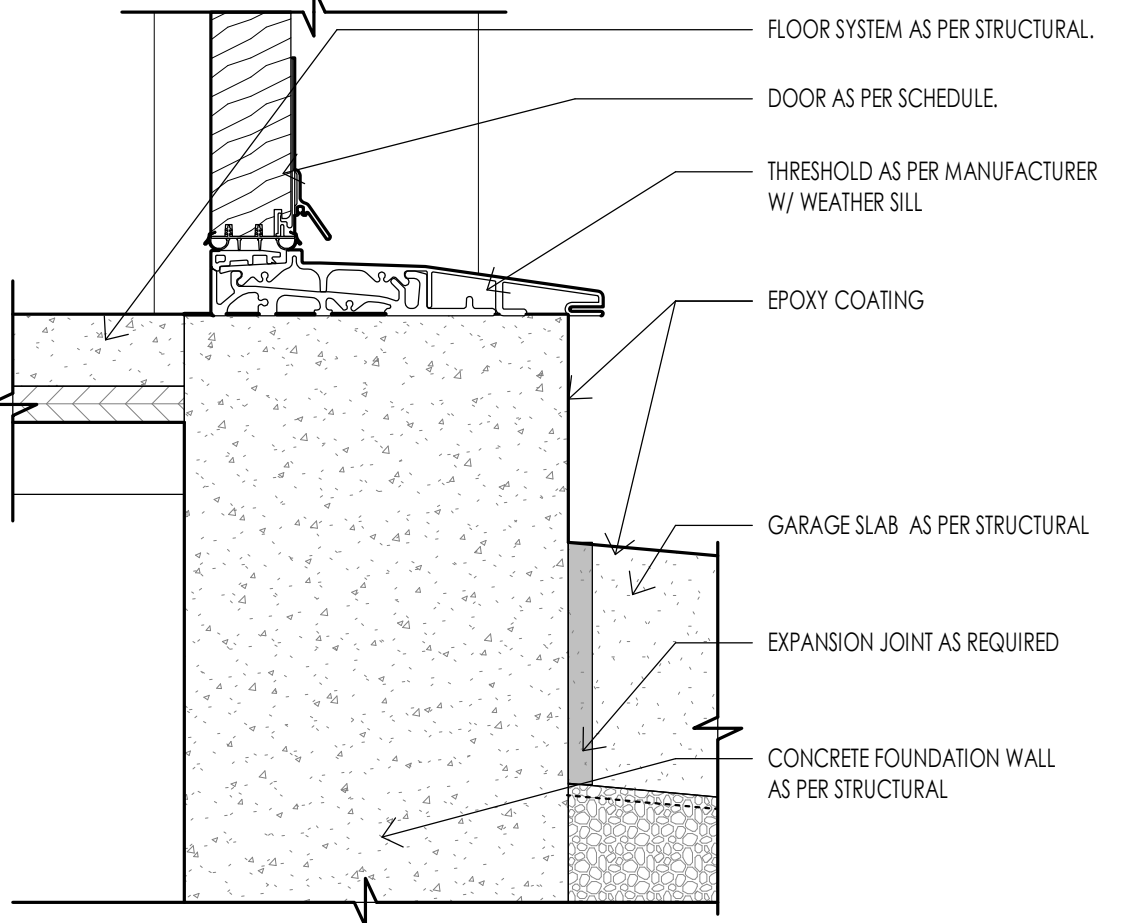
TYPE F1 **TYPE F2** **TYPE F3**



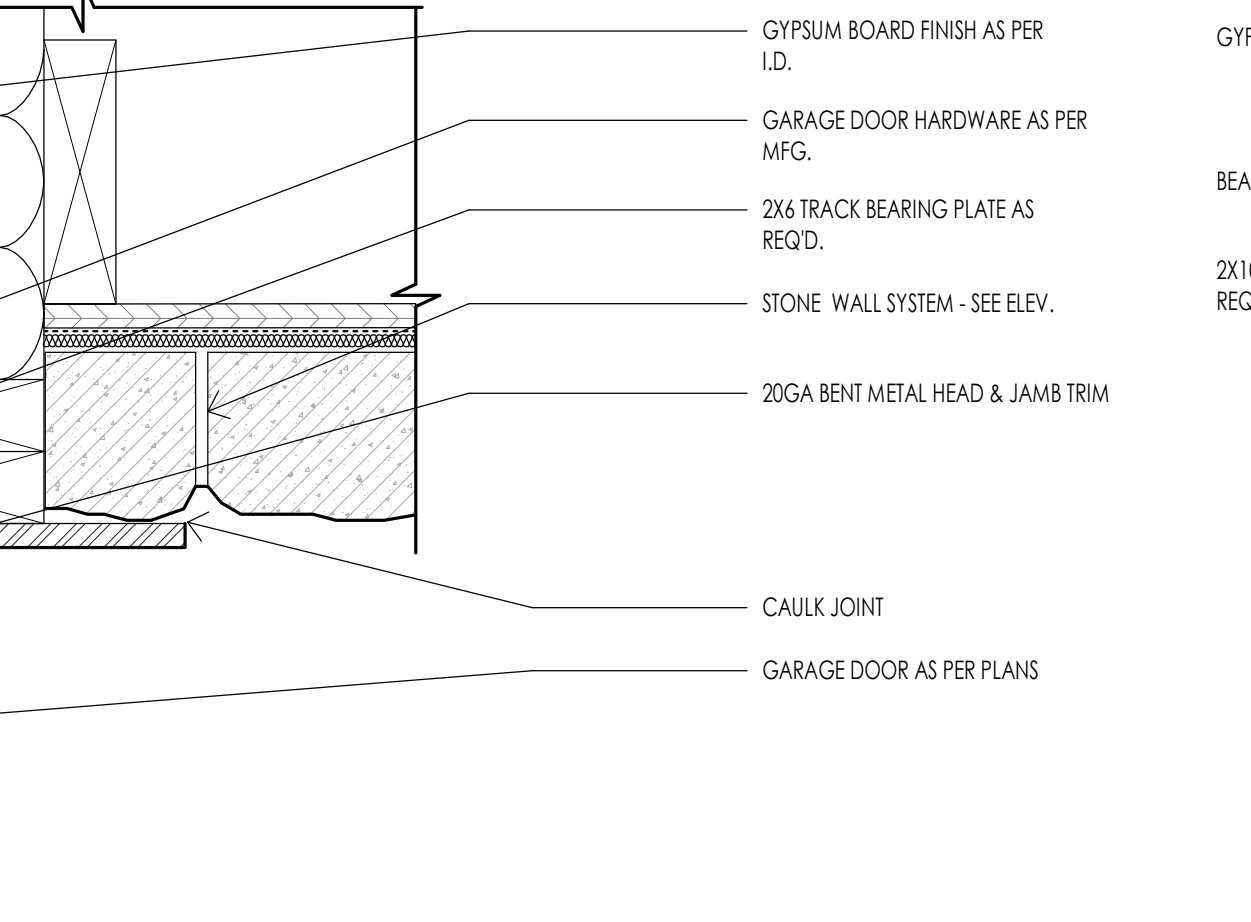
DOOR - TYPICAL INTERIOR HEAD
3" = 1'-0"



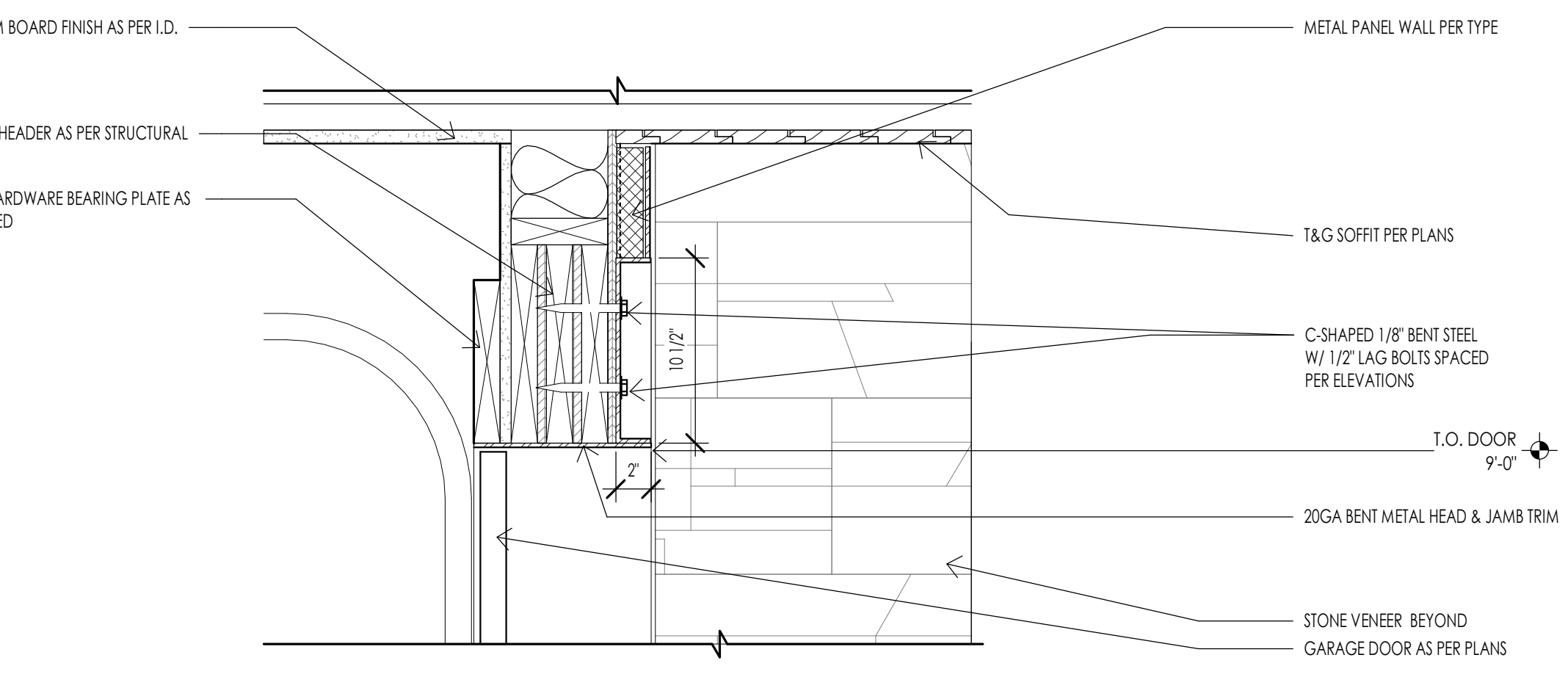
DOOR - INTERIOR TYPICAL JAMB DETAIL
3" = 1'-0"



DOOR - THRESHOLD DETAIL @ GARAGE
3" = 1'-0"



DOOR - GARAGE JAMB @ STONE
3" = 1'-0"



DOOR - GARAGE HEAD @ STONE
1 1/2" = 1'-0"



Architecture
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Landscape Architecture
Land Planning
Construction Management

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WARM SPRINGS RESIDENCE #35

190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

PROJECT NO. 22023
DATE: 2023.06.30

SHEET TITLE:
DOOR SCHEDULE & ELEVATIONS

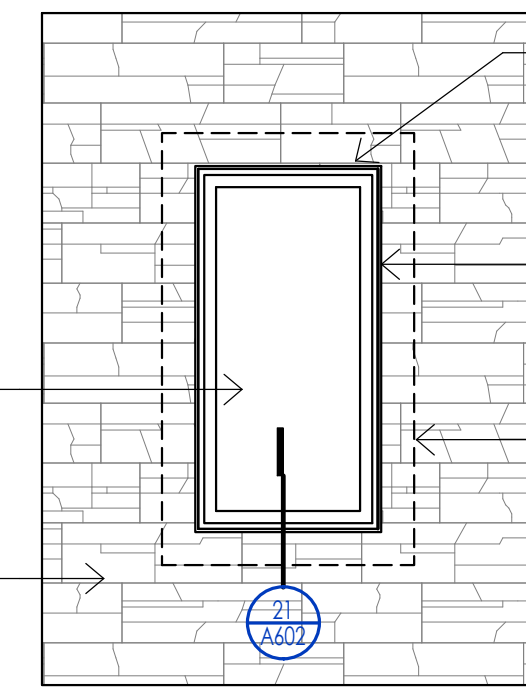
SHEET NUMBER:
A601

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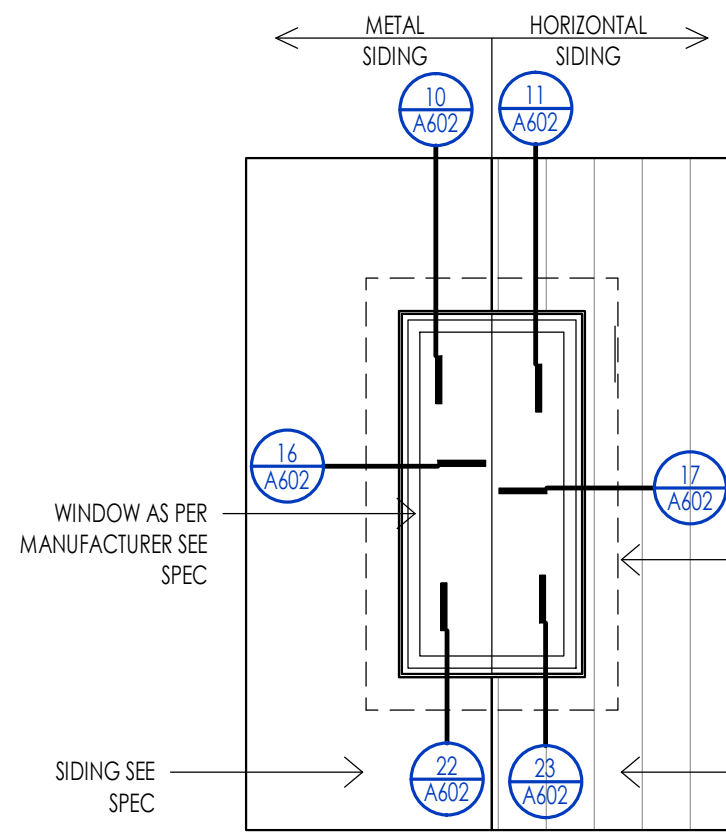
WINDOW SCHEDULE

Table with columns: MARK, UNIT SIZE (WIDTH, HEIGHT), HEAD HEIGHT, OPERATION, MATERIAL, FINISH, DETAIL (HEAD, JAMB, SILL), GLAZING (THICKNESS, TYPE), COMMENTS. Rows include W01 through W18 with various window specifications.

NOTE: COORDINATE WITH DETAILS FOR ADDITIONAL INSTALLATION REQUIREMENT.



METAL FLASHING - SEE DETAILS. CAULK CONT. ALL DISSIMILAR MATERIALS. DASHED LINE SHOWS ICE & WATER SHIELD BARRIER AT WINDOW PERIMETER TYPICAL - SEE DETAIL.



METAL FLASHING. WINDOW AS PER MANUFACTURER SEE SPEC. SIDING SEE SPEC. DASHED LINE SHOWS ICE & WATER SHIELD BARRIER AT WINDOW PERIMETER TYPICAL. SIDING SEE SPEC.

WINDOW LEGEND

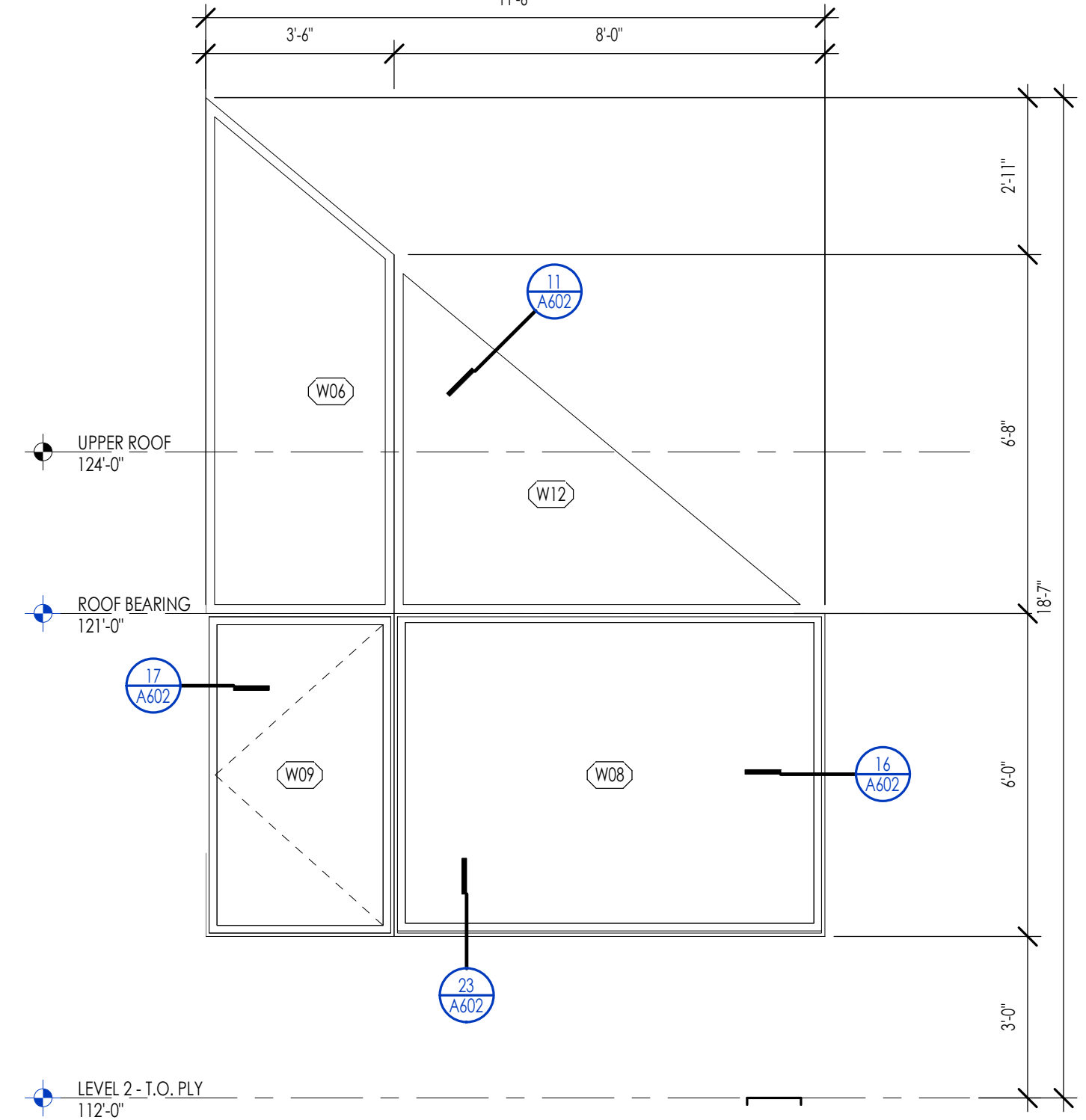
Legend table with columns: SYMBOL, DESCRIPTION. Symbols include T for tempered glazing locations, an arrow for direction of operable window door, and E for emergency egress windows.

WINDOW SPECIFICATIONS

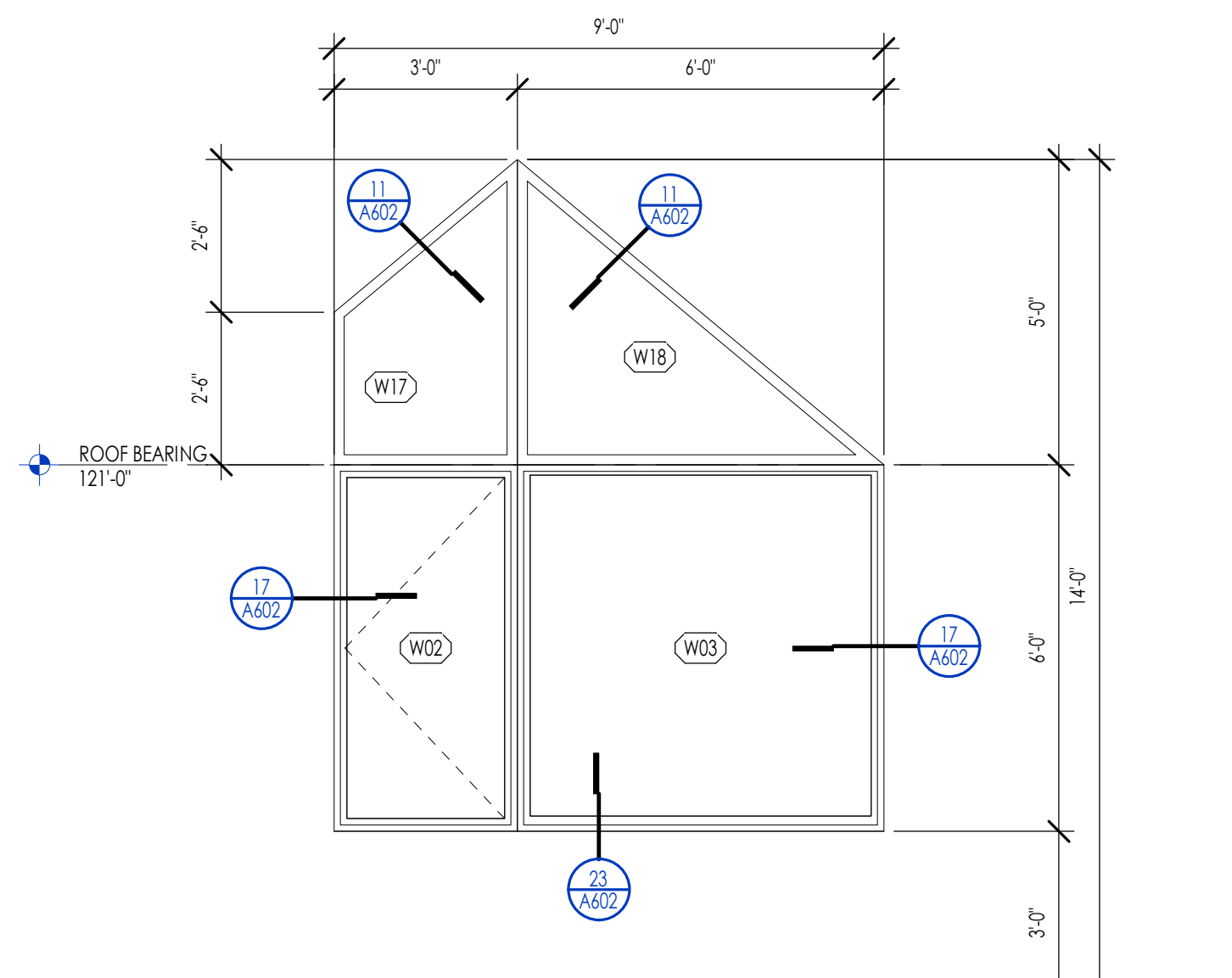
Table for window specifications including Approved Manufacturers, Basis of Design, Window Type, Window Color, Window Glazing, Minimum U-Value, Screens Required, Screen Color, Typical Jamb Width, Sill Width, and Sill Type.

WINDOW GENERAL NOTES

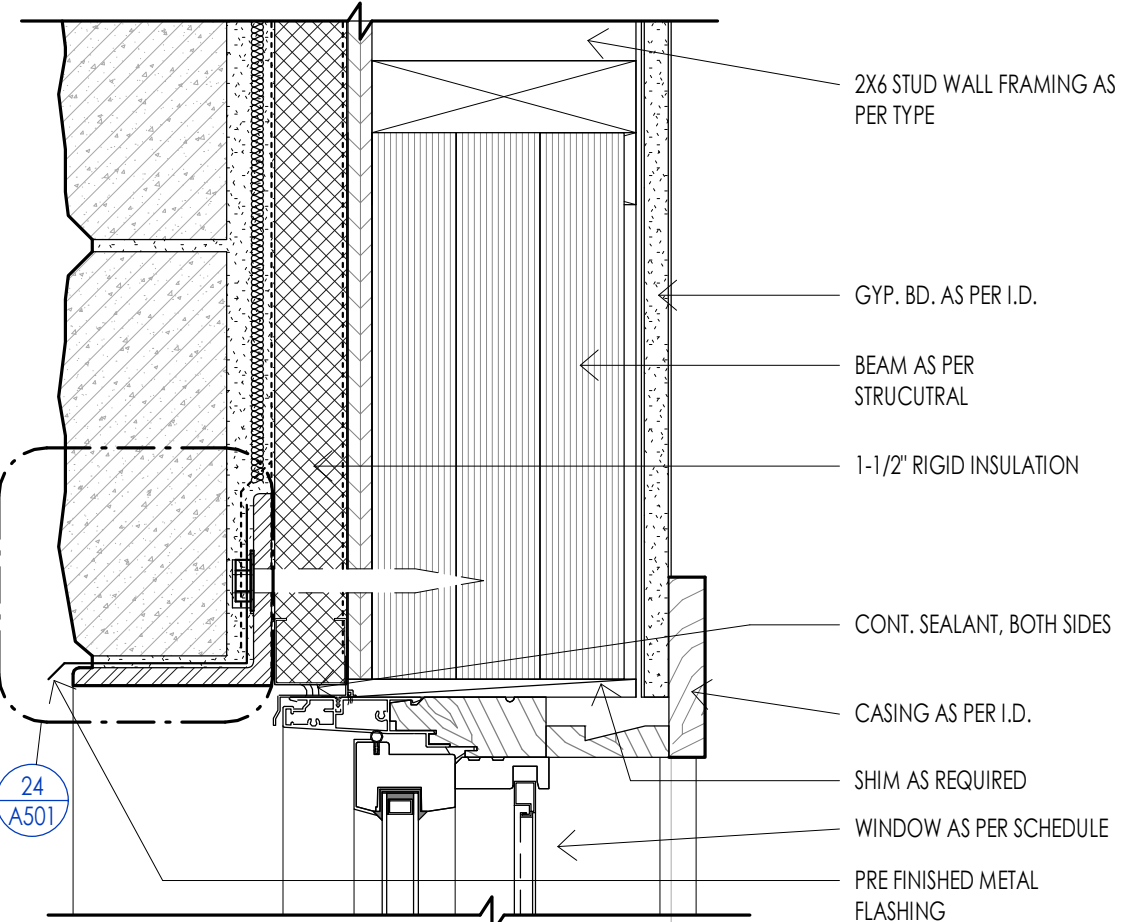
- 1. EMERGENCY ESCAPE AND RESCUE OPENINGS SHALL MEET THE FOLLOWING REQUIREMENTS. SEE I.R.C. SECTION R310.
A. BATHROOMS WITH HABITABLE SPACES SHALL HAVE AT LEAST ONE OPERABLE EMERGENCY ESCAPE AND RESCUE WINDOW OR DOOR OR ACCESS TO AN ADJOINING BERGAM WITH AN EMERGENCY ESCAPE AND RESCUE WINDOW.
B. ALL EMERGENCY OPENINGS SHALL HAVE A MINIMUM NET CLEAR OPENING AREA OF 5.7 SQUARE FEET. THE MINIMUM NET CLEAR OPENING HEIGHT SHALL BE 20 INCHES. THE MINIMUM NET CLEAR OPENING WIDTH SHALL BE 20 INCHES. EMERGENCY OPENINGS SHALL BE OPERATIONAL FROM THE INSIDE OF THE ROOM WITHOUT THE USE OF KEYS OR TOOLS. EXCEPT GROUND FLOOR, NET CLEAR AREA OF 5.0 SQUARE FEET. R310.1.1 TO R310.1.4.
C. WINDOW SHALL HAVE A SILL HEIGHT OF NOT MORE THAN 44 INCHES ABOVE THE FLOOR. OPENINGS WITH A FINISHED SILL HEIGHT BELOW THE ADJACENT GROUND ELEVATION SHALL BE PROVIDED WITH A WINDOW WELL. R310.2.2.
D. WINDOW WELLS REQUIRED FOR ESCAPE OR RESCUE SHALL HAVE HORIZONTAL DIMENSIONS THAT ALLOW THE DOOR OR WINDOW TO BE FULLY OPENED. THE HORIZONTAL DIMENSION FOR THE WINDOW WELL SHALL PROVIDE A MINIMUM NET CLEAR AREA OF 9 SQUARE FEET WITH A MINIMUM HORIZONTAL PROJECTION AND WIDTH OF 36 INCHES. R310.2.3.
E. WINDOW WELLS WITH A VERTICAL DEPTH GREATER THAN 44 INCHES BELOW THE ADJACENT GROUND LEVEL SHALL BE EQUIPPED WITH A PERMANENTLY AFFIXED LADDER OR STEPS USABLE WITH THE WINDOW IN THE FULLY OPENED POSITION. LADDERS OR RUNGS SHALL HAVE AN INSIDE WIDTH OF AT LEAST 12 INCHES. SHALL PROJECT AT LEAST 3 INCHES FROM THE WALL AND SHALL BE SPACED NOT MORE THAN 18 INCHES ON CENTER VERTICALLY FOR THE FULL HEIGHT OF THE WINDOW WELL. R310.2.3.1.
F. BARS, GRILLS, COVERS, SCREENS, ETC. SHALL BE PERMITTED TO BE PLACED OVER THE EMERGENCY EGRESS OPENING WINDOW WELL PROVIDED THE NET CLEAR OPENING SIZE IS NOT COMPROMISED AND THAT SUCH DEVICES SHALL BE RELEASED OR REMOVABLE FROM THE INSIDE WITHOUT THE USE OF A KEY, TOOL, OR FORCE GREATER THAN THAT WHICH IS REQUIRED FOR NORMAL OPERATION. R310.4.
2. SAFETY GLAZING SHALL BE INSTALLED IN HAZARDOUS LOCATIONS AND SHALL MEET THE FOLLOWING REQUIREMENTS. SEE I.R.C. SECTION R308. FOR EXCEPTIONS SEE I.R.C. R308.4.
A. EACH PANE OF GLASS INSTALLED IN HAZARDOUS LOCATIONS SHALL BE PERMANENTLY IDENTIFIED BY MANUFACTURER DESIGNATING THE TYPE, THICKNESS, AND SAFETY GLAZING STANDARD. THE LABEL SHALL BE ACID ETCHED, SANDBLASTED, CERAMIC FRIED OR EMBOSSED ON GLASS AND BE VISIBLE WHEN THE UNIT IS GLAZED. FOR EXCEPTIONS SEE I.R.C. R308.1.
B. PROVIDE SAFETY GLAZING IN FIXED AND OPERABLE PANELS OF SWINGING, SLIDING AND FOLDING DOORS (R308.1.1). SAFETY GLAZING SHALL BE PROVIDED WHEN GLAZING IN AN INDIVIDUAL FIXED OR OPERABLE PANEL ADJACENT TO A DOOR WHERE THE BOTTOM EXPOSED EDGE OF THE GLAZING IS LESS THAN 40 INCHES ABOVE THE FLOOR OR WALKING SURFACE AND IS WITHIN 24 INCHES OF EITHER SIDE OF THE DOOR IN THE PLANE OF THE DOOR IN A CLOSED POSITION OR WHERE THE GLAZING IS ON A WALL PERPENDICULAR TO THE PLANE OF THE DOOR IN A CLOSED POSITION AND WITHIN 24 INCHES OF THE HINGE SIDE OF AN IN SWINGING DOOR. (I.R.C. R308.4.2)
C. PROVIDE SAFETY GLAZING WHERE THE BOTTOM EXPOSED EDGE OF THE GLAZING IS LESS THAN 36 INCHES ABOVE THE PLANE OF THE ADJACENT WALKING SURFACE OF STAIRWAYS, LANDINGS BETWEEN FLIGHTS OF STAIRS AND RAMPS. (I.R.C. R308.4.6)
D. PROVIDE SAFETY GLAZING IN DOORS AND ENCLOSURES FOR HOT TUBS, WHIRLPOOLS, SAUNAS, STEAM ROOMS, BATHUBS AND SHOWERS. GLAZING IN ANY PORTION OF A BUILDING WALL ENCLLOSING THESE COMPARTMENTS WHERE THE BOTTOM EXPOSED EDGE OF THE GLAZING IS LESS THAN 60 INCHES ABOVE A STANDING OR WALKING SURFACE. (I.R.C. R308.4.5)
E. PROVIDE SAFETY GLAZING IN RAILINGS REGARDLESS OF AN AREA OR HEIGHT. (I.R.C. R308.4.4)
F. PROVIDE SAFETY GLAZING IN WALLS AND FENCES ENCLLOSING SWIMMING POOLS OR HOT TUBS WHERE THE BOTTOM EDGE OF THE POOL OR SPA GLASS IS LESS THAN 60 INCHES ABOVE THE WALKING SURFACE. (I.R.C. R308.4.5)
G. PROVIDE SAFETY GLAZING IN FIXED OR OPERABLE PANELS THAT MEETS ALL OF THE FOLLOWING CONDITIONS: AREAS GREATER THAN 9 SQUARE FEET; BOTTOM EDGE LESS THAN 18 INCHES ABOVE THE FLOOR; TOP EDGE GREATER THAN 36 INCHES ABOVE FLOOR; AND WITHIN 36 INCHES OF WALKING SURFACE. (I.R.C. R308.4.3)
3. THE GENERAL CONTRACTOR AND WINDOW SUPPLIER ARE RESPONSIBLE TO FIELD MEASURE ALL WINDOW OPENINGS AND PROVIDE SHOP DRAWINGS BEFORE MANUFACTURING. SHOP DRAWINGS SHALL BE PROVIDED FOR EACH BUILDING INDIVIDUALLY AND SHALL NOT BE COMBINED WITH ANY OTHER BUILDING. THE WINDOW SUPPLIER SHALL BE RESPONSIBLE TO VERIFY ALL EMERGENCY EGRESSES, LIGHT AND VENTILATION, AND TEMPERED GLASS LOCATION REQUIREMENTS PRIOR TO EACH SUBMITTAL.
4. THE GENERAL CONTRACTOR AND WINDOW SUPPLIER ARE RESPONSIBLE TO VERIFY THAT EACH OF THE ABOVE LISTED REQUIREMENTS HAVE BEEN MET AND NOTE ANY DISCREPANCIES ON SUBMITTAL. REFER TO THE PROJECT SPECIFICATIONS FOR ADDITIONAL INFORMATION.



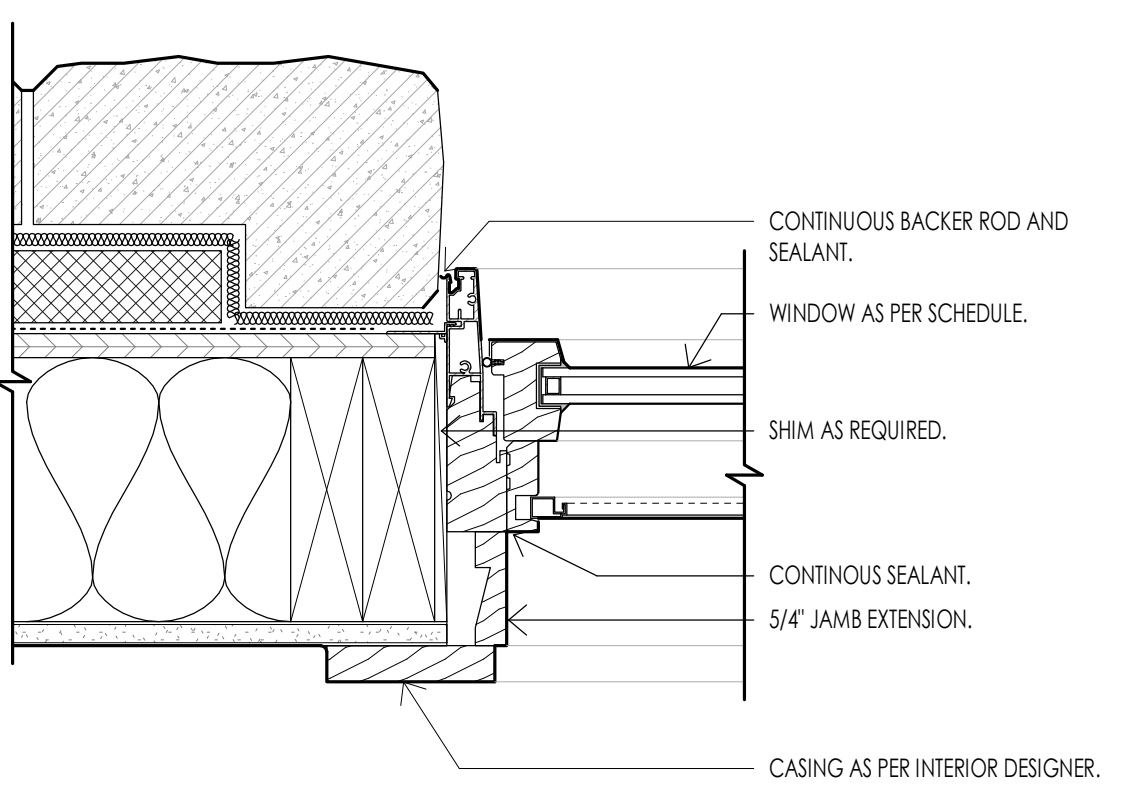
WINDOW GROUP 01



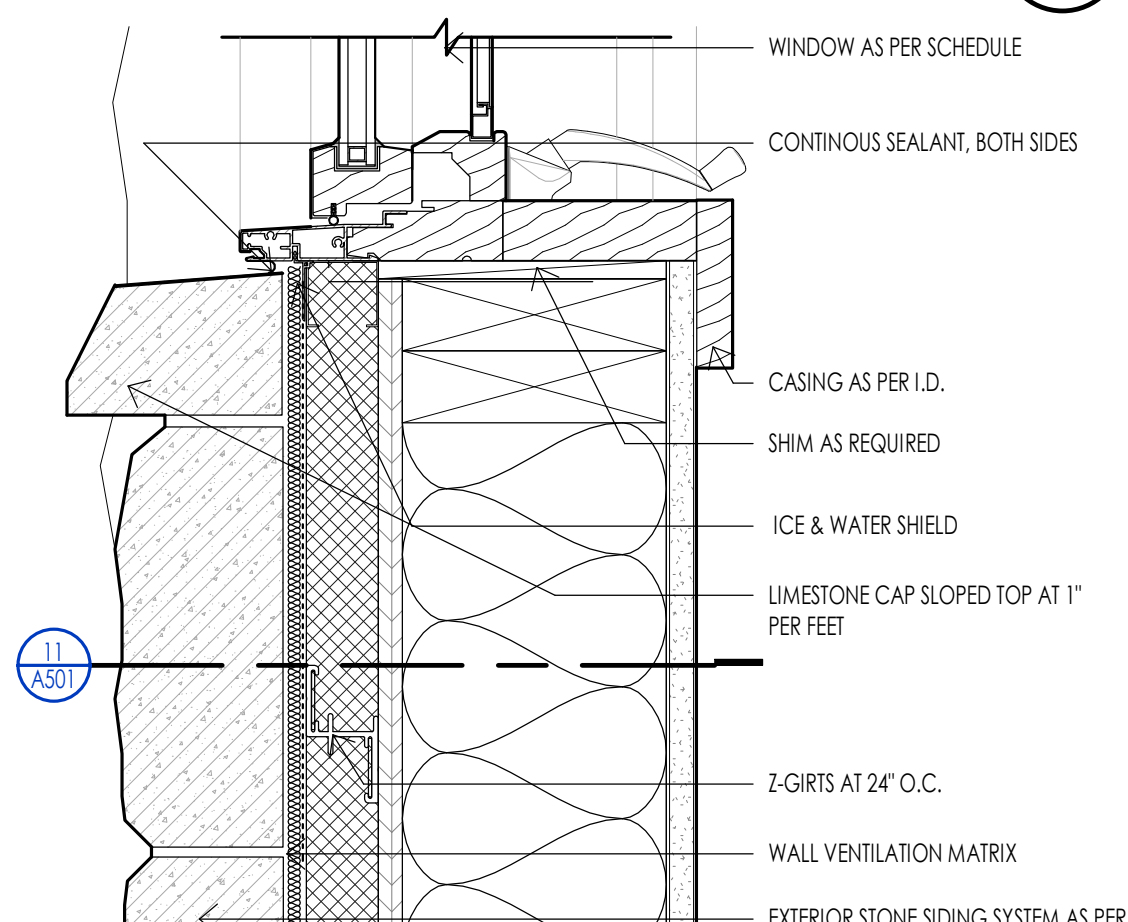
WINDOW GROUP 02



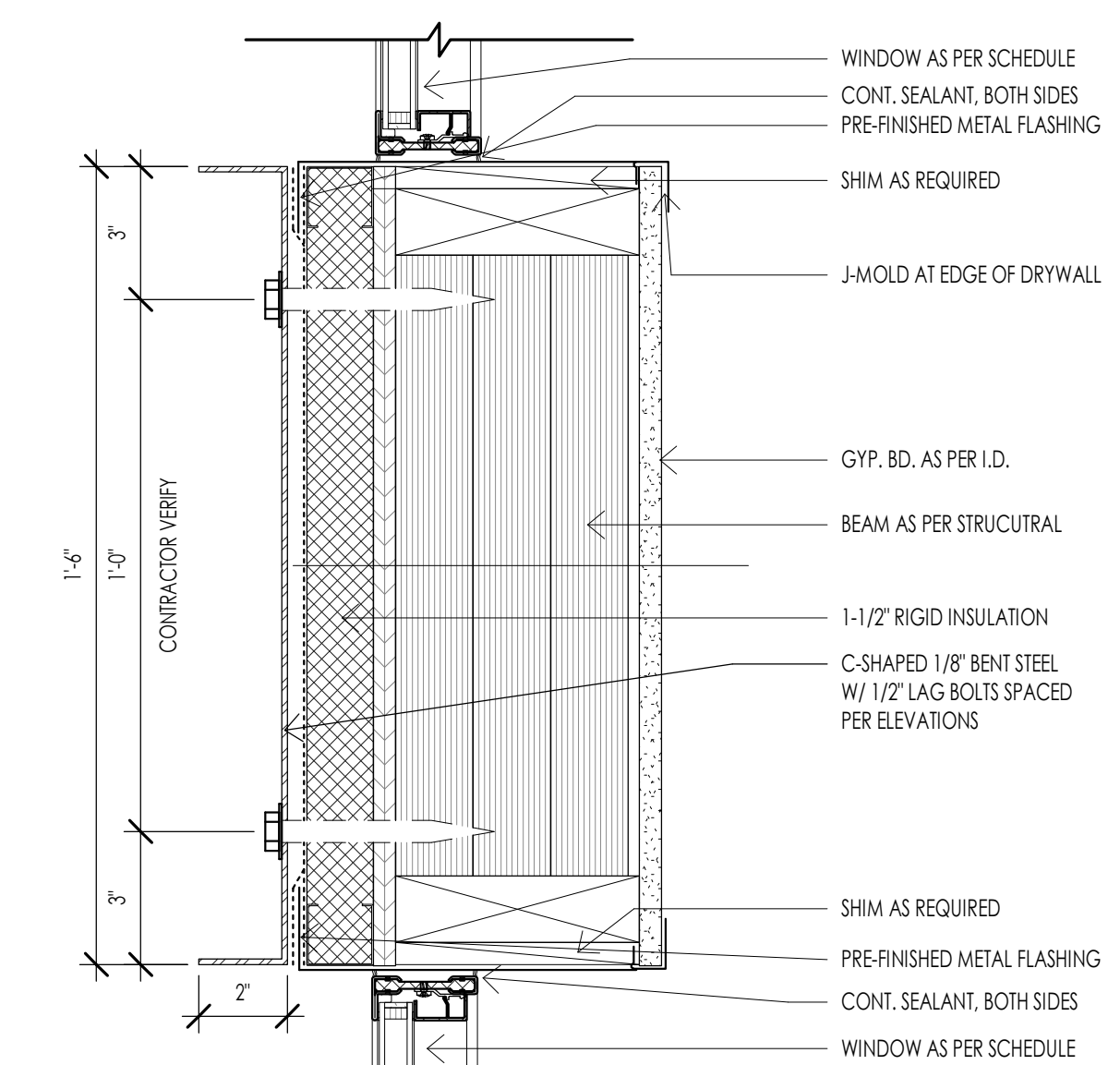
WINDOW - HEAD AT STONE



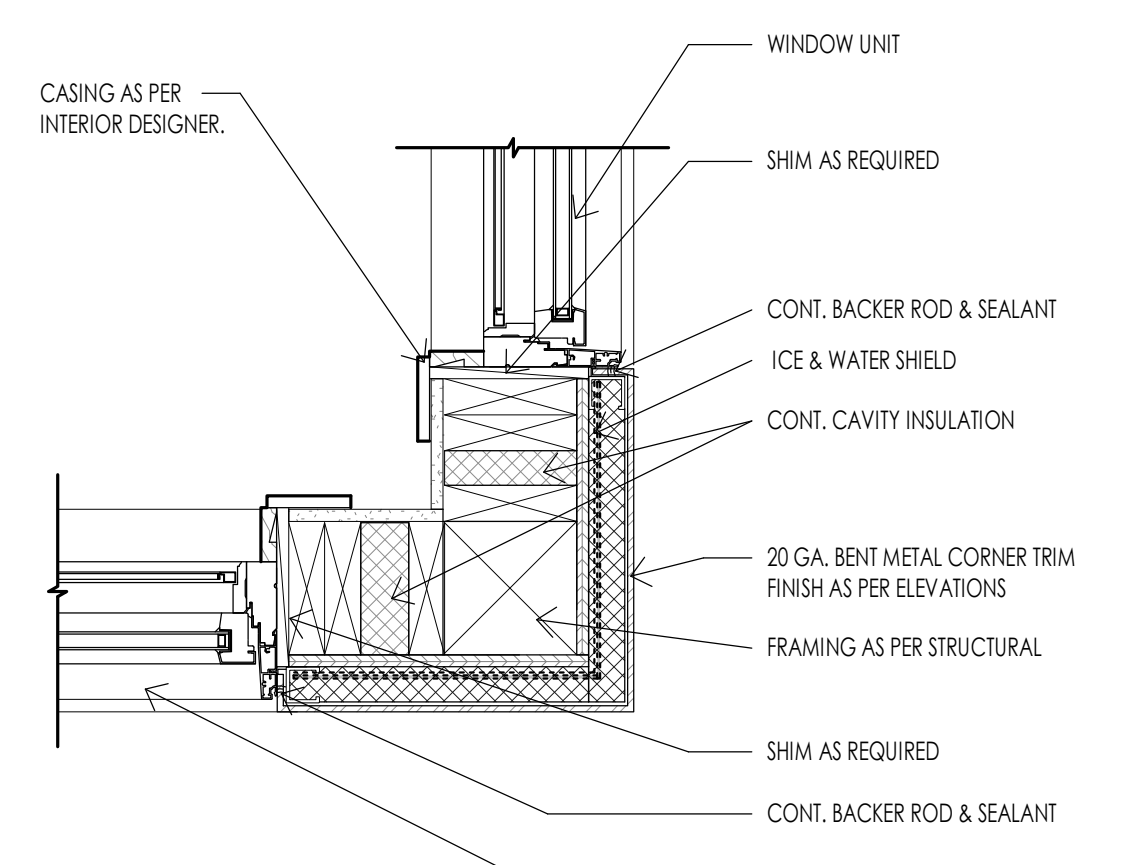
WINDOW - JAMB DETAIL AT STONE



WINDOW - SILL DETAIL AT STONE

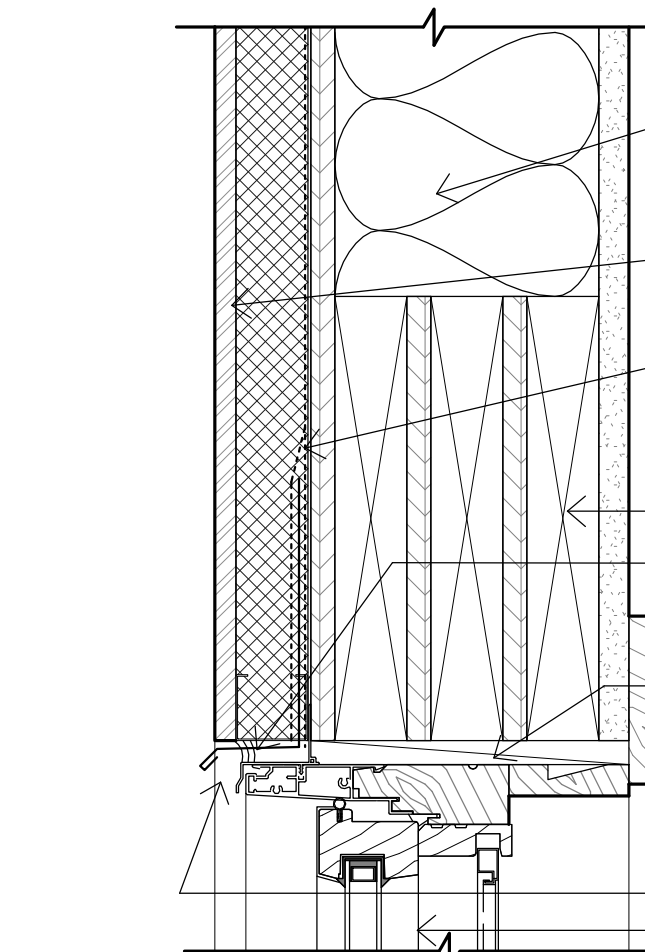


WINDOW - STEEL HEADER TRIM BAND

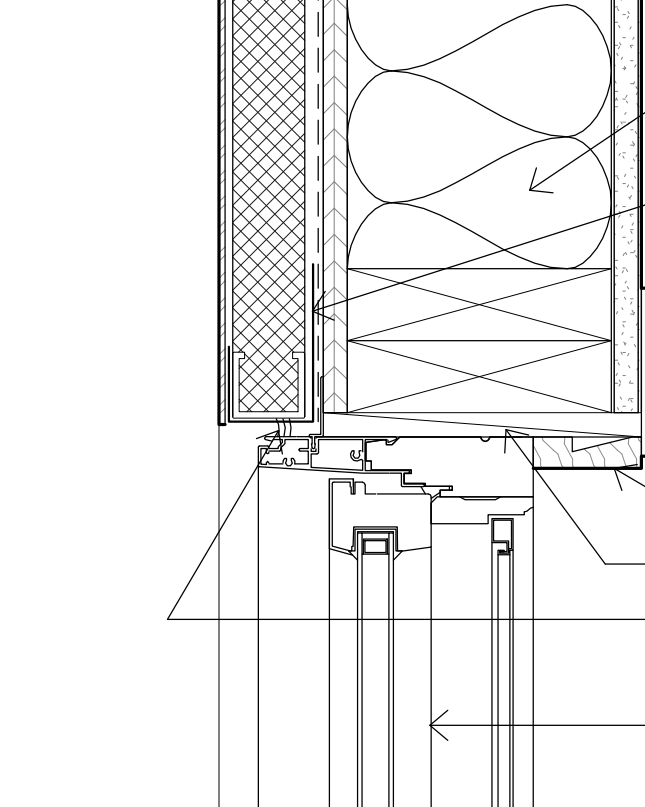


WINDOW - JAMB CORNER METAL TRIM

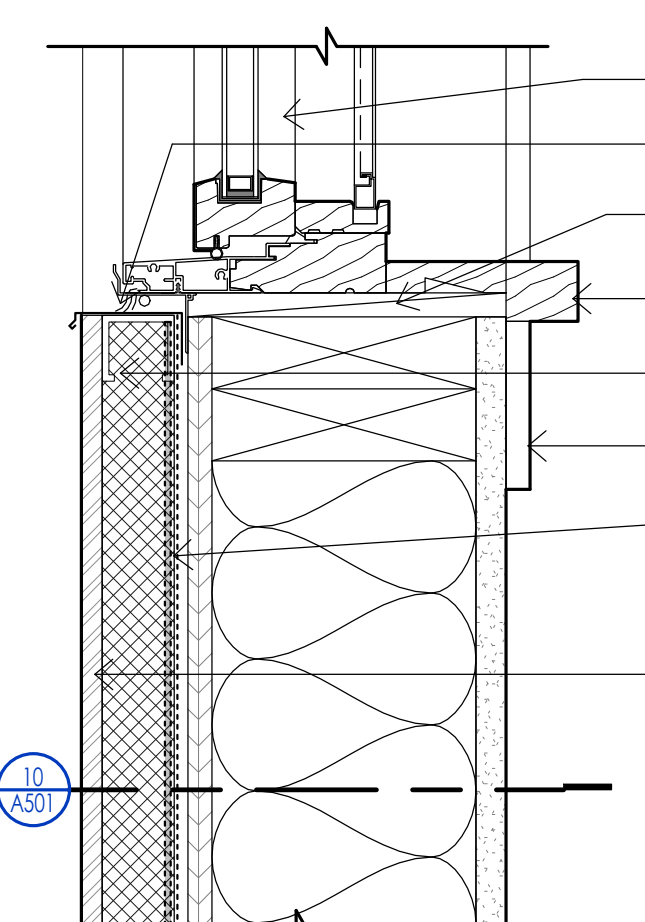
WINDOW - ELEVATION AT STONE



WINDOW - HEAD AT METAL PANEL

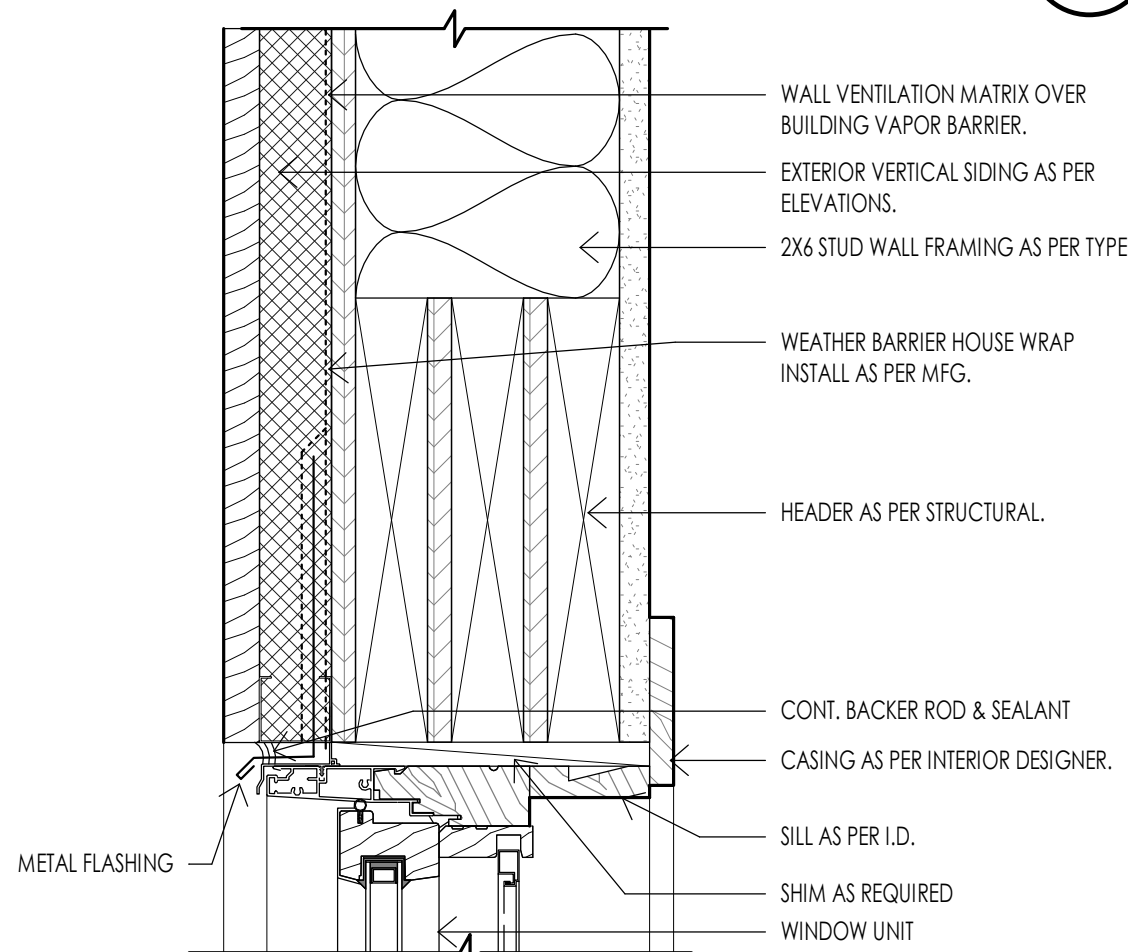


WINDOW - JAMB DETAIL AT METAL PANEL SIDING

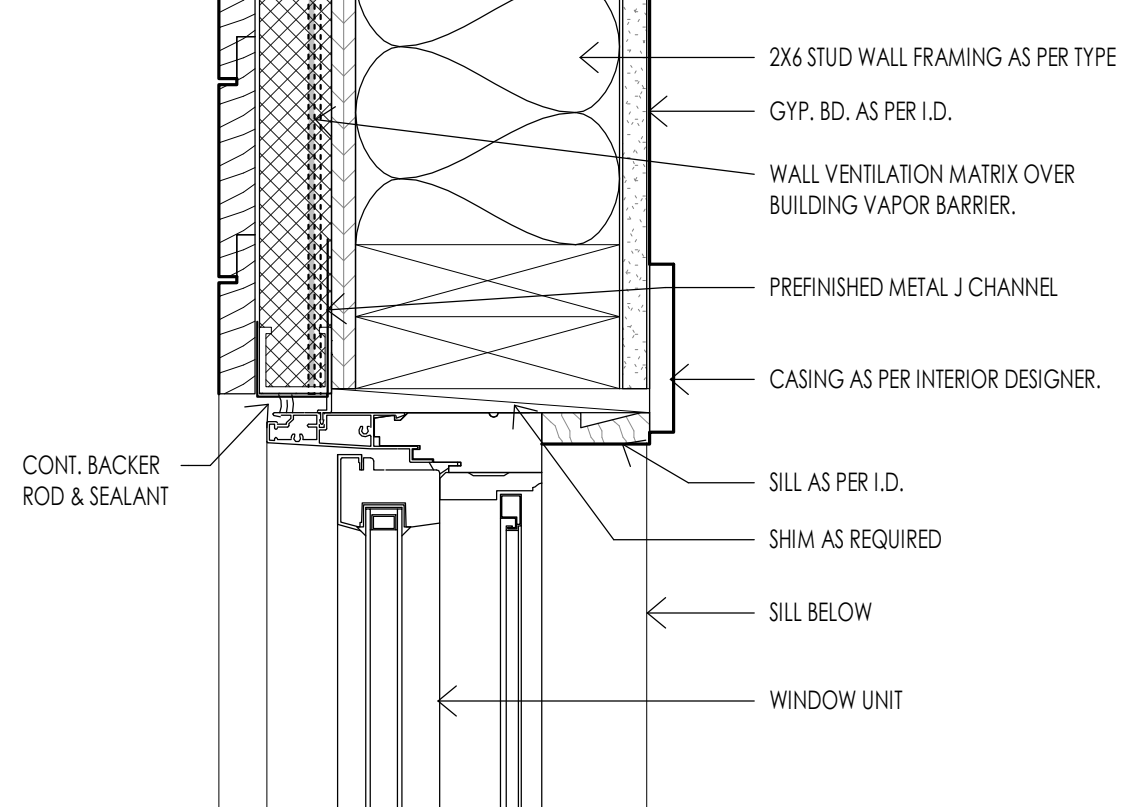


WINDOW - SILL DETAIL AT METAL PANEL

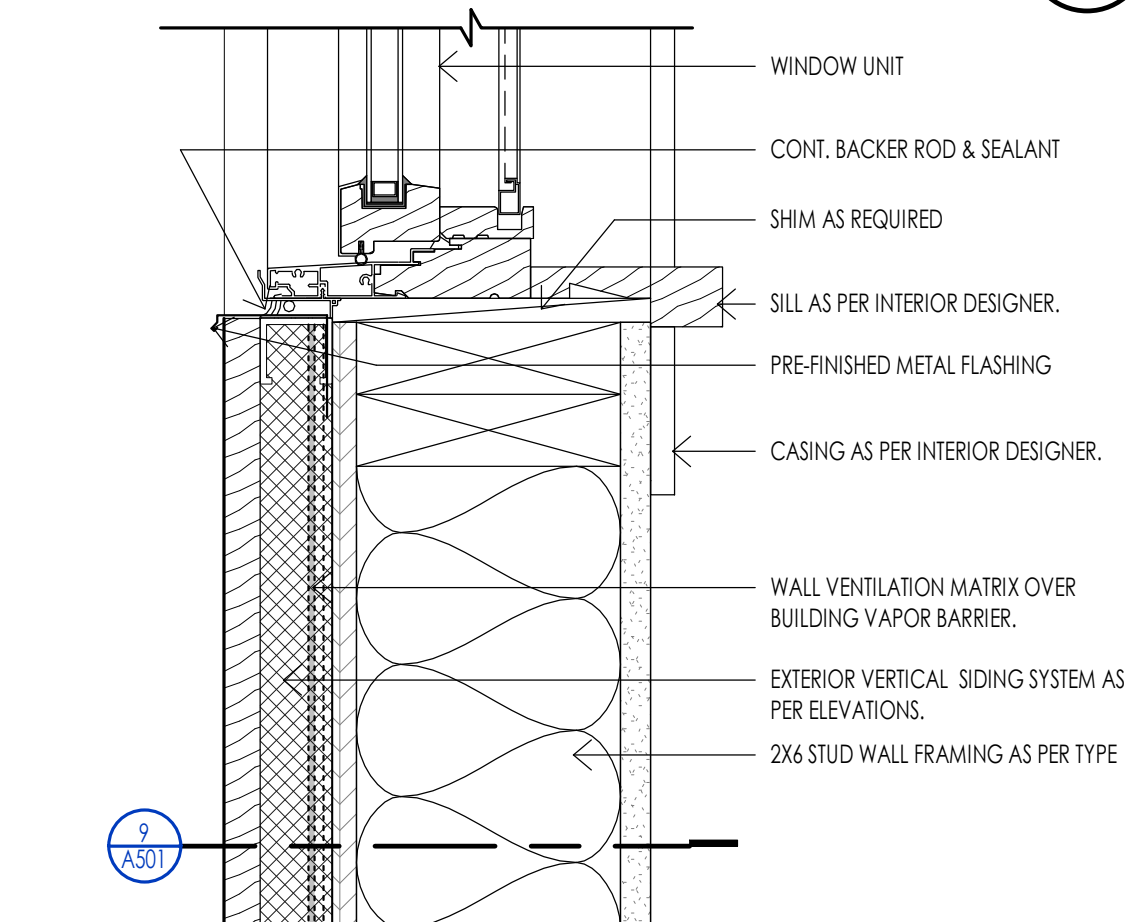
WINDOW - ELEVATION AT VERTICAL SIDING & METAL



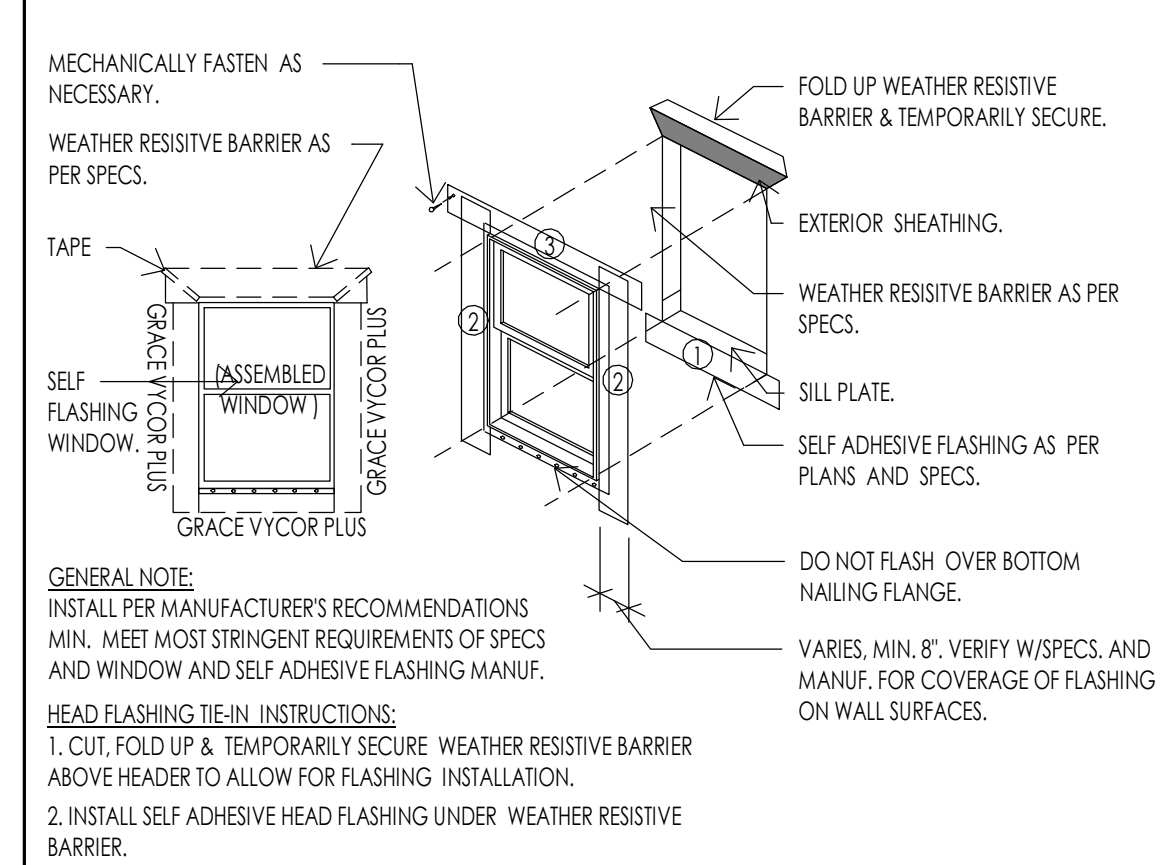
WINDOW - HEAD DETAIL AT SIDING



WINDOW - JAMB DETAIL AT SIDING



WINDOW - SILL DETAIL AT SIDING



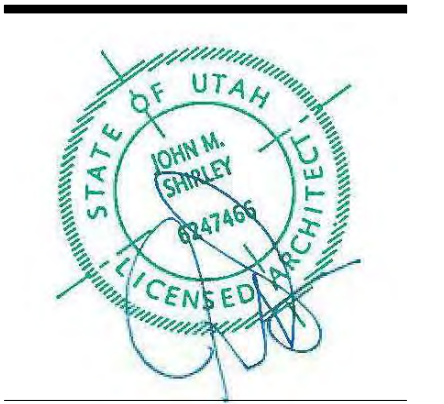
WINDOW - TYPICAL FLASHING DETAIL



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WARM SPRINGS RESIDENCE #35 190 BALD MOUNTAIN ROAD KETCHUM, IDAHO 83340

PROJECT NO. 22023 DATE: 2023.06.30

SHEET TITLE: WINDOW SCHEDULE & ELEVATIONS

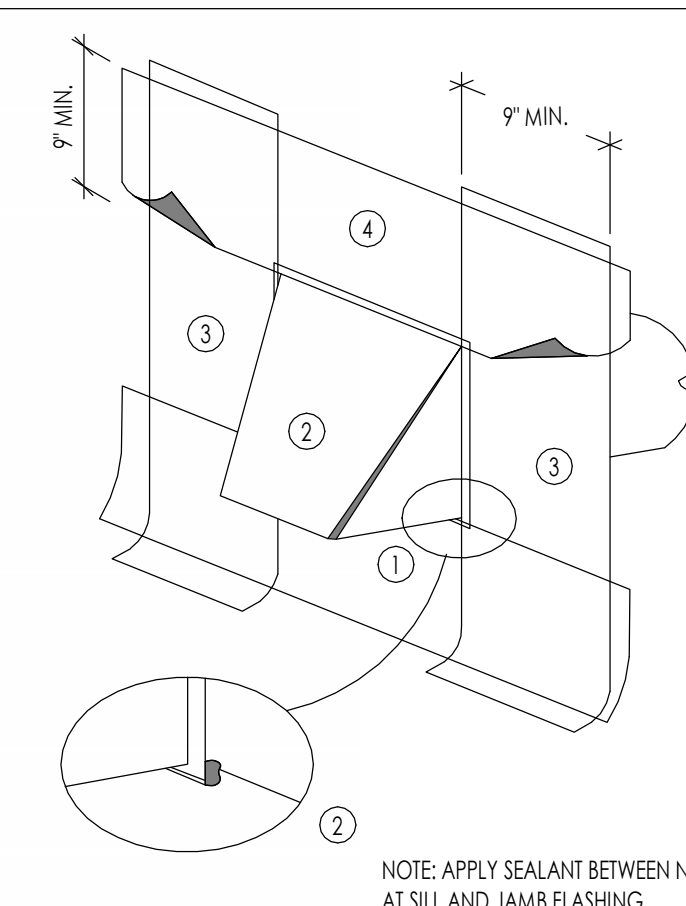
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MECHANICAL GENERAL NOTES

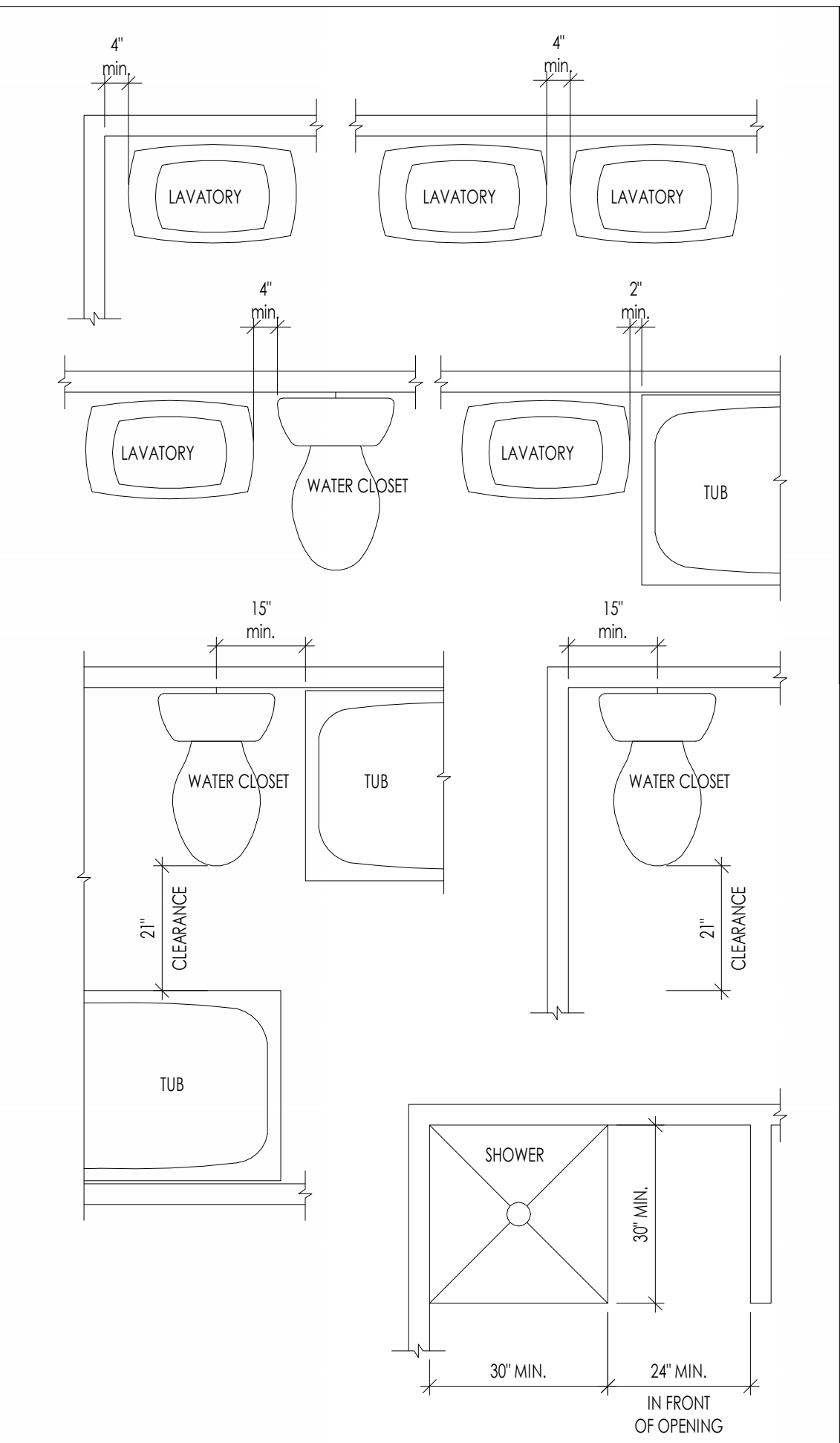
- PLUMBING GENERAL NOTES**
- THE PLUMBING SYSTEM SHALL BE DESIGNED BY A LICENSED MECHANICAL CONTRACTOR/ DESIGNER AND SHALL MEET ALL THE REQUIREMENTS OF THE 2015 IRC, IPC AND IECC.
 - THE PLUMBING CONTRACTOR IS RESPONSIBLE FOR THE COMPLETE PLUMBING SYSTEM INSTALLATION AND SHALL PROVIDE A (1) ONE YEAR WARRANTY BEGINNING FROM THE TIME OF CERTIFICATE OF OCCUPANCY. THE CONTRACTOR IS RESPONSIBLE TO PROVIDE THE OWNER COMPLETE OPERATION AND MAINTENANCE MANUALS. THE CONTRACTOR SHALL ALSO SET UP A TIME TO PROVIDE COMPLETE TRAINING OF THE SYSTEM TO THE OWNER.
 - THE PLUMBING CONTRACTOR SHALL REVIEW AND SHALL GANG ALL ROOF VENTS INTO SINGLE ROOF VENTS WHERE POSSIBLE, AND SHALL RUN THE VENTS OUT OF THE ROOF AT THE HIGHEST POINT POSSIBLE. ALL VENTS SHALL HAVE BLOCKING ON EACH SIDE OF THE VENT IN THE ROOF STRUCTURE TO ENSURE THE VENTS WILL NOT BE MOVED DUE TO SNOW ON THE ROOF. ALL VENTS SHALL BE SIZED PER THE BUILDING CODE, BUT SHALL NOT BE LESS THAN 3 INCH PIPES. THE PLUMBING CONTRACTOR SHALL COORDINATE THAT THE PROPER FLASHING HAS BEEN INSTALLED FOR EACH VENT.
 - THE ROOF VENTS SHALL EXTEND ABOVE THE ROOF AS REQUIRED BY THE LOCAL JURISDICTION AND BUILDING CODES. THE PLUMBING CONTRACTOR SHALL COORDINATE THIS INSTALLATION.
 - ALL PLUMBING FIXTURES ARE SPECIFIED ON THE MECHANICAL DRAWINGS, AND ON THE INTERIOR DRAWINGS. THE PLUMBING CONTRACTOR SHALL PROVIDE FULL AND COMPLETE SHOP DRAWING SUBMITTAL ON ALL PLUMBING FIXTURE ITEMS FOR APPROVAL BY OWNER AND DESIGN TEAM.
 - THE PLUMBING FIXTURES SHALL HAVE THE FOLLOWING REQUIREMENTS:
 - SHOWER HEADS SHALL HAVE A FLOW RATE OF 2.5 GPM OR LESS.
 - WATER CLOSETS SHALL HAVE ECONO-FLUSH TANK 1.6 GAL. MAX. FLUSH.
 - ALL NOSE BBS SHALL BE NON-FREEZE TYPE WITH BACK FLOW PREVENTERS.
 - THE PLUMBING CONTRACTOR SHALL INSTALL ALL PLUMBING FIXTURES IN STRICT ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS. TAKE CARE DURING BUILDING CONSTRUCTION TO SEE THAT PROVISIONS ARE MADE FOR PROPER FUTURE SUPPORT AND THAT PROVISIONS ARE MADE FOR PROPER FUTURE SUPPORT. ROUGH IN PIPING IS ACCURATELY SET AND PROTECTED FROM MOVEMENT OF DAMAGE DURING CONSTRUCTION.
 - THE PLUMBING CONTRACTOR SHALL MAKE SURE THAT NO PLUMBING WILL BE INSTALLED WITHIN THE EXTERIOR WALL.
 - PLUMBING CONTRACTOR SHALL ASSESS WATER PRESSURE AND ENSURE ADEQUATE PRESSURE IS AVAILABLE FOR MULTIPLE FUTURE USE SIMULTANEOUSLY WITH OUT PRESSURE DECREASE OR TEMPERATURE FLUCTUATION.
 - PLUMBING CONTRACTOR SHALL PROVIDE A TURN OFF VALVE AND DRAIN AT THE LOWEST LEVEL OF THE FACILITY. ALL FIXTURES SHALL BE ABLE TO DRAIN TO THIS POINT. PROVIDE A FLOOR DRAIN AT THE LOCATIONS OF PLUMBING SYSTEM DRAIN.
 - ALL SUPPLY WASTE AND GAS LINE MATERIALS, WORKMANSHIP, AND INSTALLATION AS PER INDUSTRY STANDARDS. ALL WATER SUPPLY LINES IN THE BUILDING SHALL BE TYPE "1" COPPER. TO INCLUDE PIPING TO MANFOLDS. EQUIPMENT SHALL BE COPPER WITHIN THE BUILDING. ALL SUPPLY TO FIXTURES MAY BE POLYETHYLENE CROSS LINK PIPING FOR ABOVE GROUND AND BUILDING APPLICATIONS. INSTALL AS PER MANUFACTURER'S SPECIFICATIONS. ALL CONNECTIONS FOR POLYETHYLENE PIPING SHALL BE BRASS. FITTINGS WITH COMPRESSION BAND FITTINGS.
 - ALL WATER LINES UNDERGROUND SHALL BE TYPE "1" COPPER. ALL FITTINGS AND JOINTS SHALL BE SWEAT SOLDER JOINTS TOGETHER.
 - WASTE LINES SHALL BE PROVIDED WITH CLEAN OUT AS REQUIRED. EXTEND CLEAN OUT TO ACCESSIBLE SURFACE. DO NOT PLACE CLEAN OUTS IN FLOORS UNLESS PREVIOUSLY APPROVED BY THE DESIGN TEAM AND OWNER.
 - GAS PIPING SHALL BE INSTALLED AS PER THE LATEST CODE REQUIREMENTS FOR THIS TYPE OF PROJECT. ALL GAS PIPING SHALL BE FULLY TESTED AND INSPECTED FOR ANY LEAKS PRIOR TO FINAL COMPLETION OF THE PROJECT. THE CONTRACTOR SHALL INSTALL SHUT OFF VALVES AT EACH GAS APPLIANCE AND SHALL LOCATE THE VALVE TO HAVE ACCESS TO THE VALVE.
 - PLUMBING CONTRACTOR SHALL TEST ALL PIPING INCLUDING DRAINAGE WASTE LINES, WATER PIPING, NATURAL GAS PIPING AND FITTINGS. ALL TEST SHALL BE PERFORMED TO MEET THE REQUIREMENTS OF THE APPLICABLE BUILDING CODE.
 - ALL WATER LINES SHALL FULLY DISINFECTED UPON THE FINAL COMPLETION OF THE PROJECT, AND BEFORE CERTIFICATE OF OCCUPANCY AND TURN OVER TO THE OWNER.
 - ALL DRAINS SHALL HAVE A TRAP PRIMER OR EQUIVALENT NECESSARY TO KEEP THE INTEGRITY OF THE PLUMBING TRAP.

- MECHANICAL GENERAL NOTES**
- THE MECHANICAL SYSTEM SHALL BE DESIGNED BY A LICENSED MECHANICAL CONTRACTOR/ DESIGNER AND SHALL MEET ALL THE REQUIREMENTS OF THE 2015 IRC, IMC AND IECC. THE MECHANICAL CONTRACTOR IS RESPONSIBLE FOR THE COMPLETE MECHANICAL SYSTEM INSTALLATION AND SHALL PROVIDE A (1) ONE YEAR WARRANTY BEGINNING FROM THE TIME OF CERTIFICATE OF OCCUPANCY. THE CONTRACTOR IS RESPONSIBLE TO PROVIDE THE OWNER COMPLETE OPERATION AND MAINTENANCE MANUALS. THE CONTRACTOR SHALL ALSO SET UP A TIME TO PROVIDE COMPLETE TRAINING OF THE SYSTEM TO THE OWNER.
 - THE MECHANICAL CONTRACTOR IS RESPONSIBLE TO VISIT THE JOB SITE AND BECOME FAMILIAR WITH ALL EXISTING CONDITIONS PRIOR TO STARTING THE WORK. THE MECHANICAL CONTRACTOR MUST ALSO PROVIDE NOTIFICATION TO THE ARCHITECT AND CONTRACTOR OF CONDITIONS THAT MAY BE DIFFERENT THAN EXPECTED DURING BIDDING.
 - ALL LINE VOLTAGE AND LOW VOLTAGE CONTROL WIRING SHALL BE RAN, INSTALLED AND CONNECTED BY THE MECHANICAL CONTRACTOR OR THE MECHANICAL CONTRACTOR SHALL CONTRACT THE SCOPE OF WORK.
 - ALL EQUIPMENT SPECIFICATIONS SHALL BE SUBMITTED TO THE ARCHITECT FOR REVIEW. THE CONTRACTOR MUST PROVIDE THE DOCUMENTATION THAT IT MEETS THE REQUIREMENTS OF THE ENERGY LEVELS BEING ACHIEVED WITHIN THIS BUILDING.
 - THE MECHANICAL CONTRACTOR SHALL REVIEW AND COORDINATE WITH THE DRAWINGS FOR LOCATIONS OF ALL MECHANICAL ZONES.
 - EXHAUST FANS WHERE SHOWN ON EITHER THE MECHANICAL OR ELECTRICAL PLANS SHALL BE SIZED FOR A MINIMAL RATE OF 50 CFM. ALL FANS SHALL BE HARD DUCTED WITH RIGID DUCT (NO FLEX DUCT SHALL BE ALLOWED), AND DIRECTED DIRECTLY TO THE EXTERIOR OF THE BUILDING IN A SOFFIT OR SIDE WALL. THE TERMINATION OF ALL EXHAUST FANS SHALL BE A MINIMUM OF 10'-0" AWAY FROM ANY OPERABLE WINDOW. TERMINATIONS SHALL BE INSTALLED AS NOT TO BE BLOCKED BY SNOW AND ICE. FANS SHALL BE A DIRECT DRIVE CENTRIFUGAL UNIT WITH SLOW SPEED MOTOR. PROVIDE AN ACOUSTICAL INSULATION, GRIPS, CAPS, ETC. AS REQUIRED.
 - ALL GRILLS AND REGISTERS MUST BLEND TO THE ADJACENT FINISH, AND SHALL BE PROVIDED TO MEET THE REQUIREMENTS FOR THE FLOW RATE AS PER THE CFM REQUIREMENTS. ALL GRILLS SHALL BE EITHER PAINTED FOR METAL FINISH SELECTED.
 - WATER HEATERS
 - THE REQUIRED NUMBER OF WATER HEATERS ARE SHOWN ON THE MECHANICAL PLANS. ALL WATER HEATERS SHALL BE 90% OR BETTER HIGH EFFICIENCY WATER HEATERS WITH RAPID RECOVERY. ALL WATER HEATERS SHALL BE INSTALLED WITH SEISMIC ANCHORING, AS PER DETAILS.
 - ALL WATER HEATERS SHALL BE VENTED TO THE EXTERIOR.
 - THE CONTRACTOR SHALL PROVIDE A FLOOR DRAIN WHETHER SHOWN OR NOT AT THE BASE OF ALL WATER HEATERS. THE FLOOR DRAIN MUST BE LOCATED, AND THE FLOOR MUST SLOPE TOWARD THE DRAIN IN A POSITIVE FLOW.
 - GAS FIRED FURNACES
 - THE REQUIRED NUMBER OF GAS FIRED FURNACES SHALL BE PER THE MECHANICAL DESIGNER/ ENGINEER. THE LOCATION IS SHOWN ON THE MECHANICAL DRAWINGS WHERE THE LOCATIONS ARE PROVIDED FOR THE GAS FIRED FURNACES.
 - THE GAS FIRED FURNACES SHALL BE A MINIMUM OF 90% OR BETTER HIGH EFFICIENCY FURNACE. THE EXACT SIZE OF EACH OF THESE UNITS SHALL BE PER THE MECHANICAL DESIGNER/ ENGINEER.
 - THE VENTING OF EACH GAS FIRED FURNACE SHALL BE PVC PIPE AND SHALL BE LOCATED AWAY FROM THE MAIN ENTRIES OF THE BUILDING, AND WINDOW LOCATIONS. COORDINATE THE EXACT LOCATION WITH THE OWNER AND ARCHITECT.
 - THE CONTRACTOR SHALL PROVIDE A FLOOR DRAIN BY THE GAS FIRED FURNACES FOR THE UNIT CONDENSATE LINES.
 - GAS FIRED BOILERS
 - THE REQUIRED NUMBER OF GAS FIRED BOILERS SHALL BE PER THE MECHANICAL DESIGNER/ ENGINEER. THE LOCATION IS SHOWN ON THE MECHANICAL DRAWINGS WHERE THE LOCATIONS ARE PROVIDED FOR THE GAS FIRED BOILERS.
 - THE GAS FIRED BOILER SHALL BE A MINIMUM OF 90% OR BETTER HIGH EFFICIENCY FURNACE. THE EXACT SIZE OF EACH OF THESE UNITS SHALL BE PER THE MECHANICAL DESIGNER/ ENGINEER.
 - THE VENTING OF EACH GAS FIRED BOILER SHALL BE PVC PIPE AND SHALL BE LOCATED AWAY FROM THE MAIN ENTRIES OF THE BUILDING, AND WINDOW LOCATIONS. COORDINATE THE EXACT LOCATION WITH THE OWNER AND ARCHITECT.
 - THE CONTRACTOR SHALL PROVIDE A FLOOR DRAIN BY THE GAS FIRED BOILER FOR THE UNIT CONDENSATE LINES.
 - DUCTWORK
 - ALL DUCTWORK SHALL BE 24 GA. MINIMUM RIGID DUCT AND SHALL BE FULL SEALED AT EACH JOINT LOCATION. NO FLEXIBLE DUCT IS ALLOWED WITHIN THE INSTALLATION.
 - ALL DUCTWORK IN CEILINGS OF UNHEATED ROOM OR UNDER SLAB SHALL BE INSULATED DUCT WORK. ALL DUCTWORK WITHIN THE HEATING ENVELOPE OF THE STRUCTURE DOES NOT REQUIRE TO BE INSULATED, UNLESS SPECIFICALLY NOTED.
 - ALL DUCTWORK SHALL BE IN THE SPACE ALLOCATED, AND SHALL NOT BE DROPPED BELOW FLOOR JOISTS, UNLESS NOTED ON DRAWINGS, OR PREVIOUSLY APPROVED BY THE ARCHITECT AND OWNER.

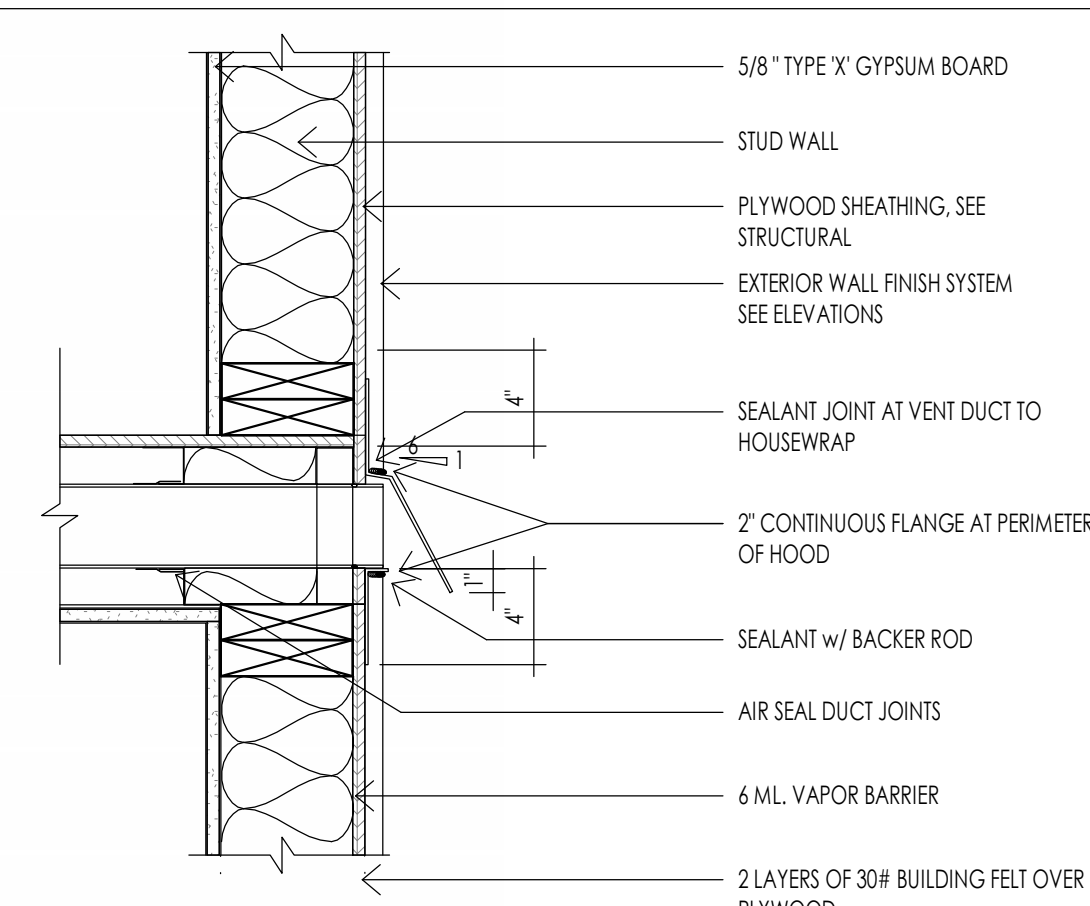


- SILL FLASHING**
9 INCH MIN. WIDE SELF-ADHERED SELF-HEALING RUBBERIZED ELASTOMERIC ASPHALT FLASHING MEMBRANE INSTALLED A MIN. 9 INCHES BEYOND ROUGH OPENING
-DO NOT OVERLAP THE TOP OF SILL FRAMING
-ADHERE ONLY AT TOP EDGE. LEAVE UNATTACHED AT BOTTOM SO THAT THE PAPER CAN BE INSTALLED UNDERNEATH
- METAL EXTERIOR VENT**
24 GA. MIN. GALV. SHEET METAL VENT MUST BE INSTALLED OVER SILL FLASHING. INSTALL JAMB FLASHING OVER OR UNDER NAILING FLANGE. SET VENT IN A CONTINUOUS BED OF SEALANT.
- JAMB FLASHING**
9 INCH MIN. WIDE SELF-ADHERED SELF-HEALING RUBBERIZED ELASTOMERIC ASPHALT FLASHING MEMBRANE FLASHING INSTALLED OVER AND BELOW SILL FLASHING AND ABOVE TOP OF FUTURE HEAD FLASHING. -DO NOT FASTEN THE BOTTOM 9 INCHES OF THE JAMB FLASHING SO THE WEATHER-RESISTANT BARRIER APPLIED LATER MAY BE SUPPLIED UNDERNEATH THE FLASHING IN A WEATHERBOARD FASHION.
- HEAD FLASHING**
APPLY SELF-ADHERED SELF-HEALING RUBBERIZED ELASTOMERIC ASPHALT FLASHING MEMBRANE OVER DRYER VENT FLANGE. EXTEND HEAD FLASHING BEYOND EACH JAMB FLASHING.

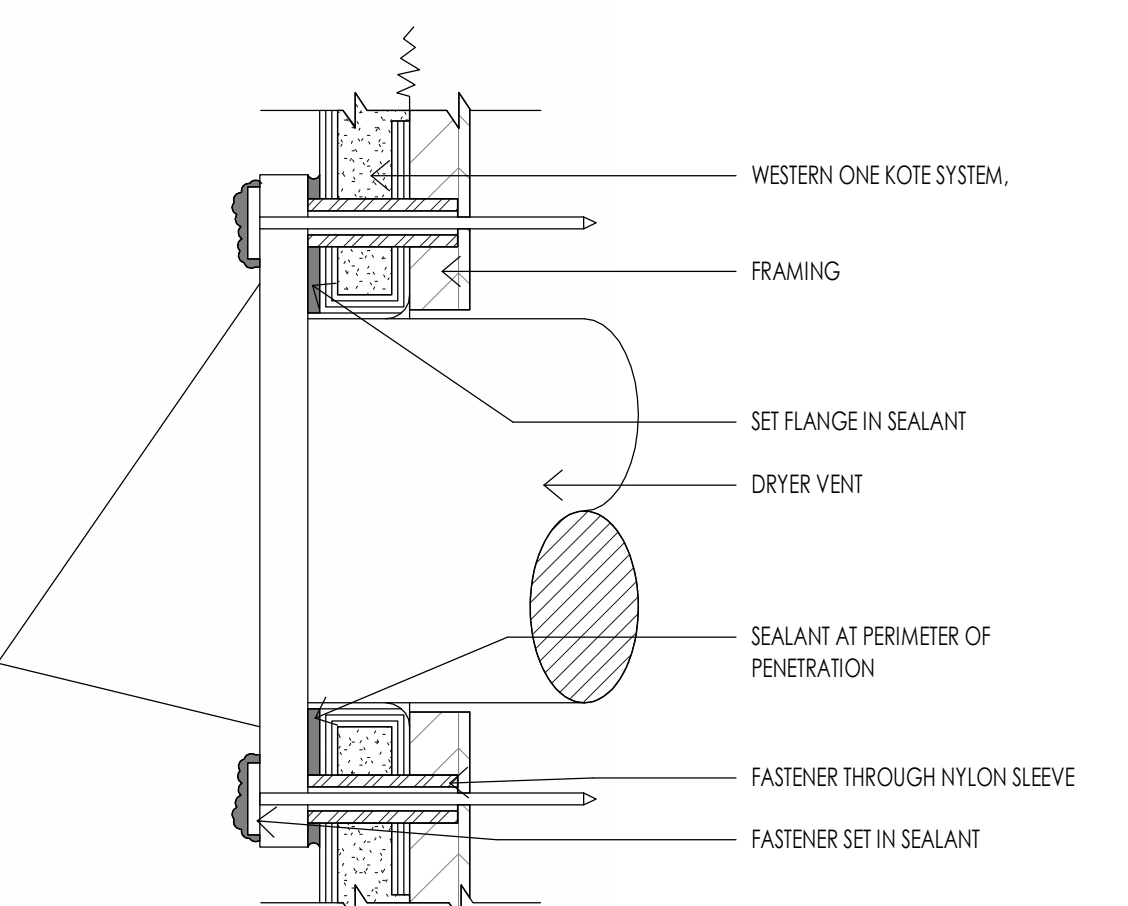
EXHAUST VENT DETAIL 1
1 1/2" = 1'-0"



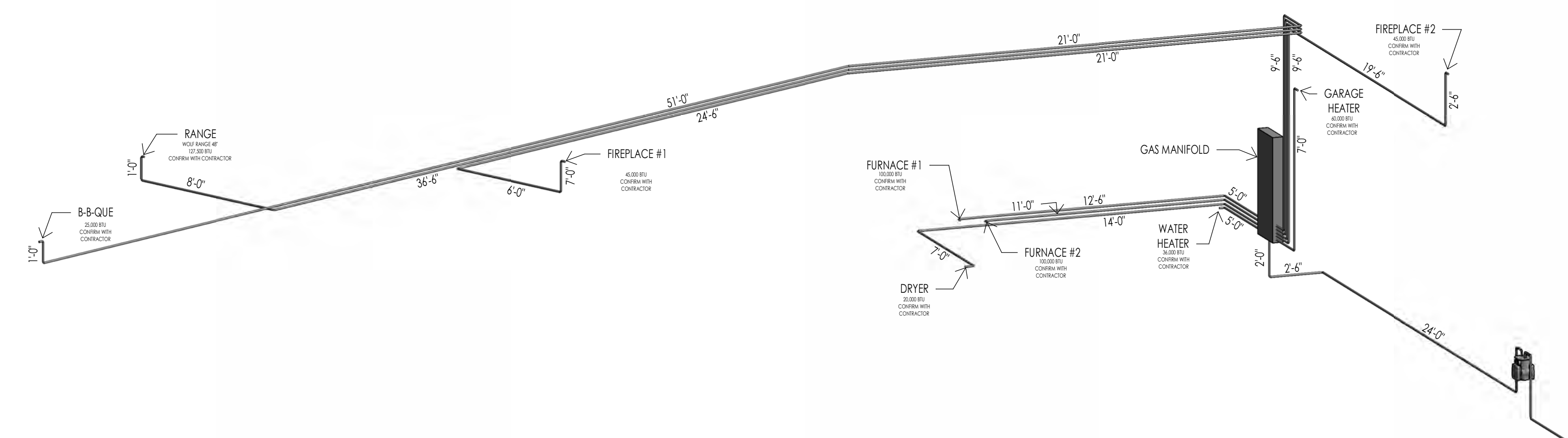
MINIMUM PLUMBING FIXTURE CLEARANCES 2
1/8" = 1'-0"



WALL EXHAUST VENT 3
1 1/2" = 1'-0"



TYPICAL DRYER VENT (SHOWN WITH STUCCO) 4
6" = 1'-0"



GAS SCHEMATIC 5
M101



Architecture
Interior Design
Landscape Architecture
Land Planning
Construction Management

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WARM SPRINGS RESIDENCE #35
190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

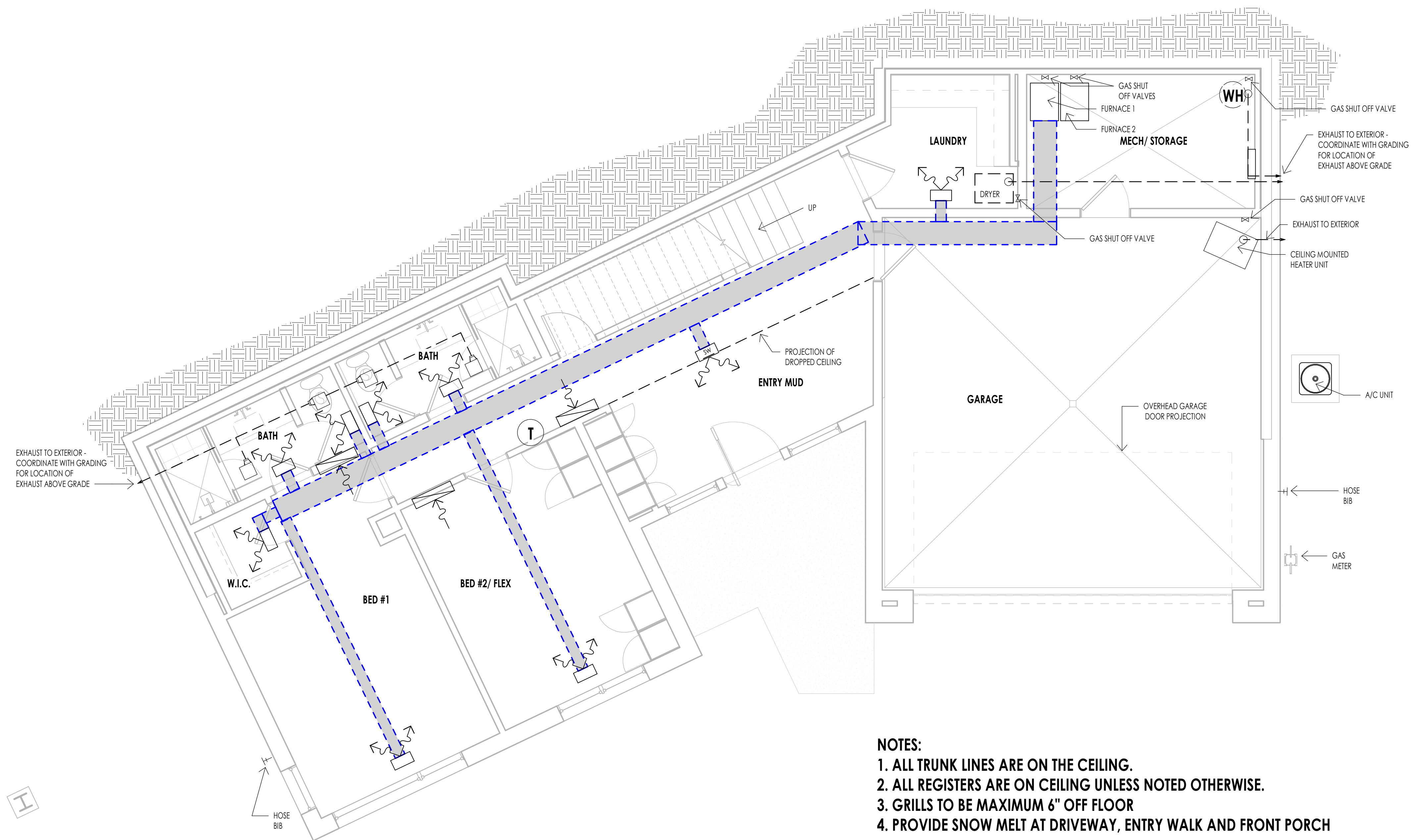
PROJECT NO. 22023
DATE: 2023.06.30

REVISIONS:

SHEET TITLE:
MECHANICAL GENERAL NOTES

SHEET NUMBER:
M101

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- NOTES:**
1. ALL TRUNK LINES ARE ON THE CEILING.
 2. ALL REGISTERS ARE ON CEILING UNLESS NOTED OTHERWISE.
 3. GRILLS TO BE MAXIMUM 6" OFF FLOOR
 4. PROVIDE SNOW MELT AT DRIVEWAY, ENTRY WALK AND FRONT PORCH

LEVEL 1 - MECHANICAL PLAN
1/4" = 1'-0"

MECHANICAL LEGEND	
SYMBOL	TYPE
	FLOOR OR CEILING MOUNTED HVAC REGISTER
	SW = SIDE WALL T.K. = TOE KICK
	HVAC RETURN AIR REGISTER
	HOSE BIB
	GAS SHUT OFF VALVE
	EXHAUST FAN
	THERMOSTAT
	WATER HEATER

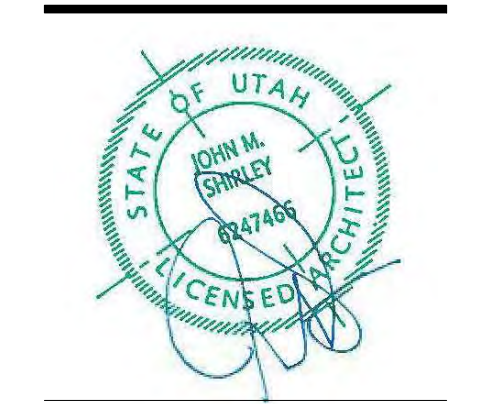
- MECHANICAL GENERAL NOTES**
1. SEE SHEETS A0.3 FOR MECHANICAL AND PLUMBING PROJECT KEY NOTES AND MECHANICAL/PLUMBING INFORMATION.
 2. MECHANICAL AND PLUMBING LAYOUTS ARE SHOWN IN SCHEMATIC. THE PLUMBING AND MECHANICAL CONTRACTORS ARE RESPONSIBLE TO DESIGN AND SIZE EQUIPMENT CAPACITY, PIPE AND DUCT LINES, PLUMBING LINES AND ALL OTHER EQUIPMENT AS PER NATIONAL, STATE AND LOCAL CODES AND AS PER THE GENERAL NOTE REQUIREMENTS.
 3. THE CONTRACTOR IS RESPONSIBLE TO COORDINATE THE LAYOUT AND INSTALLATION OF ALL RELATED ITEMS WITH EXISTING CONDITIONS AND ALL OTHER TRADES.
 4. COORDINATE WITH OWNER, INTERIOR DESIGNER AND/OR PLANS FOR FIXTURE SCHEDULES, STYLES, FINISHES, ETC.
 5. ALL REGISTERS AT LOWER LEVEL TO BE CEILING MOUNT UNLESS OTHERWISE NOTED.
 6. COORDINATE BETWEEN MECH. SUB AND ELECTRICAL SUB AT PRECONSTRUCTION MEETING FOR DUCT LOCATIONS AND RECESSED CAN LOCATIONS.
 7. ALL PLUMBING FIXTURE/MECHANICAL EQUIPMENT SELECTIONS TO BE APPROVED BY OWNER/DEVELOPER.
 8. PROVIDE REQUIRED COMBUSTION AIR VENT DUCTS AT CEILING FOR WATER HEATER AND FURNACE AS REQUIRED BY BLDG. CODES AND MANUFACTURER.
 9. MECHANICAL DESIGN SHOULD BE IN ACCORDANCE WITH 2006 INTERNATIONAL RESIDENTIAL CODE.
 10. DUCT PENETRATIONS IN GARAGES SHALL BE 26 GAUGE SHEET METAL MIN. AND SHALL HAVE NO OPENINGS INTO THE GARAGE.
 11. FLUES SHALL NOT PENETRATE THE ROOF WITHIN 4'-0" OF PARTY WALLS.
 12. RADON: THE MECHANICAL CONTRACTOR IS RESPONSIBLE TO PROVIDE RADON TESTING AND APPLY AN APPROPRIATE MITIGATION SYSTEM.



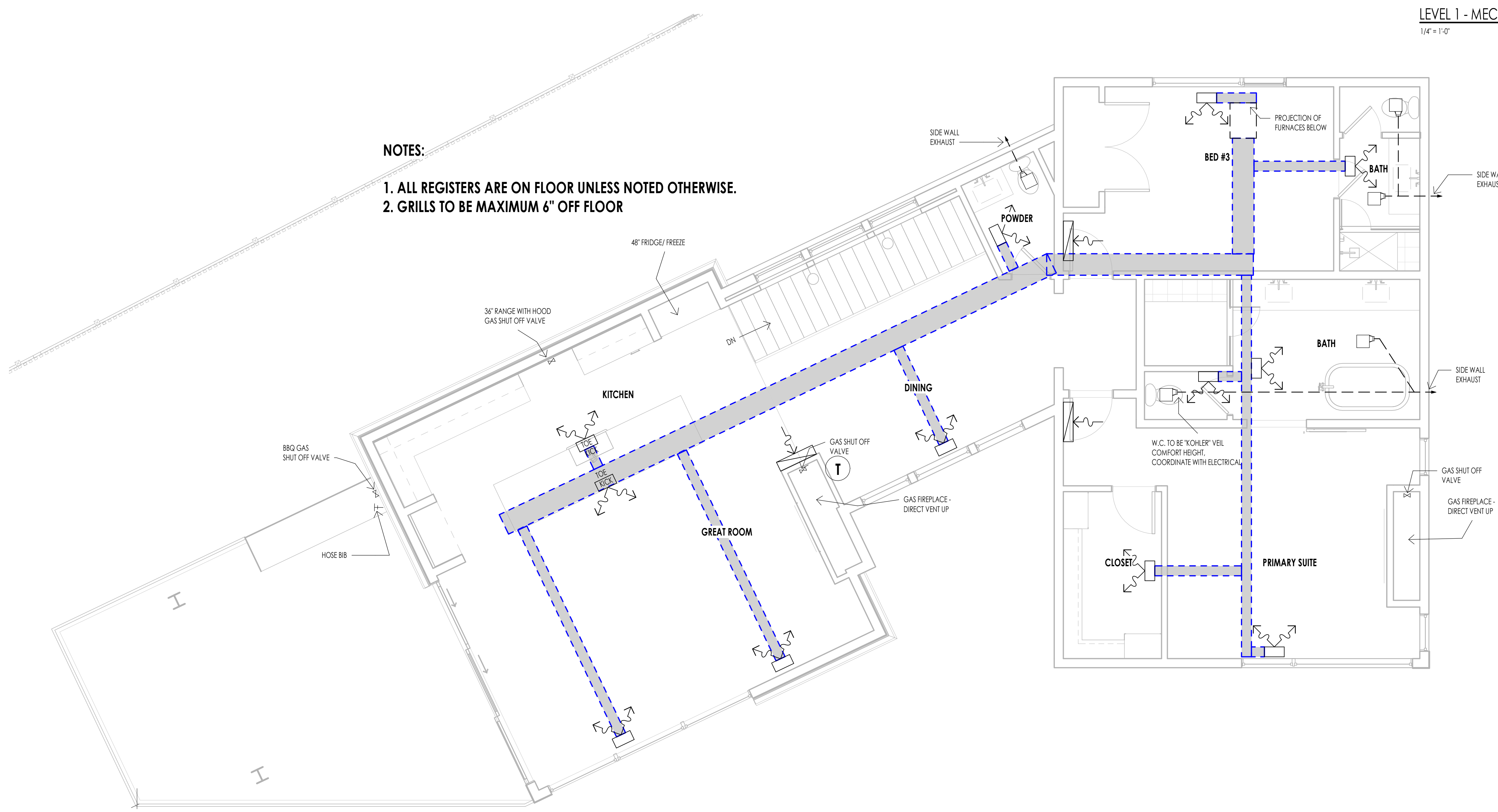
Architecture
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- NOTES:**
1. ALL REGISTERS ARE ON FLOOR UNLESS NOTED OTHERWISE.
 2. GRILLS TO BE MAXIMUM 6" OFF FLOOR



LEVEL 2 - MECHANICAL PLAN
1/4" = 1'-0"

WARM SPRINGS RESIDENCE #35
190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

PROJECT NO. 22023
DATE: 2023.06.30
REVISIONS:

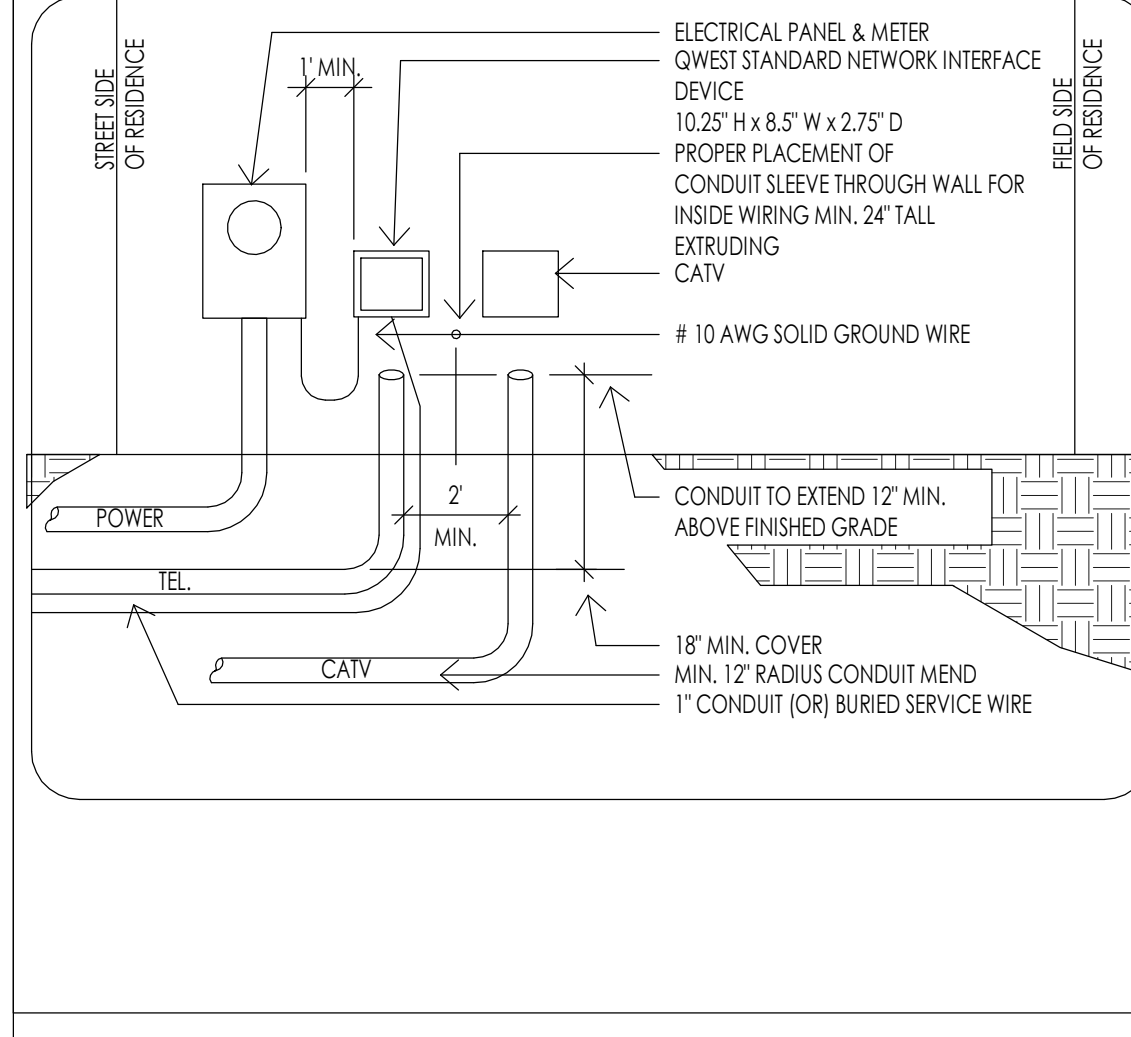
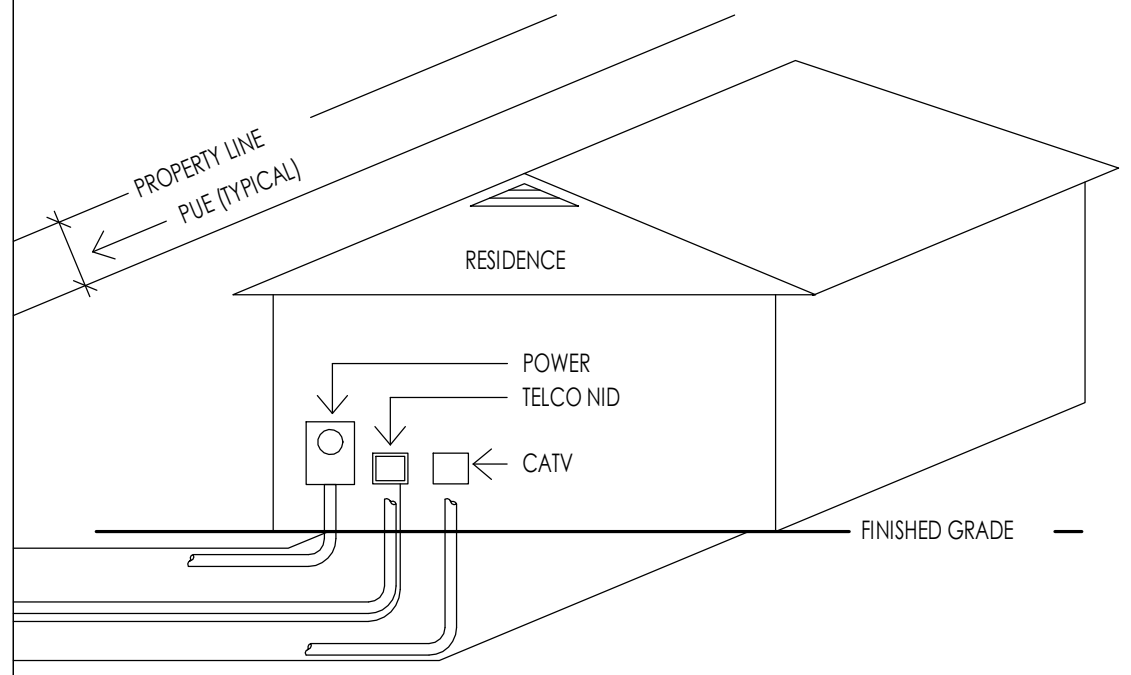
SHEET TITLE:
MECHANICAL PLAN

SHEET NUMBER:
M102

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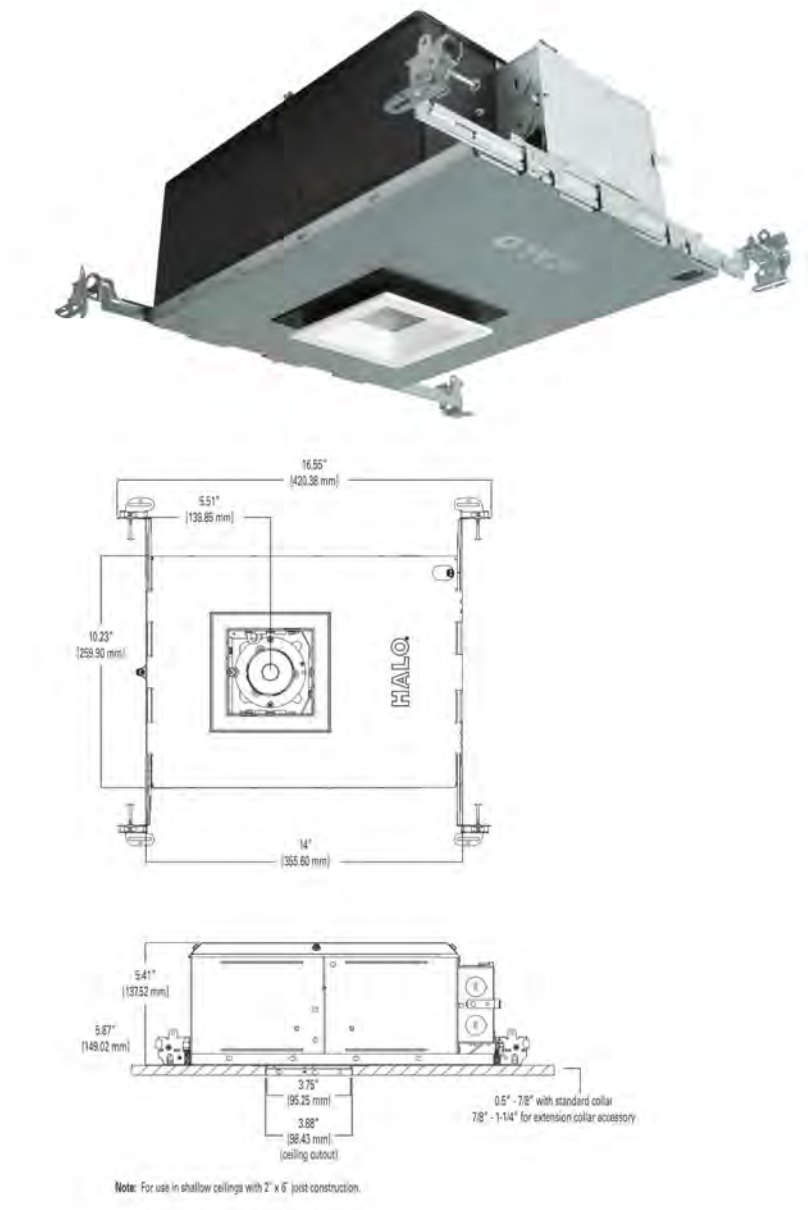
ELECTRICAL GENERAL NOTES

- ALL WORK DONE BY ELECTRICAL CONTRACTOR SHALL COMPLY WITH THE CURRENT ADOPTED EDITION OF THE NATIONAL ELECTRICAL CODE AND ALL LOCAL CODE REGULATIONS AND AMENDMENTS. THE CONTRACTOR SHALL PERFORM ALL WORK IN CONFORMANCE WITH THESE REGULATIONS WHETHER OR NOT SUCH WORK IS SPECIFICALLY SHOWN ON THE DRAWINGS.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO FURNISH AND INSTALL FEEDERS, PANELS, BOARDS, RELAY BRANCH CIRCUIT WIRING, CONDUITS, WIRE, METER BASES, COMPLETE WIRING FOR MOTORS, EXHAUST FANS, LINE VOLTAGE CONNECTIONS FOR HVAC EQUIPMENT, SPECIALTY LIGHTING - FIXTURES, OUTLET BOXES, COVER PLATES, WALL SWITCHES, FIXTURES, RECEPTACLES, ETC.
- ALL DRAWINGS INDICATE LOCATIONS AS DIAGRAMMATIC. LOCATIONS SHALL BE PER APPROPRIATE CODES AND OWNER. CONTRACTOR TO COORDINATE WITH MECHANICAL CONTRACTOR FOR ALL POWER REQUIREMENTS.
- THE CONTRACTOR SHALL SET ALL THE BOXES AND NOTIFY THE ARCHITECT AND OWNER OF PLACEMENT OF BOXES. THE ARCHITECT, OWNER AND INTERIOR DESIGNER SHALL WALK THE HOUSE WITH THE ELECTRICAL CONTRACTOR AND SHALL VERIFY ALL THE LOCATIONS. THIS SHALL BE DONE PRIOR TO ANY WIRE BEING PULLED.
- IF WIRE IS PULLED, AND BOXES ARE REQUIRED TO BE MOVED, ALL COSTS SHALL BE THE RESPONSIBILITY OF THE ELECTRICAL CONTRACTOR AND NOT THE OWNER/ DESIGN TEAM.
- ELECTRICAL SERVICE CAPACITY AND SIZE SHALL BE COMPUTED BY METHOD INDICATED IN THE NATIONAL ELECTRICAL CODE. PANELS OR CABINETS INCLUDING FUSES, CIRCUIT BREAKERS, SWITCHES OR OTHER ELECTRICAL SERVICE EQUIPMENT SHALL BE IN AN INCONSPICUOUS, ACCESSIBLE AND PROTECTED LOCATION. ELECTRICAL PANEL CLEARANCE TO BE MINIMUM 30" WIDTH AND 6" HEAD ROOM. ELECTRICAL TO COMPLY WITH N.E.C. 110.16. ELECTRICAL METER BASE SHALL BE LOCATED IN AN AREA THAT IS PROTECTED FROM OUTSIDE WEATHER.
- ALL RECEPTACLES LOCATED WITH THE FOLLOWING CONDITIONS TO BE GFCI PROTECTED: ALL KITCHEN COUNTERS, IN BATHROOMS, OUTSIDE AT GRADE LEVEL, UNFINISHED BASEMENTS, AND IN GARAGES. GARAGE RECEPTACLES TO BE 18" ABOVE FINISHED FLOOR.
- ALL SWITCHES, RECEPTACLES, TELEPHONE JACKS AND CATV JACKS TO BE "LEVITON" 5601 ROCKER SERIES IN WHITE. DIMMER SWITCHES TO BE "LUTRON" DIVA ROCKER SERIES IN WHITE. HEIGHT OF LIGHT SWITCHES FROM FINISHED FLOOR TO TOP OF SWITCH TO BE 48" UNLESS NOTED. OTHERWISE, THE MOUNTING FROM THE FINISH FLOOR TO THE CENTER OF OUTLETS INCLUDING TELEPHONE, CATV, ETC. SHALL BE 12" TYPICAL. AT DESKS AND OTHER SURFACES THE OUTLETS SHALL BE 10" TO CENTERLINE ABOVE SURFACE. SWITCHES, OUTLETS, TELEPHONE, CATV, ETC. LOCATIONS SHALL BE APPROVED PRIOR TO COMMENCEMENT OF WIRING.
- UNLESS NOTED OTHERWISE LOCATE AND INSTALL ONE (1) GFCI WEATHER PROTECTED RECEPTACLE AT GRADE LEVEL AND OUTSIDE AT SOFFIT AT EACH EXTERIOR DOOR WHETHER INDICATED ON DRAWINGS OR NOT. PLEASE REFER TO THE ELECTRICAL DRAWINGS FOR ADDITIONAL OUTLETS AT SOFFITS.
- ALL FIXTURES SHALL HAVE A U.L. LABEL LISTING. IF NOT U.L. LISTED FIXTURE SHALL NOT BE USED. ALL RECESS DOWN LIGHTS TO BE THERMAL RATED, AND ALL CAST IN PLACE FIXTURES TO BE INCLUDED IN BASE BID. ALL RECESS DOWN LIGHTS TO BE INCLUDED IN BASE BID WITH TRIM RINGS AS SELECTED BY DESIGNER OR OWNER. ALL LIGHTS IN CLOSETS SHALL MEET N.E.C. 410.8 REQUIREMENTS. ALL LIGHTS LOCATED IN WET OR DAMP LOCATIONS SHALL MEET N.E.C. 410.4 REQUIREMENTS.
- SMOKE DETECTORS TO BE HARD WIRED TO BUILDING CIRCUIT WITH BATTERY BACK UP. PROVIDE SMOKE DETECTORS AT ALL BUILDING LEVELS, IN ALL BEDROOMS, ACCESS TO ALL BEDROOMS, ETC. (UBC 310.9)
- ELECTRICAL PANEL (PANELBOARD/SWITCHBOARD) MAY NOT BE LOCATED BEHIND A DOOR OR IN A ROOM THAT MAY BE LOCKED AND MUST HAVE PROPER WORKING CLEARANCES. PLEASE REFER TO THE ELECTRICAL DRAWINGS FOR THE LOCATIONS FOR ALL ELECTRICAL PANELS. IF THE PANEL BOARD NEEDS TO BE RELOCATED, PLEASE CONSULT THE OWNER AND OR ARCHITECT PRIOR TO MOVING.
- SMALL WALL SECTIONS 2' OR WIDER (INCLUDES BETWEEN DOORS) REQUIRE AN OUTLET.
- GFCI PROTECTION MUST BE PROVIDED FOR ANY RECEPTACLE OUTLET IN THE FOLLOWING: A BATHROOM, ANY COUNTERTOP KITCHEN/LAUNDRY, GARAGE OUTLETS MINIMUM 18" ABOVE FINISHED FLOOR HEIGHT, OUTSIDE FRONT AND REAR OUTLETS MUST HAVE WATERPROOF COVERPLATE.
- A RECEPTACLE OUTLET MUST BE PROVIDED AT EACH SECTION OF KITCHEN COUNTERTOP 12" OR WIDER. THERE MUST ALSO BE A MINIMUM OF TWO (2) DEDICATED COUNTERTOP CIRCUITS.
- A SWITCH CONTROLLED LIGHT MUST BE PROVIDED AT HALLWAYS, STAIRWAYS, EXITS, AND EACH ROOM.
- A HARD-WIRED WITH BATTERY BACKUP SMOKE DETECTOR MUST BE INSTALLED IN ALL BEDROOMS (NEW AND EXISTING) IN THE ACCESS AREA TO ALL BEDROOMS, AND AT LEAST ONE PER FLOOR. TWO (2) FOOT CHANGES IN CEILING HEIGHT ALSO REQUIRE AN ADDITIONAL SMOKE DETECTOR. ALARM SOUND MUST BE AUDIBLE IN ALL AREAS OF HOME.
- WHEN BEDROOMS OCCUR ON 2ND STORES, THE DETECTOR SHOULD BE LOCATED AT THE TOP OF THE STAIRWAY.
- KITCHEN OUTLETS REQUIRED TO BE GFCI PROTECTED, NOT MORE THAN 4'-0" APART.
- CLOSET LIGHT FIXTURES MIN. 12" CLEARANCE TO SHELF (LATERAL MEASURED)



TYPICAL DRY UTILITY LOCATION DETAIL
1/2" = 1'-0"

HL36SA



Ordering Information

SAMPLE ORDER NUMBERS: HL36SA02P270010CAT, TRIMFL40, TL3390W

Series:
HL36SA - 3-inch square shallow new construction directional housing

Options:
N1 = 10W (nominal)
N2 = 15W (nominal)
N3 = 20W (nominal)

Beam Spread:
(blank) = Purchase optic separately
SP = 10° beam
NFL = 27° beam
FL = 40° beam
WFL = 55° beam

Light Wavelength:
27K = 2700K
30K = 3000K
35K = 3500K
40K = 4000K
50K = 5000K
65K = 6500K

Input/Output:
E010 = 120V-277V, 50/60Hz leading or trailing edge phase cut 1% dimming at 120V only and UNV
120/277V 50/60Hz 0-10V 1% dimming
TL33 = 1-Lumen/ft Same Forward Phase Dimming, 1% to 100%, 120V-277V
DE010 = 0-10V Dimming, 0% to 100%, 120V-277V

Housing Type:
ICAT+ insulation contact and airtight

Accessories:
RAC2+ = remote adapter for HL36SA housings and trims
CES3+ = collar adapter for HL36SA housings, adjusts from 7/16" to 1-1/4" thick ceilings

Optics & Media:
TRIMSP15 = 15° beam
TRIMSP15+ = 15° beam
TRIMSP40 = 40° beam
TRIMSP40+ = 40° beam
TRIMSP55 = 55° beam
TRIMSP55+ = 55° beam
TRIMSP20PC = replacement media holder, package of 12
L100 Series = 2" trim and filters, see spec sheet
Overcast Trim Ring
OTL300W = overcast trim ring for TL33 trims

See page 18 for trim information.
© 2019 Luminaire - 100 and 150

L-1 RECESSED EXTERIOR SOFFIT LIGHT



CHAMPEAUX LINEAR SCONCE

Starting at \$275. Member \$293. Member

Designer Jonathan Browning's Champeaux collection brings refinement and low materials to the classic sconce, a standard of 1970s utility. Crafted in solid brass, Browning's thoughtful reimagining gives beyond function to reveal the cast as a thing of beauty. The design evokes the clean lines inherent in the concept, but sets the form in a new context.

SHOP THE ENTIRE COLLECTION

FINISH OPTIONS

Polished Chrome | Lacquered Burnished Brass | Bronze

DETAILS

DIMENSIONS

INSTALLATION & ELECTRICAL

MAXIMUM OF 2.78K COLOR TEMPERATURE
MAXIMUM OF 700 LUMENS
CAP TOP DOWNLIGHT ONLY

CHAT WITH A DESIGNER WE'RE HERE TO HELP



L-2 DECORATIVE EXTERIOR WALL SCONCE

LED RECESSED STRIP TAPE LIGHT

SPECIFY YOUR PRODUCT

Use our tool below to create a custom product code and view product compatibility. After making the selections, save the product to a project to keep all your products and specs together in one organized place. Once final, you can order from a nearby distributor.

Your configuration is complete!

Product Code: UNI-TL-O-100-27K-24V-NA

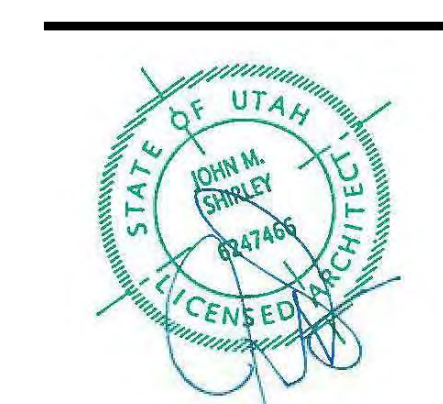
Make a selection in each category to complete your product code.

1. Series	2. Location	3. Output*	4. Temp/Color*	5. Voltage	6. Modifications
UNI-TL - Uniform Thin Line	O - Outdoor	100 - 100 lm/ft 200 - 200 lm/ft 300 - 300 lm/ft	18K - 1800K 22K - 2200K 24K - 2400K 27K - 2700K 30K - 3000K 35K - 3500K 40K - 4000K 50K - 5000K 65K - 6500K	24V - 24V DC	NA - None

* Customizable—Consult Factory

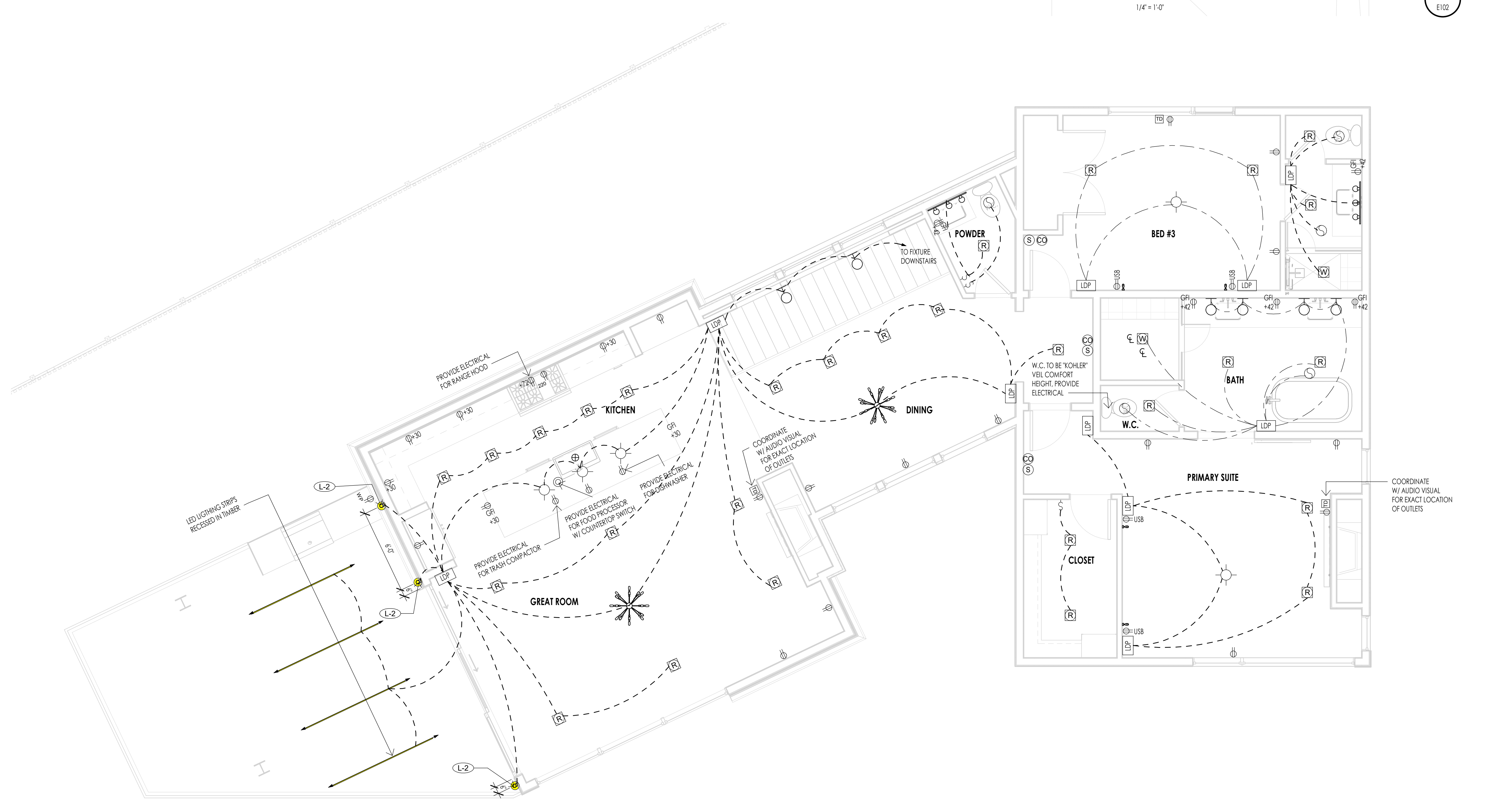
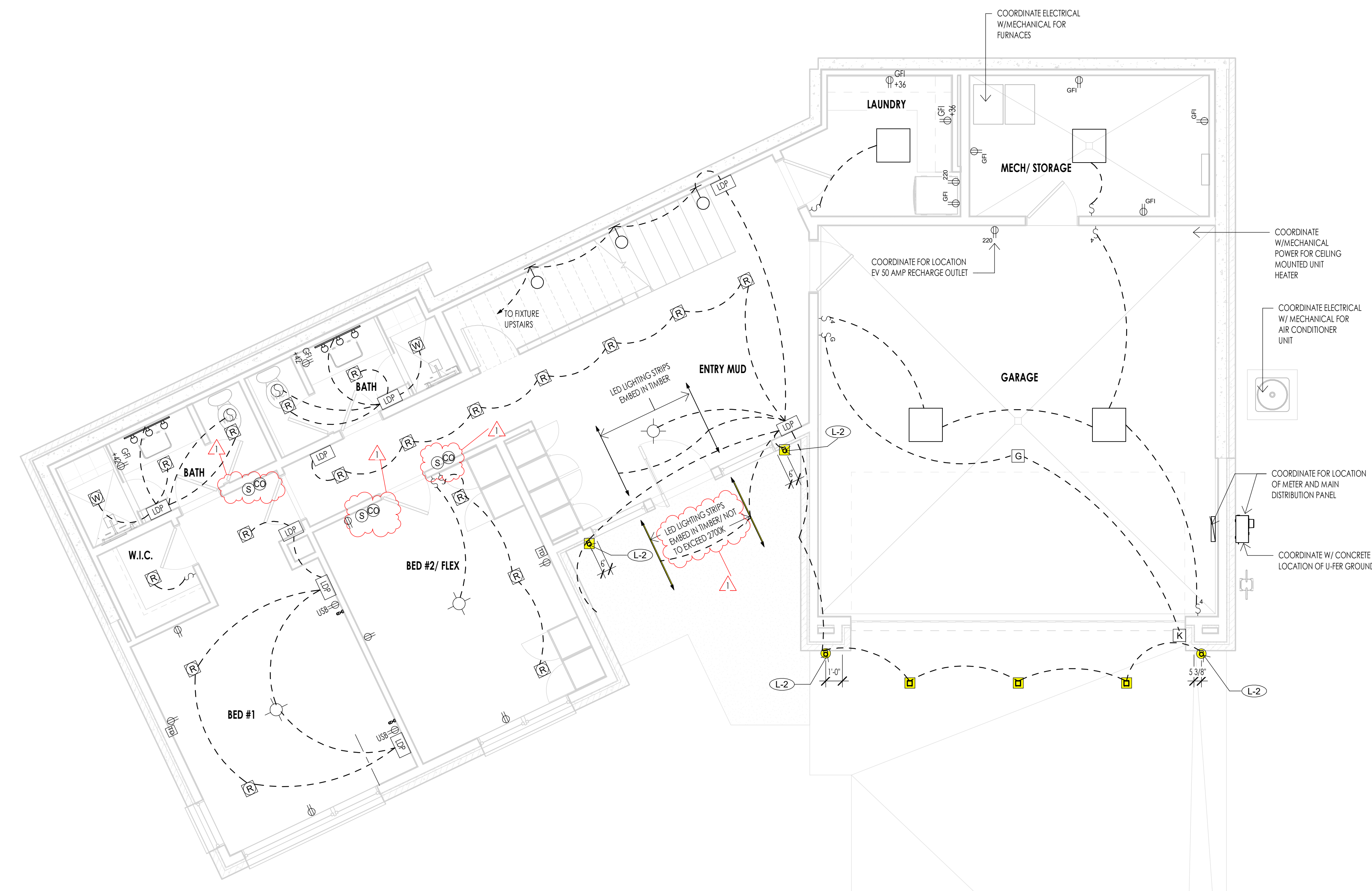
L-6 MOTORCOURT EXTERIOR LIGHTING

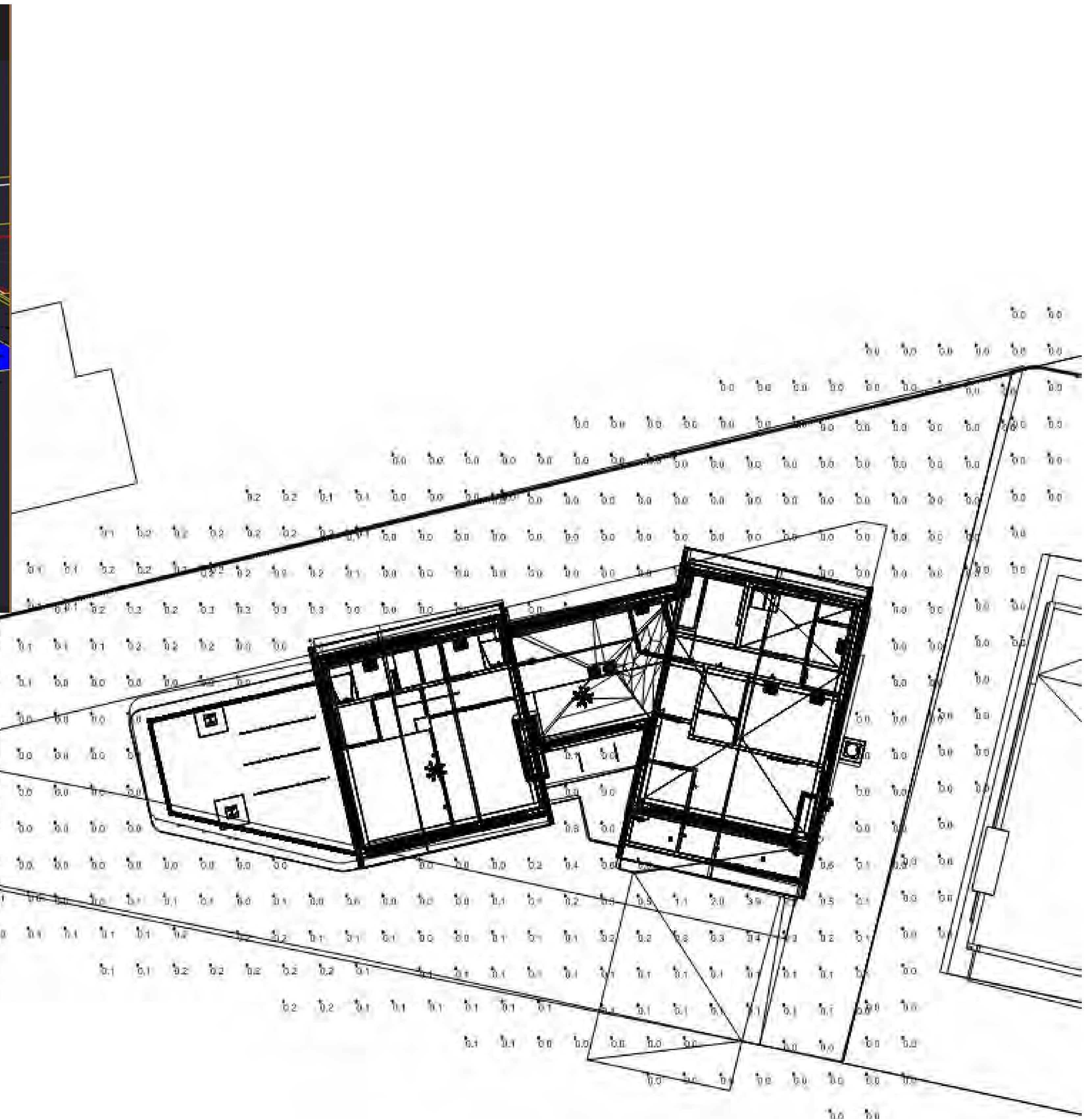
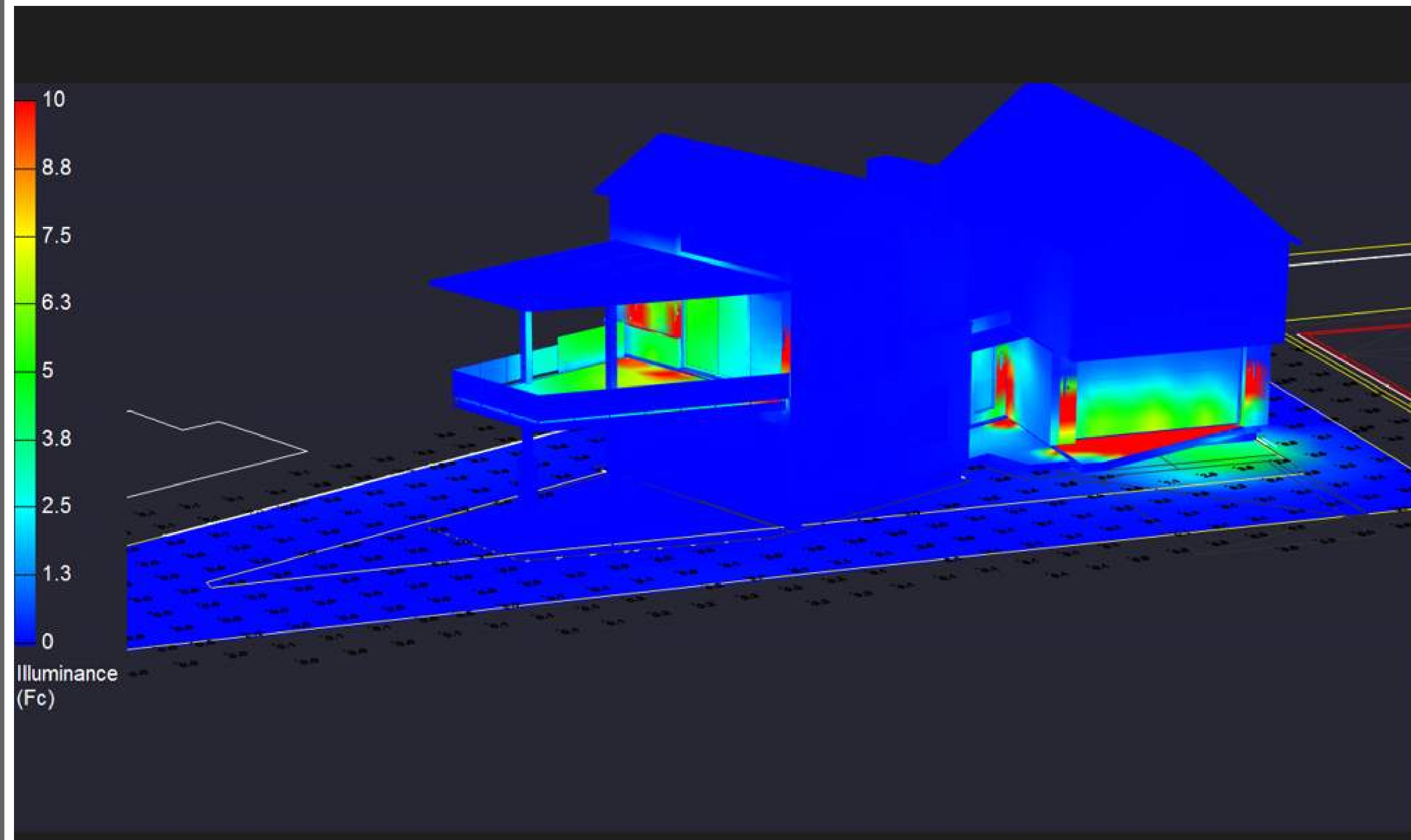




ELECTRICAL LEGEND	
SYMBOL	DESCRIPTION
S	SINGLE POLE TOGGLE SWITCH
3S	THREE WAY TOGGLE SWITCH
4S	FOUR WAY TOGGLE SWITCH
S ^G	GARAGE DOOR OPENER
S ^D	DIMMER TOGGLE SWITCH
Ⓛ	110 V DUPLEX OUTLET ON AN (AFP) ARC FAULT PROTECTED CIRCUIT
Ⓛ ^{GFI}	110 V GROUND FAULT INTERRUPTER
Ⓛ ^{WFP}	110 V WATERPROOF GFI OUTLET
Ⓛ ²²⁰	220 V OUTLET
Ⓛ ⁴	QUADRUPLUX OUTLET
Ⓛ ¹¹⁰	110 V FLOOR DUPLEX OUTLET
Ⓛ ^S	110 V SMOKE DETECTOR W/BATTI BACK-UP
Ⓛ ^{CM}	CARBON MONOXIDE DETECTOR
Ⓛ ^F	EXHAUST FAN
Ⓛ ^{FL}	EXHAUST FAN WITH LIGHT FIXTURE
Ⓛ ^R	4" LED RECESSED CAN (FITTURE & TRIM PER SCHEDULE)
Ⓛ ^C	4" LED RECESSED CAN (CLOSET FITTURE & TRIM PER SCHEDULE)
Ⓛ ^W	RECESSED CAN (WET LOCATION-FITTURE & TRIM PER SCHEDULE)
Ⓛ ^{CM}	CEILING MOUNT FIXTURE
Ⓛ ^B	BATHROOM WALL SCONCE
Ⓛ ^W	WALL MOUNT FIXTURE
Ⓛ ^{2X2}	2X2 OR 2X4 FLUORESCENT CEILING FIXTURE
Ⓛ ^{FL}	FLUORESCENT STRIP LIGHT
Ⓛ ^{UL}	LED UNDERCOUNTER LIGHTING
Ⓛ ^G	GARAGE DOOR OPENER
Ⓛ ^K	KEYLESS ENTRY
Ⓛ ^B	DOORBELL
Ⓛ ^T	TELEPHONE (CAT SE WIRING) SINGLE LINE UNLESS NOTED (NUMBER) DESIGNATES PORT OUTLETS REQUIRED
Ⓛ ^M	MULTI-MEDIA NETWORK OUTLET (CAT SE WIRE) W/4 PORT OUTLET
Ⓛ ^{SW}	STRUCTURED WIRING (FUTURE SMART WIRING) (E) R66 QUAD SHIELD, (S) CAT 6E WIRE - FOR CABLE TV, VIDEO, SATELLITE, ETC. (M) PORT OUTLET
Ⓛ [⊕]	GARBAGE DISPOSAL
Ⓛ ^{LV}	LOW VOLTAGE RECESSED CAN
Ⓛ ^{E1}	RECESSED EXTERIOR SOFFIT LIGHT - SEE SPECS ON SHEET E101
Ⓛ ^{E2}	DECORATIVE EXTERIOR WALL SCONCE - SEE SPECS ON SHEET E101
Ⓛ ^{E3}	MOTOR COURT EXTERIOR LIGHTING - SEE SPECS ON SHEET E101
Ⓛ ^{LDP}	LIGHTING DIGITAL PAD
Ⓛ [⊕]	DOOR BELL SWITCH
Ⓛ ^W	WALL MOUNTED BED LIGHT
Ⓛ ^{LS}	LED STRIP LIGHTING

- ELECTRICAL GENERAL NOTES**
- SEE SPECS FOR ELECTRICAL INFORMATION.
 - ELECTRICAL LAYOUTS ARE SHOWN IN SCHEMATIC. THE CONTRACTOR IS RESPONSIBLE TO COORDINATE THE LAYOUT AND INSTALLATION OF ALL RELATED ITEMS WITH EXISTING CONDITIONS AND RELATED TRADES.
 - COORDINATE WITH OWNER, INTERIOR DESIGNER AND/OR PLANS FOR FIXTURE SCHEDULES, STYLES, FINISHES, ETC.
 - ALL WORK TO COMPLY WITH 2014 N.E.C. CODES AND 2015 I.R.C. CODES.
 - CENTER OF ALL OUTLETS TO BE 18" ABOVE FINISH FLOOR UNLESS NOTED OTHERWISE. CENTER OF OUTLETS OVER CABINETS, VANITIES, ETC. TO BE 12" ABOVE FINISH COUNTER HEIGHT UNLESS NOTED OTHERWISE.
 - CONTRACTOR TO FIELD VERIFY LOCATION OF ALL ELECTRICAL FIXTURES, SWITCHES, ETC. WITH OWNER AND DESIGNER PRIOR TO WIRING.
 - PROVIDE SLOPED RECESSED CANS FOR SLOPED CEILING APPLICATIONS & THERMAL PROTECTION CANS WHERE IN CONTACT WITH INSULATION AS REQUIRED.
 - CONTRACTOR TO PROVIDE ELECTRICAL SERVICE TO MECHANICAL EQUIPMENT AS REQUIRED.
 - ALL BRANCH CIRCUITS BE PROTECTED BY AN ARCH FAULT CIRCUIT INTERRUPTER LISTED TO PROVIDE PROTECTION OF THE ENTIRE BRANCH CIRCUIT.
 - PROVIDE A U-FER GROUND. AN ELECTRODE ENCASED BY AT LEAST 2" OF CONCRETE SHALL BE LOCATED NEAR THE BOTTOM OF THE CONCRETE FOUNDATION SYSTEM. AND SHALL BE IN DIRECT CONTACT WITH THE EARTH. CONSISTING OF AT LEAST 20 FEET OF BARE ELECTRICALLY CONDUCTIVE ROD AT LEAST 1/2 INCH IN DIAMETER OR BARE COPPER CONDUCTOR NOT SMALLER THAN 4 AWG. (I.R.C. E55B.1.2 AND N.E.C. 250.30)
 - THE CONTRACTOR SHALL VERIFY OUTLET LOCATIONS AND VOLTAGE REQUIREMENTS AS PER APPLIANCE SPECIFICATIONS.
 - STRUCTURED WIRE MEDIA PANEL TO BE "TELETON" (O.A.E.) AND INCLUDE: A/C POWER MODULE, CAT 5 VOICE AND DATA MODULES, 10/100 Mbps SATA HUB, CAT V BOOSTER AND AUDIO / VIDEO CONTROL MODULES.
 - SMOKE AND/OR CARBON MONOXIDE DETECTORS ARE TO BE HARD WIRED TOGETHER IN SERIES WITH BATTERY BACKUP AS PER CODE REQUIREMENTS. COMBINATION UNITS ARE PERMITTED AS APPROVED.
 - ALL EXTERIOR ELECTRICAL OUTLETS TO HAVE WEATHERPROOF COVERS.
 - ALL 125V 15 AND 20 AMP RECEPTACLES WITHIN DWELLING UNITS MUST BE TAMPER PROOF.



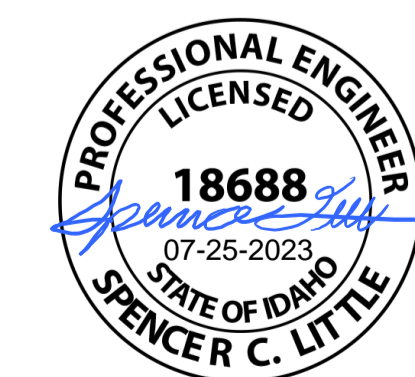



PSEUDO RENDERING WITH ILLUMINANCE SCALE

Luminaire Schedule					
Tag	Qty	LLF	Luminaire Lumens	Luminaire Watts	Total Watts
LED	34	0.810	133	1.12329	38.192
L2	7	0.810	590	9.8	68.6
L1	3	0.810	1255	14.2	42.6

Calculation Summary								
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min	
PROPERTY	Illuminance	Fc	0.08	3.9	0.0	N.A.	N.A.	
TRESPASS	Illuminance	Fc	0.04	0.3	0.0	N.A.	N.A.	

POINT-BY-POINT CALCULATION AND SUMMARIES
(5 FOOT GRID)




WARM SPRING RESIDENCES
 KETCHUM, ID

 LOT 35 CALCULATIONS AND
 RENDERINGS
SPECTRUM
 ENGINEERS



EXPOSED TIMBER AND STEEL
FRAMED DECKING

METAL SIDING

WOOD
SIDING

ALUMINUM
GARAGE
DOOR

STONE
VENEER



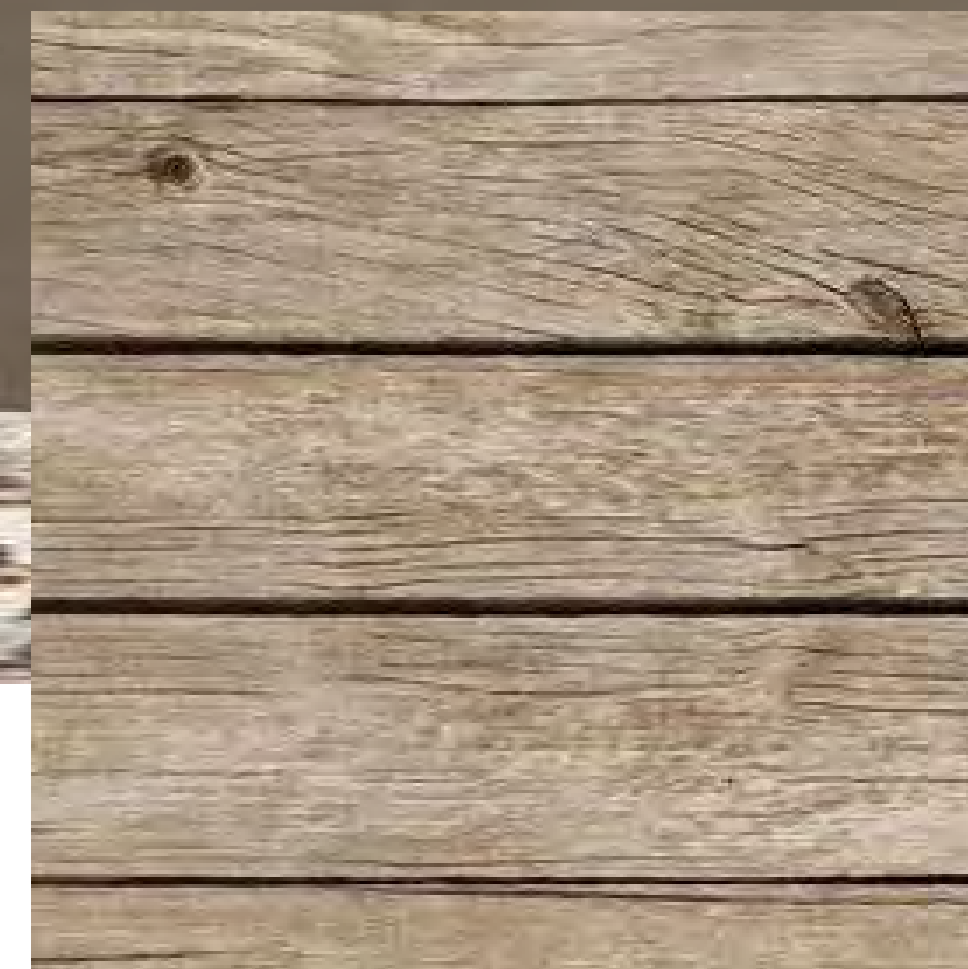
THINSET MASONRY:

ELDORADO STONE:
TIGHT DRY STACKED STONE
BLACK RIVER



VERTICAL SHIP LAP CEDAR SIDING:

PRESTAINED CEDAR SIDING



TIMBER COLUMNS, RAFTERS, TRIM:

RECLAIMED DOUGLAS FIR



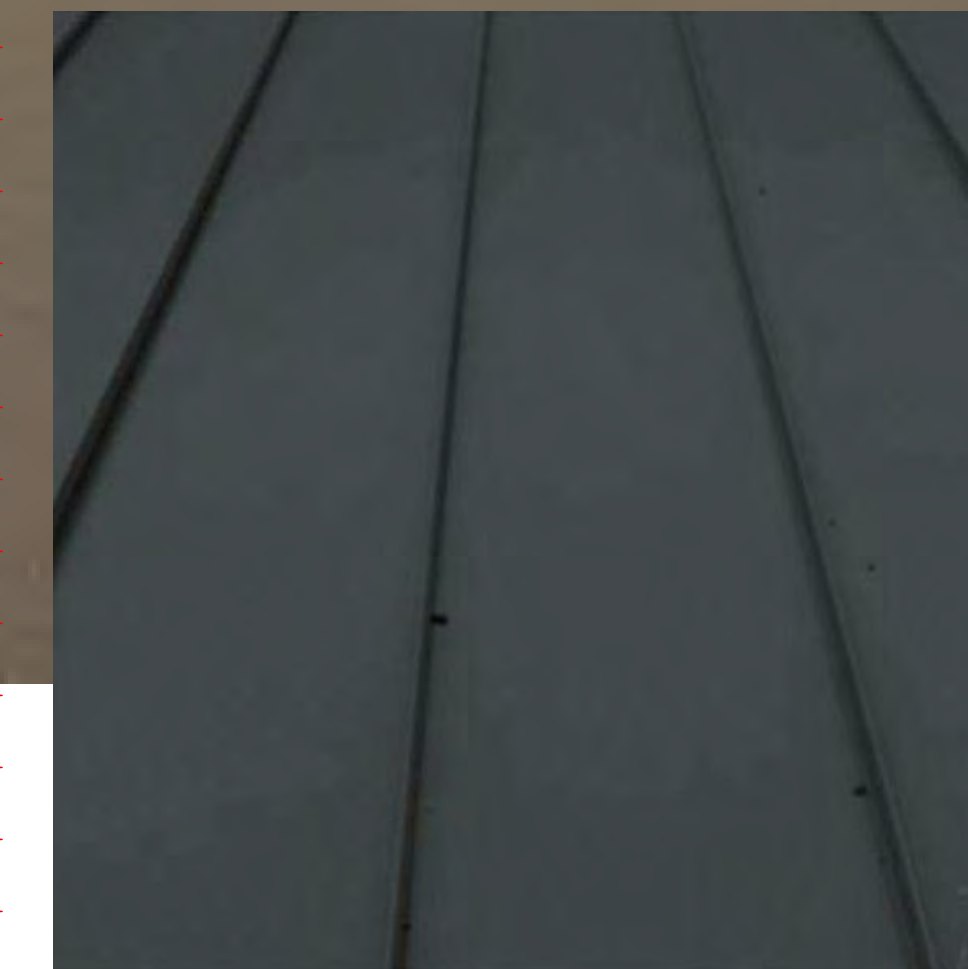
METAL SIDING AND TRIM:

STEEL TRIM
COLOR: AGED STEEL, LIGHT RUST FINISH



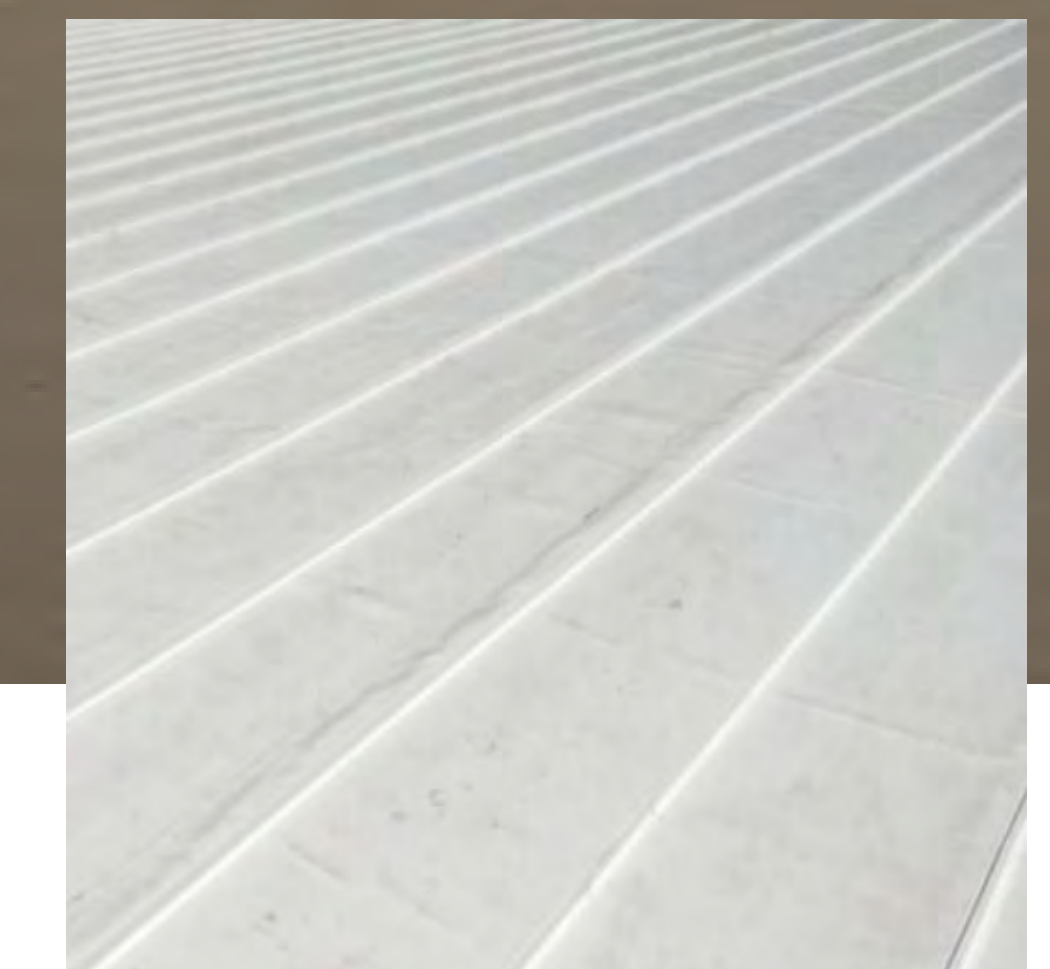
WINDOWS:

ALUMINUM CLAD WOOD WINDOWS
COLOR: DARK BRONZE



METAL ROOFING:

STANDING SEAM METAL ROOF
BONDED ZINC NATURAL PATINA



ROOF:

SINGLE PLY ROOFING MEMBRANE



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WARM SPRINGS RESIDENCE #35

190 BALD MOUNTAIN ROAD
KETCHUM, IDAHO 83340

MATERIAL BOARD

D202

2023.06.30



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