

RIVERBEND ESTATES PUD

OUTLINE DEVELOPMENT PLAN

21941 COUNTY ROAD 17, JOHNSTOWN, COLORADO 80534

APRIL 08, 2021

LEGAL DESCRIPTION

PARCEL 1: THAT PART OF THE NORTHEAST QUARTER OF SECTION 17, TOWNSHIP 4 NORTH, RANGE 67 WEST OF THE 6TH P.M., TOWN OF JOHNSTOWN, WELD COUNTY, COLORADO, MORE PARTICULARLY DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHEAST CORNER OF SAID NORTHEAST QUARTER; THENCE WEST 274 FEET; THENCE SOUTH 200 FEET; THENCE EAST PARALLEL WITH THE NORTH LINE OF SAID NORTHEAST QUARTER, 274 FEET MORE OR LESS, TO THE EAST LINE OF SAID NORTHEAST QUARTER; THENCE NORTH ALONG SAID EAST LINE TO THE PLACE OF BEGINNING, COUNTY OF WELD, STATE OF COLORADO.

PARCEL 2: THAT PART OF THE EAST HALF OF SECTION 17, TOWNSHIP 4 NORTH, RANGE 67 WEST OF THE 6TH P.M., TOWN OF JOHNSTOWN, WELD COUNTY, COLORADO, MORE PARTICULARLY DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE NORTH LINE OF SAID SECTION 17, A DISTANCE OF 2395 FEET WEST OF THE NORTHEAST CORNER THEREOF; THENCE EAST 2121 FEET ALONG SAID NORTH LINE TO A POINT 274 FEET WEST OF THE NORTHEAST CORNER OF SAID SECTION 17; THENCE SOUTH 200 FEET PARALLEL WITH THE EAST LINE OF SAID SECTION 17; THENCE EAST 274 FEET PARALLEL WITH THE NORTH LINE OF SAID SECTION 17 TO A POINT ON THE EAST LINE THEREOF; THENCE SOUTH 1174 FEET ALONG SAID EAST LINE TO ITS INTERSECTION WITH THE WESTERLY RIGHT OF WAY LINE OF THE GREAT WESTERN RAILROAD; THENCE SOUTH 37 DEGREES 8' WEST 1033 FEET ALONG SAID WESTERLY RIGHT OF WAY LINE TO A POINT OF CURVE OF A CURVE TO THE LEFT; THENCE SOUTHWESTERLY 590 FEET ALONG SAID CURVE TO THE LEFT, WHOSE RADIUS IS 1400 FEET, TO A POINT OF TANGENT; THENCE FOLLOWING SAID WESTERLY RIGHT OF WAY LINE SOUTH 13 DEGREES 5' WEST 654 FEET TO A POINT OF CURVE OF A CURVE TO THE LEFT; THENCE SOUTHERLY 395 FEET ALONG SAID CURVE TO THE LEFT WHOSE RADIUS IS 1472.7 FEET, TO A POINT OF TANGENT; THENCE SOUTH 2 DEGREES 16' EAST 263 FEET ALONG SAID RIGHT OF WAY TO A POINT; THENCE SOUTH 85 DEGREES 47' WEST 325 FEET; THENCE NORTH 45 DEGREES 48' WEST 95.7 FEET; THENCE NORTH 45 DEGREES 33' WEST 94 FEET; THENCE NORTH 60 DEGREES 53' WEST 89.8 FEET; THENCE NORTH 83 DEGREES 54' WEST 76.5 FEET; THENCE SOUTH 71 DEGREES 50' WEST 61.3 FEET; THENCE SOUTH 56 DEGREES 58' WEST 502.5 FEET; THENCE NORTH 61 DEGREES 5' WEST 154 FEET; THENCE NORTHERLY 4159 FEET IN A STRAIGHT LINE TO THE PLACE OF BEGINNING, COUNTY OF WELD, STATE OF COLORADO.

NARRATIVE / OPERATIONAL PLAN

THIS OUTLINE DEVELOPMENT PLAN (ODP) PROVIDES A FRAMEWORK FOR THE PROPOSED RIVERBEND ESTATES RESIDENTIAL PUD. LOCATED A MILE FROM DOWNTOWN JOHNSTOWN, RIVERBEND ESTATES IS EASILY ACCESSED VIA MULTI-USE TRAILS AND THE ARTERIAL ROADS, TO THE NORTH & EAST.

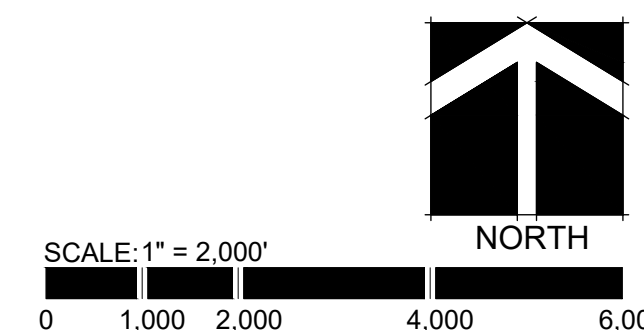
AS A PEDESTRIAN FRIENDLY COMMUNITY, RIVERBEND BOASTS NUMEROUS TRAILS, AS WELL AS, AN ABUNDANCE OF OPEN SPACE. THE LARGEST TRACT OF OPEN SPACE FOLLOWS THE LITTLE THOMPSON RIVER AS IT FLOWS THROUGH THE SITE, BETWEEN FILINGS 1 & 2.

RIVERBEND ESTATES IS CURRENTLY ZONED PUD-R. PUD ZONING PROVIDES FLEXIBILITY IN LOT SIZES & SETBACKS INSPIRING CREATIVITY IN HOUSING TYPES TO FIT THE MARKET DEMAND, WHILE ALLOWING FOR TRAILS & OPEN SPACE ADJACENT TO EACH LOT.

DEVELOPMENT PHASING

PHASING WILL OCCUR IN A LOGICAL AND COST EFFECTIVE MANNER BASED ON INFRASTRUCTURE EXTENSION, AVAILABILITY OF UTILITY SERVICE AND MARKET CONDITIONS. THE PROJECT WILL BE BUILT IN MULTIPLE PHASES, AS CONDITIONS DICTATE.

VICINITY MAP



PROJECT CONTACTS

OWNER / APPLICANT

LEE M. LOWREY
RIVERBEND ESTATES PARTNERS, LLC
4040 MACARTHUR BOULEVARD, SUITE 250
NEWPORT BEACH, CALIFORNIA 92660
(949) 566-8165
LMLOWREY@STERLINGPACIFIC HOLDINGS.COM

LANDSCAPE ARCHITECT / PLANNER

JOSH McCARN
FRONTERRA DESIGN
P.O. BOX 178
LOVELAND, COLORADO 80539
(970) 669-3737
JOSH@FRONTERRA.DESIGN

CIVIL ENGINEER

MARY WOHNRADE
WOHNRADE CIVIL ENGINEERS
11582 COLONY ROW
BROOMFIELD, COLORADO 80021
(720) 259-0965
MARY@WCCIVIL.COM

TRAFFIC ENGINEER

LOU DAVENPORT
IRONSTRIDE SOLUTIONS
7535 E HAMPPDEN AVENUE, SUITE 400
DENVER, COLORADO 80231
(720) 290-0532
LOU@IRONSTRIDESOLUTIONS.COM

SURVEYOR

STEVE PARKS
MAJESTIC SURVEYING
1111 DIAMOND VALLEY DRIVE, SUITE 104
WINDSOR, COLORADO 80550
(970) 443-0882
STEVENP@MAJESTICSURVEYING.COM

TOWN COUNCIL

THIS OUTLINE DEVELOPMENT PLAN, TO BE KNOWN AS _____, IS APPROVED AND ACCEPTED BY THE TOWN OF JOHNSTOWN, BY ORDINANCE NUMBER _____, PASSED AND ADOPTED ON FINAL READING AT THE REGULAR MEETING OF THE TOWN COUNCIL OF THE TOWN OF JOHNSTOWN, COLORADO HELD ON THE _____ DAY OF _____.

BY: _____
MAYOR

ATTEST: _____
TOWN CLERK

APPROVED BY ORDINANCE 2021-214
NOVEMBER 2021
-KM

SHEET INDEX

- 01 COVER SHEET
- 02 OUTLINE DEVELOPMENT PLAN
- 03 ILLUSTRATIVE CONCEPT PLAN
- 04 ODP DEVELOPMENT STANDARDS
- 05 MASTER UTILITY PLAN
- 06 MASTER GRADING & DRAINAGE PLAN

GENERAL NOTES

1. ACCESS POINTS SHOWN ON THIS ODP ARE APPROXIMATE. EXACT LOCATIONS TO BE DETERMINED DURING THE PRELIMINARY AND FINAL PLATTING PROCESS, BASED ON THE FINAL TRAFFIC IMPACT STUDY.
2. LOCAL AND COLLECTOR STREETS MAY CHANGE LOCATION, SIZE AND CONFIGURATION AT TIME OF PLATTING. LOCAL AND COLLECTOR STREETS SHALL CONFORM TO THE TOWN OF JOHNSTOWN STREET STANDARDS AT THE TIME OF PLATTING.
3. ALL DENSITIES AND UNIT COUNTS ARE PROJECTIONS, FINAL DENSITIES TO BE DETERMINED AT TIME OF PLATTING. DENSITY OF ANY GIVEN PHASE SHALL NOT EXCEED 9 DU/AC. ACTUAL NUMBER OF UNITS WILL BE DETERMINED AT TIME OF PLATTING.
4. GROSS DENSITY SHALL NOT EXCEED 5 DU/AC.
5. AREAS OF DEVELOPMENT PARCELS SHOWN ARE APPROXIMATE AND MAY VARY. EXACT LOT SIZES WILL BE DETERMINED WITH EACH PRELIMINARY AND FINAL PUD SUBMITTAL.
6. FINAL CONFIGURATION OF PARCELS, OPEN SPACE AND STREETS MAY VARY FROM THAT SHOWN.
7. WHERE A PROPOSED USE IS NOT LISTED IN THE ODP, IT MAY BE ALLOWED IF DETERMINED BY THE TOWN OF JOHNSTOWN TO BE SIMILAR IN CHARACTER AND OPERATION, AND HAVING THE SAME OR LESSER IMPACT, AS USES THAT ARE ALLOWED.
8. ALL DRAINAGE ELEMENTS ARE CONCEPTUAL IN NATURE AND FINAL DETERMINATIONS OF THE DRAINAGE SYSTEM, TO INCLUDE THE PLACEMENT OF DETENTION/RETENTION PONDS, CHANNELS, AND STORM SEWER, WILL BE MADE DURING THE PRELIMINARY AND FINAL DRAINAGE REPORTS AND DURING THE PRELIMINARY DEVELOPMENT PLAN PROCESS AS A PART OF THE PLATTING.
9. LANDSCAPING, SIGNAGE, ARCHITECTURE, NON-RESIDENTIAL PARKING, FENCING AND LIGHTING FOR DEVELOPMENT WITHIN THIS PUD SHALL EITHER FOLLOW THOSE APPLICABLE STANDARDS IN THE TOWN OF JOHNSTOWN MUNICIPAL CODE OR THE DEVELOPER MAY FORMULATE DESIGN STANDARDS REGULATING THE DESIGN, CHARACTER, LOCATION AND OTHER DETAILS OF THESE ELEMENTS PRIOR TO THEIR IMPLEMENTATION. THE DEVELOPER FORMULATED DESIGN STANDARDS MUST RECEIVE APPROVAL BY THE PLANNING AND DEVELOPMENT DIRECTOR PRIOR TO IMPLEMENTATION.
10. THIS DOCUMENT IS FOR CONCEPTUAL PURPOSES, INTENDED ONLY TO SHOW THE FEASIBILITY OF DEVELOPMENT ON THIS PROPERTY.
11. AS PER FEMA MAP NUMBER 08069C1410G, EFFECTIVE 01/15/2021, A LARGE PORTION OF THIS PROPERTY IS WITHIN THE DESIGNATED FLOODPLAIN ZONE. ANY DEVELOPMENT THAT OCCURS ON THIS SITE MUST BE IN FULL COMPLIANCE WITH FLOODPLAIN RULES AND REGULATIONS.



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TFG Design, LLC
P.O. Box 178
Loveland CO 80539
(970) 669-3737
JOSH@FRONTERRA.DESIGN

OUTLINE DEVELOPMENT PLAN
RIVERBEND ESTATES PUD

RIVERBEND ESTATES ANNEX
ILLUSTRATIVE CONCEPT PLAN
JOHNSTOWN, COUNTY OF WELD, STATE OF COLORADO
SHEET 3 OF 6



TFG Design, LLC
P.O. Box
Loveland CO 80539
(970) 669-3737
josh@fronterra.design

OWNER:
Riverbend Estates Partners, LLC
4040 MacArthur Boulevard, Suite 250
Newport Beach, California 92660
lmlowrey@sterlingpacificholdings.com

RIVERBEND ESTATES PUD

OUTLINE DEVELOPMENT PLAN

21941 COUNTY ROAD 17
JOHNSTOWN, COLORADO 80534
WELD COUNTY

PROJECT INFORMATION:
PROJECT NUMBER: 298-2101JT
DATE: APRIL 08, 2021
PHASE: ODP

REVISIONS:
2021.06.22 ODP RESUBMITTAL 1
2021.08.09 ODP RESUBMITTAL 2

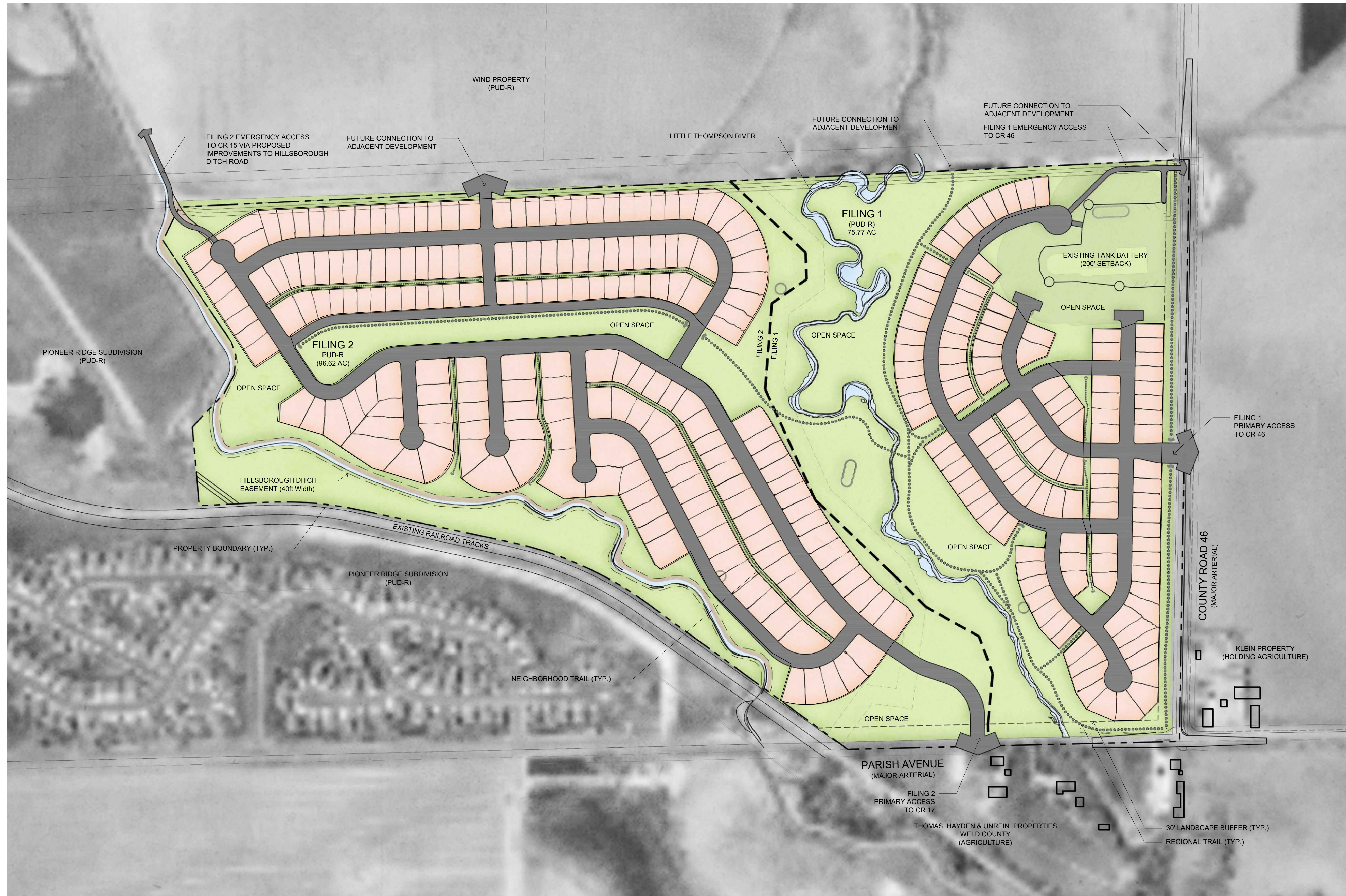
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SHEET TITLE:
ILLUSTRATIVE CONCEPT PLAN

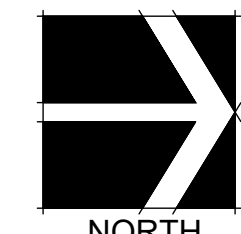
SHEET NUMBER:

ODP-3

SHEET 3 OF 6



SCALE: 1" = 200'
0 100 200 400 600



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FILE LOCATION: Z:\Drawings\2021\08\2101JT\Riverbend Estates\Annex\ODP\CONCEPT PLAN.dwg
PLOT DATE: 8/20/21 10:57 AM

OUTLINE DEVELOPMENT PLAN RIVERBEND ESTATES PUD

RIVERBEND ESTATES ANNEX
ODP DEVELOPMENT STANDARDS
JOHNSTOWN, COUNTY OF WELD, STATE OF COLORADO
SHEET 4 OF 6



TFG Design, LLC
P.O. Box
Loveland CO 80539
(970) 669-3737

josh@fronterra.design

OWNER:

Riverbend Estates Partners, LLC
4040 MacArthur Boulevard, Suite 250
Newport Beach, California 92660

Imlowrey@sterlingapacificholdings.com

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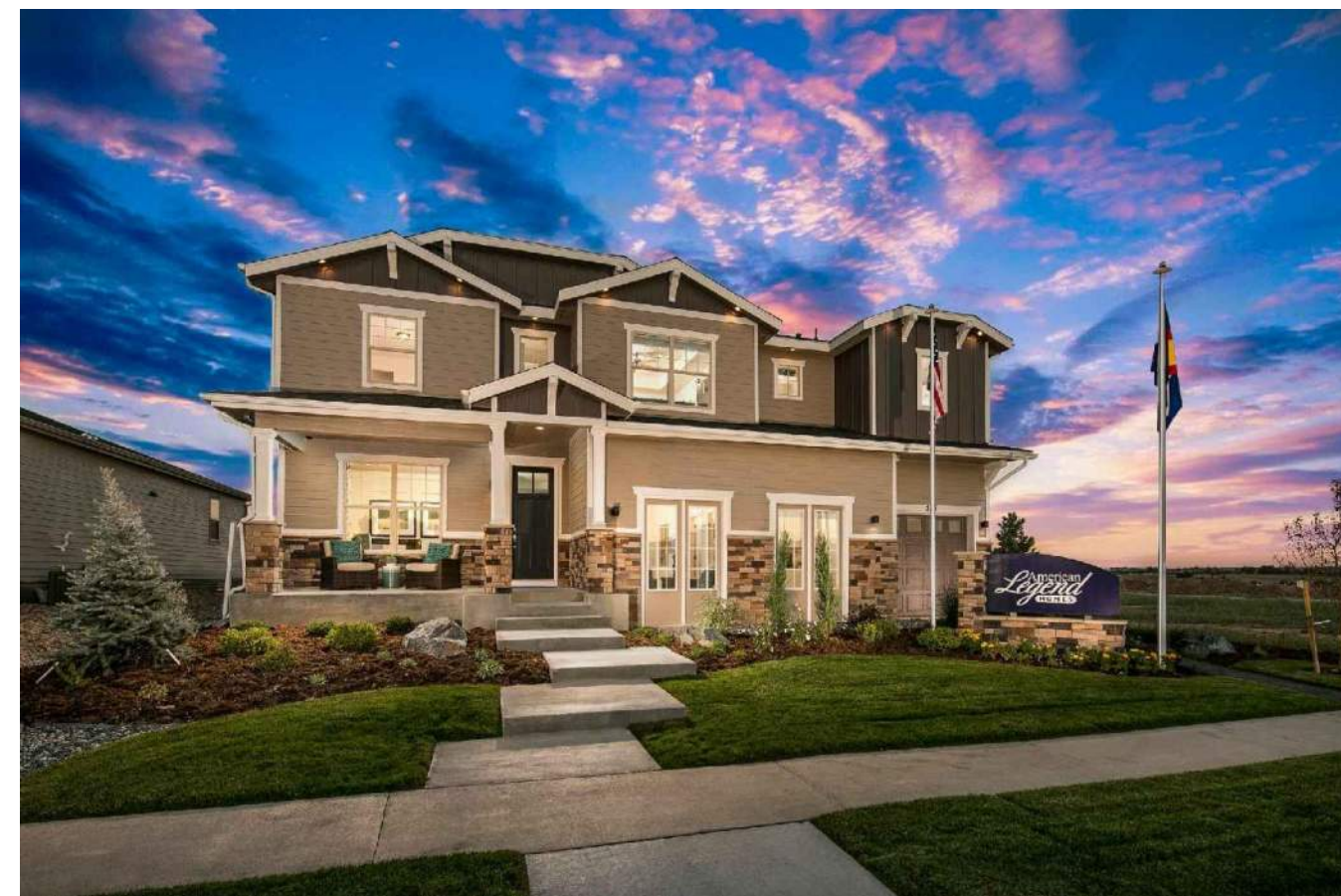
ODP DEVELOPMENT STANDARDS

SHEET NUMBER:

ODP-4

SHEET 4 OF 6

CONCEPTUAL ARCHITECTURE



ARCHITECTURAL DESIGN STANDARDS VISTA COMMONS - DETACHED RESIDENTIAL

THE UNDERLYING GOAL OF THE GUIDELINES AND STANDARDS THAT FOLLOW IS TO ACHIEVE A HIGHLY FUNCTIONAL, AESTHETICALLY PLEASING NEIGHBORHOOD WITH MUCH ATTENTION PAID TO ARCHITECTURAL QUALITY AND SITE DESIGN. EMPHASIS IS PLACED ON DIVERSITY IN LOCATION, STYLE AND BUILDING FORM. OF PARTICULAR IMPORTANCE, IS FOUR-SIDED ARCHITECTURE AND AN OVERALL DESIGN THAT PROMOTES THE HOME'S ENTRY, WHILE DIMINISHING THE VISUAL PROMINENCE OF THE GARAGE.

FUNDAMENTAL OBJECTIVES:

- THE INCLUSION OF SEVERAL HOUSE MODELS WITH ALTERNATE MODEL ELEVATIONS FOR THE MOST POPULAR MODELS. ALTERNATE MODEL ELEVATIONS SHALL INCLUDE A SIGNIFICANT CHANGE IN THEIR APPEARANCE, SO AS TO BE UNRECOGNIZABLE AS THE SAME MODEL FROM THE STREET.
- INCORPORATE A VARIETY OF ARCHITECTURAL DETAILS, WALL ARTICULATION, MULTIPLE ROOF PLANES IN EACH HOME TO CREATE BALANCED DESIGNS THAT VARY IN MASSING AND SCALE.
- ACHIEVE AN APPEALING 'NEIGHBOR FRIENDLY' AESTHETIC BY VARYING SETBACKS BASED ON THE GARAGE LOCATION AND INCORPORATING A SIGNIFICANT FRONT PORCH.

GENERAL

INDIVIDUAL HOUSE STYLES ARE TO BE VARIED, BUT COMPLEMENTARY, USING MULTIPLE ARCHITECTURAL FEATURES SUCH AS, FRONT PORCHES WITH RAILING, VARYING ROOF PITCHES. HOUSES ARE TO HAVE WALLS THAT JOG AND SEVERAL ROOFLINES AND TRIM DETAILS TO PROVIDE A MORE COMPLEX ARCHITECTURAL STYLE. THE INTENT IS TO AVOID THE FEELING OF A MANUFACTURED DEVELOPMENT AND RATHER CREATE THE SENSE OF A NEIGHBORHOOD THAT GREW IN A WAY THAT WAS THOUGHTFULLY ORGANIC.

SETBACK

A WELL-DEFINED, STRONG STREET EDGE HELPS TO REINFORCE THE PEDESTRIAN ORIENTED GOALS OF THE COMMUNITY. WELL-ARTICULATED DWELLINGS WITH PROMINENT ENTRANCES AND FRONT PORCHES WILL BE REQUIRED. A BALANCED, VISUALLY INTERESTING, NON-GARAGE DOMINANT STREETScape WITH A HUMAN SCALE, WILL BE ACHIEVED BY VARYING SETBACKS BASED ON THE HOME'S GARAGE LOCATION. MINIMUM SETBACKS OF ALL STRUCTURES SHALL BE A MINIMUM OF FIFTEEN (15) FEET FROM FRONT OF LOT AND FIVE (5) FEET FROM PROPERTY LINE ON ALL SIDE YARDS, EXCEPT ON CORNER LOTS, WHERE SIDE SETBACKS SHALL BE TEN (10) FEET FROM THE SIDEWALK. A TEN (10) FOOT MINIMUM SETBACK IS REQUIRED FROM THE REAR PROPERTY LINE.

SQUARE FOOTAGE

1,600 SQUARE FOOT MINIMUM FINISHED FLOOR ARE FOR MULTI-STORY WITH 1,000 SQUARE FEET ON THE MAIN FLOOR, 1,400 SQUARE FOOT MINIMUM FOR RANCHES. GARDEN LEVEL BASEMENTS WHERE POSSIBLE. THERE ARE TO BE A VARIETY OF SQUARE FOOTAGES AVAILABLE ON THE SAME STREET TO PROVIDE AESTHETIC VARIATION.

GARAGE

ALL GARAGE FACES SHALL COMPLIMENT, RATHER THAN DOMINATE, THE ARCHITECTURAL DETAILS OF DWELLING'S LIVING PORTION. GARAGES SHOULD NOT DOMINATE THE MASSING OF THE STREET-FACING FACADE. MASSING OF THE GARAGE SHOULD BE MINIMIZED BY GIVING THE HABITABLE PORTION OF THE DWELLING A LARGER AND MORE DOMINANT MASS, INTEGRATING THE MASSING OF THE GARAGE INTO THE MAIN MASSING OF THE DWELLING, POSITIONING THE MAIN FRONT WALL AND PORCH FACE CLOSER TO THE STREET AND LIMITING THE PROJECTION OF THE GARAGE. THE STREETScape SHOULD INCLUDE A VARIETY OF GARAGE DOOR STYLES TO AVOID MONOTONY AND REPETITION OF A SINGLE GARAGE DOOR STYLE. WHERE THE GARAGE PROJECTS IN FRONT OF THE GROUND LEVEL WALL OR COVERED FRONT PORCH, A WINDOW IN THE INTERIOR SIDE OF THE GARAGE CONSISTENT WITH THE HEIGHT AND STYLE OF THE FRONT ELEVATION WINDOWS SHALL BE INCORPORATED. A MINIMUM OF A TWO-CAR GARAGE IS REQUIRED. GARAGES THAT HAVE DOORS FOR MORE THAN TWO (2) VEHICLES MUST HAVE ONE DOOR RECESSED A MINIMUM OF TWO (2) FEET FROM THE OTHERS. GARAGE DOORS, VISIBLE AS PART OF THE FRONT BUILDING ELEVATION, SHALL NOT COMPRISE MORE THAN FORTY (40) PERCENT OF THE GROUND FLOOR, STREET FACING, LINEAR BUILDING FRONTAGE. THE MAJORITY OF ALL DWELLING UNITS PROPOSED SHOULD HAVE GARAGES WHICH ARE EITHER FLUSH WITH OR RECESSED BEHIND THE GROUND FLOOR FRONT WALL OR COVERED FRONT PORCH. THE MAXIMUM GARAGE PROJECTION SHALL BE FIVE (5) FEET IN FRONT OF EITHER THE GROUND FLOOR FRONT WALL OR COVERED FRONT PORCH. THE SECOND STORY WALL FACE ABOVE THE GARAGE MUST NOT BE FLUSH WITH THE GARAGE AND AT LEAST SIXTY (60) PERCENT MUST SET BACK FURTHER THAN EIGHT (8) FEET FROM THE FRONT FACE OF THE GARAGE. GARAGES ARE TO HAVE VARIED LOCATIONS, INCLUDING DIFFERENT ROOF ANGLES AND VARIOUS SETBACKS FROM THE STREET. NO DETACHED GARAGES SHALL BE CONSTRUCTED ON A LOT.

DRIVEWAY

A MIX OF PAIRED AND UNPAIRED DRIVEWAY COMBINATIONS IS DESIRABLE TO CONTRIBUTE VISUAL INTEREST ALONG THE STREET AND TO PROVIDE SUFFICIENT SPACE FOR PARKWAY TREES. DRIVEWAY LOCATIONS SHALL BE PREDETERMINED ON THE PROJECT ENGINEER'S SITE PLANS. PRIORITY LOCATIONS AND PROMINENT VIEWS WITHIN THE COMMUNITY SHALL BE TAKEN INTO ACCOUNT IN THIS DETERMINATION. DRIVEWAYS SHALL BE LOCATED AS FAR AS POSSIBLE FROM OPEN SPACE, INSTITUTIONAL USES, COMMERCIAL SITES, WALKWAYS, TRANSIT STOPS AND INTERSECTIONS. CORNER LOT DRIVEWAYS SHALL BE ORIENTED TO THE SHORT SIDE OF THE LOT OR MINOR STREET. DRIVEWAY SURFACES SHALL BE COMPRISED OF CONCRETE, NATURAL STONE, BRICK OR DECORATIVE CONCRETE PAVERS. THERE WILL BE NO AGGREGATE PARKING PADS ALLOWED ON A LOT.

ROOF

ROOFS PLAY A SIGNIFICANT ROLE IN THE MASSING OF A DWELLING AND THE OVERALL BUILT FORM OF A RESIDENTIAL DEVELOPMENT. A VARIETY OF TRADITIONAL ROOF TYPES ARE ENCOURAGED, PARTICULARLY FOR ALTERNATE ELEVATIONS OF A MODEL. WITHIN THE DESIGN OF A STREETScape, ATTENTION SHOULD BE PAID TO THE RELATIONSHIPS OF ADJACENT ROOF FORMS TO ENSURE APPROPRIATE TRANSITIONS. THE ROOF IS TO BE HIGH DEFINITION ROOF SHINGLES, ROOF SLOPE MINIMUM 6:12 PITCH WITH ROOF PITCHES VARYING FROM HOUSE TO HOUSE. SECONDARY ROOFS OF A MINIMUM 3:12 ARE ACCEPTED BUT NOT REQUIRED. OVERHANGS ARE TO BE A MINIMUM 24".

FRONT FACADE

THE MAIN ENTRANCE TO THE DWELLING SHOULD ACT AS THE FOCAL POINT OF THE DWELLING AND BE GIVEN APPROPRIATE DESIGN EMPHASIS. THE MAIN ENTRANCE SHALL BE DIRECTLY VISIBLE FROM THE STREET. A VARIETY OF FRONT DOOR STYLES IS REQUIRED, INCLUDING SOME WITH GLAZING. DECORATIVE DOOR SURROUNDS ARE REQUIRED IF A COVERED ENTRY FEATURE HAS NOT BEEN PROVIDED, INCLUDING THE USE OF GLAZED SIDELIGHTS AND TRANSOMS. FRONT PORCHES AND/OR INTERESTING ARCHITECTURAL ENTRY FEATURES ARE TO BE PROVIDED ON STREET-FACING FACADES IN ORDER TO RELATE TO THE PEDESTRIAN SCALE. A COVERED ENTRY FEATURE IS REQUIRED FOR AT LEAST FIFTY (50) PERCENT OF MODEL DESIGNS. WRAPAROUND PORCHES ARE REQUIRED FOR CORNER LOTS AND END LOTS ABUTTING OPEN SPACE. A COVERED FRONT PORCH SHOULD BE MAXIMIZED WHEREVER POSSIBLE. A MINIMUM DEPTH OF SIX (6) FEET SHOULD BE PROVIDED TO COMFORTABLY ACCOMMODATE CHAIRS, HOWEVER DEEPER PORCHES ARE ENCOURAGED. ALL PORCHES TO BE A MINIMUM OF THIRTY-SIX (36) SQUARE FEET IN AREA. HANDRAILS SHOULD BE CONSISTENT WITH THE CHARACTER OF THE HOUSE AND PAINTED TO MATCH THE TRIM.

SIDE FACADES

PROVIDE GROUND FLOOR PROJECTIONS, WINDOWS, AND OTHER ELEMENTS TO BREAK UP LONG WALL EXPANSES.

VERTICAL SUPPORT POSTS

COLUMN STYLES AND WIDTHS SHOULD BE CONSISTENT WITH THE CHARACTER OF THE HOUSE AND SHOULD TYPICALLY BE NO LESS THAN EIGHT (8) INCHES SQUARE OR DIAMETER.

FOUNDATION

EXPOSED FOUNDATION IS TO MEET MINIMUM CODE REQUIREMENTS BUT IS TO BE NO MORE THAN TWELVE (12) INCHES HIGH. EXPOSED CONCRETE, CONCRETE BLOCK FOUNDATIONS AND/OR DECK FOOTINGS WHICH EXCEED SIX (6) INCHES IN HEIGHT ABOVE FINISHED GRADE SHALL BE PAINTED TO BLEND WITH THE PRINCIPAL COLOR.

WALL CLADDING

THE CHOICE OF EXTERIOR CLADDING MATERIAL SHOULD BE COMPATIBLE WITH THE ARCHITECTURAL STYLE OF THE HOUSE. MAIN WALL CLADDING IS REQUIRED TO BE CONSISTENT ON ALL ELEVATIONS OF THE HOUSE TO AVOID THE EFFECT OF A FALSE FACADE. WHERE CHANGES IN MATERIALS DO OCCUR, THEY SHOULD HAPPEN AT LOGICAL LOCATIONS, SUCH AS, A CHANGE IN PLANE, WALL OPENING, DOWNSPOUT OR CORNER DETAIL. MATERIAL CHANGES WHICH HELP TO ARTICULATE THE TRANSITION BETWEEN THE BASE, MIDDLE AND TOP OF THE BUILDING ARE APPROPRIATE. THE USE OF SECONDARY MATERIALS WHICH BLEND HARMONIOUSLY WITH THE PRIMARY CLADDING MATERIAL ARE ENCOURAGED.

ARCHITECTURAL DETAILS

A VARIETY OF TRIM DETAILING IS REQUIRED WHERE ARCHITECTURALLY APPROPRIATE. TRIM BOARDS SHOULD BE ACCENTUATED BY USING A CONTRASTING BUT COMPATIBLE COLOR TO THAT OF THE CLADDING. THE SAME AMOUNT OF THOUGHT AND CARE SHOULD BE PUT INTO THE SELECTION AND INSTALLATION OF OTHER ARCHITECTURAL FEATURES, AS FOR MORE OBVIOUS FEATURES, SUCH AS ROOFS, DOORS AND WINDOWS, THESE MAY INCLUDE DOOR HANDLES AND HINGES, SHUTTERS AND LIGHTING.

GUTTERS/DOWNSPOUTS AND UTILITIES

GUTTERS AND DOWNSPOUTS ARE TO BE PAINTED A MATCHING COLOR TO THE SOFFITS AND FASCIA. DOWNSPOUTS, UTILITY BOXES, METERS, ETC. ARE TO BE LOCATED AS VISUALLY UNOBTRUSIVELY AS POSSIBLE. WHERE FEASIBLE, THEY SHOULD NOT BE VISIBLE FROM THE STREET.

COLORS

TRADITIONAL EARTH-TONE COLORS TO BLEND WITH THE CHARACTER OF THE NEIGHBORHOOD. ALL EXTERIOR RAILINGS, WOOD, ALL TRIM, SOFFITS, FASCIA AND ANY DECORATIVE BRACKETS ARE TO BE OF ONE COLOR AND ARE TO MATCH IN A COMPLEMENTARY, CONTRASTING COLOR TO THE MAIN HOUSE COLOR. ADEQUATE REPRESENTATIONS OF PROPOSED COLORS OR COLOR BOARDS MUST BE SUBMITTED FOR APPROVAL.

WINDOWS

ALL WINDOWS MUST BE WOOD, METAL CLAD, OR VINYL. ALUMINUM WINDOWS ARE NOT ALLOWED.

PLEASE NOTE:

THE PRECEDING ARCHITECTURAL STANDARDS SUPERSEDE ANY MUNICIPAL GUIDELINES. ADHERENCE TO THE MUNICIPAL GUIDELINES IS REQUIRED IF A SPECIFIC ARCHITECTURAL STANDARD IS NOT DESCRIBED HEREIN.

DEVELOPMENT STANDARDS

SINGLE FAMILY DETACHED

MINIMUM LOT SIZE	6,000 SF
MAXIMUM HEIGHT	35 FT
FRONT SETBACK TO BUILDING	15 FT
FRONT SETBACK TO GARAGE	20 FT
SIDE YARD SETBACK	5 FT
SIDE YARD CORNER SETBACK	10 FT
REAR YARD SETBACK	10 FT

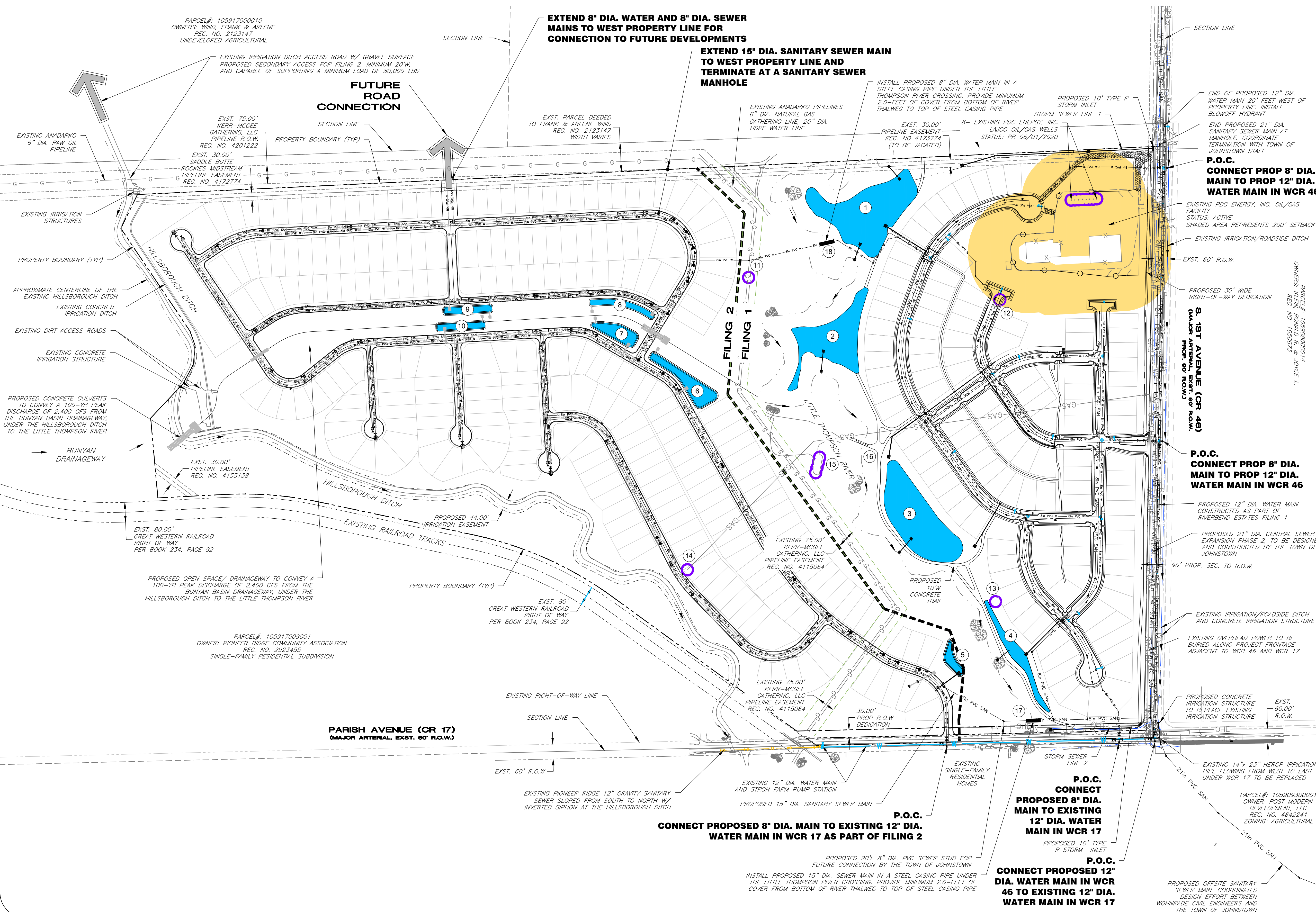


NOTE:
THIS DEVELOPMENT PROPOSAL IS BASED ON CURRENT CONCEPTUAL DRAWINGS AND DESIGN STANDARDS AND MAY CHANGE AS PART OF THE DEVELOPMENT REVIEW PROCESS

Riverbend Estates P.U.D.																		
Basin ID	Description	Acres	Units	Sanitary Sewer Design Flows					Water Demands									
				Assumptions	Calculations				Assumptions (Section 2.2, Water System Design Standards)	Calculations								
				Gallons per Capita per Day	Capita per Unit	Flow Rate (GPD)	Flow Rate (CFS)	Peaking Factor	Peak Flow Rate (GPD)	Peak Flow Rate (CFS)	Capita per Unit	Average Day Demand (Gallons per Day per Capita)	Maximum Day Demand (Gallons per Day per Capita)	Peak Hour Demand (Gallons per Day per Capita)	Average Flow Rate (GPD)	Maximum Flow Rate (GPD)	Peak Flow Rate (GPD)	Peak Flow Rate (CFS)
Filing 1	Single-Family Residential	75.77	119	80	2.76	26,275	0.039	4.362	114,609	0.172	3.0	150	2x Average Day Demand	15x Maximum Day Demand	53,550	107,100	160,650	0.241
Filing 2	Single-Family Residential	96.62	244	80	2.76	53,875	0.081	3.888	209,491	0.314	3.0	150	2x Average Day Demand	15x Maximum Day Demand	108,800	164,700	247,050	0.371
Total		172.39	363															

GENERAL NOTES:

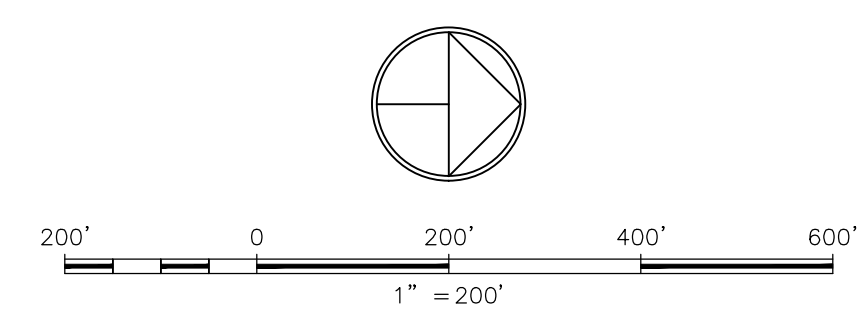
- All existing oil/gas pipelines that are no longer in use shall be removed as part of Filing 1. See Riverbend Estates Subdivision Filing 1 Existing Conditions and Demolition Plan.
- A future local road connection to properties to the west will be included as part of Filing 2 with a 60' right-of-way being dedicated on the Filing 2 Plat.
- The proposed 21" diameter offsite sanitary sewer will be designed by the Town of Johnstown with design support from Wohnrade Civil Engineers, Inc.
- Unless otherwise indicated herein, all onsite water and sewer mains in both Filing 1 and 2 shall be 8" diameter, with the exception of the 15" diameter sanitary sewer main to serve the property to the west.
- Section 1.3 of the Town of Johnstown Sanitary Sewer Design Standards have been referenced in the preparation of design flow calculations for determining proposed peak sewer system demands for this project.
- Section 1.2 of the Town of Johnstown Water System Design Standards have been referenced in the preparation of design flow calculations for determining proposed peak water system demands for this project. Assume 3.0 people per dwelling unit.
- Fire hydrants shall be at a maximum 500-foot spacing for residential subdivisions, and shall have a minimum residual pressure of 20 psi during maximum day + fire flow, to provide minimum fire protection. As per Part III, paragraph 3.2.3 of the Town of Johnstown Water Design System Specifications.
- Oil/gas setbacks shown on this plan are 200-feet from both existing wells and oil/gas facilities to proposed residential building footprints. Setbacks from plugged and abandoned wells are 25-feet from the well to proposed building footprints.
- This topographic survey was prepared by Wohnrade Civil Engineers using precision UAS aerial mapping. Supplemental survey was prepared by King Surveyors. Survey was prepared in February 2018.



NUMBERED LEGEND	
1 - 4	FILING 1 BIORETENTION PONDS
5 - 10	FILING 2 BIORETENTION PONDS
11 - 15	PLUGGED AND ABANDONED WELLS
16	PROPOSED PEDESTRIAN BRIDGE CROSSING
17 - 18	PROPOSED STEEL CASING PIPE

LEGEND

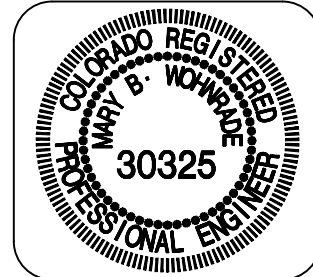
- 12in PVC W — PROPOSED WATER MAIN
- PROPOSED GATE VALVE
- PROPOSED FIRE HYDRANT
- PROPOSED THRUST RESTRAINT
- 0.75N TYPE K — PROPOSED WATER SERVICE
- 8in PVC SAN — PROPOSED SEWER MAIN
- 6in PVC SAN — PROPOSED SEWER SERVICE
- 18in RCP STRM — PROPOSED STORM SEWER OR CULVERT
- 18in RCP STRM — PROPOSED STORM SEWER W/ INLET
- PROPOSED SWALE
- PROPERTY BOUNDARY
- LIMITS OF DISTURBANCE
- 5020 — EXISTING CONTOUR
- 20 — PROPOSED CONTOUR
- 4650.00 — EXISTING SPOT ELEVATION
- 5267.38 — PROPOSED SPOT ELEVATION
- FL — PROPOSED SPOT ELEVATION
- IRR — IRR — PROPOSED NON-POTABLE WATER
- SS — SS — EXISTING SEWER MAIN
- 12in PVC W — EXISTING WATER MAIN
- EXISTING WATER VALVE
- EXISTING FIRE HYDRANT
- EXISTING POWER POLE
- EXISTING POWER POLE W/ GUY WIRE
- UE — UE — EXISTING UNDERGROUND ELECTRIC
- G — G — EXISTING UNDERGROUND GAS
- FO — FO — EXISTING FIBER OPTICS LINE
- OHP — OHP — EXISTING POWER LINES
- EXISTING TELEPHONE LINES
- EXISTING TELEPHONE PEDESTAL
- EXISTING STREET LIGHT
- EXISTING POWER VAULT
- EXISTING STORM SEWER
- 18in RCP STRM — EXISTING STORM SEWER AND INLET
- 18in RCP STRM — EXISTING STORM SEWER SIGN
- EXISTING FENCE
- EXISTING MAILBOX
- OIL/GAS 200' SETBACK
- DRAINAGE FLOW ARROW



RIVERBEND ESTATES PARTNERS, LLC

RIVERBEND ESTATES P.U.D. OUTLINE DEVELOPMENT PLAN JOHNSTOWN, COLORADO MASTER UTILITY PLAN

WOHNRADE CIVIL ENGINEERS, INC.
11500 Chaffery Ave.
Johnstown, CO 80521
Phone: (720) 503-0887
Fax: (720) 506-1519



Project: RIV-2027-00
Date: 8/5/2021
Scale: 1:200
Designed By: JR
Reviewed By: ABW

