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**PLANNING & DEVELOPMENT DEPARTMENT  
STAFF REPORT  
JANUARY 9, 2024**

*\*JANUARY 30, 2024 (Planning Commission)*

*Project continued from January 9, 2024 to January 30, 2024 Planning Commission.*

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**Application #:** 2023-37  
**Project Name:** Geode Flats  
**Application:** Preliminary PUD Plan  
**Representative:** McCool Development Solutions, LLC  
**Property Owner:** TWG Raptor LP  
**Location:** 614 Raptor Road  
**Current Zone:** Commercial 2 (C-2)  
**Request:** This is a request for a Preliminary Planned Unit Development (PUD) Plan to establish zoning standards for a multi-family development on approximately 5.07 acres.

**PROJECT DESCRIPTION:**

This application was continued from the January 9, 2024, Planning Commission meeting to a special Planning Commission meeting for January 30, 2024. The Planning Commission made a motion to accept the continuance request and a motion to hold a Special Planning Commission meeting on January 30, 2024.

This is an application for a Preliminary PUD Plan to establish zoning standards for a multi-family development on approximately 5.07 acres of property located at 614 Raptor Road. This application began back on January 24, 2023, when the Planning Commission and City Council held a joint workshop on the Concept Plan for this project previously named the Residences at Fruita. Concept Plans are designed for PUDs as an optional 1<sup>st</sup> step in the review process where the Planning Commission and City Council can provide non-binding feedback in a workshop/work session and are intended to provide overall direction to the applicant.

The property is approximately 5.07 acres located directly west of the La Quinta Inn and northeast of James M. Robb State Park. The property is also bordered by Raptor Road on the north and Jurassic Court on the south. Based on the project narrative, “The proposed Preliminary PUD Plan takes a form-based approach to multifamily development to provide new housing that matches the look and feel of the surrounding area and promotes more affordable housing while providing

a demonstrated public benefit that would not be available under conventional zoning restrictions in terms of density, building heights, parking, and public benefit.”

The plan includes four (4) multi-family residential buildings and a clubhouse with proposed access coming from Jurassic Court on the south and another access coming from the west side. The plan identifies two (2) separate planning areas, Planning Area A and B, with a total of five (5) buildings and four (4) of which are designed for multi-family. Submitted with the application is a PUD Guide, which will serve as the primary zoning standards for the property with the primary goal of this project is to create affordable housing. The PUD Guide details out zoning standards for the property including land uses, density, setbacks, building heights, design standards, parking standards, landscaping standards, site lighting standards, and signage. Much like other PUD Guides, anything not mentioned in the PUD Guide will revert to the City’s Land Use Code.

**SURROUNDING LAND USES AND ZONING:**

Surrounding land uses are primarily commercial. There is a Mack Truck Sale and repair shop directly to the north. East of that is also a large semi-truck repair shop. South and West are the State Park and the City of Fruita old sewer lagoon site. The map below identifies the various zones in this area.



## AERIAL PHOTO



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### REVIEW OF APPLICABLE LAND USE CODE REQUIREMENTS:

#### PLANNED UNIT DEVELOPMENTS (PUD'S)

Section 17.19.010 explains the purpose of a Planned Unit Development and states, “*The purpose of this Chapter is to encourage flexibility and innovation in developments in exchange for a community benefit that could not otherwise be realized through the strict adherence to the Code.*”

The Planned Unit Development request must be reviewed in accordance with Section 17.19.030 of the Land Use Code which are addressed within this Staff Report.

**17.19.030 (A)(1) (a-d)**

***a. Conformance to the City of Fruita’s Comprehensive Plan, Land Use Code, Design Criteria and Construction Specifications Manual and other city policies and regulations;***

The applicant has submitted enough information to consider the zoning application as it’s proposed. More information will be required to be provided with a subsequent application to determine compliance with the City’s Engineering Standards (Design Criteria and Construction Specifications Manual).

As mentioned with the Concept Plan, The City of Fruita’s transportation plan requires right-of-way construction connecting Raptor Road and Jurassic Court.

The primary purpose of this PUD request is to allow for more density, parking reductions, and greater building height than what is allowed under the conventional Commercial-2 (C-2) zone district.

The C-2 zone allows for 12 dwelling units per acre, giving the total allowed residential count of 60 dwelling units (12 du \* 5 acres).

The C-2 zone allows for a maximum building height of 35 feet. At the January 24, 2023 workshop, there was no concern about building heights overall.

The PUD Guide states that all uses permitted by right and conditional uses in the C-2 zone district be allowed, including Multifamily Residential and a Clubhouse. It is the recommendation of Staff that the only allowed uses be Multifamily Residential and a Clubhouse as an accessory use and that all other allowed uses and conditionally allowed uses be removed. The purpose of the PUD is to provide either affordable housing or market rate rental housing in Fruita, not commercial uses. To name a few allowed uses supported in the C-2 zone are: Alcohol sales, Bar/Nightclub, Short-Term Rentals, General Retail Sales (indoor and outdoor), Small Engine/appliance repair shops, Outdoor Storage, Self-storage/Mini warehouse.

**PARKING:**

Section 17.37.030 contains parking requirements for Multi-Family developments. The requirements are as follows:



<b>Use Categories</b> (Examples of Uses are in Chapter 17.03)	<b>Minimum Motorized Vehicle Parking Per Land Use</b> (fractions rounded down to the closest whole number)	<b>Minimum Bicycle Parking Per Land Use</b> (fractions rounded down to the closest whole number)	<b>Minimum Motorized Vehicle Parking Per Land Use</b> (DMU Zone District)
<b>Residential Categories</b>			
Multi-family	1 space per studio or 1-bedroom unit  1.5 spaces per 2-bedroom unit  2 spaces/unit per 3-bedroom or larger unit	1 space per unit	0.95 spaces per studio or 1-bedroom unit  1.25 spaces per 2-bedroom unit  2 spaces per 3-bedroom or larger unit

The application is proposing two (2) separate parking rate ratios which are contained on page 12 of the PUD Guide submitted. The ratios breakdown either Market Rate Rentals or Affordable Rentals.

	<b>Market Rate Rental</b>	<b>Affordable Rental</b>
<b>Parking Rate (per dwelling unit)</b>	1.32	0.75
<b>180 Units</b>	238 Spaces	135 Spaces

These proposed ratios are accompanied by a Parking Analysis Study provided by transportation engineering firm McDowell Engineering, LLC. At the January 24, 2023, workshop, the Planning Commission and City Council requested that this be completed with the Preliminary PUD Plan application. The Concept Plan proposed a parking ratio of 0.5 spaces per unit.

**CHILDCARE:**

The Concept Plan proposed a childcare center in the form of land dedication or a cash payment in lieu of dedicating land. It appears that the developer is open to creative solutions with regards to the Childcare proposal as a community benefit. The Planning Commission and City Council were supportive of this idea. This application proposes a cash payment of \$100,000 due upon issuance of building permits. Staff is supportive of the amount proposed, however, Staff recommends that the cash in lieu payment be paid upon Site Plan approval.

## **Fruita In Motion: Plan Like a Local Comprehensive Plan (Master Plan):**

With regards to the Master Plan, Fruita is an exceptional community. Throughout the comprehensive plan process, residents brought up how much they love living in Fruita, its small-town character, and their desire to preserve the community's most desirable qualities into the future. The plan starts by declaring what makes Fruita special. In turn, these community values are the foundation of the plan—shaping the plan vision, goals, policies, and actions. Two community values represented include:

- *Fruita is a place where you run into neighbors, friends, and acquaintances at local stores and restaurants, parks, and the community center. (Community Values, Page 2, Comprehensive Plan)*
- *Fruita is a community where people are invested and constantly work to make the community better. (Community Values, Page 2, Comprehensive Plan)*
- *Fruita is committed to a land use pattern and supporting policies that promote access to housing across the income spectrum of its residents. (Community Values, Page 2, Comprehensive Plan)*

Influenced by the community values expressed on page 2 of the Comprehensive Plan, the Plan Vision states, *“The City of Fruita values quality of place. It’s an inclusive city, with a small-town feel and vibrant downtown, surrounded by public lands. People love to live, work, and play in Fruita because the City facilitates community, safe neighborhoods, family-friendly events, and walking and biking. The City governs in a way that’s responsive to its citizens and prioritizes high-impact services and projects. Fruita fosters a fun and funky ambiance around the arts, agriculture, and recreation.”*

The Community Snapshot section within the Comprehensive Plan also identified affordability as a growing concern within Fruita. *“Affordability issues are greatest among renters in Fruita, with about half of all renters paying more than 30% of their income towards housing costs. This is defined as being cost burdened, wherein a household is paying too much towards housing. The rental supply in Fruita is extremely limited with essentially zero vacancy, allowing landlords to charge higher rents. The percentage of renters in Fruita has increased, even though most housing being built is in the form of single-family homes. Some people may be renting single-family homes by choice; for others it may be the only option and they would prefer a lower cost option such as an apartment or duplex. Housing affordability issues affect the ability of local businesses to attract and retain employees. This is a threat to economic sustainability if left unchecked.”* (Community Snapshot, Page 12, Comprehensive Plan).

*The Future Land Use Map prioritizes infill over sprawling residential development at the edge of the city limits. The policies in this plan aim to spur residential development within the existing city limits and UGB. It aims to transform the State Highway 6&50 corridor by allowing and encouraging multifamily housing on parcels and blocks adjacent to this corridor. (Chapter 3 Land Use & Growth, Page 26, Comprehensive Plan).*

Goal #4. *Allow and encourage a diversity of housing types to fit the needs of the Fruita community and provide the diverse “funky” character that is treasured by residents. Fruita’s housing stock is getting more homogenous and more expensive. As a community that prides itself on being inclusive, this ethos should extend to providing types of housing for people of different ages, income ranges, family structures, and aesthetic preference. Allowing and encouraging more apartments and/or townhomes in appropriate locations could contribute to more affordable housing options.* (Chapter 3 Land Use & Growth, Page 39, Comprehensive Plan).

Goal #9. *Support flexibility in zoning and the development of diverse housing types as part of an economic sustainability strategy.*

The availability of workforce housing is now an economic development issue in Fruita. Additionally, businesses are looking for the right fit in terms of buildings, land, and space to move, expand, or start businesses. Fruita can look to the experience of high-cost mountain resort towns to observe the risks of waiting too long to act. Local businesses are experiencing workforce shortages. The Land Use & Growth chapter of this plan (Chapter 3) contains strategies and policies on increasing the diversity and supply of housing in Fruita. For workforce housing, emphasis is on market rate and affordable (income restricted) rental housing and attainably priced ownership housing. For commercial spaces, Land Use Code changes can lay the groundwork for being an attractive place for a new or existing business. Flexibility in the design and types of buildings that can be built would be a business-friendly approach. (Chapter 4 Economic Development, Page 58, Comprehensive Plan).

The City’s Master Plan also states that the City of Fruita, “*Support the Mesa County Public Health Department and its Childcare 8,000 initiative in seeking to increase the number of available spaces for childcare in the county*” (Goal #2, Policy 2.E, on Page 66). This has been a continued goal of the City of Fruita for many years. Additionally, Page 96 of the Master Plan states that the City of Fruita, “*support ample, affordable early learning and childcare centers for city residents.*”

Because this application is seeking to meet many community goals contained in the Master Plan, this application should be able to meet all applicable construction standards, and can be consistent with the Land Use Code, this criterion can be met.

***b. Consistency with one or more of the following general goals for a PUD justifying a deviation from the requirements of the Code, including but not limited to:***

***i. More convenient location of residences, places of employment, and services in order to minimize the strain on transportation systems, to ease burdens of traffic on streets and highways, and to promote more efficient placement and utilization of utilities and public services; or***

This application appears to be taking many housing goals into consideration based on the proposed PUD Guide. The location of the property and the emphasis on housing are

conveniently located near places of employment, public transportation networks/locations, and is close to the highway networks. If developed for multifamily, the location of the property should allow residents to easily commute to other areas or even locally without too much driving. Furthermore, there are trails nearby that will allow for multimodal transportation opportunities.

- ii. To promote greater variety and innovation in residential design, resulting in adequate housing opportunities for individuals of varying income levels and greater variety and innovation in commercial and industrial design; or*

The proposed application for a PUD zoning classification meets this general goal for justifying a PUD application. The project is designed so that a large multifamily project could be constructed at either a market rate rental or as an affordable housing project. If constructed, the project would provide greater variety and innovation to the City of Fruita's housing market. Staff is hopeful that if constructed, this project would be able to bring additional housing opportunities for individuals of varying income levels and greater opportunities for housing options in Fruita.

- iii. To relate development of particular sites to the physiographic features of that site in order to encourage the preservation of its natural wildlife, vegetation, drainage, and scenic characteristics; or*

- iv. To conserve and make available open space; or*

- v. To provide greater flexibility for the achievement of these purposes than would otherwise be available under conventional zoning restrictions; or*

The proposed application is requesting deviations from the C-2 zone district that would only allow for 50 dwelling units to be constructed at a density of 12 dwelling units per acre. Under this conventional zoning standard, the project would be difficult to develop. Over the past few years, the property has seen other development proposals such as self-storage units. Although allowed in the C-2 zone district, Staff feels that this proposal for multifamily residential is something supported in the City's Master Plan. The major deviation from the C-2 zone district is the residential density component, which Staff is supportive of. Additionally, Staff believes that this application is making an efficient use of the land.

- vi. To encourage a more efficient use of land and of public services, or private services in lieu thereof, and to reflect changes in the technology of land development so that resulting economies may inure to the benefit of those who need homes; or*

- vii. To conserve the value of land and to provide a procedure which relates the type, design, and layout of residential, commercial and industrial development to the*

*particular site proposed to be developed, thereby encouraging the preservation of the site's natural characteristics.*

Based on the application submittal and the project narrative, the overall goal of this project is to implement many of the housing related goals contained in the City of Fruita's Comprehensive Plan. The proposed PUD plan should allow for efficient development and layout of the residential buildings.

- c. Conformance to the approval criteria for Subdivisions (Chapter 17.21) and/or Site Design Review (Chapter 17.09), as applicable; except where Adjustments to the standards of this Title are allowed, and;***

The application will be reviewed in accordance with Section 17.09 for a Site Design Review application if this PUD is approved. As it stands now, there has been much consideration as to how the applicant wants the project to be built out and most of that has been shown in the PUD Guide related to the Design Standards. These Design Standards will be used to review the application in its future steps. Most all deviations have been reviewed by Staff and are supported as they are minor. There are no direct review criteria for Site Design Reviews, however, Staff and other review agencies will have an opportunity to review the site design and building elevations when a future application is submitted. Staff will also use the PUD Guide to review a Site Design Review application.

This criterion can be met.

- d. Conformance with applicable Design Standards and Guidelines as outlined in Chapter 17.13, unless approved as an Adjustment pursuant to the Adjustment criteria set forth in Section 17.13.020(B).***

The application proposes deviations from the Design Standards contained in Chapter 17.13 of the Land Use Code. These deviations are outlined in the proposed PUD Guide and are overall supported by Staff as they do meet the intents and purpose of the Business Design Standards related to residential types of developments.

The one deviation that Staff recommends follow the Business Design Standards is related to Building Design and the overall architecture to address all sides of the buildings visual from the street. This section in particular is contained in Section 17.13.060 (B)(1) and reads, “*Architectural designs shall address all facades of a building visible from the street with materials, detailing, and color. Architectural elements should wrap around building corners,*” and the PUD deviation proposes that only the architectural designs address the street-facing facades of a building.

If the architectural features wrap around all sides of each building, then this criterion can be met.



**17.19.030 (B)(1) (a-e)**

***a) Adequate resolution of all review comments; and***

Because this is a zoning application, no review agencies were needed to provide input. Review Agencies will be communicated with if a Site Design Review Application is submitted. Review Agencies like CDOT, Ute Water, School District 51, Mesa County Transportation Planning Office, Xcel Energy, GVP, and others will be provided with time to comment.

***b) Proposed zoning and adjustments are generally consistent with the character in the immediate area, or are necessary to address an important community purpose, as determined by City Council.***

The proposed PUD Guide, which contains the zoning related elements of the development, are consistent with the character in the immediate area and are written to ensure that the development can be completed in accordance with the City's Design Standards and other applicable requirements. The modifications proposed in the PUD Guide provided also appear to be justified where necessary. The character of the adjacent area is unique, but the application, if developed, should provide for a consistent transition of what is already built.

***c) Conformance to the approval criteria for Subdivisions (Chapter 17.21) and/or Site Design Review (Chapter 17.09), as applicable; except where Adjustments to the standards of this Title are allowed, and;***

This is strictly just a zoning application. In accordance with the submitted documents and if approved, Staff expects that the next step would be the submittal of a Site Design Review application.

If a Site Design Review application were to be submitted in the future, Staff would recommend that the application be reviewed and approved administratively.

***d) Conformance with applicable Design Standards and Guidelines as outlined in Chapter 17.13, unless approved as an Adjustment pursuant to the Adjustment criteria set forth in Section 17.13.020(B).***

This criterion is also considered above in the Staff Report. The application proposes deviations from the Design Standards contained in Chapter 17.13 of the Land Use Code. These deviations are outlined in the proposed PUD Guide and are overall supported by Staff as they do meet the intents and purpose of the Business Design Standards related to residential types of developments.

The one deviation that Staff recommends follow the Business Design Standards and not be deviated from are related to Building Design and the overall architecture to address all sides of the buildings visual from the street. This section in particular is contained in Section 17.13.060

(B)(1) and reads, “*Architectural designs shall address all facades of a building visible from the street with materials, detailing, and color. Architectural elements should wrap around building corners.*” And the PUD deviation proposes that only the architectural designs address the street-facing facades of a building.

If the architectural features wrap around all sides of each building, then this criterion can be met.

***e) Compliance with conditions of approval on the Concept Plan, if any.***

Based on the feedback provided by the Planning Commission and City Council at their January 24, 2023, workshop, it appears that the application has taken the necessary steps to address concerns discussed at this meeting. The application provides an approach to Childcare as a community benefit, has provided a Parking Demand analysis, and proposes building design standards that should allow for consistent and transitional standards to blend with existing buildings.

This criterion appears to have been met.

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**LEGAL NOTICE:**

Legal Notice (minimum of 15 days prior to Planning Commission)	
December 14, 2023 (26 days prior)	Post Cards [17.07.040 (E)(1)(d)]
December 14, 2023 (26 days prior)	Sign Posting [17.07.040 (E)(1)(c)]
December 15, 2023 (25 days prior)	Legal Ad [17.07.040 (E)(1)(a)]

\*Supplemental legal notice information attached with the Staff Report

**REVIEW COMMENTS:**

All review comments received are included with this Staff Report.

**PUBLIC COMMENTS:**

No written public comments have been received at this time.

**PUBLIC HEARING SCHEDULE:**

Planning Commission – January 9, 2024

City Council 1<sup>st</sup> Reading of zoning Ordinance – February 6, 2024 (*tentative*)

City Council 2<sup>nd</sup> Reading of zoning Ordinance – February 20, 2024 (*tentative*)

**STAFF RECOMMENDATION:**

Because the application meets the requirements of Section 17.19.030 (A)(1) (a-d) and Section 17.19.030 (B)(1) (a-e) of the Fruita Land Use Code, **Staff recommends approval** of the proposed Geode Flats Preliminary PUD Plan with the condition that the application adequately resolve all review comments and conditions in the Staff Report with either the zoning ordinance or the Final PUD application.

Recommendations/Conditions by Staff:

1. Remove all commercial land uses from the PUD Guide.
2. \$100k childcare cash in lieu fee be paid upon Site Plan approval.
3. Design Standards contained in Section 17.13.060 (B)(1) be met as written in the Land Use Code.
4. If this PUD Guide is approved with these conditions by Staff and fully met, then a future Site Design Review application be reviewed administratively unless there is a major modification or change in the application deemed by the Community Development Director.

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**SUGGESTED MOTION:**

Mr. Chair, because the application meets or can meet all applicable approval criteria for a Preliminary PUD Plan including Section 17.19.030 (A)(1) (a-d) and Section 17.19.030 (B)(1) (a-c) of the Fruita Land Use Code, I move to recommend **approval** to the Fruita City Council with the conditional that all review comments and conditions in the Staff Report be adequately resolved with either the zoning ordinance or the Final PUD Plan application.

Geode Flats – Preliminary PUD Plan (zoning)

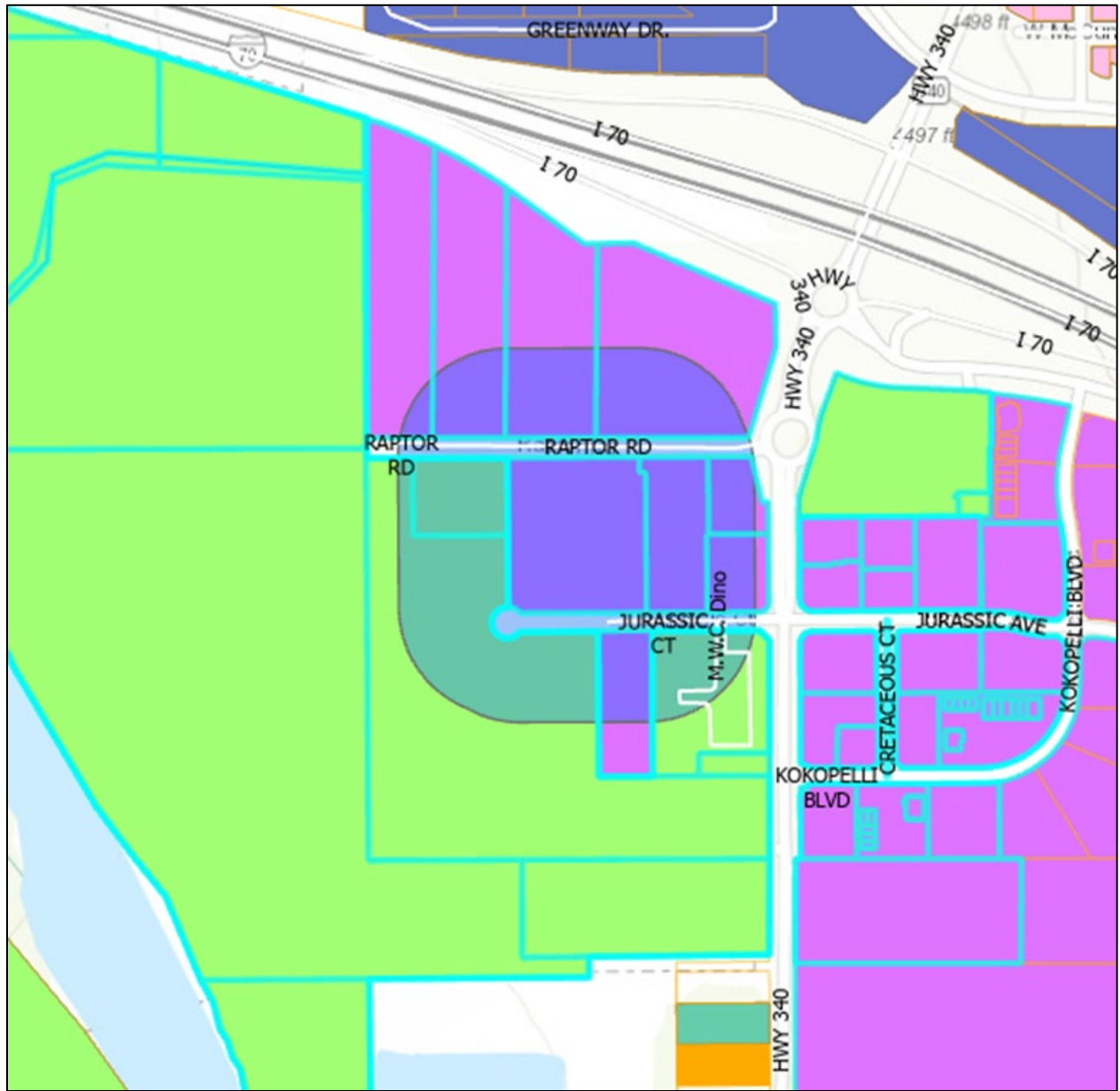
Supplemental Legal Notice

Legal Notice (minimum of 15 days prior to Planning Commission)	
December 14, 2023 (26 days prior)	Post Cards [17.07.040 (E)(1)(d)]
December 14, 2023 (26 days prior)	Sign Posting [17.07.040 (E)(1)(c)]
December 15, 2023 (25 days prior)	Legal Ad [17.07.040 (E)(1)(a)]
*February 7, 2024	Post Card
*February 14, 2024	Legal Ad (initiated by City Clerk)









**From:** [Carrie McCool](#)  
**To:** [Henry Hemphill](#)  
**Cc:** [Ryan Kelly](#); [Megan Adams](#)  
**Subject:** Re: Planning Commission packet\_Continuance Request  
**Date:** Tuesday, January 9, 2024 8:23:38 AM

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Good morning, Henry.

Our team has reviewed the Planning Commission Staff Report and appreciates staff work on the Report. In general, we are in concurrence with the analysis; however, there are concerns with the recommended conditions of approval 1 -3.

We would like to have the opportunity to discuss the conditions with Staff prior to moving forward to the Planning Commission public hearing. As such, we kindly request a continuance of the Planning Commission hearing to Tuesday, January 30, 2024, with the City Council public hearing 1st ready to be scheduled on February 6, 2024 agenda, to keep us on schedule for 2nd Reading on February 20, 2024.

On behalf of TWG, we remain committed to our primary goal of constructing affordable housing in Fruita to implement the vision and goals of the City's Comprehensive Plan. Thank you for your consideration.

Best,



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## **GEODE FLATS PRELIMINARY PUD PLAN (REZONING) PROJECT NARRATIVE**

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### **Project Description**

The subject property is an infill site located at 614 Raptor Road, is approximately 5.07 acres and is currently zoned Commercial-Two (C-2). The intent of the Planned Unit Development (PUD) is to allow flexibility and innovation in the development of rental market rate or affordable multifamily housing in exchange for a community benefit that could not otherwise be realized through the strict adherence to the Fruita Land Use Code. The proposed uses include all uses permitted by right and conditional use in the C-2 zone district, including but not limited to multifamily residential and clubhouse. The proposed Preliminary PUD Plan takes a form-based approach to multifamily development to provide new housing that matches the look and feel of the surrounding area and promotes more affordable housing while providing a demonstrated public benefit that would not be available under conventional zoning restrictions in terms of density, building heights, parking, and public benefit.

TWG's primary goal of the rezone request is to work closely with the City to create this PUD to allow construction of affordable housing in Fruita who earn at and below 140% of the area median income (AMI) to fulfill the need for affordable housing and thus improve the quality of life for Fruita residents. The project would include the build-out of Jurassic Court, Raptor Road, and the construction of a north/south minor collector to provide connectivity between Raptor Road and Jurassic Court as illustrated on the Road Classification Map in the Comprehensive Plan. Additionally, this connection will promote circulation with convenient connections via streets and pedestrian ways to parks, and adjacent neighborhood-oriented services and amenities in conformance with the Fruita Design Principals as outlined in Chapter 17.13 of the Fruita Land Use Code. The community component of the project includes a payment of \$100,000 to the city in lieu of performance to assist in providing additional childcare facilities in the area.

The Preliminary PUD Plan is divided into two (2) planning areas (A and B) to allow the construction of four (4) multifamily residential buildings, a clubhouse, and a minimum of two on site amenity elements that could include playground equipment, common outdoor gathering spaces, pocket park, raised garden beds, trails, furnished exercise room, community room or computer room. The proposed overall site dimensional standards include a minimum lot size of 4.5 acres with a maximum of five (5) buildings with no specific density standard and a parking rate of 0.75 spaces per dwelling unit for affordable rental and 1.31 spaces per dwelling unit for market rate rental. Planning Area A would allow two (2) multifamily buildings and the clubhouse building. A maximum structure height of four (4) stories is proposed for consistency with the building mass of the La Quinta hotel located along the eastern boundary of the site. Planning Area B would allow two 3-story multifamily buildings to provide an appropriate transition to the James M. Robb – Colorado River State Park to the west.

## Neighborhood Meeting

The TWG Team hosted a neighborhood meeting on Wednesday, September 6, 2023, from 5:30 – 6:30 P.M., at the Fruita Community Center. One person attended the meeting (see attached sign-in sheet). The meeting participant did not have any concerns with the proposal but noted there were multifamily projects in Fruita that were approved with a parking reduction and he felt they had problems.

## Justification of Modifications of Zoning and Design Standards

The submittal includes a Preliminary PUD Plan and PUD Guide. Below is an overview of the proposed deviations from the underlying zone district density and design standards, as well applicable narrative on how the Preliminary PUD complies with the conditions of approval on the Concept Plan:

- **Allowed uses.** In addition to all uses permitted by right and conditional use in the C-2 zone district, multi-family residential and clubhouse are also included in the allowed land uses to provide greater flexibility for the achievement of the purpose of PUDs and provide a more convenient location of residences, places of employment, and services in order to minimize the strain on transportation systems, to ease burdens of traffic on streets and highways, and to promote more efficient placement and utilization of utilities and public services.
- **On site amenities.** Based on input from the Fruita Planning Commission and City Council, the Preliminary PUD includes a list of amenities wherein a minimum of two elements must be included in the Site Design plan set. This allows for flexibility in selecting the most appropriate amenities to serve the needs of future residents.
- **Public Benefit.** In addition to providing a much-needed diversity in Fruita's housing stock, the Preliminary PUD Plan proposes a childcare center as the community component of the project. A payment of \$100,000 is due upon issuance of building permits in lieu of dedicated land for a childcare center to be provided off site pursuant to Council's directive at Concept Plan approval. The cash-in-lieu contribution will provide the city with the resources needed to select a childcare provider partner and the best location for a new childcare center to meet community needs.
- **Density and Building Heights.** Instead of setting forth a maximum density limitation, the Geode Flats PUD focus is placed on the size and mass of the multifamily buildings per planning area. You'll see that the two planning areas propose height limitations in terms of the number of stories versus the 35-foot height limitation of the C-2 zone district. Planning Area A would include two 4-story buildings to complement the building form of the La Quinta hotel, with Planning Area B transitioning down to 3-story buildings along the southwestern portion of the site. The Preliminary PUD Guide includes a diagram to illustrate how to measure building heights (see Figures 1.1 A and 1.1 B) with a slight deviation to allow elevator overrun and mechanical to extend beyond the 4-story height in Planning Area A. This approach would allow for unique architecture with appropriate building mass and scale that is consistent with the building form in the area.
- **Minimum Lot Area.** Instead of setting forth 5,000 square feet per dwelling unit, the minimum lot area is proposed to match the developable size of the lot.
- **Minimum Front/Street Yard, Side, and Rear Yard Standards for multi-family.** The deviations from the setback requirements outlined below are due to the property's unique configuration, as the lot has three street frontages and an interior side yard (no rear yard). The proposed yard standards will achieve several goals, including preserving scenic

characteristics, creating visually appealing street frontages, and promoting compatibility with the surrounding land uses.

- The minimum front yard has been increased from zero to 14 feet to allow for landscape buffers along all street frontages.
- Minimum side yard has been revised to clarify that it is a minimum interior side yard with no deviation proposed to the standard.
- Minimum rear yard standards are not applicable as there is only one interior side yard that forms the eastern boundary of the site.
- **Maximum Lot Coverage.** Reduction in the maximum lot coverage from 80% to approximately 60% with 35.3% for Planning Area A and 24.4 percent for Planning Area B. The reduction would allow for more land to be allocated to common areas and recreational amenities, which will enhance the overall quality of life for residents. The Preliminary PUD Guide includes graphics to illustrate maximum lot coverage (see Figure 1.1).
- **Design Standards.** The Preliminary PUD Guide includes the Business Design Standards from Section 17.13.060 of the Land Use Code with slight deviations to primary entrances, preventing blank walls, architectural features, window transparency, building mass, materials, and color choices to account for the specific lot configuration, which includes three street frontages. Proposed deviations demonstrate conformance to approval criteria for subdivision (Chapter 17.21) and/or site design (Chapter 17.09) and will promote greater variety and innovation in multi-family residential design resulting in housing opportunities for individuals of varying income levels.
- **Parking Standards.** The Preliminary PUD Plan submittal includes a Parking Study as required by Council at the time of Concept Plan approval. Based on the findings of the Study and connectivity from Geode Flats to the greater multimodal network, surface parking will be provided at 1.32 spaces per dwelling unit for market rate rental and .075 spaces per affordable dwelling unit. The PUD Guide includes parking management provisions to be included in the individual apartment leases, specifying the parking requirements and enforcement policies. Additionally, garages and other parking areas are required to be oriented away from neighborhood streets or provide a 6-foot landscape buffer between the street and parking areas to implement the residential and subdivision principles. If the property developed non-residential, off-street parking is required to be provided in accordance with Chapter 17.37 Parking Standards for commercial development.
- **Landscape Standards.** A minimum of 10 percent (10%) of the land area will be desert design-inspired xeriscape landscaping. The PUD Guide requires drought-tolerant plant species provided in conformance with Section 17.11.050 Landscape Standards of the Land Use Code.

**Project compliance with, compatibility with and impacts on:**

**Fruita in Motion: Plan like a Local 2020 Comprehensive Plan:** Adding diversity in Fruita's housing stock is a pervasive theme throughout the 2020 Fruita in Motion that is grounded in the City's community values with the commitment to a land use pattern and supporting policies that promote access to housing across the income spectrum of its residents. The following is an overview of the concepts for which the Residences at Fruita PUD further the goals of the Comprehensive Plan:

- Chapter 3 Land Use + Growth, Goal #2. The proposal represents an infill development opportunity that takes advantage of existing nearby roads and James M. Robb Colorado River State Park.



- Chapter 3 Land Use + Growth Goal #4. The proposed multifamily residential development provides a diversity of housing types to fit the needs of the Fruita community and will provide the diverse “funky” character that is treasured by residents.
- Chapter 4 Economic Development, Economic Vitality. The addition of either market rate and affordable rental housing apartments to the City’s housing stock increases Fruita residents’ housing choices close to jobs to support the City’s labor force.
- Chapter 4 Economic Development, Workforce. The project increases the availability and cost of suitable rental housing that would help alleviate local businesses’ challenges related to the shortage of workers and residents’ experiences at local businesses.
- Chapter 4 Economic Development Goal 9. Approval and implementation of a Planned Unit Development would support flexibility in zoning and development of diverse housing types as part of an economic sustainability strategy.
- Education, Arts, and Historic Preservation Goal #4, Policy 4.D. The Preliminary PUD Plan includes a Childcare Center as the community component to provide educationally enriching opportunities by supporting affordable childcare centers for city residents.

**Land use in surrounding area including parks and open space.** Goede Flats is located directly west of the La Quinta Inn within an existing commercial area with access to trails and transportation resources. It is in close proximity to the city owned Fruita Lagoon redevelopment site that will include a combination of open space and recreation opportunities. Surrounding land uses include the Dinosaur Journey Museum and the Colorado State Patrol office to the south, and truck dealerships and a gas station to the north. James M. Robb State Park is located to the east and offers future residents access to camping and outdoor recreation. All of the surrounding land uses and nearby amenities provide excellent recreation and employment opportunities for future Geode Flats residents.

**Site access and traffic patterns.** All project traffic would access the site via SH 340 by Raptor Road or Jurassic Court. Both streets will be completed to City standards as they are partially built out at this time. Additionally, the project will include the build-out of a collector street along the western property boundary to connect Raptor Road and Jurassic Court, aligning with the City's long-range transportation goals.

**Availability of utilities.** All utilities are readily available. Water service is provided by the Ute Water Conservancy District. The City of Fruita provides sewer and stormwater. Power and gas are provided by Xcel Energy. No special or unusual demand on utilities is anticipated. The landscape standards include desert-inspired landscaping that would reduce water demand.

**Effects on public facilities and services.** The subject property can be adequately served by City water, sanitation, storm drainage facilities, and police protection. The property is located with the Lower Valley Fire District and the Mesa County School District No. 51. At time of Site Design Review, it is anticipated that payment of the school land dedication in an amount per unit, based upon the increased number of dwelling units. Depending on the on-site amenities proposed at time of Site Design review, a waiver from the Parks, Health, Recreation, Open Space and Trails impact fee or partial payment toward the impact fee could be requested for City Council’s consideration.

**Site soils and geology.** The project site is vegetated with sparse grass and underlain by low strength, compressible sands and clays. The site slopes down from the east towards the west/southwest approximately four (4) feet. A Geotechnical Report will be submitted at the time of Site Design Review.

### **Development Schedule and Phase/Filing Plan**

TWG intends to submit the Final PUD application and Major Site Design review upon Preliminary PUD approval. Development of the property is proposed in a single phase.

### **Credits Against Impact Fees**

Raptor Road and Jurassic Court will be completed to City standards as they are partially built out at this time. Fruita in Motion delineates a collector road between Raptor Road and Jurassic Court. As such, the project will include the build-out of a collector street between Raptor Road and Jurassic Court per the city's street standards. As such, TWG respectfully requests 100 percent of the construction costs of the collector street connection be credited against the Transportation Impact fee.

### **Consistency with Concept Plan**

The Fruita Planning Commission and City Council conducted a joint workshop on January 24, 2023. Below is an overview how the Preliminary PUD complies with all conditions of the approval on the Concept Plan:

- The proposed Preliminary PUD includes Council's preferred public benefit of providing for childcare versus parks and trails since parks and trails already have several revenue sources. A cash in lieu of a childcare land dedication is proposed to benefit the community as a whole.
- A Parking Demand Study is included in the Preliminary PUD submittal to support the parking rates proposed for market and affordable housing.
- The building design standards set forth requirements for architecture that have been adapted to fit Fruita's unique location/historical context (Colorado National Monument/Grand Valley and the desert environment. Additionally, proposed building materials will complement the beauty of the State Patrol building with the allowance of exterior materials such as brick, stone, adobe, wood shingle or imitation wood singles wall, slump block or fiber cement panel, lap siding, vinyl siding, adobe brick, or suitable split block or brick.
- The intent of the PUD is to allow flexibility and innovation in the development of rental market rate or affordable multifamily housing; however, TWG's primary goal is to construction of affordable housing in Fruita who earn at and below 140% of the area median income (AMI) to fulfill the need for affordable housing in Fruita.
- Since City Council was not concerned about density due to Fruita's housing crisis, the PUD does not include a limitation on density.

**Enclosed:** Neighborhood meeting sign in sheet



# WELCOME AND PLEASE SIGN IN

Neighborhood Meeting – September 6, 2023 at 5:30 p.m.

Residences at Fruita Preliminary PUD

NAME	EMAIL	PHONE	ORGANIZATION [if Any]
Chris Bennetts	papa.bennetts@yahoo.com	970-250-9950	

Thank you!

# GEODE FLATS

City of Fruita, Colorado

## PRELIMINARY PUD GUIDE

November 29, 2023

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### WRITTEN RESTRICTIONS

#### **Intent**

The purpose of this Planned Unit Development is to allow flexibility and innovation in the development of market rate or workforce/affordable multifamily housing in exchange for a community benefit that could not otherwise be realized through the strict adherence to the Fruita Land Use Code. This PUD Guide will serve as the zone district regulations for the PUD. This PUD Guide is a site-specific development plan and creates vested rights pursuant to Article 68 of Title 24, Colorado Revised Statutes, as amended.

#### **Underlying Zoning**

Commercial Two (C-2)

#### **Allowed Land Uses**

All uses permitted by right and conditional use in the C-2 zone district, including but not limited to:

- Multifamily Residential
- Clubhouse

#### **On Site Amenities**

On site amenities shall include at least two of the following elements:

- Playground equipment
- Common outdoor gathering spaces
- Pocket Park
- Raised garden beds
- Trails
- Furnished exercise room
- Community room
- Computer room

#### **Community Component**

A payment of \$100,000 is due upon issuance of building permits in lieu of dedicated land for a Childcare Center to be provided off-site.

#### **Workforce/Affordable Housing**

Multifamily development may be either market rate or affordable housing.

**Overall Site Dimensional Standards**

Density: No specific standard  
 Maximum Number of Buildings: 5  
 Minimum Landscape Area: 10%

**Planning Area A**

Maximum number of Buildings: 3

**Planning Area B**

Maximum number of Buildings: 2

**Density and Dimensional Standards Table**

Use	Max Density	Min Lot Area	Min Front/Street Yard	Min Interior Side Yard	Min Rear Yard	Max Structure Height	Max Lot Coverage
Multi-family	12 DU/acre Minimum  <b>Deviation:</b> No Specific Standard  Minimum Unit Size: 500 sf	5,000 sq ft per dwelling unit  <b>Deviation:</b> 4.5 Acres	0 feet  <b>Deviation:</b> PA A: 14' PA B 14'	10 feet for primary structures  5 feet for accessory structures except 0 feet where common wall or zero-lot line dev. Allowed  <b>Deviation:</b> 14 feet for primary structures	20 for primary and accessory structures  except 0 feet where common wall or zero-lot line dev. Allowed  <b>Deviation:</b> Not Applicable	35 feet for primary structures  25 feet for accessory structures  <b>Deviation:</b> PA A: 4 Stories PA B: 3Stories  Elevator overrun and mechanical equipment exempt from maximum building height  See Figure 1.1 B for building height measurement	80%  <b>Deviation:</b> PA A: 35.3% PA B: 24.4%  See Figure 1.1 for lot coverage diagram.



Use	Max Density	Min Lot Area	Min Front/Street Yard	Min Interior Side Yard	Min Rear Yard	Max Structure Height	Max Lot Coverage
Non-Residential Development	Not Applicable	5,000 sq ft	0 Feet	10 feet for primary structures  5 feet for accessory structures except 0 feet where common wall or zero-lot line dev. allowed	20 feet for primary structures  5 feet for accessory structures except 0 feet where common wall or zero-lot line dev. Allowed	35 feet for primary structures  25 feet for accessory structures	80%

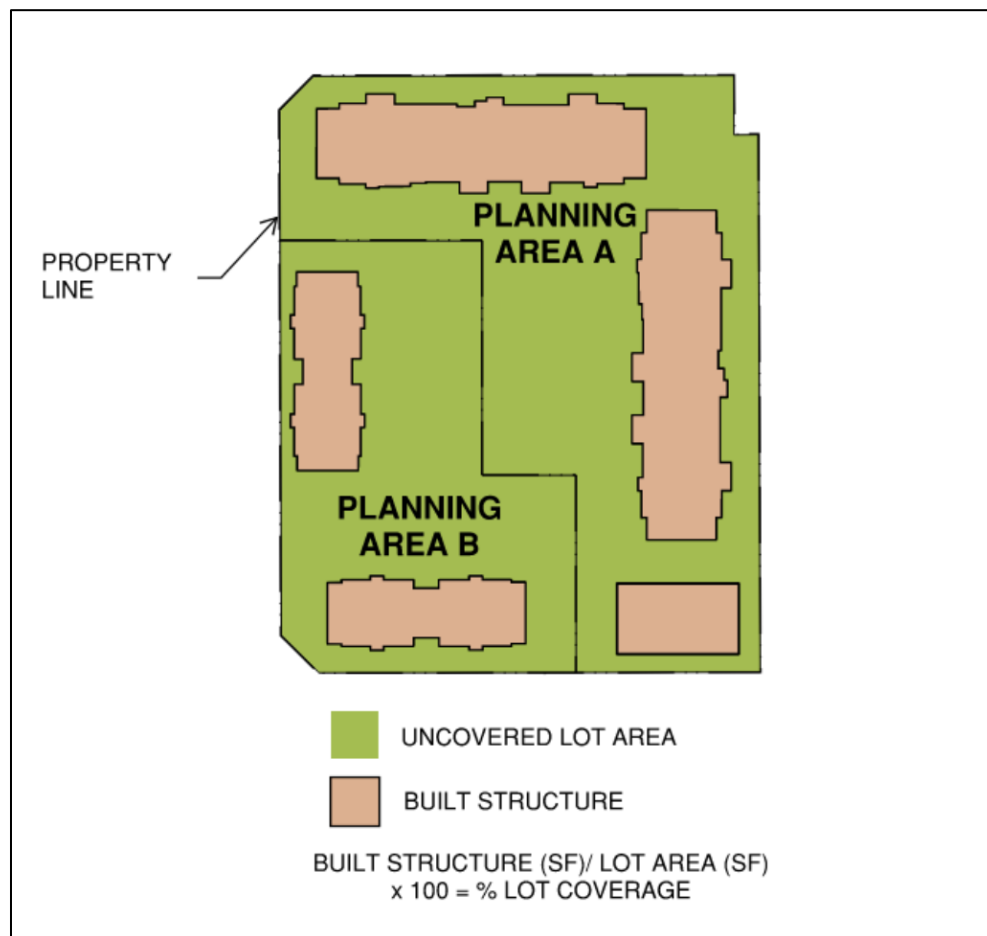
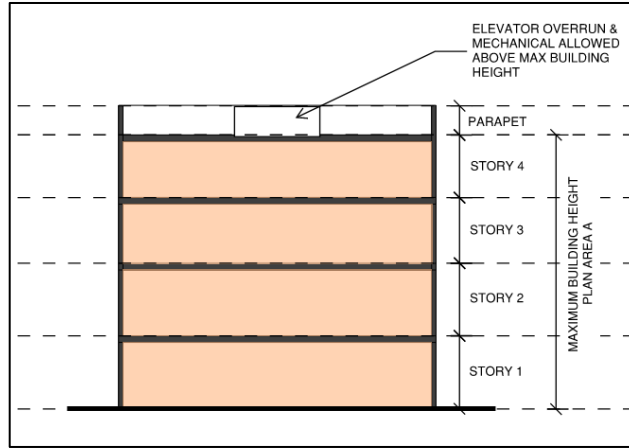
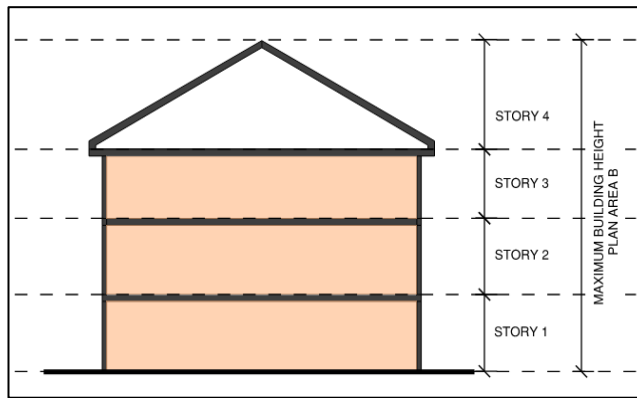


Figure 1.1: Lot Coverage Diagram



**Figure 1.1 A: Plan Area A Height**



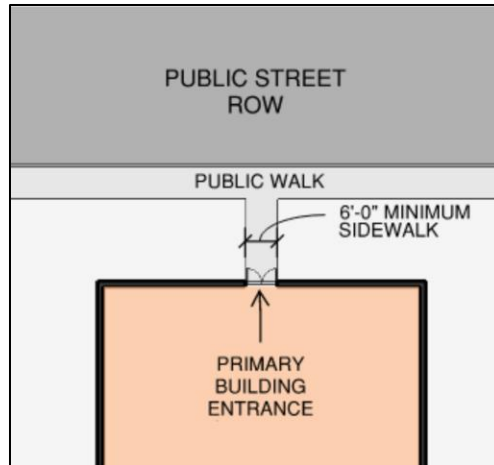
**Figure 1.1 B: Plan Area B Height**

**SITE DESIGN**

**Primary Entrances**

- a. Primary building entrances shall be oriented to the public street right-of-way and/or public sidewalk and shall be connected to the public street right-of-way and/or public sidewalk by a concrete walkway not less than six (6) feet in width. Primary building entrances shall be within twenty (20) feet of the public street right-of-way and/or public sidewalk.

**Proposed Deviation:** When a building has street frontage the building should incorporate one primary entrance that faces the street and/or public sidewalk and shall be connected to the public street right-of-way and/or public sidewalk by a concrete walkway not less than six (6) feet in width. See Figure 1.2 below.



**Figure 1.2: Concrete Walkway Requirement**

- b. Where it is not practical to locate primary building entrances within twenty (20) feet of the public street right-of-way or public sidewalk, the concrete walkway connecting primary building entrances to the public sidewalk or public street right-of-way shall be no less than ten (10) feet in width. This concrete walkway must have three-foot wide planter strips on each side.

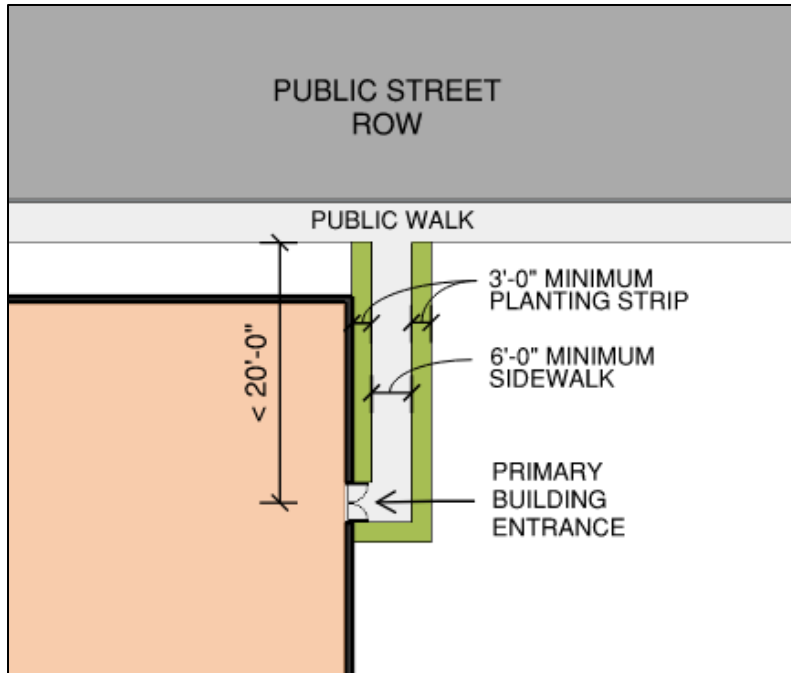
**Proposed Deviation:** Where it is not practical to locate primary building entrances within twenty (20) feet of the public street right-of-way or public sidewalk, the concrete walkway connecting primary building entrances to the public sidewalk or public street right-of-way shall be no less than six (6) feet in width. This concrete walkway must have three-foot wide planter strips on each side.

- ~~c. Primary building entrances located more than forty (40) feet from the public street right-of-way or public sidewalk will require a pedestrian plaza outdoor seating area, courtyard, or other civic amenity is provided between the building and street.~~

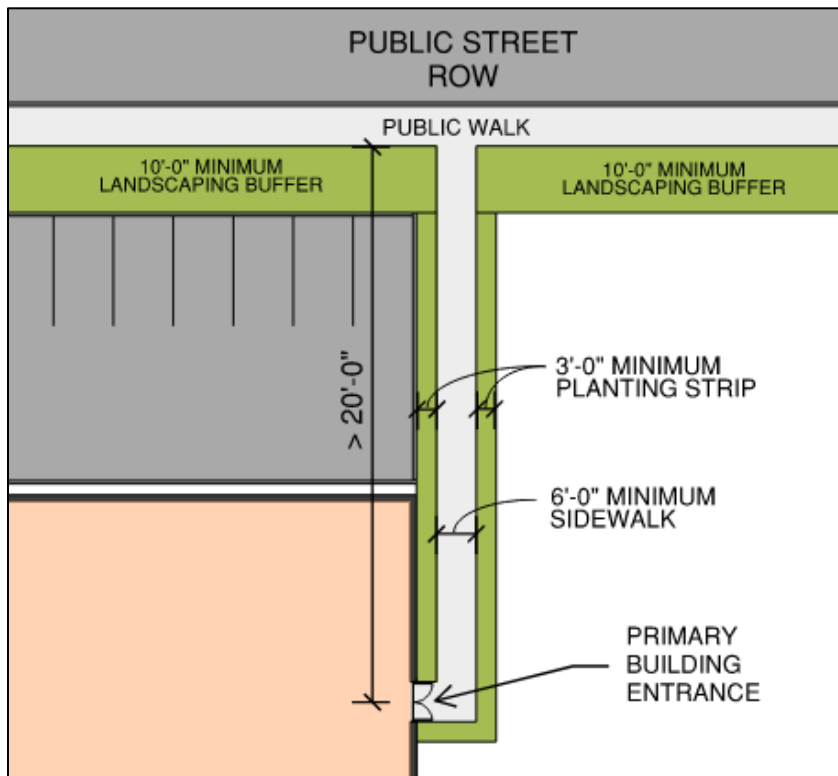
**Proposed Deviation:** Strike completely as the lot is bounded by three street frontages.

- d. Where a primary building entrance is located more than twenty (20) feet from a public street right-of-way and/or public sidewalk, or where parking and/or driving aisles are provided between the primary building entrance and public street right-of-way and/or public sidewalk, a fifteen (15) foot wide minimum landscape screen shall separate all off-street parking areas from adjacent public street rights-of-way or public sidewalks.

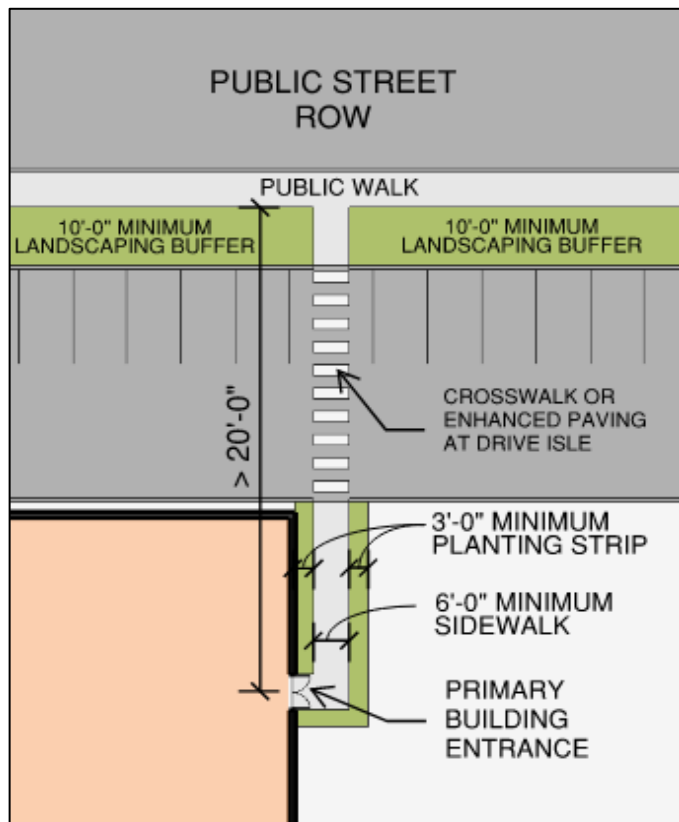
**Proposed Deviation:** Where no building entrance is located within a (20) feet of a public street right-of-way and/or public sidewalk, a ten (10) foot wide minimum landscape screen shall separate all off-street parking areas from adjacent public street rights-of-way or public sidewalks. See Figures 1.3a, 1.3b, 1.3c below.



**Figure 1.3a: Landscape Screen Requirement**



**Figure 1.3b: Landscape Screen Requirement**



**Figure 1.3c: Landscape Screen Requirement**

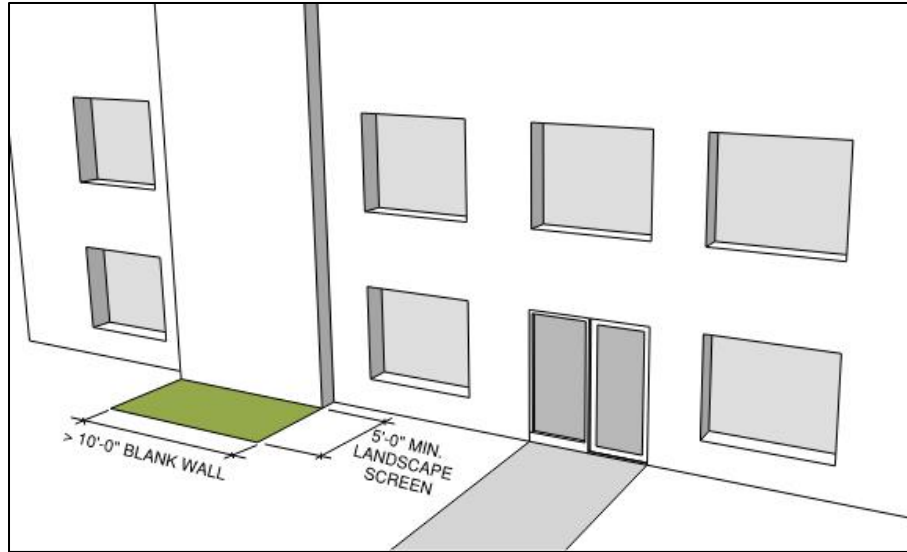
- e. Buildings shall have clearly defined primary entrances that provide a weather-protection shelter for a depth of not less than five (5) feet (e.g., either by recess, overhang, canopy, portico and/or awning) extending from the building entry.

**Proposed Deviation:** None

**Blank Wall Prevention**

- a. Buildings shall meet transparency and weather protection standards (Subsection B, Building Design below) along all street-facing elevations and any elevations containing a primary building entrance. A landscape screen at least five (5) feet wide shall cover any blank building walls (i.e., lacking windows and weather protection) and contain materials of sufficient size/species to screen the blank wall.

**Proposed Deviation:** Buildings elevations that are public street-facing shall meet transparency and weather protection standards (Subsection B, Building Design below) along all street-facing elevations. A landscape screen at least five (5) feet wide shall cover any blank building walls over 10'-0" in length (i.e., lacking windows and weather protection) and contain materials of sufficient size/species to screen the blank wall. See Figure 1.4 below.



**Figure 1.4: Blank Wall Landscape Screen Requirement**

- b. Public art and murals may be used to minimize the visual impacts of a blank façade.

**Proposed Deviation:** None.

### **Building Design**

#### Overall Design.

- a. Architectural designs shall address all facades of a building visible from the street with materials, detailing, and color. Architectural elements should wrap around building corners. Where a proposed design is based on the applicant’s corporate style guide, as in formula retail stores, restaurants, discount outlets, or similar proposals where a similar building design has been used previously, the applicant must demonstrate that the design has been adapted to fit Fruita’s unique location/historical context (Colorado National Monument/Grand Valley) and desert environment.

**Proposed Deviation:** Architectural designs shall address all street-facing facades of a building with materials, detailing, and color. Architectural elements should wrap around building corners. Where a proposed design is based on the applicant’s corporate style guide, as in formula retail stores, restaurants, discount outlets, or similar proposals where a similar building design has been used previously, the applicant must demonstrate that the design has been adapted to fit Fruita’s unique location/historical context (Colorado National Monument/Grand Valley) and desert environment.

#### Stepped Rooflines.

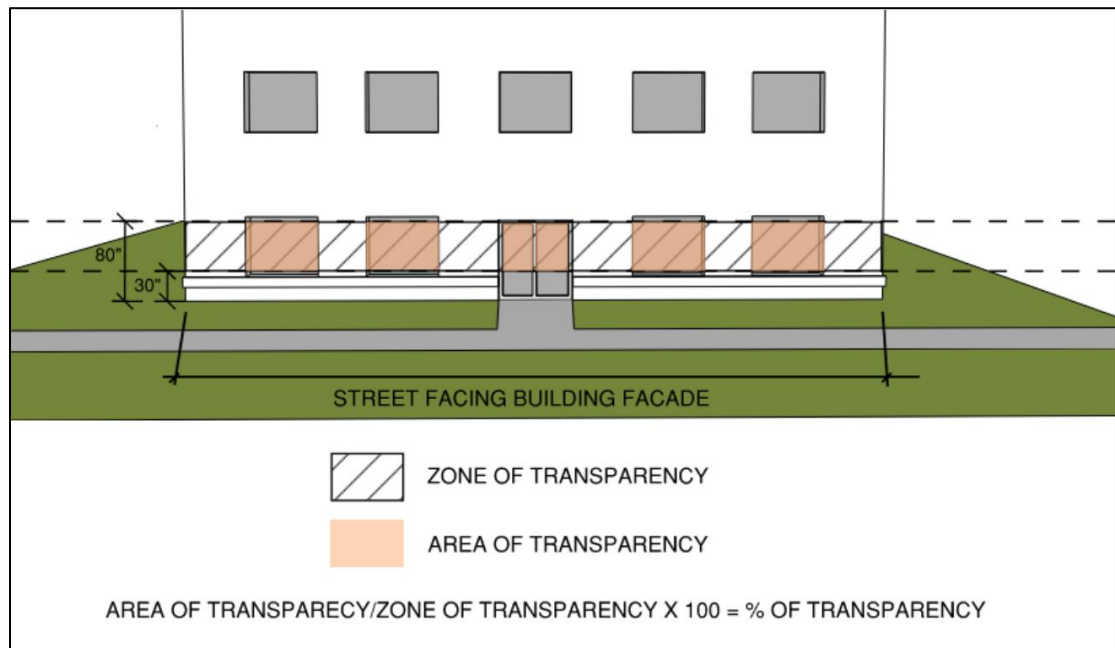
- a. Height should vary from building to building to avoid a homogenous appearance. This standard is met by using stepped parapets, gables, or slightly dissimilar height from building-to-building.

**Proposed Deviation:** Rooflines should vary to avoid a homogenous appearance. This standard is met by using stepped parapets, gables, changes in roof slope, or slightly dissimilar heights.

Window Transparency.

- a. Building elevations that face a street, parking area, civic space, or open space shall comprise at least forty (40) percent transparent windows, measured as a section extending the width of the street-facing elevation between the building base (or thirty (30) inches above the sidewalk grade, whichever is less) and a plane eighty (80) inches above the sidewalk grade. Upper floors may have less window area but should follow the vertical lines of the lower level piers and the horizontal definition of spandrels and any cornices.

**Proposed Deviation:** Building elevations that face a street or civic space, shall comprise at least thirty (30) percent transparent windows, measured as a section extending the width of the street-facing elevation between the building base (or thirty (30) inches above the sidewalk grade, whichever is less) and a plane eighty (80) inches above the sidewalk grade. Upper floors may have less window area but should follow the vertical lines of the lower-level piers and the horizontal definition of spandrels and any cornices. See Figure 1.5 below.



**Figure 1.5: Transparency Requirement**

- b. Where the Community Development Director determines, based on physical site constraints or the functional requirements of a non-residential building, that providing window transparency is not practical or does not further intent of these standards as stated above, other alternative means of breaking up large elevations (e.g., columns, belt course, and upper story panels/transom, with landscaping) shall be employed.

**Proposed Deviation:** None.

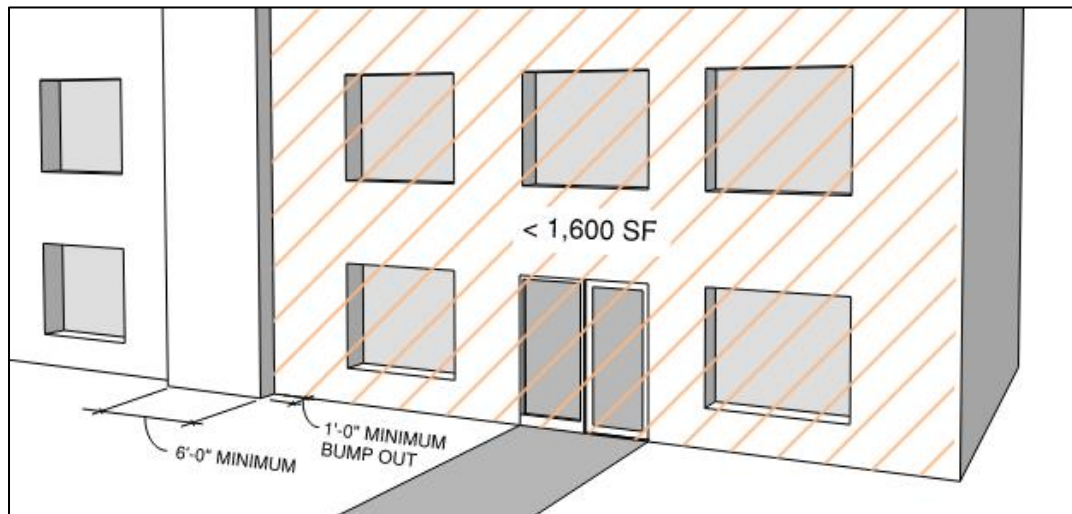
Building Mass.

- a. Building elevations shall incorporate offsets or divisions to reduce the apparent building scale and to improve aesthetics. Elevations of a structure shall be divided into smaller areas or planes to minimize the appearance of bulk as viewed from any street, civic space, or adjacent property.

**Proposed Deviation:** None.

- b. When an elevation of a primary structure is more than eight hundred (800) square feet in area, the elevation must be divided into distinct planes of not more than eight hundred (800) square feet. For the purpose of this standard, areas of wall planes that are entirely separated from other wall planes are those that result in a change in plane such as a recessed or projecting section of the structure that projects or recedes at least one (1) foot from the adjacent plane, for a length of at least six (6) feet.

Proposed Deviation: When an elevation of a primary structure is more than ~~eight hundred (800) square~~ twenty-five hundred (2,500) square feet in area, the elevation must be divided into distinct planes of not more than (1,600) square feet. For the purpose of this standard, areas of wall planes that are entirely separated from other wall planes are those that result in a change in plane such as a recessed or projecting section of the structure that projects or recedes at least one (1) foot from the adjacent plane, for a length of at least six (6) feet. See Figure 1.6 below.



**Figure 1.6: Blank Wall Landscape Screen Requirement**

- c. Changes in plane may include but are not limited to recessed entries, bays, stepped parapets, secondary roof forms (e.g., gables, lower roof sheds, dormers and towers), building bases, canopies, awnings, projections, recesses, alcoves, pergolas, porticos, roof overhangs, columns, or other features that are consistent with the overall composition of the building.



**Proposed Deviation:** None.

- d. The distinction between street level and upper floors shall be established, for example, through the use of awnings, canopies, belt course, or similar detailing, materials and/or fenestration.

**Proposed Deviation:** None.

#### Materials and Colors.

- a. Exterior materials shall consist of brick, stone, adobe, wood shingle or imitation wood shingle walls, slump block, adobe brick or suitable split block or brick.

**Proposed Deviation:** Exterior materials shall consist of brick, stone, adobe, wood shingle or imitation wood shingle walls, slump block or fiber cement panel, lap siding, vinyl siding, adobe brick, or suitable split block or brick.

- b. Wood timbers and metal (brushed steel, iron, copper, or similar architectural-grade metals) may be used on canopies, arbors, trellises, pergolas, porticos, brackets, fasteners, lighting, signage, and other detailing, as appropriate, to provide visual interest and contrast. In general, color selection should complement, not compete with, the surrounding desert landscape. Warm earth tone colors (e.g., sandstone reds, desert greens and browns) are generally preferred over cool colors, such as blue and white/off-white. Substitute materials that are equal in appearance and durability may be approved.

**Proposed Deviation:** None.

#### ATMs and Service Windows.

- a. Where walkup ATMs or service windows are proposed on any street-facing elevation, they shall be visible from the street for security and have a canopy, awning, or other weather protection shelter. Where drive-up windows or similar facilities are provided the drive-up window and associated vehicle queuing area shall be set back at least twenty (20) feet from all adjacent rights-of-way. The applicant may be required to install textured pavement (e.g., pavers or stamped concrete) for pedestrian crossings of any drive aisle.

**Proposed Deviation:** None.

### **PARKING STANDARDS**

Off street parking to be provided in accordance with Chapter 17.37 Parking Standards for commercial development.

The following parking rates shall apply to multifamily development.

	Per Dwelling Unit	
	Market Rate Rental	Affordable Rental
Parking Rate	1.32	0.75
180 Units	238 Spaces	135 spaces

- a. Garages and other parking areas are setback and oriented away from neighborhood streets or a 6-foot landscape buffer shall be provided between the street and parking areas.
- b. Parking management included in the individual apartment leases, specifying the parking requirements and enforcement policies.
- c. Bicycle parking provided in accordance with Chapter 17.37 Parking Standards of the Land Use Code.

**LANDSCAPE STANDARDS**

Site development to employ desert design-inspired xeriscape landscaping. A minimum of 10 percent (10%) of the land area shall be landscaped with drought-tolerant plant species provided in conformance with Section 17.11.050 Landscape Standards of the Land Use Code.

**SIGNAGE**

All signs shall comply with the City’s Sign Code, unless the developer chooses to submit a master sign program application for consideration by the City.

**LIGHTING**

All exterior lighting shall be in conformance with the lighting standards of the Land Use Code.

**CONFLICTS**

In the event a standard in this PUD Guide is in conflict, the standard in this PUD Guide shall govern.

**City of Fruita Planning Department**  
325 E. Aspen Avenue  
Fruita, CO 81521

November 29, 2023

Re: **Geode Flats**  
**Parking Analysis Study**  
**Fruita, Colorado**

**Purpose:**

This memorandum was developed to give a parking rate recommendation for the proposed Geode Flats apartment project. The Geode Flats is located at 614 Raptor Road in Fruita, Colorado. This is a 5-acre proposed workforce/affordable housing project. The specifics of the workforce/affordable housing component are still being detailed. Therefore, the parking analysis includes alternative scenarios for a workforce housing project and an affordable housing project.

The proposed project is currently proposed to include 180 dwelling units in a mix of three and four-story residential apartment buildings.

**Residential Parking Analysis:**

The parking required to accommodate the proposed Geode Flats residents and guests was taken from national rates in general urban/suburban multifamily complexes throughout the United States. Alternatives for both market rate apartments and affordable apartments are included in this analysis.

**Market Rate Analysis:**

The Institute of Transportation Engineers' *Parking Generation Manual, 5<sup>th</sup> Edition*<sup>1</sup> provides parking generation rates based upon numerous parking studies accumulated across the United States. The data provided is based upon observations for apartment complexes in a general urban/suburban setting that is not located within ½ mile of rail transit.

The Institute of Transportation Engineers' *Parking Generation Manual*<sup>1</sup> states that the average peak period parking demand for a suburban mid-rise (3+ floor/story) apartment (Land Use #221) is 1.32 parking spaces per occupied dwelling unit. The Geode Flats development is anticipated to be 100% occupied.

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<sup>1</sup> *Parking Generation Manual, 5<sup>th</sup> Edition*, Institute of Transportation Engineers, 2019

The statistic is given based upon the 95<sup>th</sup> Percent Confidence Interval for a nationwide study of 48 apartment complexes. The 95<sup>th</sup> Percent Confidence Interval indicates that there is a 95% likelihood that the parking demand will fall within 1.25 to 1.39 parking spaces per occupied dwelling unit.

**Affordable Housing Analysis:**

The Institute of Transportation Engineers' *Parking Generation Manual*<sup>1</sup> gives national parking demand data for affordable multifamily housing. This includes housing complexes where 75% or more of the units are designated as affordable and rented at below market rate. The *Parking Generation Manual*<sup>1</sup> states that the average peak period parking demand for per affordable dwelling unit (Land Use #223) is 0.78 parking spaces per occupied dwelling unit. The statistic is given based upon the 95<sup>th</sup> Percent Confidence Interval for a nationwide study of 6 apartment complexes. The Geode Flats development is anticipated to be 100% occupied.

However, the *Parking Generation Manual*<sup>1</sup> also states that the manual 'should be considered only the beginning point of information to be used in estimating parking demand. Local conditions and area type can influence parking demand... Therefore, a survey of a site in a comparable local condition should always be considered as one potential means to estimate parking demand.'<sup>1</sup>

Therefore, local data sources were reviewed. A Colorado-specific document, *Parking & Affordable Housing 2020/2021 Report*<sup>2</sup>, provided a detailed analysis of 19 affordable housing developments and determined that across the board the affordable housing facilities are overparked and requirements can be up to five times the need. A parking rate range from 0.36 to 1.10 parking spaces per unit is recommended. The average of this study is a rate of 0.73 parking spaces per affordable housing unit.

Averaging the two rates, results in a recommended parking rate of 0.75 parking spaces per occupied affordable dwelling unit.

**Table 1** summarizes the parking rate analysis per dwelling unit.

**Table 1: Parking Rate Summary**

	Per Occupied Dwelling Unit	
	Market Rate Rental <sup>1</sup>	Affordable Rental <sup>1,2</sup>
Number of Occupied Dwelling Units	180	180
Parking Rate	1.32	0.75
Required Parking Spaces	238	135



**City of Fruita’s Market Rate Analysis per Bedroom:**

The City requested additional parking analysis utilizing the parking rates per number of bedrooms as defined by the City of Fruita’s updated *Chapter 17.37 Parking Standards* of the *Land Use Code*<sup>3</sup>. The exact number of bedroom units is unknown at this time. Therefore, this analysis is a hypothetical look at an assumed unit mix. This calculation was performed for both market rate and affordable housing alternatives.

**Table 2** summarizes the parking rate analysis using an assumed unit mix of bedrooms.

**Table 2: Parking Rate Summary per Bedroom (Hypothetical Unit Mix)\***

	Per Bedroom (Hypothetical)							
	Market Rate Rental <sup>3</sup>				Affordable Rental <sup>3</sup>			
	1- Bed Unit	2- Bed Unit	3- Bed Unit	Add'l Parking Spaces	1- Bed Unit	2- Bed Unit	3- Bed Unit	Add'l Parking Spaces
Hypothetical Number of Bedrooms	54	134	40		54	134	40	
Parking Rate	1.00	1.5	2	1 space/ 6 du	0.50	0.75	1.25	N/A
Required Parking Spaces	54	201	80	30	27	101	50	-
Total Parking Spaces Required	365				178			

\*Uses an assumed unit mix of bedrooms.

**Multimodal Infrastructure:**

Connectivity from Geode Flats to the greater multimodal network will support the parking ratios in **Table 1**. Transit access, sidewalk connectivity, bicycle facilities, and adjacent transit access provide the ability for residents to have multiple options for safe and efficient travel in the Grand Valley.

Bicycle parking will be provided in accordance with *Chapter 17.37 Parking Standards* of the *Land Use Code*<sup>3</sup>.


**Parking Rate Summary:**

Geode Flats' anticipated unit mix has not yet been determined. Therefore, it is necessary to apply the per unit parking rates. If the project is developed for market rate residents, the site should include 238 parking spaces. If the project is an affordable housing project, the site should include 135 parking spaces.

Additionally, parking management should be included in the individual apartment leases, specifying the parking requirements and enforcement policies.

Please call if you would like any additional information or have any questions regarding this parking analysis.

Sincerely,  
McDowell Engineering, LLC



Kari J. McDowell Schroeder, PE, PTOE  
Senior Traffic Engineer

**Enclosed:** Excerpts from *Parking & Affordable Housing 2020/2021 Report*<sup>2</sup>





# Parking & Affordable Housing

## 2020/2021 Report



50% of parking  
in affordable  
housing  
projects go  
unused.

## Estimating Parking Demand

In summer/fall 2020, Fox Tuttle and Shopworks Architecture partnered to perform an audit of parking usage in affordable housing along the Front Range, with a specific focus on supportive housing to determine whether the current requirements are in line with the demand. [We found that across the board the affordable housing facilities are overparked and requirements can be up to 5 times the need, especially in lower Area Median Income \(AMI\) levels.](#) For example, for supportive housing (0-30% AMI) within the Denver metro area, the average vehicle ownership was <sup>1</sup>8.8% which equates to 1 vehicle per 12 units.

It is generally agreed that affordable housing communities, especially those serving individuals who have experienced, or are at risk of homelessness, generate significantly less parking demand than other residential uses.

Unfortunately, there is no industry standard for how to reduce typical residential parking rates for lower-income residential properties. The reduction of parking demand for affordable housing communities is due to a number of factors:

1. Their typical location is in more urban conditions with better access to transit use, and
2. The lack of funds lower-income residents have to cover the cost of vehicle ownership and insurance/maintenance.

The following pages demonstrate what we learned, and our understanding of the current demand for parking for affordable housing across the Front Range.

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<sup>1</sup> In our study 32 individuals in PSH had cars out of a total of 365 units.



## Impact of Proximity to Multi-Modal Facilities

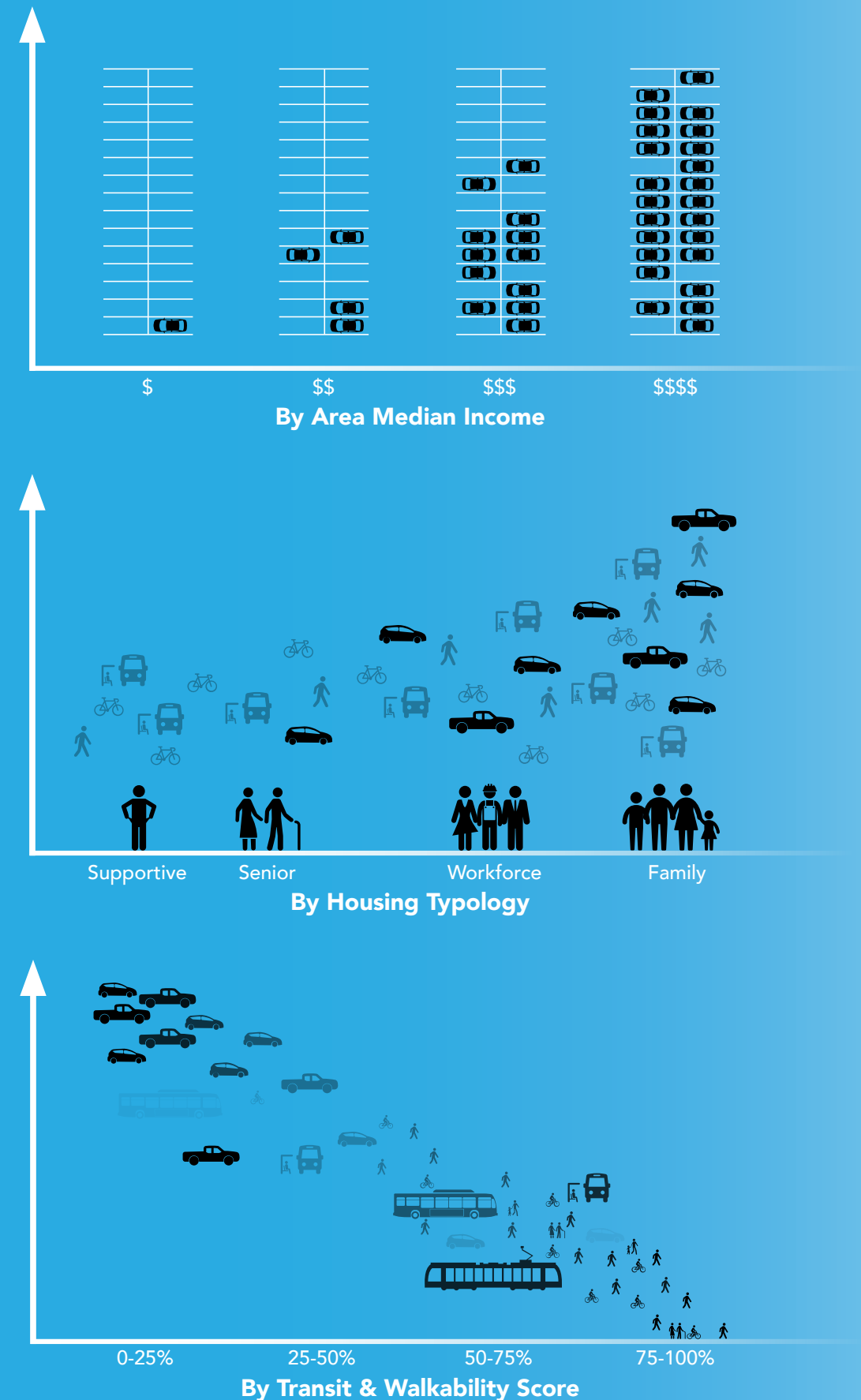
The walk, bike, and transit scores were added together to get the total non-auto score for each property. The data indicates that the property's proximity to quality walking and biking facilities and transit services deeply impacts vehicle ownership.

The need to own a vehicle is reduced when the surrounding neighborhood is walkable, bikeable, and has reliable transit that can get the residents to work, school, shopping, appointments, etc.

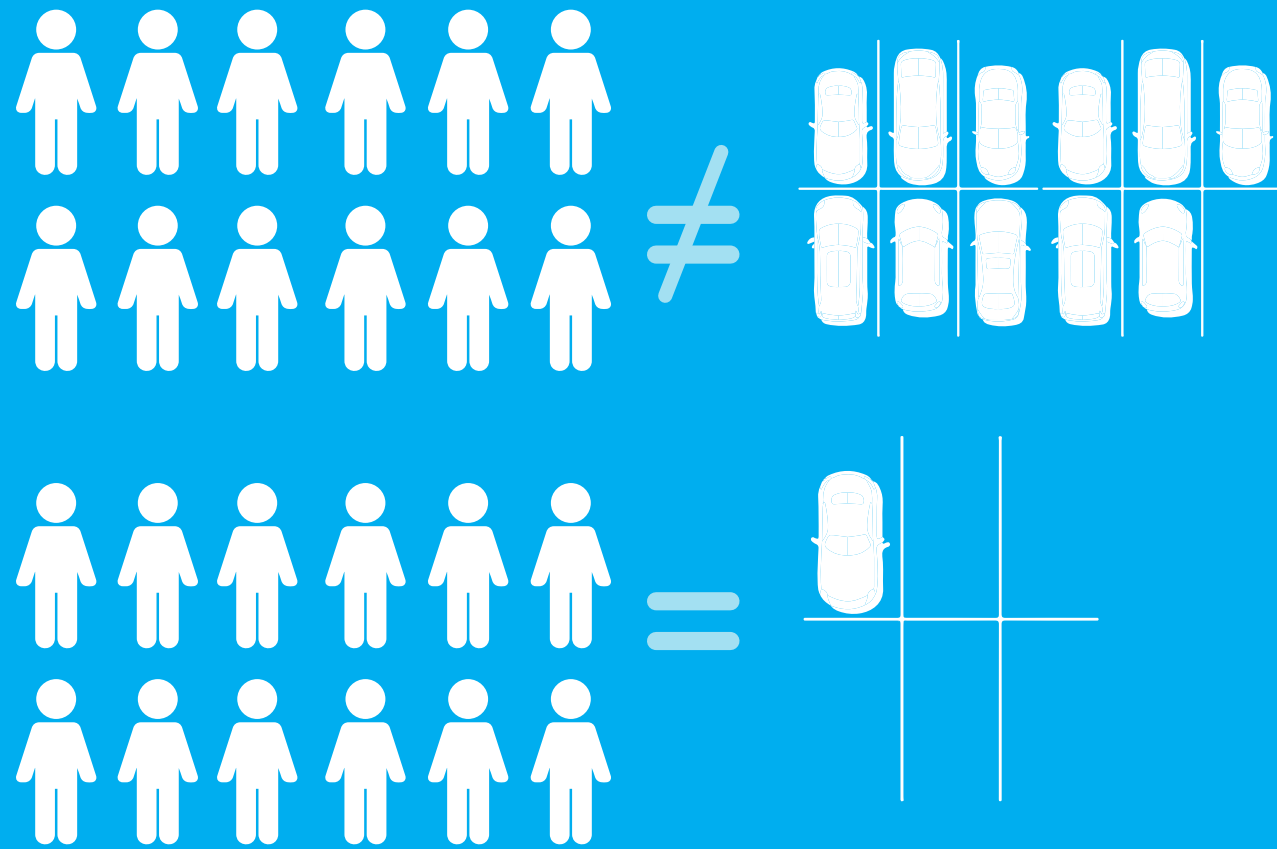
The majority of the surveyed properties were purposefully located within communities and neighborhoods that are walkable, bikeable, and near transit.

This is to serve people that do not typically own a vehicle due to the affordability, credit, maintenance, and insurance, as well as to meet the rental cost of their unit.

## Parking Needs



# Parking Supply



## vs. Demand

## Parking Supply Verses Parking Demand

Combining the parking supply of all the surveyed properties, there are 883 parking spaces for 1,353 affordable housing units with an approximate demand of 461 spaces. To understand the impact of over parking for affordable housing projects, the unused parking spaces were valued at \$22,000 per space. This equates to an estimated cost of \$9,460,000 on parking that is in excess of the demand. If parking requirements for affordable projects were lowered closer to the parking demand, then the funds could be reallocated for support services or providing transportation options. The physical space could be repurposed for additional affordable housing units or amenity space for residents.

**883** parking spaces

**1,353** units

**461** spaces

**422** unused

**\$22,000** cost per space

---

**\$9,284,000\*** funds spent on unnecessary parking

\*Figure is over 6 years and 19 projects

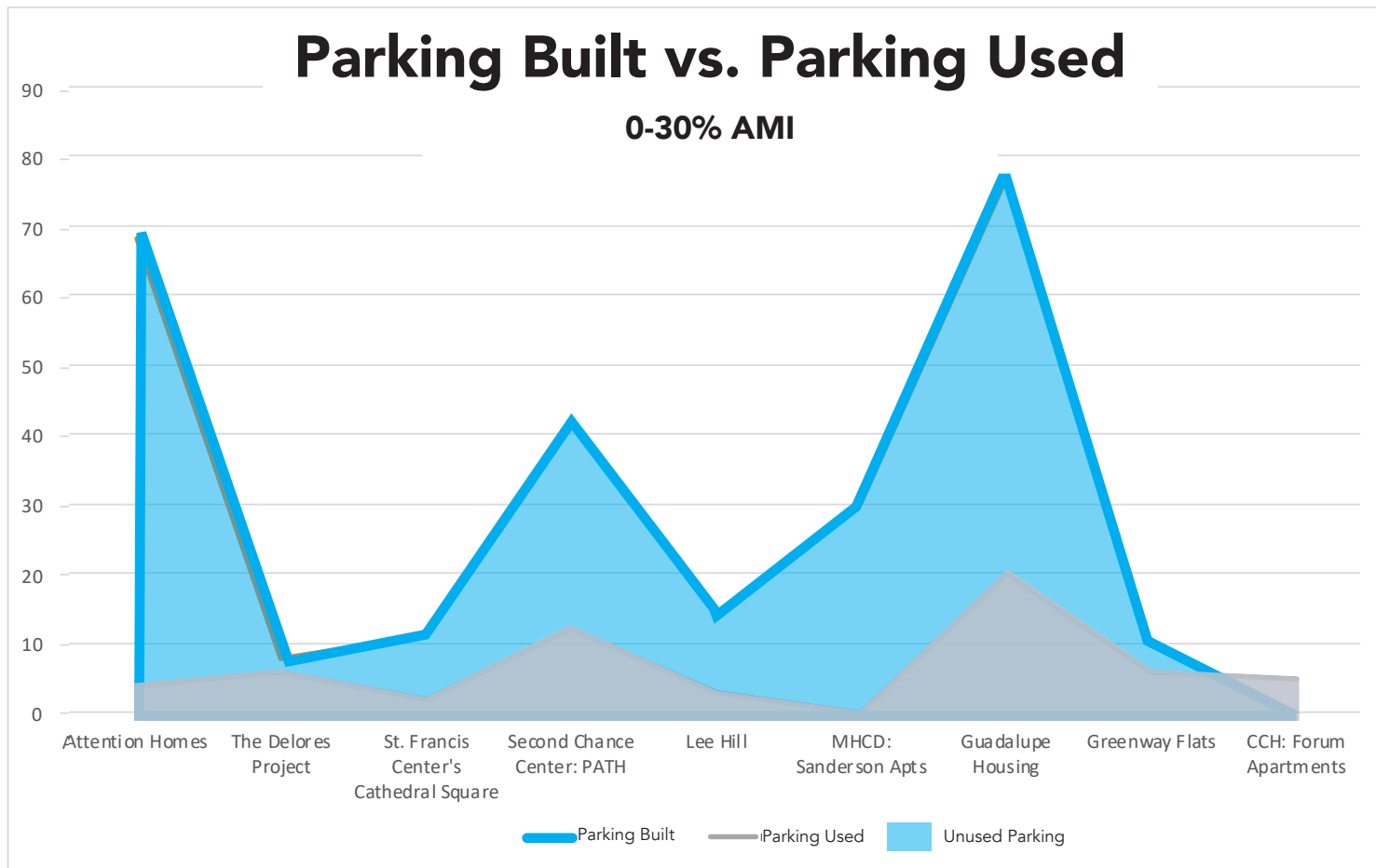
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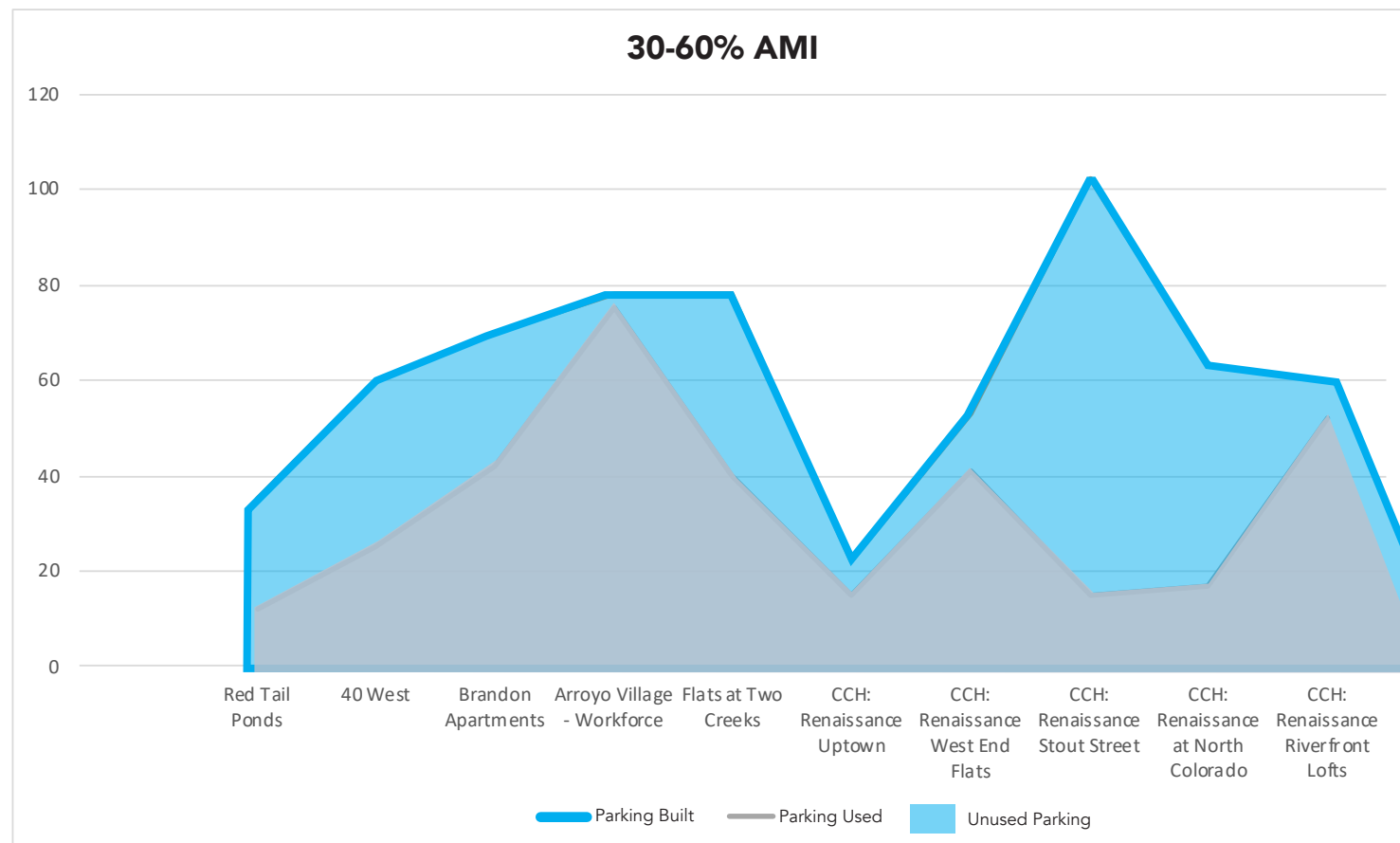
**\*Equates to one 40 unit affordable housing building.**

# Parking Built vs. Parking Used

0-30% AMI



30-60% AMI



## Vehicle Ownership

The Area Median Income (AMI) was compared to resident's vehicle ownership and it can be seen that residents with lower income levels own fewer vehicles and as the income increases the vehicle ownership increases.

The survey data provided by the twenty properties indicated the following trends:

- 29.0% of current residents own a vehicle (equates to 1 vehicle per 4 units) across all affordable housing studied.
- 8.8% of Permanent Supportive Housing Residents own a vehicle (equates to 1 vehicle per 12 units)
- On average 0.9 parking spaces per unit are provided to meet the municipal requirements.

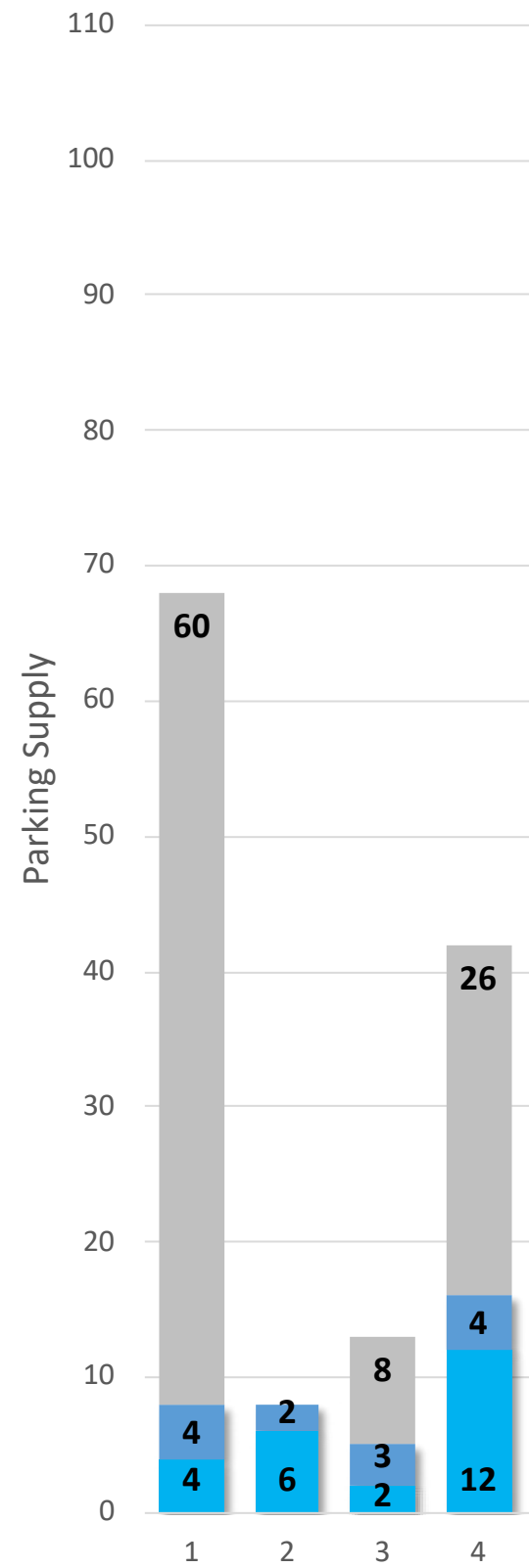
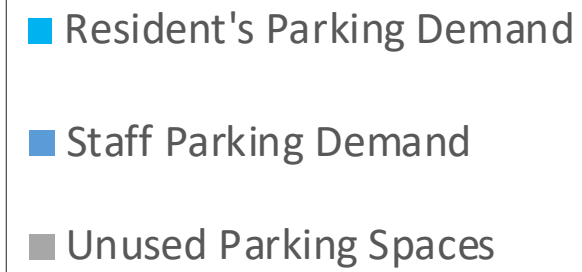
The vehicle ownership equates to the parking demand needed at each property to serve the residents and should help inform the parking supply needed.

Comparing the vehicle ownership to the parking spaces supplied indicated that affordable housing projects are overparked.

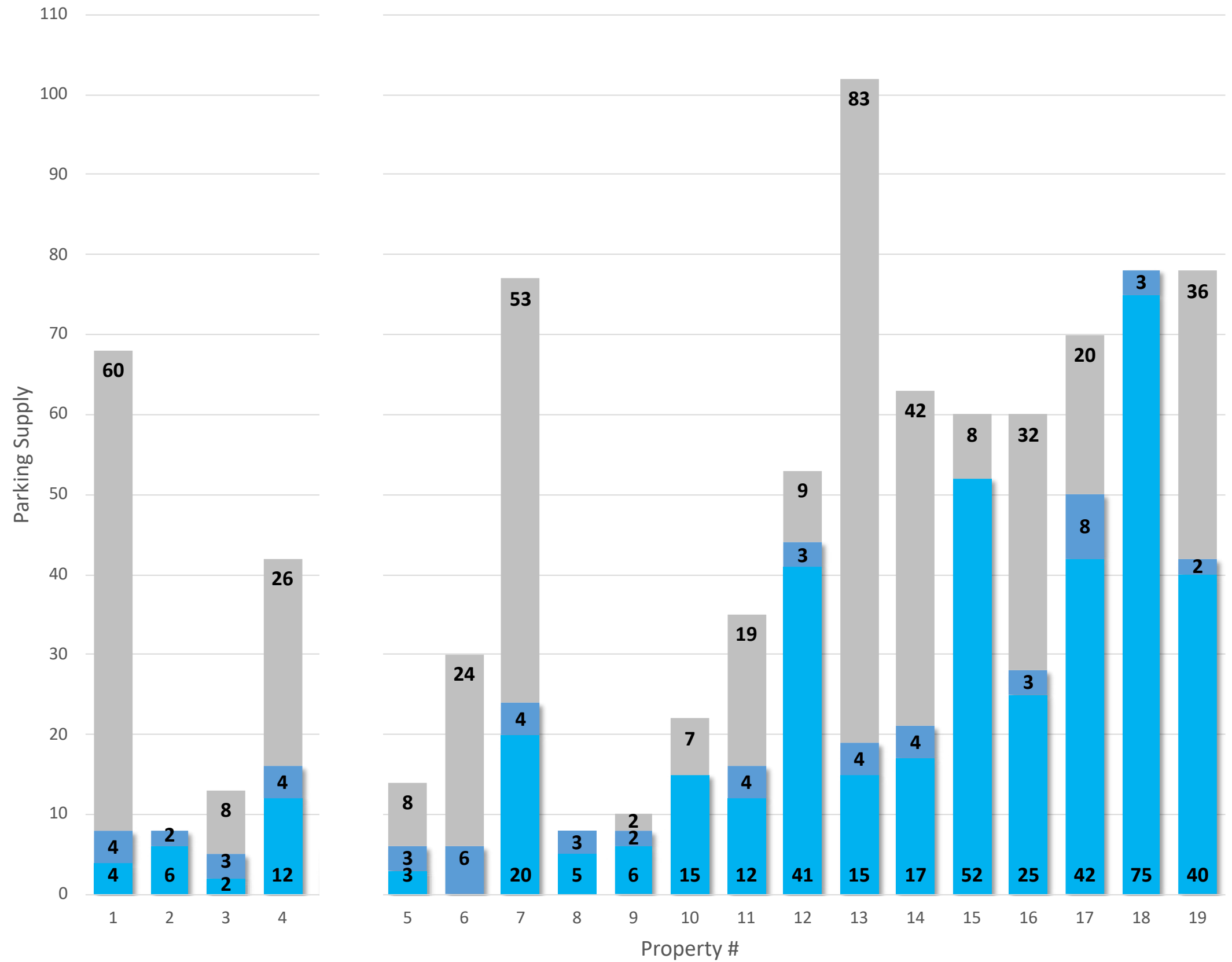
# Parking Utilization

## Property Key

#	Property	City	Denver Metro PSH	Outside Denver Metro PSH
1	Attention Homes	Boulder	X	
2	The Delores Apartments at Arroyo Village	Denver	X	
3	St. Francis Center's Cathedral Square	Denver	X	
4	Second Chance Center: PATH	Aurora	X	
5	Lee Hill	Boulder	X	
6	Mental Health Center of Denver: Sanderson Apts	Denver	X	
7	Guadalupe Apartments	Greeley		X
8	CCH: Forum Apartments	Denver	X	
9	Greenway Flats	Colo. Springs		X
10	CCH: Renaissance Uptown Lofts	Denver		
11	Red Tail Ponds	Fort Collins		
12	CCH: Renaissance West End Flats	Denver		
13	CCH: Renaissance Stout Street Lofts	Denver		
14	CCH: Renaissance at North Colorado Station	Denver		
15	CCH: Renaissance Riverfront Lofts	Denver		
16	40 West	Lakewood		
17	Brandon Apartments	Denver		
18	Arroyo Village - Workforce	Denver		
19	Flats at Two Creeks	Lakewood		



Overall Parking Demand







## RTD's 2020 Transit-Oriented Development Parking Study

In late 2019 and early 2020, Regional Transportation District (RTD) performed a parking survey of 86 properties located within a 10-minute walk to a train or bus rapid transit station. The survey included discussions with property managers, counting parking supply and utilization, and analyzing the data. Results were based on resident income; policy for including a parking space in a tenant's lease; neighborhood transit quality; property age; policy for including a transit pass in a tenant's lease; distance between the property and the station; location in the City and County of Denver or other municipality; and location in related to TOD typology.

The RTD study stated **"the most significant finding from the combined survey-count analysis ties a strong correlation between the over-supply of expensive parking and the under-utilization of parking at income-restricted properties.** Metro Denver TODs analyzed here provide approximately 40 percent more parking than residents use at peak demand." RTD intends to use their research to initiate and guide discussions amongst Metro Denver professionals and development partners about more appropriate parking provision appropriate for properties in transit-rich neighborhoods.

**"Excess parking is particularly inappropriate in transit-rich neighborhoods. Not only does it effectively prohibit affordable housing, but it unnecessarily increases development costs, reduces project savings, and obstructs access to transit, and by extension, to economic opportunity for a growing number of low-income households."**



## RTD's 2020 Transit-Oriented Development Parking Study

### RTD Parking Study Findings

Resident Income	# Of Properties	# Of Units	# Of Parking Spaces	Spaces Available Per Unit	Spaces Utilized Per Unit	Parking Utilization
All Properties	86	22,422	26,442	1.18	0.70	59%
Market-Rate	65	19,850	24,462	1.23	0.74	60%
Mixed-Income	5	985	845	0.86	0.49	57%
Income-Restricted	16	1,587	1,135	0.72	0.36	50%

The table above summarizes the data gathered and evaluated as part of the RTD study. It can be seen that the income restricted properties are over-parked, which supports the findings in the Shopworks and Fox Tuttle study.

You can find RTD's full report [here](https://www.rtd-denver.com/sites/default/files/files/2020-12/RTD-Residential-TOD-Parking-Study_Final-R_0.pdf): [https://www.rtd-denver.com/sites/default/files/files/2020-12/RTD-Residential-TOD-Parking-Study\\_Final-R\\_0.pdf](https://www.rtd-denver.com/sites/default/files/files/2020-12/RTD-Residential-TOD-Parking-Study_Final-R_0.pdf)

# RTD Report: Summary

1.



The most significant finding from the combined survey and analysis was the strong correlation between the oversupply of expensive parking and the significantly low utilization of parking at income-restricted properties.

2.



Market-rate properties provide approximately 40% more parking than residents use, and income-restricted properties provide approximately 50% more parking than residents use.

3.



Income-restricted properties (0-99% AMI) provide 0.72 parking spaces per unit, but residents use only 0.36 parking spaces per unit.

4.



Of the properties located less than 0.3 miles from a station, residents utilized 0.10 per unit compared to those properties located between 0.3 to 0.5 miles from a station. In other words, for every 30 units, a property within a five-minute walk of a station provides five fewer parking spaces and its residents use three fewer parking spaces than a comparable station-area property farther away.



## Parking Needs For Staff

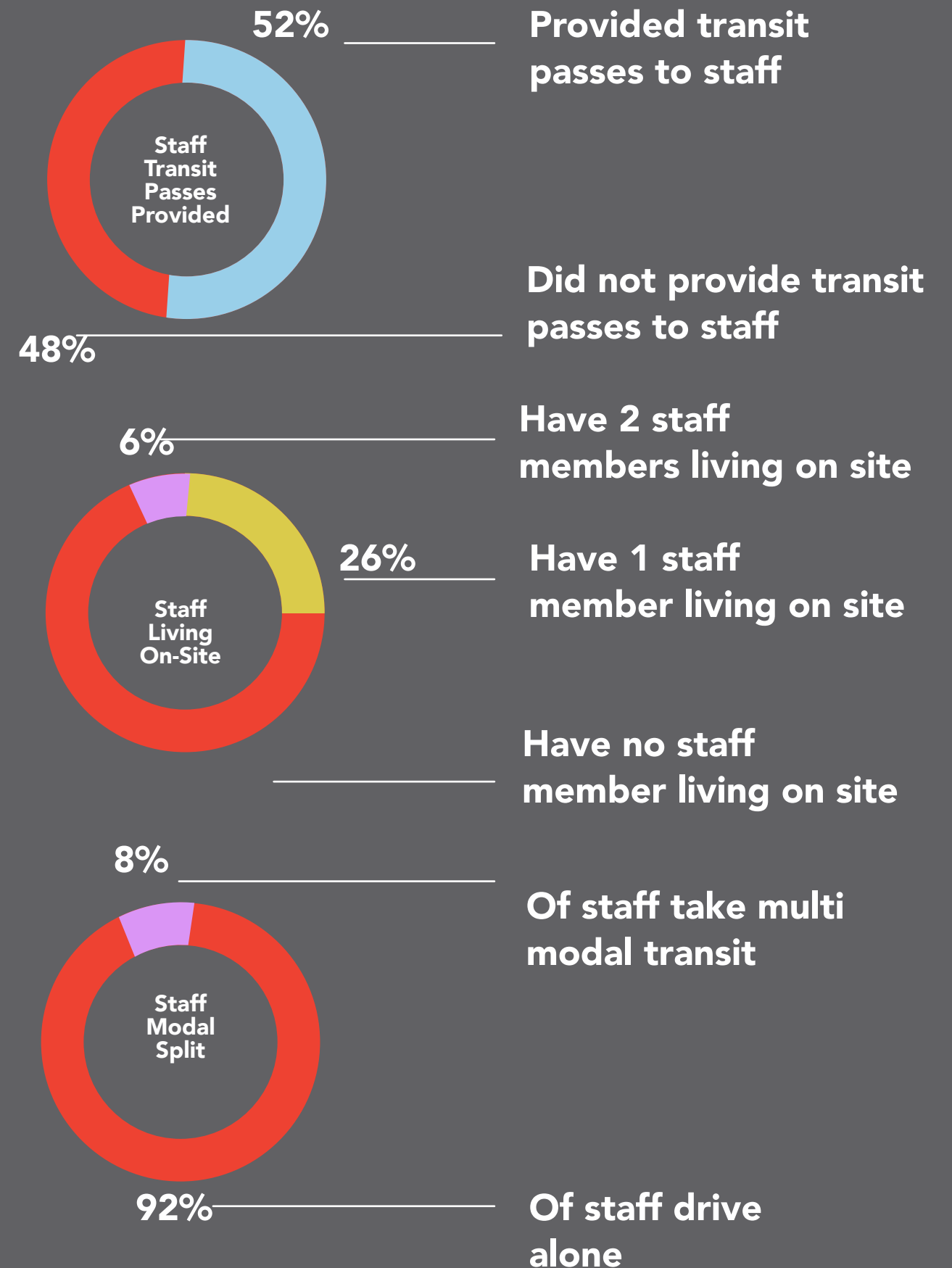
The same affordable housing properties that were surveyed by Fox Tuttle/Shopworks for resident vehicle ownership were asked about staffing to understand the parking demand needed by staff. On average, the majority of the affordable home properties have 5 full time staff members (1 per 17 units) and 4 part-time staff members (1 per 45 units).

There are a few exceptions to this data - The Attention Homes, Delores Project/Arroyo Village, and CCH: Renaissance at Civic Center Apartments. These buildings are staffed significantly more than the rest of the properties and reported having between 14 and 22 full-time staff members. However, upon further research it was determined that these organizations host all of their administrative staff for the entire organization in these buildings.

Half of the properties are staffed 24 hours a day, 7 days a week; while the other half have staff on-site only during typical daytime work hours. On average, there are 4 staff members on-site during any given shift. Typically, the evening and weekend shifts have a maximum of 1-2 staff members (front desk staff/security). The majority of the supportive housing properties have 3 staff shifts during weekdays and some have staff shifts on weekends. Approximately 1/3 of the properties have up to 2 staff members that live on-site.

Based on the provided data, 92% of staff members drive to work and need a parking space on or near the property.

The staff parking demand is additional to the residents' parking demand. However, the shift work that is typical of these types of properties, especially permanent supportive housing, lowers the need for parking since the entire staff are not on-site at the same time.



Takes the bus

Owens a car

One unit out of every 12 utilize parking.

Takes the bus

Walks

Rides bike

Rides bike

Takes the bus

Takes the bus

Light Rail

Takes the bus

Walks

Rides bike

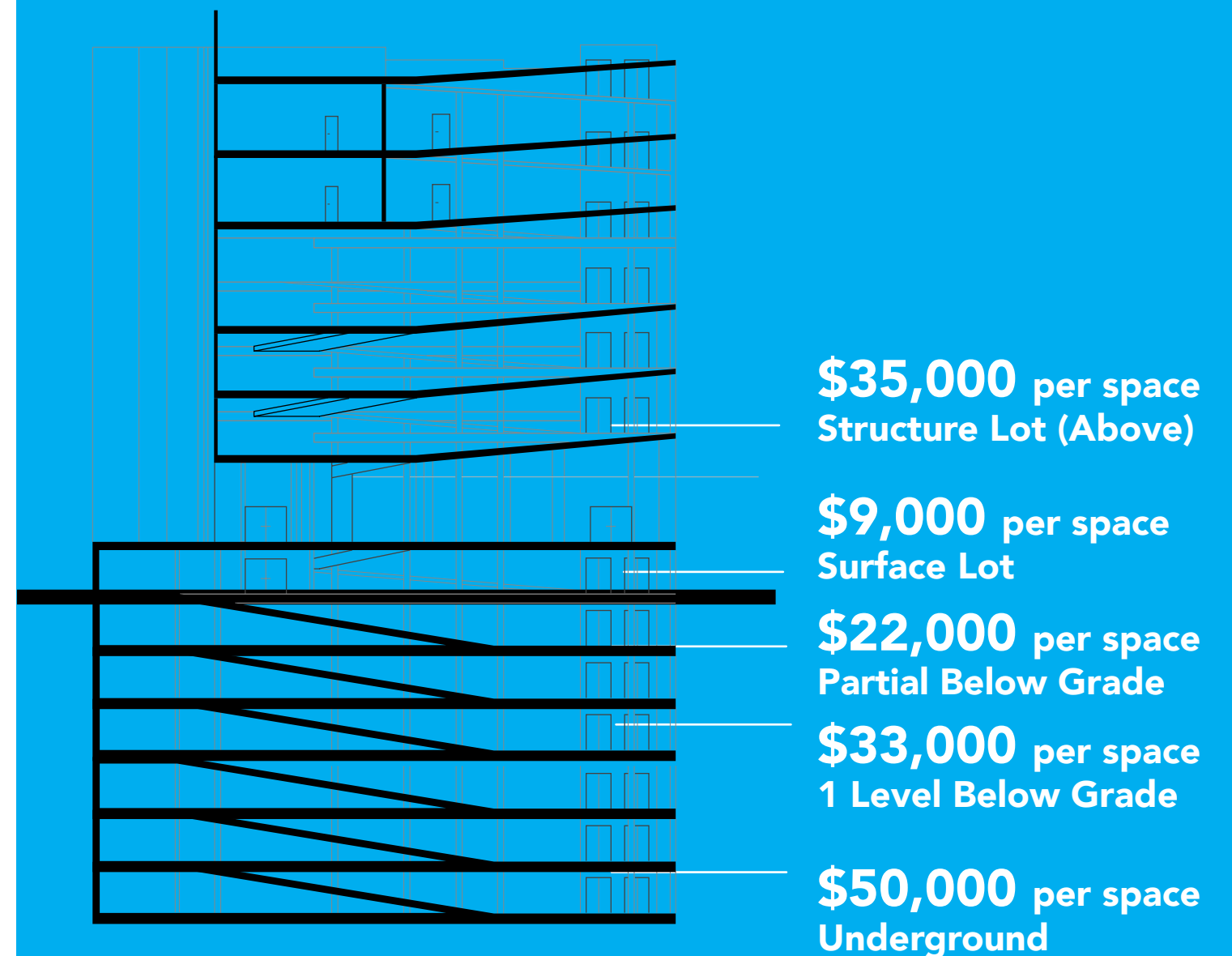


# Parking facilities are costly to build.

## Cost of Parking

Regardless of housing type, parking spaces are expensive to construct and maintain. The cost of parking impacts affordable housing projects more significantly than market-rate apartments since they cannot typically recoup the cost within rental fees. Parking also takes up valuable space that could be utilized for additional housing units or amenities for the people living on the property. As the data in this study indicates, parking demand is significantly less than the actual parking supply.

Existing and future sites would greatly benefit from reduced parking requirements to be able to repurpose the cost of parking and gain development area. Listed to the right are typical construction costs of one parking space in different parking facility types within the Front Range. This does not include the cost of the land or maintenance.







Across affordable housing, there are 0.29 vehicles per unit, equating to less than one vehicle per 6 units.



In Denver PSH there are .088 vehicles owned per unit, equating to less than one vehicle per 12 units.



Current Denver zoning requires on average 0.48 per unit across housing buildings. Thus the zoning requires 5.5 times more than the parking demand.

## In Conclusion

We are building parking when we could be building more housing. The Department of Housing Stability in Denver, HOST, estimates that after LIHTC financing there is a funding gap equal to \$37,000 per unit in the City of Denver; a gap that City and State funds have to cover to ensure these apartments get built. We are taking tax payer dollars and spending it on unnecessary parking, when we could be creating more units or beautiful parks that help people heal. Our report shows that we spent \$9.3 million over the last 6 years on parking that is not used in affordable housing. With those funds we could have built an entire new PSH project with 40 units. We built parking when we could have housed people.

## Thank You

We would like to extend our deepest appreciation for all the participants in this research who answered question after question as we refined our study. This report would not have been possible without each and every one of you, and we are so grateful for your assistance!



# Appendix A.

	Overview of Building				Parking Requirement	
	How many units and unit type	How many residents: adults, kids	AMI for building	Median AMI	Zoning Required Parking Ratio (per DU)	Reductions (if available)
Attention Homes, Boulder	40 units: 23 studios, 16 1-bed, 1 2-bed	41	All 0-30%	30%	1.00	0.25
Arroyo Village - Delores PSH	35 units: all 1-bed	40	All 0-30%	30%	1.00	0.40
Arroyo Village - Workforce	95 units: 25 1-bed, 58 2-bed, 12 3-bed	267	All 0-50%	50%	1.00	0.32
St. Francis Center's Cathedral Square	50 units, all 1-bed	55	All 0-30%	30%	0.25	0.00
Second Chance Center: PATH	50 units, 40 1-bed, 10 2-bed	49	All 0-30%	30%	1.50-2.00	0.54
Lee Hill	31 units, all 1-bed	31	All 0-30%	30%	1.00	0.25
Red Tail Ponds	60 units, 54 one-bedroom and 6 two-bedrooms	60	40@0-30%, 20 @0-60%	40%	0.75-1.00	0.50
Mental Health Center of Denver: Sanderson Apts	60 units	60	All 0-30%	30%	1.25	0.60
40 West	60 units: 54 1-bed, 6 2-bed	67	9@0-30%, 10@0-40%, 34@0-50%, 6@0-60%	46%	0.75	0.00
Flats at Two Creeks	78 units: 70 1-bed, 8 2-bed	106	16@0-30%, 21@0-50%, 40@0-60%	43%	0.75	0.00
Greenway Flats	65 one-bedroom units	68	62@0-30% 3@0-40%	30%	1.50	0.00
Guadalupe Apartments Archdiocesan Housing	47 units: 18 studios, 19 one-bed, 10 two-bed	68	All 0-30%	30%	1.25-1.75	0.00
Brandon Apartments	103 – 47 one-bedroom, 45 two-bedroom, 11 three-bedroom	86	20@0-30%, 39@0-40%, 44@0-60, 1 staff unit	47%	1.00	0.25
CCH: Renaissance West End Flats	101 units, 75 one-bedroom, 26 two-bedroom	115	35@0-30%, 31@0-40%, 26@0-50%, 8@0-60%	40%	0.75	0.25

Parking Requirement		Parking Analysis			Alternative Modes			
Minimum Required Parking	Parking Provided	Parking Ratio (Provided per DU)	Residents With Cars	Parking Lot Utilization	Walk/Transit/ Bike Score	Number of bike parking spaces	Do you provide transit passes or bus tickets to residents	Shared Bikes / Cars?
30.00	68	1.70	4	5.88%	95/62/95	40	Yes	No
21.00	8	0.23	6	75.00%	73/63/65	100	Yes	Yes
64.60	78	0.82	75	96.15%	73/63/65	100	No	Yes
12.50	13	0.26	2	15.38%	94/82/91	30+	Yes	No (but have access to shared van)
92.00	42	0.84	12	28.57%	59/58/70	50	Yes	Yes
23.25	14	0.45	3	21.43%	56/40/88	20	Yes	No
23.25	35	0.58	12	34.29%	38/36/66	66	Yes	Yes
75.00	30	0.50	0	0.00%	79/48/68	15	No	No
45.00	60	1.00	25	41.67%	67/55/73	15	No	No
58.50	78	1.00	40	51.28%	61/55/69	At least 20	No	No
8.00	10	0.15	6	60.00%	48/33/71	6 rentals, 48 bike parking spots	Yes	Yes
76.00	77	1.64	20	25.97%	1/0/27	20 spaces	No	No
66.00	70	0.68	42	60.00%	74/54/94	92	Yes	No
49.00	53	0.52	41	77.36%	77/55/90	N/A	No	

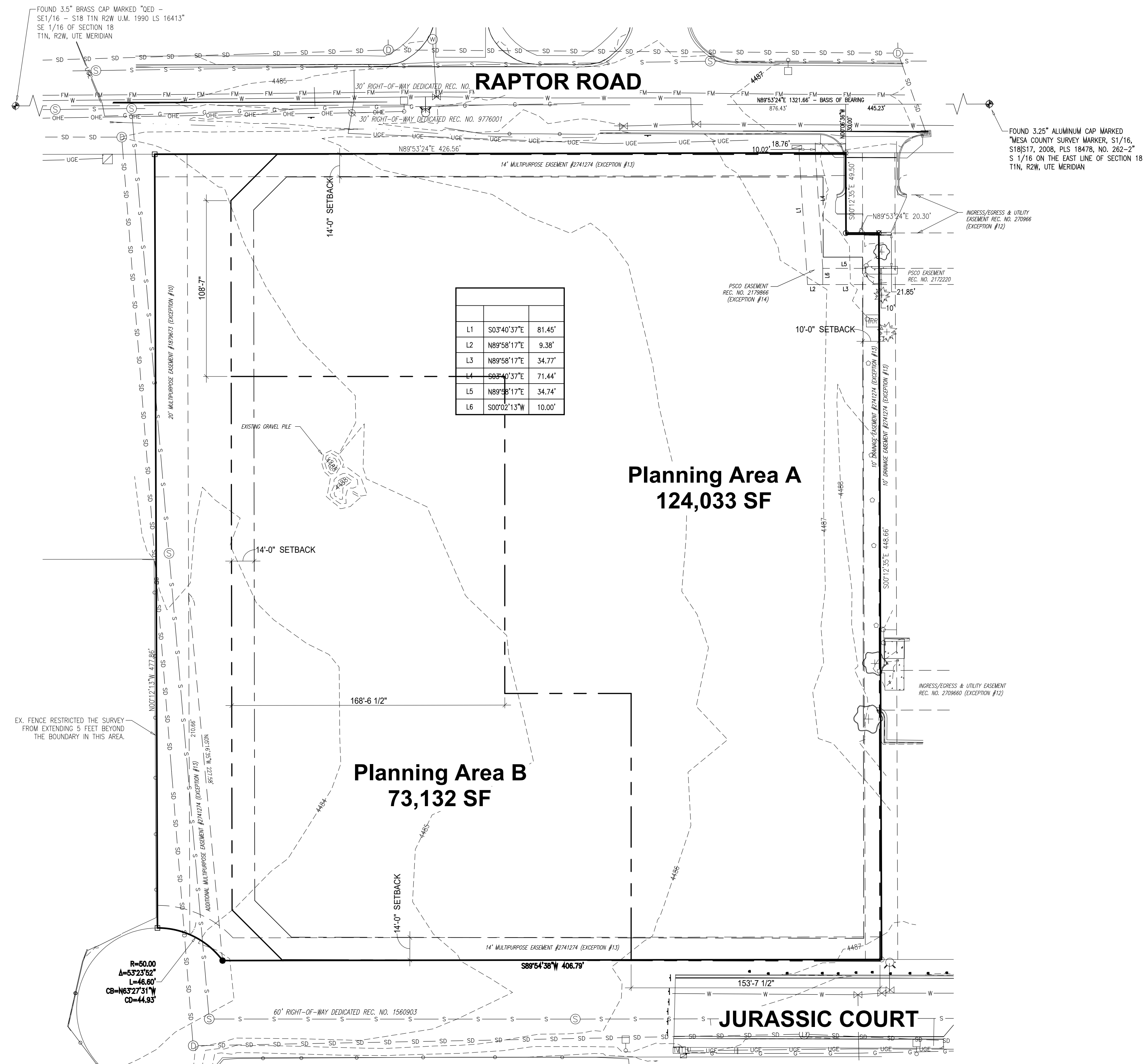
# Appendix A.

	Overview of Building				Parking Requirement	
	How many units and unit type	How many residents: adults, kids	AMI for building	Median AMI	Zoning Required Parking Ratio (per DU)	Reductions (if available)
<b>CCH: Renaissance at North Colorado Station</b>	103 Units: 19 studio, 54 one-, 24 two-, and 6 three-bedroom apartments	<b>112</b>	38@0-30%, 19@0-40%, 27@0-50%, 18@0-60%	<b>42%</b>	0.75	0.20
<b>CCH: Forum Apartments</b>	100 studio apartments	<b>98</b>	All 0-30%	<b>30%</b>	0.75	0.00
<b>CCH: Renaissance at Civic Center Apartments</b>	216 units: 200 studio, 16 one-bed	<b>188</b>	68@0-30%, 26@0-40%, 46@0-50%, 76@0-60%	<b>46%</b>	0.00	0.00
<b>CCH: Renaissance Riverfront Lofts</b>	100 Units: 88 one-bedroom apartments, 12 two-bedroom apartments	<b>Did not receive</b>	30@0-30%, 22@0-40%, 24@0-50%, 23@0-60%	<b>44%</b>	1.00-1.50	0.25+0.25
<b>CCH: Renaissance Uptown Lofts</b>	98 Units: 4 studios, 90 one-bedroom apartments, 4 two-bedroom apartments	<b>Did not receive</b>	41@0-30%, 31@0-40%, 17@0-50%, 8@0-60%	<b>39%</b>	0.75	0.25
<b>CCH: Renaissance Stout Street Lofts</b>	78 Units: 59 one-bedroom apartments, 19 two-bedroom apartments	<b>63</b>	26@0-30%, 23@0-40%, 22@0-50%, 6@0-60%	<b>41%</b>	0.75	0.20

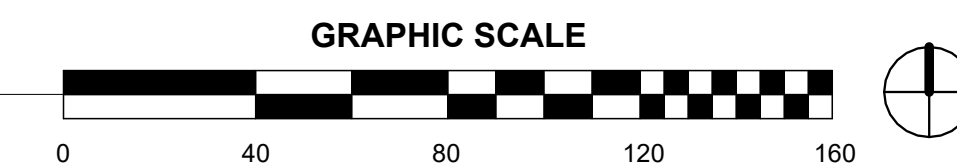
Parking Requirement		Parking Analysis			Alternative Modes			
Minimum Required Parking	Parking Provided	Parking Ratio (Provided per DU)	Residents With Cars	Parking Lot Utilization	Walk/Transit/ Bike Score	Number of bike parking spaces	Do you provide transit passes or bus tickets to residents	Shared Bikes / Cars?
54.00	63	0.61	17	26.98%	61/53/66	None	No	
75.00	0	0.00	5	N/A	96/89/95	3	Yes	
0.00	0	0.00	Unknown	N/A	99/91/88	0	Yes	
60.00	60	0.60	52	86.67%	62/66/84	0	No	
25.00	22	0.22	15	68.18%	94/86/98	2	Yes	
59.00	102	1.31	15	14.71%	93/90/99	0	Yes	

LOT 1, HORSETHIEF COMMERCIAL SUBDIVISION (REC. NO. 2741274)  
SITUATED IN THE SE 1/4 SE 1/4 OF SECTION 18

TOWNSHIP 1 NORTH, RANGE 2 WEST, UTE MERIDIAN  
CITY OF FRUITA, MESA COUNTY, COLORADO



1 | PRELIMINARY PUD PLAN  
1" = 40'-0"



**STUDIO ARCHITECTURE**  
3575 RINGSBY CT, SUITE 300A  
DENVER CO, 80216  
theSTUDIOarchitecture.com  
866.529.9130



# GEODE FLATS

614 RAPTOR ROAD, FRUITA CO

THIS GRAPHIC EQUALS 0'-1"  
DO NOT SCALE DRAWINGS

**STUDIO Project No:** Project #  
**Sheet Issue & Revision Log**

No	Date	Description
	10/12/23	PRELIMINARY PUD PLAN

It is the client's responsibility prior to or during construction to notify the architect in writing of any perceived errors or omissions in the plans and specifications of which a contractor thoroughly knowledgeable with the building codes and methods of construction should reasonably be aware. Written instructions addressing such perceived errors or omissions shall be received from the architect prior to the client or client's subcontractors proceeding with the work. The client shall be responsible for any defects in construction if these procedures are not followed.

PRELIMINARY PUD  
PLAN

**A1.0**

**FRUITA CITY COUNCIL  
WORKSHOP  
JANUARY 24, 2023  
6:30 P.M.**

**CALL TO ORDER AND ROLL CALL**

The workshop of the Fruita City Council was called to order at 6:30 p.m. by Mayor Kincaid. City Council members present were Mayor Pro Tem Matthew Breman, and City Councilors Jeannine Purser, James Williams, Ken Kreie and Aaron Hancey (Councilor Hancey arrived at 7:20 p.m.). Councilor Amy Miller was excused absent.

City Staff present were City Manager Mike Bennett, Assistant City Manager Shannon Vassen, Deputy City Clerk/Finance Director Margaret Sell, Deputy City Clerk Deb Woods, Parks and Recreation Director Marc Mancuso, City Planner Henry Hemphill and Planning and Development Director Dan Caris.

Also in attendance were members of the Fruita Planning Commission and Carrie McCool with McCool Development Solutions, LLC.

**AGENDA ITEMS**

**1. DISCUSSION ON POST IMPACT FEE IMPLEMENTATION**

City Clerk/Finance Director Margaret Sell reviewed staff's recommendation for implementation of an increase in the Parks, Open Space and Trails (POST) development impact fee for new construction based on a POST Impact Fee Study conducted by TischlerBise, Inc.

The study provides the necessary analysis and documentation to support an increase in the POST impact fee up to a maximum of the following amounts:

Current POST Impact Fee:                    \$1,860 per dwelling unit (in effect for over 10 years)

Proposed Maximum Supportable Fee:

Single-family Residential	\$3,179 per dwelling unit
Multi-family Residential*	\$2,154 per dwelling unit

*\*The difference in the proposed fee is based on the assumption that single-family residential has 2.42 persons per housing unit and multi-family residential has 1.64 persons per housing unit.*

Staff is proposing that the fee be implemented in two stages for single-family residences and one stage for multi-family residences.

City Manager Mike Bennett reviewed the history of the Council's previous discussions and a presentation from the City's hired consultant TischlerBise, Inc. Mrs. Sell requested feedback from the Council, noting that they could make any adjustments to the proposed fee increases that they preferred.

The Council noted that although the increases were high, they were something that haven't been updated in a considerable number of years. They reached a consensus to move forward with the fee increases as proposed by staff.

## **2. RESIDENCES AT FRUITA – CONCEPT PLAN**

### **STAFF PRESENTATION:**

City Planner Henry Hemphill provided a PowerPoint presentation of the Concept Plan for Residences at Fruita, a Planned Unit Development (PUD) proposal for a multi-family development on approximately 5.07 acres located directly west of the La Quinta Inn and northeast of the James M. Robb – Colorado River State Park. A Concept Plan for a PUD is optional in Fruita, but was submitted by the applicant's representative, McCool Development Solutions, in order to obtain general, non-binding feedback from the City Council, Planning Commission and City staff.

Mayor Kincaid explained that this was a new process that resulted from recent amendments to the Fruita Land Use Code whereby the process for reviewing a Concept Plan is done in a workshop setting instead of a public hearing at regular meeting of the Planning Commission and City Council.

Mr. Hemphill's presentation included a zoning map, aerial photo and several street-level photos of the property, an outline of the Concept Plan process for a PUD, list of Land Use Code requirements and a listing of the next steps that will include:

- Preliminary PUD Plan submission within 180 days
- Pre-Application meeting
- Outside agency review and legal notice
- Planning Commission public hearing (recommending body to the City Council)
- City Council public hearing (decision of approval or denial on the application)
- Zoning Ordinance

### **APPLICANT PRESENTATION:**

Carrie McCool with McCool Development Solutions, LLC (the applicant's representative) also provided a PowerPoint presentation that included:

- An overview of their client, developer TWG Development, LLC
- A Concept Plan illustration showing two planning areas
- PUD parameters concerning density and building heights, parking, and public benefit,
- Examples of how the project aligns with the City's Comprehensive Plan
- Photos of other completed projects as examples of architecture that may be used for the Residences at Fruita

Ms. McCool noted that the developer was also proposing a childcare center as a public benefit in the form of land dedication or a cash payment in lieu of dedicating land. She requested feedback from the City Council.

*Councilor Aaron Hancey arrived at 7:20 p.m.*

**STAFF, APPLICANT, PLANNING COMMISSOIN AND CITY COUNTY DISCUSSION:**

Discussions were held concerning the following:

- Council's preference of the public benefit being childcare versus parks and trails since parks and trails already have several revenue sources
- Staff will research childcare land dedication versus cash in lieu to determine which is a better benefit to the community
- Parking was a concern to the Council: applicant will submit a Parking Study
- Keeping the architecture in line with geologic features such as the Bookcliffs, Colorado National Monument and the desert
- The beauty of the State Patrol building
- The pricing of the multi-family housing is proposed at market rate
- Applicant will work with staff on further details and specific pricing for dwelling units
- There did not appear to be a concern about density as Fruita is in a housing crisis

**3. OTHER****PROCLAMATION REQUEST: COUNTERING ANTISEMITISM**

Councilor Breman provided hate crime statistics. The Council was in support of issuing a Proclamation titled, "Affirming a Commitment to Countering Antisemitism" at the next regular meeting of the City Council (February 7, 2023) and Councilor Breman said he could find some people who would attend and accept the Proclamation.

He noted that the League of Women Voters was taking the lead on an article that will be published in the Daily Sentinel on February 5<sup>th</sup> concerning antisemitism and asked for the Council's permission to include their names and titles in the piece, which he received.

**CITY MANAGER UPDATES:**

- Reminder of the Municipalities (Multi-Jurisdictional) Dinner next Tuesday. Councilor Kreie will be unable to attend.
- The Church of Jesus Christ of Latter-Day Saints submitted plans the previous Friday and they are out to the review agencies. The new church will be at the same location as the old one that burned down.
- Staff will be sending out a Press Release about the City's new Building Department, which will "go live" beginning on February 20, 2023.
- The Fruita Mews and Car Barn projects pulled their building permits from Mesa County to submit them to the City's new Building Division.
- Staff has been getting good feedback on the new Building Department.
- Vectra Bank will host a mixer in the second or third week of February for the Homebuilders Association and industry members. Fruita City Council will be invited to attend.

**CITY COUNCIL DISCUSSION:**

- Councilor Purser asked about when it is appropriate for Council members to respond to emails sent to them by the public. There was discussion about the importance of making clear any



personal opinions versus opinions of the entire Council and to not “reply all,” which constitutes a meeting of the City Council that must be noticed to the public 24 hours prior. Mr. Bennett stated that it was best for City Council members to respond to those emails that they would be discussing with the rest of the Council at a later date. He also recommended that the Council refer matters to staff if the Council does not have enough information to respond.

- Councilor Purser also mentioned that enrollment in public schools is declining across the state, including in School District #51, who has decided to adopt school consolidation as a strategy for relieving staffing and safety issues. District 51’s contracted demographer will present recommendations for school consolidations at the Board of Education meeting scheduled for Tuesday, February 21. Mike noted that he would be meeting with Brian Hill with the District and ask if they want to give the Council an update. Matthew stated that he is in support of School District #51.

#### **4. ADJOURN**

With no further business before the Council, Mayor Kincaid adjourned the meeting at 9:40 p.m.

Respectfully submitted,

Deb Woods  
Deputy City Clerk