

**E. JENNIE AVENUE
LOCAL IMPROVEMENT DISTRICT
ENGINEERING REPORT**

JUNE 2026



Prepared for the
City of Hermiston, Oregon

**E. JENNIE AVENUE
LOCAL IMPROVEMENT DISTRICT
ENGINEERING REPORT**

FOR

CITY OF HERMISTON, OREGON

2026



ANDERSON PERRY & ASSOCIATES, INC.

**La Grande, Redmond, Hermiston, and Enterprise, Oregon
Walla Walla, Washington**

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Section 1 - Project Description

General

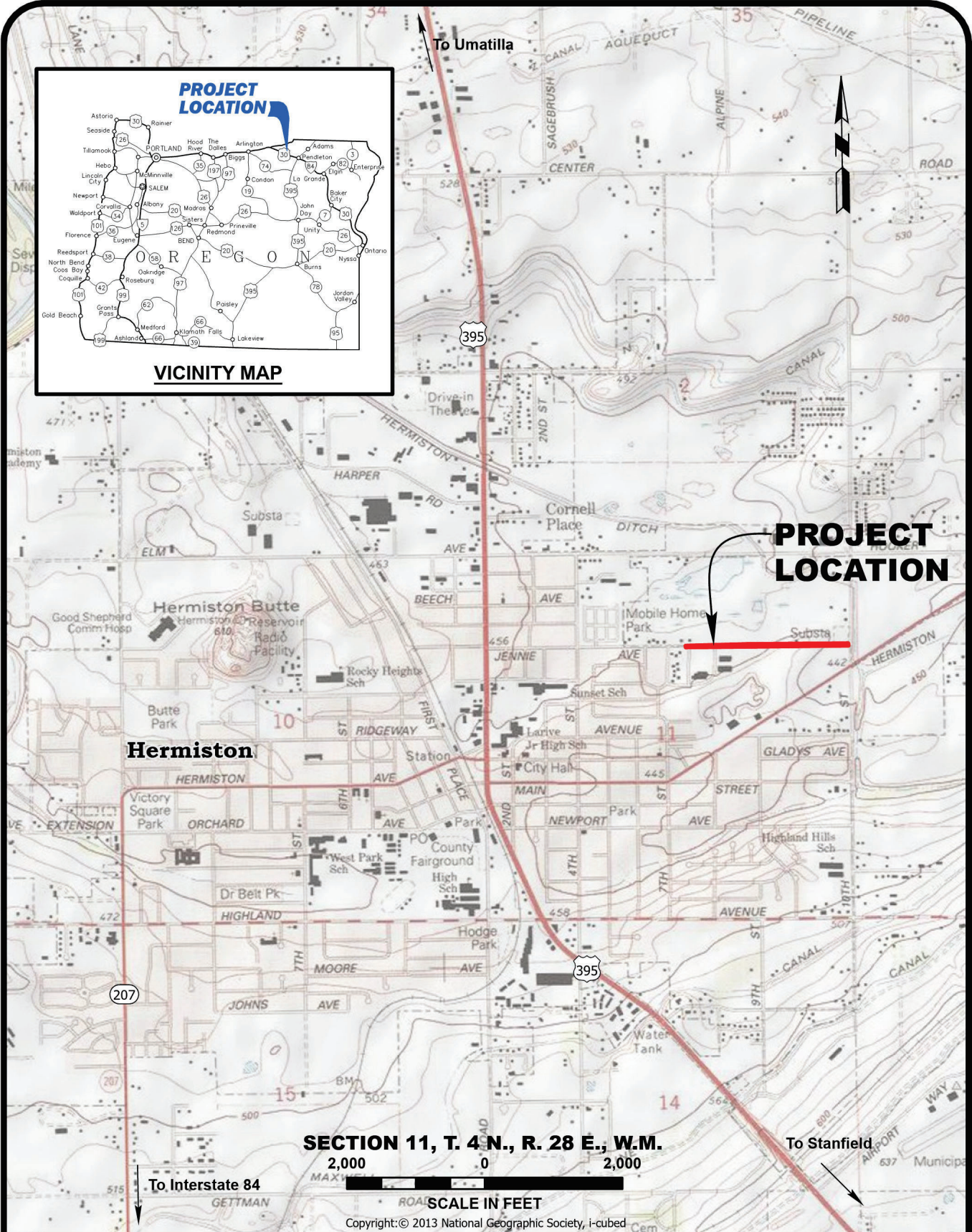
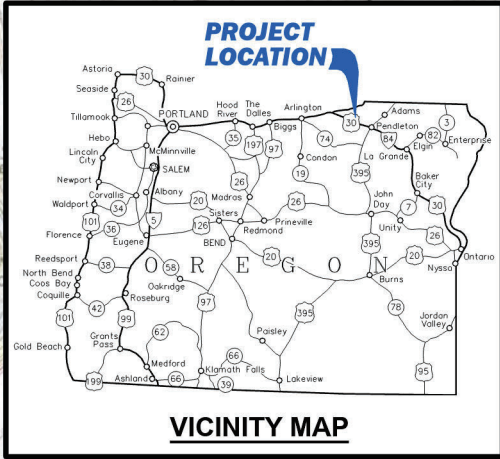
The City of Hermiston, Oregon, is considering the formation of a Local Improvement District (LID) for E. Jennie Avenue from N.E. 8th Place to N.E. 10th Street to provide street improvements in anticipation of increased traffic due to property development and to provide better traffic circulation south of Highway 207 and east of U.S. Highway 395. These improvements would generally include replacing existing dirt and gravel roads with a half-street improvement plus an additional 10 feet of paving on the north side of the street to accommodate two-way traffic. The street would be constructed to meet urban local residential street city requirements. Sidewalk, swale, and ribbon curb improvements would be constructed on the south side but not on the north side, since no development is currently planned there.

Project Description

The proposed LID location is shown on Figure 1-1, and an aerial view of the proposed project area is shown on Figure 1-2. The LID boundary is shown on Figure 1-3. The proposed LID consists of five properties, comprising a total of approximately 35 acres. The LID would begin at N.E. 8th Place, where there is an existing 60-foot wide right-of-way (ROW). The existing ROW narrows down to a newly acquired 50-foot wide ROW approximately 171 feet east of the intersection of N.E. 8th Place and E. Jennie Avenue. Heading east from that point, approximately 1,100 feet of the newly acquired 50-foot wide ROW connects to the ROW to the east. Continuing east, approximately 1,036 feet of 50-foot ROW extends to the eastern boundary of the LID and then approximately 33 feet of additional ROW connects to N.E. 10th Street. Figure 1-4 shows the ROW for the E. Jennie Avenue LID.

The improvements for the proposed LID would consist of approximately 2,307 linear feet (LF) of street improvements plus an additional 33 LF of City funded improvements, including 5-foot sidewalk on the south side, an infiltration swale, a ribbon curb, one 6-foot parking lane on the south side, and two 11-foot travel lanes (a half-street improvement plus 10 feet of additional paving to allow for two-way traffic). The pavement section would consist of 3 inches of asphalt concrete pavement on 3 inches of 3/4"-0 aggregate base and 6 inches of 1"-0 aggregate base. These proposed street improvements are based on a planning-level design and are preliminary in nature. Figure 1-5 shows a typical street cross section of the proposed street improvements. This cross section is based on a modified City Standard Drawing ST12 Urban Local Street - Option 1 (Parking Both Sides) with Infiltration Swale Option 2 and is shown in Appendix A.

A property benefit analysis is included in Chapter 3, with property boundaries current as of June 1 2026.



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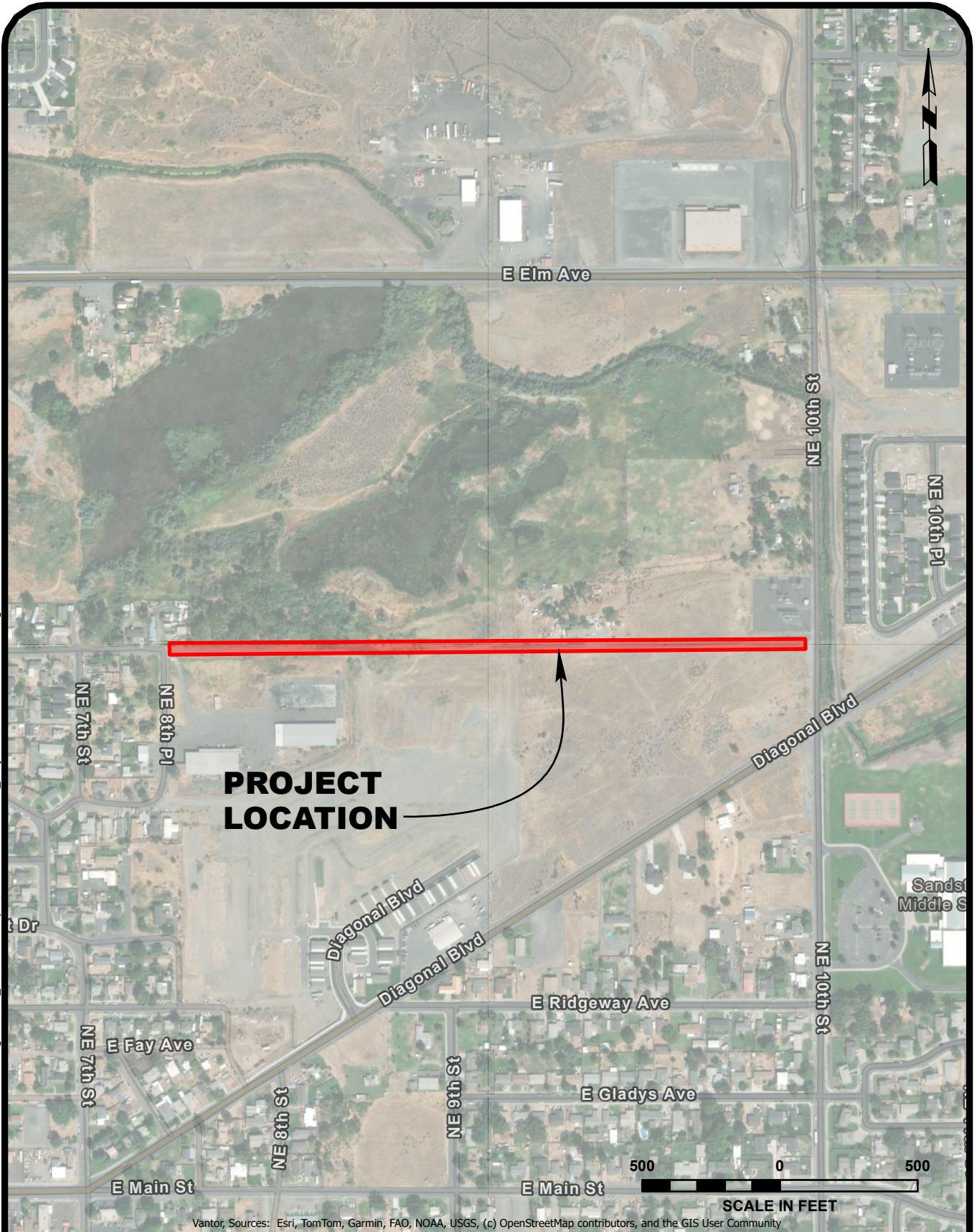


**CITY OF
HERMISTON, OREGON
E. JENNIE AVENUE LOCAL IMPROVEMENT DISTRICT**

LOCATION AND VICINITY MAPS

**FIGURE
1-1**

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Vantor, Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, (c) OpenStreetMap contributors, and the GIS User Community






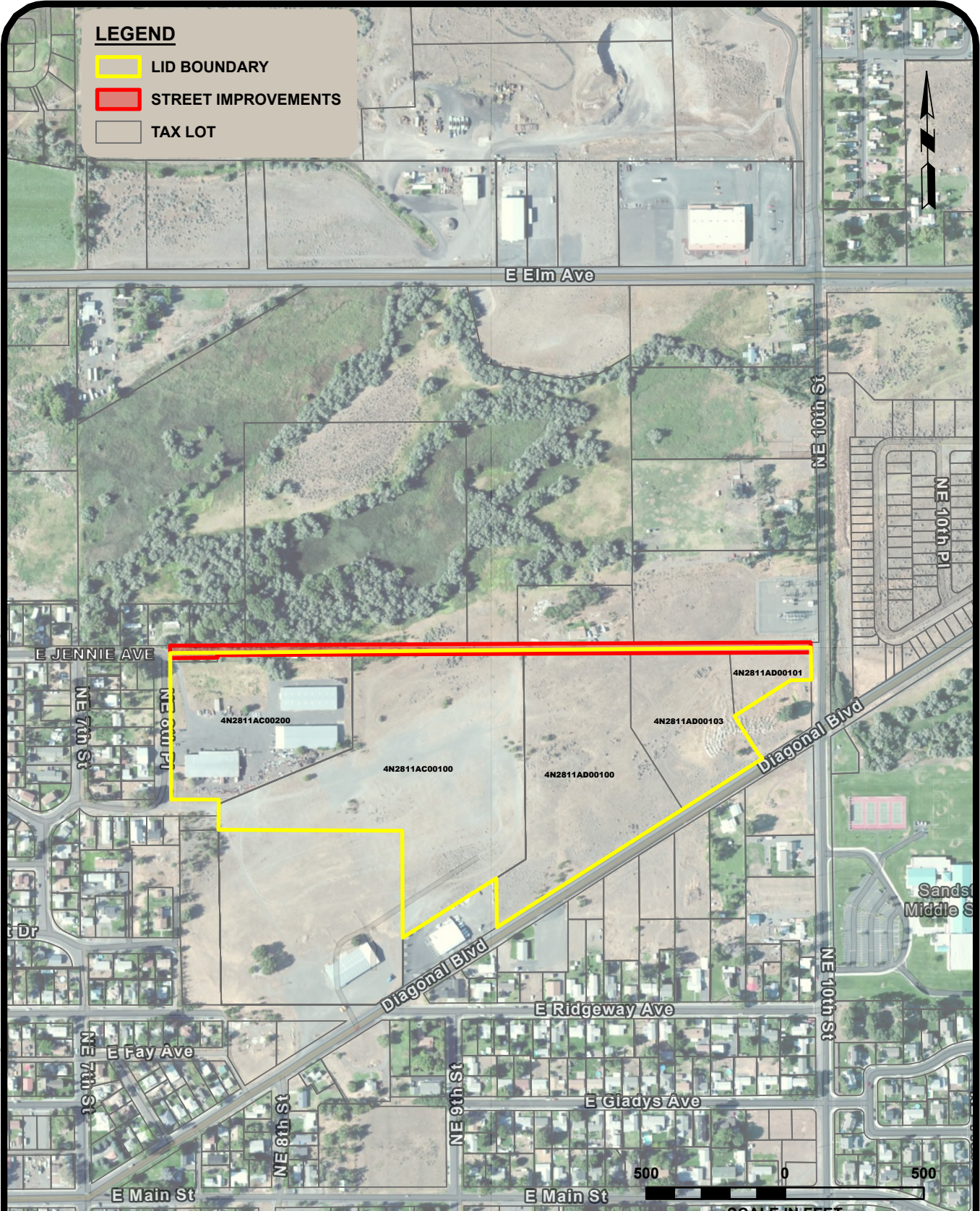
CITY OF
 HERMISTON, OREGON
 E. JENNIE AVENUE LOCAL IMPROVEMENT DISTRICT

FIGURE
 1-2

PROJECT AREA

LEGEND

-  LID BOUNDARY
-  STREET IMPROVEMENTS
-  TAX LOT



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Source: Esri, Vantor, Earthstar Geographics, IGN, and the GIS User Community, Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, (c) OpenStreetMap contributors, and the GIS User Community

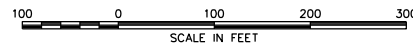


**CITY OF
HERMISTON, OREGON
E. JENNIE AVENUE LOCAL IMPROVEMENT DISTRICT**

**FIGURE
1-3**

BOUNDARY

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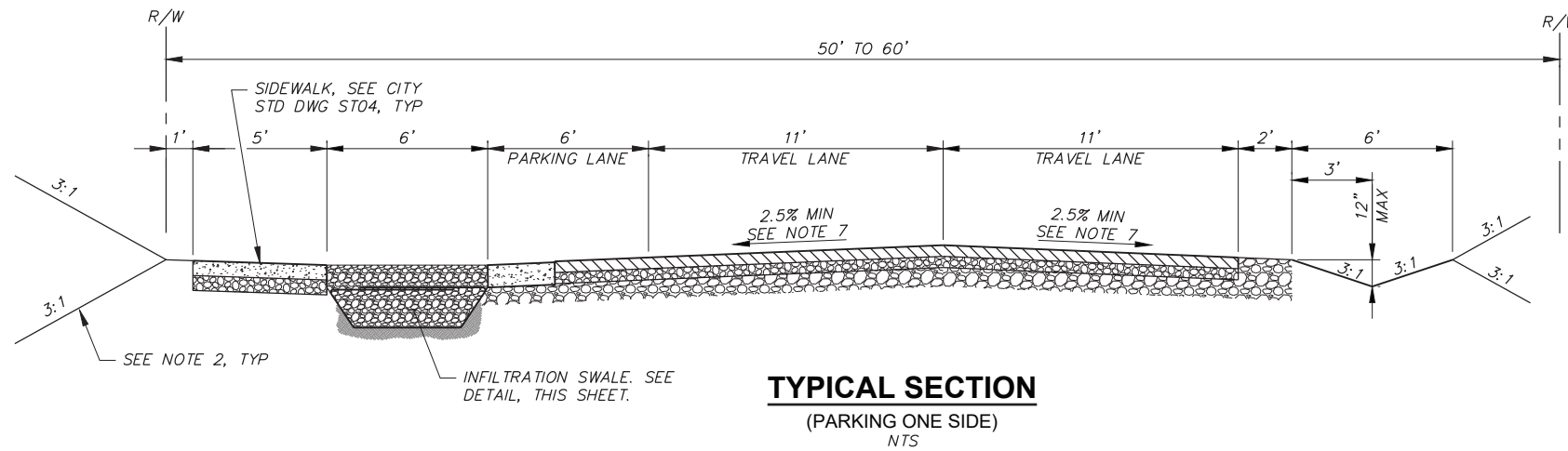
CITY OF
HERMISTON, OREGON
E. JENNIE AVENUE LOCAL IMPROVEMENT DISTRICT

RIGHT-OF-WAY

FIGURE

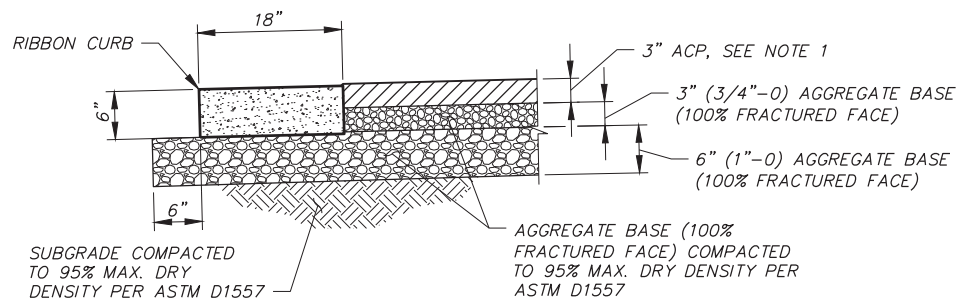
1-4

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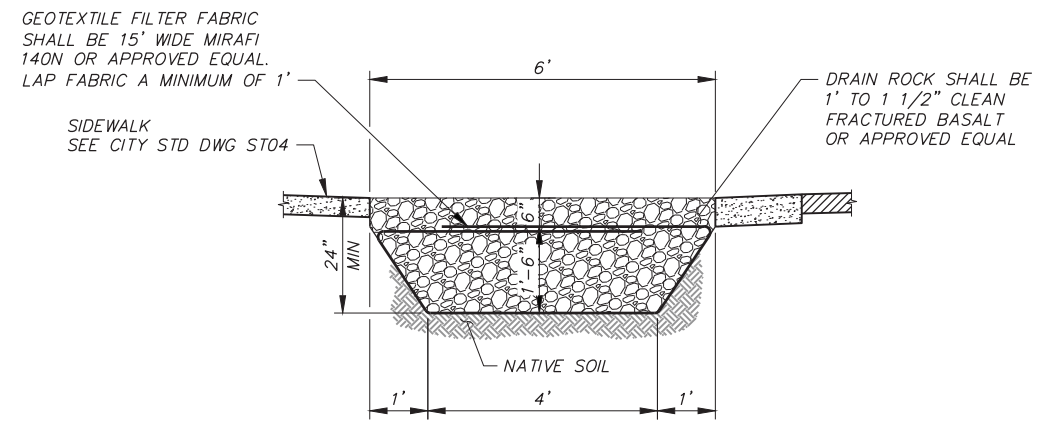


NOTES

1. ASPHALT CONCRETE PAVEMENT SHALL BE CONSTRUCTED IN ONE LIFT.
2. SLOPES SHALL NOT EXCEED 3:1 UNLESS OTHERWISE APPROVED BY THE CITY.
3. COMPACT AGGREGATE BASE (100% FRACTURED FACE) AND SUBGRADE TO 95% MAXIMUM DRY DENSITY PER ASTM D1557.
4. THE DISTANCE BETWEEN TOP BACK OF RIBBON CURBS AND RIGHT-OF-WAY CENTERLINE IS A CRITICAL DIMENSION. RIBBON CURBS POURED IN AN INCORRECT LOCATION SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR.
5. DESIGN STREET CROSS SLOPE SHALL BE 2.5%. CONSTRUCTED STREET CROSS SLOPE SHALL BE MINIMUM 2% AND MAXIMUM 3% WHERE NEWLY INSTALLED RIBBON CURB ELEVATIONS DEVIATE FROM THE APPROVED PLANS, THE FINISH GRADE OF CENTERLINE SHALL BE BASED ON THE ACTUAL EDGE OF GUTTER ELEVATIONS TO ENSURE STREET CROSS SLOPE REQUIREMENTS ARE MET.



PAVEMENT SECTION
NTS



INFILTRATION SWALE
NTS

	<p>CITY OF HERMISTON, OREGON E. JENNIE AVENUE LOCAL IMPROVEMENT DISTRICT</p>	<p>FIGURE 1-5</p>
<p>URBAN LOCAL STREET WITH SWALE</p>		

Section 2 - Preliminary Feasibility Analysis

Street Improvement

The proposed street improvements required for the E. Jennie Avenue Local Improvement District (LID) are feasible; however, they present certain challenges. The street section shown in Appendix A will fit entirely within the existing 60-foot wide section of the right-of-way (ROW) but will not completely fit within the newly acquired 50-foot wide ROW at full buildout. However, the portion of the road cross section proposed to be constructed as part of this LID will fit within the 50-foot wide ROW. In addition, the power poles near the center of the ROW would need to be relocated as part of the project. Overcoming these obstacles will require cooperation and coordination with local property owners and the electrical utility.

Project Cost

A preliminary cost estimate was prepared as part of the feasibility analysis and is shown on Figure 2-1. Due to the preliminary nature of the cost estimate, a 20 percent construction contingency has been included to account for unforeseen issues and potential variability in the bidding environment when the improvements are to be constructed. As shown, the estimate includes anticipated engineering as a percentage of construction costs. The preliminary cost estimate assumes construction of the improvements would occur in 2026; if construction occurs later than anticipated, an annual inflation rate of 5 percent should be used to inflate project costs. The cost estimate should be refined during the Design phase of the project. The ROW acquisition cost was not included in this estimate.

CITY OF HERMISTON, OREGON
E. JENNIE AVENUE LOCAL IMPROVEMENT DISTRICT
PRELIMINARY COST ESTIMATE¹
JUNE 2026

NO.	DESCRIPTION	UNIT	UNIT PRICE	ESTIMATED QUANTITY	TOTAL PRICE
1	Mobilization/Demobilization	LS	\$ 41,000	All Req'd	\$ 41,000
2	Temporary Protection and Direction of Traffic/Project Safety/QA/QC	LS	12,000	All Req'd	12,000
3	Erosion and Sediment Control	LS	5,000	All Req'd	5,000
4	Removal of Structures and Obstructions ²	LS	10,000	All Req'd	10,000
5	General Earthwork	LS	10,000	All Req'd	10,000
6	3/4"-0 Aggregate Base (3 inches)	CY	85	390	33,150
7	1"-0 Aggregate Base (6 inches)	CY	85	940	79,900
8	3-inch Asphalt Concrete Pavement (ACP)	TON	175	750	131,250
9	Stormwater, Southside ³	LS	45,000	All Req'd	45,000
10	Concrete Driveway Approaches	SF	20	600	12,000
11	Concrete Sidewalk	SF	15	11,300	169,500
12	Ribbon Curb	LF	45	2,307	103,815
CITY-FUNDED IMPROVEMENTS					
13	3/4"-0 Aggregate Base (3 inches)	CY	85	290	24,650
14	1"-0 Aggregate Base (6 inches)	CY	85	580	49,300
15	3-inch ACP	TON	175	470	82,250
16	Stormwater, Northside ³	LS	15,000	All Req'd	15,000
17	Americans with Disabilities Act Ramp	EA	5,000	4	20,000
18	Concrete Sidewalk	SF	15	50	750
19	Ribbon Curb	LF	45	50	2,250
20	Surface Restoration	LS	10,000	All Req'd	10,000
21	Permanent Striping and Signing	LS	5,185	All Req'd	5,185
Estimated City Construction Cost					\$ 209,385
Estimated Total Construction Cost					\$ 862,000
Construction Contingency (20% Rounded)					172,000
Design Engineering (10% Rounded)					86,000
Construction Engineering (10% Rounded)					86,000
TOTAL ESTIMATED IMPROVEMENTS COST (Rounded)					\$ 1,206,000
TOTAL ESTIMATED CITY IMPROVEMENTS COST (Rounded)					\$ 293,000

¹Right-of-way (ROW) acquisition costs are not included in this cost estimate.

²Power pole relocation is not included in this cost estimate since its impact to the ROW improvement has not been fully determined.

³Stormwater is assumed to include Option 2 - Infiltration Swale on the south side and a 2-foot wide gravel shoulder with a 6-foot wide ditch at a maximum slope of 3:1 on the north side.

QA/QC = quality assurance/quality control



Section 3 - Property Benefit Analysis

Hermiston Municipal Code 157.164

Property identification information, ownership information, and a description of the benefit that each property would receive from the proposed improvements are included on Figure 3-1. The benefit for each property in this E. Jennie Avenue Local Improvement District (LID) is that the street infrastructure that would be installed for the proposed development area would be in conformance with Hermiston Municipal Code 157.164. The proposed street improvements would ensure each tax lot has paved access to and from the remainder of the street system.

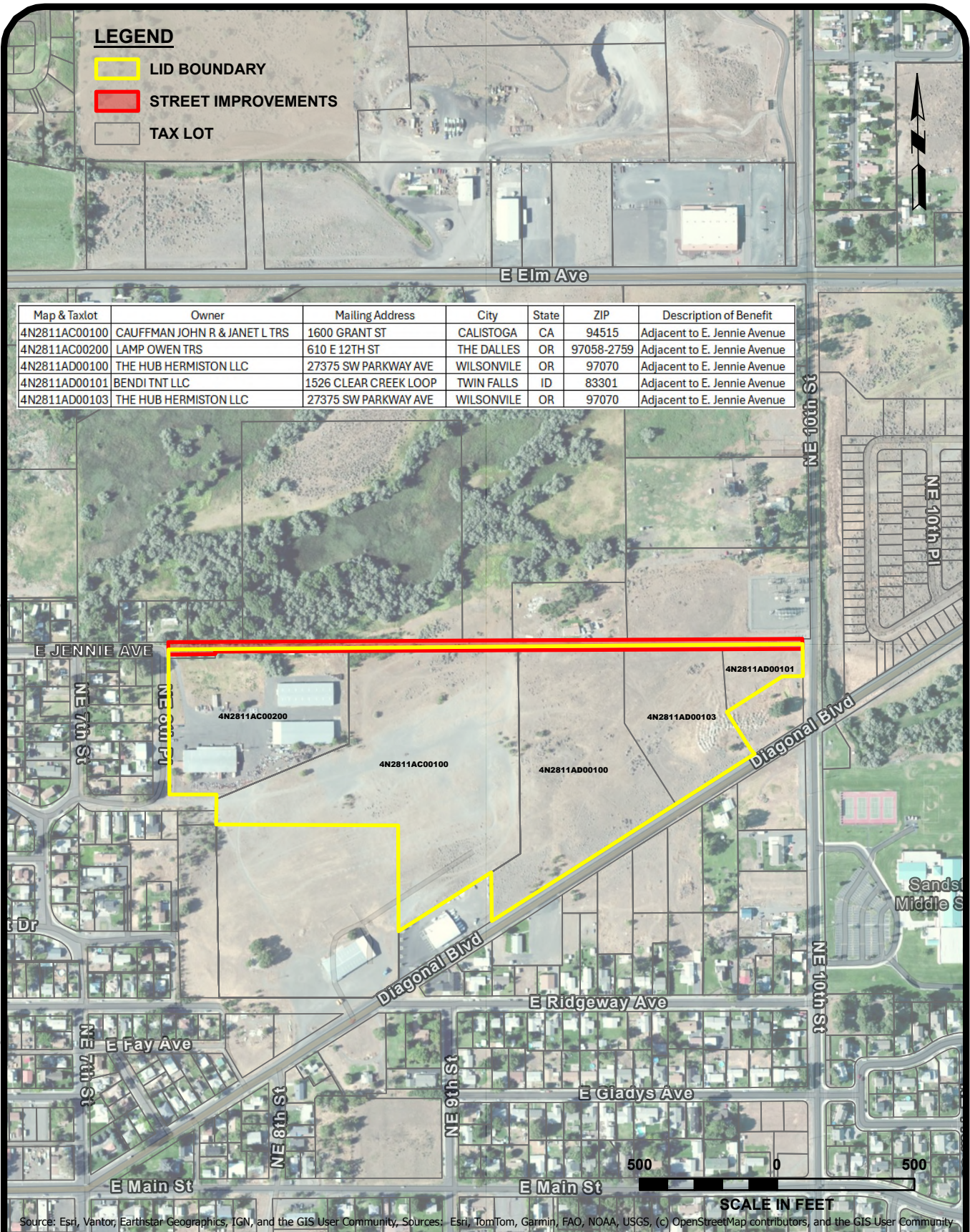
The methodology for assigning costs would be based on the frontage of the properties adjacent to the street improvements as outlined in Section 4 of this LID Engineering Report. The City will cover the cost of the additional 10 feet of paving on the north side of the street and any portion of the improvements east or west to connect to N.E. 8th Place and N.E. 10th Street. The benefit that each property would receive is directly proportional to the remaining improvements installed along the frontage of their properties.

LEGEND

- LID BOUNDARY
- STREET IMPROVEMENTS
- TAX LOT

Map & Taxlot	Owner	Mailing Address	City	State	ZIP	Description of Benefit
4N2811AC00100	CAUFFMAN JOHN R & JANET L TRS	1600 GRANT ST	CALISTOGA	CA	94515	Adjacent to E. Jennie Avenue
4N2811AC00200	LAMP OWEN TRS	610 E 12TH ST	THE DALLES	OR	97058-2759	Adjacent to E. Jennie Avenue
4N2811AD00100	THE HUB HERMISTON LLC	27375 SW PARKWAY AVE	WILSONVILLE	OR	97070	Adjacent to E. Jennie Avenue
4N2811AD00101	BENDI TNT LLC	1526 CLEAR CREEK LOOP	TWIN FALLS	ID	83301	Adjacent to E. Jennie Avenue
4N2811AD00103	THE HUB HERMISTON LLC	27375 SW PARKWAY AVE	WILSONVILLE	OR	97070	Adjacent to E. Jennie Avenue

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Source: Esri, Vantor, Earthstar Geographics, IGN, and the GIS User Community, Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, (c) OpenStreetMap contributors, and the GIS User Community



**CITY OF
HERMISTON, OREGON**
E. JENNIE AVENUE LOCAL IMPROVEMENT DISTRICT

**FIGURE
3-1**

PROPERTY BENEFIT ANALYSIS

Section 4 - Project Cost Allocation

Methodology

The City will pay a portion of the cost of the street improvement project to tie into the existing intersections. The property owner-assessed portion of the street improvement project included in the proposed E. Jennie Avenue Local Improvement District (LID) lends itself to allocating costs based on each property's frontage along the street improvements. The property frontage along the proposed street improvements relative to the total property frontage on the proposed street improvements for all properties in the LID is directly proportional to the assessment of each property. This method would distribute the property owner assessment costs of the LID equitably. Figure 4-1 shows each affected property, its associated frontage length, and its associated estimated LID assessment.

Description of Other Funds

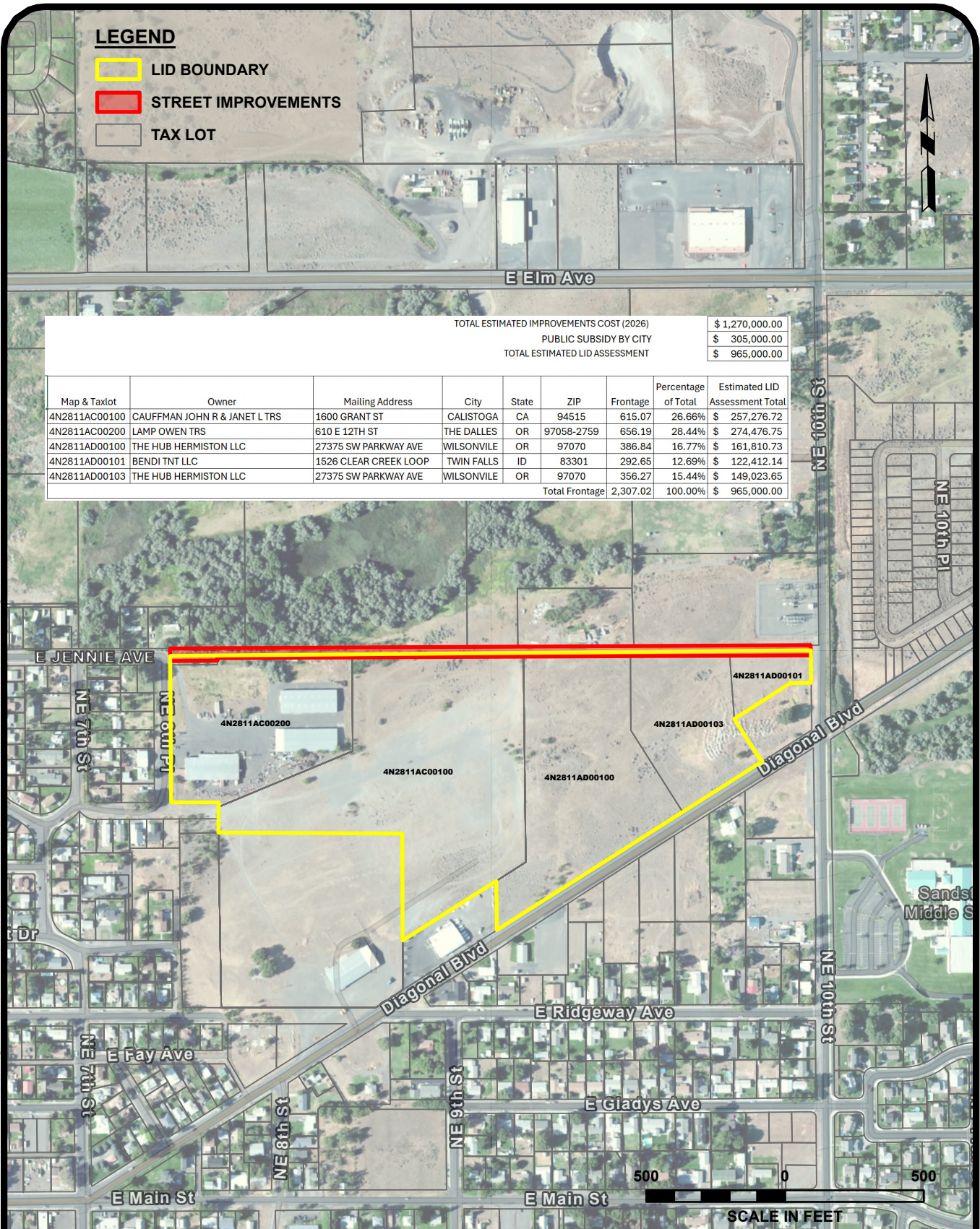
The City will cover the cost of the additional 10 feet of paving on the north side of the street and any portion of the improvements east or west to connect to N.E. 8th Place and N.E. 10th Street from the LID property frontages.

LEGEND

- LID BOUNDARY
- STREET IMPROVEMENTS
- TAX LOT

TOTAL ESTIMATED IMPROVEMENTS COST (2026)								\$ 1,270,000.00	
PUBLIC SUBSIDY BY CITY								\$ 305,000.00	
TOTAL ESTIMATED LID ASSESSMENT								\$ 965,000.00	
Map & Taxlot	Owner	Mailing Address	City	State	ZIP	Frontage	Percentage of Total	Estimated LID Assessment Total	
4N2811AC00100	CAUFFMAN JOHN R & JANET L TRS	1600 GRANT ST	CALISTOGA	CA	94515	615.07	26.66%	\$ 257,276.72	
4N2811AC00200	LAMP OWEN TRS	610 E 12TH ST	THE DALLES	OR	97058-2759	656.19	28.44%	\$ 274,476.75	
4N2811AD00100	THE HUB HERMISTON LLC	27375 SW PARKWAY AVE	WILSONVILLE	OR	97070	386.84	16.77%	\$ 161,810.73	
4N2811AD00101	BENDI TNT LLC	1526 CLEAR CREEK LOOP	TWIN FALLS	ID	83301	292.65	12.69%	\$ 122,412.14	
4N2811AD00103	THE HUB HERMISTON LLC	27375 SW PARKWAY AVE	WILSONVILLE	OR	97070	356.27	15.44%	\$ 149,023.65	
						Total Frontage	2,307.02	100.00%	\$ 965,000.00

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Source: Esri, Vantor, Earthstar Geographics, IGN, and the GIS User Community, Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, (c) OpenStreetMap contributors, and the GIS User Community

	<p>CITY OF HERMISTON, OREGON</p> <p>E. JENNIE AVENUE LOCAL IMPROVEMENT DISTRICT</p> <p>PROJECT COST ALLOCATION</p>	<p>FIGURE 4-1</p>
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Section 5 - Property Valuation and Assessment Estimate

General

The valuations for each property shown on Table 5-1 below represent the latest figures available from the Umatilla County Assessor’s office. Table 5-1 shows an estimate of the amount to be assessed for each property in the E. Jennie Avenue Local Improvement District (LID). These amounts represent the total current value of the assessments without regard for potential long-term repayment plans and associated interest fees. An explanation of options for long-term payment plans is presented below. A full accounting of how the assessment was calculated, using the methodology outlined in Section 4, is shown on Figure 4-1. Actual costs would be determined after construction is completed and final project costs are known.

**TABLE 5-1
UMATILLA COUNTY ASSESSOR’S PROPERTY VALUATIONS**

Account ID	Map and Tax Lot	Assessed Value*	Real Market Value*	Outstanding Tax Assessments	Estimated LID Assessment Total
124301	4N2811AC00100	\$434,830	\$1,311,800	\$0	\$257,276
124302	4N2811AC00200	\$686,830	\$1,437,760	\$0	\$274,477
124367	4N2811AD00100	\$705,870	\$1,194,430	\$0	\$161,811
168622	4N2811AD00101	\$94,750	\$94,750	\$0	\$122,412
174339	4N2811AD00103	\$346,835	\$434,734	\$0	\$149,024
TOTAL		\$1,847,130	\$4,071,900	\$0	\$965,000
Public Subsidy by City					\$305,000
Total Estimated Improvement Costs (2026)					\$1,270,000

^a Values for Tax Lot 4N2811AD00103 were not available on the Umatilla County assessor’s website due to the lot being recently created. Values were calculated by taking the values of the parent tax lot (4N2811AD00100) per acre and multiplying them by the acreage of the child parcel (4N2811AD00103).

Long-term Repayment Plan

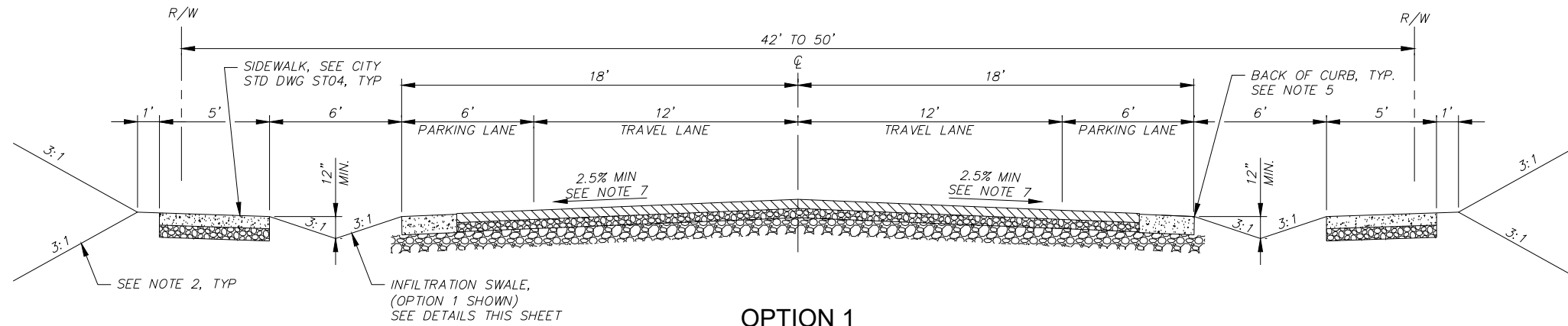
The construction of the proposed improvements considered in the LID would be coordinated by the City of Hermiston on behalf of each of the properties. Each property owner would have the option to pay their LID assessment in its entirety, or they may choose to make installment payments over ten years. The City intends to finance the improvements up front through the most cost-effective means available. Any costs associated with the carrying costs of financing the proposed improvements would be passed on directly to the benefiting properties; however, the City would not add on any administrative or overhead fees associated with obtaining and managing the financing instrument.

Appendix Table of Contents

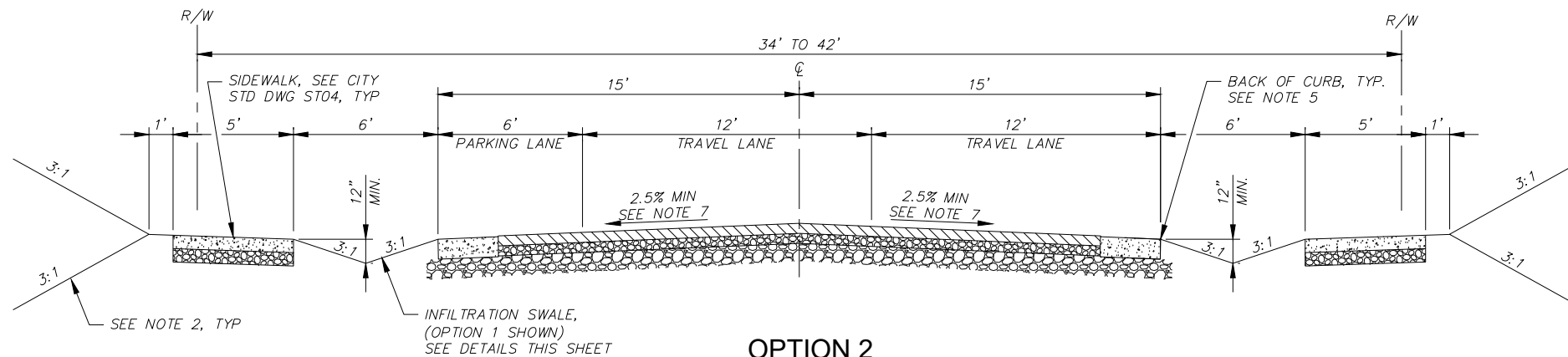
Appendix A Standard Drawing

APPENDIX A
Standard Drawing

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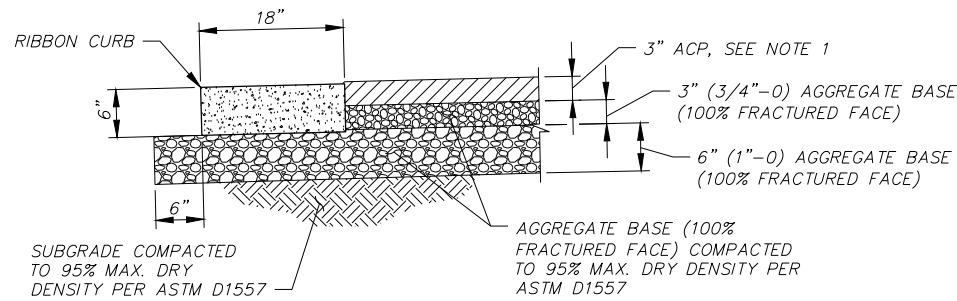
OPTION 1
(PARKING BOTH SIDES)
NTS



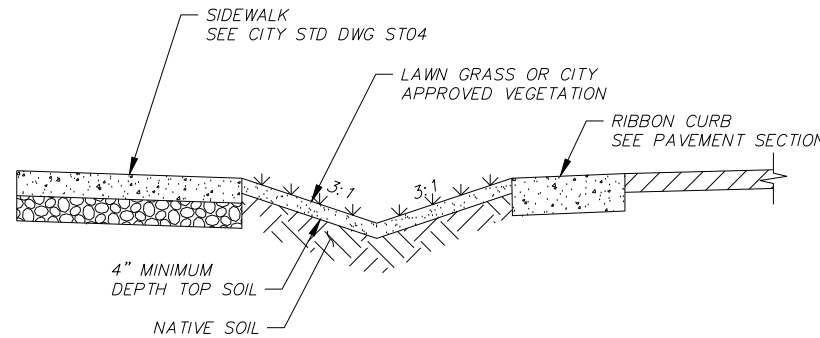
OPTION 2
(PARKING ONE SIDE)
NTS

NOTES

1. ASPHALT CONCRETE PAVEMENT SHALL BE CONSTRUCTED IN ONE LIFT.
2. SLOPES SHALL NOT EXCEED 3:1 UNLESS OTHERWISE APPROVED BY THE CITY.
3. PARKING LANE MAY BE LOCATED ON EITHER SIDE OF STREET.
4. COMPACT AGGREGATE BASE (100% FRACTURED FACE) AND SUBGRADE TO 95% MAXIMUM DRY DENSITY PER ASTM D1557.
5. THE DISTANCE BETWEEN TOP BACK OF CURBS IS A CRITICAL DIMENSION. CURBS POURED IN AN INCORRECT LOCATION SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR.
6. INFILTRATION SWALE OPTION 2 ASSUMES POORLY GRADED SAND WITH SILT WITH A DESIGN INFILTRATION RATE OF 3" PER HOUR. SHOULD SOIL CONDITIONS SUCH AS CALICHE, HARD PAN, CLAY, SILT, ETC. BE ENCOUNTERED, THE CONTRACTOR SHALL NOTIFY THE CITY IMMEDIATELY. THE CROSS SECTIONAL AREA MAY BE ADJUSTED TO ACCOMMODATE DIFFERENT SOIL TYPES WHEN APPROVED BY THE CITY.
7. DESIGN STREET CROSS SLOPE SHALL BE 2.5%. CONSTRUCTED STREET CROSS SLOPE SHALL BE MINIMUM 2% AND MAXIMUM 3% WHERE NEWLY INSTALLED CURB AND GUTTER ELEVATIONS DEVIATE FROM THE APPROVED PLANS, THE FINISH GRADE OF CENTERLINE SHALL BE BASED ON THE ACTUAL EDGE OF GUTTER ELEVATIONS TO ENSURE STREET CROSS SLOPE REQUIREMENTS ARE MET.
8. THE MINIMUM INFILTRATION SWALE LENGTH SHALL BE 50% OF THE LENGTH OF THE PROPERTY FRONTAGE UNLESS OTHERWISE APPROVED BY THE CITY.



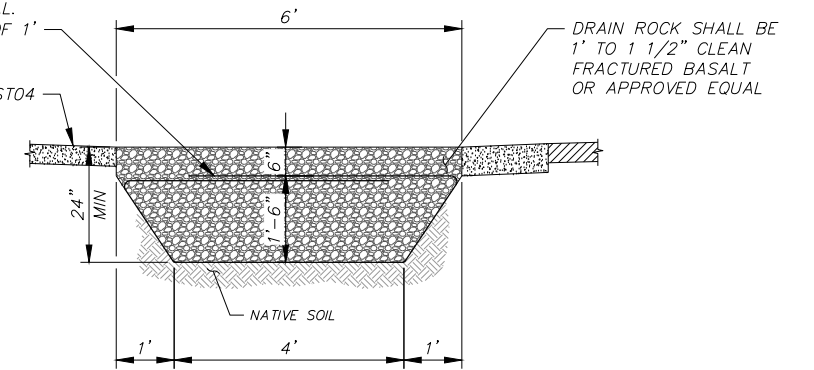
PAVEMENT SECTION
N.T.S.



INFILTRATION SWALE OPTION 1
NTS

GEOTEXTILE FILTER FABRIC SHALL BE 15' WIDE MIRAFI 140N OR APPROVED EQUAL. LAP FABRIC A MINIMUM OF 1'

SIDEWALK SEE CITY STD DWG ST04



INFILTRATION SWALE OPTION 2
NTS



**CITY OF
HERMISTON, OREGON
STANDARD DRAWINGS**

**FIGURE
ST12**

URBAN LOCAL STREET WITH SWALE