

THE VILLAGES AT JOHNSTOWN PERFORMANCE STANDARDS

t h o m p s o n a r c h i t e c t s

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revised 11/13/18
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ACKNOWLEDGEMENTS

The Villages at Johnstown welcomes you to our community.

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JOHNSTOWN DOCUMENTS:

TOWN OF JOHNSTOWN BUILDING AND ZONING CODES

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1. INTRODUCTION

A.1 PURPOSE OF THE VILLAGES AT JOHNSTOWN PERFORMANCE STANDARDS:

...IMAGINE, BUILD, AND MAINTAIN A QUALITY OF LIFE AT THE VILLAGES AT JOHNSTOWN...

The purpose of **The Villages at Johnstown Performance Standards** is to provide private developers and owners, public agencies, and service providers the procedures for development of the area. The Performance Standards outline guidelines for property owners and the community. They are designed to protect quality of life, minimize adverse development, protect investments, encourage quality buildings, discourage undesirable buildings, and enhance security and safety for all users. The Performance Standards apply to all new and proposed renovation projects throughout the Villages.

THE VILLAGES AT JOHNSTOWN PERFORMANCE STANDARDS ARE TO BE USED BY:

- All proposed developments
- Town of Johnstown and other local governments
- Sales and real estate personnel
- Economic development groups
- Owners within the Villages
- Utility Providers

THE PURPOSE OF THE STANDARDS:

- Maintain the vision, and aspire to develop a Village for the Johnstown community
- Establish consistency of land uses and adjacencies
- Create economic strength as a planned community
- Establish a reference for developers, buyers, and Town officials
- Foster collaboration, teamwork and sense of community
- Supplement the Town of Johnstown's building and zoning codes, ordinances, and Town standards

The Villages at Johnstown

A.2 SUBMITTAL REQUIREMENTS

The property comprising The Villages at Johnstown was annexed to the Town in 2000 subject to the terms and conditions of the R & D Development and Cross Annexation Agreement dated March 1, 2002. As part of that Annexation Agreement, (Section 36) Johnstown and the Property Owners agreed to develop performance standards for the purpose of addressing design considerations. These include architectural, site planning, landscaping, streetscape, and signage basics for land use within The Villages at Johnstown.

The following information outlines the order for submittals and approvals for development projects. Projects must first be submitted to The Villages at Johnstown Design Review Committee prior to submission to the Johnstown Review Committee. After approval is gained by both, the project may be submitted for development and/or building permit application. For single family residential projects, refer to Section N for additional information.

Design Review Committee (DRC): The purpose of the DRC is to ensure that proposed developments adhere to the guidelines established in the Performance Standards. This will maintain a consistency of planning and design for the entire project. DRC members may include a licensed Architect, Licensed Civil Engineer, Landscape Architect, Land Developer, and rotating professional. The Villages at Johnstown Performance Standards apply legally to all land that is part of The Villages at Johnstown, regardless of ownership, and are in addition to the zoning and land use regulations of local government. The Performance Standards and supporting documents are administered and enforced by the DRC.

DRC Approval Process: Any proposal to construct, modify or renovate within The Villages at Johnstown must have plan approval from the DRC prior to project start. The DRC's review and approval process also applies to site plans, landscape plans, photometric plans, building elevations, signage, changes in property use, and maintenance activities. After approval by the DRC, the applicant may proceed to the JRC, in strict compliance with the terms and conditions of the approval. The Town or DRC may perform periodic site inspections, both during development and on an ongoing basis to ensure compliance.

Projects are placed on a formal meeting agenda by the DRC only after applications have been submitted, and at least two weeks prior to a meeting. Formal presentations to the DRC are mandatory for most development projects, however, most details are reviewed through informal meetings with DRC representatives.

This process is designed to expedite the preparation and approval of the plans for a specific site where development is considered. There are three phases in the development approval process:

1. Pre-design Conference
2. Design Development (includes plans and elevations)
3. Final submittal

DRC Required Documents:

Pre-Design Documents: Submit application and fees as applicable. Include site plan and building elevations of the proposed development, with a written description of the proposed use. Contact the DRC to schedule an appointment to review the planned development. Upon approval, proceed to the Design Submittal Phase.

Design Submittal Documents: Provide project schedule, phasing plans (if anticipated), alta survey, geotechnical report, traffic report, site plan, concept floor plan, colored building elevations, grading/utilities, drainage plans, landscape plans, photometric plan, material and color boards. Upon approval by DRC, submit the planning package to Johnstown. Proceed to the Final Submittal Phase after Town approval.

Final Submittal Documents: Provide final civil plans, utility plans, hydrology study, off-site improvement plans, landscape and irrigation plans, architectural construction documents, structural documents, mechanical documents, plumbing documents, electrical documents, specifications, structural calculations, energy calculations, material boards, signage package, and copy of permits from all Town agencies. An updated schedule for the project from start date to move in date will also be required.

Approval is contingent upon the submittal of materials, payment of any designated fees or expenses, and a favorable review. A "Notice of Committee Action" letter from the DRC will be sent to each applicant upon approval. This notice will state whether project approval has been granted, and outline any conditions associated with the ruling.

Review fees will be required for all DRC submittals, and shall be paid on or before the phase scheduled for review. The DRC may require that the applicant attend additional meetings due to incomplete, inadequate, or improper submittals. The applicant shall be responsible for paying the full costs of such services. No written confirmation of a DRC action will be issued until all appropriate fees have been paid. A current schedule of fees is contained in the submittal procedures packet.

Johnstown Review: The Town will conduct their own review of the submittals, and may seek the assistance of other Town employees or consultants whose expertise is deemed necessary. All Town subdivision and re-subdivision requirements, building codes, permits, fees, and other documents as adopted by the Town apply.

JRC Approval Process: All individual developments in The Villages at Johnstown shall be reviewed and approved by the Town. The approval process shall also meet land use developments set forth in the Town of Johnstown's Zoning Code.

Pre-Application

The applicant shall schedule a pre-application conference with the Town Planner prior to submittal of any project proposal.

Final Development Plan Submittal and Process

All development projects shall be submitted in compliance with the current Town of Johnstown Community Development Application requirements for Final PUD Development Plan. (Forms are periodically updated.) The application shall include all required fees, as well as a certification from

The Villages at Johnstown DRC stating that the development meets all the applicable standards and guidelines of The Villages at Johnstown Performance Standards. If the Town determines that the application is complete, the application will then be reviewed by the JRC. If the Town determines that the application is incomplete, the Town shall specify in writing the specific ways in which the application is insufficient or incomplete.

The JRC will review the complete application for conformance with all the applicable terms and conditions of The Villages at Johnstown's Performance Standards. The JRC is comprised of the Town Manager, Town Planner, Town Attorney and Town Engineer. The review shall be completed within 45 calendar days of the Town's determination. The 45-day period may be extended in writing by the applicant. Review of the application by the JRC is administrative in nature, for the purpose of determining that the proposed development, as set forth in the application, complies with the terms and conditions of The Villages at Johnstown Performance Standards.

JRC Approval: The JRC will approve the application if it complies with the applicable terms and conditions of The Villages at Johnstown Performance Standards, and other Town requirements. All subdivisions and re-subdivisions are to be approved in accordance with the Johnstown Subdivision Regulations.

Additional Criteria & Updates: In addition to the criteria herein, the DRC and the JRC may develop additional criteria, consistent with those already established. Occasionally, any of these additional criteria may be amended by action of the DRC and the Town. Changes in land use, or changes greater than the 20 percent dimensional criteria that will become a permanent part of the design guideline document, shall constitute a major change. This must be brought back to the Planning Commission and Town Board for review and approval.

Variances: The DRC and JRC may authorize variances from these criteria when circumstances such as topography, natural obstructions, hardship, aesthetic considerations, or environmental objectives may warrant as long as they are not superseded by applicable JRC of Johnstown zoning regulations. A variation of up to 20 percent in dimensional standard is allowed if it improves the project design, or an unreasonable hardship can be demonstrated.

Final Plan Amendments: Amendments to final plans must be approved by the DRC and the JRC.

Relationship to Other Documents: The Performance Standards establish the guiding principles for review and processing of each development. Other documents were used as a reference for the Performance Standards. The version currently in effect when the Performance Standards were adopted shall apply.

- R & D Development and Cross Annexation Agreement – March 1, 2002
- Town of Johnstown Zoning Code
- Town of Johnstown Sign Code
- Town of Johnstown Comprehensive Plan – April 1, 2001
- Johnstown/Milliken Parks, Trails, Recreation and Open Space Plan – May 2003
- Town of Johnstown Landscape Standards and Specifications
- Johnstown Public Improvement Criteria and Construction Regulations – April 2004
- Johnstown Transportation Plan

DRC and Town of Johnstown Application:

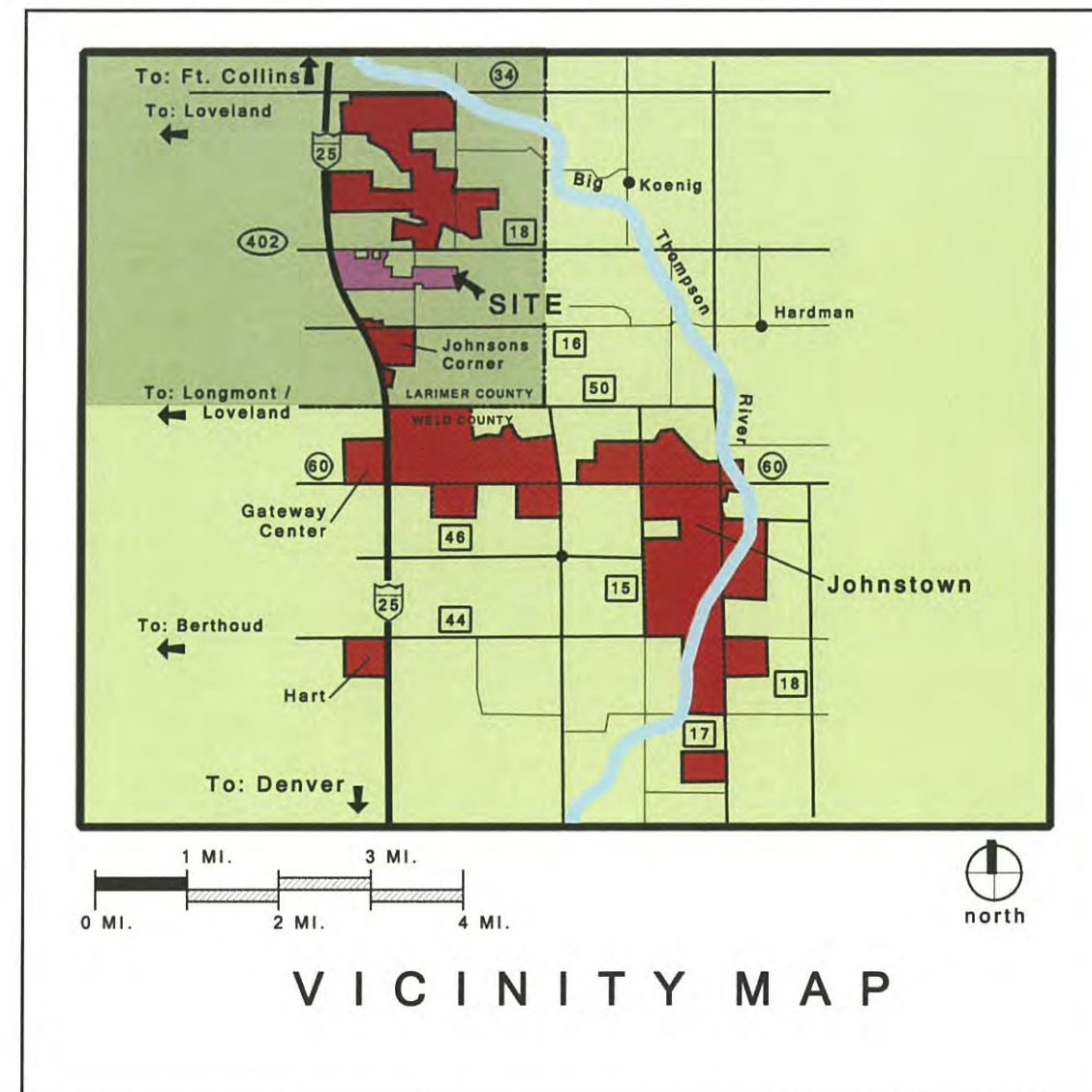
Application forms, fee schedules, and required checklist forms for each applicant are available from the DRC. Single Family Residential has a simplified submittal process. Contact the DRC directly for information and submittal application requirements.

Contact the Town of Johnstown for Final Development Plan application packet.

B. THE VILLAGES AT JOHNSTOWN BOUNDARIES

The Villages at Johnstown are located along the I-25 corridor. The development has excellent vehicle access. It is centrally located to major population areas, and is ideally located for maximum visibility. The land area to the east is slightly elevated and provides spectacular views of the Colorado Rocky Mountains. The site is conveniently located to the west of Johnstown. The location is also south of Greeley and Fort Collins, east of Longmont and Loveland, and north of Denver.

The Villages at Johnstown design criteria are enclosed in this Performance Standard booklet. Additional standards (such as road designs) are available from the Town of Johnstown. The Performance Standards are based on a collaboration of design principles, sound recommendations, and strategic land use in order to achieve the desired objectives for the Villages. Welcome to The Villages at Johnstown.



VILLAGES

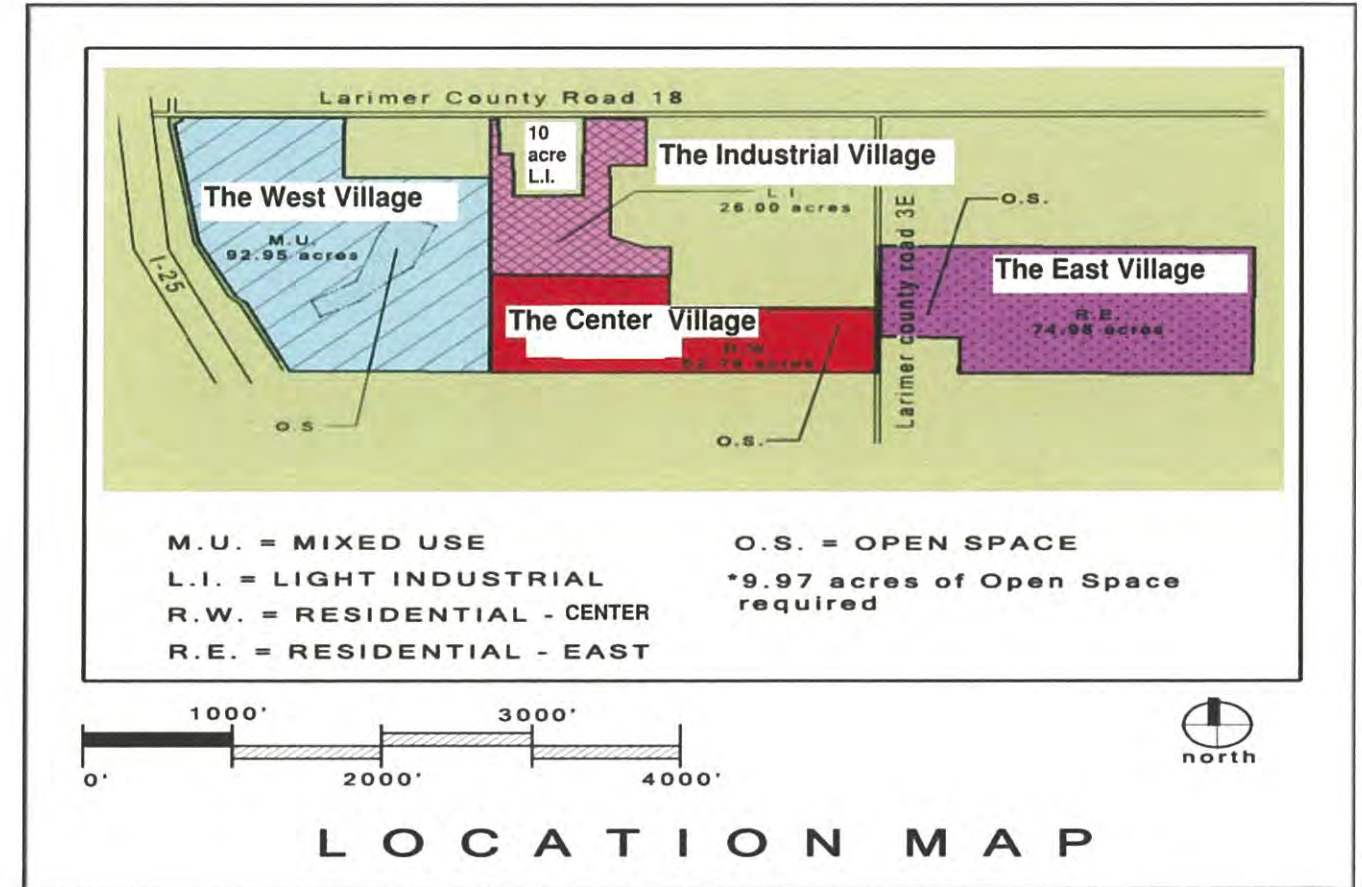
B.1 MASTER PLAN FOR THE VILLAGES AT JOHNSTOWN

It is the intent that The Villages at Johnstown Performance Standards be the guideline for quality design for all land uses within the development. It is also the goal of the Performance Standards to allow professional planners, architects, and designers flexibility. For example, the needs of Mixed Use/Commercial/Retail will be very different from the needs of the Residential area. Therefore flexibility in specific land uses, design requirements, and development approaches should be evaluated for each project. Each intended use has unique goals to collectively support the overall theme of The Villages at Johnstown.

B.2 - 6

Within the annexed area, the following land uses have been identified. These relate to the level of current planning studies, anticipated development schedules, and public/owner involvement in the planning process. The Villages at Johnstown community includes:

- **THE WEST VILLAGE** : Contains approximately 93 acres of mixed use commercial, retail and office
- **THE INDUSTRIAL VILLAGE** 36 acres
- **THE CENTER VILLAGE** R.W. (Residential West) 53 acres including high density residential use and several lots for single family use
- **THE EAST VILLAGE** R.E. (Residential East) contains lower density residential, made up of primarily single family residential. Open space and trail Corridors will be required throughout. Each development will be required to contribute their pro-rated share to the open space.



Note: Acreage listed above is approximate

C. PLANNING

New projects must be designed to improve The Villages at Johnstown. Projects must be master planned to be accessible for visitors and all users. Circulation must be well situated for all modes of transportation. Designs must consider all aspects of neighboring parcels, existing or proposed, and traffic arteries. Proposals must respect the site's uniqueness. Common shared spaces may be created through a collection of buildings framing the center of activities, pockets of spaces, exterior rooms or other space utilization methods.

New developments should respect the indigenous amenities, natural features and the relationships of buildings to each other. All types of circulation must be well thought out. Group buildings in order to generate a sense of space, create meeting areas, and enhance pedestrian centers. Creating physical and visual connections between separate structures and sites are required to link the Village. Design for interaction within the Village is a driving force of the DRC. Sites should be developed to provide order and compatibility. Avoid confusing building massing or stand-alone developments that don't relate to other areas. All proposals must respect wind direction, natural drainage, angle of the sun, and microclimate considerations.

C.1 SETBACKS

Provide a thoughtful plan with respect to building setbacks and incorporate pedestrian links along major and minor streets. Setbacks are established to ensure an ample distance for a pleasant and functional pedestrian walkway, and provide landscape settings between buildings, streets, and adjacent parking. These setbacks will also act as a planned buffer to screen automobiles, large paved areas, and service areas, using a diverse selection of planting materials.

Buildings surrounded by parking lots are not allowed, with the exception of hardship cases or for emergency vehicle routes. In these instances, a comprehensive plan with a well-conceived landscape design must be submitted to the DRC. Locating buildings to shield automobiles from public view is encouraged.

Minimum Building Setbacks:

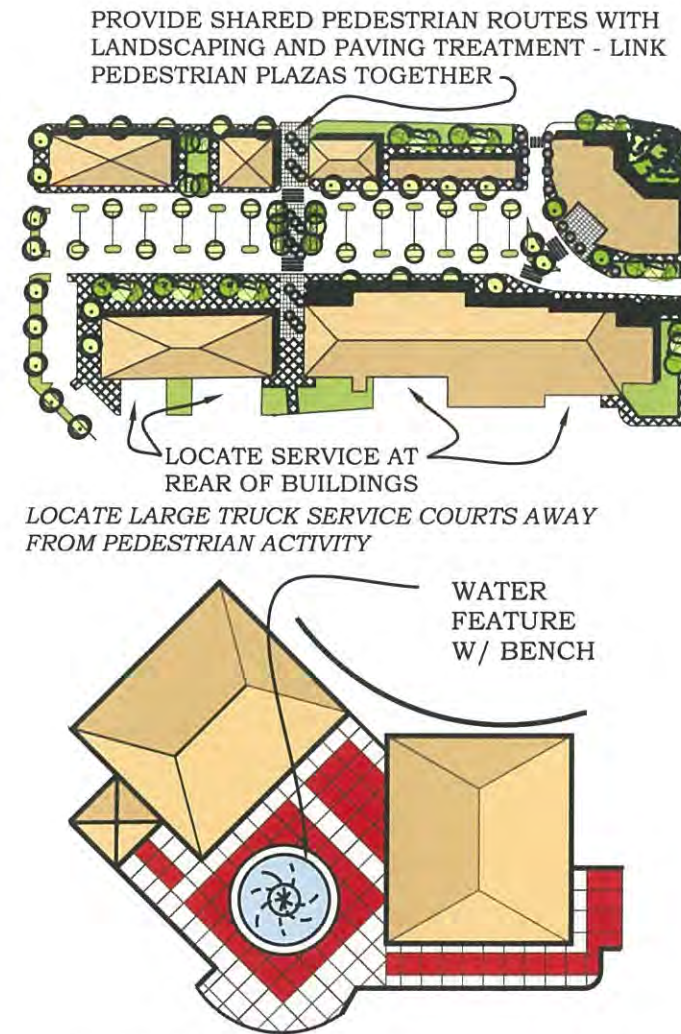
▪ Freeway / Highways	80'
▪ Major Arterial	30'
▪ Major Collector	30'
▪ Frontage Road	20'
▪ Minor Arterial	20'
▪ Minor Collector	20'
▪ Front	20'
▪ Side and Rear	10'
▪ Adjacent to Residential	50'
▪ Over 25,000 GSF	25'
▪ Multi Residential	20'
▪ Single Family	See page 28

Minimum Parking Setbacks:

From all perimeter property lines or right-of-way: setback is 20' for front parking, 10' for side parking and 5' for rear property or lot lines. Parking setbacks shall be landscaped.

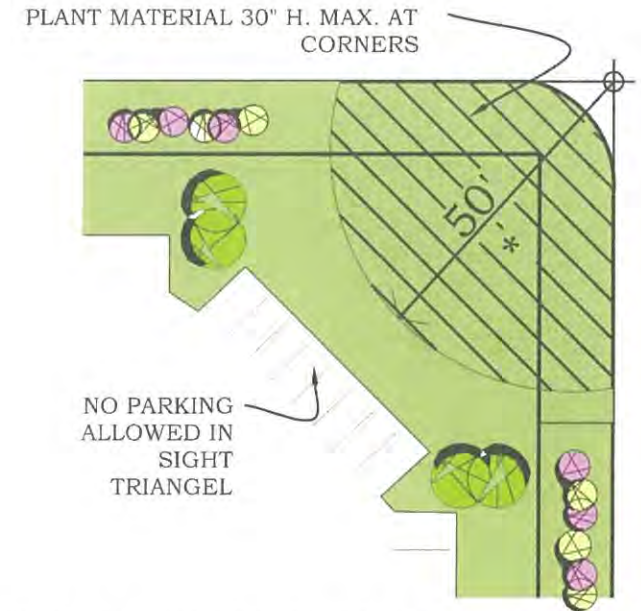
Setbacks Reductions:

Setback distance for minor arterial and major collector property lines can be reduced to 20' minimum provided an abundance of year-round foliage with winter color is utilized. Reduction in setback is allowed providing pedestrian links are safely designed. This can be accomplished through the use of raised planters, screen walls or other acceptable solutions.



GROUP BUILDING TO CREATE CENTERS OF ACTIVITY - EXAMPLES: FOUNTAINS, PLAZAS, LANDSCAPING WITH SEATING AREAS, ETC.

The Villages at Johnstown will accept zero lot lines, and reduced setbacks for "public spaces" based upon well-conceived and comprehensive planning solutions.



CORNER SITE TRIANGLES TYP - 50' AT ARTERIAL STREETS AND * 30' FOR COLLECTOR STREETS.

Providing an abundance of pedestrian paths and trail connections will increase credits for reduced setbacks. Donating or granting land for approved open space and trails may reduce building setbacks by as much as 50% in commercial zones.

In all cases any reduction in setbacks must be approved by the DRC and the JRC.

C.2 USES, AMENITIES AND NON-PERMITTED USES

Uses:

In general, most uses are permitted within the Village. Mixed use of commercial, retail, and light industrial along with mixed use of residential densities allow for a wide range of uses and planning flexibility.

Amenities:

The Villages at Johnstown's beautiful Rocky Mountain surroundings, plus proposed ponds, park setting and open space designs are amenities to be enjoyed by all. Capturing vistas and creating built features to experience these amenities are encouraged.

All new developments should be designed to protect nature. Create manmade features that will enhance the overall Villages at Johnstown concept.

Outdoor spaces, courtyards, sitting areas, amphitheaters, and pedestrian amenities should be designed to be easily accessible. Include manmade and natural amenities into the proposed concept and final construction documents.

Non-Permitted Uses:

Excluded uses in the Industrial Villages are as follow:

- H4 and H5 high hazard manufacturing
- Meat packing
- RV and Auto Sales dealerships
- Concrete Batch Plants
- Asphalt Batch Plants
- Packing Houses
- Foundries

C.3 SITE COVERAGE

Landscaped areas, plazas, trails and exterior rooms are greatly encouraged, and will be a major feature of The Village @ Johnstown. These valued amenities will naturally regulate the guidelines for design. The impact will be as follows:

- Areas including the “footprint” of buildings, asphalt or concrete paving, and the drive lanes of each parcel in the entire development exceed 75%. Light Industrial may be increased to 85%.

Required open space for residential parcels is 30%. Open space includes streetscapes, hardscapes, plazas, fountain areas, ponds, and areas such as sidewalks with bench seating. All areas should be pedestrian friendly. Larger developments may want to consider dedicating public land for parks and open trail systems. Buildings with public “indoor spaces” may also receive credit. Indoor spaces are allowed to be considered as part of the open space requirement, if approved in advance by The Villages at Johnstown, the DRC and JRC.

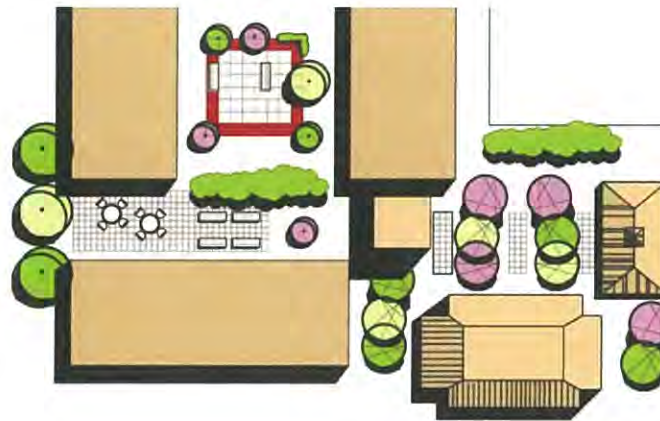
- Shared, adjacent or abutting open spaces with adjacent property owners will allow a reduction in setback requirements. See section C.2.
- 10% of all single-family residential land is required to be designated as land used for parks and recreation.

C.4 UTILITIES

Above ground utilities are generally unsightly. Utilities such as rooftop units, miscellaneous electrical equipment, satellite dishes, transformers, communication antennas etc. must be screened from view. All sign transformers are to be hidden from public view and no exposed conduit is allowed.

Screen all transformers, utility equipment and other utility cabinets from pedestrian views, building views, streets and adjacent property. Utilize green walls of densely placed plant materials or vertical screen walls that match building materials and finishes.

Truck service courts, delivery and storage docks are to be screened from pedestrian views, including views from sidewalks, trails, public and private open plazas, streets and other amenities.



UTILIZE POCKETS OF EXTERIOR SPACE FOR OUTDOOR AMENITIES

All trash enclosures and screen walls must match the primary building materials per the individual site. Wood screen walls for trash enclosures are not allowed. Landscaping should be incorporated with screen walls for a pleasing appearance.

C.5 WATER QUALITY

Wherever possible, drainage collection systems should attempt to be an amenity. Planted holding areas and designed rockscapes should be visually pleasing. Minimizing runoff from parcels and to land downstream is vital. Grouping detention areas and water quality ponds for regional collection is a preferred solution. Creating planted features such as shallow pools of cattails and indigenous grasses can beautify detention areas. Ditches, swells, low holding ponds, and collection areas shall be planted to capture storm waters, and provide a means for water quality treatment.

Locate water quality systems away from primary elements such as buildings, low-level pedestrian areas, and highly active areas.

Design water collection systems to simulate natural ponds, instead of man-made tanks or pits.

C.6 NON-POTABLE IRRIGATION

Non-potable water may be available to the West and Industrial Villages. Non-potable irrigation systems may be utilized provided that such collection systems are approved by the DRC and JRC (and must follow applicable Colorado laws). Residential developments are also encouraged to utilize similar methods for water conservation programs. Low water landscaping and xeriscaping is encouraged for all developments.

C.7 DESIREABLE DEVELOPMENTS

- Development for new building, renovations and land uses are required to be in keeping with the overall Village concept. Following are a few requirements: Address all types of transportation systems, including future system projections, for traffic related volume.
- Provide roadway designs for initial and final phases.
- Provide a safe network of systems that allow free movement of pedestrians from automobile traffic.
- Provide effective and safe access connections to major roadways.
- Provide warning signs and directional signs to aid all modes of transportation and pedestrian traffic.
- Promote sustainable and green designs.
- Promote intelligent usage of water.
- Promote and link amenities within the Village.
- Promote the use of bicycles.
- Promote the concept of work, live and play as part of one community.

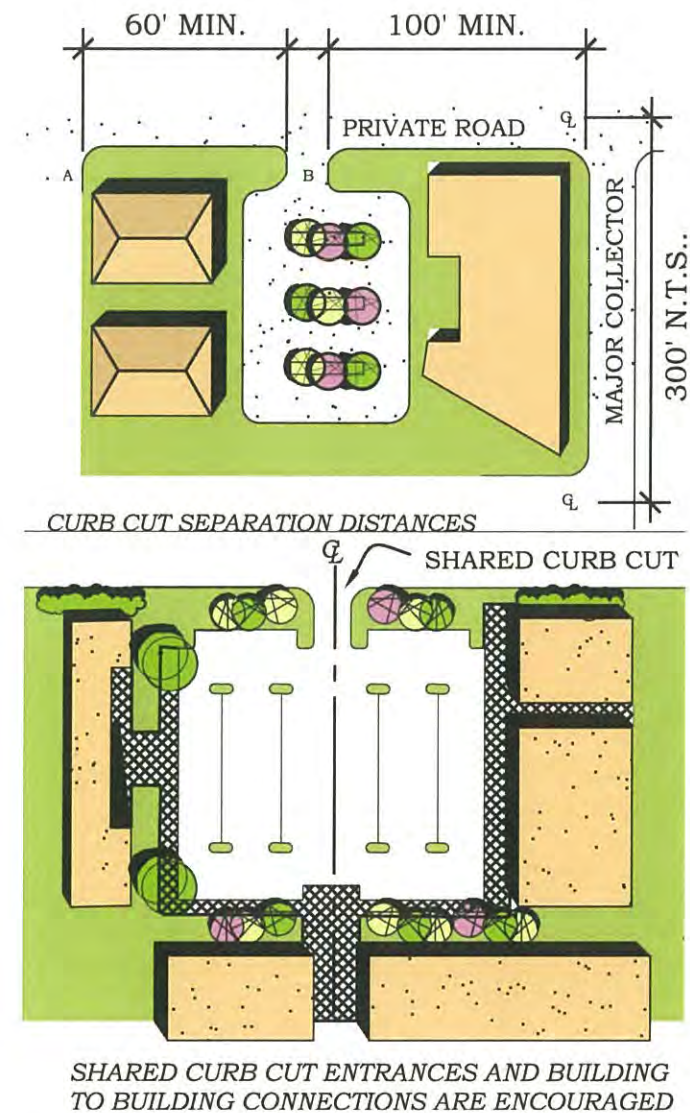
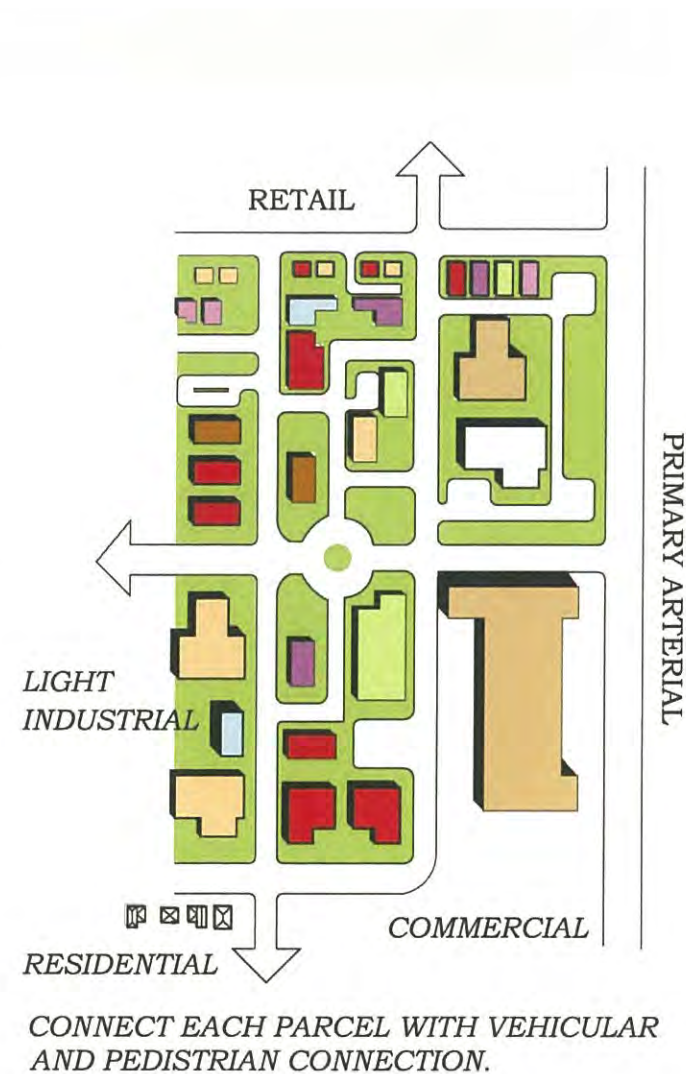
C. PARKING

The Villages at Johnstown's arterial streets, collector streets, drive lanes, curb cuts, vehicular circulation, and parking designs are an important design element in the Villages. This network of systems must be designed with safety as a principal component. Bicycles and pedestrians should be given priority in the overall design and safety plans of each individual site design.

D.1 ACCESS

Designers must conform to the protection and welfare of the community. Parcels are encouraged to minimize the number of curb cuts from major roadways. Provide safe, clear and efficient service to each parcel, and utilize safety measures. A comprehensive traffic analysis demonstrating access and the impact of traffic may be required.

Design elements including benches, water features, signs, brick paving, distinctive landscaping, rock walls, and feature lighting are to be incorporated into the design concept. Designs must be compatible within the Villages as a whole. Position curb cuts far enough apart to allow ample queuing, turning, deceleration or acceleration for vehicles. Develop adequate distances between curb cuts and corners for anticipated speed limits. The Village encourages common curb cuts between property owners, with access easements and connecting pedestrian routes. Shared parking concepts, and emergency drive lanes are also encouraged. For shared entries, develop clear lines of vision and colored crosswalks.



D.2 CIRCULATION

Developments must provide a well-designed circulation plan. This includes streets, drive lanes, trails, and service roadways. A deliberate, protected, and well-designed network of systems is paramount. All transportation methods must connect the Villages with peripheral land uses through paved roads, concrete sidewalks and/or walking and biking trails.

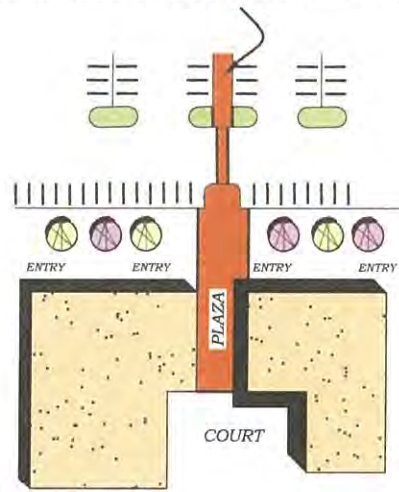
D.3 SERVICE, DELIVERY, EMERGENCY AND UTILITY ACCESS

- Clearly marked routes for all access and services routes should be apparent, and directional signage must be provided. Safe circulation for all facilities should utilize proper turning radiuses for fire trucks, ambulances, service trucks and delivery trucks. Inside and outside turning radiuses, as well as bumper and trailer swing paths must be clearly understood and designed into the development. Shared access is an excellent and efficient method, as is aligning curb cuts with existing curb cuts across roadways. Screened service areas within building designs are encouraged. Wall screens and landscape screens must be provided to screen service areas and service doors from view.

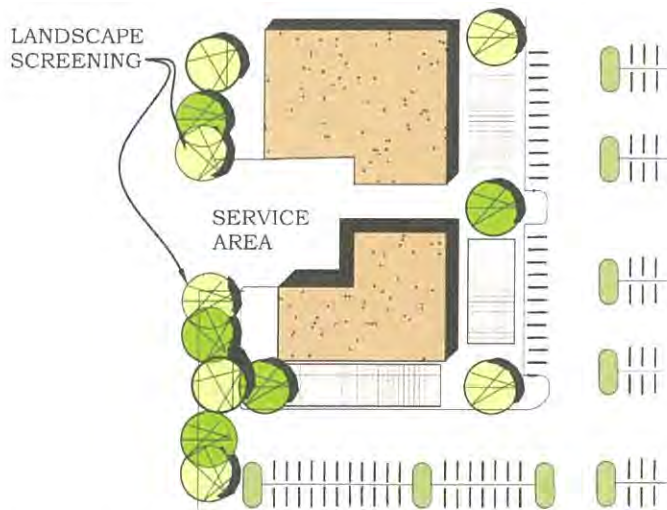


USE BRICK PAVERS, RECESSED STONE AND COLORED CONCRETE IN PEDISTRIAN ROUTES AND DROP OFF AREAS.

PROVIDE PEDESTRIAN ROUTES FROM PARKING LOTS TO PEDESTRIAN PLAZAS



ARRANGE PARKING AISLES AND PEDESTRIAN APPROACH PERPENDICULAR TO BUILDING ENTRANCES.



SCREEN SERVICE AND DELIVERY AREAS FROM PUBLIC VIEW AND STREETS. USE LANDSCAPING, SCREEN WALLS OR CONFIGURE BUILDING TO SCREEN THESE SERVICES.

D.4 PARKING LOTS

Parking lots must be designed for the required number of spaces per the development's intended use, or future use. All parking areas are to be surfaced with asphalt or concrete,

and screened to the outside public. Trees and shrubs placed in "parking lot islands" are required for shading lots, and areas set aside for snow stacking are encouraged.

Standard parking stalls are to be 9' wide by 18' long. In limited areas adjacent to appropriate landscaping, parking stalls may be reduced by 2'. Painted parking aisles perpendicular to buildings instead of parallel parking is encouraged. Link large parking areas with pedestrian walks. Connect paths to buildings as well as the primary trail system and perimeter sidewalks. Sidewalks should connect to modes of transportation, open space, parks and other pedestrian features. Cross walks should be of red concrete paving with a 10" x 10" tooled pattern. Paths for pedestrians can be complimented using seating, path lighting, lit bollards, trellises, gazebos, playgrounds or other amenities. Divide parking areas which accommodate more than 105 vehicles into smaller grouped lots. Larger projects with more than 400 cars will not be held to this formula. However, they will require a detailed parking scheme demonstrating landscape design, pedestrian amenities, as well as the use of landscape earth berms to reduce the "sea of asphalt" appearance. Use landscaping or other techniques to reduce the visual impact of large parking areas. Avoid travel lanes in aligned configurations that may encourage speeding, posing a danger to pedestrians.

Minimum Parking Ratios:

Required parking spaces are based on the type of building use. The following formulas for parking spaces per gross square feet of area apply. (Round up to nearest whole number.)

- Professional Offices 4 spaces/1000 GLF
- Showroom 2.5 spaces/1000 GLF
- Hotel or Motel 1 space/guest room plus 1 space/2 employees

- Retail Uses 5 spaces/1000 GLF
- Theaters And Restaurants 1 space/3 seats
- Warehouse 1 space per employee per shift
- Single Family 2 space per unit
- Townhomes 1.5 spaces per unit
- Condo 1.5 spaces per unit

Large projects over 100,000 square feet may use a compact stall size of 8.5' x 15' with compact spaces not to exceed 30% of the total parking count. The DRC and JRC may allow reduced parking requirements by the use of shared parking programs. For projects over 200,000 gross square feet, the ratio of compact to standard spaces may be increased to 40%. Because there is a high degree of larger vehicles in Colorado, wherever possible, standard stall spaces of 9' W. x 18' L. are encouraged.

Reductions in parking areas are suggested. Examples are shared parking spaces, off hour/offset hour's programs, etc.

Parking spaces for the disabled in accordance with ADA is required. Provide handicap routes with tactile warnings.

D.5 PARKING STRUCTURES

Underground structures and parking below buildings is acceptable. For above ground and free-standing garages, "open garages" are encouraged. Provide open garages with light wells for natural light, and include plant materials. Enclosed secure garages will require ventilation systems as well as fire suppression assemblies. All parking structures exposed to view must match the adjoining building finishes, or be approved by the DRC and JRC.

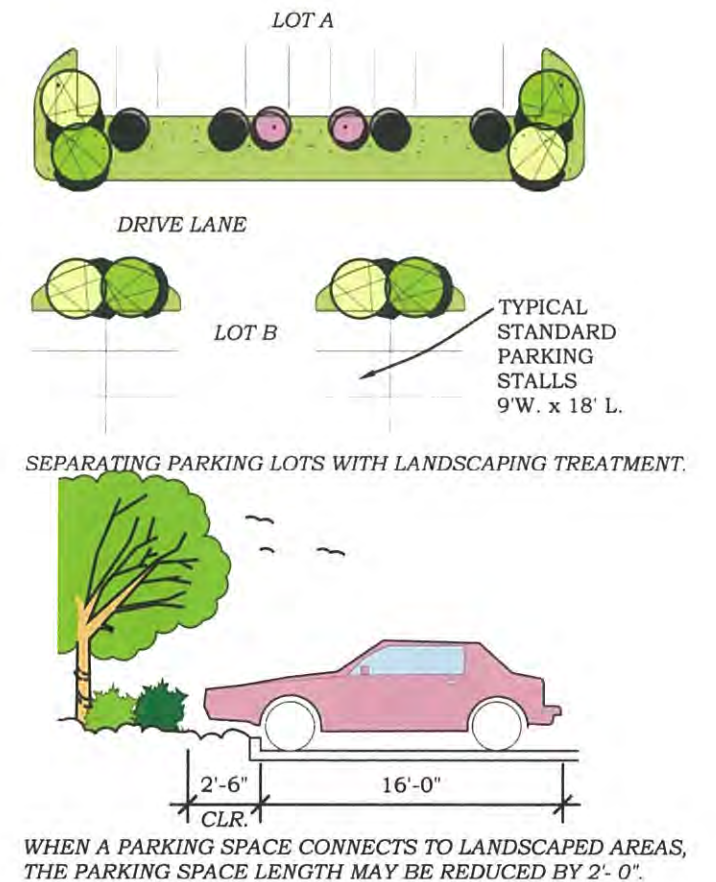
Where a parking structure is anticipated, it may not encumber adjacent parcels. The visual impact of such structures will need to

be carefully orchestrated in the overall design, and corridor views from other developments must not be hindered.

Above-ground garages and parking structures will be held to the same design and performance standards that pertain to buildings. Linking free-standing parking structures to buildings must meet performance standards as pertaining to pedestrian paths, landscaping and safety.

D.6 FUTURE PARKING

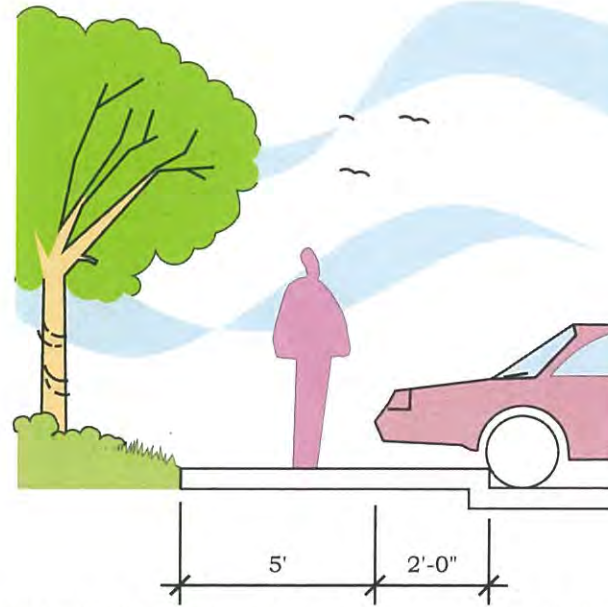
Allowing for parking phasing may be a necessity. In these cases, the DRC and JRC will require a master planning and phasing plan. Construction must not disrupt existing day-to-day operations for the user or the community. While phasing is a normal device to planned growth, minimizing the impact to the development must be thought out prior to any improvements.



D.7 MOTORCYCLE PARKING

Motorcycle parking should be provided in each development, and can reduce standard parking stall requirements. For every two motorcycle parking stalls, one car stall may be reduced. For a maximum of 5% standard parking stall reduction. Example: for every 100 standard sized parking stalls, 5 stalls may be omitted provided 10 motorcycle stalls are utilized. Use clearly worded signage in motorcycle areas, and paint stripe motorcycle parking stalls. Signs must indicate stalls as “motorcycles parking only”

Boat and RV parking is permitted in the Villages, provided that they are screened from view.



SIDEWALK WIDTHS SHOULD BE INCREASED BY 2'-0" WHERE ADJACENT TO HEAD-IN-STALLS.



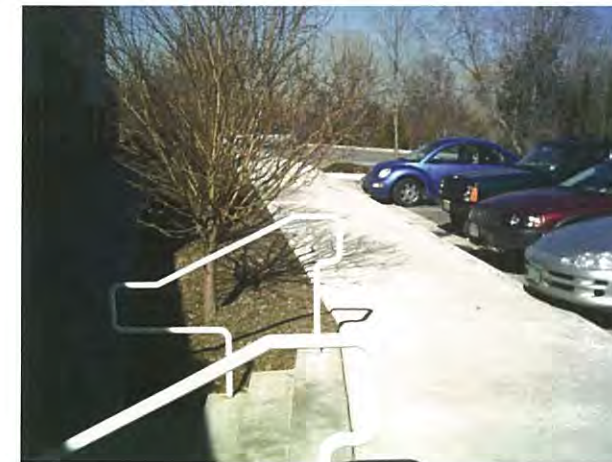
EXAMPLE OF A MEDICAL OFFICE BUILDING

E. PEDESTRIANS

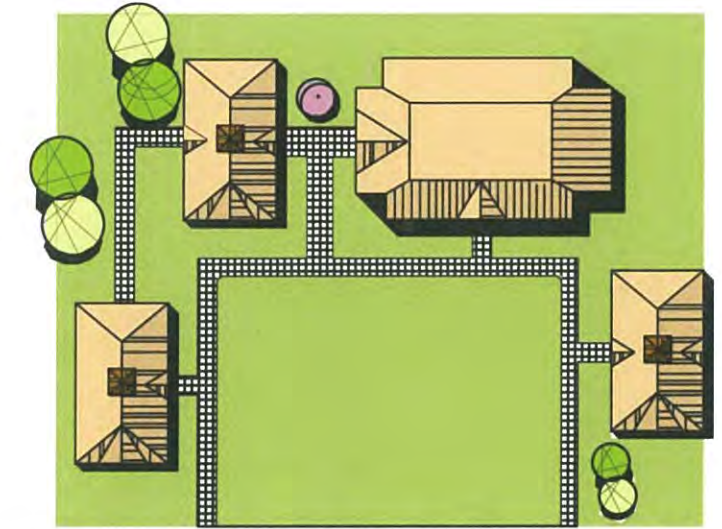
E.1 PEDESTRIAN CIRCULATION

Walkways, trails and bicycle routes should be designed to create a pleasant experience in the Villages. Bike paths and bike parking should be established to develop a network of accessible, safe and attractive experiences for users.

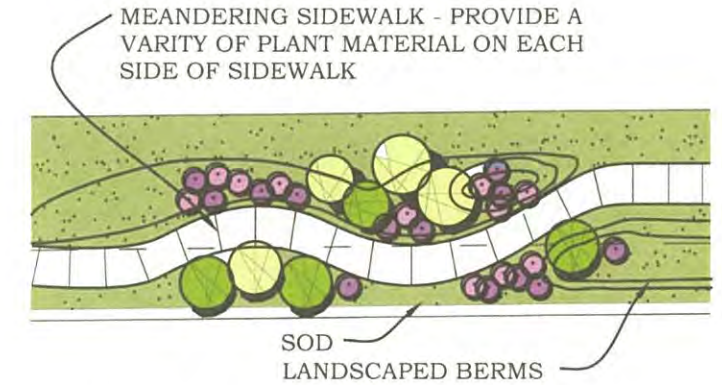
Separate well-defined pedestrian and vehicle routes, utilizing landscaping buffers, walls, planters, bollards or other means of protecting the public. Define areas of pedestrian, bicycle, and vehicle interface using accent colored concrete. Differentiate zones by changing the paving material is a useful means of minimizing traffic conflicts. Other safety measures include directional signage, and low plant materials (in sight triangles). Signaled devices and warning tracks are also



PROVIDE OVERSIZED SIDEWALKS AT FRONT END PARKING – MIN. 7' WIDE.



PLACE SIDEWALKS TO LINK AREAS OF PEDESTRIAN TRAVEL.



MEANDERING SIDEWALKS - TYPICAL AT STREET SIDE

Connect buildings to buildings, and buildings to drop off and pick up zones. Locate pedestrian routes in sunny areas, not in areas that may develop ice problems during winter months. Design sidewalks to be a minimum of 5' wide and detached from all streets by providing a 6' wide (minimum) landscaped planting zone. Design designated drop-off areas outside of the traffic flows. Install 7' wide sidewalks at front end parking to accommodate “car nosing”.

E.2 PEDESTRIAN CONNECTIONS THROUGH PARKING LOTS

Bicycle and pedestrian paths thru private parking lots must be separated from automobiles. The separation of car and people are a primary safety requirement throughout the Villages. Sidewalks must clearly define pedestrian paths from parking zones to a building entry. All efforts to reduce the interfacing of automobile and pedestrian traffic must be considered. Try to avoid pedestrians interacting with service areas and traffic zones. Clearly separate all vehicles from the pedestrian as much as possible.

E.3 THE DISABLED

The Villages must be accessible by all users. ADA standards will be in full effect and it is up to each development to meet these standards. Contact the International Code Council for the latest edition for ADA requirements and amendments. Provide parking for the disabled immediately adjacent to entrances. All aspects of the ADA are required and a written letter to the DRC is mandatory at the conclusion of construction verifying that ADA conformance has been met.

E.4 VEHICLE SEPARATION

Separate automobiles, motorcycles and truck traffic from pedestrians and bike paths whenever possible. If necessary, use site walls, earth berms and landscape features as tools to distinguish clear paths and routes. Separations should be architecturally pleasing, and designed as an asset to the Villages. Other devices may include raised curbs and walkways, masonry walls, bollards, control gates and operable gates with card readers. When applying these methods of separation, utilize materials that will complement the building materials and meet the overall design concept implemented in the development. For example, install benches that are similar in color and style with trash receptacles, signage or pedestrian light bollards.

Design walkways to include safety features such as bike racks allowing for adequate dismounting space. Strategically locate barriers such as raised planters to define paths or seating areas. The intent of these features is to prevent dangerous shortcuts.



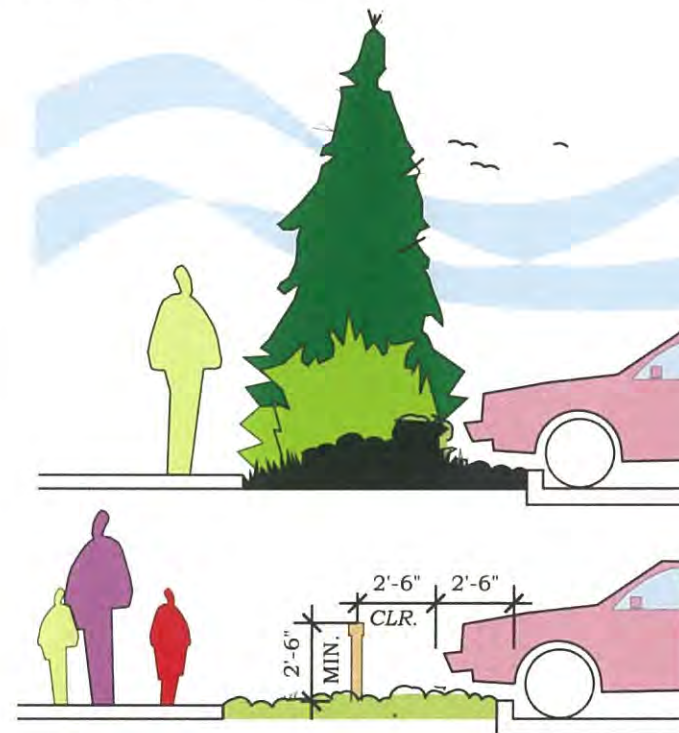
PROVIDE HANDICAP AND DIRECT ROUTES TO BUILDING ENTRANCES



PROVIDE MEANDERING SIDWALKS AT PERIMETER OF SITE.



PROVIDE LANDSCAPING BETWEEN PARKING AND BUILDINGS.



SCREEN PARKING LOTS AT PERIMETER OF SITES W/ LOW WALLS OR LANDSCAPE SCREENING.

E.5 BICYCLE ROUTES AND BICYCLE PARKING

All developments are required to provide and identify bike routes that are functional, easy to use, pleasurable, and are sized sufficient for all users of the Villages. Bike routes are required to connect all areas of the Villages, to allow users to work and play in the same community. Bike routes are required on one side of all public arterial streets. This will interconnect the community, and future development at the Villages' perimeter. No bike paths are allowed within 200 feet of the I-25 right-of-way. Neighborhood trails must also be connected with parks, businesses, residential developments and Village amenities. Provide meandering routes away from streets that inter-mingle with interior green spaces, water features and resting areas. Each project must connect and utilize directional routes to advance the live/work/play community concept. Each development will be required to provide near-road and off-road bike paths and sidewalks as needed to maintain continuity and connection of the community regional system.

One bicycle space is required for every 30 vehicle parking spaces. Round up fractions to the nearest whole number. Bike racks must be located near major points of building entries and provided at points of interest such as water features, resting areas etc. Buildings over 100,000 G.S.F. will be allowed to reduce this requirement by 25% and buildings over 300,000 G.S.F. will be allowed a 50% reduction. Bicycle parking must be located in concrete paved areas not to exceed 2% slopes and racks must be pre-finished in a "U" style and secured to the ground.

F. TRANSPORTATION

The Villages at Johnstown are ideally located adjacent to the heavily traveled Interstate 25 corridor. This Interstate is the most traveled corridor in the Rocky Mountain region. Other major roads and connecting routes make the Villages an ideal location. This development will encourage a well-connected community, as well as offering exceptional access and connections to the areas around them.

The Villages at Johnstown is intended as a live/work/play community. Through the cooperative effort of the developer, community residents, the Town of Johnstown, and the DRC, the Villages can strive to support a self-sufficient community. The Villages will be a community in which residents can still be within reach of other cities, the lively I-25 corridor, nearby businesses, and connecting residential communities.

F.1 TRANSPORTATION DESIGN

All parcels and uses within The Villages at Johnstown will be united by new and future transportation systems. A network of linking transportation systems has been proposed as an integral element within the Villages. All arterial and collector streets are planned to benefit all sizes of vehicles within the Villages.

F.2 STREETS

Arterial streets in and around the Villages must meet the Town Transportation Plan specifications and be built for the developments' initial and final phases. All roads must be designed to accommodate emergency vehicle access.

Emergency vehicles must be able to service the development freely and without obstructions. Overhead clearance for bridges must meet state and federal requirements. Canopies, roof

overhangs, servicing drive thru and Portecocheres may be lowered to 12' minimum provided no service utilities are proposed beneath them. Otherwise, they will be required to meet all code, zoning and judicial requirements.

The primary purpose of the Performance Standards is to establish and implement a "road map" for the development of a thriving community along the Interstate 25 corridor. The standards are intended to provide meaningful and flexible design solutions, and to develop the Villages as a thriving center. Architectural features play a key role in the community development.

The Performance Standards will set the guidelines for the East, West, Center and Industrial Villages.

G. ARCHITECTURAL

G.1 MASSING AND COMPATIBILITY

Each structure must relate to each other and be in harmony with adjacent structures. Single family development is encouraged not be similar, but to appear as individual and stand-alone structures. This will be true for custom homes, but also encouraged with tract style developments. High density housing developments must be designed to be sensitive to adjacent property, as well as provide a building scale that is in keeping with adjacent buildings and views.

Set building footprints so structures maintain preferred views for future development. Position structures in groups to create useful and functional gathering spaces. Locate site entries to maximize their solar orientation and effectiveness. Address topography and natural site conditions through the use of walkout basements, sub-terrain levels and stepped building foundations. Provide desirable design features and use forms that provide shade and shadow lines as well as introduce creative massing, a sense of dimension and building up of forms. Embrace architectural compatibility as it relates to neighboring structure setbacks, landscaping, orientation, massing, details, features, textures and color, fenestration, height, parking schemes, shapes and building materials.



G.2 ALLOWABLE HEIGHTS

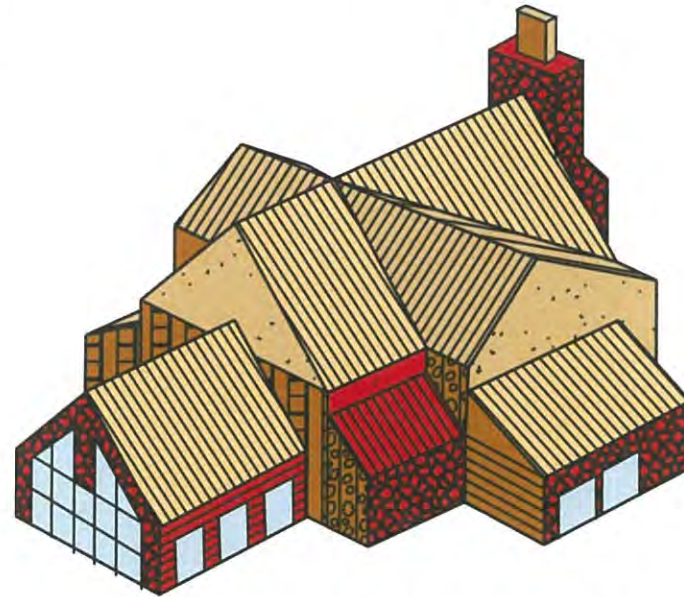
Allowable heights throughout The Villages at Johnstown vary within the development. Each building, structure or manmade element must address designs that respect and relate to the “human scale”. Structures must have an “architectural order” as a stand-alone building, as well as considering adjacent sites. The Villages will encourage creativity. However, if this order is not acceptable to the DRC and JRC then a general order of “top, middle and base” or a well-defined composition and balanced solution will be required. Structures should always be grounded and have a mass to the base of a structure softened by landscaping and people spaces. Heights are anticipated to fluctuate, thus, buildings over four stories must complete shadow studies and site analysis of the impact of shading and shadow ground profiles.

Structures adjacent to open spaces may take advantage of natural sunlight and air. Buildings must make every attempt to allow un-obstructed views to the Front Range for adjacent projects. Designers must consider prevailing winds, and the impact of wind on designs. Examples include vestibule door operations, tunneling wind effects, lateral wind loads on fenestration, awnings, and roof uplifting. Buildings heights can also be impacted by topography and may necessitate walk out levels in lieu of with expensive earth retaining walls. Designs may encourage relationship, compatibility, transitioning, clustering and building up of massing as it pertains to individual or groups of buildings.

In general, use the following maximum building heights:

- High Tech Research & Development: 6 stories
- Corporate Campus: 12 stories
- Corporate Offices: 10 stories
- Professional Offices: 7 stories
- Light Manufacturing: 2 stories
- Aviation and Distribution: 2 stories
- Commercial/Retail/Hotel: 13 stories

- Multi-Family Residential 6 stories
- Transit Stations 8 stories
- Single-Family Residential 3 stories



PROVIDE BUILDINGS OF INTEREST WITH ROOF FORMS, MASSING OR A VARIETY OF BUILDING MATERIALS.



PROVIDE A VARIETY OF ROOF FORMS.

G.3 FORMS

Fit architectural forms and selected materials to the natural topography. Buildings must be designed as a balanced composition with forms and elements compatible to the human experience of the buildings, and spaces created. Buildings must have a “sense of balance” as it pertains to form, massing, material selection, and color. Elevations with a consistent spacing of columns, punched openings, loggias, or wall elements that relate to passing vehicles and pedestrians can address scale at grade. Simple elegant detailing and designs will enhance the community and be an asset to the Town of Johnstown. Celebrate architectural elements and utilize features that lift the human experience.

The pedestrian experience near the base of the building, as well as the approach to the building, is worthy of study. Pedestrians are drawn to elements that suggest direction of the path. Assist the user in defining areas of use that relate to the pedestrian. Building mass should attempt to achieve a relationship with the human scale by transitioning down forms and providing warm colors and variations of texture. The building up of landscaping color, texture, and shapes can also make the pedestrian experience more pleasing. Use roof elements to shelter the pedestrian and automobile passengers at drop off points. Roof elements at grade level can be an excellent method to reduce scale to the human level.

If possible, provide balanced compositional facades on buildings through the expression of their structural elements. Break down large walls with relief, recessed or applied functional architectural elements. This diminishes massive facade expanses. As a general rule, massing should be reduced to increments of 40’ -50’ L.F. maximum (or less) both in height and width.

Provide a variety of architectural treatment in an effort to reduce large massive forms. At the lower levels introduce trellises, arches, engaged wall columns, arcades, canopies, and upscale detailing. Single story buildings over 100,000 GSF will be allowed larger wall surfaces if landscaping materials or other architectural

devices are used to soften the massing. However, avoid blank walls where ever possible. If unavoidable, place blank walls at the rear of developments, away from public views.



EXAMPLE OF RETAIL

G.4 ROOFS

Roofs are an important design element in the coordinated appearance of the Villages. Each development must try to introduce an assortment of diverse roof forms. Parcels must also consider the vantage and viewpoints of other parcels. Views from taller surrounding buildings looking down on nearby roof forms should not see unsightly straps, rods, and kickers or false façades or fake roof elements.

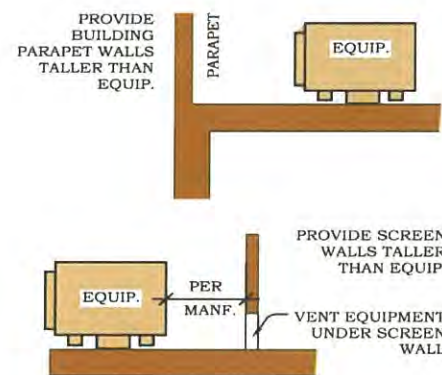
Blending of massing, pitch and direction on buildings create a "roofscape". Parapet and roof forms that are flat, without undulation or change in elevation are discouraged. A diversified mix and change of roof elevations provide relief to an otherwise plain and ordinary design. Mingle scale, line, and volume. Fuse forms to create individual characteristics to each new development. Exposed flat roofs are discouraged. If implementing flat roofs, disguise them with attached elements such as awnings, dormers, oversized parapets with curves, or step up of facades and cornices. Construction materials must be of a durable quality. Utilize natural stone roof tiles, clay tiles, and concrete tile with pre-finished standing-seam, copper, synthetic cedar shingles. Incorporate a variety roof forms and roof materials, and screen all rooftop equipment. Capitalize on roof slopes, pitch, and roof cavities to conceal rooftop equipment. Provide decorative and innovative screens to shield equipment, and match screen materials to primary building façade materials. Exploit roof forms for features that establish entry or vehicular drop-off zones. Use such roof elements for pedestrian protection from the elements interlaced with columns, arches, trellises, service protection etc. There are many roof shapes and styles to consider, such as mansard, pyramid, gabled, gambrel, arched or asymmetrical. Provide protection from the sun in play areas, public pick-up and drop-off zones, and rest areas. Architectural detailing including finials, cornices, barge boards, brackets and snow guard standards are also encouraged.

G.5 MATERIALS AND COLORS

All materials are to be long lasting and manufactured of sound quality or enduring materials. Use complementing palettes of textures, finishes, colors and materials to meet the highest aesthetic standards. Materials must be approved by the DRC and JRC. Submittal of material boards and colored renderings will be required for all development. Warm, welcoming materials are suggested. Individual architectural design schemes are encouraged in the Village. Each proposal will be reviewed on a case-by-case basis. It's been said that there are no such thing as bad colors, only bad combinations of color. When it comes to color, preferences are intensely personal.



CREATE BUILDINGS THAT HAVE A SENSE OF HUMAN SCALE.



PROVIDE STONE, OR BRICK AT BASE OF ALL BUILDINGS

Construction material allowed:

- Brick
- Stone
- Textured concrete
- Masonry block
- Wood is appropriate only in limited details, and must be weather treated.
- Stucco with a high degree of detail is allowed above stone or brick wainscoting, and may not comprise more than 60 % of any building
- Pre-cast and site cast concrete with relief detailing is allowed provided other materials such as stone, slate, or brick are used in combination with concrete. Brick mixed with CMU masonry or stone is encouraged.
- Fabricated metal frame buildings are allowed, provided that they are "skinned" with a mixture of veneer materials listed above. Fabricated metal buildings are subject to approval by the DRC and JRC.
- High quality metal skins such as

"Alucabond" (or similar systems) as trim or special features are allowed but must be pre-approved by DRC and JRC.

Select materials that reduce glare, and do not use materials that will create "hot spots" or oil canning. No mirrors, reflective glass or snow-white colors are allowed. The Villages encourage the use of indigenous materials such as natural stone or masonry materials on the base. Materials can be used as a wainscoting to the buildings to architecturally anchor them to the ground, and prevent unsightly splash up of rain and moisture. Maximize the use of low or no maintenance building materials. Select materials that will stand the test of time.

Color is encouraged in the Villages but must be tastefully interspersed into projects. DRC and JRC approval must be given to proceed from the preliminary phase, and a colored rendering will be required to present "color proposals". Innovation and creative designs are encouraged. Subdued or muted colors are more acceptable to the public. Warm palettes with indigenous materials are recommended.

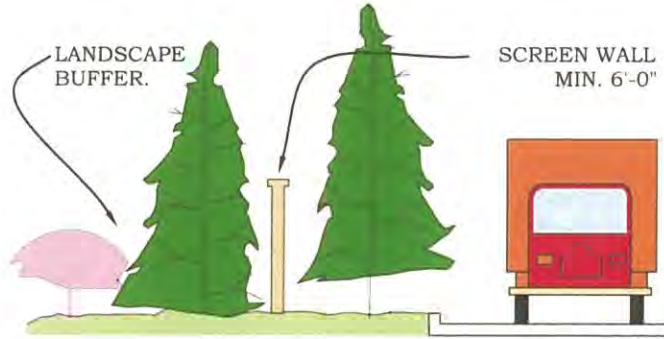
G.6 ENTRANCES

Entry must be visually identified for visitors. Primary elements or points of entry are encouraged to stand out, and be enhanced with architectural features. Entrances must be visible to users from streets, approaching drive lanes and pedestrian paths. For commercial developments, entrances are suggested to be protected with "cover" and be designed with an air lock or vestibules. Identifying entry with signage, roof forms or architectural expression aids the user.

H. LANDSCAPING

G.7 SERVICE AREAS

Truck courts, loading, outside storage and all service areas must be screened from public view. Screening may be made of structurally designed screen walls, landscaping, or building configurations. Locate service and trash pickup areas near service roads and minimize service interface with the public, pedestrians, parking areas and drive lanes. Screen walls must be softened by landscaping buffer zones and plant materials that will reduce the visual impact of these walls. Minimize the impact of service areas with neighboring buildings and views from these buildings into the service area.



SERVICE AND TRASH ENCLOSURES MUST HAVE SCREEN WALLS WITH LANDSCAPING.

When using landscaping for visual buffers, provide the correct density and layering of planting materials to adequately screen desired areas. Consider plant material performances, seasonal uses, and life expectancy to ensure screening effectiveness.

G.8 CONSERVATION

All new development must try to conserve energy and resources to be sustainable. Creating designs with passive and/or active solar designs is intelligent design. Employ site orientation, natural site factors and microclimate analysis for all development. These collective influences will maximize the use of solar gain for energy savings. Environmentally sound design will benefit all areas of The Villages at Johnstown.

Possible concepts to be considered shall include, but are not limited to the following:

- Solar orientation
- Natural sunlight and day lighting
- Energy conservation
- Water conservation
- Passive design solutions
- Active design solutions
- Sun shading devices
- Microclimate factors
- Sustainable building materials
- Natural materials
- Recycled materials
- Building insulation
- Natural ventilation and smart technology
- Airtight building skins
- Glass shading coefficient
- Energy saving and smart technologies
- Grey water systems

H.1 GENERAL

The Villages at Johnstown will implement consistent plant material and landscape themes. Commercial, single family residential and larger, multi-parcel projects must use harmonious landscaping. Landscape design is crucial for creating a consistent, high-quality community. A unified landscape plan creates a cohesive design development and creates a pleasant experience within the boundaries of the Villages. While unity is encouraged, individuality can still be utilized. Individual landscape treatments for sites must complement adjacent sites, streetscapes and encourage distinctive settings.

The minimum landscape area required within each building site is 20%. Landscaping must always be in compliance with landscaping standards and specifications available from Johnstown.

Landscaping must be included for all new commercial and residential developments. Landscaping is required to:

- Enhance the Villages
- Define spaces, paths and provide separation
- Provide color and foliage
- Visually “soften” parking lots
- Improve the pedestrian experience
- Provide “green screens”
- Provide scale and massing
- Beautify “hardscapes”
- Reduce heat and provide shade

For all plant material designs, incorporate low water usage and conservation measures. Include indigenous and low water consumption plant materials.



CREATE ENTRY PLAZAS WITH LANDSCAPING

H.2 LANDSCAPING STREETSCAPES

Streets within The Villages at Johnstown shall be accompanied with landscaped boundaries. These integrated green zones should be used at collector and arterial streets, intersections, monument signs, ground mounted signs, entries, and parks. Plant species, grouping, spacing, and height similarity must be a common theme in the Villages. Open space, parks and amenities should be landscaped as visual amenities, and are an integral part of the overall Villages concept. A variety of smaller landscape themes can be used to express individuality, but caution should be taken to ensure that these themes do not overshadow the continuity of the community. Grading and material selection must perform together to create a varied landscape experience.

Streets should have trees planted in compliance with Johnstown landscape standards. All medians should be planted with trees and flowering perennials consisting of low shrubs, grasses and ground cover. Streets and

roadways must have a flowering plan that is in harmony with the Villages theme.

Detached and meandering sidewalks will be the standard throughout the Villages. Sidewalks are required to link every building, park, amenity and feature. In addition to trees, provide a variety of flowerbeds, perennials, mulch, rockscapes, ground cover and grasses.

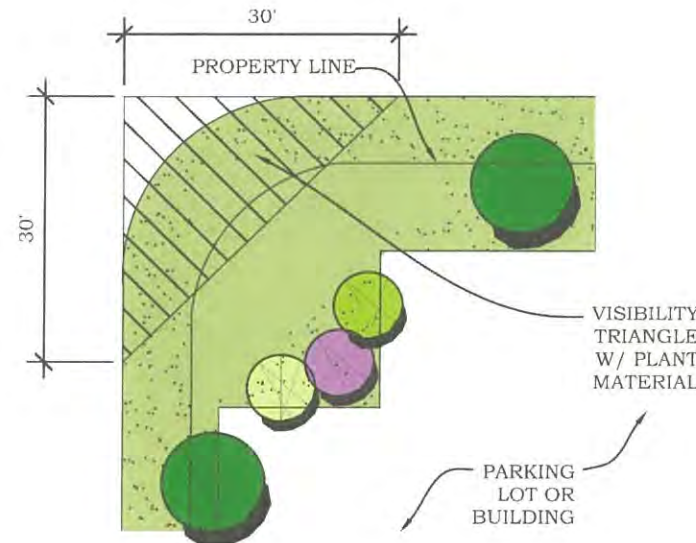
Attached walks are discouraged. They are only allowed at transitions between streets, pedestrian crosswalks and/or attached to plazas or entries to buildings.

Sidewalks near streets must embrace a combination of manicured and natural landscape beds. At primary entrances, signed medians and signed curb cuts, place a manicured flowering bed that enhances signage without blocking sign text or creating hazardous conditions. Landscaping should be low to the earth and not exceed 30" in height. Landscaping should frame and assist with the focus for signed elements.

At primary entries, designers are encouraged to provide an abundant level of plant materials, providing color and texture throughout the year. Provide a "building up or structured massing" of landscape forms low to high, or a balanced assortment of plant material which enhances the experience of pedestrians. Provide landscaped areas at entries, and at ground mounted signage. Landscape along the peripheral edge and bend with adjacent sites. Install a minimum of four types (or categories) of landscaping materials. Integrate the plant design with the entry sign and pedestrian path. Plantings should frame or provide a visual base for the signs and path. Sight triangles are necessary at every corner. Low plantings in the site triangle are required. Along private streets sight triangles are required at a min of 30'; at major or public streets site triangles are required to be 50'.

Sight lines must be unobstructed with low, mature plant materials no higher than 30", or provide trees with canopies that begin at 8' above roadway paving.

Sidewalks are required to be 10' wide and 6" deep of reinforced concrete at all arterial streets. Most other sidewalks will be required to be 5' to 8 feet wide, and no sidewalk will be allowed less than five feet wide.



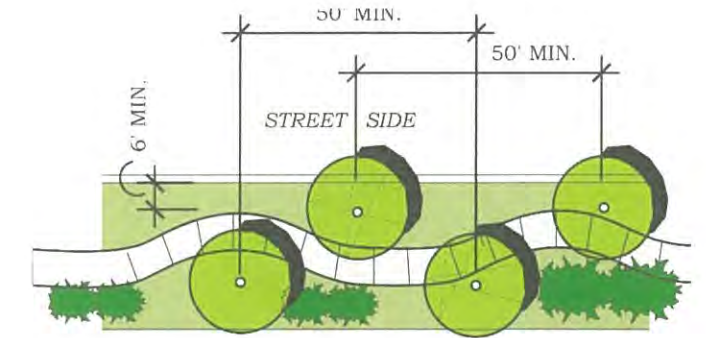
PAVING AREAS LOWER THAN PERIPHERAL ROADWAYS.



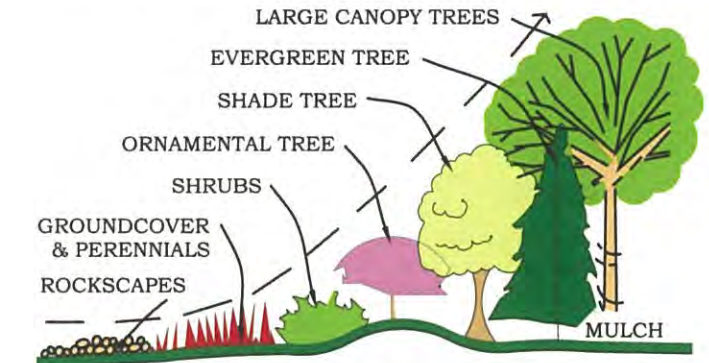
EXAMPLE OF STONE RETAINING WALL.



INVERTED 'U' STYLE BIKE RACK USED FOR VARIOUS BIKE TYPES AND SIZES.



TYPICAL TREE SPACING ALONG STREETS



UTILIZE AN ASSORTMENT OF PLANT MATERIALS AND A "BUILDING UP" OF PLANT MATERIALS. SHOWN ARE TYPES OR "CATEGORIES" OF LANDSCAPING.

H.3 PARKING LOT LANDSCAPING

Disperse landscaping in parking lots to break up the "sea of pavement" appearance. This will also provide welcome shade. This landscaping will also break up the massing that parking areas ultimately create. Properly landscaped, parking lots can help reduce heat and provide visually serenity. The parking at The Villages at Johnstown must be designed and orchestrated to have a sense of continuity from one parcel to the next.

Where possible, provide landscape berms at perimeter streets vs. parking lots. If not possible, provide low screen walls or "screen walls of landscaping". All developments are required to screen parked vehicles from public view. Developments should encourage views of the architectural elements, in lieu of parked vehicles. If grading permits, place interior paved areas lower than peripheral roadways.

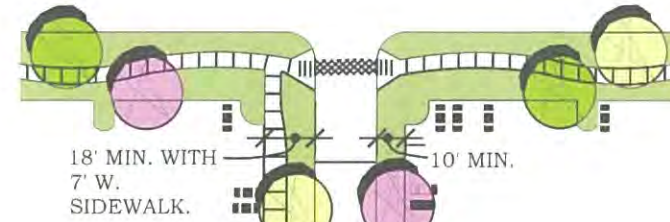
Rows of parking may not exceed 15 spaces. Each 15 spaces require a minimum of one tree. The trees must be in landscaped islands with curb and gutter not part of the perimeter zone. Each island is to be planted with an additional six low shrubs along with ground cover. Wherever 15 spaces face each other (in parallel) the island must double the formula for planted materials and must be a minimum of 10' W. x 36' L. Also utilize these islands with raised sidewalks with pedestrian connections. Islands are required to be 10' wide and in the event, sidewalks are located within a landscape island; they will be required to be 18' wide. Utilize landscaped islands and medians to improve pedestrian circulation, shading and break up constant rows of parking.

Incorporate 5' minimum wide sidewalks in strip islands with planting bed, ground cover and sod on each side of walk. For shared drive lanes, curb cuts and shared points of access provide a minimum of a 9' wide landscape zone without sidewalk, and 12' - 18' with sidewalk.

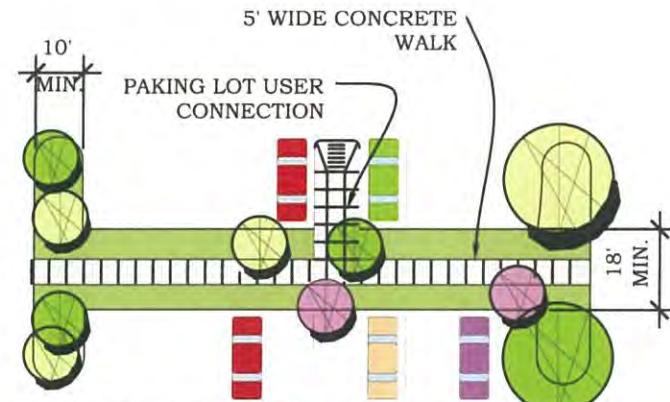
Landscape berms and medians that interface with car overhangs must utilize plant materials that will not mature above 6" high. Sidewalks that are integral with curb and gutter are required to be a minimum of 7' W. Plant blooming species with changing color adjacent to pedestrian walkways. Where medians do not have sidewalks, the medians may be reduced to 10' W. with sidewalk, medians are required to be 18' W. Wherever possible landscape slopes between parking and on adjacent hillside must not to exceed 3:1.



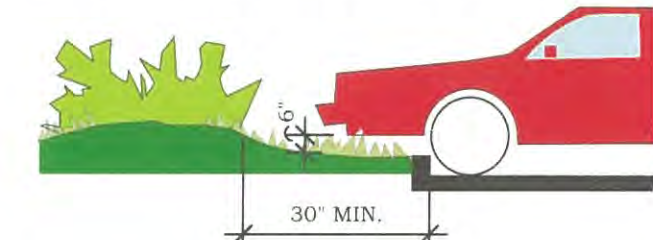
LANDSCAPE BETWEEN ALL DISSIMILAR USES.



LANDSCAPE BUFFERS AT CURB CUTS



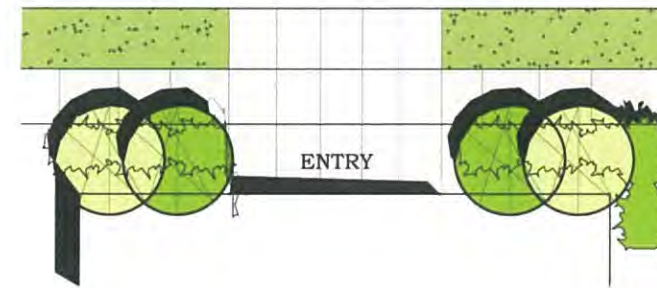
PROVIDE PEDESTRIAN PATHS THROUGH PARKING LOTS. LANDSCAPE EACH SIDE OF SIDEWALKS AND PROVIDE AUTOMOBILE USER CONNECTION TO 5' W. SIDEWALK.



LOCATE PLANT MATERIAL WHICH GROWS 6" OR HIGHER 30" BACK FROM CURB TO PROTECT LANDSCAPING FROM VEHICLE DAMAGE.



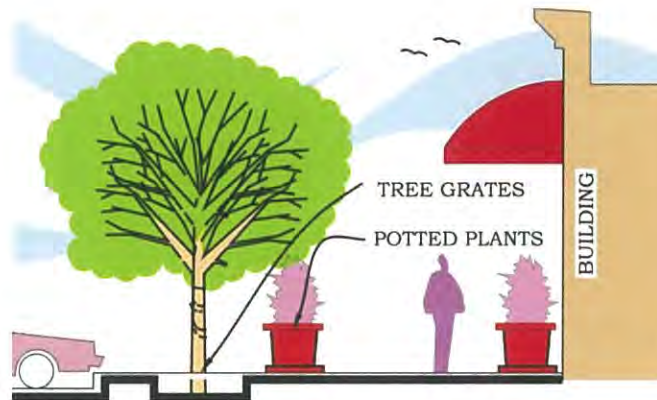
EXAMPLE OF AN INDUSTRIAL BUILDING



PROVIDE LANDSCAPING BETWEEN BUILDINGS AND SIDEWALKS AS MUCH AS POSSIBLE



USE SCREEN WALLS, RAISED PLANTERS OR OTHER DEVICES TO PROTECT LANDSCAPING FROM PEDESTRIANS AND AUTOMOBILES.



IN HARDSCAPES ADJACENT TO BUILDINGS USE TREE GRATES AND POTTED PLANTS

H.4 LANDSCAPING TRANSITIONS

Landscape "zones" between dissimilar uses and "visual landscape buffers" between similar uses are necessary buffers for the Villages. Compatible plant material can accomplish visual transitions.

This applies to landscaping between dissimilar uses, including any change in occupancy, use, or zoning between buildings or parcels.

(Example would be a retail building abutting an office building or industrial building abutting an office building, etc.) In these cases, between buildings (doesn't include parking lots) provide a minimum of a 30' wide planting strip incorporating a 3' high berm containing a minimum of 1 tree for every 20 lineal feet of property line or a 3' high landscape screen that will retain winter color and incorporate a dense mixture of deciduous and evergreen shrubs. Another option would be to provide a 6' high fence in combination with landscape. Interface stone or brick columns at 100' O.C. Column materials should match the architectural elements in nearby buildings and 40% of the fence must be covered in front with landscape. Fences cannot exceed 50% of the cumulative length of a parcel's boundary.

For landscaping between similar uses, provide a minimum 15' wide "zone" containing 1 tree for every 30 lineal feet of property line and a landscaping screen hedge that includes a combination of evergreens and deciduous materials an average of 3' high.

H.5 LANDSCAPE WATER CONSERVATION

All landscaping designs should attempt to conserve water. Landscaped zones operating irrigation methods must take advantage of water conservation techniques by implementing low water management, sensors, selective plant species, and non-potable irrigation water systems.

Landscape architects should design planting strategies to minimize irrigation demands. Group analogous mixtures of low-water indigenous and native materials away from primary entries, and heavily used pedestrian areas. Species must be disease and drought resistant. Integrate mulched zones and beds of plant materials which limit excessive moisture loss. Prepare soil and planting beds to improve water absorption and to safeguard against moisture depletion.

H.6 LANDSCAPE STANDARDS

Mass or align plant species for visual interest. Planning schemes should accessorize, balance and complete a development.

Landscape development within The Villages at Johnstown should fall into one of the following zones:

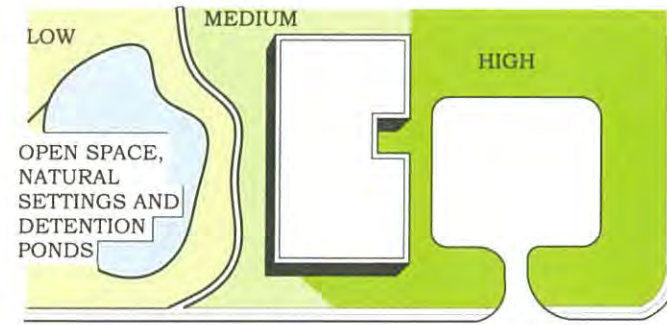
Zone One: Located at primary pedestrian plazas, walks, entry, sitting areas and primary vehicle access points. This zone may require a high degree of maintenance. Use decorative schemes with an assortment of species and variety of materials, including perennials and annuals with a maintenance program.

Zone Two: Located at perimeter of property along streets, drive lanes and secondary entries. This zone will require less maintenance and less familiar plant materials.

Zone Three: This area is located in susceptible areas near waterways, ponds and open natural settings. Typically, these will be open fields, meadows, wetlands, foot trails and undisturbed areas. Deliberate attention must be taken in plant selection and materials for these areas. Select species of plants which endure seasonal changes, and provide diversity. Carefully place plants in visible areas to maximize interest and/or to alleviate erosion. Include a variety of wood and rock mulched to minimize erosion.

H.7 MAINTENANCE

All owners, developers, and landlords are responsible for maintenance year-round. All landscaping must be vibrant, hearty, vigorous and in healthy condition. Plant materials which have died must be immediately replaced with equal or higher quality, and must be of similar scale and quantity.



LANDSCAPING ZONES.



REPLACE DEAD LANDSCAPING AS SOON AS POSSIBLE.

H.8 FENCES AND SCREENING

Screen walls, trash enclosures and fencing must be built of similar materials, matching the architectural materials of the adjacent buildings. These walls should also be softened with a combination of landscaping and landscape screening.

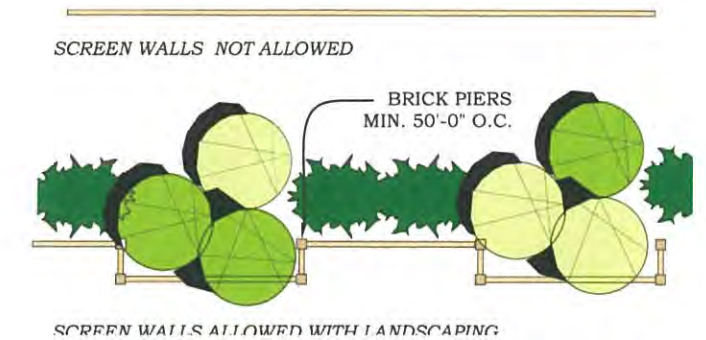
Earth retaining walls should be limited in height as much as possible. All zoning and building codes will be in effect for earth retaining wall systems or engineered walls. Avoid walls adjacent to pedestrian walks or drive lanes as much as possible. If unavoidable, provide handrails, guardrails and safety zones. Stagger small retaining walls in lieu of one massive wall.

Provide landscaping along retaining walls, fences, screens and enclosures. Enhance these walls with "green zones" of plant material. Undulate walls and create pockets of landscaping. Offset walls and create wells of landscaping materials and create a pocket series of beds to provide a progression, succession and rhythm with the wall and landscaping features. Include architectural elements using columns, cornices, caps and inserts in walls for added architectural interest. Walls should be finished on all sides with durable and aesthetically pleasing materials. Brick and stone are encouraged and a combination of iron and steel decorative fencing can be considered.

Screen walls for trash, electrical transformers, or walls that screen electrical panels must be opaque. Landscaping is required at these walls but to a lesser degree. Wood, diamond-link and chain fences are not acceptable screening materials. All metals must be coated or finished at the factory.

All types of walls must be an integral design element with the architectural palette. Interface buildings, wall and topography to create visual interest and well thought out designs.

Design walls to appropriate heights for the task. Example screening large service trucks (semi-tractor trailers) will require taller walls. Electrical transformers come in varieties of sizes and must be screened accordingly. All fencing and screen wall will be reviewed and must be approved by the DRC and JRC.



EXAMPLE OF TRASH ENCLOSURE WITH BOLLARD PROTECTION.

I. LIGHTING

Site lighting must be incorporated for all commercial and residential parcels. Metal halide fixtures are the accepted type of light source for The Villages at Johnstown. Lighting must be provided for the security and safety of all users, and is subject to approval by the DRC and JRC. Lighting provided for a particular site is not allowed to “spill” beyond the limits of its boundaries, and “blackout style” fixtures are required.

I.1 LIGHTING FIXTURES

All lighting fixtures, standards, and bollard lighting must be of the same “pedigree” of fixtures. They should match in color, style and aesthetics and be compatible throughout the Villages. Consult the Town for footcandle requirements and provide a safe lighting environment for all users. DO NOT allow offsite lighting spills and/or glare to adjacent properties. Provide solutions that enhance pedestrian lighting. Site lighting should be provided at the minimum level (per Town Standards) to the degree which illuminates traffic movement zones and parking areas. Photometrics will be required with all submittals to the DRC and JRC.

Select standards and fixtures that are architecturally compatible with adjacent properties and architectural styles. It is important to illuminate all high traffic zones where vehicles and pedestrians interface, as well as curb cuts and entries. Use cutoff style fixtures which are adjustable and shield light pollution.

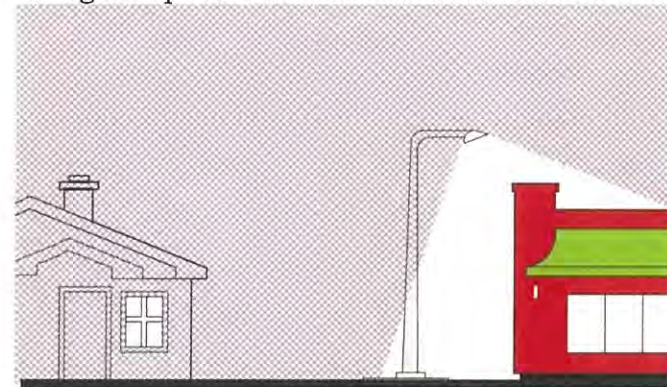
Lighting should be considered for safety first and illuminate sites without “over lighting” an area. Lights should also be placed on photo cells with a manually adjusted timer switch for all exterior light fixtures.

Poles and fixture heads should be architecturally enhancing, and compatible

within a development. Architectural lighting is allowed on buildings and may be allowed to cast upward and downward to create a “soft wash” lighting feature. However, these soft wash areas are not allowed to be overly bright and disturb adjacent properties. In no case will they be allowed adjacent to single family housing.

I.2 DECORATIVE LIGHTING

Accent lighting and architectural lighting such as wall sconces, exterior torchieres, bollard lighting, ground lighting, in wall, stair riser light fixtures and other special lighting that accentuates building functions are acceptable in the Villages. These are allowed in the Villages provided that they are designed as a feature or to light an amenity, building or provide a concept that enhances the overall Villages experience.



PROVIDE PHOTOMETRICS FOR EACH NEW COMMERCIAL DEVELOPMENT. DO NOT ALLOW LIGHT TO “SPILL” OUTSIDE OF PROPERTY LINES.

Such lighting is allowed provided that it augments architectural or landscape features. Example would be a landscape bench sitting area, up lighting on landscaping features, spot lighting on entry facades, pedestrian plazas, specialty signage, art and other architecturally or site-specific elements of interest.

I.3 PARKING LOT, PEDESTRIAN, LANDSCAPE LIGHTING

Parking lot light fixtures should be located in areas that shall not be a hazard to vehicle parking, and provide the necessary clearances for pedestrian traffic. At perimeter areas

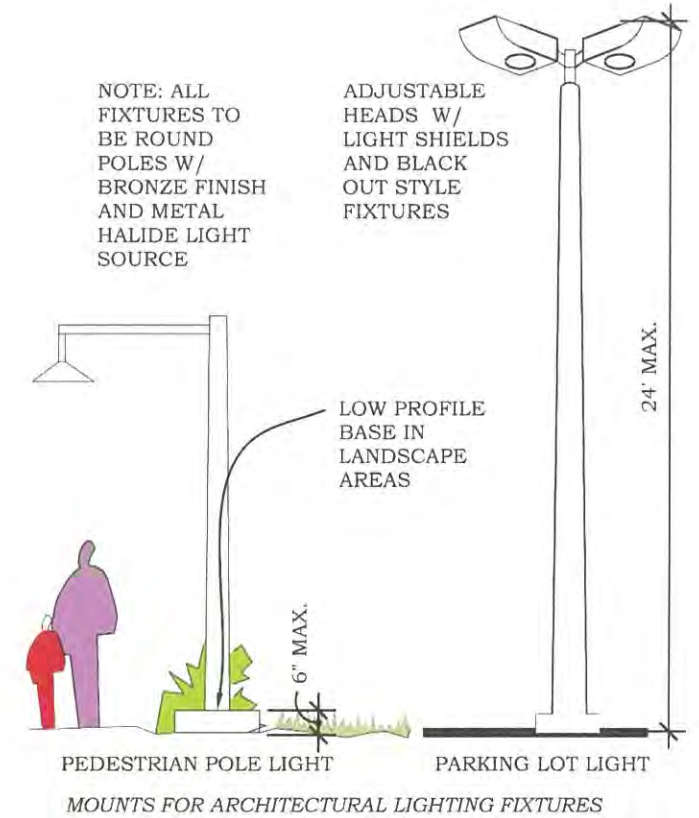
locate light a minimum of 3’ deep into landscape zones thus preventing the nosing (overhang) of vehicles striking them. Locate light poles in parking lots at the “quadrant point at paint line intersections” (at the intersection of four stalls) to create as much clearance as possible for vehicles pulling into the parking space. Standards shall be unassuming as much as possible. Fixtures not call attention to themselves. They must be positioned in locations to prevent damage to vehicles, and provide lighting for security and safety. All light sources are to be metal halide with uniform distribution of light levels.

Path lighting must be selected for the pedestrians, typically 4’ to 16’ H. In all cases select the appropriate footcandle to ensure a safe means of access. Use path lighting techniques for such areas as stairs, bike racks, railings, sloped walkways, site amenities, etc.

Implement lighted pedestrian bollards along path, pick-up zones and entrances. In all cases provide safety lighting at pedestrian-to-vehicle intersections.

Lighting may feature plantings, building elements, fountains and other amenity features by a number of lighting techniques and provide for a dramatic “after hour” concept.

Electrical engineers must take into consideration lighting for seasonal cycles of plant materials. Designers must consider lighting conditions as they relate to plant sizes at installation including sequence and finally the mature size of the plant to achieve the most desirable lighting outcome. For landscaping areas only, concealed fixtures or black out style lighting is allowed. Provide lighting in ground vaults, buried in rock beds or screened by rock or plant formations. Control glare, and avoid damaging hot spots on live materials. Use only lights that are less susceptible to vandalism and require little maintenance. Lighting fixtures must be of high quality and impact resistant.



Public street lighting must adhere to the Town of Johnstown design standards. Location of lights must be positioned to maximize public safety and be a decorative “breakaway” davit style pole/ shaft assembly and are subject to DRC and JRC approval.



USE LIGHT BOLLARDS AT PEDESTRIAN ROUTES.

J. SIGNAGE

Signs are a vital component of any development and The Villages at Johnstown understands the importance of signage to owners and visitors. Signs must play an integral part of the Villages community. Signs should contribute to the visual correlation within the entire area. They must also be properly landscaped and lighted. All projects are required to include a signage program that includes project identification at each vehicular entry as well as address mounted signage, building mounted signage, handicap signage and tenant signage. All signs should take into consideration materials, shapes and size, illumination, style and font typeface, structure and compatibility.

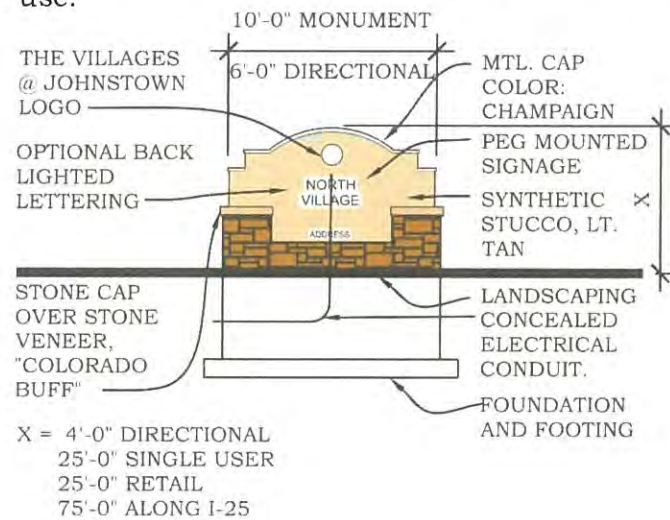
All signage selections for The Villages at Johnstown are required to be submitted to the DRC and JRC, and must meet signage codes for the Town of Johnstown. The most restrictive of interpretations shall govern.

J.1 MATERIALS

Signs shall be fabricated to be sturdy, self-supporting and able to withstand wind loads. Electrical transformers, switches and conduit must not be visible. Signs must be reflective of the architectural signature consistent with the development. All signs at points of entry and street curb cuts must follow the prototype sign (shown to the right). This sign consists of stone base with light tan stucco field and champagne cap. Specification can be obtained by the DRC. The "Villages @ Johnstown" logo must be on all projects with the exception of the individual single-family residence. The logo can be found on the cover page of this Performance Standard in the lower right-hand corner.

J.2 SHAPES AND SIZES

The size of signage at street fronts and major access points (curb cut) allows a variety of heights, depending on use of land or type of fonts utilized. All parcels are allowed a minimum of a 10' high sign which has a 10' setback. Monuments signs are allowed to increase 1' in height for every additional 1' in setback. Thus, a sign 15' high has a 15' setback requirement. This formula determining sign heights may be used, however, there are maximum heights allowed by the Town. Please check your signage codes to determine allowable heights for your specific use.



Sign must be consistent in design concept and style, however, height and width may be varied to meet the particular needs and objectives of the end user. It is encouraged that the style, general shape and concepts be carried throughout the applicant's signage program.



SIGNS WITH ALL STONE BASES ONLY ARE ALLOWED.



EXAMPLE OF RETAIL SIGNAGE

The following conditions shall apply to signage within The Villages at Johnstown: users/tenants are allowed a minimum of one sign per street side. Directional signs are allowed to help direct, and provide useful information to end users. Each sign must have the "The Villages at Johnstown" Logo. In addition to the logo, text area shall not exceed 50 square feet per side (100 square feet per sign). This excludes address numbers which are to be located at the bottom of the sign, 4" above the stone base, centered, 6" high and champagne colored.

The allowable area for signage on a commercial building shall be computed by the Town of Johnstown signage codes.

Corner signs are limited to 1 per building elevation per user/tenant. The maximum number of flush-mounted signs allowed for each freestanding building is to be determined by frontage. Each tenant is allowed a minimum of 200 square feet.

For multi-tenant retail buildings, if a tenant requires signage on a monument sign then this area will be deducted from the allowable signage allowed by code. Example: tenant has determined they are allowed 200 square feet in signage. They then elect to have 40 square feet placed on a multi-tenant monument, and use the remaining 160 square feet for individual surface mounted lettering on an electrical signage rail in the allowable sign band facade.

In all cases sign codes and building codes must be met, and no exposed transformers or conduit is permitted. The designer must use simple and readable fonts. Logo signage is allowed, as well as corporate artwork. Displays must be clearly readable from a distance, and unobtrusive to others. Signage can be an interesting element in design, and augment the users, experience in the Villages.

No roof signs will be permitted. Street signs and pole mounted signs are subject to DRC and JRC review and approvals.

J.3 LOCATION

All monument signs have a 10'-0" minimum setback from property lines and must not be in the sight visibility triangle. The same visibility triangle requirements apply to private curb cuts along public streets. Signage must not be located in these sight triangles, but are encouraged to be located as near as possible. Each sign must be easily discernable by moving traffic, as well as pedestrians, from across the street or approaching a building on foot or bicycle. Location plays a key role in communicating to the public, and signage locations must be utilized to maximize the legibility of each sign.

Monument signs must also be accompanied by surrounding landscaping, including annuals mixed in with perennials with episodic bloom.

J.4 ILLUMINATION

The use of individually peg mounted letters and numbers mounted 1" clear of the sign surface is encouraged. Use ground vault light sources directed at the sign surface or conceal the light source from pedestrians' and motorists' line of sight. Avoid light pollution and light spill outside of signage pedestals.

Flashing signs are not permitted under any circumstances. However, signs may change on a 30-minute cycle for retail signs only.

Back lit signs are allowed. Lettering must be individual script, and boxes must match pedestal field materials texture and color. Example: if the monument pedestal field is stucco beige, and a user wishes to use an aluminum box with cut out lettering, back lit, this is allowed with the exception that the box must be flush mounted in the pedestal and finished in the same stucco tan color and texture. All illumination transformers and conduit must be concealed within the box, underground in a waterproof vault or remotely located inside a building.

J.5 TYPES

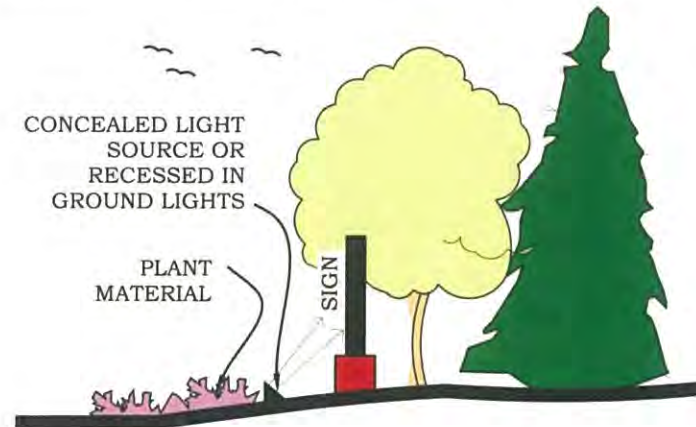
The Villages at Johnstown must have a consistent, approved signage program throughout the development. In general, designs

of signs shall be of similar, compatible materials that reinforce the design and style of the Villages.

The following signs, styles and types can be considered:

- Monument Signs:

These signs must be positioned along the primary roadway leading to a development. Monument signs are also required in center islands, at major points of vehicular entry, and in center landscaped islands serving residential neighborhoods. The monument sign's purpose is to display tenant name(s) and address. Miscellaneous directional signs less than 24 S.F. may be located on sites to provide single user business names and addresses. This is in lieu of monument scale signs.



ALL LIGHTS MUST BE SCREENED FROM VIEW WITH LANDSCAPING, ROCKSCAPES OR RECESSED IN THE GROUND. NO VISIBLE LIGHT SOURCES OR TRANSFORMERS ARE ALLOWED.

Signs may contain logo or "corporation scripted lettering". Multi-tenant buildings or complexes of several buildings may contain removable signage panels for individual users. For multiple tenant buildings such as retail centers, strip malls, flex offices, or multi-story, multi-tenant buildings, signage space on a shared monument sign can be utilized. All signs must be submitted to the DRC and JRC for approval, and the Town for review, approval and permit. All monument signs must use individual letters and/or numbers.

- Flush Wall Mounted Signs:

Flush mounted signs on buildings are allowed provided they do not extend more than 7" from a building face or signage band. They must be low profile, and integral with building facade. All flush mounted signs are suggested to use individual lettering. Signs allow corporate logos, colors and corporate fonts. If corporate art uses script then script is allowed. However, in the event this is not the case then all letters shall be of individual characters.

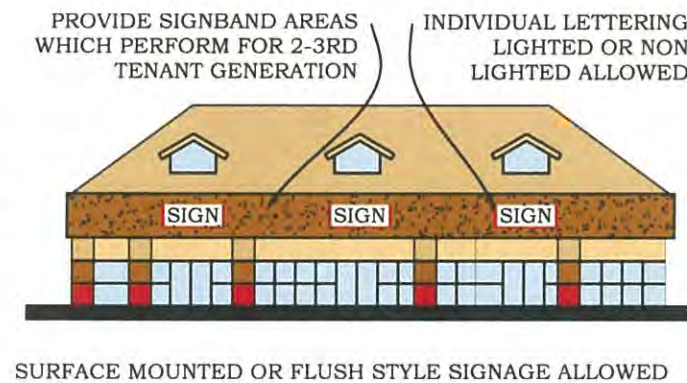
Flush mounted signs are allowed for retail, industrial and commercial use. Typically, these signs are located at grade levels for retail buildings, but are also allowed higher for multi-story mixed use or office buildings.

- Projecting Signs:

Signs may project from a building provided they are a minimum of 12 feet above the finished grade, floor, or slab below them. Signs may not be more than 8 S.F. on any side or extend beyond a surface more than 6 feet. All projected signs must meet ADA.

- Flashing Signs:

Flashing or kinetic signs are generally not allowed. However, the DRC and JRC may upon individual submittal approve of such a sign provided that they do not affect the safety of passing motorist. Signs are allowed with changing displays provide that the interval of change is 30 minutes or more.



- Surface Mounted Signs:

Surface mounted signs on buildings are allowed provided they are securely fastened to substrate. These are typically for address signs, and in some cases used in a sign band application. It is recommended that the letters be a minimum of 1" deep and contract on substrate surfaces.

- Miscellaneous Signs:

Some miscellaneous signs on buildings are allowed while others are not. For example, vinyl signs adhered to interior glass surfaces are allowed for suite address and identification. Signage to identifying service doors and dock doors are allowed for directional information. These signs must be limited in scale without calling too much attention.

Color wash, or paints applied to windows, is not allowed. However, gold gilded lettering with addresses or names can be used, provided they do not exceed 8" in height.

- Pole Mounted Signs:

Pole mounted signs may be utilized for handicap parking, reserved parking, motorcycle parking, visitor parking, directional, and height restriction signage.



EXAMPLE OF PEG MOUNTED SIGN – PEG MOUNTED SIGNS ARE ENCOURAGED.

K. FURNISHINGS

Site furnishings are encouraged with all public and private projects. Furnishings should be an attractive component within the development, and are subject to DRC and JRC approval. It is essential that all furnishing be similar in style and color. For this reason, all furnishings are to be pre-finished heather green unless otherwise approved by the DRC. Furnishings are suggested to be factory finished, not painted in the field. This requirement will be enforced throughout The Villages at Johnstown, in both commercial developments and residential public areas.

K.1 FURNISHINGS

Install benches that are visually attractive and comfortable to sit in. Outdoor seating should be available in rest areas, plazas, bus stops, areas of interest, or in areas that capture views. They may also be considered at building entries and pick up zones under Porte-cocheres or protected zones. Locate furniture in shady areas protected from the sun and out of areas that may freeze and become icy. Allow for a variety of arrangements and sizes. Design planters that allow seating areas that match the architectural building finishes.

Trash and miscellaneous receptacles must also match standard furnishings within the development. Select matching styles, finishes, textures and materials.

Utilize tree grates in areas where other landscaping designs are difficult to implement. This is a great way to “green up” large areas of concrete, plazas, sidewalks and expanses of paved areas. Trees will add welcome shade. Landscape designers must consider canopy sizes at maturity, and not over-size or under-size

trees for a particular application. Grates must also be traffic rated for paths used by maintenance or service vehicles.

Trash enclosures must contain metal dumpster bins with lids located near service entries. Protect enclosures with steel bollards filled with concrete, and paint a “safety color”. Place concrete slabs in all enclosures. Consult your geotechnical engineer and structural engineer for proper slab design. Always extend the concrete slab to support trash truck point and turning loads anticipated in these areas. Trash enclosures adjacent to non-paved areas should include plant material to aid in screening trash enclosures from view.

Small pedestrian trash receptacles are allowed provided that they are in keeping with the seating, benches and light bollards style etc. They must be secured in place, and a trash service program must be in place for daily trash pickup.

Bike racks must be “U” style which can accommodate several sizes and types of bicycles. They must be located to encourage use and ease of accessibility. Position them in activity areas within The Villages at Johnstown. Locate racks adjacent to primary entries, and allow ample space for dismount and passage around parked bikes.

K.2 MISCELLANOUS FURNISHINGS

Select furnishings which complement each other.

L. ENVIRONMENTAL

There are many people and types of uses in The Villages at Johnstown. The Villages have all the ingredients for stimulating economic growth in a self-sufficient community. The setting is ideal, overlooking the Rocky Mountains. Protecting the new development, its land and resources is an ongoing process. The key to the ongoing stability and quality of The Villages at Johnstown area must be shared by all.

Each newly developed parcel should practice environmentally friendly practices. Respecting the environment, smart material selections, and respecting the natural surroundings will achieve an enduring value for all. The Villages at Johnstown encourages sensible and environmentally conscious growth. Utilize and implement environmentally friendly programs, sustainable construction, energy conservation, and other “green” solutions. The Villages at Johnstown encourages environmentally sound designs that will demonstrate a forward-thinking community.

All applicants are encouraged to submit an environmental plan for each development. Conservation of resources such as low water shower faucet heads and water closets help conserve water resources. Selecting recycled building materials, and taking advantage of solar orientation are additional examples. Environmentally sound programs by all new applicants will be required to satisfy the DRC and JRC.

Efficient systems such as computer operated/guided energy controls, recycled ceiling and flooring products, smart technologies and local materials are great ideas that can easily be incorporated. Applicants, developers and owners in The Villages at Johnstown will be expected to “make a difference” in managing sustainable design concepts. Many systems and programs on the market today are cost efficient and may save upfront investments or long run operating cost.



EXAMPLE OF TYPICAL EXTERIOR BENCH FURNITURE

M. CONSTRUCTION

Construction and phasing of new work in The Villages at Johnstown requires essential planning and execution. The development is vast in scale and ongoing construction and maintenance will impact the community. Strategic planning and properly enacted construction methods are critical. All projects will be required to implement methods of construction that will minimize the impact on the community. Owners will be responsible for ongoing operation, maintenance, and services that will minimize any inconvenience to surrounding parcels. Each development will be required to meet with the DRC to review construction procedures from project start to finish.

M.1 CONSTRUCTION STAGING

New and ongoing construction teams must implement strategies to guard against improper methods of construction. Adjacent owners must be notified of short or long-term inconveniences during construction. It is mandatory that any parcel under construction will prepare and have accessible a "Construction Phasing Plan". This plan must outline the schedule of trade services, staging areas, erosion control methods, fire protection, emergency vehicular access, site services, vehicle delivery zones, parking controls, and general construction time frame.

M.2 TEMPORARY FACILITIES

The Villages at Johnstown will not allow temporary facilities other than temporary construction trailers and temporary sales offices. Each must obtain permits from the Town including temporary water, power and communication services. The Construction Phasing Plan must also include the placing and removal of temporary structures, construction trailers, signage, utilities, and schedule the repair, patching or landscaping of the temporary facilities.

M.3 MAINTENANCE

Ongoing maintenance, operations, and site services are required to continually demonstrate the Villages quality of life.

M.4 CONSTRUCTION CONTROLS

Implement a well-conceived construction control plan to manage dust, mud, contamination, etc. on and off site. All forms of construction debris must be kept to a minimum. Eliminate downwind fumes, gasses and construction debris. Minimize onsite trash. Do not allow materials off site and on adjacent property. Sites must be secured at all times during construction. Measures must be taken to provide protective structures to ensure pedestrians, workers and the public are protected from construction. Emergency access must be provided for fire and rescue, and emergency lanes must be clear for all emergency equipment at all times.

All sites must implement mud and debris control onsite and remove mud and debris off site. Contractors are requested to submit a cleanup schedule and will be required to minimize the construction impact to The Villages at Johnstown and the Town of Johnstown.

N. RESIDENTIAL

N.1 SUBMITTALS

The submittal process for Single Family and Multi-Family residential is very similar to the submittal process outlined in A.2, page 3. However, for the single family/single owners the process is abbreviated. To assist the applicant, a "Single Family Submittal Kit" from the DRC is available. This kit will outline the documents and drawings required for DRC submittals.

N.2 ARCHITECTURAL STANDARDS

The items listed below are the Performance Standards for single family residential areas. Upon acceptance of the applicant's preliminary plans, elevations, and materials boards by the DRC and JRC, applicants may apply for a building permit to the Town of Johnstown.

Each applicant must provide construction documents for submittal. Stock plans are discouraged and modular or factory-built dwellings are not allowed. Multi-family plans will require approval of the DRC and JRC prior to applying for building permit. Plans must meet the design guidelines available from the Town of Johnstown.

Builders and owners are required to create a variety of architectural styles in the development. Encouraging diversity is preferred. No single family detached home model elevation should be repeated for a minimum of three lots on either side, or across the street. All homes must have the DRC and JRC approval, and permits from the town.

Please refer to Section K for site furniture information.

Roofing shall be of simulated cedar shake shingle, simulated stone, stone, terracotta,

concrete tile, or other approved materials accepted by the DRC and JRC. In the case of asphalt shingles, a 40-year architectural shake style is required. Roofing materials shall be of earth tone colors and in the case of simulated shingles, may also include dark gray and dark green. Traditional terra cotta colored clay and concrete tiles (a variety of colors) are acceptable.

Side entry garages are preferred. Staggered front yard setbacks are highly encouraged to alleviate garages dominating the streetscape. A minimum of 50% of the front façade of any home will be non-garage area for two-story homes and ranch style homes 30%.

Trim will be required around the perimeter of all doors and windows on all four sides, unless replaced by brick or stone.

All plans will include brick, stone or stucco (optional) on front elevations.

The following minimum percentage will apply to stucco, brick and/or stone for single and multi-family.

- Front elevation: 30%
- Side elevation: Wrap all sides 4'-0", consistent with front elevation.
- Rear: Optional

The DRC and JRC may allow a modification to the percentage, and are allowed to view each proposal on a case by case basis.

The following maximum percentages of stucco are allowed:

- Side elevation: 50%
- Rear elevation: 80%

Percentage shall be calculated as a percentage less doors, windows and garage doors. Cantilevered areas where masonry cannot be attached shall be included in the calculation. The material needed to meet the minimum calculation shall be provided in an alternative location on the same façade.

Landscaping must be installed as soon as possible either during or immediately following construction. Weather permitting, landscaping must be installed no later than five months, and not exceed one growing season. The lot shall be suitably landscaped with grass, shrubs and trees. It is the intent that each lot shall be fully landscaped. Plant material native to this climate, and the addition of sprinkler systems for areas requiring weekly mowing, are strongly encouraged. All lot landscaping plans must be approved by the DRC and architectural control committee prior to installation of materials. All landscaping shall be maintained by the property owner.

The dominant colors on homes are encouraged to be earth tones such as, beige, putty, taupe, browns, greens, slates and grays. Other colors will be subject to approval by the DRC and JRC.

There will be a variation in porch and covered entry design. Front porches must have a minimum depth of six feet. All plans will have a covered entry, either built-in as part of the roof, or by means of a porch.

Owners and builders are encouraged to use a variation in roof massing and style, including but not limited to gable, hip, arched or a combination of these.

The following items will be allowed to encroach into the setback a maximum of 24": eaves, overhangs, fireplaces, and cantilevers, vents, projecting windows, decks and similar architectural features.

Decks under 30" in height, with or without a rail may encroach into the front setback a maximum of 5'. Covered decks and porches may extend 10' into front setbacks.

No deck shall be above the main level of the home, unless integrated into the home design through the support of at least two walls of the home, or, one wall with structural columns.

Stairs from a raised deck shall vary in treatment so that adjacent decks do not have

identical stair treatment. This includes inset steps, horseshoe configuring and straight line (parallel) to departure from the deck. No second story stairs shall be at right angles to the deck.

Accessory structures as permitted by the DRC and JRC shall be architecturally compatible with the dwelling.

Accessory structures permitted: garages, work shelters, sheds (provided they match the house), gazebos, sun decks with shade trellis, observatories, arbors, weather protection shelters, green houses, pool houses and pools, outdoor barbecues, eating preparation and sitting areas.

Any element of construction, not specifically shown in graphic or written form shall conform to the requirements of International Residential Code.

No temporary building or structure shall be placed upon any lot, except by the developer or real estate company for use in connection with sales lots. Prior written consent of the DRC and permit from the Town of Johnstown is required. Consent for temporary buildings shall be granted only for a specific social, cultural, entertainment or recreational purpose of short-term duration.

N.3 FENCES

Picket style fences will be allowed in front yards, not to exceed 36" in height. 6' high fencing is allowed at the rear and sides of the dwelling, and shall not extend beyond the front of the house. Fencing along arterial streets must be installed by developers. Fences bordering streets must be 6' high using decorative materials with masonry columns at 100' O.C. All fencing must be approved by the DRC and JRC.

Rear and side yard fencing for lots that are adjacent to open space, or parks for The Villages at Johnstown shall be installed and maintained by homeowner, and shall be a 3-rail, 6' high opaque cedar fence with 4" wide

planks. Installation of fencing is required prior to issuance of Certificate of Occupancy. All alternative fencing must be approved by the DRC and JRC.

All drive lanes, drive way and entry sidewalks must be paved with concrete, brick, stone or a combination of.

Home setback should vary in depth lot to lot for visual interest.

Building setbacks:

- Front: 20'-0" minimum
Clear of sidewalk, pedestrian paths, or property line with no sidewalks
- Side: 5'-0"
- Rear: 5'-0"

Garage setbacks:

- Front loaded garages allowed
- Rear: 5'-0"
- Side: 5'-0"
- Front: 10'-0" side loaded (only for multi-family residential)

All single-family garages must accommodate a minimum of two cars per household.

N.4 RESIDENTIAL PHOTO EXAMPLES



STONE FRONT FACADE WITH 8' DEEP PORCH.

N.5 MINIMUM SIZES

Single family residences and multi-family housing will have requirements for minimum sizes allowed in the Village. These sizes are as follows:

Apartment units: 650 G.S.F.

25% maximum allowed

Single family home: 1,400 G.S.F.

15 -20% maximum allowed

RESIDENTIAL PHOTO EXAMPLES



EXAMPLE OF BAY WINDOWS



EXAMPLE OF HOME WITH COVERED FRONT PORCH



EXAMPLE OF COVERED ENTRANCE



EXAMPLE OF HOME WITH MASONRY

MULTI-FAMILY RESIDENTIAL PHOTO EXAMPLES



EXAMPLE OF APARTMENTS



EXAMPLE OF RESIDENTIAL VILLAGE WITH MULTI USES



EXAMPLE OF TOWNHOMES WITH SOLAR

RETAIL EXAMPLES



EXAMPLE OF BRICK AND SIGNAGE



EXAMPLE OF ONE STORY BUILDING WITH STUCCO AND AWNINGS



EXAMPLE OF BRICK WITH ROOF ELEMENTS AND MULTI-TENANT SCHEME

OFFICE EXAMPLES



EXAMPLE OF MULTI-STORY AND MIXED MATERIALS ON EXTERIOR



EXAMPLE OF ONE-STORY WITH SIMULATED STONE WALLS



EXAMPLE OF TWO-STORY WITH BRICK ENTRY AND BRICK ON LOWER LEVEL

INDUSTRIAL EXAMPLES



EXAMPLE OF TWO-STORY OFFICE SHOWROOM AND DISTRIBUTION



EXAMPLES OF OFFICE AND DISTRIBUTION CENTER



EXAMPLE OF BRICK BUILDING WITH ENTRY TOWER ELEMENTS

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