

# EAST CARPENTER IMPROVEMENT COMPANY, LLC

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June 4, 2020

Mr. Scott Johnson  
Planning Manager  
Community Development Department  
City of Folsom  
50 Natoma Street  
Folsom, CA 95630

**Re: Mangini Ranch – Phase 2 (Rockcross) Tentative Map Compliance with Chapter 17.104- Inclusionary Housing**

Dear Mr. Johnson,

In accordance with Chapter 17.104 of the Folsom Municipal Code, Mangini Improvement Company, Inc. hereby elects to satisfy the Inclusionary Housing Ordinance requirements for the proposed Small Lot Tentative Map (Mangini Phase 2 – Rockcross) with the payment of the In-Lieu Fee as permitted in Section 17.104.060(G).

If you have any questions or comments, please feel free to contact me.

Sincerely,

**East Carpenter Improvement Company, LLC,**  
a California limited liability company

By: HBT ECIC, LLC,  
a California limited liability company

Its: Managing Member

By:   
Name: William B. Bunce  
Its: Manager

## Attachment 14

### Summary of Amendments to the Folsom Plan Area Specific Plan 2011-2020

## **Summary of Amendments to the Folsom Plan Area Specific Plan, 2011-2016**

The FPASP, approved in 2011, is a development plan for over 3,500 acres of previously undeveloped land located south of Highway 50, north of White Rock Road, east of Prairie City Road, and adjacent to the Sacramento County/El Dorado County line in the southeastern portion of the City.

The FPASP in its current form includes 11,461 residential units at various densities on approximately 1,622 acres; 320 acres designated for commercial and industrial use; +/- 275 acres designated for public/quasi-public uses, elementary/middle school/high schools, and community/neighborhood parks; and +/-1,109 acres for open-space areas.

Since FPASP adoption in 2011, the City Council has approved 7 amendments to the Specific Plan with land use and density refinements as summarized below.

- In August 2014, the Folsom City Council approved an amendment to the FPASP (Resolution No. 9420) relative to the alignment and design guidelines for the future Capital Southeast Connector (White Rock Road).
- On May 12, 2015, the Folsom City Council approved the Russell Ranch Specific Plan Amendment (Resolution No. 9566), the Final Environmental Impact Report (Resolution No. 9564) and a General Plan Amendment (Resolution No. 9566) for the Russell Ranch Project. The approved specific plan amendment (SPA) reduced the Plan Area residential area by approximately 17.8 acres and 264 dwelling units and reduced the commercial, office park/industrial and mixed-use area by approximately 59.5 acres and 0.65 million square feet of potential building area.
- On September 22, 2015, the Folsom City Council approved the Westland/Eagle Specific Plan Amendment, an Amendment to the Folsom General Plan (Resolution No. 9655) and an Addendum to the Final Environmental Impact Report/Environment Impact Statement (Resolution No. 9654) for the Westland/Eagle project. The approved SPA increased the residential dwelling unit count by 889 units and decreased the amount of commercial, office park/industrial and mixed-use area by approximately 82.5 acres and 1.4 million square feet of potential building area.
- On May 24, 2016, the Folsom City Council approved the Hillsborough Specific Plan Amendment (Resolution No. 9763), an Amendment to the Folsom General Plan (Resolution No. 9762), and an Addendum to the Final Environmental Impact Report/Environmental Impact Statement (Resolution No. 9761) for the Hillsborough Project. The approved SPA includes 394 additional housing units with about 65 additional acres of residential uses, approximately 49 fewer acres of public/quasi-public uses, approximately 16 acres less open space, approximately 5 additional acres of park space, and approximately 4 fewer acres of community commercial land

uses.

- On June 28, 2016, the Folsom City Council approved the Carr Trust Specific Plan Amendment and General Plan Amendment (Resolution No. 9789) and an Addendum to the Final Environmental Impact Report/Environmental Impact Statement (Resolution No. 9788) for the Carr Trust Project. The approved SPA decreased the residential dwelling unit count by 28 units by modifying the land use designation from medium low density residential to single-family high density residential.
- On June 28, 2016, the Folsom City Council approved the Folsom Heights Specific Plan Amendment and an Amendment to the Folsom General Plan (Resolution No. 9785) and an Addendum to the Final Environmental Impact Report/Environmental Impact Statement (Resolution No. 9784) for the Folsom Heights Project. The approved SPA did not change the number of dwelling units; however, the residential density was decreased, and the amount of general commercial was reduced by 23 acres.
- On June 28, 2016, the Folsom City Council approved the Broadstone Estates Specific Plan Amendment and an Amendment to the Folsom General Plan (Resolution No. 9787) and an Addendum to the Final Environmental Impact Report/Environmental Impact Statement (Resolution No. 9786) for the Broadstone Estates Project. The approved SPA eliminated the industrial office and general commercial land uses (10.5 acres and 13.3 acres, respectively), increased the single-family residential land use by approximately 21 acres and 71 additional dwelling units, and increased the open space area by 2.7 acres.

## Attachment 15

# Folsom Ranch Central District Design Guidelines

# 2

## ARCHITECTURAL DESIGN GUIDELINES

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### ARCHITECTURAL GUIDING PRINCIPLES

The following residential guiding principles will guide the architecture to ensure quality development:

- Provide a varied and interesting streetscene.
- Focus of the home is the front elevation, not the garage.
- Provide a variety of garage placements.
- Provide detail on rear elevations where visible from the public streets.
- Choose appropriate massing and roof forms to define the architectural styles.
- Ensure that plans and styles provide a degree of individuality.
- Use architectural elements and details to reinforce individual architectural styles.

### GENERAL ARCHITECTURAL GUIDELINES

#### Edge Conditions

Rear elevations visible from open spaces and major roadways shall incorporate enhanced details used on the front elevation of the home. Rear elevations observable from open spaces and major roadways shall be visually aesthetically pleasing from surrounding viewpoints and adjacencies. Silhouettes and massing of homes along edges require design sensitivity. A row of homes with a single front or rear facing gable are prohibited. The following should be considered, and at least one element incorporated, in the design of the side and rear elevations along edge conditions:

- A balance of hip and gable roof forms;
- Single-story plan;
- Single-story elements on two-story homes;
- Offset massing or wall planes (on individual plans or between plans);
- Roof plane breaks (on individual plans or between plans);
- Detail elements on the front elevation shall be applied to the side and rear elevations along edge conditions.





## Roof Forms

Rows of homes seen along major community roadways are perceived by their contrast against the skyline or background. The dominant impact is the shape of the building and roofline. To minimize the visual impact of repetitious flat planes, similar building silhouettes and similar ridge heights, discernibly different roof plans for each home plan shall be designed. Individual roof plans may be simple but, between different plans, should exhibit variety by using front to rear, side-to-side, gables, hipped roofs, and/or the introduction of single story elements.

The following roof design guidelines should also be considered:

- Provide a mix of gable and hip roofs along the streetscene.
- Design roofs for maximum solar exposure for the potential installation of solar features.
- Consider deep overhangs where appropriate to the style to provide additional shade and interior cooling.
- Offset roof planes, eave heights, and ridge lines.



## Corner Buildings

Buildings located on corners often times function as neighborhood entries and highlight the architecture for the overall Folsom Ranch, Central District community. Buildings located on corners shall include one of the following:

- Front and side facade articulation using materials that wrap around the corner-side of the building;
- Awning on corner side;
- Home entry on corner side;
- Corner facing garage;
- A pop-out side hip, gable, or shed form roof;
- An added single-story element, such as a wrap-around porch or balcony;
- Recessed second- or third-story (up to 35' max.); or
- Balcony on corner side.



## Front Elevations

Front elevations shall be detailed to achieve a variety along the street scene. Each front elevation shall incorporate a Feature Window treatment (see Feature Window requirements on page 2-6). In addition, each front elevation shall incorporate one or more of the following techniques:

- Provide enhanced style-appropriate details on the front elevation.
- Offset the second story from the first level for a portion of the second story.
- Vary the wall plane by providing projections of elements such as bay windows, porches, and similar architectural features.
- Create recessed alcoves and/or bump-out portions of the building.
- Incorporate second-story balconies.
- Create interesting entries that integrate features such as porches, courtyards, large recessed entry alcoves, or projecting covered entries with columns.
- Use a minimum of two building materials or colors on the front elevation.

- If due to building configuration the front entry location is not immediately apparent, direct and draw the observer to it with added elements such as signs, lighting, and landscape.



## Multi-family Entries

Entries for multi-family homes should create an initial impression, locate and frame the doorway, act as a link between public and private spaces, and further identify individual unit entries.

- Wherever possible, orient the front door and principal access towards the roadway, paseo, or common open space.
- Incorporate appropriate roof elements, columns, Feature Windows and/or architectural forms in the entry statement to emphasize the building character and the location of individual doorways.



## Feature Windows

All front and visible edge elevations shall incorporate one Feature Window treatment that articulates the elevation. Feature Window options include:

- A window of unique size or shape;
- Picture window;
- A bay window projecting a minimum of 24 inches, or a 12 inch pop-out surround;
- A window with a substantial surround matching or contrasting the primary color of the home;
- A window recess a minimum of 2 inches;
- Decorative iron window grilles;
- Decorative window shelves or sill treatments;
- Grouped or ganged windows with complete trim surrounds or unifying head and/or sill trim:
- A Juliet balcony with architectural style appropriate materials;
- Window shutters; or
- Trellis protruding a minimum of 12 inches from the wall plane of the window.

## Windows

Windows on south-facing exposures should be designed, to the greatest extent possible, to maximize light and heat entering the home in the winter, and to minimize light and heat entering in the summer.

West-facing windows should be shaded where feasible to avoid prolonged sun exposure/overheating of the homes.

For additional window requirements addressing Sound Attenuation requirements refer to the Mangini Ranch Residential Development Environmental Noise Assessment document prepared by Bollard Acoustical Consultants, Inc. on January 29, 2015.



Example of Feature Window



Example of Juliet Balcony



## Garage Door Treatments

Appropriate treatment of garage doors will further enhance the building elevation and decrease the utilitarian appearance of the garage door. Various garage door patterns, windows, and/or color schemes should be applied as appropriate to individual architectural styles, where feasible.

- Garage doors shall be consistent with the architecture of the building to reduce the overall visual mass of the garage.
- Garage doors shall be recessed 8 inches from the wall plane.
- All garage doors shall be automatic section roll-up doors.
- When appropriate, single garage doors are encouraged.
- Carriage-style garage doors of upgraded design are encouraged.



Porte Cochere with garage at rear of house



## Street Facing Garages

All street facing garages should vary the garage door appearance along the streetscene. Below are options for the door variety:

- Vary the garage door pattern, windows, and/or color as appropriate to individual architectural styles.
- Use an attached overhead trellis installed beneath the garage roof fascia and/or above garage door header trim.
- Span the driveway with a gated element or overhead trellis.
- Provide a porte cochere.
- Street facing garages on corner lots at neighborhood entries shall be located on the side of the house furthest away from the corner.



## Alley Treatments

The use of alleys should be elevated from purely functional, simple garage access to an enjoyable space that residents experience and utilize daily. Design of alleys shall address the functional and aesthetic features of the space to create a positive experience for the residents. At least one of the following shall be implemented along the alley:

- Building size and shape shall have stepped massing (recessed or cantilevered, i.e., stepping back upper floors or protruding forward upper floors) of at least one foot.
- Window trim, color, and appropriate details from the front elevation.
- Rear privacy walls and pedestrian gates designed and located for ease of unit access.
- Enhanced garage door patterns or finishes; garage door shall complement the design intent of the home and neighborhood.
- Provide sufficient planting areas between garages to soften the vertical architectural planes at alleys.

## Building Forms

Building form, detail, and placement greatly influences how a structure is perceived based on how light strikes and frames the building. The effect of sunlight is a strong design consideration, as shadow and shade can lend a sense of substance and depth to a building. The following elements and considerations can be used to facilitate the dynamic of light and depth perception of the building.

## Architectural Projections

Projections can create shadow and provide strong visual focal points. This can be used to emphasize design features such as entries, major windows, or outdoor spaces. Projections are encouraged on residential building forms. Projections may include, but are not limited to:

- Awnings (wood, metal, cloth)
- Balconies
- Shutters
- Eave overhangs
- Projecting second- or third-story elements
- Window/door surrounds
- Tower elements
- Trellis elements
- Recessed windows
- Porch elements
- Bay windows or dormers
- Shed roof elements

## Offset Massing Forms

Front and street-facing elevations may have offset masses or wall planes (vertically or horizontally) to help break up the overall mass of a building.

- Offset forms are effective in creating a transition:
  - Vertically between stories, or
  - Horizontally between spaces, such as recessed entries.
- Offset massing features are appropriate for changes in materials and colors.
- Offsets should be incorporated as a functional element or detail enhancement.
- Over-complicated streetscenes and elevations should be avoided.



- Streetscenes should provide a mix of simple massing elevation with offset massing elements to compose an aesthetic and understandable streetscape.

### Floor Plan Plotting

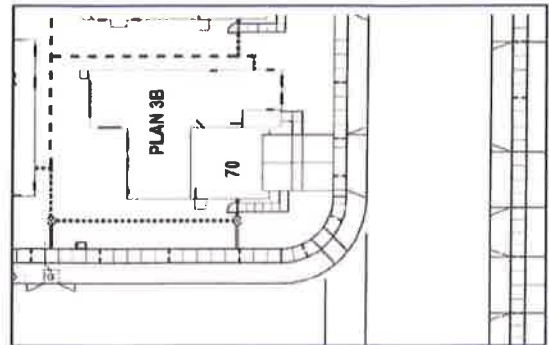
In each single-family detached neighborhood with a **minimum** of up to 80 homes, provide:

- Three floor plans.
- Four elevations for each floor plan using a minimum of **two** architectural styles. If only two styles are selected, elevations shall be significantly different in appearance.
- Four different color schemes for each floor plan.

In each single-family detached neighborhood with **more** than 80 homes, provide:

- Three floor plans.
- Four elevations for each floor plan using a minimum of **three** architectural styles. If only three styles per floor plan are selected, elevations shall be significantly different in appearance.
- Four different color schemes for each floor plan.

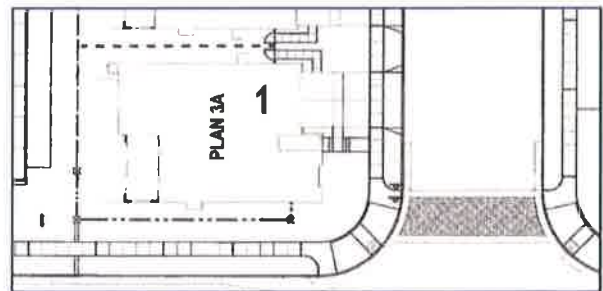
In each single-family detached neighborhood, street facing garages on corner lots at neighborhood entries shall be located on the side of the house furthest away from entry corner.



Example of undesirable Corner Lot Street Facing Garage Placement



Example of undesirable Corner Lot Street Facing Garage Placement



Example of preferred Corner Lot Plotting Garage Placement



Example of preferred Corner Lot Plotting Garage Placement



## Style Plotting

To ensure that architectural variety occurs, similar elevations cannot be plotted adjacent to or immediately across the street from one another. No more than two of the same floor plan/elevations shall be plotted next to each other or directly across the street from one another. (Refer to Section Four for Design Review process.) The following describes the minimum criteria for style plotting:

- For a home on a selected lot, the same floor plan and elevation is not permitted on the lot most directly across from it and the one lot on either side of it.
- Identical floor plans may be plotted on adjacent lots, provided a different elevation style is selected for each floor plan.
- Identical floor plans may be plotted on lots across the street from each other provided a different elevation style is selected for each floor plan.

## Color Criteria

To ensure variety of color schemes, like color schemes cannot be plotted adjacent to or immediately across the street from one another. Color and material sample boards shall be submitted for review along with the Master Plot Plan. (Refer to Section Four.)

A color scheme for a home on a selected lot may not be repeated (even if on a different floor plan) on the three lots most directly across from it and on the single lot to each side of it.



## Lower Height Elements

Lower height elements are important to streetscene variety, especially for larger buildings or masses, as they articulate massing to avoid monotonous single planes. These elements also provide a transition from the higher story vertical planes to the horizontal planes of sidewalk and street, and help to transition between public and private spaces. Lower height elements are encouraged to establish pedestrian scale and add variety to the streetscene. Lower height elements may include, but are not limited to:

- Porches
- Entry features
- Interior living spaces
- Courtyards
- Bay windows
- Trellises



### Balconies

Balconies break up large wall planes, offset floors, create visual interest to the facade, provide outdoor living opportunities, and adds human scale to a building. Scaled second- or third-story balconies can have as much impact on stepped massing and building articulation as a front porch or lower height elements. Balcony elements:

- May be covered or open, recessed into or projecting from the building mass.
- Shall be an integral element of, and in scale with, the building mass, where appropriate.
- Are discouraged from being plotted side-by-side at the same massing level (i.e. mirrored second-story balconies).



### Roof Considerations

Composition and balance of roof forms are as definitive of a streetscape as the street trees, active architecture, or architectural character.

- Rooflines and pitches, ridgelines and ridge heights should create a balanced form to the architecture and elevation.
- Direction of ridgelines and/or ridge heights should vary along a streetscene.
- Roof overhangs (eaves and rakes) may be used as projections to define design vocabulary and create light and shade patterns.
- Hip, gable, shed, and conical roof forms may be used separately or together on the same roof or streetscene composition.
- Roof form and pitch shall be appropriate to the massing and design vocabulary of the home.





## Outdoor Living Spaces

Outdoor living spaces, including porches, balconies, and courtyards, activate the streetscene and promote interaction among neighbors. Outdoor living spaces can also create indoor/outdoor environments opening up the home to enhance indoor environmental quality. Wherever possible, outdoor living space is encouraged.

## Materials

The selection and use of materials has an important impact on the character of each neighborhood and the community as a whole. Wood is a natural material reflective of many architectural styles; however, maintenance concerns, a design for long-term architectural quality and new high-quality manufactured alternative wood materials make the use of real wood elements less desirable. Where “wood” is referred to in these guidelines, it can also be interpreted as simulated wood trim with style-appropriate wood texture. Additionally, some styles can be appropriately expressed without the wood elements, in which case stucco-wrapped, high-density foam trim (with style-appropriate stucco finish) is acceptable. Precast elements can also be satisfied by high-density foam or other similar materials in a style-appropriate finish.



- Brick, wood, and stone cladding shall appear as structural materials, not as applied veneers.
- Material changes should occur at logical break points.
- Columns, tower elements, and pilasters should be wrapped in its entirety.
- Materials and colors should be varied to add texture and depth to the overall character of the neighborhood.
- The use of flashy or non-traditional materials or colors that will not integrate with the overall character of the community is prohibited.
- Material breaks at garage corners shall have a return dimension equal to or greater than the width of the materials on the garage plane elevation.
- Use durable roofing and siding materials to reduce the need for replacement.
- Use local, recycled and/or rapidly renewable materials to conserve resources and reduce energy consumption associated with the manufacturing and transport of the materials. (Refer to Section Four for Design Review process.)



## Exterior Structures

Exterior structures, including but not limited to, porches, patio covers, and trellises shall reflect the character, color, and materials of the building to which they are related.

- Columns and posts should project a substantial and durable image.
- Stairs should be compatible in type and material to the deck and landing.
- Railings shall be appropriately scaled, consistent with the design vernacular of the building, and constructed of durable materials.
- Exposed gutters and downspouts shall be colored to complement or match the fascia material or surface to which they are attached.

## Accessory Structures

Accessory structures should conform to the design standards, setbacks, and height requirements of the primary structure. If visible from the front or side lot line, the visible elevation should be considered a front elevation and should meet the design criteria of the applicable architectural style.



## Lighting

Appropriate lighting is essential in creating a welcoming evening atmosphere for the Folsom Ranch, Central District community. As a forward-thinking community, The Folsom Ranch, Central District will institute dark sky recommendations to mitigate light pollution, cut energy waste, and protect wildlife. All lighting shall be aesthetically pleasing and non-obtrusive, and meet the dark sky recommendations.

- All exterior lighting shall be limited to the minimum necessary for public safety.
- All exterior lighting shall be shielded to conceal the light source, lamp, or bulb. Fixtures with frosted or heavy seeded glass are permitted.
- Each residence shall have an exterior porch light at its entry that complements the architectural style of the building.
- Where feasible, lighting should be on a photocell or timer.
- Low voltage lighting shall be used whenever possible.

## Address Numbers

To ensure public safety and ease of identifying residences by the Fire and Police Departments, address numbers shall be lighted or reflective and easily visible from the street.



## RESIDENTIAL ARCHITECTURAL STYLES

Folsom Ranch, Central District is envisioned as a sustainable, contemporary community where architectural massing, roof forms, detailing, walls, and landscape collaborate to reflect historic, regional, and climate-appropriate styles.

The design criteria established in this section encourages a minimum quality design and a level of style through the use of appropriate elements. Although the details are important elements that convey the style, the massing and roof forms are essential to establishing a recognizable style. The appropriate scale and proportion of architectural elements and the proper choice of details are all factors in achieving the architectural style.

### *ARCHITECTURAL THEME: CALIFORNIA HERITAGE*

The styles selected for Folsom Ranch, Central District have been chosen from the traditional heritage of the California home styles, a majority of which have been influenced by the Spanish Mission and Mexican Rancho eras. Over the years, architectural styles in California became reinterpreted traditional styles that reflect the indoor-outdoor lifestyle choices available in the Mediterranean climate. These styles included the addition of western materials while retaining the decorative detailing of exposed wood work, wrought iron hardware, and shaped stucco of the original Spanish styles. Mixing of style attributes occurs in both directions, such as adapting Spanish detailing to colonial style form, or introducing colonial materials and details to the Hacienda form and function. The landscape and climate of California has also generated styles that acknowledge and blend with its unique setting. The Italian Villa is a prime example of a transplanted style developed in a climate zone similar to the climate found in California.

The following styles can be used within Folsom Ranch, Central District:

- Italian Villa
- Spanish Colonial
- Monterey
- Western Farmhouse
- European Cottage
- Craftsman
- Early California Ranch
- American Traditional

Additional architectural styles compatible with the intent of these guidelines may be added when it can be demonstrated to the Architectural Review Committee that they are regionally appropriate.

The following pages provide images and individual “style elements” that best illustrate and describe the key elements of each style. They are not all mandatory elements, nor are they a comprehensive list of possibilities. Photographs of historic and current interpretations of each style are provided to inspire and assist the designer in achieving strong, recognizable architectural style elevations. The degree of detailing and/or finish expressed in these guidelines should be relative to the size and type of building upon which they are applied.

These images are for concept and inspiration only and should not be exactly replicated.



## ITALIAN VILLA

The Italian Villa was one of the most fashionable architectural styles in the United States in the 1860's. Appearing on architect-designed landmarks in larger cities, the style was based on formal and rigidly symmetrical palaces of the Italian Renaissance.

Although residential adaptations generated less formality, traditional classical elements, such as the symmetrical facade, squared tower entry forms, arched windows, and bracketed eaves, persisted as the enduring traits of this style. When cast iron became a popular building material, it became a part of the Italianate vocabulary, embellishing homes with a variety of designs for balconies, porches, railings, and fences.

### *Italian Villa Style Elements:*

- Eave and exaggerated overhangs.
- Wall materials typically consist of stucco with stone and precast accents.
- Decorative brackets below eaves may be added accents.
- Barrel tile or "S" tile roof
- The entry may be detailed with a precast surround feature.
- Stucco or precast columns with ornate cap and base trim are typical.
- Wrought iron elements, arched windows or elements, and quoins are frequently used as details.



Example of Italian Villa Architecture



Example of Italian Villa Architecture



Example of Italian Villa Architecture



## SPANISH COLONIAL

This style evolved in California and the southwest as an adaptation of Mission Revival infused with additional elements and details from Latin America. The style attained widespread popularity after its use in the Panama-California Exposition of 1915.

Key features of this style were adapted to the California lifestyle. Plans were informally organized around a courtyard with the front elevation very simply articulated and detailed. The charm of this style lies in the directness, adaptability, and contrasts of materials and textures.

### *Spanish Colonial Style Elements:*

- Plan form is typically rectangular or “L”-shaped.
- Roofs are typically of shallower pitch with “S” or barrel tiles and typical overhangs.
- Roof forms are typically comprised of a main front-to-back gable with front-facing gables.
- Wall materials are typically stucco.
- Decorative “wood” beams or trim are typical.
- Segmented or full-arch elements are typical in conjunction with windows, entry, or the porch.
- Round or half-round tile profiles are typical at front-facing gable ends.
- Arcades are sometimes utilized.
- Windows may be recessed, have projecting head or sill trim, or be flanked by plank-style shutters.
- Decorative wrought-iron accents, grille work, post or balcony railing may be used.



Example of Spanish Colonial Architecture



Example of Spanish Colonial Architecture



Example of Spanish Colonial Architecture



## MONTEREY

The Monterey style is a combination of the original Spanish Colonial adobe construction methods with the basic two-story New England colonial house. Prior to this innovation in Monterey, all Spanish colonial houses were of single story construction.

First built in Monterey by Thomas Larkin in 1835, this style introduced two story residential construction and shingle roofs to California. This Monterey style and its single story counterpart eventually had a major influence on the development of modern architecture in the 1930's.

The style was popularized by the used of simple building forms. Roofs featured gables or hips with broad overhangs, often with exposed rafter tails. Shutters, balconies, verandas, and porches are integral to the Monterey character. Traditionally, the first and second stories had distinctly different cladding material; respectively siding above with stucco and brick veneer base below.

The introduction of siding and manufactured materials to the home building scene allowed for the evolution of the Monterey home from strictly Spanish Adobe construction to a hybrid of local form and contemporary materials. Siding, steeper pitched flat tile roofing, and the cantilevered balcony elements on the Monterey house define this native California style.



Example of Monterey Architecture

### Monterey Style Elements:

- Plan form is typically a simple two-story box.
- Roofs are typically shallow to moderately pitched with flat concrete tile or equal; "S" tile or barrel tile are also appropriate.
- Roof forms are typically a front-to-back gable with typical overhangs.
- Wall materials are typically comprised of stucco, brick, or siding.
- Materials may contrast between first and second floors.
- A prominent second-story cantilevered balcony is typically the main feature of the elevation; two-story balconies with simple posts are also appropriate.
- Simple Colonial corbels and beams typically detail roof overhangs and cantilevers.
- Balcony or porch is typically detailed by simple columns without cap or base trim.
- Front entry is typically traditionally pedimented by a surround, porch, or portico.
- Windows are typically accented with window head or sill trim of colonial-style and louvered shutters.
- Corbel and post sometimes lean toward more "rustic" details and sometimes toward more "Colonial" details.



Example of Monterey Architecture



## WESTERN FARMHOUSE

The Farmhouse represents a practical and picturesque country house. Its beginnings are traced to both Colonial styles from New England and the Midwest. As the American frontier moved westward, the American Farmhouse style evolved according to the availability of materials and technological advancements, such as balloon framing.

Predominant features of the style are large wrapping front porches with a variety of wood columns and railings. Two story massing, dormers, and symmetrical elevations occur most often on the New England Farmhouse variations. The asymmetrical, casual cottage look, with a more decorated appearance, is typical of the Western American Farmhouse. Roof ornamentation is a characteristic detail consisting of cupolas, weather vanes, and dovecotes.

### *Western Farmhouse Style Elements:*

- Plan form is typically simple.
- Roofs are typically of steeper pitch with flat concrete tiles or equal.
- Roof forms are typically a gable roof with front-facing gables and typical overhangs.
- Roof accents sometimes include standing-seam metal or shed forms at porches.
- Wall materials may include stucco, horizontal siding, and brick.
- A front porch typically shelters the main entry with simple posts.
- Windows are typically trimmed in simple colonial-style; built-up head and sill trim is typical.
- Shaped porch columns typically have knee braces.



Example of Western Farmhouse Architecture



Example of Western Farmhouse Architecture



Example of Western Farmhouse Architecture



## EUROPEAN COTTAGE

The European Cottage is a style that evolved out of medieval Tudor and Normandy architecture. This evolving character that eventually resulted in the English and French “Cottage” became extremely popular when the addition of stone and brick veneer details was developed in the 1920’s.

Although the cottage is looked upon as small and unpretentious, the style was quickly recognized as one of the most popular in America. Designs for the homes typically reflected the rural setting in which they evolved. Many established older neighborhoods across the United States contain homes with the charm and character of this unpretentious style.

Roof pitches for these homes are steeper than traditional homes, and are comprised of gables, hips, and half-hip forms. The primary material is stucco with heavy use of stone and brick at bases, chimneys, and entry elements. Some of the most recognizable features for this style are the accent details in gable ends, sculptured swooping walls at the front elevation, and tower or alcove elements at the entry.

### *European Cottage Style Elements:*

- Rectangular plan form massing with some recessed second floor area is desirable.
- Main roof hip or gable with intersecting gable roofs is typical of this style.
- Steep roof pitches with swooping roof forms are encouraged.
- Roof appearance of flat concrete tile or equal is typical of the European Cottage style.
- Recessed entry alcoves are encouraged.
- Wall materials are typically comprised of stucco with brick and/or stone veneer.
- Bay windows, curved or round top accent windows, and vertical windows with mullions and simple 2x trim are utilized at front elevations and high visibility areas.
- Stone or brick accent details at the building base, entry, and chimney elements are typical.
- Horizontal siding accents and wrought iron or wood balconies and pot shelves are encouraged.



Example of European Cottage Architecture



Example of European Cottage Architecture





## CRAFTSMAN

Influenced by the English Arts and Crafts movement of the late 19th century and stylized by California architects like Bernard Maybeck in Berkeley and the Greene brothers in Pasadena, the style focused on exterior elements with tasteful and artful attention. Originating in California, Craftsman architecture relied on the simple house tradition, combining hip and gable roof forms with wide, livable porches, and broad overhanging eaves. The style was quickly spread across the state and across the country by pattern books, mail-order catalogs, and popular magazines.

Extensive built-in elements define this style, treating details such as windows and porches as if they were furniture. The horizontal nature is emphasized by exposed rafter tails and knee braces below broad overhanging eaves constructed in rustic-textured building materials. The overall effect was the creation of a natural, warm, and livable home of artful and expressive character. Substantial, tapered porch columns with stone piers lend a Greene character, while simpler double posts on square brick piers and larger knee braces indicate a direct Craftsman reference to the style of California architect Bernard Maybeck, who was greatly influenced by the English Arts and Crafts Movement of the late 19th Century.



Example of Craftsman Architecture

### *Craftsman Style Elements:*

- Plan form is typically a simple box.
- Roofs are typically of shallower pitch with flat concrete tiles (or equal) and exaggerated eaves.
- Roof forms are typically a side-to-side gable with cross gables.
- Roof pitch ranges from 3:12 to 5:12 typically with flat concrete tiles or equal.
- Wall materials may include stucco, horizontal siding, and stone.
- Siding accents at gable ends are typical.
- A front porch typically shelters the main entry.
- Exposed rafter tails are common under eaves.
- Porch column options are typical of the Craftsman style:
  - Battered tapered columns of stone, brick, or stucco
  - Battered columns resting on brick or stone piers (either or both elements are tapered)
  - Simpler porch supports of double square post resting on piers (brick, stone, or stucco); piers may be square or tapered.
- Windows are typically fully trimmed.
- Window accents commonly include dormers or ganged windows with continuous head or sill trim.



Example of Craftsman Architecture



## EARLY CALIFORNIA RANCH

A building form rather than an architectural style, the Ranch is primarily a one-story rambling home with strong horizontal lines and connections between indoor and outdoor spaces. The “U”- or “L”-shaped open floor plan focused on windows, doors, and living activities on the porch or courtyard. The horizontal plan form is what defines the Ranch.

The applied materials, style, and character applied to the Ranch have been mixed, interpreted, adapted, and modernized based on function, location, era, and popularity.

This single-story family oriented home became the American dream with the development of tract homes in the post-World War II era. Simple and affordable to build, the elevation of the Ranch was done in a variety of styles. Spanish styling with rusticated exposed wood beams, rafter tails under broad front porches, and elegantly simple recessed windows were just as appropriate on the Ranch as the clean lines of siding and floor to ceiling divided-light windows under broad overhanging laminate roofs.

Details and elements of the elevation of a Ranch should be chosen as a set identifying a cohesive style. Brick and stucco combinations with overly simple sill trim under wide windows with no other detailing suggests a Prairie feel, while all stucco, recessed windows, and exposed rusticated wood calls to mind a Hacienda ranch.



Example of California Ranch Architecture

## California Ranch Style Elements:

- Plan form is typically one-story with strong horizontal design.
- Roofs are typically shallow pitched with “S” tile, barrel tile, or flat concrete tile.
- Roof forms are typically gable or hip with exaggerated overhangs.
- Wall materials are commonly comprised of stucco, siding, or brick.
- A porch, terrace, or courtyard is typically the prominent feature of the elevation.
- Exposed rafter tails are typical.
- Porch is commonly detailed by simple posts or beams with simple cap or base trim.
- Front entry is typically traditionally pedimented by a surround, porch, or portico.
- Windows are typically broad and accented with window head and sill trim, shutters, or are recessed.
- A strong indoor/outdoor relationship joined by sliding or French doors, or bay windows is common.



Example of California Ranch Architecture



## AMERICAN TRADITIONAL

The American Traditional style is a combination of the early English and Dutch house found on the Atlantic coast. Their origins were sampled from the Adam style and other classical styles. Details from these original styles are loosely combined in many examples.

Current interpretations have maintained the simple elegance of the early prototypes, but added many refinements and new design details. This style relies on its asymmetrical form and colonial details to differentiate it from the strict colonial styles.

Highly detailed entries having decorative pediments extended and supported by semi-engaged columns typically. Detailed doors with sidelights and symmetrically designed front facades. Cornices with dentils are an important feature and help identify this style.



Example of American Traditional Architecture



Example of American Traditional Architecture

### *American Traditional Style Elements:*

- Plan form is typically asymmetric “L”-shaped.
- Roofs are typically of moderate to steeper pitch with flat concrete tile (or equal) roof and exaggerated boxed eaves.
- Roof forms are typically hip or gable with dominant forward facing gables.
- Front facade is typically one solid material which may include stucco, brick, or horizontal siding.
- The front entry is typically sheltered within a front porch with traditionally detailed columns and railings.
- A curved or round-top accent window is commonly used on the front elevation.
- Windows are typically fully trimmed with flanking louvered shutters.
- Gable ends are typically detailed by full or partial cornice, sometimes emphasized with dentils or decorative molding.
- Decorative or pedimented head and sill trim on windows is typical.



Example of American Traditional Architecture



**Attachment 16**

**Planning Commission Staff Report  
Dated July 1, 2020**



CITY OF  
**FOLSOM**  
DISTINCTIVE BY NATURE

**AGENDA ITEM NO. 1**  
**Type: Public Hearing**  
Date: July 1, 2020

## **Planning Commission Staff Report**

50 Natoma Street, Council Chambers  
Folsom, CA 95630

**Project:** Rockcross Subdivision  
**File #:** PN-19-388  
**Requests:** Small-Lot Vesting Tentative Subdivision Map  
Design Review  
Minor Administrative Modification  
**Location:** The proposed Rockcross Subdivision project is located in the Mangini West sub-area of the Folsom Plan Area Specific Plan at the northeast corner of the intersection of East Bidwell Street and Savannah Parkway  
**Staff Contact:** Steve Banks, Principal Planner, 916-461-6207  
sbanks@folsom.ca.us

### **Property Owner**

Name: East Carpenter  
Improvement Co., LLC  
Address: 4370 Town Center Blvd,  
Suite 100, El Dorado Hills,  
CA 95762

### **Applicant**

Name: East Carpenter  
Improvement Co., LLC  
Address: 4370 Town Center  
Blvd, Suite 100, El Dorado Hills,  
CA 95762

**Recommendation:** Conduct a public hearing and upon conclusion recommend approval of the following, subject to the findings (Findings A-R) and conditions of approval (Conditions 1-53) attached to this report:

- Small-Lot Vesting Tentative Subdivision Map
- Design Review
- Minor Administrative Modification for Transfer of Development Rights

**Project Summary:** The proposed project involves several related actions associated with a proposed residential development:

- **A Small-Lot Vesting Tentative Subdivision Map** to subdivide the 14.2-acre project site into 118 residential lots.
- **Design Review** of architecture and designs for the proposed homes.



CITY OF  
**FOLSOM**  
DISTINCTIVE BY NATURE

**AGENDA ITEM NO. 1**  
**Type: Public Hearing**  
Date: July 1, 2020

- **A Minor Administrative Modification** to transfer 35 allocated dwelling units from the Rockcross Subdivision project to three other locations within the Folsom Plan Area Specific Plan.

These proposed actions are described in detail and analyzed later in this report.

**Table of Contents:**

Attachment 1 - Background and Setting

Attachment 2 - Project Description

- Small-Lot Vesting Tentative Subdivision Map
- Design Review
- Minor Administrative Modification (Shift of Dwelling Units to Other Parcels)

Attachment 3 - Analysis

- Small-Lot Vesting Tentative Subdivision Map
- Design Review
- Minor Administrative Modification (Shift of Dwelling Units to Other Parcels)

Attachment 4 - Conditions of Approval

Attachment 5 - Vicinity Map

Attachment 6 - Small-Lot Vesting Tentative Subdivision Map, dated February 18, 2020

Attachment 7 - Preliminary Grading, Drainage, and Utility Plan, dated February 18, 2020

Attachment 8 - Conceptual Front Yard Landscaping, dated March 18, 2020

Attachment 9 - Wall and Fence Exhibit, dated February, 2020

Attachment 10 - Residential Schematic Design, dated June 17, 2020

Attachment 11 - Exterior Color/Materials Specification, dated February 19, 2020

Attachment 12 - CEQA Exemption and Streamlining Analysis for the Rockcross Subdivision Project

Attachment 13 - Access and Circulation Analysis, dated May 12, 2020

Attachment 14 - Environmental Noise Analysis, dated April 24, 2020



CITY OF  
**FOLSOM**  
DISTINCTIVE BY NATURE

**AGENDA ITEM NO. 1**  
**Type: Public Hearing**  
Date: July 1, 2020

**Attachment 15 - Site Photographs**

**Attachment 16 - Rockcross Subdivision Booklet (Separate Bound Document) including the following, except where superseded by separate documents or illustrations listed above:**

- Illustrative Site Plan (Booklet page 15)
- Residential Architecture (Booklet page 16)
- Conceptual Landscape Design (Booklet page 20)
- Elevations and Floor Plans (Booklet page A1 to A38)

**Attachment 17 - Applicant's Inclusionary Housing Letter, June 4, 2020**

**Attachment 18 - Summary of Amendments to the Folsom Plan Area Specific Plan, 2011-2020**

**Attachment 19 - Folsom Ranch Central District Design Guidelines**

**Attachment 20 - Planning Commission PowerPoint Presentation**

**Submitted,**

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**PAM JOHNS**  
Community Development Director

## ATTACHMENT 1 BACKGROUND AND SETTING

### A. Background: Folsom Plan Area Specific Plan

The proposed project site is part of the approved Folsom Plan Area Specific Plan (FPASP), a comprehensively planned community that proposes new development based “Smart Growth” and Transit Oriented Development principles.

The FPASP, approved in 2011, is a development plan for over 3,500 acres of previously undeveloped land located south of Highway 50, north of White Rock Road, east of Prairie City Road, and adjacent to the Sacramento County/El Dorado County line in the southeastern portion of the City.

The FPASP includes a mix of residential, commercial, employment and public uses, complemented by recreational amenities including a significant system of parks and open space, all within close proximity to one another and interconnected by a network of “complete streets”, trails and bikeways. The Specific Plan is consistent with the SACOG Blueprint Principles and the requirements of SB 375 (Sustainable Communities and Climate Protection Act).

The FPASP includes 11,461 residential units at various densities on approximately 1,630 acres; 310 acres designated for commercial and industrial use; +/-130 acres designated for public/quasi-public uses, elementary/middle school/high schools, and community/neighborhood parks; and +/-1,110 acres for open-space areas.

Since FPASP adoption in 2011, the City Council has approved eight amendments to the Specific Plan with land use and density refinements (summarized in Attachment 18 to this staff report).

Overall, the changes to the Specific Plan have *reduced* the amount of commercial development planned for the area and *increased* the amount of residential development:

	<b>Approved 2011</b>	<b>As Amended to Date</b>
<b>Commercial:</b>	5,199,408 SF	2,788,844 SF (-2,410,564 SF)
<b>Residential Units:</b>	10,210 Units	11,461 Units (+1,251 Units)

Based on the approved changes, the projected population of the FPASP has increased from 24,362 (based on approved development in 2010) to 27,965 (as approved to date).

In addition to the amendments listed in Appendix 18, a number of Minor Administrative Modifications have been approved. These minor modifications have moved allocated dwelling units to new locations in the FPASP area but did not affect the overall number of



approved units. Because they do not increase or decrease units, these minor modifications do not affect the ultimate population of the FPASP area.

The Rockcross project site is designated MLD in the FPASP, which provides for development at 7.0 to 12.0 units per acre. An excerpt from the FPASP Land Use Map is shown below. This designation is consistent with the site's MLD designation in the Folsom General Plan.

FIGURE 1: FPASP LAND USE MAP EXCERPT

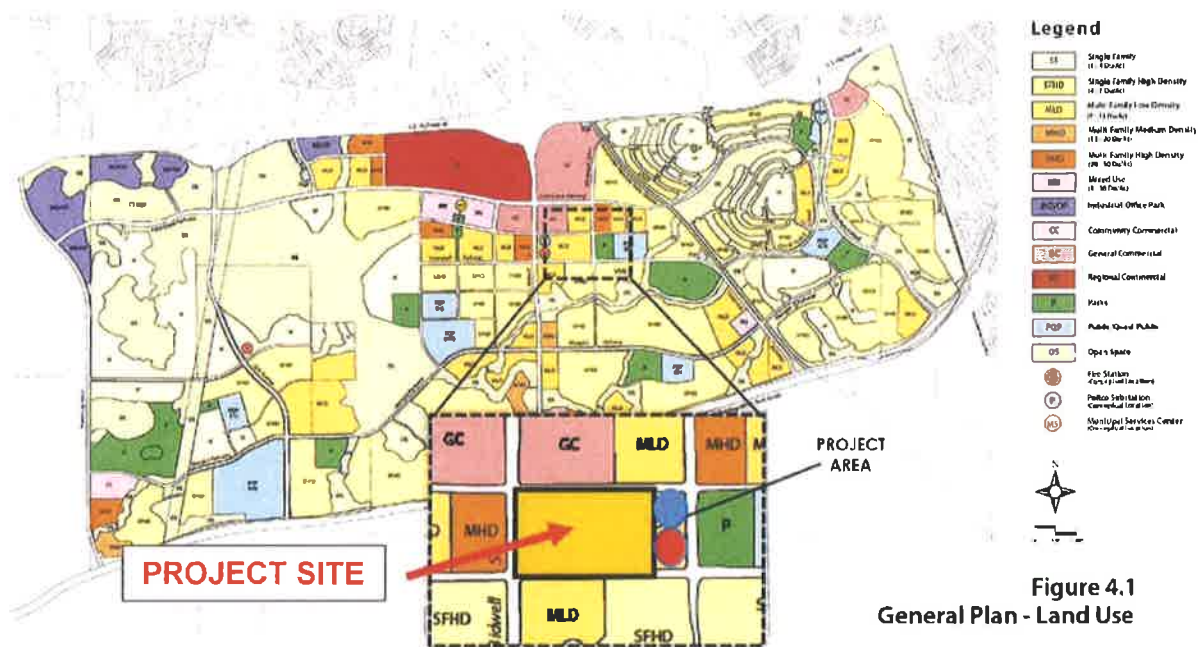


Figure 4.1  
General Plan - Land Use

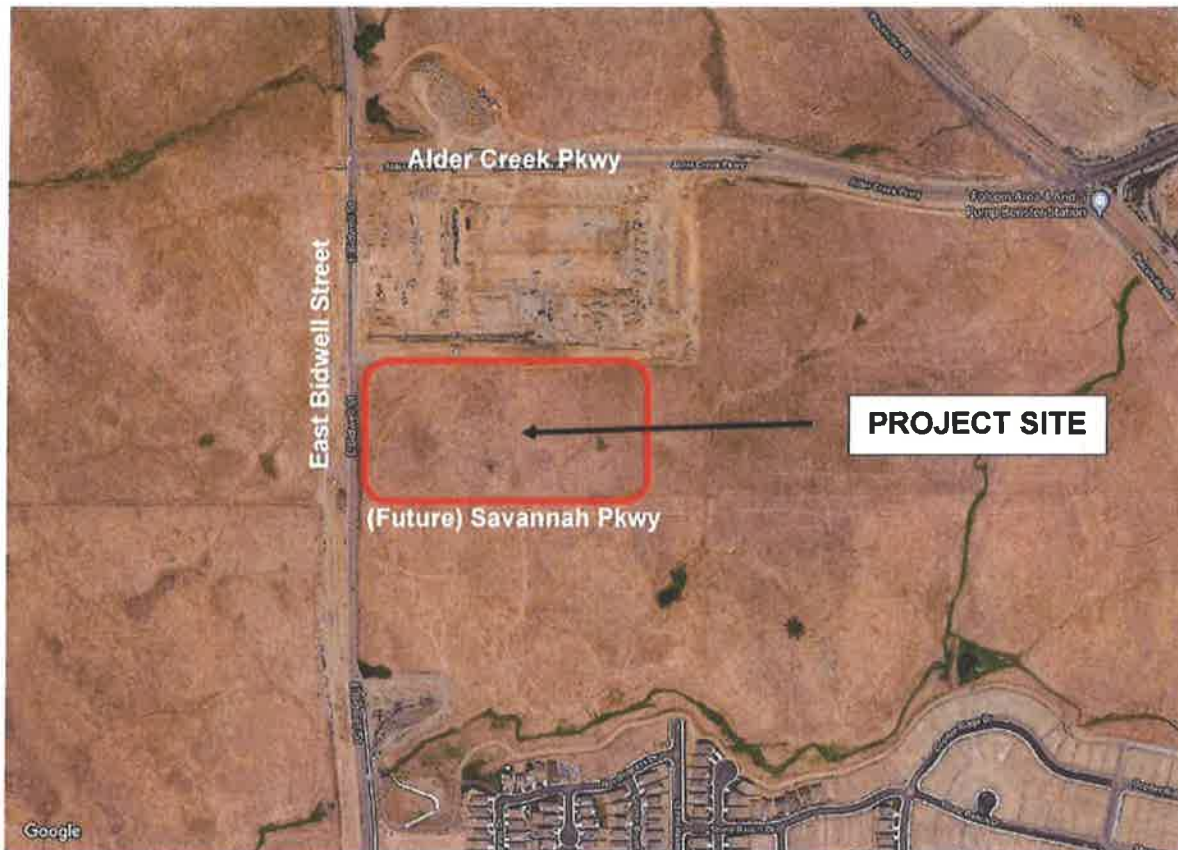
## B. Physical Setting

The project site is vacant and has been mass graded as part of the development of Mangini Ranch Phase 2 Subdivision.

Figure 2, on the following page, shows an aerial photo of the Rockcross project site. The balance of the Mangini Ranch Phase 2 Subdivision project, currently under various stages of development, is visible to the south, east, and northeast of the Rockcross Subdivision site.

As shown on the aerial photograph, pre-existing vegetation (native/non-native grasses) on the site was removed as part of the mass grading associated with the Mangini Ranch Phase 2 Subdivision project, which was conducted in accordance with mitigation measures in the FPASP EIR/EIS and monitored by the City. No oaks trees are located on the project site.

FIGURE 2: AERIAL PHOTO (2020)



**ATTACHMENT 2  
PROJECT DESCRIPTION**

**APPLICANT'S PROPOSAL**

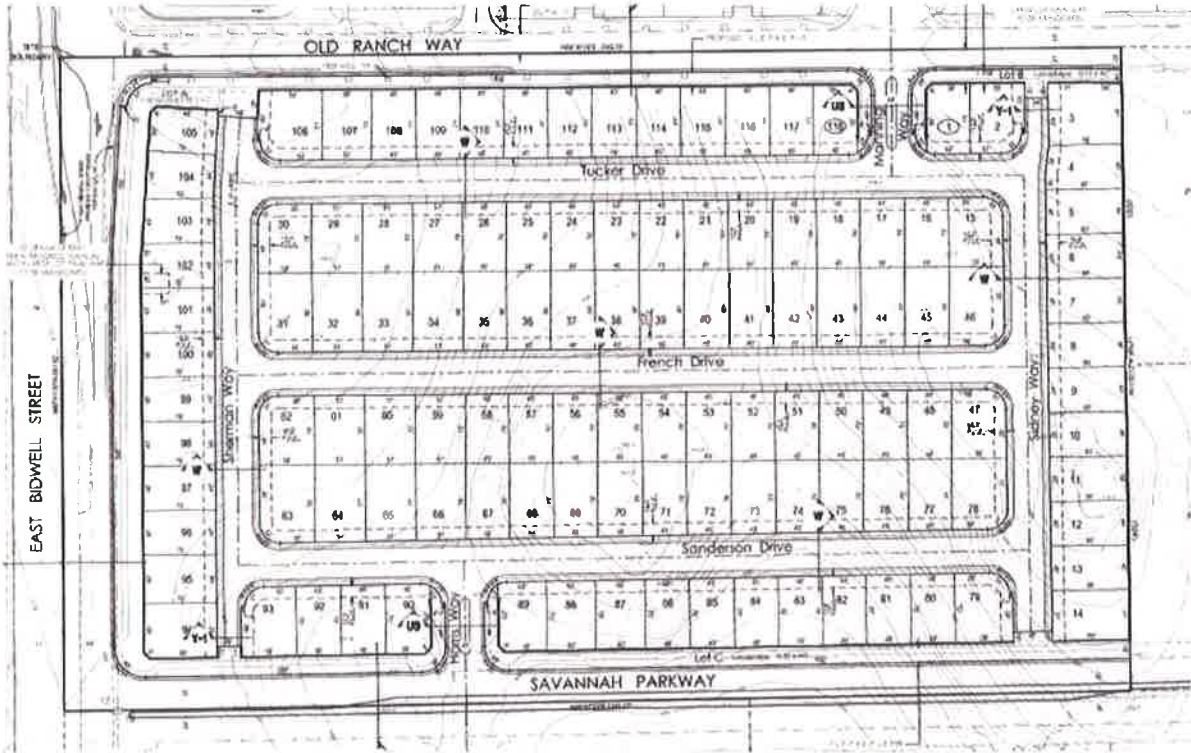
The applicant is requesting approval of several related actions to allow the development of 118 single family homes on a 14.2-acre project site. This Attachment examines the following requested approvals:

- A. Small-Lot Vesting Tentative Subdivision Map (Creation of 118 Residential Lots)
- B. Design Review (Architectural Review of Master Plans)
- C. Minor Administrative Modification (Transfer of 35 Dwelling Units)

**A. Small-Lot Vesting Tentative Subdivision Map**

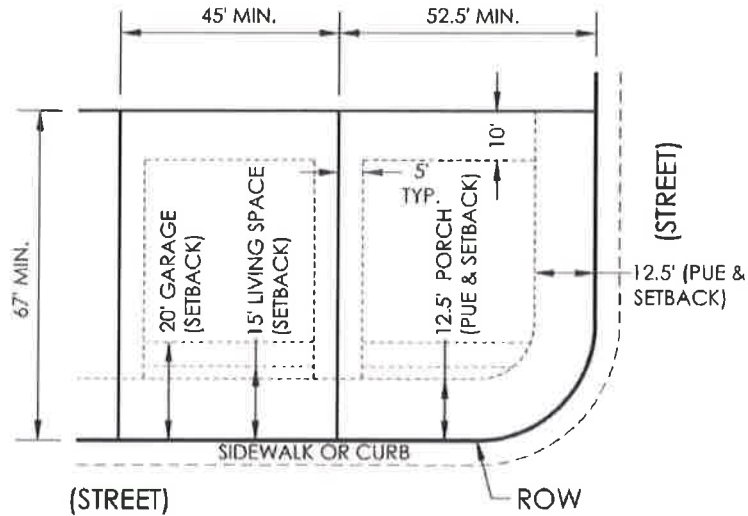
The first component of the applicant's proposal is a Small-Lot Vesting Tentative Subdivision Map to create 118 single-family residential lots and three landscape lots. The proposed subdivision layout is shown in Figure 3 on the following page. (A more detailed version of the subdivision map is included as Attachment 6 to this staff report.)

FIGURE 3: PROPOSED SUBDIVISION LAYOUT



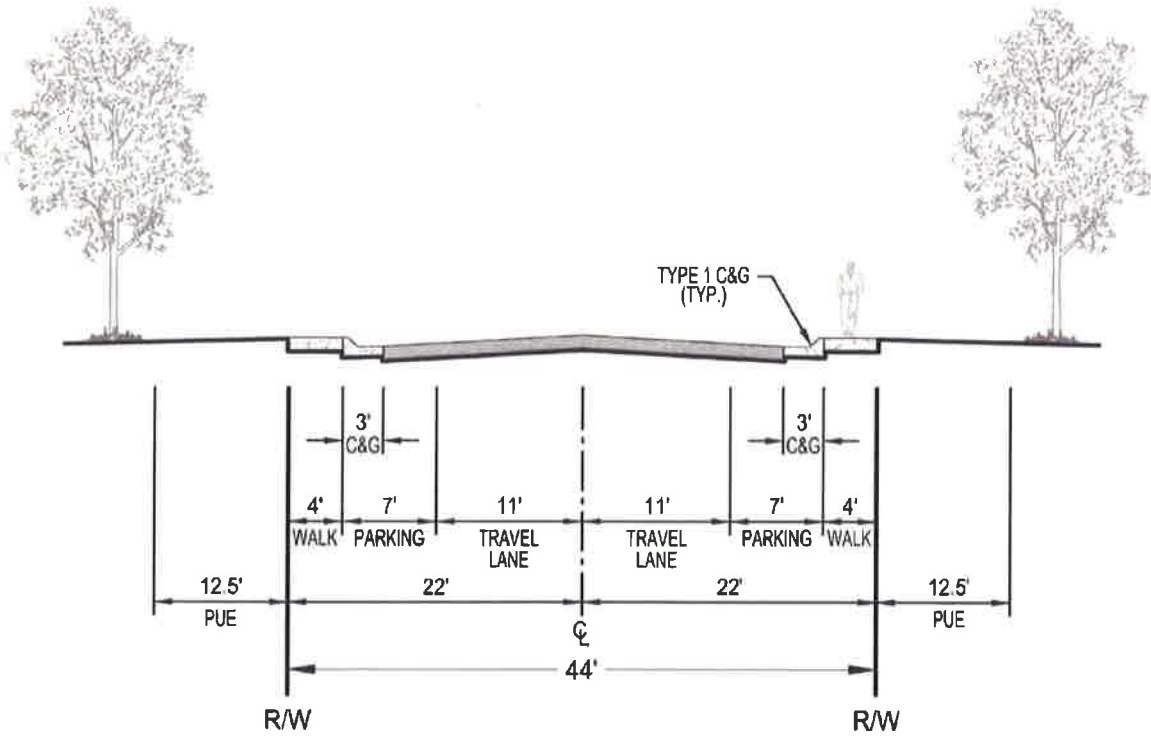
The proposed subdivision features interior lots with sizes that generally range from 3,420 square feet (45'x76') to 3,570 square feet (51'x70'). Corner lots as proposed generally range from 3,850 square feet (55'x70') to 4,720 square feet (59'x80'). All of the lots are consistent with the development standards for the MLD land use district of the FPASP. In addition, all of the lots will have a standard 12.5-foot-wide public utility easement in the front yard (and street side yard for corner lots). Proposed minimum lot sizes and dimensions are shown in Figure 4 on the following page.

FIGURE 4: PROPOSED MINIMUM LOT DIMENSIONS



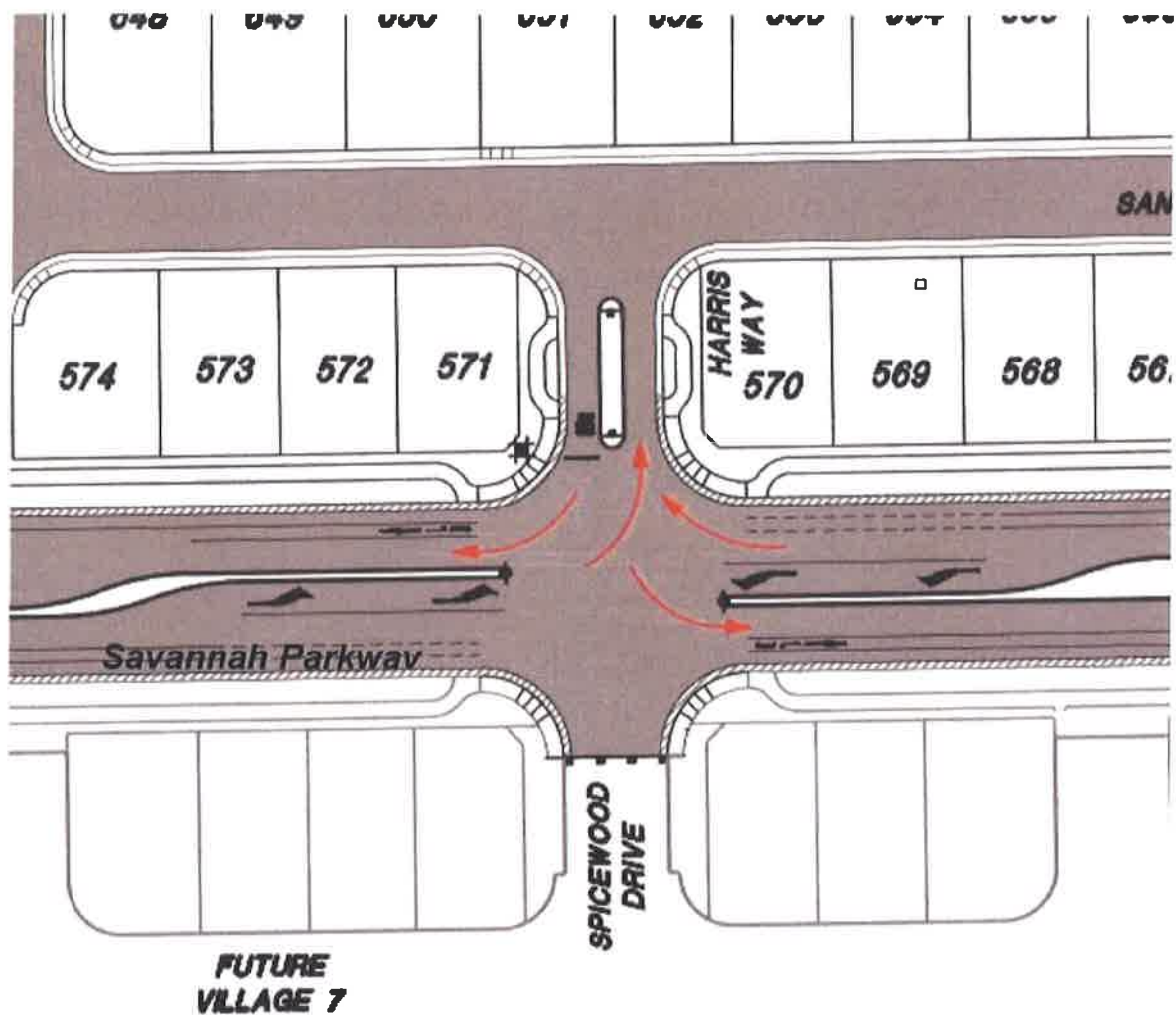
The subdivision uses standard public street right-of-way dimensions, including an internal roadway system with sidewalks on both sides of the street, as shown in Figure 5 below.

FIGURE 5: INTERNAL ROADWAY CROSS SECTION



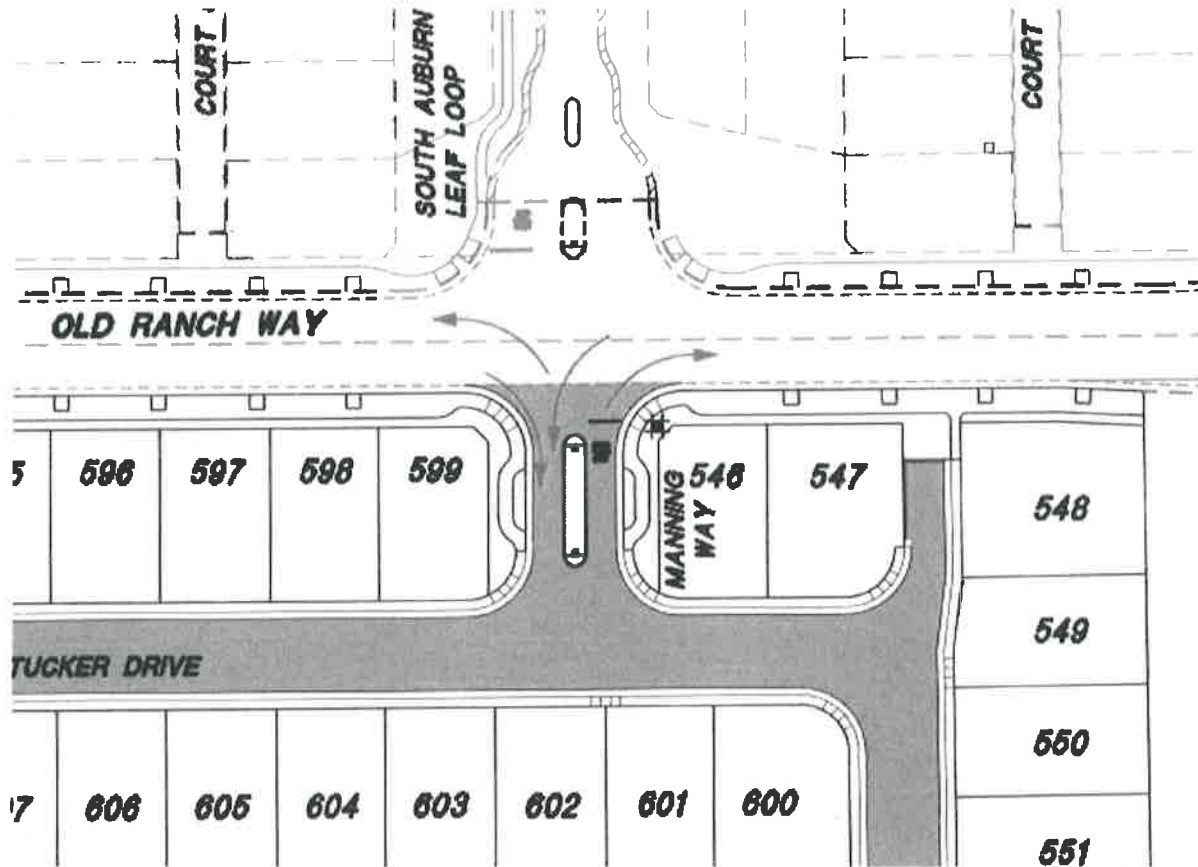
Typical residential street entries into the subdivision are provided from Savannah Parkway and Old Ranch Way. No direct access to East Bidwell Street is provided. These street entries correspond with street entries into the subdivisions to the north and south of the project site. As shown on Figure 6 below, the street entrance on Savannah Parkway will allow full turning movements, while also allowing direct access from the project site to the Mangini Ranch Village 7 Subdivision directly to the south.

FIGURE 6: SAVANNAH PARKWAY ENTRY



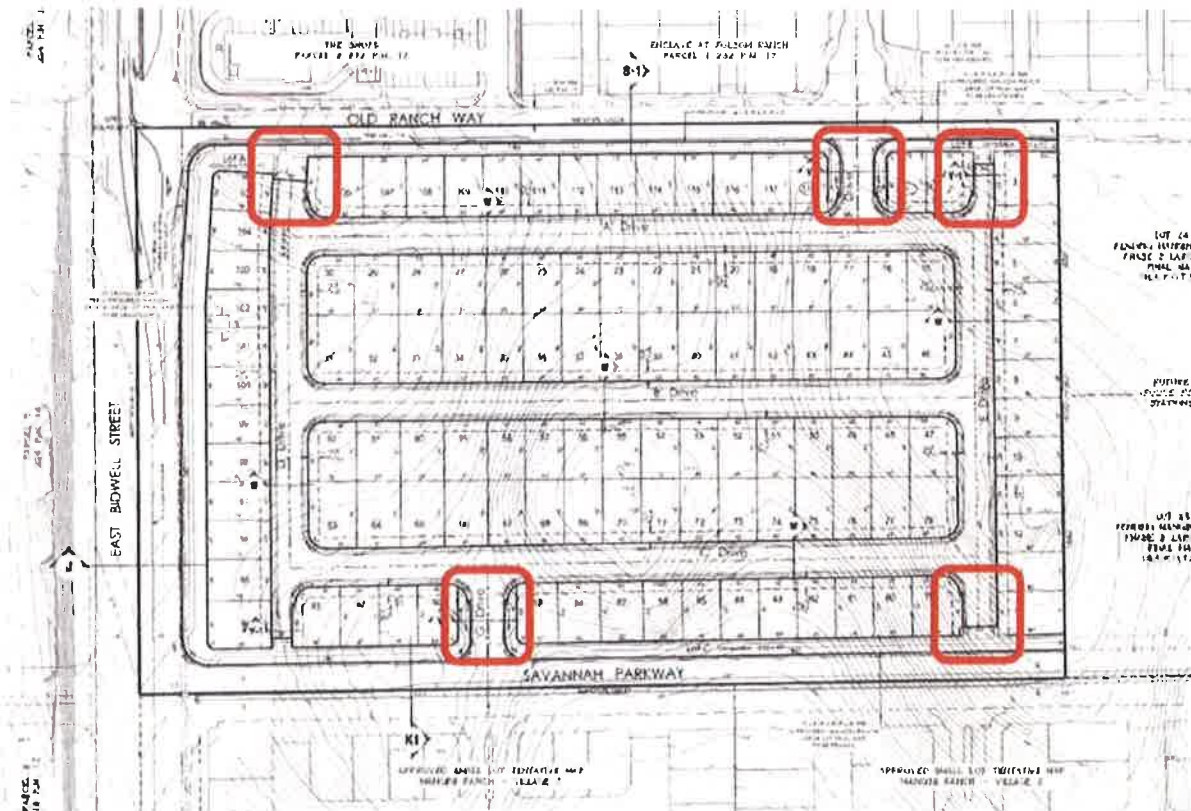
As shown on Figure 7 below, the street entrance on Old Ranch Way will allow full turning movements, while also allowing direct access from the project site to the Enclave Subdivision directly to the north.

FIGURE 7: OLD RANCH WAY ENTRY



Pedestrian access into and out of the subdivision will be provided at the two project entrances and also at pedestrian-only access points located in the northwest, northeast, and southeast corners of the project site as shown in the figure below in Figure 8. Site grading constraints do not permit pedestrian access at the southwest corner of the project site.

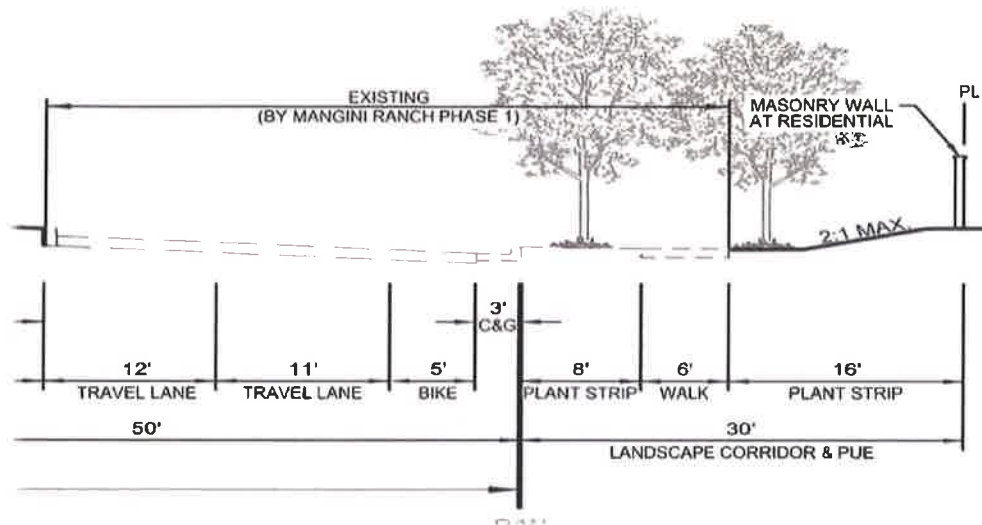
FIGURE 8: PEDESTRIAN ACCESS



In response to projected traffic levels on East Bidwell Street and to minimize potential noise impacts associated with these traffic levels, the project proposes a seven-foot-tall soundwall for the homes adjacent to this roadway, as shown in Figure 9 on the following page.



FIGURE 9: EAST BIDWELL STREET-TO-REAR YARD CROSS SECTION



## B. Design Review

The proposed project includes the construction of 118 single family homes with four different master plans, three architectural styles, and nine color schemes. All of the homes are proposed in a two-story configuration. The largest homes (Plan 4) will include a downstairs bedroom/office. In Plans 1-3, all bedrooms are on the second floor only.

Three architectural styles are proposed:

- Spanish Colonial
- Craftsman
- American Traditional

All three architectural styles are proposed to be used for all unit types, with a variety of colors and materials as shown in the applicant's bound submittal booklet (Attachment 16).

The applicant's submittal says the following about the proposed architecture:

*The three aesthetics will work collectively to create a diverse, yet unified character for the community. The three styles are interspersed throughout the plans, displaying a variety of massing, wall plane and roof configurations to establish an inviting and organic street scene. Altogether, Rockcross at Folsom Ranch's architecture will enhance the overall experience of the community through the beauty of elevation styles, details, and color palette.*

*Unique Exterior Architectural Elements – Materials have been incorporated into the front and rear exteriors that offer unique flair to the homes which in turn make this community unique:*

- *The use of arched entry ways and garage door soffits on the Spanish Colonial elevation.*
- *The use of stone veneer on the Craftsman elevation.*
- *The use of brick veneer on the American Traditional elevation.*
- *The use of board and batten combined with tapered columns on the Craftsman elevation.*
- *The use of lap siding and double porch columns on the American traditional elevation.*
- *Roof Lines – The roofs also provide visual interest to the homes utilizing hips, gables or a combination of both.*

The applicant's submittal describes the architectural styles as follows:

- *Spanish Colonial – This style is native to California with its large expanses of un-interrupted walls punctuated with judicious window placement. Often shown with head and sill trim and “clay” pipe detailing in the gables. Roof forms are a low pitch combination of hip and gable forms.*
- *Craftsman – This style home is a simple informal, efficient, and the exteriors emphasize the use of natural materials. The Craftsman style primarily exhibits horizontal floor plans, covered porches, and low-slung roof forms. True to the nature of the design, exteriors are often painted in colors found naturally in the surrounding environment. Craftsman homes are characterized by exposed or expressive structural elements like battered columns and corbels at the eaves.*
- *American Traditional - While the Traditional style is not indicative of any specific regional style or time period, it is meant as an overarching theme made up of traditional details. A deep colored base wainscot anchors the elevation. Multi-pane windows and gable end roofs round out the style.*

Illustrations of the proposed architectural styles applied to the proposed residential designs are shown on the following pages.

FIGURE 10: PLAN 1 ELEVATIONS



SPANISH COLONIAL ELEVATION



CRAFTSMAN ELEVATION



AMERICAN TRADITIONAL ELEVATION

**FIGURE 11: PLAN 2 ELEVATIONS**



**FIGURE 12: PLAN 3 ELEVATIONS**



**FIGURE 13: PLAN 4 ELEVATIONS**



Typical floorplans for each unit type are shown on the following pages. As noted earlier, only Plan 4 includes a downstairs bedroom. Responding to a variety of lifestyle preferences, some of the other plans offer an option to convert a second-floor bedroom into an open office.

FIGURE 14: PLAN 1 FLOORPLAN



FIGURE 15: PLAN 2 FLOORPLAN

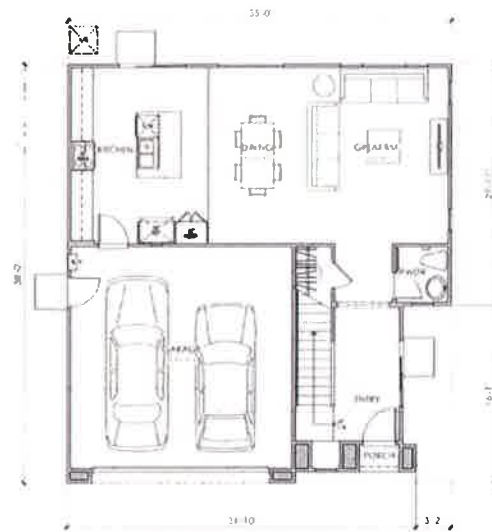
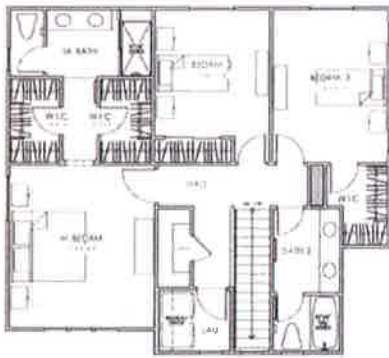


FIGURE 16: PLAN 3 FLOORPLAN

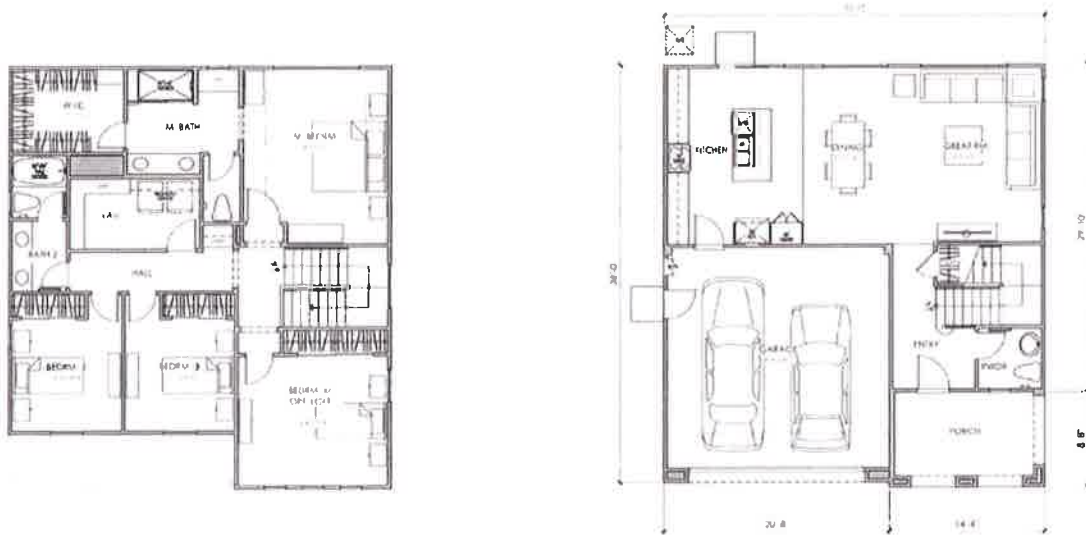
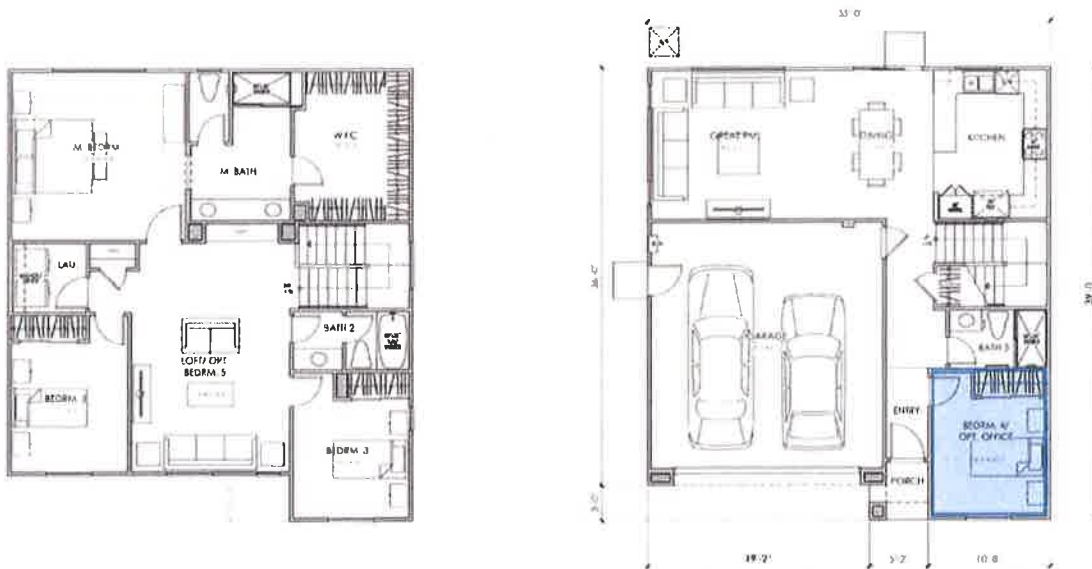


FIGURE 17: PLAN 4 FLOORPLAN



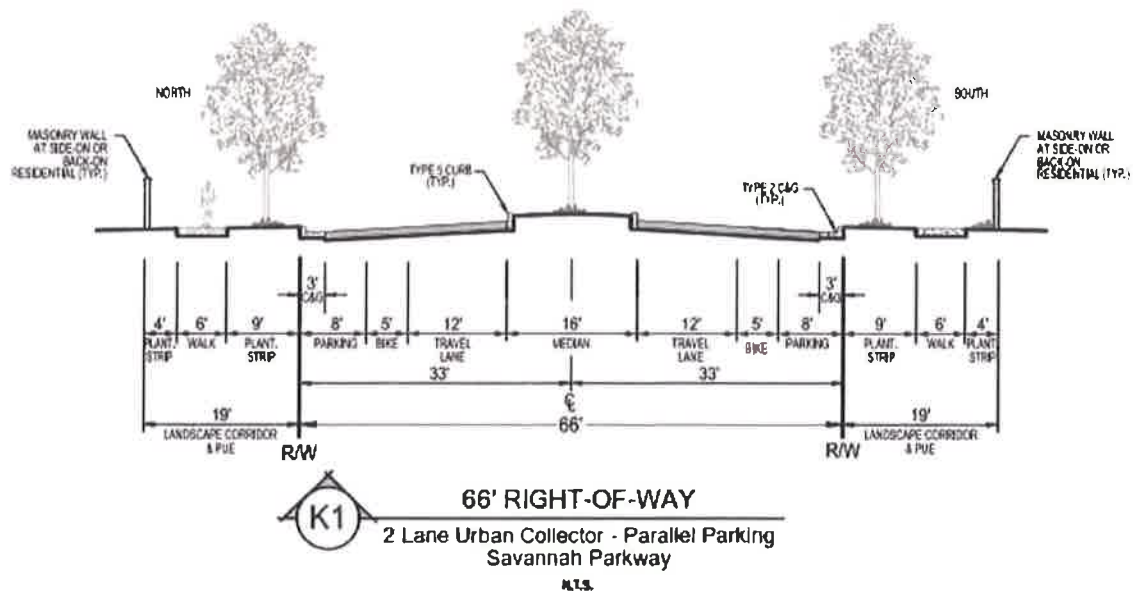
Downstairs bedroom highlighted

### Landscape Buffers and Proposed Landscaping

There is currently a designated 30-foot-wide landscape corridor located along the east side of East Bidwell Street as shown on the Small-Lot Vesting Tentative Subdivision Map (Attachment 6). The 30-foot-wide landscape buffer was established as part of the Mangini Ranch Phase 2 Subdivision project and is shown on the recorded Mangini Ranch Phase 2 Subdivision Parcel Map.

There is currently a designated 15-foot-wide landscape corridor located along the north side of Savannah Parkway. The applicant is proposing to provide an additional four feet of landscaping along Savannah Parkway to provide an additional landscape buffer between the six-foot-wide sidewalk and the six-foot-tall soundwall that will be located along the rear property line of residential lots within the subdivision. Accordingly, the existing 15-foot-wide landscape easement located along the Savannah Parkway frontage is being widened to 19 feet as shown on the Small-Lot Vesting Tentative Subdivision Map. A cross-section of Savannah Parkway is shown in Figure 18 below illustrating the location of landscaping, sidewalk, and soundwall.

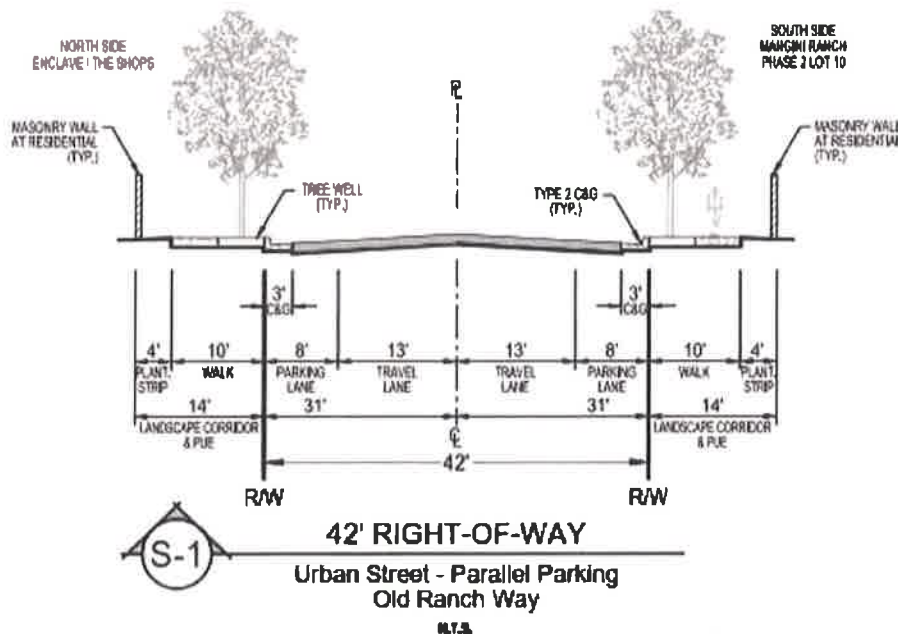
FIGURE 18: SAVANNAH PARKWAY CROSS SECTION





There is currently a designated 10-foot-wide landscape corridor located along the south side of Old Ranch Way that is designed to include a 10-foot-wide sidewalk interspersed with tree wells to accommodate tree plantings and ornamental tree grates. The applicant is proposing to provide an additional four feet of landscaping along Old Ranch Way to provide an additional landscape buffer between the ten-foot-wide sidewalk and the six-foot-tall soundwall that will be located along the rear property line of residential lots within the subdivision. Accordingly, the existing 10-foot-wide landscape easement located along the Old Ranch Way frontage is being widened to 14 feet as shown on the Small-Lot Vesting Tentative Subdivision Map. A cross-section of Old Ranch Way is shown in Figure 19 below illustrating the location of landscaping, sidewalk, and soundwall.

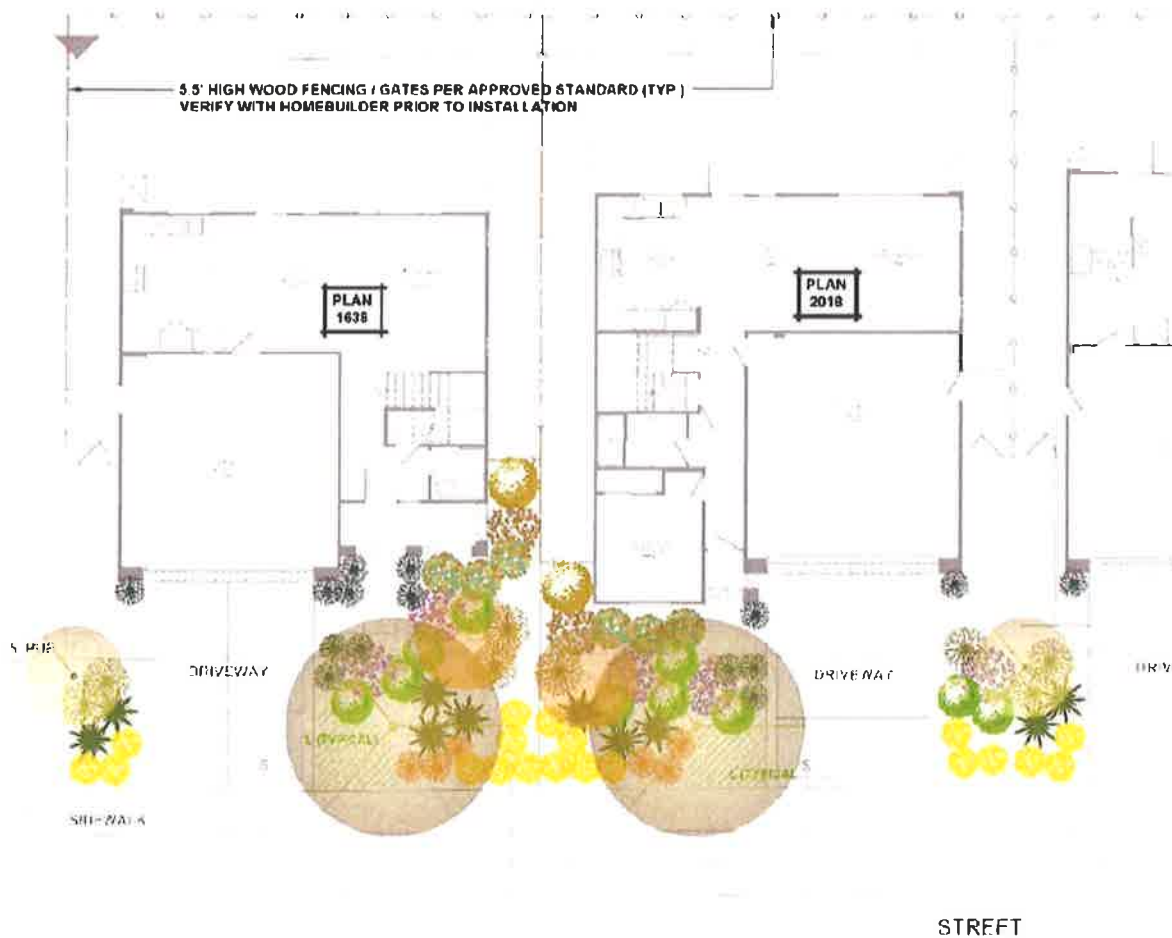
FIGURE 19: OLD RANCH WAY CROSS SECTION



Landscaping installed along three of the project's perimeters (East Bidwell Street, Savannah Parkway, and Old Ranch Way) as described above will be required to be installed per City standards to match already installed landscaping along street corridors within the Folsom Plan Area. The eastern boundary of the subdivision, which is adjacent to future fire and police station sites, will include a six-foot tall masonry and landscaping will be provided by homeowners in the rear yards of the individual homes.

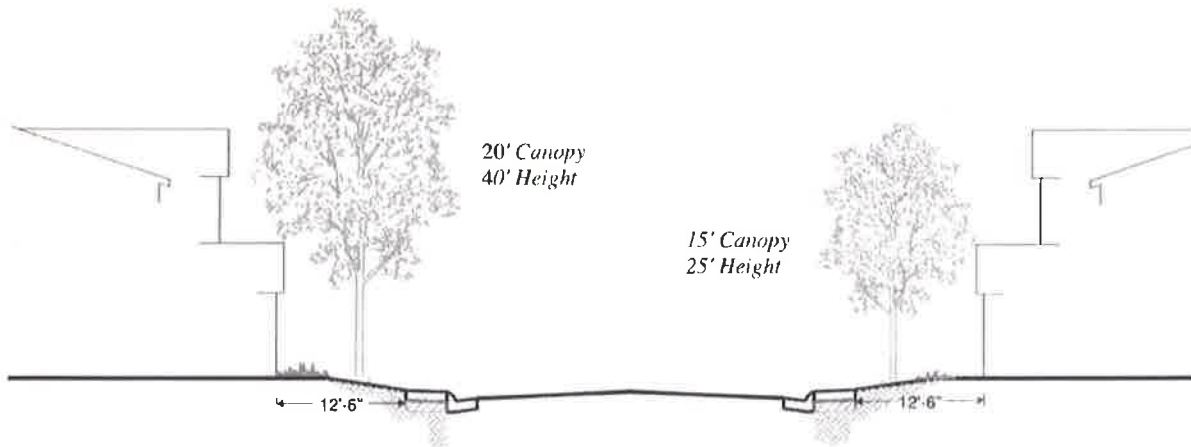
The applicant is proposing to install new landscaping in the front yards and street side yards of the new homes within the subdivision. Homeowners will be responsible for landscaping the rear yards of the individual homes. Front yard landscaping has been designed by the applicant to complement the proposed architecture and to work within the front yard areas available. Front and rear yard landscaping will be maintained by the individual homeowners. An illustration of proposed front yard landscaping is shown in Figure 20 on the following page:

FIGURE 20: FRONT YARD LANDSCAPING



The applicant has discussed appropriate tree species with the City's Arborist and has selected a list of trees which will fit within space available (shown on the following page). The proposed tree list is included in the applicant's submittal booklet, attached to this staff report (Attachment 16).

FIGURE 21: TREES IN FRONT YARD AREAS



Selected trees for the front yard areas include:

- *Acer buergerianum* "Trident Maple"
- *Koelreuteria paniculata* "Goldenrain Tree"
- *Chionanthus retusus* "Chinese Fringe Tree"
- *Magnolia grandiflora* "Company Southern Magnolia"
- *Pyrus kawakamii* "Evergreen Flowering Pear"
- *Prunus caroliniana* "Dwarf Carolina Cherry Laurel"

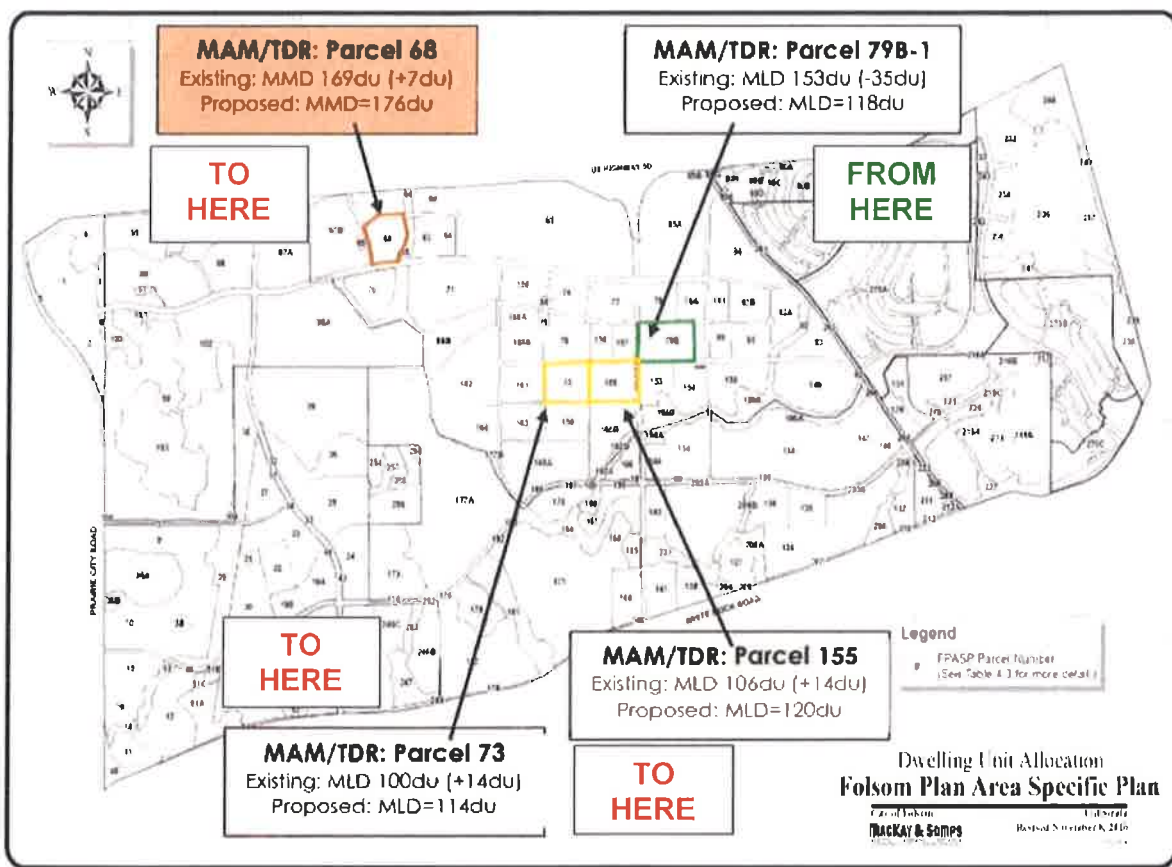
All of these trees have either a relatively small canopy size (e.g., the Trident Maple) or have a tall, vertical form (e.g., the yew pine) that will fit in the proposed front yard areas. Due to their size, these species are more commonly used as "accent" trees in a palette that includes larger "canopy" trees when enough space is available.

### C. Minor Administrative Modification

The parcel (Parcel 79B) on which the Rockcross Subdivision project is located is designated by the FPASP for the development of 153 residential units on formerly 17.2-acres. The Mangini Ranch Phase 2 Tentative Map, and recorded Final Map, set aside 3 acres of lands on the east side of the subject parcel for future police and fire stations, as per the Folsom Plan Area Specific Plan, which has reduced the residential area of the subject parcel to 14.2-acres while the allocated dwelling units remained unchanged. Largely due to the reduction in land area, the proposed site plan achieves 118 residential units on the subject parcel, and a Minor Administrative Modification is proposed to reallocate the 35 unallocated residential units to three other sites (Parcels 68, 73, and 155) within the Folsom Plan Area. These other three sites or parcels have not been mapped, and no development applications are currently on file with the City.

Parcel 68 is designated MMD and Parcels 73 and 155 are designated MLD by the FPASP (Rockcross Subdivision parcel is designated MLD). The increase in the number of units allocated to these sites (7 units added to Parcel 68, 14 units added to Parcel 73, and 14 units added to Parcel 155) would not require a change in the land use designation for any of the three sites as each parcel has available capacity to accept additional units. The Rockcross Subdivision site and the proposed locations (all of which are under the same ownership group/East Carpenter Improvement Company., LLC) for the reallocated residential units are shown in Figure 22 below.

FIGURE 22: PROPOSED REALLOCATION OF 35 DWELLING UNITS



**ATTACHMENT 3  
ANALYSIS**

The following sections provide an analysis of the applicant's proposal. Staff's analysis addresses the following:

- A. Small-Lot Vesting Tentative Subdivision Map to subdivide the 14.2-acre project site into 118 residential lots.
- B. Design Review (Architectural Review of Master Plans)
- C. Traffic/Access/Circulation
- D. Parking
- E. Noise Impacts
- F. Walls/Fencing
- G. Inclusionary Housing
- H. Frontage Improvements
- I. Minor Administrative Modification (Shift of Dwelling Units to Other Parcels)

This section also includes a discussion of the project's performance with relation to relevant policies in the Folsom General Plan and the Folsom Plan Area Specific Plan:

- J. Conformance with Relevant Folsom General Plan Folsom Plan Area Specific Plan Objectives and Policies

**A. Small Lot Vesting Tentative Subdivision Map**

As shown on the submitted Small-Lot Vesting Tentative Subdivision Map (Attachment 6), the proposed subdivision includes 118 single family residential lots, three landscape lots, and seven internal public streets (French Drive, Harris Way, Manning Way, Sanderson Drive, Sherman Way, Sidney Way, and Tucker Drive). The proposed project will be required to dedicate public right-of-way for the internal public streets. The project is not required to dedicate any additional public right-of-way along East Bidwell Street, Savannah Parkway, or Old Ranch Way as the right-of-way for these three roadways has previously been dedicated. As shown on the Subdivision Map, the applicant is also proposing to expand an existing landscape easements located along the Savannah Parkway frontage (15 feet to 19 feet), and the Old Ranch Way frontage (10 feet to 14 feet) to accommodate additional landscaping.

As mentioned previously, all roadways within the subdivision are proposed to be public streets. As a result, staff has included a condition (Condition No. 42) that requires the applicant to dedicate public utility easements for underground facilities (i.e., SMUD, Pacific Gas and Electric, cable television, telephone) on properties adjacent to the streets.

Staff has determined that the proposed Small-Lot Vesting Tentative Subdivision Map complies with all City requirements, as well as with the requirements of the State Subdivision Map Act.

The proposed street names (French Drive, Harris Way, Manning Way, Sanderson Drive, Sherman Way, Sidney Way, and Tucker Drive) for the subdivision were selected from the Historic Street Name List and from the approved Mangini Ranch Phase 1 Subdivision Street Name List. Street names were not selected from the Mangini Ranch Phase 2 Subdivision Street Name List as all of those street names had been utilized. City staff reviewed the proposed street names in coordination the postal service, the Police Department, and the Fire Department, and determined that the names are acceptable.

As noted earlier within this staff report, the proposed project conforms to all development standards established by the FPASP for the MLD land use category including minimum lot size, maximum lot coverage, and setbacks as shown in the table below. No deviations from these standards are proposed by the applicant.

FIGURE 23: SP-MLD Development Standards Table

<b>SP-MLD Single Family High Density Development Standards Table</b>		
<b>Development Standard</b>	<b>Requirement</b>	<b>Proposed Project</b>
Front Porch Setback	12.5 Feet	12.5 Feet
Front Primary Structure Setback	15 Feet	15 Feet
Front Garage Setback	20 Feet	20 Feet
Side Yard Setbacks	5 Feet/5 Feet	5 Feet/5 Feet
Rear Yard Setback	10 Feet	10 Feet
Maximum Lot Coverage	50%	50%

#### **B. Design Review (Architectural Review of Master Plans)**

The following are discussed below:

- Proposed Residential Designs
- Proposed Landscaping

These are discussed below.

##### Proposed Residential Designs

The proposed project is located within the central portion of the Folsom Plan Area; thus, it is subject to the Folsom Ranch Central District Design Guidelines (Attachment 19), which were approved by the City Council in 2015, and amended in 2018. The Design

Guidelines are a complementary document to the Folsom Plan Area Specific Plan and the Folsom Plan Area Specific Plan Community Guidelines.

The Design Guidelines, which are intended to act as an implementation tool for residential development within the Central District of the Folsom Plan Area, provide the design framework for architecture, street scene, and landscaping to convey a master plan identity. The Design Guidelines also establish the pattern and intensity of development for the Central District to ensure a high quality and aesthetically cohesive environment. While these Design Guidelines establish the quality of architectural and landscape development for the master plan, they are not intended to prevent alternative designs and/or concepts that are compatible with the overall project theme.

As a regulatory tool, the Design Guidelines are intended to assist applicants in creating single-family residential neighborhoods that reflect the City's rich history, reinforce the sense of community, and utilize sustainable best practices. The Design Guidelines also provide the framework for design review approval of Folsom Ranch, Central District residential projects. In addition, the Design Guidelines are intended to be used by builders and developers when designing their Master Plot Plans. Any development project that is submitted to the City must be reviewed for consistency with these Design Guidelines.

The following are the general architectural principles intended to guide the design of the Folsom Ranch, Central District to ensure quality development:

- Provide a varied and interesting street scene
- Focus of the home is the front elevation, not the garage
- Provide a variety of garage placements
- Provide detail on rear elevations where visible from the public streets
- Choose appropriate massing and roof forms to define the architectural styles
- Ensure that plans and styles provide a degree of individuality
- Use architectural elements and details to reinforce individual architectural styles

In addition to the general architectural principles referenced previously, the Design Guidelines also provide specific direction regarding a number of architectural situations and features including: edge conditions, corner buildings, building forms, off-set massing forms, front elevations, roof forms, feature windows, architectural projects, balconies, lower height elements, garage door treatments, outdoor living spaces, exterior structures, building materials, and color criteria. The following are examples of architectural situations and features that are relevant to the proposed project:

- Provide a mix of hip and gable roof forms along the street scene

- Provide off-set massing, forms, or wall planes
- Provide recessed second-story elements
- Provide enhanced style-appropriate details on the front building elevation
- Provide decorative window shelves or sill treatments
- Provide architectural projections (recessed windows, eaves, shutters, etc.)
- Provide garage doors that are consistent with the architecture of the building
- Provide variety in the garage door patterns
- Provide outdoor living spaces (porches, balconies, courtyards, etc.)

As mentioned above, the Design Guidelines provide specific direction regarding “edge conditions” within a subdivision. Edge conditions refer to the rear and side building elevations of a home that are visible from open spaces and major roadways. The Design Guidelines require that specific homes within a subdivision that meet the definition of an “edge condition” lot are required to incorporate enhanced architectural details on the rear and side building elevations, similar to the enhanced architectural details provided on the front building elevation of the home. The exhibit below shows the individual lots within the Rockcross Subdivision that are considered “edge condition” lots.

FIGURE 24: EDGE CONDITION (ENHANCED) LOT EXHIBIT





The architectural design styles selected for the Folsom Ranch Central District have been chosen from the traditional heritage of California home styles, a majority of which have been influenced by the Spanish Mission and Mexican Rancho eras. Over the years, architectural styles in California have become reinterpreted traditional styles that reflect the indoor-outdoor lifestyle choices available in the Mediterranean climate. Suggested architectural styles in the Design Guidelines include American Traditional, Craftsman, Early California Ranch, European Cottage, Italian Villa, Monterey, Spanish Colonial, and Western Farmhouse. Additional architectural styles compatible with the intent of the Design Guidelines may be added if they are regionally appropriate.

As discussed earlier, the applicant has provided proposed architectural designs for the homes to be built in the Rockcross subdivision. As described in the applicant's proposal, the proposed project features three architectural styles:

- Spanish Colonial
- Craftsman
- American Traditional

In evaluating the proposed project, staff also took into consideration building and design elements that could be considered unique to the Folsom Plan Area. Staff has determined that the proposed master plans do include many unique building and design elements and are consistent with the Folsom Ranch Design Guidelines. Based on this analysis, staff forwards the following design recommendations to the Commission for consideration:

1. This approval is for one product line with four two-story master plans in three architectural styles with nine color and material options. The applicant shall submit building plans that comply with this approval and the attached building elevations dated June 17, 2020.
2. The design, materials, and colors of the single-family residential units shall be consistent with the approved building elevations, materials samples, and color schemes to the satisfaction of the Community Development Department.
3. The Community Development Department shall approve the individual lot permits to assure no duplication or repetition of the same house, same roof-line, same elevation style, side-by-side, or across the street from each other.
4. All mechanical equipment shall be ground-mounted and concealed from view of public streets, neighboring properties and nearby higher buildings.

5. Decorative light fixtures, consistent with the Folsom Ranch Central District Design Guidelines and unique to each architectural design theme, shall be added to the front elevation of each Master Plan to the satisfaction of the Community Development Department.
6. A minimum of one street tree shall be planted in the front yard of each residential lot within the subdivision. A minimum of two trees are required along the street-side of all corner lots. All front yard irrigation and landscaping shall be installed prior to a Building Permit Final.

These recommendations listed above are included in the conditions of approval presented for consideration by the Planning Commission (Condition No. 51).

#### **D. Traffic/Access/Circulation**

The Folsom Plan Area Specific Plan established a series of plans and policies for the circulation system within the entire Plan Area. The FPASP circulation system was designed with a sustainable community focus on the movement of people and provides a number of mobility alternatives such as walking, cycling, carpooling, and viable forms of public transportation in addition to vehicular circulation. The circulation plan evaluated regional travel, both in terms of connectivity and capacity as well as local internal connections and access. The circulation plan also addressed the concerns of regional traffic, including parallel capacity to U.S. Highway 50, and connectivity with surrounding jurisdictions while considering community-wide connectivity, alternative modes of travel, and the provision of complete streets.

The 2011 Folsom Plan Area Specific Plan Environmental Impact Report/Environmental Impact Statement included not only a detailed analysis of traffic-related impacts within the Plan Area, but also an evaluation of traffic-related impacts on the surrounding communities. In total, there are fifty-five (55) traffic-related mitigation measures associated with development of the FPASP which are included as conditions of approval for the Rockcross Subdivision project. Many of these mitigation measures are expected to reduce traffic impacts to East Bidwell Street. Included among the mitigation measures are requirements to; fund and construct roadway improvements within the Plan Area, pay a fair-share contribution for construction of improvements north of U.S. Highway 50, participate in the City's Transportation System Management Fee Program, and Participate in the U.S. Highway 50 Corridor Transportation Management Association. The Rockcross Subdivision project is subject to all traffic-related mitigation measures required by the 2011 FPASP EIR/EIS (Condition Nos 53-25 to 53-79).

On December 1, 2017, Kimley Horn completed a Traffic Impact Analysis (included in the attachments to the CEQA Exemption Analysis, included as Attachment 12 to this staff report) for the Mangini Ranch Phase 2 Subdivision project (proposed project is located within Village 10 of the Mangini Ranch Phase 2 Subdivision) to determine whether additional impacts would occur that were not previously identified and addressed by the

## 2011 FPASP EIR/EIS.

The Kimley Horn Traffic Impact Analysis analyzed traffic operations at twenty-one street intersections, three arterial roadway segments, and eight freeway on/off-ramp segments. The Analysis identified five deficient study intersections (East Bidwell Street/Iron Point Road, East Bidwell Street/White Rock Road, White Rock Road/Placerville Road, East Bidwell Street/Alder Creek Parkway, and East Bidwell/Street/Savannah Parkway). To address these deficiencies, the Analysis included a number of recommendations (included as Conditions of Approval for the Mangini Ranch Phase 2 Subdivision project) to reduce the identified impacts to a less than significant level.

As shown on the submitted Small-Lot Vesting Tentative Subdivision Map (Attachment 6), access to the project site is provided by a new driveway on the south side of Old Ranch Way (Manning Way) and a new driveway on north side of Savannah Parkway (Harris Way). Internal circulation is facilitated by seven public streets (French Drive, Harris Way, Manning Way, Sanderson Drive, Sherman Way, Sidney Way, and Tucker Drive). that provide circulation throughout the project site.

On May 12, 2020, Kimley Horn completed a Supplemental Access and Circulation Analysis (included as Attachment 13 to this staff report) that evaluated specific access and circulation related issues associated with the proposed project under two different scenarios (Scenario 1 and Scenario 2). Scenario 1 is a condition that assumes that the Enclave Subdivision (north of project site) project roadway improvements have been constructed and that the Mangini Ranch Village 7 project (south of project site) roadway improvements have not been constructed, while Scenario 2 is a condition that assumes Enclave Subdivision project and Mangini Ranch Village 7 roadway improvements have all been constructed. As it relates to the proposed project, the Enclave Subdivision includes improvements to East Bidwell Street and Old Ranch Way, while Mangini Ranch Village 7 includes improvements to East Bidwell Street and a portion of Savannah Parkway.

With respect to project access, the Analysis determined that the Old Ranch Way and Savannah Way project driveways will accommodate all turning movements into and out of the project site. In terms of access at the intersection of Old Ranch Way and East Bidwell Street, the Analysis determined that this intersection should be limited to right-turns in, right-turns out, and left-turns in. With respect to access at the intersection of Savannah Parkway and East Bidwell Street, the Analysis determined that this intersection should allow full turning movements. Figure 25 on the following page illustrates access for the proposed subdivision.

FIGURE 25: ACCESS AND CIRCULATION EXHIBIT



The following are recommendations from the Supplemental Access and Circulation Analysis which have been included as a condition (Condition Nos. 49-50) of approval for the Rockcross Subdivision project.

**Condition No. 49:**

**Scenario 1 (Enclave Subdivision Improvements Completed/Mangini Ranch Village 7 Subdivision Improvements Not Completed)**

- The owner/applicant shall construct a southbound median left turn pocket on East Bidwell Street with a minimum storage length of 315 feet (255-foot deceleration lane plus 60-foot taper) to provide left turn access to Savannah Parkway.

- The owner/applicant shall construct Savannah Parkway from East Bidwell Street to the eastern boundary of the Rockcross Subdivision and the provide a temporary U-Turn at the eastbound intersection of Savannah Parkway and Shale Rock Way (Mangini Ranch Village 2) until such time that the segment of Savannah Parkway between Shale Rock Way and Westwood Drive is completed and Westwood Drive is completed between Savannah Parkway and Alder Creek Parkway.

Scenario 2 (Enclave/Mangini Ranch Village 7 Subdivision Improvements Completed)

- The owner/applicant shall construct the eastern extension of Savannah Parkway from the Mangini Ranch Village 7 Subdivision boundary to the eastern boundary of the Rockcross Subdivision (including the Shale Rock Way intersection).

Condition No. 50:

Until such time that a traffic signal is required (issuance of 496<sup>th</sup> building permit within Mangini Ranch Phase 2 Subdivision project) at the East Bidwell Street/Savannah Parkway intersection, the owner/applicant shall construct a southbound median acceleration lane to assist in facilitating a two-stage outbound left-turn lane from Savannah Parkway onto southbound East Bidwell Street. The length of this lane, which is understood to be a temporary improvement that is repurposed with the ultimate East Bidwell Street corridor improvements, should total approximately 250 feet.

**D. Parking**

The Folsom Plan Area Specific Plan requires that single-family residential units located within a Multi-Family Low Density (MLD) designated area provide two covered parking spaces per unit. The FPASP also requires that single-family residential units located within an MLD designated area provide a minimum of 0.8 guest parking spaces per unit.

As shown on the submitted residential schematic design (Attachment 10), each of the homes will include a two-car attached garage, thus meeting the covered parking requirement of the FPASP. In addition, the project provides 118 on-street parking spaces (one space per unit), which exceeds the minimum of 0.8 on-street guest parking spaces required by the FPASP.

**E. Noise Impacts**

A Noise Assessment (Attachment 14) was prepared by Bollard Acoustical Consultants on April 24, 2020 to determine whether East Bidwell Road/Savannah Parkway/Old Ranch Way traffic-related noise and future fire/police station-related noise would cause noise levels at the project site to exceed acceptable limits as described in the Noise Element of the City of Folsom General Plan, and to evaluate compliance with the Folsom South of

## U.S. Highway 50 Specific Plan EIR Noise Mitigation Measures.

### Outdoor Noise Levels

The noise analysis projected noise levels adjacent to these roadways (based on future traffic levels) and adjacent to the future fire/police station sites (based on operational characteristics) and determined what types of measures would be needed to ensure that noise levels at homes adjacent to the roadways and fire/police station sites would not exceed City standards, which are:

- 60 dB  $L_{dn}$ <sup>1</sup> for outdoor activity areas (such as rear yards)
- 45 dB  $L_{dn}$  for interior areas in dwellings

The noise analysis concluded that, *without mitigation*, noise levels along East Bidwell Street would reach 68 dB  $L_{dn}$  in the rear yards of homes, and 64 dB  $L_{dn}$  in the rear yards of homes along Savannah Parkway and Old Ranch Way. These levels exceed the City's standard for outdoor activity areas.

However, the noise analysis also concluded that the installation of a 7-foot-high masonry wall along the East Bidwell Street frontage and a 6-foot-high masonry wall along the Savannah Parkway and Old Ranch Way frontages would reduce rear yard noise levels to 60 dB  $L_{dn}$  on East Bidwell Street and <60 dB  $L_{dn}$  on Savannah Parkway and Old Ranch Way, which would comply with the City's outdoor noise level standard. It is important to note that the noise analysis assumed that the rear yard elevations of homes adjacent to East Bidwell Street would be a minimum of three feet above the elevation of East Bidwell Street. As shown on the preliminary grading plan (Attachment 7), the rear yard elevations of homes adjacent to East Bidwell Street range from 3 to 9 feet above the elevation of East Bidwell Street.

The properties directly to the east of the project site are designated for future fire and police stations. Noise from fire and police station operations are exempt from the provisions of the City of Folsom noise standards as that noise (i.e. sirens, vehicles responding to calls, etc.) falls under the category of emergency operations. Nonetheless, the operation of those future facilities could result in periodic periods of elevated noise levels. To minimize potential noise impacts generated by the future fire/police station site, the applicant is proposing to install a six-foot-high masonry wall along the rear of all residential lots on the eastern property boundary.

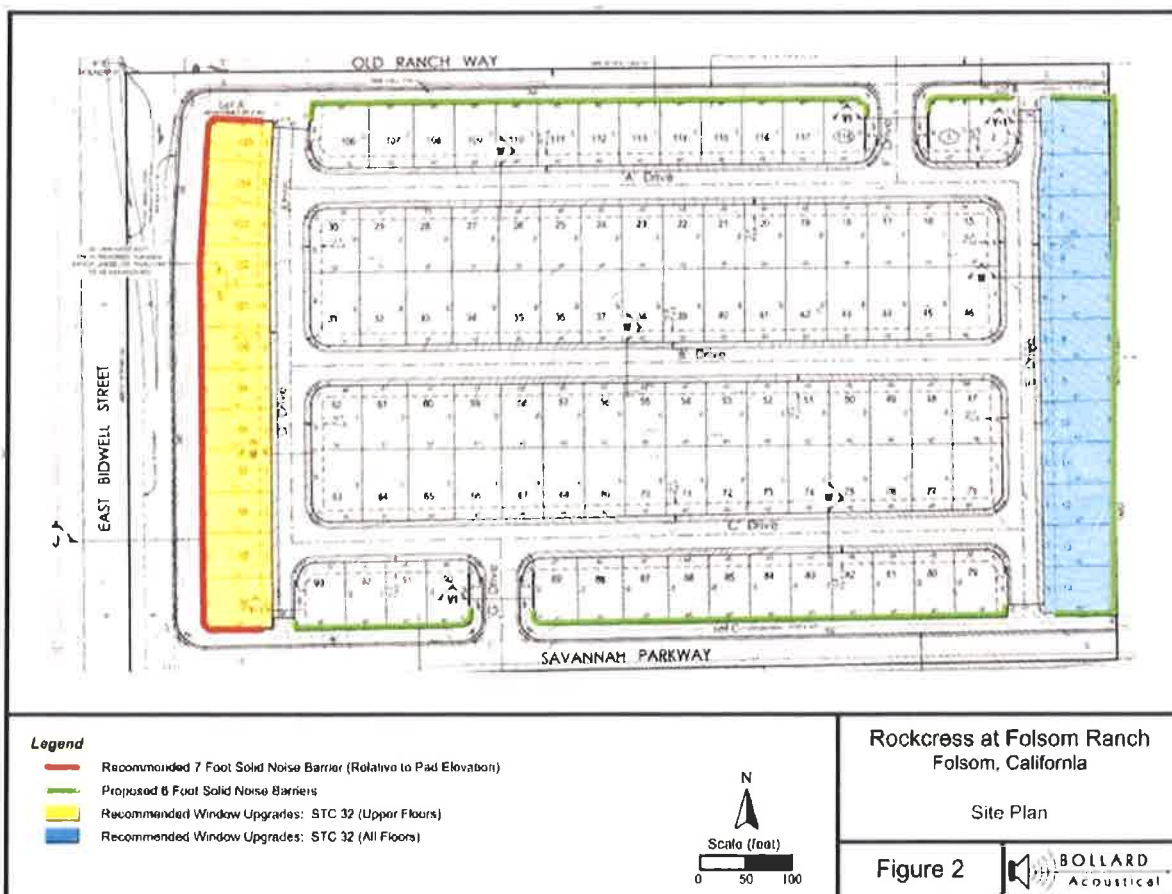
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<sup>1</sup> dB  $L_{dn}$  is average noise level over a 24-hour day, measured in decibels (dB). The average includes a +10 decibel weighing applied to noise occurring during nighttime (10:00 p.m. to 7:00 a.m.) hours.

### Interior Noise Levels

The noise analysis concluded, based on projected noise adjacent to the nearby roadways, that standard residential construction (including STC 32 window assemblies on the second floor of units adjacent to East Bidwell Parkway) would reduce interior noise levels to acceptable levels. The noise analysis also recommended that standard residential construction (including STC 32 window assemblies) be utilized on the first and second floor of units adjacent to fire/police station sites to reduce interior noise levels. In addition, the noise analysis recommended that a disclosure statement be provided to all prospective residents within the Rockcross Subdivision notifying them that future fire/police stations are planned to be development adjacent to the project site, and that operations of these facilities could periodically result in elevated noise levels. A map of the noise analysis recommendations is shown in Figure 26 below.

FIGURE 26: RECOMMENDED NOISE WALL/WINDOW ASSEMBLY LOCATIONS



## **F. Walls/Fencing**

The applicant is proposing a combination of masonry walls and wood fencing for the Rockcross Subdivision project:

- Along the East Bidwell Street frontage, a seven-foot-high masonry wall will be constructed to provide an attractive appearance for the subdivision and to reduce traffic-related noise for the homes adjacent to these roadways. Along the Savannah Parkway and Old Ranch Way frontages, a six-foot-high masonry wall will be constructed to provide an attractive appearance for the subdivision and to reduce traffic-related noise for the homes adjacent to these roadways (see the previous discussion of Noise within this staff report).
- Along the eastern property boundary, a six-foot-high masonry wall will be constructed to provide a physical separation between the subdivision and future fire and police station sites, to minimize potential noise impacts, and to provide an attractive appearance for the subdivision.
- Wooden fencing will be provided between residential units. Wooden fencing will be consistent with the guidelines for fencing provided in the Folsom Ranch Design Guidelines.

The recommended conditions of approval (Condition No. 19) require the applicant to provide a final design for all walls and fences for review and approval by staff prior to construction.

## **G. Inclusionary Housing**

The applicant proposes to comply with Folsom Municipal Code Chapter 17.104 (Inclusionary Housing) by paying in-lieu fees per Municipal Code Section 17.104.060(G). (See the applicant's Inclusionary Housing letter, included as Attachment 17 to this staff report). Homes within the subdivision will be sold at market prices. Fees paid by the applicant will help provide affordable housing elsewhere in the city. The applicant is required to enter into an Inclusionary Housing Agreement with the City. The Final Inclusionary Housing Plan is subject to approval by the City Council. In addition, the Inclusionary Housing Agreement, which will be approved by the City Attorney, must be executed prior to recordation of the Final Map for the Rockcross Subdivision project. Condition No. 40 is included to reflect these requirements.

## **H. Frontage Improvements**

Although some of the physical improvements (underground utilities, travel lanes, raised medians, curbs, gutters, and street lights) to East Bidwell Street adjacent to the project site have already been constructed, a sidewalk, landscaping, and a seven-foot-high masonry soundwall are still required to be constructed and installed. The East Bidwell



Street sidewalk and landscaping improvements referenced above will be constructed by others as part of the Mangini Ranch Phase 2 Subdivision project. The owner/applicant will be required to install the seven-foot-tall masonry wall along the subdivision boundary on the East Bidwell Street frontage.

Existing improvements to Old Ranch Way include underground utilities, travel lanes, curbs, and gutters. The owner/applicant will be required to install all landscaping and the sidewalk along the project's frontage adjacent to Old Ranch Way and a six-foot-high masonry soundwall.

Improvements to Savannah Parkway (underground utilities, travel lanes, curbs, and gutters) from East Bidwell Street to the eastern boundary of the Mangini Ranch Village 7 Subdivision are currently under construction. The owner/applicant will be required to install all landscaping and the sidewalk along the project's frontage adjacent to Savannah Parkway as well as a six-foot-high masonry soundwall. The recommended conditions of approval require the applicant to submit detailed plans for all landscaping and walls prior to construction to ensure compliance with the Folsom Ranch Central District Design Guidelines.

#### **I. Minor Administrative Modification**

As described earlier within this report, the parcel (Parcel 79B) on which the Rockcross Subdivision project is located is designated by the FPASP for the development of 153 residential units. Based on the fact that the applicant is proposing to construct 118 residential units on the subject parcel, a Minor Administrative Modification is being requested to relocate the 35 unallocated residential units to three other parcels (Parcels 68, 73 and 155) situated within the Folsom Plan Area.

The Folsom Plan Area Specific Plan provides for Minor Administrative Modifications,

*"... that are consistent with and do not substantially change its overall intent, such as minor adjustments to the land use locations and parcel boundaries shown in Figure 4.1 – Land Use and Figure 4.4 – Plan Area Parcels and the land use acreages shown in Table 4.1 – Land Use Summary."* [FPASP Section 13.3]

Minor administrative modifications can be approved at a staff level, provided the following criteria are met:

- The proposed modification is within the Plan Area.
- The modification does not reduce the size of the proposed town center.
- The modification retains compliance with City Charter Article 7.08, previously known as Measure W.

- The general land use pattern remains consistent with the intent and spirit of the FPASP
- The proposed changes do not substantially alter the backbone infrastructure network.
- The proposed modification offers equal or superior improvements to development capacity or standards.
- The proposed modification does not increase environmental impacts beyond those identified in the EIR/EIS.
- Relocated park or school parcels continue to meet the standards for the type of park or school proposed.
- Relocated park or school parcels remain within walking distance of the residents they serve.

Based on staff's review, the proposed reallocation of 35 residential units from the Rockcross Subdivision site to three other parcels within the Folsom Plan Area meets all of the required criteria mentioned above. As a result, staff is able to approve the proposed Minor Administrative Modification.

#### **J. Conformance with Relevant General Plan and Folsom Plan Area Specific Plan Objectives and Policies**

The applicant prepared a detailed analysis of the project's consistency with all of the policies in the Folsom Plan Area Specific Plan; that analysis is included in the CEQA Exemption and Streamlining Analysis in Attachment 12 to this report. Staff concurs with the applicant's analysis that the project is consistent with the Specific Plan.

The following is a summary analysis of the project's consistency with the Folsom General Plan and with key policies of the Folsom Plan Area Specific Plan.

#### **GP and SP OBJECTIVE H-1 (Housing)**

To provide an adequate supply of suitable sites for the development of a range of housing types to meet the housing needs of all segments of the population.

#### **GP and SP POLICY H-1.1**

The City shall ensure that sufficient land is designated and zoned in a range of residential densities to accommodate the City's regional share of housing.

**Analysis:** The City provides residential lands at a variety of residential densities as specified in the General Plan and in the Folsom Municipal Code. The Folsom Plan Area Specific Plan includes specialized zoning (Specific Plan Designations) that are customized to the Plan Area as adopted in 2011 and as Amended over time. The FPASP provides residential lands at densities ranging from 1-4 dwelling unit per acre (SF), 4-7 dwelling units per acre (SFHD), 7-12 dwelling units per acre

(MLD), 12-20 dwelling units per acre (MMD), 20-30 dwelling units per acre (MHD), and 9-30 dwelling units per acre (MU).

The Rockcross Subdivision project is designated MLD and is proposed to be developed at 8.3 units per acre, which is within the density range for the MLD designation.

#### SP POLICY 4.1

Create pedestrian-oriented neighborhoods through the use of a grid system of streets where feasible, sidewalks, bike paths and trails. Residential neighborhoods shall be linked, where appropriate, to encourage pedestrian and bicycle travel.

Analysis: The Rockcross Subdivision project proposes a traditional single family neighborhood with a grid system of local streets provided with sidewalks on both sides of the street. Biking and walking will be accommodated within the project, and will be connect via external sidewalks and Class II and Class III bicycle lanes with nearby neighborhoods, parks, schools, and open space trails with Class I bicycle trails.

#### SP POLICY 4.4

Provide a variety of housing opportunities for residents to participate in the home-ownership market.

Analysis: The Folsom Plan Area Specific Plan provides home ownership opportunities within the SF (Single-Family), SFHD (Single-Family High Density), and MLD (Multi-Family Low Density) land use designated areas. Residential development in the MLD (Multi-Family Low Density), MMD (Multi-Family Medium Density), MHD (Multi-Family High Density) and MU (Mixed-Use) land use categories may provide 'for rent' opportunities; however home ownership may also be accommodated in 'for sale' condos, townhomes, etc. at the time of development of these particular parcels.

The Rockcross Subdivision project is consistent with this policy in that it will provide detached single family home ownership opportunities within the MLD designation zoned parcels at a more affordable price point than in other, less dense residential developments.

#### SP POLICY 4.6

As established by the Folsom Plan Area Specific Plan, the total number of dwelling units for the Plan Area shall not exceed 11,461. The number of units within individual land use parcels may vary, so long as the number of units falls within the allowable density range for a particular land use designation.

Analysis: There have been a number of Specific Plan Amendments approved by the City Council within the Folsom Plan Area, which has generally led to an

increase in residentially zoned land and a decrease in commercially zoned land. As a result, the number of residential units within the Plan Area increased from 10,210 to 11,461. The various Specific Plan Amendment EIRs and Addenda analyzed impacts from the conversion of the commercial lands to residential lands; impacts and associated mitigations measures can be found in the individual project-specific environmental documents. The increase in population was analyzed and can be accommodated in the excess capacity of the school sites provided in the Plan Area.

The proposed project does not result in any change in total dwelling units in the FPASP. Allocated units originally planned to built on this site that are not part of the current proposal will be reallocated to other parcels. The reallocation of units to these parcels will not exceed the allowable density for the parcels.

**SP OBJECTIVE 7.1 (Circulation)**

Consistent with the California Complete Streets Act of 2008 and the Sustainable Communities and Climate Protection Act (SB 375), create a safe and efficient circulation system for all modes of travel.

**SP POLICY 7.1**

The roadway network in the Plan Area shall be organized in a grid-like pattern of streets and blocks, except where topography and natural features make it infeasible, for the majority of the Plan Area in order to create neighborhoods that encourage walking, biking, public transit, and other alternative modes of transportation.

**Analysis:** Consistent with the requirements of the California Complete Streets Act, the FPASP identified and planned for hierarchy of connect “complete streets” to ensure that pedestrian, bike, bus, and automobile modes are travel are designed to have direct and continuous connections throughout the Plan Area. Every option, from regional connector roadways to arterial and local streets, has been carefully planned and designed. Recent California legislation to reduce greenhouse gas emissions (AB 32 and SB 375) has resulted in an increased market demand for public transit and housing located closer to service needs and employment centers. In response to these changes, the FPASP includes a regional transit corridor that will provide public transportation links between the major commercial, public, and multi-family residential land uses in the Plan Area.

The Rockcross Subdivision project has been designed with multiple modes of transportation options (vehicles, bicycle, walking, access to transit) and internal street organized in a grid pattern consistent with the approved FPASP circulation plan.

## **ENVIRONMENTAL REVIEW**

The California Environmental Quality Act (CEQA) provides that residential projects which are consistent with an approved Specific Plan for which an EIR was prepared are exempt from a requirement to prepare additional environmental analysis. CEQA Guidelines section 15182(c) provides specific criteria to determine whether this exemption applies:

*(c) Residential Projects Implementing Specific Plans.*

*(1) Eligibility. Where a public agency has prepared an EIR on a specific plan after January 1, 1980, a residential project undertaken pursuant to and in conformity to that specific plan is exempt from CEQA if the project meets the requirements of this section. Residential projects covered by this section include but are not limited to land subdivisions, zoning changes, and residential planned unit developments. [CEQA Guidelines section 15182]*

The applicant has prepared an analysis (included as Attachment 12 to this staff report), which determined that the Rockcross project qualifies for the exemption provided in CEQA Guidelines 15182(c), since it is consistent with the Folsom Plan Area Specific Plan.

The applicant's analysis also includes a review of the impacts and mitigation measures addressed in the EIR for the FPASP, which concluded that the project will not result in any impacts not already identified, and that mitigation measures in the EIR will be sufficient to address project impacts. None of the events described in CEQA Guidelines 15162 which would require preparation of a subsequent EIR (substantial changes to the project, substantial changes in the circumstances under which the project is undertaken, or new information of substantial performance) have occurred, as detailed in the CEQA Exemption Analysis (Attachment 12 to this staff report).

The City has reviewed the applicant's analysis and concurs that the project is exempt from additional environmental review as provided in CEQA Guidelines 15182(c).

## **RECOMMENDATION/PLANNING COMMISSION ACTION**

Move to recommend that the City Council:

- Approve the CEQA Exemption for the proposed project pursuant to CEQA Guidelines section 15182(c),
- Approve a Small-Lot Vesting Tentative Subdivision Map creating 118 single-family residential lots and three lettered landscape lots,
- Approve Design Review of the applicant's master plan residential designs, and

- Approve a Minor Administrative Modification to reallocate 35 single family units to three other parcels in the FPASP area

These approvals are subject to the proposed findings below (Findings A-R) and the recommended conditions of approval (Conditions 1-53) attached to this report.

### **GENERAL FINDINGS**

- A. NOTICE OF HEARING HAS BEEN GIVEN AT THE TIME AND IN THE MANNER REQUIRED BY STATE LAW AND CITY CODE.
- B. THE PROJECT IS GENERALLY CONSISTENT WITH THE GENERAL PLAN, THE FOLSOM PLAN AREA SPECIFIC PLAN AS AMENDED, AND THE FOLSOM RANCH CENTRAL DISTRICT DESIGN GUIDELINES.

### **CEQA FINDINGS**

- C. THE CITY, AS LEAD AGENCY, PREVIOUSLY CERTIFIED AN ENVIRONMENTAL IMPACT REPORT/ENVIRONMENTAL IMPACT STATEMENT FOR THE FOLSOM PLAN AREA SPECIFIC PLAN.
- D. THE CITY HAS DETERMINED THAT THE ROCKCRESS SUBDIVISION PROJECT IS UNDERTAKEN TO IMPLEMENT AND IS CONSISTENT WITH THE FOLSOM PLAN AREA SPECIFIC PLAN.
- E. THE CITY HAS DETERMINED THAT THE IMPACTS OF THE ROCKCRESS SUBDIVISION PROJECT ARE ADEQUATELY ADDRESSED BY THE FINAL ENVIRONMENTAL IMPACT REPORT FOR THE FOLSOM PLAN AREA SPECIFIC PLAN AND ASSOCIATED MITIGATION MEASURES AND THAT THE ROCKCRESS SUBDIVISION PROJECT IS EXEMPT FROM THE REQUIREMENTS OF CEQA PURSUANT TO GOVERNMENT CODE SECTION 65457 AND CEQA GUIDELINES 15182(c).
- F. NONE OF THE EVENTS SPECIFIED IN SECTION 21166 OF THE PUBLIC RESOURCES CODE OR SECTION 15162 OF THE CEQA GUIDELINES HAVE OCCURRED.
- G. THIS PROJECT IS EXEMPT FROM CEQA IN ACCORDANCE WITH GOVERNMENT CODE SECTION 65457 AND SECTION 15182 OF THE CEQA GUIDELINES.

**TENTATIVE SUBDIVISION MAP FINDINGS**

- H. THE PROPOSED SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP IS CONSISTENT WITH THE CITY'S SUBDIVISION ORDINANCE AND THE SUBDIVISION MAP ACT IN THAT THE PROJECT IS SUBJECT TO CONDITIONS OF APPROVAL THAT WILL ENSURE THAT THE PROJECT IS DEVELOPED IN COMPLIANCE WITH CITY STANDARDS.
- I. THE PROPOSED SUBDIVISION, TOGETHER WITH THE PROVISIONS FOR ITS DESIGN AND IMPROVEMENT, IS CONSISTENT WITH THE GENERAL PLAN (AS AMENDED), THE FOLSOM PLAN AREA SPECIFIC PLAN (AS AMENDED), AND ALL APPLICABLE PROVISIONS OF THE FOLSOM MUNICIPAL CODE.
- J. THE SITE IS PHYSICALLY SUITABLE FOR THE TYPE OF DEVELOPMENT PROPOSED.
- K. THE SITE IS PHYSICALLY SUITABLE FOR THE PROPOSED DENSITY OF THE DEVELOPMENT.
- L. AS CONDITIONED, THE DESIGN OF THE SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP AND THE PROPOSED IMPROVEMENTS ARE NOT LIKELY TO CAUSE SUBSTANTIAL ENVIRONMENTAL DAMAGE OR SUBSTANTIALLY AND AVOIDABLY INJURY FISH OR WILDLIFE OR THEIR HABITAT.
- M. AS CONDITIONED, THE DESIGN OF THE SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP AND THE PROPOSED IMPROVEMENTS ARE NOT LIKELY TO CAUSE SERIOUS PUBLIC HEALTH OR SAFETY PROBLEMS.
- N. THE DESIGN OF THE SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP AND THE TYPE OF IMPROVEMENTS WILL NOT CONFLICT WITH EASEMENTS FOR ACCESS THROUGH OR USE OF PROPERTY WITHIN THE PROPOSED SUBDIVISION.
- O. SUBJECT TO SECTION 66474.4 OF THE SUBDIVISION MAP ACT, THE LAND IS NOT SUBJECT TO A CONTRACT ENTERED INTO PURSUANT TO THE CALIFORNIA LAND CONSERVATION ACT OF 1965 (COMMENCING WITH SECTION 51200 OF THE GOVERNMENT CODE).

**DESIGN REVIEW FINDINGS**

- P. THE PROJECT IS IN COMPLIANCE WITH THE GENERAL PLAN, THE FOLSOM PLAN AREA SPECIFIC PLAN AND THE APPLICABLE ZONING ORDINANCES.

- Q. THE PROJECT IS IN CONFORMANCE WITH THE FOLSOM RANCH  
CENTRAL DISTRICT DESIGN GUIDELINES.**
  
- R. THE BUILDING MATERIALS, TEXTURES, AND COLORS OF THE PROJECT  
WILL BE COMPATIBLE WITH SURROUNDING DEVELOPMENT AND  
CONSISTENT WITH THE GENERAL DESIGN THEME OF THE  
NEIGHBORHOOD.**



**Attachment 4**

**Conditions of Approval**

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<b>CONDITIONS OF APPROVAL FOR THE ROCKCRESS SUBDIVISION (PN 19-388)</b> <b>NORTHEAST CORNER OF THE INTERSECTION OF EAST BIDWELL STREET AND SAVANNAH PARKWAY</b> <b>SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATION</b>				
Condition No.	Mitigation Measure	Condition of Approval	When Required	Responsible Department
1.		<p><b><i>Final Development Plans</i></b>            The owner/applicant shall submit final site development plans to the Community Development Department that shall substantially conform to the exhibits referenced below:</p> <ol style="list-style-type: none"> <li>1. Small-Lot Vesting Tentative Subdivision Map, dated February 18, 2020</li> <li>2. Preliminary Grading, Drainage, and Utility Plan, dated February 18, 2020</li> <li>3. Conceptual Front Yard Landscaping, dated March 18, 2020</li> <li>4. Wall and Fence Exhibit, dated February, 2020</li> <li>5. Residential Schematic Design, dated June 17, 2020</li> <li>6. Access and Circulation Analysis, dated May 12, 2020</li> <li>7. Environmental Noise Analysis, dated April 24, 2020</li> <li>8. Applicant's Inclusionary Housing Letter, June 4, 2020</li> </ol> <p>The Small-Lot Vesting Tentative Subdivision Map, Design Review, and Inclusionary Housing Plan are approved for the development of a 118-unit single-family residential subdivision (Rockcross Subdivision). Implementation of the project shall be consistent with the above referenced items and these conditions of approval.</p>	G, I, M, B	CD (P)(E)
2.		<p><b><i>Plan Submittal</i></b>            All civil engineering, improvement, and landscape and irrigation plans, shall be submitted to the Community Development Department for review and approval to ensure conformance with this approval and with relevant codes, policies, standards and other requirements of the City of Folsom.</p>	G, I	CD (P)(E)

3.		<p><b>Validity</b>          This approval of the Small-Lot Vesting Tentative Subdivision Map shall be valid for a period of twenty-four (24) months pursuant to Section 16.16.110A of the Folsom Municipal Code and the Subdivision Map Act. The term of the Planned Development Permit and approved Inclusionary Housing Agreement shall track the term of the Small-Lot Vesting Tentative Subdivision Map, as may be extended from time to time pursuant to Section 16.16.110.A and 16.16.120 of the Folsom Municipal Code and the Subdivision Map Act.</p>	M	CD (P)
4.		<p><b>FMC Compliance</b>          The Small-Lot Final Map shall comply with the Folsom Municipal Code and the Subdivision Map Act.</p>	M	CD (E)
5.		<p><b>Development Rights</b>          The approval of this Small-Lot Vesting Tentative Subdivision Map conveys the right to develop. As noted in these conditions of approval for the Small-Lot Vesting Tentative Subdivision Map, the City has identified improvements necessary to develop the subject parcels. These improvements include on and off-site roadways, water, sewer, storm drainage, landscaping, soundwalls, and other improvements.</p>	OG	CD (P)(E)(B) PW, PR, FD, PD
6.		<p><b>Public Right of Way Dedication</b>          As provided for in the First Amended and Restated Development Agreement (ARDA) and the Amendments No. 1 and 2 thereto, and any approved amendments thereafter, the owner/applicant shall dedicate all public rights-of-way and corresponding public utility easements such that public access is provided to each and every lot within the Rockcross Subdivision project as shown on the Small-Lot Vesting Tentative Subdivision Map (Lots 1-118).</p>	M	CD (E)(P)
7.		<p><b>Street Names</b>          The street names identified below shall be used for the Final Small-Lot Map(s):          French Drive, Harris Way, Manning Way, Sanderson Drive, Sherman Way, Sidney Way, and Tucker Drive.</p>	M	CD (E)(P)

8.		<p><b><i>Indemnity for City</i></b>          The owner/applicant shall protect, defend, indemnify, and hold harmless the City and its agents, officers and employees from any claim, action or proceeding against the City or its agents, officers or employees to attack, set aside, void, or annul any approval by the City or any of its agencies, departments, commissions, agents, officers, employees, or legislative body concerning the project, which claim, action or proceeding is brought within the time period provided therefore in Government Code Section 66499.37 or other applicable statutes of limitation. The City will promptly notify the owner/applicant of any such claim, action or proceeding, and will cooperate fully in the defense. If the City should fail to cooperate fully in the defense, the owner owner/applicant shall not thereafter be responsible to defend, indemnify and hold harmless the City or its agents, officers, and employees, pursuant to this condition. The City may, within its unlimited discretion, participate in the defense of any such claim, action or proceeding if both of the following occur:</p> <ul style="list-style-type: none"> <li>• The City bears its own attorney’s fees and costs; and</li> <li>• The City defends the claim, action or proceeding in good faith</li> </ul> <p>The owner/applicant shall not be required to pay or perform any settlement of such claim, action or proceeding unless the settlement is approved by the owner/applicant. The owner/applicant’s obligations under this condition shall apply regardless of whether a Final Map is ultimately recorded with respect to this project.</p>	OG	CD (P)(E)(B) PW, PR, FD, PD
9.		<p><b><i>Small-Lot Vesting Tentative Subdivision Map</i></b>          The Small-Lot Vesting Tentative Subdivision map is expressly conditioned upon compliance with all environmental mitigation measures identified in the Folsom Plan Area Specific Plan EIR/EIS as amended by the Revised Proposed Water Supply Facility Alternative (November 2012), the Folsom South of U.S. Highway 50 Backbone Infrastructure Mitigated Negative Declaration (December 2014), and the Westland Eagle Specific Plan Amendment (September 2015).</p>	OG	CD
10.		<p><b><i>ARDA and Amendments</i></b>          The owner/applicant shall comply with all provisions of Amendments No. 1 and 2 to the First Amended and Restated Tier 1 Development Agreement and any approved amendments thereafter by and between the City and the owner/applicant of the project.</p>	M	CD (E)

11.	✓	<p><b>Mitigation Monitoring</b>          The owner/applicant shall participate in a mitigation monitoring and reporting program pursuant to City Council Resolution No. 2634 and Public Resources Code 21081.6. The mitigation monitoring and reporting measures identified in the Folsom Plan Area Specific Plan FEIR/EIS have been incorporated into these conditions of approval in order to mitigate or avoid significant effects on the environment. These mitigation monitoring and reporting measures are identified in the mitigation measure column. Applicant shall fund on a Time and Materials basis all mitigation monitoring (e.g., staff and consultant time).</p>	OG	CD (P)
12.		<p>The owner/applicant acknowledges that the State adopted amendments to Section 65850 of the California Government Code (specifically Section 65850(9)), effective January 1, 2018, to allow for the implementation of inclusionary housing requirements in residential rental units, upon adoption of an ordinance by the City. The Landowner is not currently contemplating any residential rental projects within the Subject Property; however, in the event the City amends its Inclusionary Housing Ordinance with respect to rental housing pursuant to Section 65850(9), Landowner (or successor in interest) agrees that the Subject Property shall be subject to said City Ordinance, as amended, should any residential rental project be proposed within the Subject Property.</p>	OG	CD (P)
<b>POLICE/SECURITY REQUIREMENT</b>				
13.		<p>The owner/applicant shall consult with the Police Department in order to incorporate all reasonable crime prevention measures. The following security/safety measures shall be considered:</p> <ul style="list-style-type: none"> <li>• A security guard on-duty at all times at the site or a six-foot security fence shall be constructed around the perimeter of construction areas.</li> <li>• Security measures for the safety of all construction equipment and unit appliances.</li> <li>• Landscaping shall not cover exterior doors or windows, block line-of-sight at intersections or screen overhead lighting.</li> </ul>	G, I, B	PD

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**DEVELOPMENT COSTS AND FEE REQUIREMENTS**

14.		<p><b>Taxes and Fees</b>          The owner/applicant shall pay all applicable taxes, fees and charges for the project at the rate and amount required by the Public Facilities Financing Plan and Amendments No. 1 and No. 2 to the Amended and Restated Tier 1 Development Agreement.</p>	M	CD (P)(E)
15.		<p><b>Assessments</b>          If applicable, the owner/applicant shall pay off any existing assessments against the property, or file necessary segregation request and pay applicable fees.</p>	M	CD (E)
16.		<p><b>FPASP Development Impact Fees</b>          The owner/applicant shall be subject to all Folsom Plan Area Specific Plan Area development impact fees in place at the time of approval or subsequently adopted consistent with the Public Facilities Financing Plan (PFFP), Development Agreement and amendments thereto, unless exempt by previous agreement. The owner/applicant shall be subject to all applicable Folsom Plan Area plan-wide development impact fees in effect at such time that a building permit is issued. These fees may include, but are not limited to, the Folsom Plan Area Specific Plan Fee, Specific Plan Infrastructure Fee (SPIF), Solid Waste Fee, Corporation Yard Fee, Transportation Management Fee, Transit Fee, Highway 50 Interchange Fee, General Park Equipment Fee, Housing Trust Fee, etc.</p> <p>Any protest to such for all fees, dedications, reservations or other exactions imposed on this project will begin on the date of final approval (July 1, 2020), or otherwise shall be governed by the terms of Amendments No. 1 and 2 to ARDA. The fees shall be calculated at the fee rate set forth in the PFFP and the ARDA.</p>	B	CD (P), PW, PK

17.		<p><b><i>Legal Counsel</i></b>          The City, at its sole discretion, may utilize the services of outside legal counsel to assist in the implementation of this project, including, but not limited to, drafting, reviewing and/or revising agreements and/or other documentation for the project. If the City utilizes the services of such outside legal counsel, the City shall provide notice to the owner/applicant of the outside counsel selected, the scope of work and hourly rates, and the owner/applicant shall reimburse the City for all outside legal fees and costs incurred and documented by the City for such services. The owner/applicant may be required, at the sole discretion of the City Attorney, to submit a deposit to the City for these services prior to initiation of the services. The owner/applicant shall be responsible for reimbursement to the City for the services regardless of whether a deposit is required.</p>	OG	CD (P)(E)
18.		<p><b><i>Consultant Services</i></b>          If the City utilizes the services of consultants to prepare special studies or provide specialized design review or inspection services for the project, the City shall provide notice to the owner/applicant of the outside consultant selected, the scope of work and hourly rates, and the owner/applicant shall reimburse the City for actual costs incurred and documented in utilizing these services, including administrative costs for City personnel. A deposit for these services shall be provided prior to initiating review of the Grading Plan, Final Map, improvement plans, or beginning inspection, whichever is applicable.</p>	G, I, M, B	CD (P)(E)

GRADING PERMIT REQUIREMENTS				
19.		<p><b>Walls/Fences</b>            The final location, design, height, materials, and colors of the walls and fences shall consistent with the submitted Wall and Fence Exhibit, dated February, 2020 subject to review and approval by the Community Development Department to ensure consistency with the Folsom Ranch Central District Design Guidelines.</p>	G, I, B	CD (P)(E), FD
20.		<p><b>Mine Shaft Remediation</b>            The owner/applicant shall locate and remediate all antiquated mine shafts, drifts, open cuts, tunnels, and water conveyance or impoundment structures existing on the project site, with specific recommendations for the sealing, filling, or removal of each that meet all applicable health, safety and engineering standards. Recommendations shall be prepared by an appropriately licensed engineer or geologist. All remedial plans shall be reviewed and approved by the City prior to approval of grading plans.</p>	G	CD (E)
21.		<p><b>Prepare Traffic Control Plan.</b>            Prior to construction, a Traffic Control Plan for roadways and intersections affected by construction shall be prepared by the owner/applicant. The Traffic Control Plan prepared by the owner/applicant shall, at minimum, include the following measures:</p> <ul style="list-style-type: none"> <li>• Maintaining the maximum amount of travel lane capacity during non-construction periods, possible, and advanced notice to drivers through the provision of construction signage.</li> <li>• Maintaining alternate one-way traffic flow past the lay down area and site access when feasible.</li> <li>• Heavy trucks and other construction transport vehicles shall avoid the busiest commute hours (7 a.m. to 8 a.m. and 5 p.m. to 6 p.m. on weekdays).</li> <li>• A minimum 72-hour advance notice of access restrictions for residents, businesses, and local emergency response agencies. This shall include the identification of alternative routes and detours to enable for the avoidance of the immediate construction zone.</li> <li>• A phone number and City contact for inquiries about the schedule of the construction throughout the construction period. This information will be posted in a local newspaper, via the City's web site, or at City Hall and will be updated on a monthly basis.</li> </ul>	G	CD (E)



22.	<b>State and Federal Permits</b> The owner/applicant shall obtain all required State and Federal permits and provide evidence that said permits have been obtained, or that the permit is not required, subject to staff review prior to approval of any <u>grading</u> or improvement plan.	G, I	CD (P)(E)
23.	<b>Landslide /Slope Failure</b> The owner/applicant shall retain an appropriately licensed engineer during grading activities to identify existing landslides and potential slope failure hazards. The said engineer shall be notified a minimum of two days prior to any site clearing or grading to facilitate meetings with the grading contractor in the field.	G	CD (E) PW
<b>IMPROVEMENT PLAN REQUIREMENTS</b>			
24.	<b>Improvement Plans</b> The improvement plans for the required public and private subdivision improvements necessary to serve any and all phases of development shall be reviewed and approved by the Community Development Department prior to approval of a Final Map.	M	CD (E)
25.	<b>Standard Construction Specifications and Details</b> Public and private improvements, including roadways, curbs, gutters, sidewalks, bicycle lanes and trails, streetlights, underground infrastructure and all other improvements shall be provided in accordance with the latest edition of the City of Folsom <u>Standard Construction Specifications and Details</u> and the <u>Design and Procedures Manual and Improvement Standards</u> .	I	CD (P)(E)
26.	<b>Water and Sewer Infrastructure</b> All City-owned water and sewer infrastructure shall be placed within the street right of way. In the event that a City-maintained public water or sewer main needs to be placed in an area other than the public right of way, such as through an open space corridor, landscaped area, etc., the following criteria shall be met;  <ul style="list-style-type: none"> <li>• The owner/applicant shall provide public sewer and water main easements</li> <li>• An access road shall be designed and constructed to allow for the operations, maintenance and replacement of the public water or sewer line by the City along the entire water and/or sewer line alignment.</li> <li>• In no case shall a City-maintained public water or public sewer line be placed on private residential property.</li> </ul>	I	CD (E)

27.		<p><b>Lighting Plan</b>          The owner/applicant of all project phases shall submit a lighting plan for the project to the Community Development Department. The lighting plan shall be consistent with the Folsom Ranch Central District Design Guidelines:</p> <ul style="list-style-type: none"> <li>• Shield or screen lighting fixtures to direct the light downward and prevent light spill on adjacent properties;</li> <li>• Place and shield or screen flood and area lighting needed for construction activities, nighttime sporting activities, and/or security so as not to disturb adjacent residential areas and passing motorists;</li> <li>• For public lighting in residential neighborhoods, prohibit the use of light fixtures that are of unusually high intensity or that blink or flash;</li> <li>• Use appropriate building materials (such as low-glare glass, low-glare building glaze or finish, neutral, earth toned colored paint and roofing materials), shielded or screened lighting, and appropriate signage in the office/commercial areas to prevent light and glare from adversely affecting motorists on nearby roadways; and</li> <li>• Design exterior on-site lighting as an integral part of the building and landscaping design in the Specific Plan Area. Lighting fixtures shall be architecturally consistent with the overall site design. Lights used on signage should be directed to light only the sign face with no off-site glare.</li> </ul>	I	CD (P)
28.		<p><b>Utility Coordination</b>          The owner/applicant shall coordinate the planning, development and completion of this project with the various utility agencies (i.e., SMUD, PG&amp;E, etc.). The owner/applicant shall provide the City with written confirmation of public utility service prior to approval of the final map.</p>	M	CD (P)(E)
29.		<p><b>Replacing Hazardous Facilities</b>          The owner/applicant shall be responsible for replacing any and all damaged or hazardous public sidewalk, curb and gutter, and/or bicycle trail facilities along the site frontage and/or boundaries, including pre-existing conditions and construction damage, to the satisfaction of the Community Development Department.</p>	I, OG	CD (E)

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30.		<p><b><i>Future Utility Lines</i></b>          All future utility lines lower than 69 KV that are to be built within the project shall be placed underground within and along the perimeter of the project at the developer's cost. The owner/applicant shall dedicate to SMUD all necessary underground easements for the electrical facilities that will be necessary to service development of the project.</p>	M	CD (E)
31.		<p><b><i>Water Meter Fixed Network System</i></b>          The owner owner/applicant shall pay for, furnish and install all infrastructure associated with the water meter fixed network system for any City-owned and maintained water meter within the project.</p>	I	CD (E), EWR
32.		<p><b><i>Class II Bike Lanes</i></b>          All Class II bike lanes (East Bidwell Street and Savannah Parkway) shall be striped, and the legends painted to the satisfaction of the Community Development Department. No parking shall be permitted within the Class II bike lanes.</p>	I	CD (E)(P)

33.	<p><b>Noise Barriers and Window Assemblies</b>          Based on the Environmental Noise Assessment (the “2020 Noise Assessment”) prepared by Bollard Acoustical Consultants on April 24, 2020, the following measures shall be implemented to the satisfaction of the Community Development Department:</p> <ul style="list-style-type: none"> <li>• 7-foot-tall solid noise barriers shall be constructed along all residential property boundaries adjacent to East Bidwell Street and 6-foot-tall noise barriers shall be constructed along all residential property boundaries adjacent to Old Ranch Way, Savannah Parkway, and the eastern property boundary with the future fire/police stations prior to occupancy of any residences adjacent to these streets or boundaries. The 6-foot-tall and 7-foot-tall noise barriers shall be constructed to the required height relative to the rear yard elevations.</li> <li>• Suitable materials for the traffic noise barriers include masonry and precast concrete panels. Other materials may be acceptable but shall be reviewed by an acoustical consultant and approved by the Community Development Department prior to use.</li> <li>• Mechanical ventilation (air conditioning) shall be provided for all residences in this development to allow the occupants to close doors and windows as desired to achieve compliance with the applicable interior noise level criteria.</li> <li>• Second-floor building facades shall maintain minimum window assembly STC ratings of 32 for all homes with rear yards adjacent to East Bidwell Street.</li> <li>• First and second-floor building facades shall maintain minimum window assembly STC ratings of 32 for all homes with rear yards adjacent to the future Fire and Police stations along the eastern project boundary.</li> </ul>	I, O	CD (E)(P)
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34.		<p><b><i>Master Plan Updates</i></b></p> <p>The owner/applicant shall provide sanitary sewer, water and storm drainage improvements with corresponding easements, as necessary, in accordance with these studies and the latest edition of the City of Folsom <u><i>Standard Construction Specifications and Details</i></u>, and the <u><i>Design and Procedures Manual and Improvement Standards</i></u>.</p> <p>The storm drainage design shall provide for no net increase in run-off under post-development conditions.</p>	G, I	CD(E), EWR, PW
35.		<p><b><i>Best Management Practices</i></b></p> <p>The storm drain improvement plans shall provide for “Best Management Practices” that meet the requirements of the water quality standards of the City’s National Pollutant Discharge Elimination System Permit issued by the State Regional Water Quality Control Board.</p> <p>In addition to compliance with City ordinances, the owner/applicant shall prepare a Stormwater Pollution Prevention Plan (SWPPP) and implement Best Management Practices (BMPs) that comply with the General Construction Stormwater Permit from the Central Valley RWQCB, to reduce water quality effects during construction. Detailed information about the SWPPP and BMPs are provided in Chapter 3A.9, “Hydrology and Water Quality.”</p>	G, I	CD (E)
36.		<p><b><i>Litter Control</i></b></p> <p>During Construction, the owner/applicant shall be responsible for litter control and sweeping of all paved surfaces in accordance with City standards. All on-site storm drains shall be cleaned immediately before the official start of the rainy season (October 15).</p>	OG	CD (E)

**FIRE DEPT REQUIREMENTS**

37.		<p><b><i>All-Weather Access and Fire Hydrants</i></b>          The owner/applicant shall provide all-weather access and fire hydrants before combustible materials are allowed on any project site or other approved alternative method as approved by the Fire Department. All-weather emergency access roads and fire hydrants (tested and flushed) shall be provided before combustible material or vertical construction is allowed on any project site or other approved alternative method as approved by the Fire Department. (All-weather access is defined as six inches of compacted aggregate base from May 1 to September 30 and two inches asphalt concrete over six inches aggregate base from October 1 to April 30). The buildings shall have illuminated addresses visible from the street or drive fronting the property. Size and location of address identification shall be reviewed and approved by the Fire Department.</p> <ul style="list-style-type: none"> <li>• Residential Fire-Flow with Automatic Fire Sprinkler System: The required fire-flow for the proposed subdivision is determined to be 500 gpm per minute for 30 minutes.</li> <li>• All public streets shall meet City of Folsom Street Standards.</li> <li>• The maximum length of any dead-end street shall not exceed 500 feet in accordance with the Folsom Fire Code (unless approved by the Fire Department).</li> <li>• All-weather emergency access roads and fire hydrants (tested and flushed) shall be provided before combustible material storage or vertical construction is allowed. All-weather access is defined as 6" of compacted AB from May 1 to September 30 and 2"AC over 6" AB from October 1 to April 30</li> <li>• The first Fire Station planned for the Folsom Plan Area may be required to be completed and operational at the time that the threshold of 1,500 occupied homes within the Folsom Plan Area is met.</li> </ul>	G, I, M, B	CD (P), FD
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**LANDSCAPE/TREE PRESERVATION REQUIREMENTS**

<p>38.</p>	<p><b>Landscaping Plans</b>          Final landscape plans and specifications shall be prepared by a registered landscape architect and approved by the City prior to the approval of the first building permit. Said plans shall include all on-site landscape specifications and details including a tree planting exhibit demonstrating sufficient diversity and appropriate species selection to the satisfaction of the Community Development Department. The tree exhibit shall include all street trees, accent trees, parking lot shading trees, and mitigation trees proposed within the development. Said plans shall comply with all State and local rules, regulations, Governor's declarations and restrictions pertaining to water conservation and outdoor landscaping.</p> <p>Landscaping shall meet shade requirements as outlined in the Folsom Plan Area Specific Plan where applicable. The landscape plans shall comply and implement water efficient requirements as adopted by the State of California (Assembly Bill 1881) (State Model Water Efficient Landscape Ordinance) until such time the City of Folsom adopts its own Water Efficient Landscape Ordinance at which time the owner/applicant shall comply with any new ordinance. Shade and ornamental trees shall be maintained according to the most current American National Standards for Tree Care Operations (ANSI A-300) by qualified tree care professionals. Tree topping for height reduction, view protection, light clearance or any other purpose shall not be allowed. Specialty-style pruning, such as pollarding, shall be specified within the approved landscape plans and shall be implemented during a 5-year establishment and training period. The owner/applicant shall comply with city-wide landscape rules or regulations on water usage. The owner/applicant shall comply with any state or local rules and regulations relating to landscape water usage and landscaping requirements necessitated to mitigate for drought conditions on all landscaping in the Rockcross Subdivision project.</p>	<p>B</p>	<p>CD (P)(E)</p>
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<b>MAP REQUIREMENTS</b>				
39.		<p><b><i>Subdivision Improvement Agreement</i></b>            Prior to the approval of any Final Map, the owner/applicant shall enter into a subdivision improvement agreement with the City, identifying all required improvements, if any, to be constructed with each proposed phase of development. The owner/applicant shall provide security acceptable to the City, guaranteeing construction of the improvements.</p>	M	CD (E)
40.		<p><b><i>The Final Inclusionary Housing Plan</i></b>            The Final Inclusionary Housing Plan shall be approved by the City Council. The Inclusionary Housing Agreement, which will be approved by the City Attorney, shall be executed prior to recordation of the Final Map for the Rockcross Subdivision project.</p>	M	CD (P)(E)



<p>41.</p>	<p><b>Department of Real Estate Public Report</b>          The owner/applicant shall disclose to the homebuyers in the Department of Real Estate Public Report and/or the CC&amp;R's the following items:</p> <ol style="list-style-type: none"> <li>1) Future public parks and public schools are located in relatively close proximity to the proposed subdivision, and that the public parks may include facilities (basketball courts, a baseball field, softball fields, soccer fields, and playground equipment) that may generate noise impacts during various times, including but not limited to evening and nighttime hours. The owner/applicant shall also disclose that the existing public parks include nighttime sports lighting that may generate lighting impacts during evening and nighttime hours.</li> <li>2) Future Fire and Police stations are located adjacent to the project site and may include facilities and equipment that generate noise and light impacts during various times, including but not limited to evening and nighttime hours.</li> <li>3) The soil in the subdivision may contain naturally occurring asbestos and naturally occurring arsenic.</li> <li>4) The collecting, digging, or removal of any stone, artifact, or other prehistoric or historic object located in public or open space areas, and the disturbance of any archaeological site or historic property, is prohibited.</li> <li>5) The project site is located close to the Mather Airport flight path and overflight noise may be present at various times.</li> <li>6) That all properties located within one mile of an on- or off-site area zoned or used for agricultural use (including livestock grazing) shall be accompanied by written disclosure from the transferor, in a form approved by the City of Folsom, advising any transferee of the potential adverse odor impacts from surrounding agricultural operations, which disclosure shall direct the transferee to contact the County of Sacramento concerning any such property within the County zoned for agricultural uses within one mile of the subject property being transferred.</li> </ol>	<p>M</p>	<p>CD (P, PK)</p>
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42.		<p><b>Public Utility Easements</b>          The owner/applicant shall dedicate public utility easements for underground facilities on properties adjacent to the public and private streets. A minimum of twelve and one-half-foot (12.5') wide Public Utility Easements for underground facilities (i.e., SMUD, Pacific Gas and Electric, cable television, telephone) shall be dedicated adjacent to all public and private street rights-of-way. The owner/applicant shall dedicate additional width to accommodate extraordinary facilities as determined by the City. The width of the public utility easements adjacent to public and private right of way may be reduced with prior approval from public utility companies.</p>	M	CD (E)
43.		<p><b>Backbone Infrastructure</b>          As provided for in the ARDA and the Amendment No. 1 thereto, the owner/applicant shall provide fully executed grant deeds, legal descriptions, and plats for all necessary Infrastructure to serve the project, including but not limited to lands, public rights of way, public utility easements, public water main easements, public sewer easements, irrevocable offers of dedication and temporary construction easements. All required easements as listed necessary for the Infrastructure shall be reviewed and approved by the City and recorded with the Sacramento County Recorder pursuant to the timing requirements set forth in Section 3.8 of the ARDA, and any amendments thereto.</p>	M	CD (E)
44.		<p><b>New Permanent Benchmarks</b>          The owner/applicant shall provide and establish new permanent benchmarks on the (NAVD 88) datum in various locations within the subdivision or at any other locations in the vicinity of the project/subdivision as directed by the City Engineer. The type and specifications for the permanent benchmarks shall be provided by the City. The new benchmarks shall be placed by the owner/applicant within 6 months from the date of approval of the vesting tentative subdivision map.</p>	M	CD (E)
45.		<p><b>Centralized Mail Delivery Units</b>          All Final Maps shall show easements or other mapped provisions for the placement of centralized mail delivery units. The owner/applicant shall provide a concrete base for the placement of any centralized mail delivery unit. Specifications and location of such base shall be determined pursuant to the applicable requirements of the U. S. Postal Service and the City of Folsom Community Development Department, with due consideration for street light location, traffic safety, security, and consumer convenience.</p>	M	CD (E)

46.		<p><b>Recorded Final Map</b>          Prior to the issuance of building permits, the owner/applicant shall provide a digital copy of the recorded Final Map (in AutoCAD format) to the Community Development Department. The exception to this requirement is model homes. Building permits for model homes only may be issued prior to recording of the Final Map, subject to approval by the Community Development Department.</p>	B	CD (E)
47.		<p><b>Recorded Final Map</b>          Prior to issuance of building permits, the owner/applicant shall provide the Folsom-Cordova Unified School District with a copy of the recorded Final Map.</p>	B	CD (P), FCUSD
48.		<p><b>Credit Reimbursement Agreement</b>          Prior to the recordation of the first Small-Lot Final Map, the owner/applicant and City shall enter into a credit and reimbursement agreement for constructed improvements that are included in the Folsom Plan Area's Public Facilities Financing Plan.</p>	M	CD (E)
<b>TRAFFIC/ACCESS/CIRCULATION/PARKING REQUIREMENTS</b>				
49.		<p>The following conditions of approval are related to roadway and traffic related improvements for the Rockcross Subdivision project under two (2) separate scenarios:</p> <p>A. The Enclave Subdivision project (PN 16-025) <u>has</u> constructed roadway improvements, while the Mangini Ranch Phase 2 Subdivision Village 7 project (PN 17-308) has not constructed roadway improvements.</p> <p>B. The Enclave Subdivision project (PN 16-025) <u>has</u> constructed roadway improvements and the Mangini Ranch Phase 2 Subdivision Village 7 project (PN 17-308) <u>has</u> constructed road improvements.</p> <p>See Attachment 12 (KH Memo and Exhibits) to this staff report for reference for the following improvements under each scenario:</p>	B	CD (E), PW, FD

<p>49. Cont</p>		<p><u>Scenario 1 (Enclave Subdivision Improvements Completed Mangini Ranch Village 7 Subdivision Improvements Not Completed)</u></p> <ul style="list-style-type: none"> <li>The owner/applicant shall construct a southbound median left turn pocket on East Bidwell Street with a minimum storage length of 315 feet (255-foot deceleration lane plus 60-foot taper) to provide left turn access to Savannah Parkway.</li> <li>The owner/applicant shall construct Savannah Parkway from East Bidwell Street to the eastern boundary of the Rockcross Subdivision and provide a temporary U-Turn at the eastbound intersection of Savannah Parkway and Shale Rock Way (Mangini Ranch Village 2) until such time that the segment of Savannah Parkway between Shale Rock Way and Westwood Drive is completed and Westwood Drive is completed between Savannah Parkway and Alder Creek Parkway.</li> </ul> <p><u>Scenario 2 (Enclave/Mangini Ranch Village 7 Subdivision Improvements Completed)</u></p> <ul style="list-style-type: none"> <li>The owner/applicant shall construct the eastern extension of Savannah Parkway from the Mangini Ranch Village 7 Subdivision boundary to the eastern boundary of the Rockcross Subdivision (including the Shale Rock Way intersection).</li> </ul>	<p>B</p>	<p>CD (E), PW, FD</p>
<p>50.</p>		<p>Until such time that a traffic signal is required (issuance of 496<sup>th</sup> building permit within Mangini Ranch Phase 2 Subdivision project) at the East Bidwell Street/Savannah Parkway intersection, the owner/applicant shall construct a southbound median acceleration lane to assist in facilitating a two-stage outbound left-turn lane from Savannah Parkway onto southbound East Bidwell Street. The length of this lane, which is understood to be a temporary improvement that is repurposed with the ultimate East Bidwell Street corridor improvements, should total approximately 250 feet.</p>	<p>B</p>	<p>CD (E), PW, FD</p>

<b>ARCHITECTURE/SITE DESIGN REQUIREMENTS</b>				
51.		<p>The Rockcross Subdivision project shall comply with the following architecture and design requirements:</p> <ol style="list-style-type: none"> <li>1. This approval is for one product line with four two-story master plans in three architectural styles with 9 color and material options. The applicant shall submit building plans that comply with this approval and the attached building elevations dated June 17, 2020.</li> <li>2. The design, materials, and colors of the single-family residential units shall be consistent with the approved building elevations, materials samples, and color schemes to the satisfaction of the Community Development Department.</li> <li>3. The Community Development Department shall approve the individual lot permits to assure no duplication or repetition of the same house, same roof-line, same elevation style, side-by-side, or across the street from each other.</li> <li>4. All mechanical equipment shall be ground-mounted and concealed from view of public streets, neighboring properties and nearby higher buildings.</li> <li>5. Decorative light fixtures, consistent with the Folsom Ranch Central District Design Guidelines and unique to each architectural design theme, shall be added to the front elevation of each Master Plan to the satisfaction of the Community Development Department.</li> <li>6. A minimum of one street tree shall be planted in the front yard of each residential lot within the subdivision. A minimum of two trees are required along the street-side of all corner lots. All front yard irrigation and landscaping shall be installed prior to a Building Permit Final.</li> </ol>	B	CD (P) (B)

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52.		<b><i>Trash/Recycling Containers and Air Conditioner Screening</i></b> Trash, recycling, and yard waste containers shall be placed behind the side yard fence so that they are not visible from the public right-of-way to the satisfaction of the Community Development Department. In addition, air conditioning units shall also be placed behind the side yard fence or located in the rear yard so that they are not visible from the public right-of-way to the satisfaction of the Community Development Department.	OG	CD (P) (E)
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<b>MITIGATION MEASURES</b>				
53.	✓	<p><b><i>Rockcross Subdivision Mitigation Monitoring Reporting Program (MMRP).</i></b> The conditions of approval below (numbered 53-1 to 53-89) implement the applicable mitigation measures from the FPASP (May 2011) MMRP, as amended by the Revised Proposed Water Supply Facility Alternative (November 2012), the Folsom South of U.S. Highway 50 Backbone Infrastructure Mitigated Negative Declaration (December 2014), and the Westland Eagle Specific Plan Amendment (September 2015).</p>		
Condition No.	Mitigation Number (Source)	Mitigation Measures	Timing	Responsible Agency
<b>AESTHETICS</b>				
53-1	3A.1-4 (FPASP EIR/EIS)	<p><b><i>Screen Construction Staging Areas.</i></b>            The project applicant(s) for any particular discretionary development application shall locate staging and material storage areas as far away from sensitive biological resources and sensitive land uses (e.g., residential areas, schools, parks) as feasible. Staging and material storage areas shall be approved by the appropriate agency (identified below) before the approval of grading plans for all project phases and shall be screened from adjacent occupied land uses in earlier development phases to the maximum extent practicable. Screens may include, but are not limited to, the use of such visual barriers such as berms or fences. The screen design shall be approved by the appropriate agency to further reduce visual effects to the extent possible.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries shall be developed by the project applicant(s) of each applicable project phase in consultation with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties, and Caltrans) to reduce to the extent feasible the visual effects of construction activities on adjacent project land uses that have already been developed.</p>	Before approval of grading plans and during construction for all project phases.	City of Folsom Community Development Department.
53-2	3A.1-5 (FPASP EIR/EIS)	<p><b><i>Establish and Require Conformance to Lighting Standards and Prepare and Implement a Lighting Plan.</i></b>            To reduce impacts associated with light and glare, the City shall:</p>	Before approval of building permits.	City of Folsom Community Development Department

		<ul style="list-style-type: none"> <li>▶ Establish standards for on-site outdoor lighting to reduce high-intensity nighttime lighting and glare as part of the Folsom Specific Plan design guidelines/standards. Consideration shall be given to design features, namely directional shielding for street lighting, parking lot lighting, and other substantial light sources, that would reduce effects of nighttime lighting. In addition, consideration shall be given to the use of automatic shutoffs or motion sensors for lighting features to further reduce excess nighttime light.</li>   <li>▶ Use shielded or screened public lighting fixtures to prevent the light from shining off of the surface intended to be illuminated.        To reduce impacts associated with light and glare, the project applicant(s) of all project phases shall:       <ul style="list-style-type: none"> <li>▶ Shield or screen lighting fixtures to direct the light downward and prevent light spill on adjacent properties.</li> <li>▶ Flood and area lighting needed for construction activities, nighttime sporting activities, and/or security shall be screened or aimed no higher than 45 degrees above straight down (half-way between straight down and straight to the side) when the source is visible from any off-site residential property or public roadway.</li> <li>▶ For public lighting in residential neighborhoods, prohibit the use of light fixtures that are of unusually high intensity or brightness (e.g., harsh mercury vapor, low-pressure sodium, or fluorescent bulbs) or that blink or flash.</li> <li>▶ Use appropriate building materials (such as low-glare glass, low-glare building glaze or finish, neutral, earth-toned colored paint and roofing materials), shielded or screened lighting, and appropriate signage in the office/commercial areas to prevent light and glare from adversely affecting motorists on nearby roadways.</li> <li>▶ Design exterior on-site lighting as an integral part of the building and landscape design in the Folsom Specific Plan area. Lighting fixtures shall be architecturally consistent with the overall site design.</li> </ul> </li> </ul>		
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		<ul style="list-style-type: none"> <li>▶ Lighting of off-site facilities within the City of Folsom shall be consistent with the City's General Plan standards.</li> <li>▶ Lighting of the off-site detention basin shall be consistent with Sacramento County General Plan standards.</li> </ul> <p>A lighting plan for all on- and off-site elements within each agency's jurisdictional boundaries (specified below) shall be submitted to the relevant jurisdictional agency for review and approval, which shall include the above elements. The lighting plan may be submitted concurrently with other improvement plans, and shall be submitted before the installation of any lighting or the approval of building permits for each phase. The project applicant(s) for any particular discretionary development application shall implement the approved lighting plan.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties).</p>		
<b>AIR QUALITY</b>				
53-3	3A.2-1a (FPASP EIR/EIS)	<p><b><i>Implement Measures to Control Air Pollutant Emissions Generated by Construction of On-Site Elements.</i></b></p> <p>To reduce short-term construction emissions, the project applicant(s) for any particular discretionary development application shall require their contractors to implement SMAQMD's list of Basic Construction Emission Control Practices, Enhanced Fugitive PM Dust Control Practices, and Enhanced Exhaust Control Practices (list below) in effect at the time individual portions of the site undergo construction. In addition to SMAQMD-recommended measures, construction operations shall comply with all applicable SMAQMD rules and regulations.</p>	Before the approval of all grading plans by the City and throughout project construction, where applicable, for all project phases.	City of Folsom Community Development Department

		<p><b><i>Basic Construction Emission Control Practices</i></b></p> <ul style="list-style-type: none"> <li>▶ Water all exposed surfaces two times daily. Exposed surfaces include, but are not limited to soil piles, graded areas, unpaved parking areas, staging areas, and access roads.</li> <li>▶ Cover or maintain at least two feet of free board space on haul trucks transporting soil, sand, or other loose material on the site. Any haul trucks that would be traveling along freeways or major roadways should be covered.</li> <li>▶ Use wet power vacuum street sweepers to remove any visible trackout mud or dirt onto adjacent public roads at least once a day. Use of dry power sweeping is prohibited.</li> <li>▶ Limit vehicle speeds on unpaved roads to 15 miles per hour (mph).</li> <li>▶ All roadways, driveways, sidewalks, parking lots to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used.</li> <li>▶ Minimize idling time either by shutting equipment off when not in use or reducing the time of idling to 5 minutes (as required by the state airborne toxics control measure [Title 13, Section 2485 of the California Code of Regulations]). Provide clear signage that posts this requirement for workers at the entrances to the site.</li> <li>▶ Maintain all construction equipment in proper working condition according to manufacturer's specifications. The equipment must be checked by a certified mechanic and determine to be running in proper condition before it is operated.</li> </ul> <p><b><i>Enhanced Fugitive PM Dust Control Practices – Soil Disturbance Areas</i></b></p> <ul style="list-style-type: none"> <li>▶ Water exposed soil with adequate frequency for continued moist soil. However, do not overwater to the extent that sediment flows off the site.</li> <li>▶ Suspend excavation, grading, and/or demolition activity when wind speeds exceed 20 mph.</li> </ul>		
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		<p>▶ Plant vegetative ground cover (fast-germinating native grass seed) in disturbed areas as soon as possible. Water appropriately until vegetation is established.</p> <p><b><i>Enhanced Fugitive PM Dust Control Practices – Unpaved Roads</i></b></p> <p>▶ Install wheel washers for all exiting trucks, or wash off all trucks and equipment leaving the site.</p> <p>▶ Treat site accesses to a distance of 100 feet from the paved road with a 6 to 12-inch layer of wood chips, mulch, or gravel to reduce generation of road dust and road dust carryout onto public roads.</p> <p>▶ Post a publicly visible sign with the telephone number and person to contact at the construction site regarding dust complaints. This person shall respond and take corrective action within 48 hours. The phone number of SMAQMD and the City contact person shall also be posted to ensure compliance.</p> <p><b><i>Enhanced Exhaust Control Practices</i></b></p> <p>▶ The project shall provide a plan, for approval by the City of Folsom Community Development Department and SMAQMD, demonstrating that the heavy-duty (50 horsepower [hp] or more) off-road vehicles to be used in the construction project, including owned, leased, and subcontractor vehicles, will achieve a project wide fleet-average 20% NOX reduction and 45% particulate reduction compared to the most current California Air Resources Board (ARB) fleet average that exists at the time of construction. Acceptable options for reducing emissions may include use of late-model engines, low-emission diesel products, alternative fuels, engine retrofit technology, after-treatment products, and/or other options as they become available. The project applicant(s) of each project phase or its representative shall submit to the City of Folsom Community Development Department and SMAQMD a comprehensive inventory of all off-road construction equipment, equal to or greater than 50 hp, that would be used an aggregate of 40 or more hours during any portion of the construction project. The inventory shall include the horsepower rating, engine production year, and projected hours of use for</p>		
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		<p>each piece of equipment. The inventory shall be updated and submitted monthly throughout the duration of the project, except that an inventory shall not be required for any 30-day period in which no construction activity occurs. At least 48 hours prior to the use of heavy-duty off-road equipment, the project representative shall provide SMAQMD with the anticipated construction timeline including start date, and name and phone number of the project manager and on-site foreman. SMAQMD's Construction Mitigation Calculator can be used to identify an equipment fleet that achieves this reduction (SMAQMD 2007a). The project shall ensure that emissions from all off-road diesel powered equipment used on the SPA do not exceed 40% opacity for more than three minutes in any one hour. Any equipment found to exceed 40 percent opacity (or Ringelmann 2.0) shall be repaired immediately, and the City and SMAQMD shall be notified within 48 hours of identification of noncompliant equipment. A visual survey of all in-operation equipment shall be made at least weekly, and a monthly summary of the visual survey results shall be submitted throughout the duration of the project, except that the monthly summary shall not be required for any 30-day period in which no construction activity occurs. The monthly summary shall include the quantity and type of vehicles surveyed as well as the dates of each survey. SMAQMD staff and/or other officials may conduct periodic site inspections to determine compliance. Nothing in this mitigation measure shall supersede other SMAQMD or state rules or regulations.</p> <p>► If at the time of construction, SMAQMD has adopted a regulation or new guidance applicable to construction emissions, compliance with the regulation or new guidance may completely or partially replace this mitigation if it is equal to or more effective than the mitigation contained herein, and if SMAQMD so permits.</p>		
53-4	3A.2-1b (FPASP EIR/EIS)	<p><b><i>Pay Off-site Mitigation Fee to SMAQMD to Off-Set NOX Emissions Generated by Construction of On-Site Elements.</i></b></p> <p>Implementation of the project or the other four other action alternatives would result in construction-generated NOX emissions that exceed the SMAQMD threshold of significance, even after implementation of the</p>	Before the approval of all grading plans by the City and throughout project	The City of Folsom Community Development Department shall not grant any grading permits to the respective project applicant(s) until the respective project