

# CONSTRUCTION PLANS 2023 SPORTSPLEX DR MAINTENANCE PROJECT

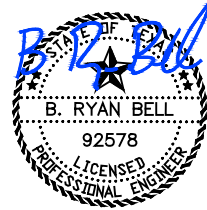
APRIL 2023

PROJECT # MAINT 2023-0001

WORK TYPE: MILL & OVERLAY, FULL DEPTH REPAIR, PAVEMENT MARKING & SIGNING  
PROJECT LENGTH: 2,338 LF

ROADWAY CLASSIFICATION:  
SPORTSPLEX DR - MC2 - 2 LANE MAJOR COLLECTOR

PREPARED BY:



4/27/2023

B. RYAN BELL, P.E.

DATE

RECOMMENDED BY:

*Chad Gilpin*

CHAD GILPIN, P.E. - CITY ENGINEER

4/27/2023

DATE

APPROVED BY:

CRAIG RICE, DEPUTY PUBLIC WORKS DIRECTOR

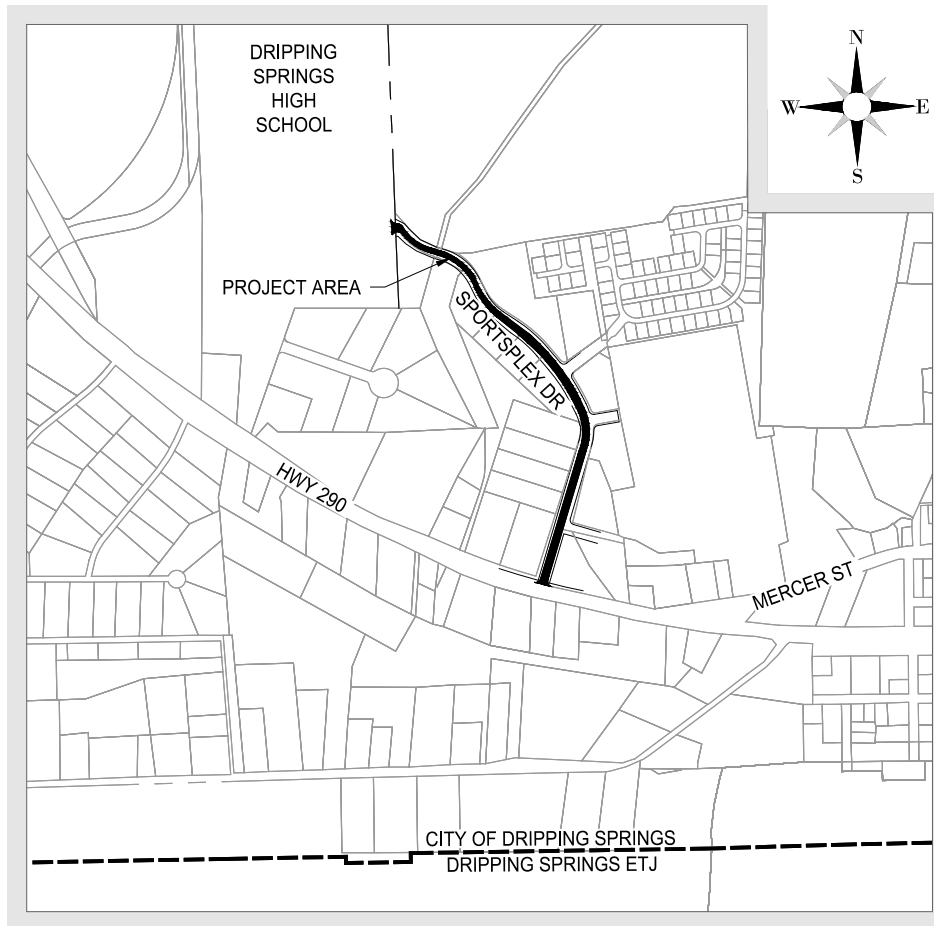
DATE

CONTRACTOR:

CONSTRUCTION START:

CONSTRUCTION ACCEPTED:

TOTAL CONSTRUCTION COST:



APPROX. SCALE: 1" = 1,000'

PREPARED FOR:

CITY OF DRIPPING SPRINGS, TEXAS



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REVISION BLOCK					
NO.	REVISION DESCRIPTION	AFFECTED SHEETS	DATE	APPROVAL SIGNATURE	APPROVAL DATE

NOTES:

1. THIS PROJECT LIES WITHIN THE CITY LIMITS OF DRIPPING SPRINGS, TEXAS.
2. THIS PROJECT LIES WITHIN THE CONTRIBUTING ZONE OF THE EDWARDS AQUIFER.
3. THIS PROJECT IS IDENTIFIED AS ZONE X 'AREA OF MINIMAL FLOOD HAZARD' BY THE FEDERAL MANAGEMENT AGENCY FLOOD INSURANCE RATE MAP COMMUNITY PANEL NO. 48209C0105F DATED SEPTEMBER 2, 2005 HAYS COUNTY, TEXAS AND INCORPORATED AREAS.
4. CONTRACTOR IS RESPONSIBLE FOR ANY ADDITIONAL SURVEY VERIFICATION REQUIRED TO COMPLETE THE PROJECT.
5. RIGHT-OF-WAY LINES SHOWN HEREON ARE APPROXIMATE.

THESE PLANS ARE FULL SIZE AT 11" X 17"

01 OF 18

GILP-BLK.ctb

PREPARED BY:



T.B.P.L.S. Firm Registration # 10193770  
T.B.P.E. Firm Registration # F-9266  
9701 BRODIE LN, SUITE 203  
AUSTIN, TX 78748  
PH: 512.220.8100

**A. GENERAL CONSTRUCTION NOTES**

- THE CONTRACTOR IS TO CONTACT ONE OF THE FOLLOWING FOR THE LOCATION OF EXISTING FACILITIES AT LEAST 48 HOURS PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION ACTIVITIES:
  - TEXAS EXCAVATION SAFETY SYSTEM (TESS) 1-800-245-4545
  - TEXAS ONE CALL SYSTEM (TOCS) 1-800-344-8377
- PRIOR TO ANY CONSTRUCTION, THE ENGINEER SHALL CONVENE A PRE-CONSTRUCTION CONFERENCE BETWEEN THE CITY, THE CONTRACTOR, OTHER UTILITY COMPANIES, ANY AFFECTED PARTIES AND ANY OTHER ENTITY THE CITY OR ENGINEER MAY REQUIRE.
- ALL CONSTRUCTION OPERATIONS SHALL BE ACCOMPLISHED IN ACCORDANCE WITH APPLICABLE REGULATIONS OF THE U.S. OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION. OSHA STANDARDS MAY BE PURCHASED FROM THE GOVERNMENT PRINTING OFFICE; INFORMATION AND RELATED REFERENCE MATERIALS MAY BE PURCHASED FROM OSHA, 611 E. 6TH STREET, AUSTIN, TEXAS.
- CONTRACTOR SHALL TAKE ALL DUE PRECAUTIONS TO PROTECT EXISTING FACILITIES FROM DAMAGE. ANY DAMAGE INCURRED TO EXISTING FACILITIES AS A RESULT OF CONSTRUCTION OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY THE CONTRACTOR, AT NO ADDITIONAL COST TO OWNER.
- CONTRACTOR TO GIVE NOTICE TO ALL AUTHORIZED INSPECTORS, SUPERINTENDENTS OR PERSONS IN CHARGE OF PUBLIC AND PRIVATE UTILITIES AFFECTED BY HIS OPERATIONS AT LEAST 48 HOURS PRIOR TO COMMENCEMENT OF WORK.
- CONTRACTOR TO COMPLY WITH ALL APPLICABLE LOCAL, STATE, AND FEDERAL REQUIREMENTS REGARDING EXCESS AND WASTE MATERIAL, INCLUDING METHODS OF HANDLING AND DISPOSAL.
- CONTRACTOR TO COORDINATE INTERRUPTIONS OF ALL UTILITIES AND SERVICES. ALL WORK TO BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE APPLICABLE UTILITY COMPANY OR AGENCY INVOLVED.
- WHEN UN-LOCATED OR INCORRECTLY LOCATED, A BREAK IN UTILITY LINES, OR OTHER UTILITIES AND SERVICES ARE ENCOUNTERED DURING SITE WORK OPERATIONS, CONTRACTOR SHALL NOTIFY THE APPLICABLE UTILITY COMPANY IMMEDIATELY TO OBTAIN PROCEDURE DIRECTIONS. CONTRACTOR SHALL COOPERATE WITH THE APPLICABLE UTILITY COMPANY IN MAINTAINING ACTIVE SERVICES IN OPERATION.
- WHEN CONSTRUCTION IS BEING CARRIED OUT WITHIN EASEMENTS, THE CONTRACTOR SHALL CONFINE HIS WORK TO WITHIN THE PERMANENT AND TEMPORARY EASEMENTS. PRIOR TO FINAL ACCEPTANCE, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING ALL TRASH AND DEBRIS WITHIN THE PERMANENT AND TEMPORARY EASEMENTS. CLEAN-UP SHALL BE TO THE SATISFACTION OF THE CITY.
- CONTRACTOR SHALL KEEP ACCURATE RECORDS OF ALL CONSTRUCTION THAT DEVIATES FROM THE PLANS. RECORD SHALL BE KEPT IN AN ONSITE SET OF MARKED-UP RECORD DRAWINGS.
- CONTRACTOR TO LOCATE, PROTECT, AND MAINTAIN BENCHMARKS, MONUMENTS, CONTROL POINTS AND PROJECT ENGINEERING REFERENCE POINTS. RE-ESTABLISH DISTURBED OR DESTROYED ITEMS BY REGISTERED PROFESSIONAL LAND SURVEYOR IN THE STATE OF TEXAS, AT NO ADDITIONAL COST TO OWNER.
- CONTRACTOR SHALL STRIP SIX (6) INCHES OF TOPSOIL FROM ALL AREAS SUBJECT TO GRADE MODIFICATION. REMOVE ALL AREAS OF WEAK SOIL.
- THE CONTRACTOR SHALL PROTECT ALL EXISTING FENCES. IN THE EVENT THAT A FENCE MUST BE REMOVED, THE CONTRACTOR SHALL REPLACE SAID FENCE OR PORTION THEREOF WITH THE SAME TYPE OF FENCING TO A QUALITY OF EQUAL OR BETTER THAN THE ORIGINAL FENCE.
- UPON COMPLETION OF THE PROJECT, THE SITE(S) AS DEFINED HEREIN SHALL BE CLEANED OF ALL DEBRIS AND LEFT IN A NEAT AND PRESENTABLE CONDITION.
- ALL ADJOINING PAVEMENT SECTIONS SHALL BE PROTECTED DURING ALL PHASES OF CONSTRUCTION AND ANY DAMAGES INCURRED DUE TO CONTRACTOR'S OPERATION SHALL BE REPAIRED AND/OR REPLACED AT THE CONTRACTOR'S EXPENSE.
- CONTRACTOR TO CONTROL DUST CAUSED BY THE WORK AND COMPLY WITH POLLUTION CONTROL REGULATIONS OF GOVERNING AUTHORITIES (NO SEPARATE PAY).
- TRAFFIC CONTROLS TO BE INSTALLED IN ACCORDANCE WITH THE CURRENT TxDOT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND TxDOT BARRICADE AND CONSTRUCTION STANDARDS.
- RE-VEGETATE ALL DISTURBED AREAS UPON COMPLETION OF THE WORK PER CITY CONSTRUCTION STANDARDS.
- CONTRACTOR TO EXERCISE CAUTION DURING CONSTRUCTION NEAR AND AROUND GAS LINES AND POWER LINES.
- ALL WORK IS TO BE PERFORMED BETWEEN THE FOLLOWING HOURS:  
8:00 A.M. TO 5:00 P.M. MONDAY - FRIDAY  
ALL WORK REQUIRING CITY INSPECTION SHALL BE PERFORMED MONDAY THRU FRIDAY. THE CITY RESERVES THE RIGHT TO REQUIRE THE CONTRACTOR TO UNCOVER ALL WORK PERFORMED WITHOUT INSPECTION.
- THE CONTRACTOR SHALL MAKE AN EXAMINATION OF THE PROJECT SITE AND COMPLETELY FAMILIARIZE HIMSELF WITH THE NATURE AND EXTENT OF ANY WORK TO BE ACCOMPLISHED. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY WORK MADE NECESSARY BY UNUSUAL CONDITIONS OR OBSTACLES ENCOUNTERED DURING THE PROGRESS OF THE WORK, WHEN SUCH CONDITIONS OR OBSTACLES ARE READILY APPARENT UPON A VISIT TO THE SITE. IF THERE ARE ANY QUESTIONS OF THIS REGARD OR IF THERE ARE ANY DISCREPANCIES BETWEEN THE PLANS AND ACTUAL SITE CONDITIONS THE CONTRACTOR SHALL NOTIFY THE ENGINEER PRIOR TO THE SUBMISSION OF BIDS.
- IN THOSE CASES WHERE FIXED FEATURES REQUIRE, THE DESIGN SLOPES INDICATED HEREIN AND ON THE CROSS SECTIONS MAY BE MODIFIED IN THE FIELD AS DETERMINED BY THE CITY IF EXISTING CONDITIONS SO REQUIRE.

- ACCESS TO RESIDENCES, BUSINESSES, AND DRIVEWAYS ALONG THE PROJECT MUST RECEIVE PRIORITY BY THE CONTRACTOR.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTION OF HIS MATERIALS AND EQUIPMENT FROM THEFT, VANDALISM, ANIMALS, FIRE, ETC. WHILE SAID MATERIALS AND EQUIPMENT ARE ON THE PROJECT, WHETHER STORED OR INSTALLED IN PLACE, UNTIL THE PROJECT HAS BEEN ACCEPTED BY THE CITY.

**B. ENVIRONMENTAL NOTES**

- THE CONTRACTOR TO INSTALL AND MAINTAIN EROSION/SEDIMENTATION CONTROLS AND TREE/NATURAL AREA PROTECTIVE FENCING PRIOR TO ANY SITE PREPARATION WORK (CLEARING, GRUBBING, GRADING, OR EXCAVATION). CONTRACTOR TO REMOVE EROSION/SEDIMENTATION CONTROLS AT THE COMPLETION OF THE PROJECT AND GRASS RESTORATION.
- THE PLACEMENT OF EROSION/SEDIMENTATION CONTROLS TO BE IN ACCORDANCE WITH THE APPROVED EROSION AND SEDIMENTATION CONTROL PLAN. DEVIATIONS FROM THE APPROVED PLAN MUST BE SUBMITTED TO AND APPROVED BY THE OWNER'S REPRESENTATIVE.
- ALL DISTURBED AREAS TO BE RESTORED UPON COMPLETION OF CONSTRUCTION. NO SEPARATE PAYMENT WILL BE MADE FOR RE-VEGETATION ACTIVITIES. ALL MATERIALS AND LABOR SHALL BE SUBSIDIARY TO OTHER BID ITEMS.
- RESTORATION TO BE ACCEPTABLE WHEN THE GRASS HAS GROWN AT LEAST 1-1/2 INCHES HIGH WITH 85% COVERAGE, PROVIDED NO BARE SPOTS LARGER THAN 25 SQUARE FEET EXIST.
- A MINIMUM OF FOUR (4) INCHES OF TOPSOIL TO BE PLACED IN ALL AREAS DISTURBED BY CONSTRUCTION.
- THE CONTRACTOR TO SEED, SOD OR HYDROMULCH ALL EXPOSED CUTS AND FILLS UPON COMPLETION OF CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE FOR ALL IRRIGATION WATER REQUIRED TO ESTABLISH GRASS TO THE REQUIRED 85% COVERAGE.
- EROSION AND SEDIMENTATION CONTROLS TO BE INSTALLED OR MAINTAINED IN A MANNER WHICH DOES NOT RESULT IN SOIL BUILDUP WITHIN TREE DRIPLINE.
- TO AVOID SOIL COMPACTION, CONTRACTOR SHALL NOT ALLOW VEHICULAR TRAFFIC, PARKING, OR STORAGE OF EQUIPMENT OR MATERIALS IN THE TREE DRIPLINE AREAS.
- WHERE A FENCE IS CLOSER THAN FOUR (4) FEET TO A TREE TRUNK, PROTECT THE TRUNK WITH STRAPPED-ON PLANKING TO A HEIGHT OF EIGHT (8) FEET (OR TO THE LIMITS OF LOWER BRANCHING) IN ADDITION TO THE FENCING.
- TREES TO BE REMOVED IN A MANNER WHICH DOES NOT IMPACT TREES TO BE PRESERVED.
- ANY ROOT EXPOSED BY THE CONSTRUCTION ACTIVITY TO BE PRUNED FLUSH WITH THE SOIL. BACKFILL ROOT AREAS WITH GOOD QUALITY TOPSOIL AS SOON AS POSSIBLE. IF EXPOSED ROOT AREAS ARE NOT BACKFILLED WITHIN TWO DAYS, COVER THEM WITH ORGANIC MATTER IN A MANNER WHICH REDUCES SOIL TEMPERATURE AND MINIMIZES WATER LOSS DUE TO EVAPORATION.
- CONTRACTOR TO PRUNE VEGETATION TO PROVIDE CLEARANCE FOR STRUCTURES, VEHICULAR TRAFFIC, AND EQUIPMENT BEFORE DAMAGE OCCURS (RIPPING OF BRANCHES, ETC.) ALL FINISHED PRUNING TO BE DONE ACCORDING TO RECOGNIZED, APPROVED STANDARDS OF THE INDUSTRY (REFERENCE THE "**NATIONAL ARBORIST ASSOCIATION PRUNING STANDARDS FOR SHADE TREES**").
- THE CONTRACTOR IS TO INSPECT THE CONTROLS AT WEEKLY INTERVALS AND AFTER EVERY RAINFALL EXCEEDING 1/4 INCH TO VERIFY THAT THEY HAVE NOT BEEN SIGNIFICANTLY DISTURBED. ANY ACCUMULATED SEDIMENT AFTER A SIGNIFICANT RAINFALL TO BE REMOVED AND PLACED IN THE OWNER DESIGNATED SPOIL DISPOSAL.

**C. EROSION & SEDIMENT CONTROL - SEQUENCE OF CONSTRUCTION:**

- TEMPORARY EROSION AND SEDIMENTATION CONTROLS ARE TO BE INSTALLED AS INDICATED ON THE APPROVED SITE PLAN CONSTRUCTION PLAN AND IN ACCORDANCE WITH THE EROSION SEDIMENTATION CONTROL PLAN (ESC) AND STORMWATER POLLUTION PREVENTION PLAN (SWPPP) THAT IS REQUIRED TO BE POSTED ON THE SITE. INSTALL TREE PROTECTION, INITIATE TREE MITIGATION MEASURES AND CONDUCT "PRE - CONSTRUCTION" TREE FERTILIZATION (IF APPLICABLE).
- THE ENVIRONMENTAL PROJECT MANAGER, AND/OR SITE SUPERVISOR, AND/OR DESIGNATED RESPONSIBLE PARTY, AND THE GENERAL CONTRACTOR WILL FOLLOW THE EROSION SEDIMENTATION CONTROL PLAN (ESC) AND STORM WATER POLLUTION PREVENTION PLAN (SWPPP) POSTED ON THE SITE. TEMPORARY EROSION AND SEDIMENTATION CONTROLS WILL BE REVISED, IF NEEDED, TO COMPLY WITH CITY INSPECTORS' DIRECTIVES, AND REVISED CONSTRUCTION SCHEDULE RELATIVE TO THE WATER QUALITY PLAN REQUIREMENTS AND THE EROSION PLAN.
- THE TEMPORARY EROSION AND SEDIMENTATION CONTROLS WILL BE INSPECTED AND MAINTAINED IN ACCORDANCE WITH THE EROSION SEDIMENTATION CONTROL PLAN (ESC) AND STORM WATER POLLUTION PREVENTION PLAN (SWPPP) POSTED ON THE SITE.
- BEGIN SITE CLEARING/CONSTRUCTION (OR DEMOLITION) ACTIVITIES.
- COMPLETE CONSTRUCTION AND START RE-VEGETATION OF THE SITE AND INSTALLATION OF LANDSCAPING.
- AFTER A FINAL INSPECTION HAS BEEN CONDUCTED BY THE CITY INSPECTOR AND WITH APPROVAL FROM THE CITY INSPECTOR, REMOVE THE TEMPORARY EROSION AND SEDIMENTATION CONTROLS AND COMPLETE ANY NECESSARY FINAL RE-VEGETATION RESULTING FROM REMOVAL OF THE CONTROLS. CONDUCT ANY MAINTENANCE AND REHABILITATION OF THE WATER QUALITY PONDS OR CONTROLS.

**D. STREET AND DRAINAGE NOTES:**

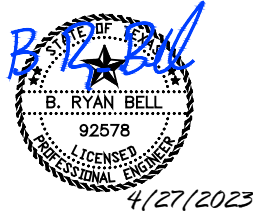
- ALL TESTING SHALL BE DONE BY AN INDEPENDENT LABORATORY AT THE CITY'S EXPENSE. ANY RETESTING SHALL BE PAID FOR BY THE CONTRACTOR. A CITY INSPECTOR SHALL BE PRESENT DURING ALL TESTS. TESTING SHALL BE COORDINATED WITH THE CITY INSPECTOR AND HE SHALL BE GIVEN A MINIMUM OF 24 HOURS NOTICE PRIOR TO ANY TESTING.
- DEPTH OF COVER FOR ALL CROSSINGS UNDER PAVEMENT INCLUDING GAS, ELECTRIC, TELEPHONE, CABLE TV, WATER SERVICES, ETC., SHALL BE A MINIMUM OF 30" BELOW SUBGRADE UNLESS OTHERWISE SPECIFIED ON THE PLAN
- ALL R.C.P. SHALL BE MINIMUM CLASS IV UNLESS OTHERWISE NOTED ON THE PLANS.

**E. TRAFFIC CONTROL:**

- REFER TO TRAFFIC CONTROL PLANS FOR RECOMMENDED SEQUENCE OF CONSTRUCTION.
- ACCESS TO ALL DRIVES AND SIDE ROADS ARE TO BE MAINTAINED AT ALL TIMES.
- ALL LANES SHALL BE OPENED TO TRAFFIC AT THE END OF EACH WORK DAY.
- TRAFFIC CONTROL PLANS SHOWN WITHIN THESE PLANS ARE A MINIMUM REQUIREMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY ADDITIONAL SIGNS, BARRICADES, FLAGMEN OR OTHER TRAFFIC CONTROL DEVICES AS NECESSARY FOR THE SAFETY OF THE TRAVELING PUBLIC. ALL TRAFFIC CONTROLS SHALL BE COMPLIANT WITH THE CURRENT TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- THE CONTRACTOR MAY SUBMIT ALTERNATE TRAFFIC CONTROL PLANS TO THE CITY ENGINEER FOR REVIEW AND APPROVAL.



ENGINEER'S SEAL:



COPYRIGHT NOTICE:  
THE DESIGNS REPRESENTED BY THESE PLANS ARE COPYRIGHTED AND ARE SUBJECT TO COPYRIGHT PROTECTION UNDER 17 U.S.C. §101, ET SEQ., AS AMENDED. UNAUTHORIZED USE OF THESE PLANS OR THE DESIGNS REPRESENTED THEREIN WILL SUBJECT THE INFRINGER TO DAMAGES AND/OR JUDICIAL ACTION AS PROVIDED BY FEDERAL LAW.

REVISIONS:

NO.	REVISION	DATE

DATE: 4/27/2023  
 DESIGNED BY: DC  
 CHECKED BY: RB  
 PROJ #: MAINT-2023-0001



PROJECT:  
**2023 SPORTSPLEX DR MAINTENANCE PROJECT**

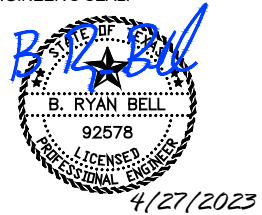
SHEET TITLE:  
**GENERAL NOTES**

SCHEDULE OF QUANTITIES:



T.B.P.L.S. Firm Registration # 10193770  
 T.B.P.E. Firm Registration # F-8266  
 9701 BRODIE LANE #203  
 AUSTIN, TX 78748  
 PH: 512.220.8100

ENGINEER'S SEAL:



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PROJECT:  
 2023 SPORTSPLEX DR  
 MAINTENANCE  
 PROJECT

SHEET TITLE:  
 SCHEDULE OF  
 QUANTITIES

TXDOT SPEC	ITEM DESCRIPTION	UNITS	(1)	(2)	(1+2)	(3)	(1+3)	(4)	(1+3+4)
			BASE BID (2" MILL & OVERLAY WITH 2" HMAC SPOT REPAIRS)	BASE BID-A (SUBSTITUTE HMAC TY B WITH HMAC TY D)	BASE BID + BASE BID-A TOTALS	ADD ALT 1 (MILL 4" & OVERLAY 2" TY B & 2" TY D HMAC)	BASE BID + ADD ALT 1 TOTALS	ADD ALT 1-A (SUBSTITUTE HMAC TY B WITH HMAC TY D)	BASE BID + ADD ALT 1 + ADD ALT 1-A TOTALS
0310 6001	PRIME COAT (MULTI OPTION)	GAL	1,743	0	1,743	0	1,743	0	1,743
* 0351 6013	FLEXIBLE PAVEMENT STRUCTURE REPAIR(4")	SY	2,745	0	2,745	-2,745	0	0	0
*** 0351 6013	FLEXIBLE PAVEMENT STRUCTURE REPAIR(4")	SY	0	0	0	8,712	8,712	0	8,712
* 0354 6002	PLAN & TEXT ASPH CONC PAV(0" TO 2")	SY	5,967	0	5,967	-5,967	0	0	0
*** 0354 6004	PLAN & TEXT ASPH CONC PAV(0" TO 4")	SY	0	0	0	8,712	8,712	0	8,712
0500 6001	MOBILIZATION	LS	1	0	1	0	1	0	1
0502 6001	BARRICADES, SIGNS AND TRAFFIC HANDLING	MO	1	0	1	0	1	0	1
0644 6060	IN SM RD SN SUP&AM TYTWT(1)WS(P)	EA	9	0	9	0	9	0	9
0666 6036	REFL PAV MRK TY I (W)8"(SLD)(100MIL)	LF	314	0	314	0	314	0	314
0666 6048	REFL PAV MRK TY I (W)24"(SLD)(100MIL)	LF	230	0	230	0	230	0	230
0666 6054	REFL PAV MRK TY I (W)(ARROW)(100MIL)	EA	6	0	6	0	6	0	6
0666 6078	REFL PAV MRK TY I (W)(WORD)(100MIL)	EA	9	0	9	0	9	0	9
0666 6126	REFL PAV MRK TY I (Y)4"(SLD)(100MIL)	LF	4,947	0	4,947	0	4,947	0	4,947
0666 6127	REFL PAV MRK TY I (Y)4"(BRK)(100MIL)	LF	451	0	451	0	451	0	451
0666 6141	REFL PAV MRK TY I (Y)12"(SLD)(100MIL)	LF	123	0	123	0	123	0	123
0666 6178	REFL PAV MRK TY II (W) 8" (SLD)	LF	30	0	30	0	30	0	30
0666 6182	REFL PAV MRK TY II (W) 24" (SLD)	LF	126	0	126	0	126	0	126
0666 6192	REFL PAV MRK TY II (W) (WORD)	EA	1	0	1	0	1	0	1
0666 6207	REFL PAV MRK TY II (Y) 4" (SLD)	LF	62	0	62	0	62	0	62
0672 6007	REFL PAV MRKR TY I-C	EA	16	0	16	0	16	0	16
0672 6009	REFL PAV MRKR TY II-A-A	EA	105	0	105	0	105	0	105
0677 6007	ELIM EXT PAV MRK & MRKS (24")	LF	126	0	126	0	126	0	126
*, **, ***, **** 3076 6003	D-GR HMA TY-B PG64-22 (EXEMPT)(2 IN)	SY	2,745	-2,745	0	5,967	8,712	-8,712	0
*, **, ***, **** 3076 6081	D-GR HMA TY-D PG70-22 (EXEMPT)(2 IN)	SY	8,712	-2,745	5,967	0	8,712	-8,712	0
**, **** 3076 6081	D-GR HMA TY-D PG70-22 (EXEMPT)(4 IN)	SY	0	2,745	2,745	0	0	8,712	8,712

- \* - ITEM APPLIES TO BASE BID
- \*\* - ITEM APPLIES TO BASE BID-A
- \*\*\* - ITEM APPLIES TO ADD ALT 1
- \*\*\*\* - ITEM APPLIES TO ADD ALT 1-A

**\*BASE BID**

PERFORM SPOT PAVEMENT REPAIRS WITH TY B HMAC. MILL 2" EXISTING ASPHALT, PLACE PRIME COAT ON EXISTING BASE, PLACE 2" HMAC TY D OVERLAY, INSTALL SIGNS AND PAVEMENT MARKINGS.

**\*\*BASE BID-A**

SAME WORK AS BASE BID, CONTRACTOR MAY CHOOSE TO USE HMAC TY D PG 70-22 IN PLACE OF HMAC TY B PG64-22.

**\*\*\*ADD ALT 1**

MILL 4" OF EXISTING ASPHALT AND BASE MATERIAL FOR ENTIRE ROADWAY LIMITS SHOWN IN PLANS. PLACE PRIME COAT, 2" HMAC TY B PG64-22, AND 2" HMAC TY D PG70-22 OVERLAY. BASE BID SIGNS AND PAVEMENT MARKINGS DO NOT CHANGE.

**\*\*\*\*ADD ALT 1-A**

SAME WORK AS ADD ALT-1, CONTRACTOR MAY CHOOSE TO USE HMAC TY D PG 70-22 IN PLACE OF HMAC TY B PG64-22.

**NOTES RELATED TO PAY ITEMS AND SPECIFICATIONS**

WHERE HAYS COUNTY SPECIFICATIONS FOR ROADWAY DESIGN, PAVING AND DRAINAGE ARE IN CONFLICT WITH TXDOT SPECIFICATIONS LISTED ABOVE HAYS COUNTY SPECIFICATIONS SHALL SUPERSEDE. WHERE ADDITIONAL INFORMATION PROVIDED BELOW CONFLICTS WITH EITHER THE TXDOT OR HAYS COUNTY SPECIFICATIONS THE INFORMATION BELOW SHALL SUPERSEDE.

TXDOT ITEM 0351 - FLEXIBLE PAVEMENT STRUCTURE REPAIR

\*FOR BASE BID WORK: SAWCUTTING, REMOVAL OF EXISTING MATERIAL, RESHAPING, PREPARING AND COMPACTING EXISTING BASE MATERIAL IS SUBSIDIARY TO THIS ITEM FOR AREAS IDENTIFIED AS PAVEMENT REPAIR IN THE PLANS.

\*\*\*FOR ADD ALT 1 WORK: RESHAPING, PREPARING AND COMPACTING EXISTING BASE MATERIAL IS SUBSIDIARY TO THIS ITEM FOR THE ENTIRE ROADWAY SHOWN IN THE PLANS. REMOVAL OF EXISTING ASPHALT AND BASE MATERIAL IS PAID FOR BY ITEM 0354 FOR ADD ALT 1 WORK.

TXDOT ITEM 0678 - PAVEMENT SURFACE PREPARATION FOR MARKINGS

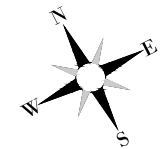
NO SEPARATE PAYMENT WILL BE MADE FOR PAVEMENT SURFACE PREPARATION, THIS IS SUBSIDIARY TO APPLICABLE BID ITEMS.

TXDOT ITEM 3076 - HOT MIX ASPHALT CONCRETE PAVEMENT

HMAC SHALL BE PAID FOR BY THE SQUARE YARD (SY).  
 HMAC SHALL BE APPLIED AT THE FOLLOWING RATES:

HMAC TY B	115 LB/SY/IN
HMAC TY D	115 LB/SY/IN
PRIME COAT	0.2 GAL/SY

\*\*, \*\*\*\* - CONTRACTOR MAY CHOOSE TO USE HMAC TY D PG 70-22 IN PLACE OF HMAC TY B PG64-22.



**LEGEND**

- MILL AND OVERLAY  
(SEE DETAIL SHEET 7)
- BASE BID PAVEMENT REPAIR  
(SEE DETAIL SHEET 7)
- EXIST CONCRETE PAVEMENT  
TO REMAIN
- EXIST R.O.W.  
(APPROX LINEWORK PER 2003  
CMA SPORTSPLEX DR PLANS)

**NOTES:**

1. CONTRACTOR TO MATCH EXISTING GRADE, MAINTAIN EXISTING DRAINAGE CONTOURS & DRAINAGE PATTERNS UNLESS OTHERWISE NOTED.
2. CONTRACTOR TO RE-VEGETATE ALL DISTURBED AREAS UPON COMPLETION OF THE WORK IN COMPLIANCE WITH THE ENVIRONMENTAL NOTES AND SPECIFICATIONS IN THESE DOCUMENTS.
3. CONTRACTOR SHALL ADJUST TO PROPOSED FINISHED GRADE ALL EXISTING VALVES, MANHOLES, MANHOLE COVERS, CLEANOUTS, INLET COVERS AND/OR ANY OTHER OBJECTS WITHIN THE PROJECT AREA, IF APPLICABLE.
4. CONTRACTOR SHALL SAW-CUT AND/OR ADJUST EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH AND CONTINUOUS TRANSITION GRADE.
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**ADD ALT 1 NOTES:**

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**GILPIN**  
ENGINEERING COMPANY

T.B.P.L.S. Firm Registration # 10193770  
T.B.P.E. Firm Registration # F-8266  
9701 BRODIE LANE #203  
AUSTIN, TX 78748  
PH: 512.220.8100

ENGINEER'S SEAL:

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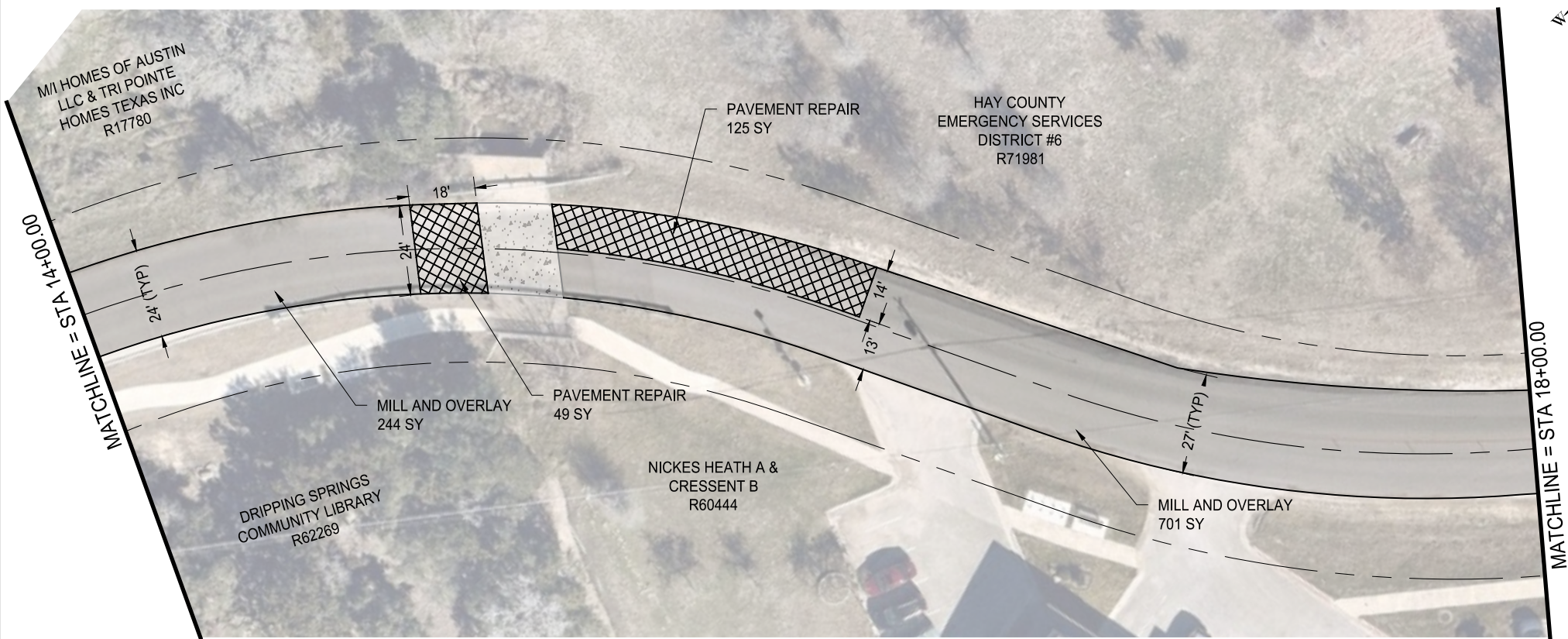
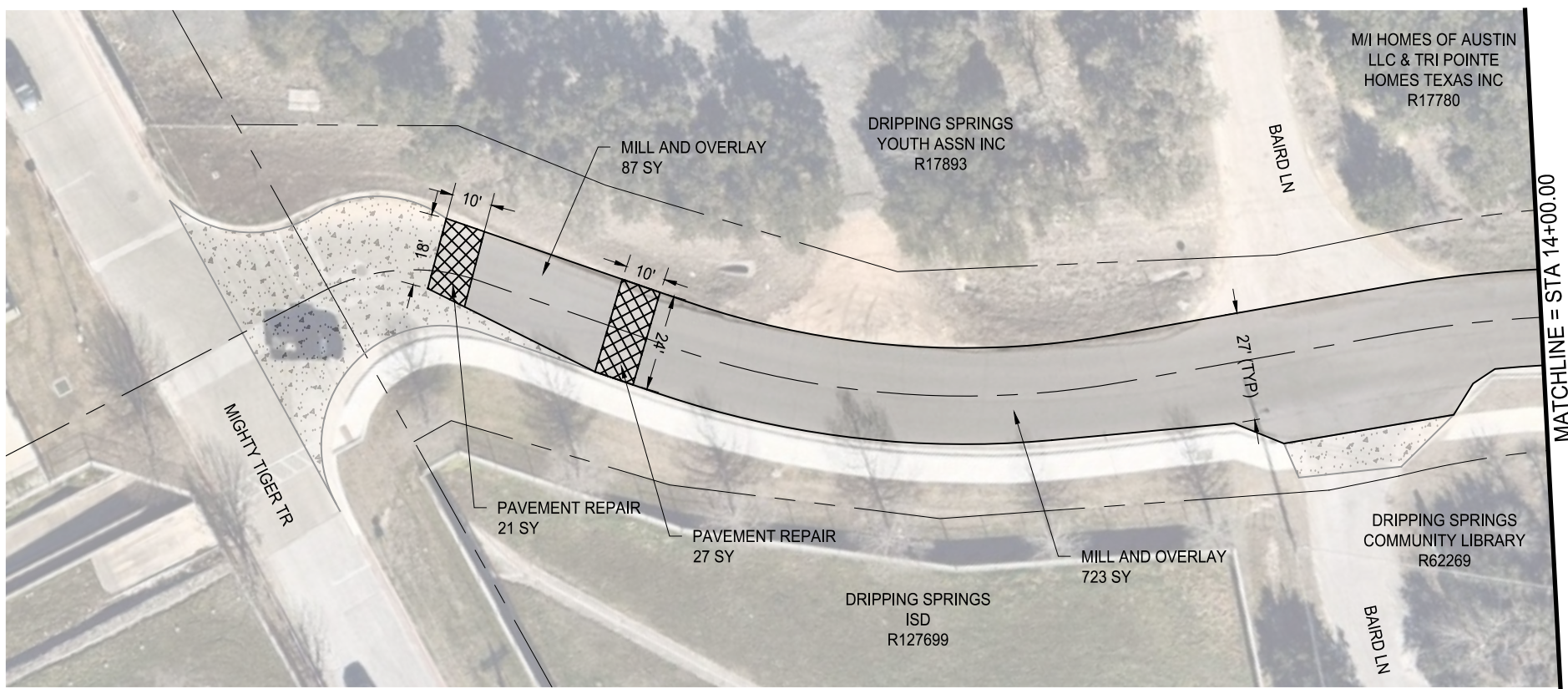
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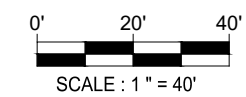
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PROJ #: MAINT-2023-0001



PROJECT:  
**2023 SPORTSPLEX DR  
MAINTENANCE  
PROJECT**

SHEET TITLE:  
**PAVING PLAN**





**GILPIN**  
ENGINEERING COMPANY

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9701 BRODIE LANE #203  
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**LEGEND**

- MILL AND OVERLAY  
(SEE DETAIL SHEET 7)
- BASE BID PAVEMENT REPAIR  
(SEE DETAIL SHEET 7)
- EXIST CONCRETE PAVEMENT  
TO REMAIN
- EXIST R.O.W.  
(APPROX LINEWORK PER 2003  
CMA SPORTSPLEX DR PLANS)

ENGINEER'S SEAL:

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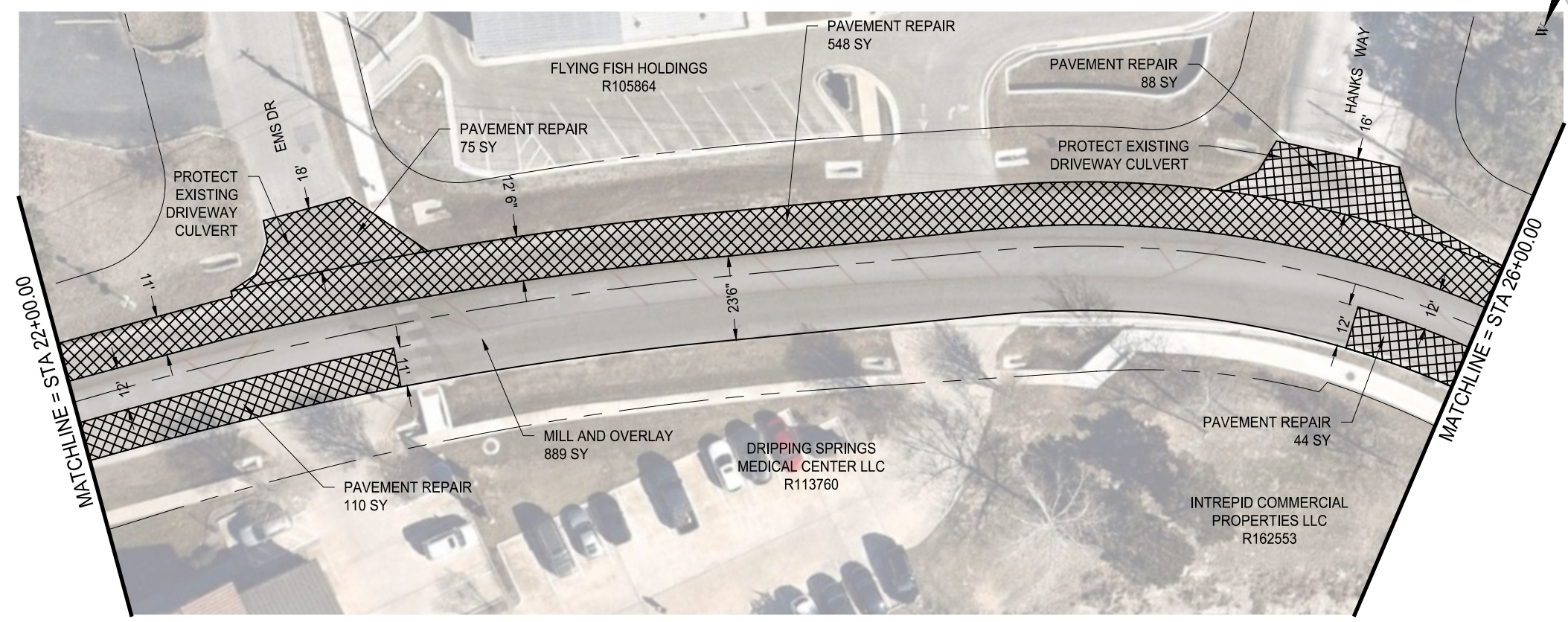
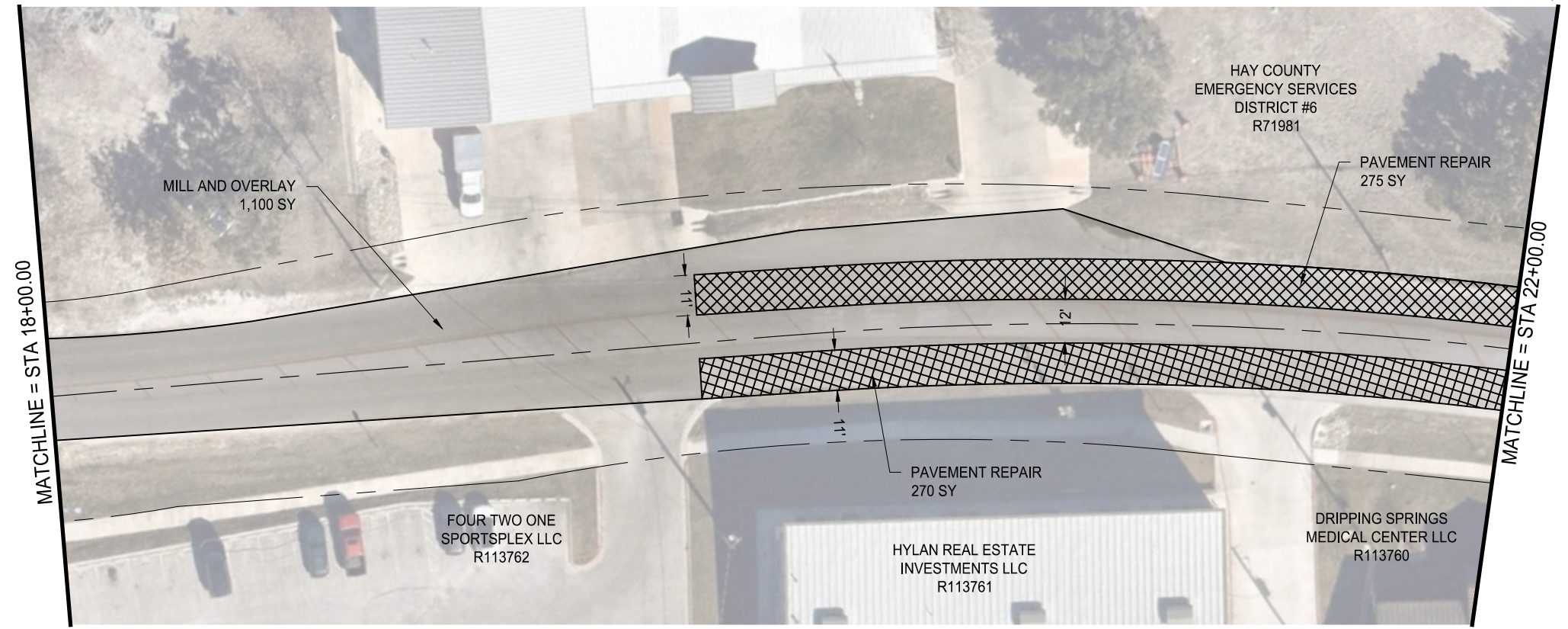
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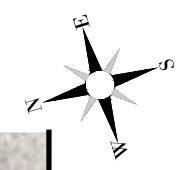
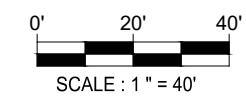
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PROJECT:  
**2023 SPORTSPLEX DR  
MAINTENANCE  
PROJECT**

SHEET TITLE:  
**PAVING PLAN**





**GILPIN**  
ENGINEERING COMPANY

T.B.P.L.S. Firm Registration # 10193770  
T.B.P.E. Firm Registration # F-9266  
9701 BRODIE LANE #203  
AUSTIN, TX 78748  
PH: 512.220.8100

**LEGEND**

- MILL AND OVERLAY (SEE DETAIL SHEET 7)
- BASE BID PAVEMENT REPAIR (SEE DETAIL SHEET 7)
- EXIST CONCRETE PAVEMENT TO REMAIN
- EXIST R.O.W. (APPROX LINEWORK PER 2003 CMA SPORTSPLEX DR PLANS)

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**ADD ALT 1 NOTES:**

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ENGINEER'S SEAL:



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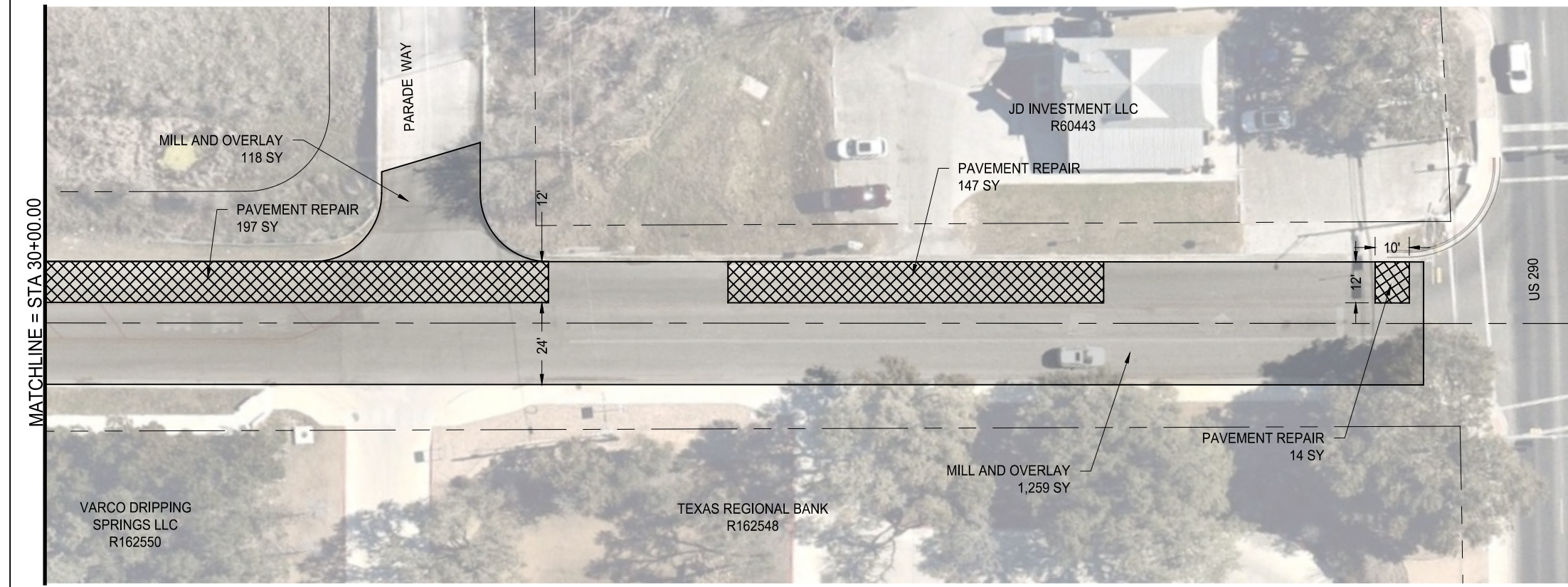
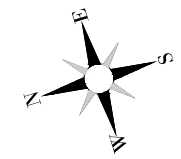
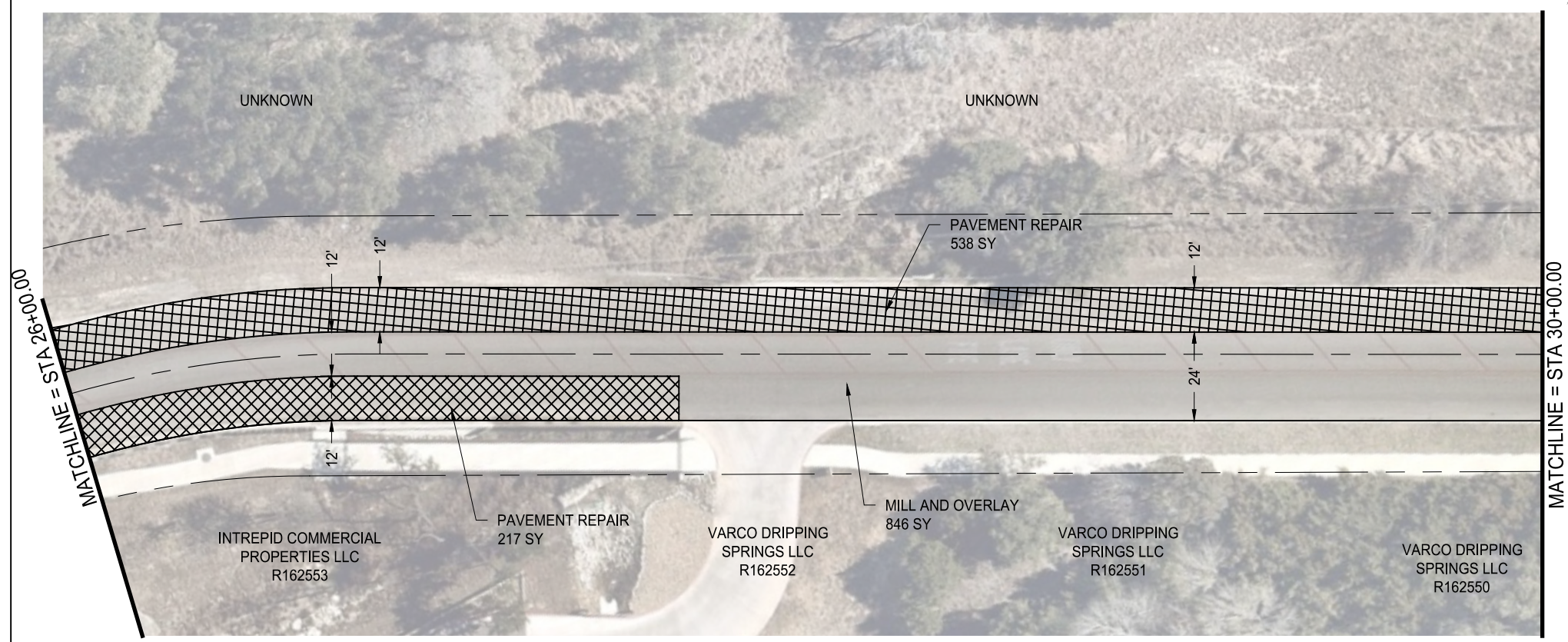
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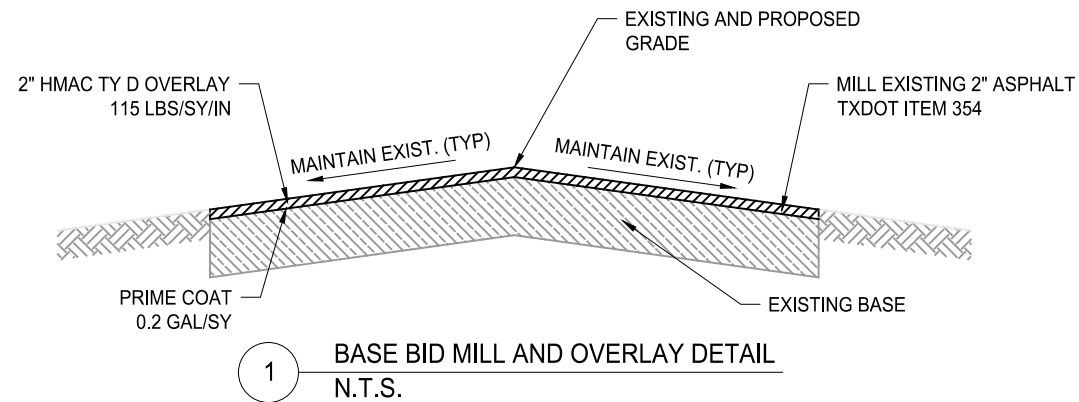
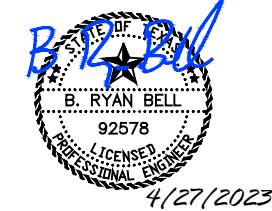


PROJECT:  
**2023 SPORTSPLEX DR MAINTENANCE PROJECT**

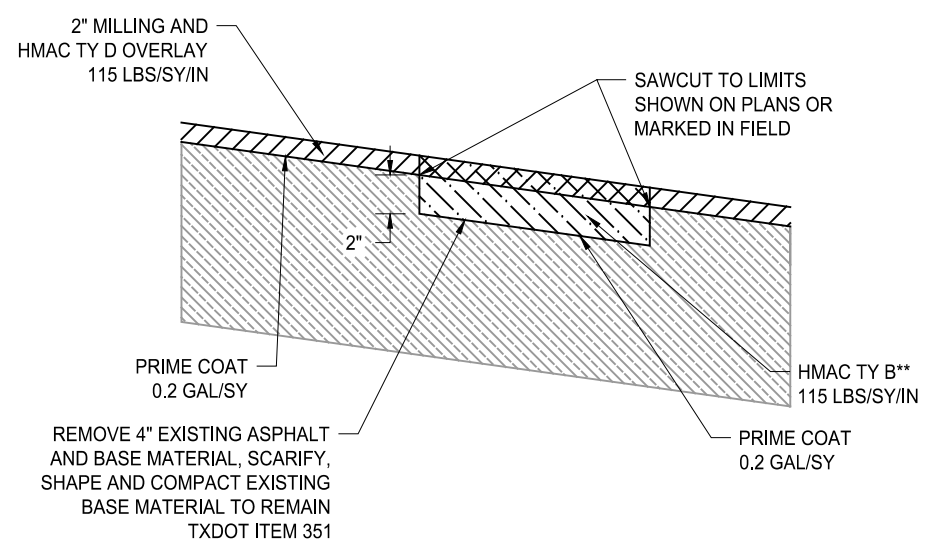
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**PAVING PLAN**



ENGINEER'S SEAL:

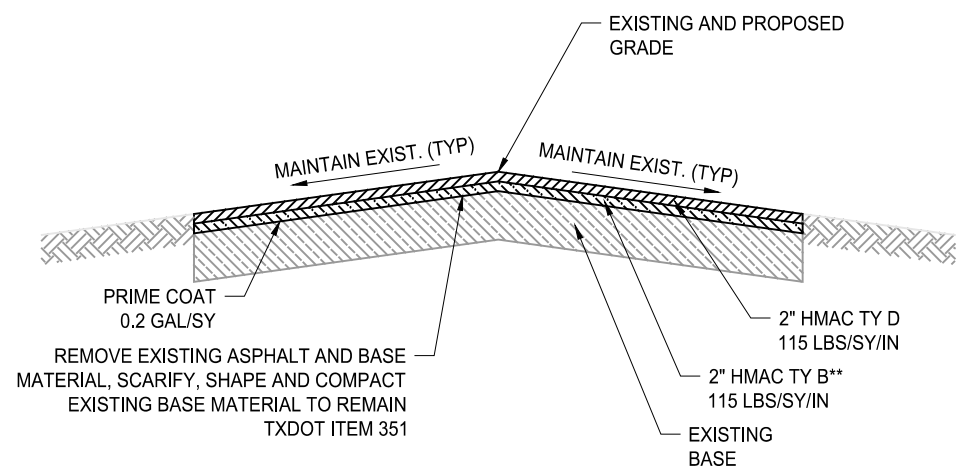


1 BASE BID MILL AND OVERLAY DETAIL  
N.T.S.



2 BASE BID PAVEMENT REPAIR DETAIL  
N.T.S.

NOTE: CONTRACTOR SHALL MARK LIMITS OF PAVEMENT REPAIR FOR REVIEW BY THE CITY ENGINEER PRIOR TO COMMENCING WORK.



3 ADD ALT 1 MILL AND OVERLAY & PAVEMENT REPAIR DETAIL  
N.T.S.

NOTES:

1. ALL HMAC TY D SHALL BE PG70-22.
  2. PLACE PRIME COAT ON BASE OR SUBGRADE SURFACE PRIOR TO PLACING NEW HMAC MATERIAL.
- \*\* BASE BID-A: CONTRACTOR MAY CHOOSE TO USE HMAC TY D PG 70-22 IN PLACE OF HMAC TY B PG64-22.

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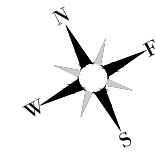
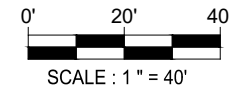
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PROJECT:  
2023 SPORTSPLEX DR MAINTENANCE PROJECT

SHEET TITLE:  
PAVING DETAILS



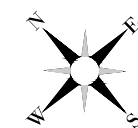
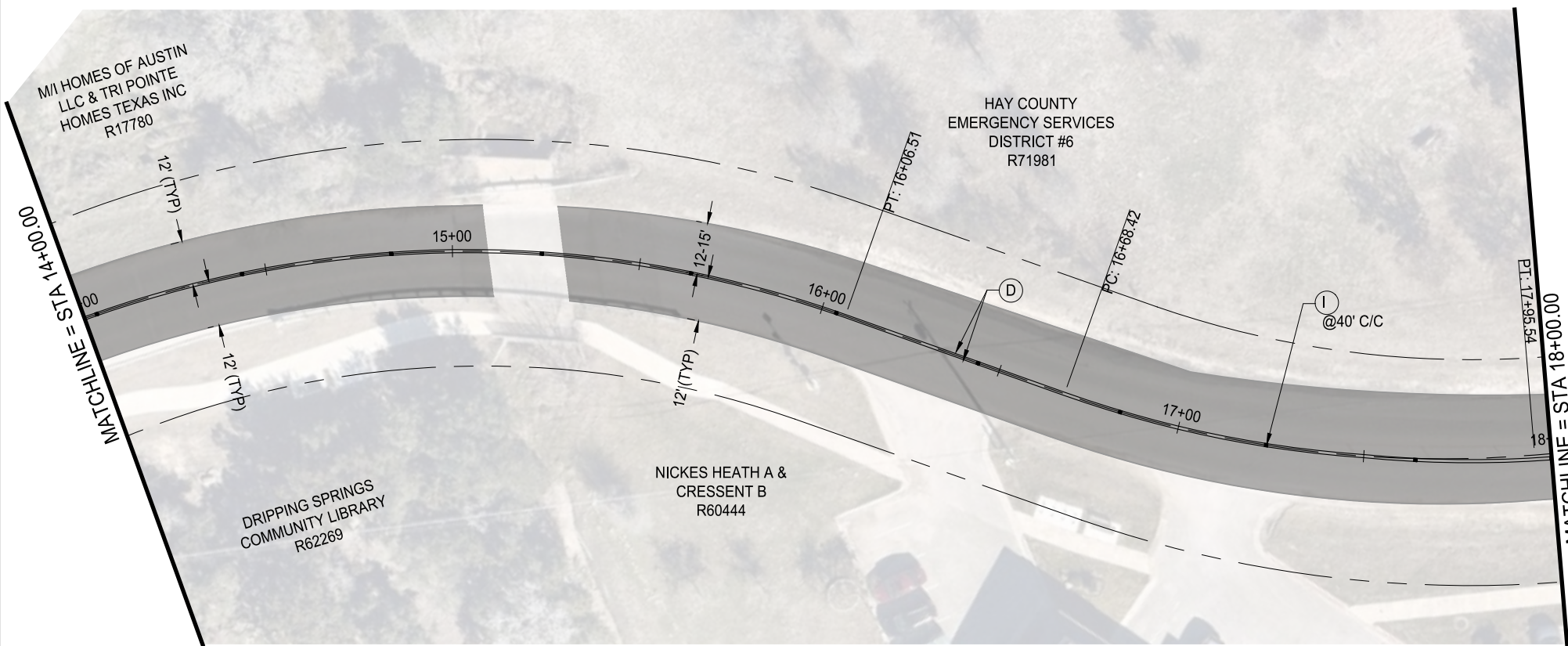
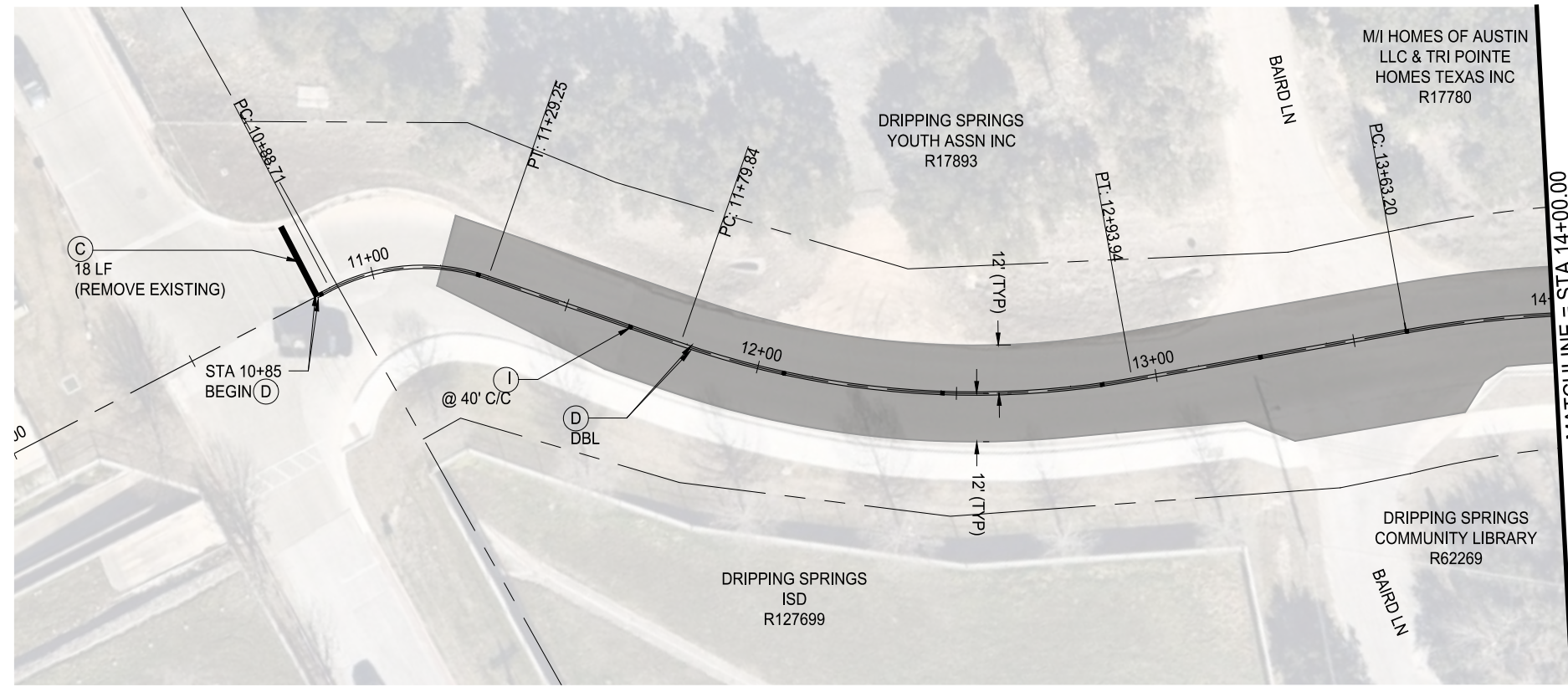
**LEGEND**

- (A) REFL PAV MRK (W) (SLD) 8"
- (B) REFL PAV MRK (W) (SLD) 12"
- (C) REFL PAV MRK (W) (SLD) 24"
- (D) REFL PAV MRK (Y) (SLD) 4"
- (E) REFL PAV MRK (Y) (BRK) 4"
- (F) REFL PAV MRK (W) (ARROW)
- (G) REFL PAV MRK (W) (WORD)
- (H) REFL PAV MRKR TY I-C @ 20' C/C
- (I) REFL PAV MRKR TY II-A-A
- (J) REFL PAV MRK (Y) (SLD) 12"

--- --- --- --- --- EXIST R.O.W.  
(APPROX LINEWORK PER 2003  
CMA SPORTSPLEX DR PLANS)

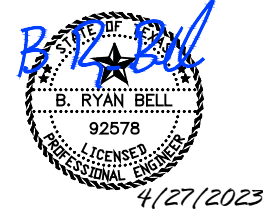
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T.B.P.L.S. Firm Registration # 10193770  
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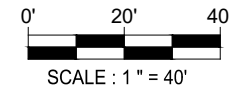
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PROJECT:  
**2023 SPORTSPLEX DR MAINTENANCE PROJECT**

SHEET TITLE:  
**STRIPING PLAN**





**LEGEND**

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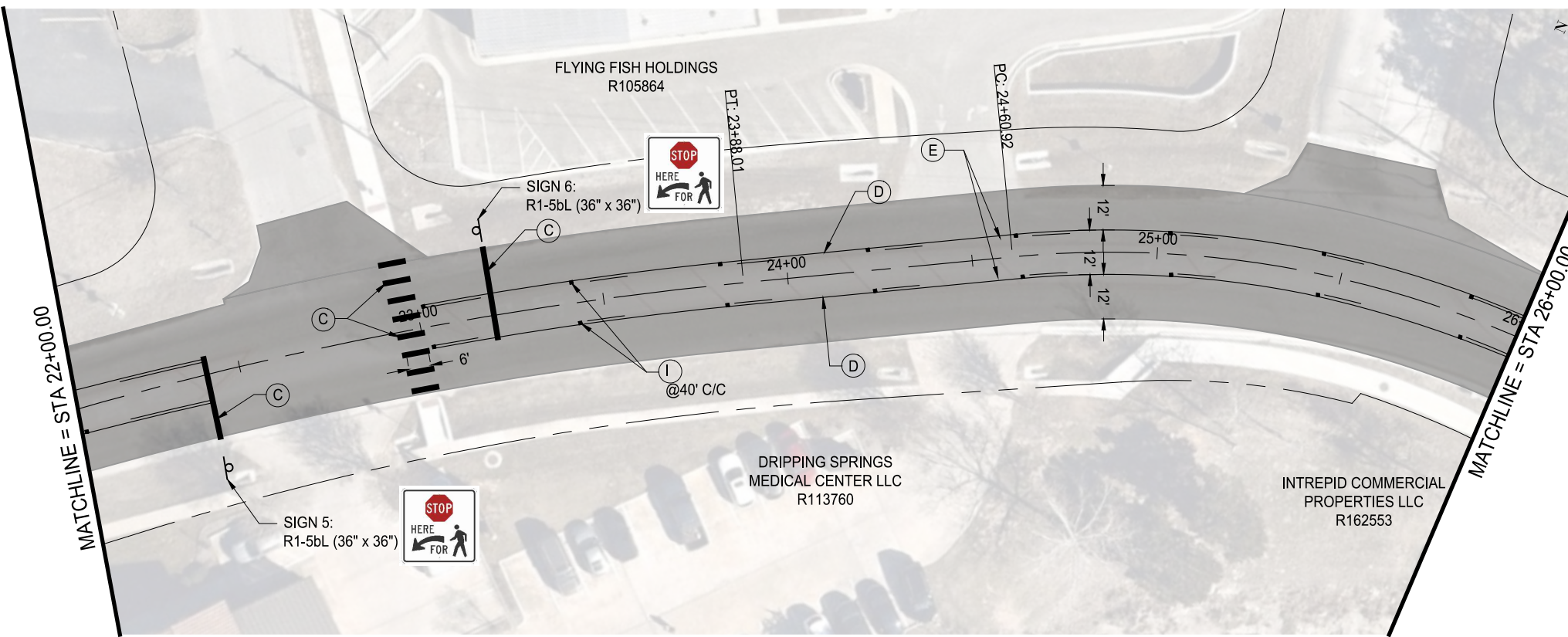
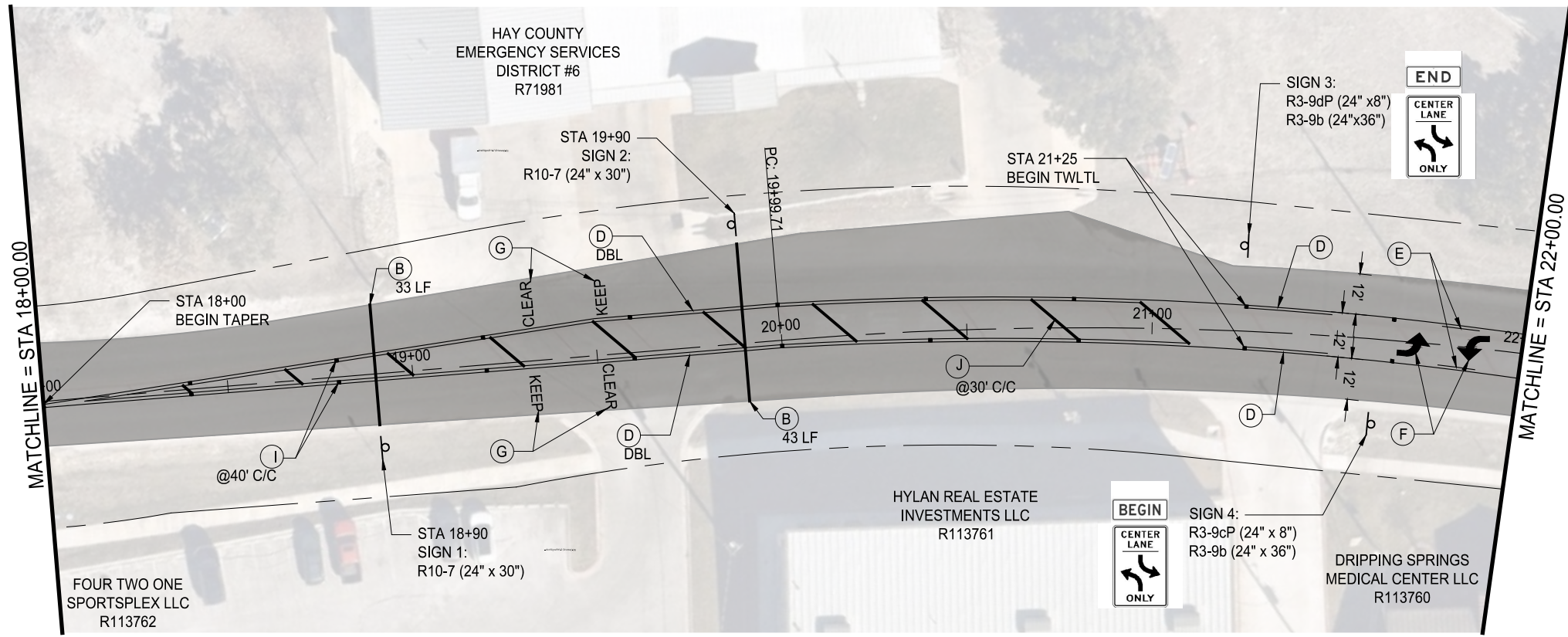
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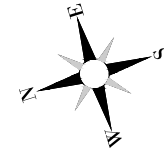
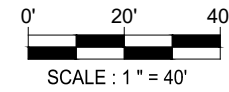
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PROJ #: MAINT-2023-0001



PROJECT:  
**2023 SPORTSPLEX DR  
MAINTENANCE  
PROJECT**

SHEET TITLE:  
**STRIPING PLAN**





**LEGEND**

- (A) REFL PAV MRK (W) (SLD) 8"
- (B) REFL PAV MRK (W) (SLD) 12"
- (C) REFL PAV MRK (W) (SLD) 24"
- (D) REFL PAV MRK (Y) (SLD) 4"
- (E) REFL PAV MRK (Y) (BRK) 4"
- (F) REFL PAV MRK (W) (ARROW)
- (G) REFL PAV MRK (W) (WORD)
- (H) REFL PAV MRKR TY I-C @ 20' C/C
- (I) REFL PAV MRKR TY II-A-A
- (J) REFL PAV MRK (Y) (SLD) 12"
- (K) REFL PAV MRK (W) (SLD) 4"

--- --- --- EXIST R.O.W.  
(APPROX LINEWORK PER 2003  
CMA SPORTSPLEX DR PLANS)

**NOTES:**

1. CONTRACTOR TO MATCH EXISTING GRADE, MAINTAIN EXISTING DRAINAGE CONTOURS & DRAINAGE PATTERNS UNLESS OTHERWISE NOTED.
2. CONTRACTOR TO RE-VEGETATE ALL DISTURBED AREAS UPON COMPLETION OF THE WORK IN COMPLIANCE WITH THE ENVIRONMENTAL NOTES AND SPECIFICATIONS IN THESE DOCUMENTS.
3. CONTRACTOR SHALL ADJUST TO PROPOSED FINISHED GRADE ALL EXISTING VALVES, MANHOLES, MANHOLE COVERS, CLEANOUTS, INLET COVERS AND/OR ANY OTHER OBJECTS WITHIN THE PROJECT AREA, IF APPLICABLE.
4. CONTRACTOR SHALL SAW-CUT AND/OR ADJUST EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH AND CONTINUOUS TRANSITION GRADE.
5. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT BOTH OVERHEAD AND UNDERGROUND UTILITIES EXIST IN THE VICINITY OF THE CONSTRUCTION AREA. THE EXACT LOCATION OF UNDERGROUND UTILITIES IS NOT CERTAIN. THE CONTRACTOR SHALL CONTACT THE APPROPRIATE AREA UTILITY COMPANIES FOR EXACT LOCATIONS AT LEAST 48 HOURS PRIOR TO CONSTRUCTION OR COMMENCING ANY WORK SO AS TO PREVENT ANY DAMAGE OR INTERFERENCE WITH PRESENT UTILITIES.
6. THE CONTRACTOR SHALL PROTECT ALL AREAS OF THE RIGHT-OF-WAY WHICH ARE NOT INCLUDED IN THE ACTUAL LIMITS OF THE PROPOSED CONSTRUCTION FROM DESTRUCTION. CARE SHALL BE EXERCISED TO PREVENT DAMAGE TO TREES, VEGETATION AND OTHER NATURAL SURROUNDINGS. THE CONTRACTOR, AT HIS EXPENSE, SHALL RESTORE ANY AREAS DISTURBED AS A RESULT OF THEIR OPERATIONS TO A CONDITION AS GOOD AS, OR BETTER THAN, THAT PRESENT PRIOR TO CONSTRUCTION.
7. A PRE-CONSTRUCTION MEETING WITH THE CITY ENGINEER AND CONTRACTOR IS REQUIRED PRIOR TO ANY SITE DISTURBANCE.
8. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY OBSTACLES THAT MAY IMPEDE OR PREVENT THE PROPER CONSTRUCTION OF THE PROJECT.
9. CONTRACTOR SHALL INSTALL EROSION AND SEDIMENTATION CONTROLS AS NEEDED TO PREVENT THE MIGRATION OF SEDIMENT DOWNSTREAM INTO EXISTING INFRASTRUCTURE OR ONTO ADJACENT PROPERTIES.
10. SEE TRAFFIC CONTROL PLAN SHEET FOR ADDITIONAL INFORMATION.
11. TY II PAVEMENT MARKINGS ARE REQUIRED WHERE TY I PAVEMENT MARKINGS ARE SHOWN TO BE PLACED ON EXISTING PAVEMENT.

**GILPIN**  
ENGINEERING COMPANY

T.B.P.L.S. Firm Registration # 10193770  
T.B.P.E. Firm Registration # F-9266  
9701 BRODIE LANE #203  
AUSTIN, TX 78748  
PH: 512.220.8100

ENGINEER'S SEAL:

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**REVISIONS:**

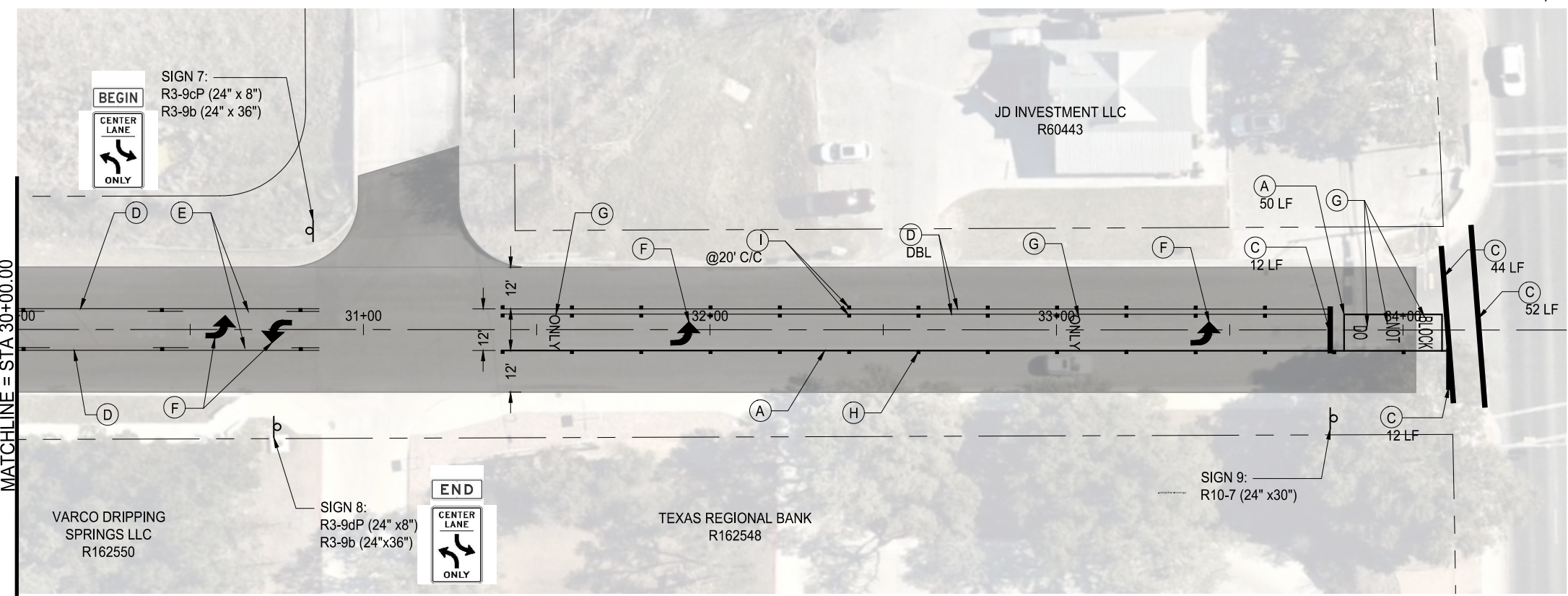
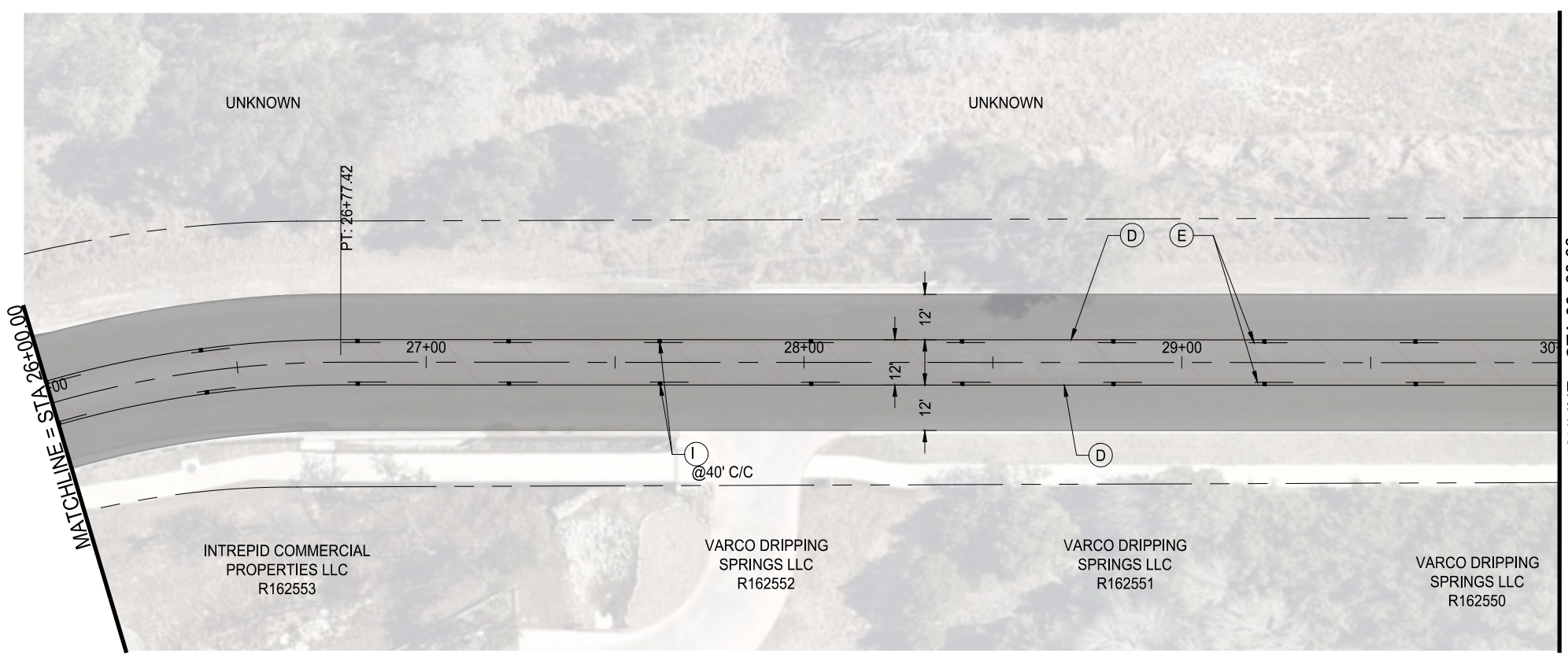
NO.	REVISION	DATE

DATE: 4/27/2023  
DESIGNED BY: DC  
CHECKED BY: RB  
PROJ #: MAINT-2023-0001

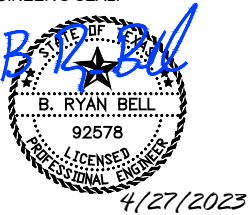


PROJECT:  
**2023 SPORTSPLEX DR MAINTENANCE PROJECT**

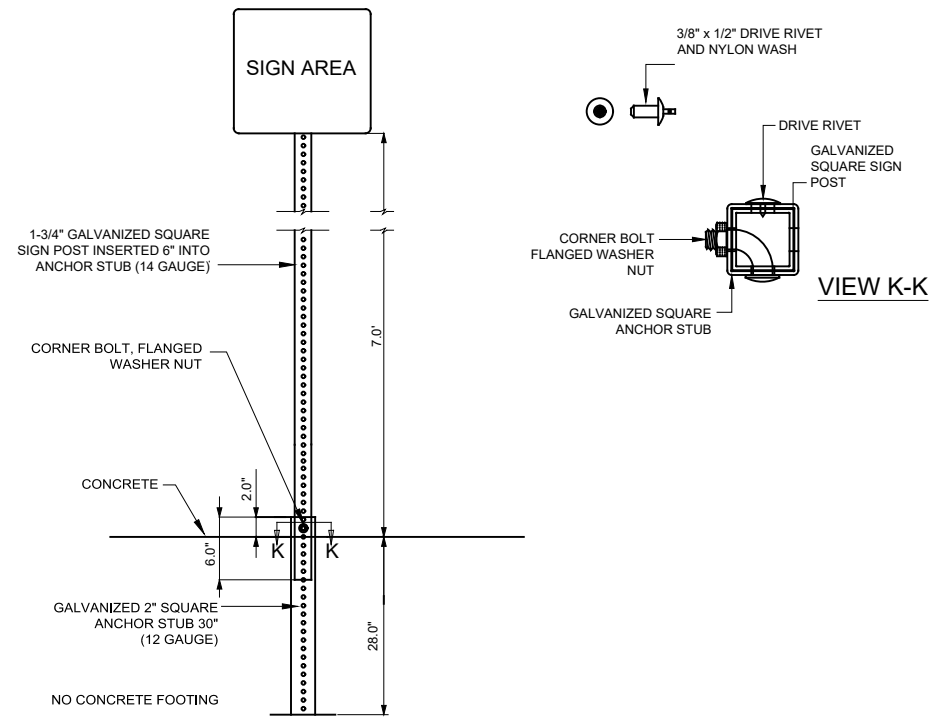
SHEET TITLE:  
**STRIPING PLAN**



ENGINEER'S SEAL:



SUMMARY OF ROAD SIGNS				
PLAN SHEET NO	SIGN #	SIGN NOMENCLATURE	SIGN INFORMATION	SIGN AREA
9	1	R10-7	DO NOT BLOCK DRIVEWAY	24" X 30"
9	2	R10-7	DO NOT BLOCK DRIVEWAY	24" X 30"
9	3	R3-9dP	END	24" X 8"
9	3	R3-9b	CENTER LANE ONLY	24" X 36"
9	4	R3-9cP	BEGIN	24" X 8"
9	4	R3-9b	CENTER LANE ONLY	24" X 36"
9	5	R1-5bL	[STOP] HERE FOR [PEDESTRIANS]	36" X 36"
9	6	R1-5bL	[STOP] HERE FOR [PEDESTRIANS]	36" X 36"
10	7	R3-9cP	BEGIN	24" X 8"
10	7	R3-9b	CENTER LANE ONLY	24" X 36"
10	8	R3-9dP	END	24" X 8"
10	8	R3-9b	CENTER LANE ONLY	24" X 36"
10	9	R10-7	DO NOT BLOCK INTERSECTION	24" X 30"



TRAFFIC SIGN POST  
N.T.S.

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REVISIONS:

NO.	REVISION	DATE

DATE: 4/27/2023  
DESIGNED BY: DC  
CHECKED BY: RB  
PROJ #: MAINT-2023-0001



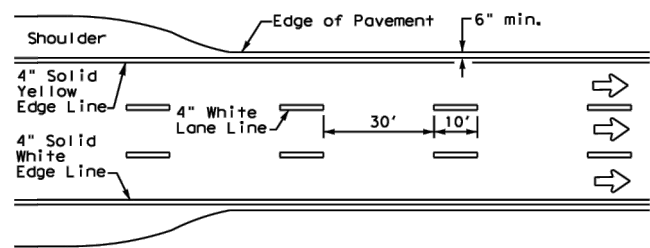
PROJECT:  
2023 SPORTSPLEX DR  
MAINTENANCE  
PROJECT

SHEET TITLE:  
SIGN DETAILS

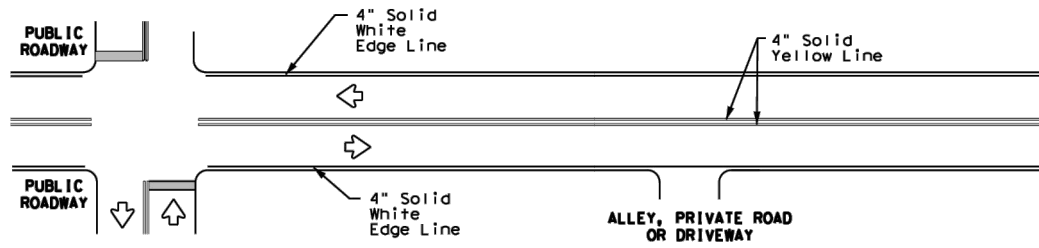
ENGINEER'S SEAL:



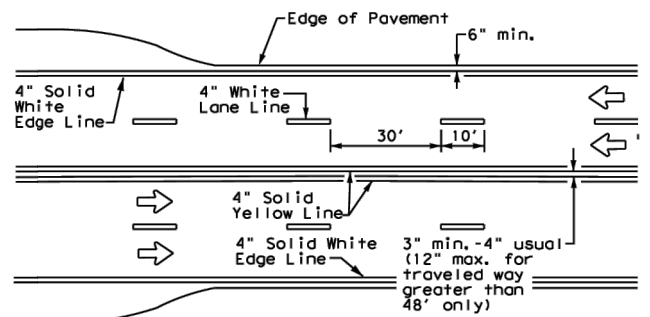
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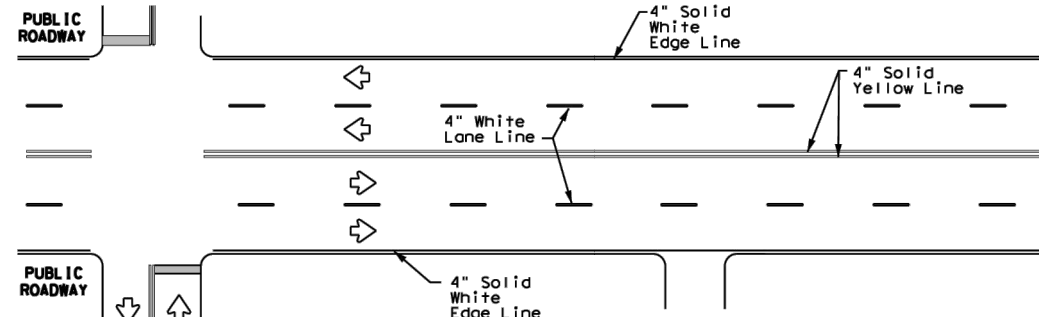
**EDGE LINE AND LANE LINES  
ONE-WAY ROADWAY  
WITH OR WITHOUT SHOULDERS**



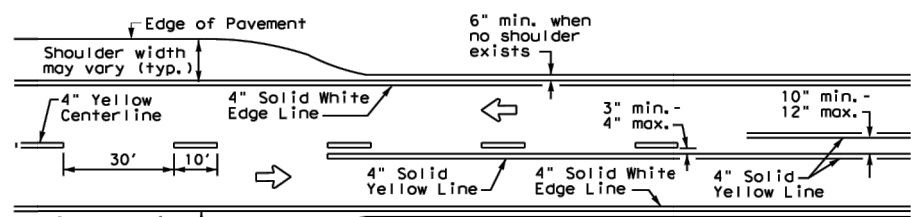
**TYPICAL TWO-LANE, TWO-WAY PAVEMENT  
MARKINGS THROUGH INTERSECTIONS**



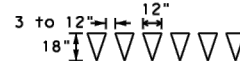
**CENTERLINE AND LANE LINES  
FOUR LANE TWO-WAY ROADWAY  
WITH OR WITHOUT SHOULDERS**



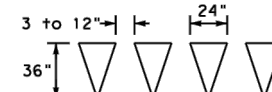
**TYPICAL MULTI-LANE, TWO-WAY PAVEMENT  
MARKINGS THROUGH INTERSECTIONS**



**TWO LANE TWO-WAY ROADWAY  
WITH OR WITHOUT SHOULDERS**



For posted speed on road being marked equal to or less than 40 MPH.



For posted speed on road being marked equal to or greater than 45 MPH.

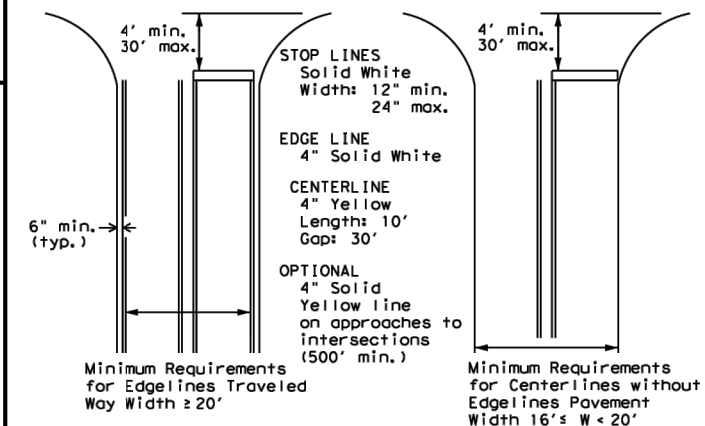
**YIELD LINES**

**GENERAL NOTES**

1. Edgeline striping shall be as shown in the plans or as directed by the Engineer. The edgeline should not be placed less than 6 inches from the edge of pavement. This distance may vary due to pavement raveling or other conditions. Edgelines are not required in curb and gutter sections of roadways.
2. The traveled way includes only that portion of the roadway used for vehicular travel. It does not include the parking lanes, sidewalks, berms and shoulders. The traveled ways shall be measured from the inside of edgeline to the inside of edgeline of a two lane roadway.

MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
TRAFFIC PAINT	DMS-8200
HOT APPLIED THERMOPLASTIC	DMS-8220
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240

All pavement marking materials shall meet the required Departmental Material Specifications as specified by the plans.

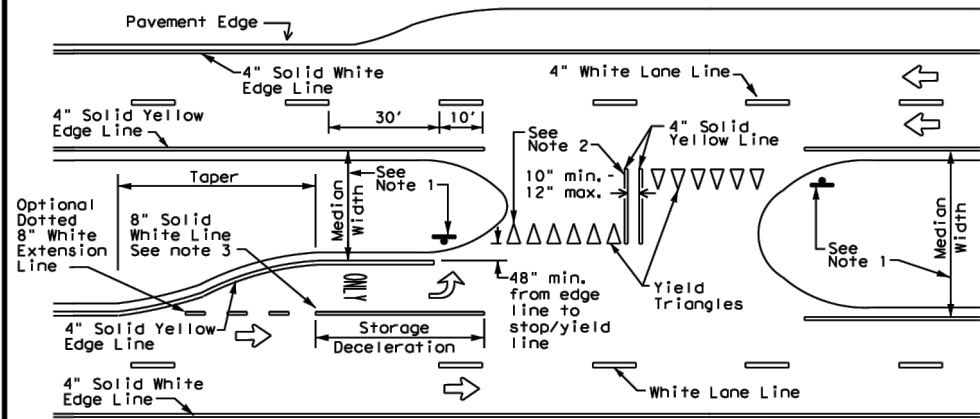


**GUIDE FOR PLACEMENT OF STOP LINES,  
EDGE LINE & CENTERLINE**

Based on Traveled Way and Pavement Widths for Undivided Highways

**NOTES**

1. Where divided highways are separated by median widths at the median opening itself of 30 feet or more, median openings shall be signed as two separate intersections. Each median opening has two width measurements, with one measurement for each approach. The narrow median width will be the controlling width to determine if signs are required. Yield signs are the typical intersection control. Stop signs are optional as determined by the Engineer.
2. Install median striping (double yellow centerlines and stop bars/yield triangles) when a 50' or greater median centerline can be placed. Stop bars shall only be used with stop signs. Yield triangles shall only be used with yield signs.
3. Length of turn bays, including taper, deceleration, and storage lengths shall be as shown on the plans or as directed by the Engineer.



**FOUR LANE DIVIDED ROADWAY CROSSOVERS**



**TYPICAL STANDARD  
PAVEMENT MARKINGS**

**PM(1) - 20**

FILE: pml-20.dgn	DN: November 1978	CK: November 1978	DW: November 1978	CK: November 1978
© TxDOT	CON: November 1978	SECT: November 1978	JOB: November 1978	HIGHWAY: November 1978
8-95	3-03	REVISIONS		
5-00	2-12			
8-00	6-20			
		DIST: COUNTY: SHEET NO.		

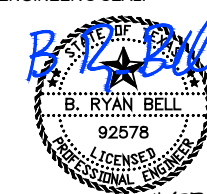
PROJECT:

2023 SPORTSPLEX DR  
MAINTENANCE  
PROJECT

SHEET TITLE:

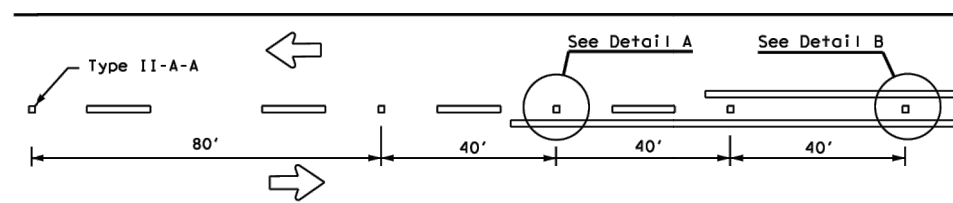
PM(1)-20

ENGINEER'S SEAL:

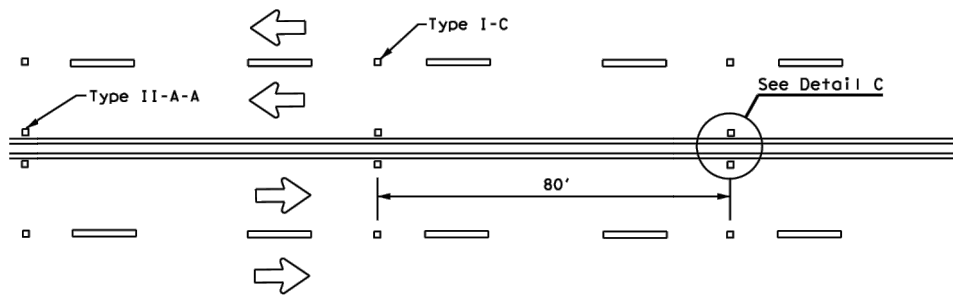


4/27/2023

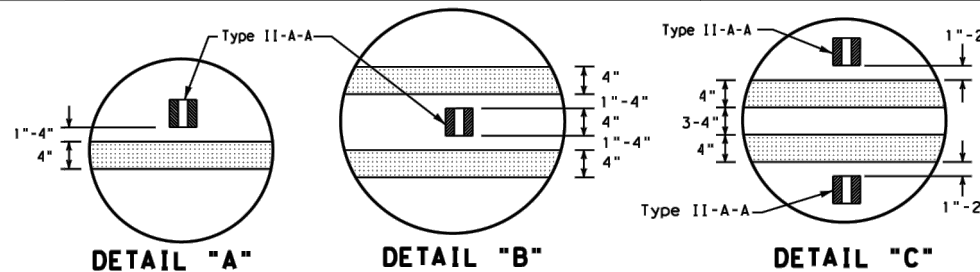
**REFLECTIVE RAISED PAVEMENT MARKERS  
FOR VEHICLE POSITIONING GUIDANCE**



**CENTERLINE FOR ALL TWO LANE ROADWAYS**



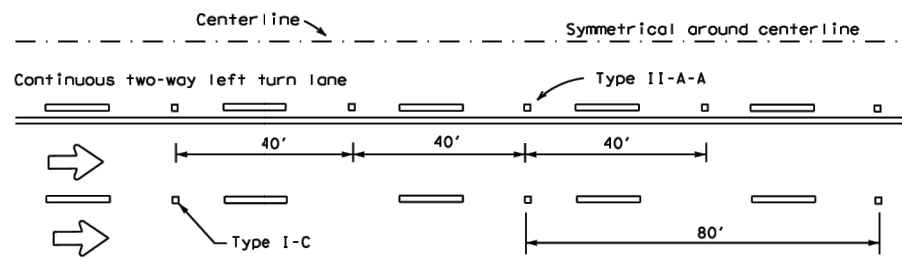
**CENTERLINE & LANE LINES  
FOR FOUR LANE TWO-WAY HIGHWAYS**



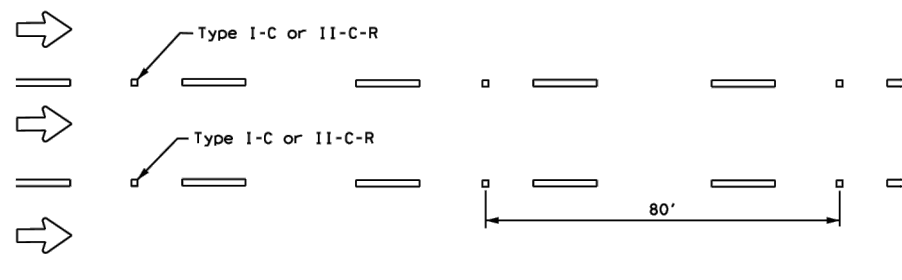
**DETAIL "A"**

**DETAIL "B"**

**DETAIL "C"**



**CENTERLINE AND LANE LINES FOR TWO-WAY LEFT TURN LANE**



**LANE LINES FOR ONE-WAY ROADWAY (NON-FREEWAY FACILITIES)**

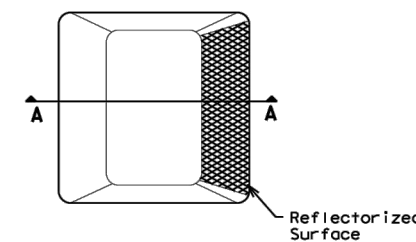
Raised pavement markers Type II-C-R shall have clear face toward normal traffic and red face toward wrong-way traffic.

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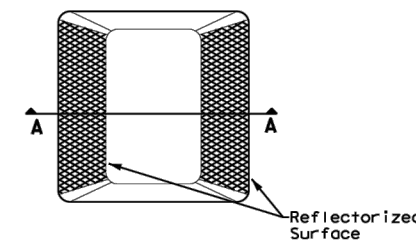
DATE: FILE:

MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
TRAFFIC PAINT	DMS-8200
HOT APPLIED THERMOPLASTIC	DMS-8220
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240

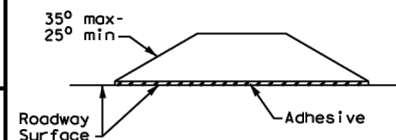
All pavement marking materials shall meet the required Departmental Material Specifications as specified by the plans.



**Type I (Top View)**



**Type II (Top View)**



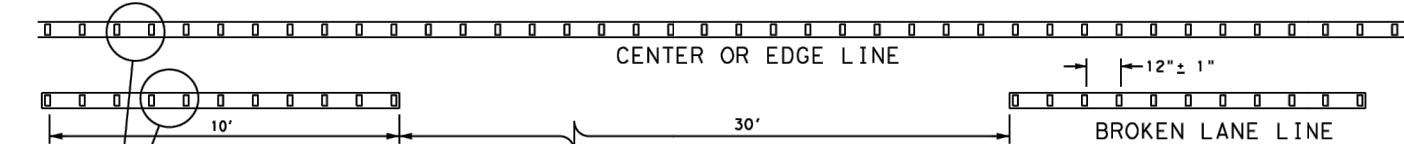
**SECTION A**

**RAISED PAVEMENT MARKERS**



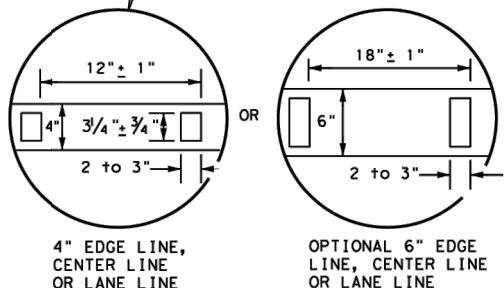
**POSITION GUIDANCE USING  
RAISED MARKERS  
REFLECTORIZED PROFILE  
MARKINGS  
PM(2) - 20**

FILE: pm2-20.dgn	DN:	CK:	DW:	CK:
© TxDOT April 1977	CONT	SECT	JOB	HIGHWAY
4-92 2-10	REVISIONS			
5-00 2-12	DIST	COUNTY	SHEET NO.	
8-00 6-20				



**REFLECTORIZED PROFILE  
PATTERN DETAIL**

USING REFLECTORIZED PROFILE PAVEMENT MARKINGS



**4" EDGE LINE,  
CENTER LINE  
OR LANE LINE**

**OPTIONAL 6" EDGE  
LINE, CENTER LINE  
OR LANE LINE**

**NOTE**

Profile markings shall not be placed on roadways with a posted speed limit of 45 MPH or less.

**GENERAL NOTES**

- All raised pavement markers placed in broken lines shall be placed in line with and midway between the stripes.
- On concrete pavements the raised pavement markers should be placed to one side of the longitudinal joints.

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OR THE DESIGNS REPRESENTED THEREIN WILL  
SUBJECT THE INFRINGER TO DAMAGES AND/OR  
JUDICIAL ACTION AS PROVIDED BY FEDERAL LAW.

**REVISIONS:**

NO.	REVISION	DATE

DATE: 4/27/2023  
DESIGNED BY: DC  
CHECKED BY: RB  
PROJ #: MAINT-2023-0001



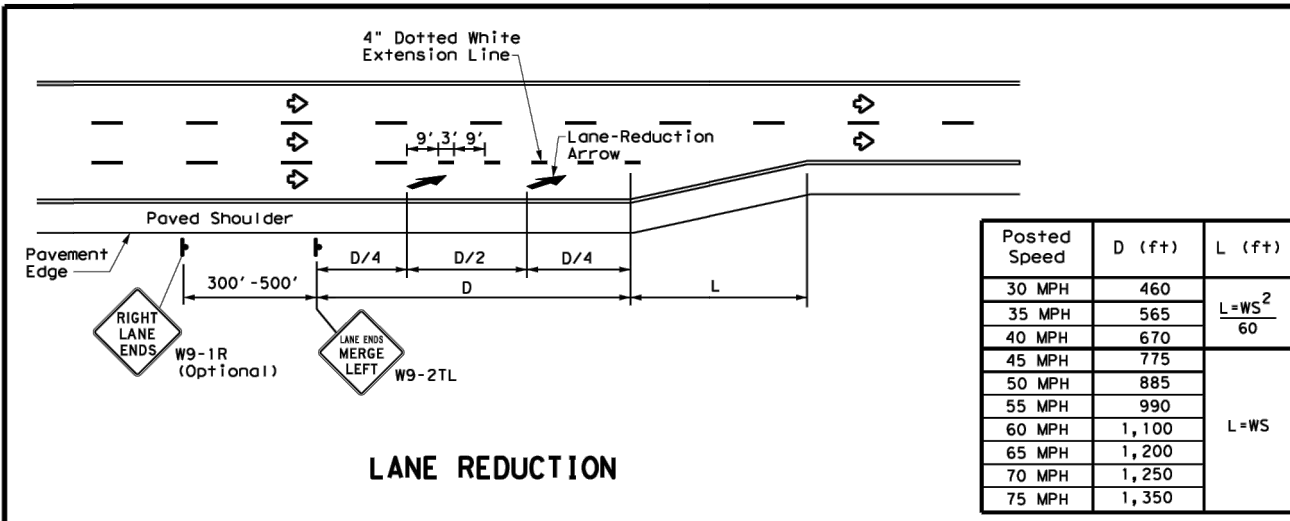
PROJECT:  
2023 SPORTSPLEX DR  
MAINTENANCE  
PROJECT

SHEET TITLE:  
PM(2)-20

ENGINEER'S SEAL:



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**LANE REDUCTION**

**NOTES**

- Lane reduction pavement markings are used where the number of through lanes is reduced because of narrowing of the roadway or because of a section of on-street parking in what would otherwise be a through lane. For Texas Super 2 Passing Lanes, see TS2(PL) standard sheets.
- On divided highways, an additional W9-1R "RIGHT LANE ENDS" sign may be installed in the median aligned with the W9-1R sign on the right side of the highway.
- Lane reduction arrows are required for speeds of 45 mph or greater. An optional third lane reduction arrow may be added based on engineering judgement. If used, the optional third lane reduction arrow should be centered between the first and last lane reduction arrows.
- For lane reductions on Freeways and Expressways, signing shall conform to the TxDOT Freeway Signing Handbook.

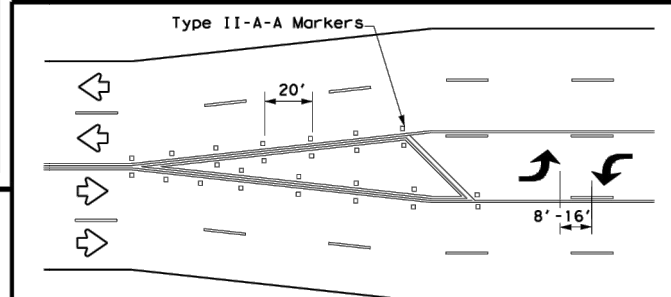
**GENERAL NOTES**

- Lane use word and arrow markings shall be used where through lanes approaching an intersection become mandatory turn lanes. Lane use word and arrow markings should be used in auxiliary lanes of substantial length. Lane use arrow markings or word and arrow markings may be used in other lanes and turn bays for emphasis. Details for words and arrows are as shown in the Standard Highway Sign Designs for Texas.
- When lane-use words and arrow markings are used, two sets of arrows should be used if the length of the bay is greater than 180 feet. When a single lane use arrow or word and arrow marking is used for a short turn lane, it should be located at or near the upstream end of the full-width turn lane.
- Use raised pavement marker Type I-C with undivided highways, flush medians and two way left turn lanes. Use raised pavement marker Type II-C-R with divided highways and raised medians.
- Length of turn bays, including taper, deceleration, and storage lengths shall be as shown on the plans or as directed by the Engineer.

**MATERIAL SPECIFICATIONS**

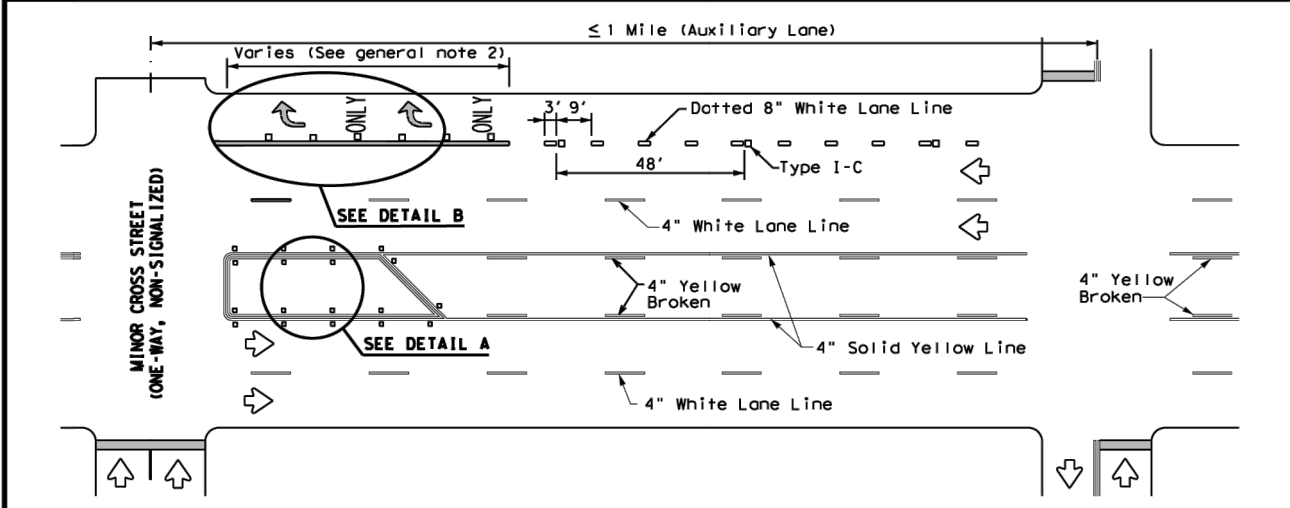
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
TRAFFIC PAINT	DMS-8200
HOT APPLIED THERMOPLASTIC	DMS-8220
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240

All pavement marking materials shall meet the required Departmental Material Specifications as specified by the plans.

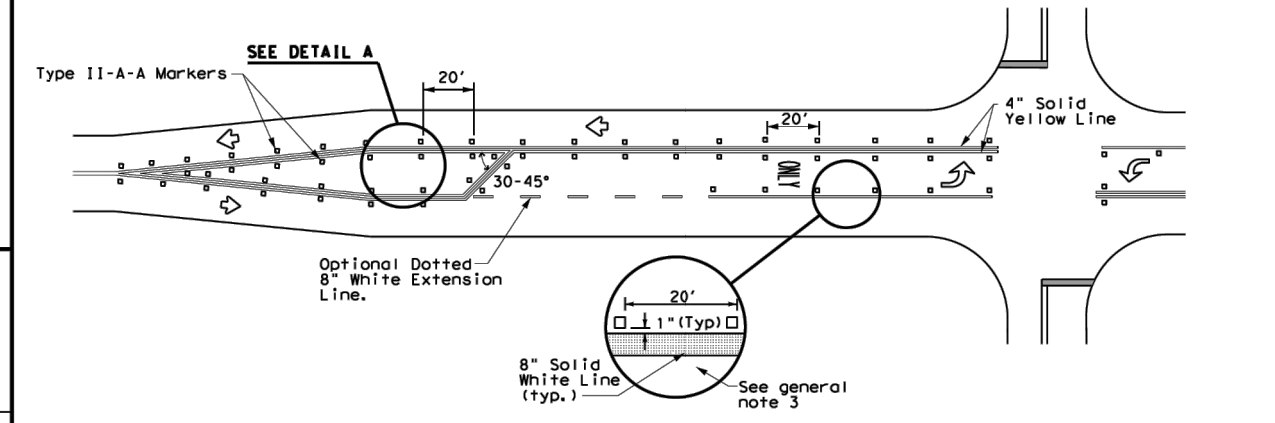


A two-way left-turn (TWLTL) lane-use arrow pavement marking should be used at or just downstream from the beginning of a two-way left-turn lane within a corridor. Repeating the marking after each intersection or dedicated turn bay is not required unless stated elsewhere in the plans.

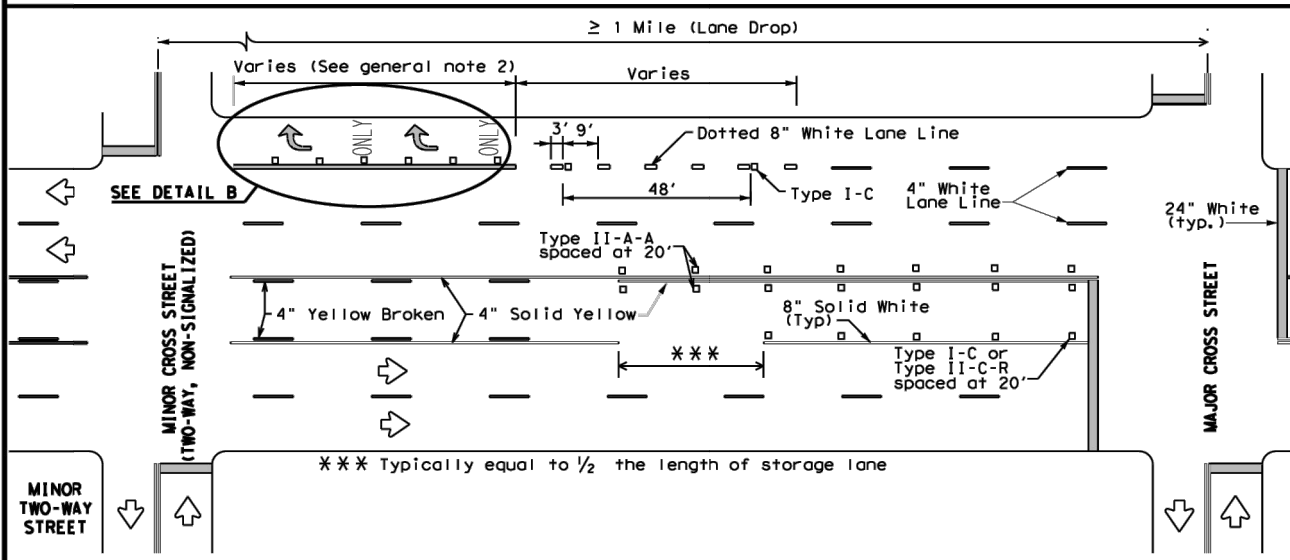
**TYPICAL TRANSITION FOR TWLTL AND DIVIDED HIGHWAY**



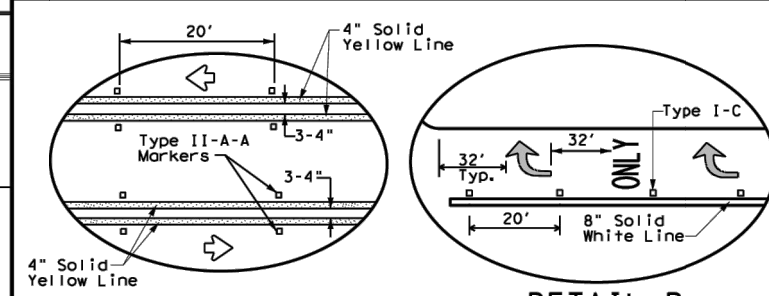
**TYPICAL TWLTL AT ONE-WAY STREET AND RIGHT TURN AUXILIARY LANE**



**TYPICAL TWO-LANE HIGHWAY INTERSECTION WITH LEFT TURN BAYS**



**TYPICAL TWLTL AT TWO-WAY CROSS STREET AND RIGHT TURN LANE DROP**



DETAIL A

DETAIL B

Texas Department of Transportation  
Traffic Safety Division Standard

**TWO-WAY LEFT TURN LANES, RURAL LEFT TURN BAYS, AND LANE REDUCTION PAVEMENT MARKINGS PM(3)-20**

FILE#	pm3-20.dgn	DN#	CK#	DW#	CK#
© TxDOT	April 1998	CONT	SECT	JOB	HIGHWAY
REVISIONS					
5-00	2-10	DIST	COUNTY	SHEET NO.	
8-00	2-12				
3-03	6-20				

**REVISIONS:**

NO.	REVISION	DATE

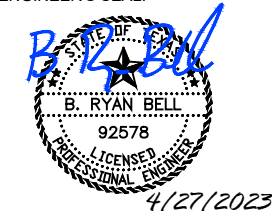
DATE: 4/27/2023  
DESIGNED BY: DC  
CHECKED BY: RB  
PROJ #: MAINT-2023-0001



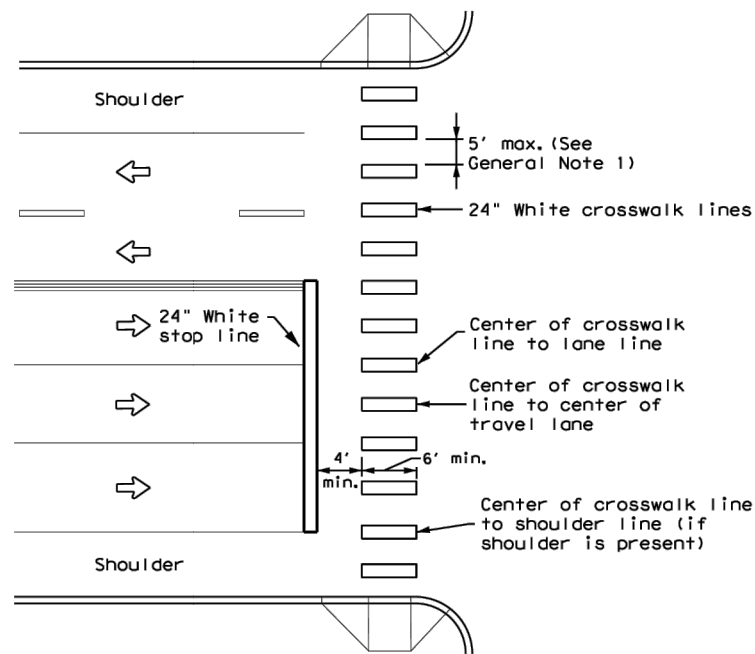
PROJECT:  
2023 SPORTSPLEX DR MAINTENANCE PROJECT

SHEET TITLE:  
PM(3)-20

ENGINEER'S SEAL:



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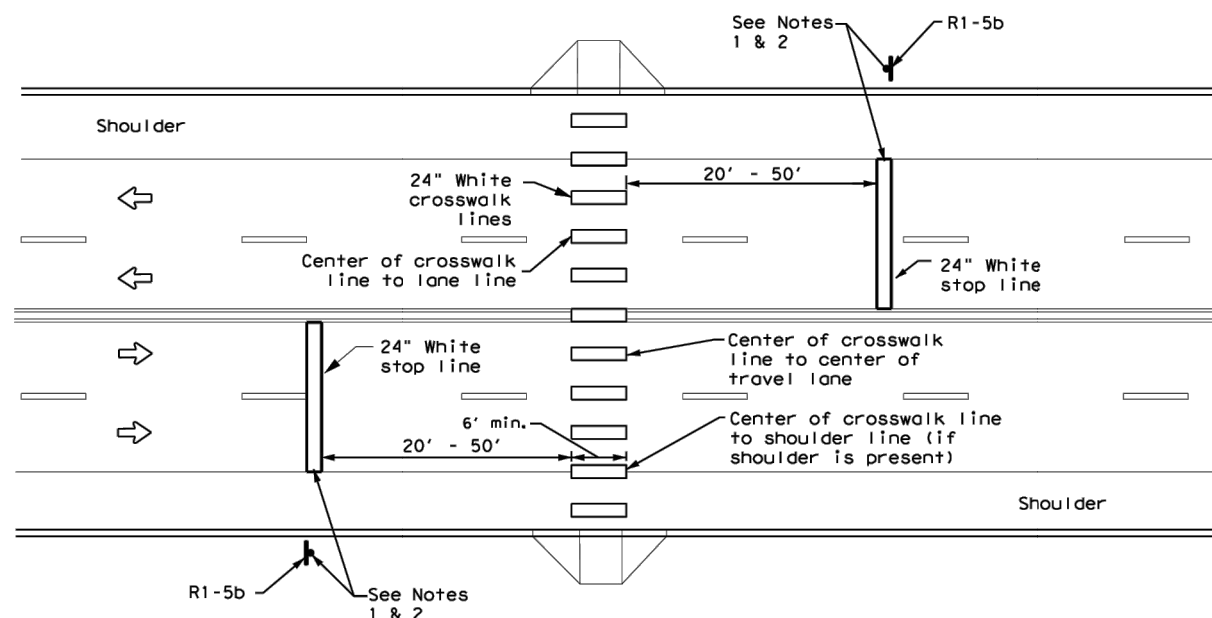
**HIGH-VISIBILITY LONGITUDINAL CROSSWALK AT CONTROLLED APPROACH**

**GENERAL NOTES**

1. Longitudinal crosswalk lines should not be placed in the wheel path of vehicles. Center the crosswalk lines on travel lanes, lane lines, and shoulder lines (if present).
2. A minimum 6" clear distance shall be provided to the curb face. If the last crosswalk line falls into this distance it must be omitted.
3. For divided roadways, adjustments in spacing of the crosswalk lines should be made in the median so that the crosswalk lines are maintained in their proper location across the travel portion of the roadway.
4. At skewed crosswalks, the crosswalk lines are to remain parallel to the lane lines.
5. Each crosswalk shall be a minimum of 6' wide.
6. The High-Visibility Longitudinal Crosswalk is the preferred crosswalk pattern on State Highways. Other crosswalk patterns as shown in the "Texas Manual on Uniform Traffic Control Devices" may be used. All crosswalk designs and dimension shall comply with the "Texas Manual on Uniform Traffic Control Devices."
7. Final placement of Stop Bar and Crosswalk shall be approved by the Engineer in the field.

MATERIAL SPECIFICATIONS	
PAVEMENT MARKERS (REFLECTORIZED)	DMS-4200
EPOXY AND ADHESIVES	DMS-6100
BITUMINOUS ADHESIVE FOR PAVEMENT MARKERS	DMS-6130
TRAFFIC PAINT	DMS-8200
HOT APPLIED THERMOPLASTIC	DMS-8220
PERMANENT PREFABRICATED PAVEMENT MARKINGS	DMS-8240

All pavement marking materials shall meet the required Departmental Material Specifications as specified by the plans.



**UNSIGNALIZED MIDBLOCK HIGH-VISIBILITY LONGITUDINAL CROSSWALK**

**NOTES:**

1. Use stop bars with Stop Here For Pedestrians (R1-5b) signs at unsignalized midblock crosswalks.
2. Use stop bars with STOP HERE ON RED (R10-6 or R10-6a) signs at mid block crosswalks controlled by traffic signals or pedestrian hybrid beacons.

<p><b>CROSSWALK PAVEMENT MARKINGS</b></p> <p><b>PM(4) - 22A</b></p>			
FILE: pm4-22a.dgn	DN:	CK:	DR:
© TxDOT December 2022	CONT	SECT	JOB
6-20	DIST	COUNTY	SHEET NO.
6-22			
12-22			
220			

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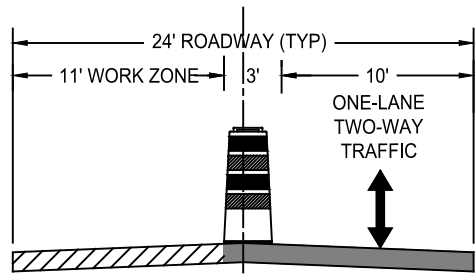
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DESIGNED BY: DC  
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PROJ #: MAINT-2023-0001



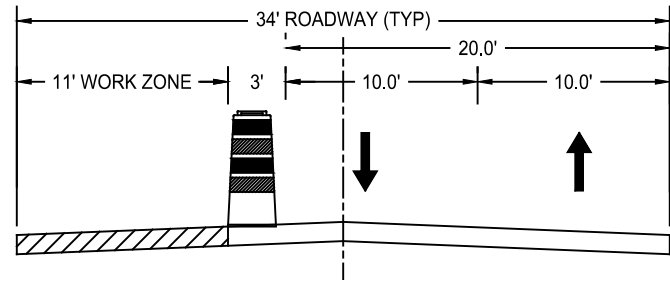
PROJECT:  
2023 SPORTSPLEX DR MAINTENANCE PROJECT

SHEET TITLE:  
PM(4)-22A

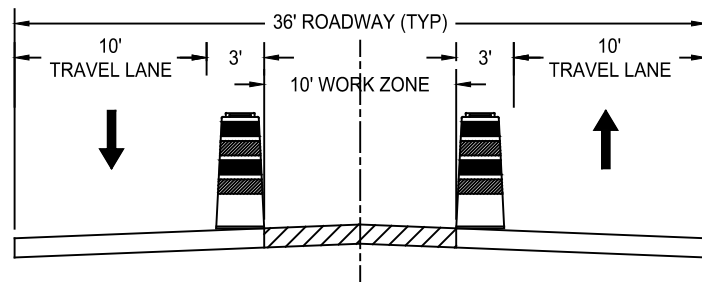
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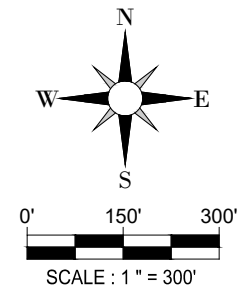
TYPICAL SECTION A-A



TYPICAL SECTION B-B



TYPICAL SECTION C-C



LEGEND

PAVEMENT REPAIR

**TRAFFIC CONTROL PLAN  
SEQUENCE OF CONSTRUCTION NARRATIVE:**

1. INSTALL ADVANCE WARNING SIGNS, BARRICADES, AND CHANNELIZING DEVICES IN ACCORDANCE WITH THE PLANS AND TEXAS MUTCD STANDARDS.
2. MILL EXISTING PAVEMENT TO THE LIMITS SHOWN ON THE PLANS.
3. PERFORM FULL-DEPTH PAVEMENT REPAIRS TO THE LIMITS SHOWN ON THE PLANS. APPLY PRIME COAT TO ALL EXPOSED BASE AND SUBGRADE PRIOR TO PLACING HMAC TY B.
4. ADJUST TRAFFIC CONTROL DEVICES TO COMPLETE MILL & OVERLAY OF SPORTSPLEX DRIVE. APPLY TACK COAT TO ALL MILLED AND HMAC TY B SURFACES PRIOR TO PLACING HMAC SURFACE COURSE. PLACE HMAC SURFACE COURSE TO THE LIMITS SHOWN ON THE PLANS.
5. PERFORM SURFACE PREPARATION FOR PAVEMENT MARKINGS PER SPECIFICATIONS.
6. INSTALL TY II MARKINGS AND SIGNS AS SHOWN ON THE PLANS. ALLOW 7 DAYS FOR TY II MARKINGS TO CURE PRIOR TO INSTALLING TY I MARKINGS.
7. RECONFIGURE TRAFFIC TO THE PERMANENT CONDITION & REMOVE ALL TRAFFIC CONTROL DEVICES.

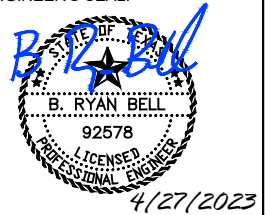
**NOTES:**

1. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS AND SIDE STREETS AT ALL TIMES UNLESS APPROVAL IS OBTAINED FROM THE CITY ENGINEER.
2. CONTRACTOR SHALL RE-VEGETATE ANY DISTURBED AREAS.



T.B.P.L.S. Firm Registration # 10193770  
T.B.P.E. Firm Registration # F-8266  
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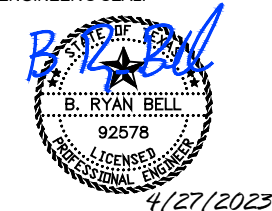


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**2023 SPORTSPLEX DR  
MAINTENANCE  
PROJECT**

SHEET TITLE:  
**TRAFFIC CONTROL  
PLAN**

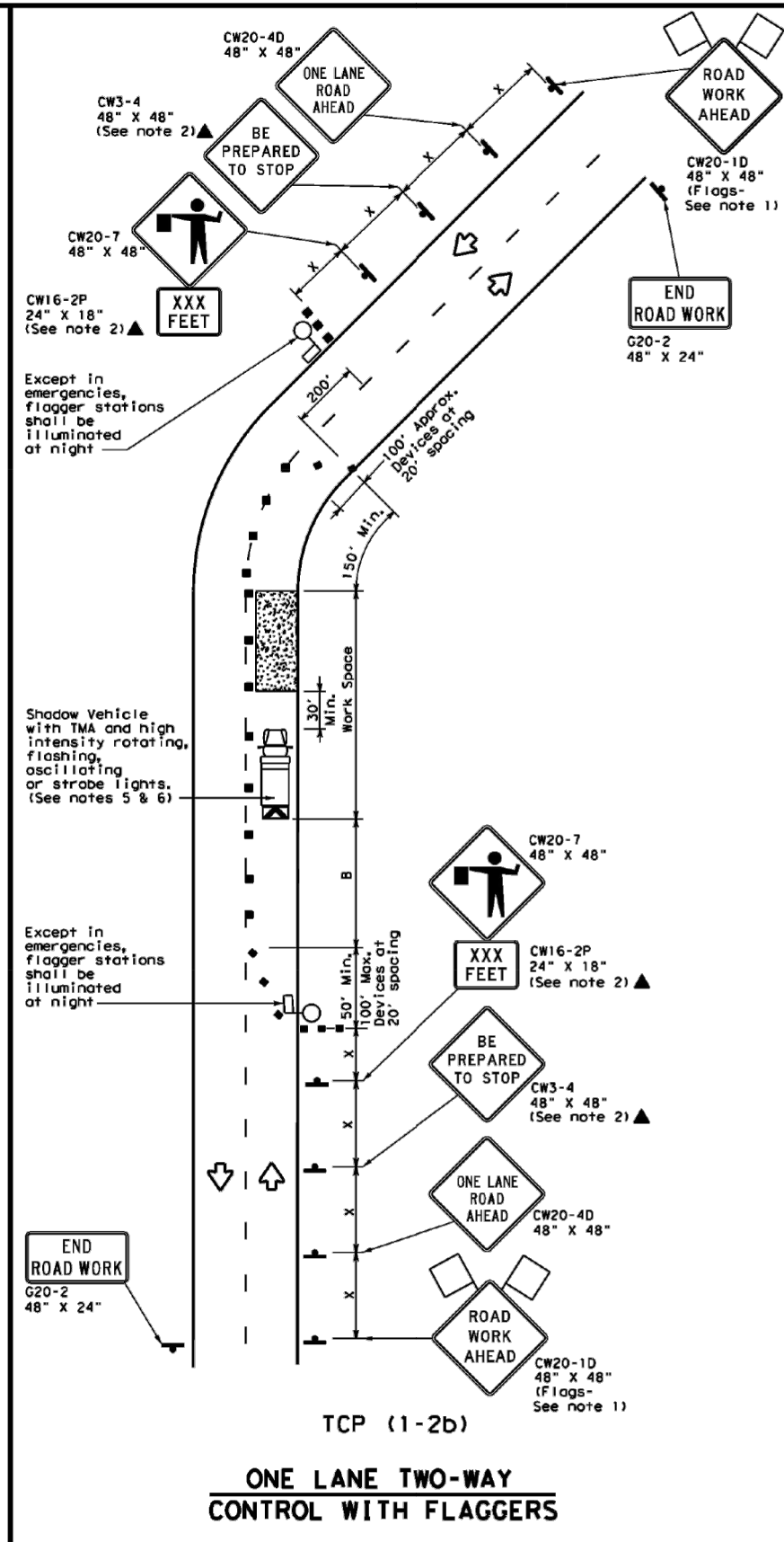
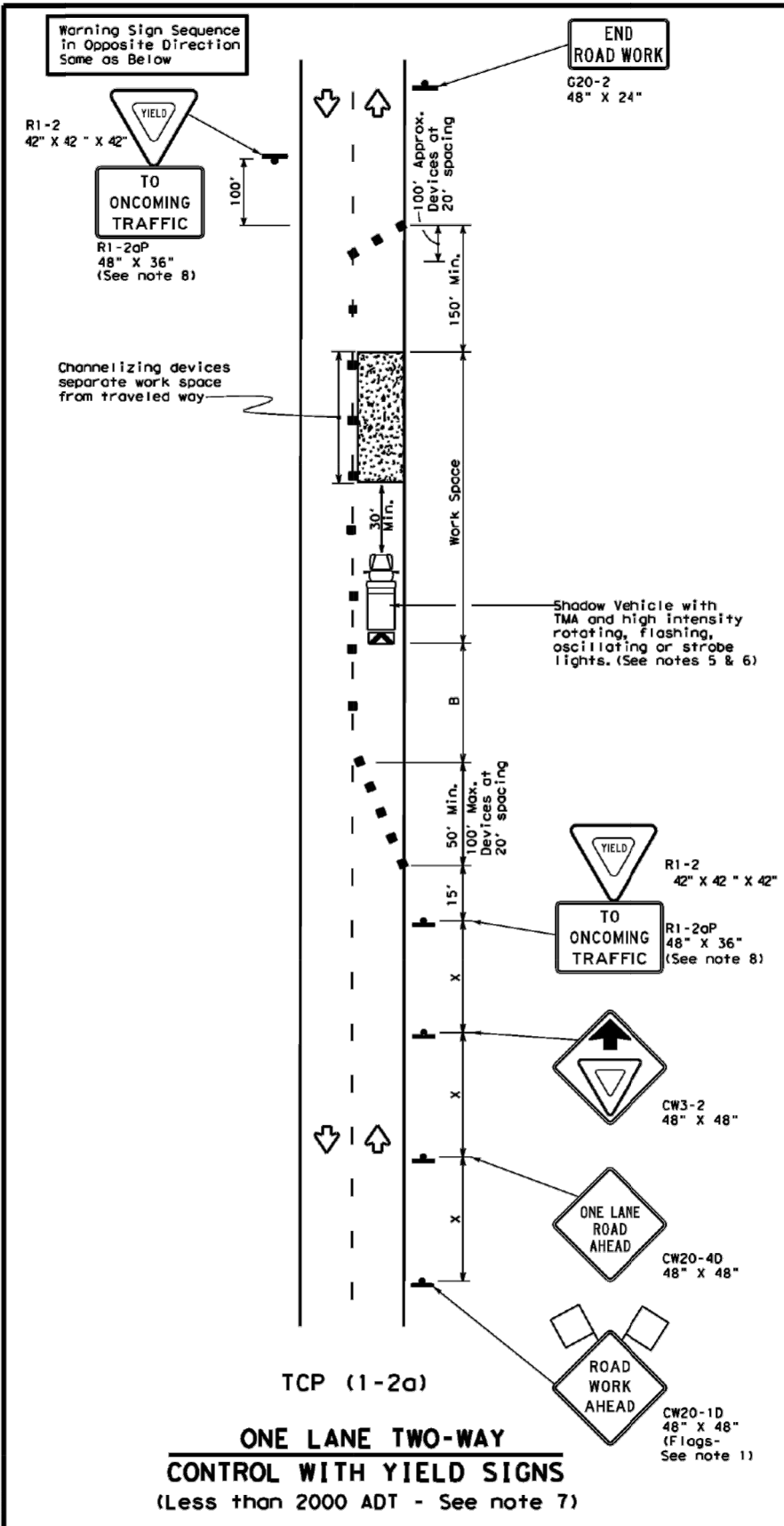


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**LEGEND**

	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed * S	Formula L = WS / 60	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "x" Distance	Suggested Longitudinal Buffer Space "B"	Stopping Sight Distance
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent			
30		150'	165'	180'	30'	60'	120'	90'	200'
35		205'	225'	245'	35'	70'	160'	120'	250'
40		265'	295'	320'	40'	80'	240'	155'	305'
45		450'	495'	540'	45'	90'	320'	195'	360'
50		500'	550'	600'	50'	100'	400'	240'	425'
55		550'	605'	660'	55'	110'	500'	295'	495'
60		600'	660'	720'	60'	120'	600'	350'	570'
65		650'	715'	780'	65'	130'	700'	410'	645'
70		700'	770'	840'	70'	140'	800'	475'	730'
75		750'	825'	900'	75'	150'	900'	540'	820'

\* Conventional Roads Only  
\*\* Taper lengths have been rounded off.  
L = Length of Taper (FT) W = Width of Offset (FT) S = Posted Speed (MPH)

**TYPICAL USAGE**

	MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
		✓	✓		

**GENERAL NOTES**

- Flags attached to signs where shown are REQUIRED.
  - All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
  - The CW3-4 "BE PREPARED TO STOP" sign may be installed after the CW20-4D "ONE LANE ROAD AHEAD" sign, but proper sign spacing shall be maintained.
  - Sign spacing may be increased or an additional CW20-1D "ROAD WORK AHEAD" sign may be used if advance warning ahead of the flagger or R1-2 "YIELD" sign is less than 1500 feet.
  - A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
  - Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect wider work spaces.
- TCP (1-2a)**
- R1-2 "YIELD" sign traffic control may be used on projects with approaches that have adequate sight distance. For projects in urban areas, work spaces should be no longer than one half city block. In rural areas on roadways with less than 2000 ADT, work spaces should be no longer than 400 feet.
  - R1-2 "YIELD" sign with R1-2aP "TO ONCOMING TRAFFIC" plaque shall be placed on a support at a 7 foot minimum mounting height.
- TCP (1-2b)**
- Flaggers should use two-way radios or other methods of communication to control traffic.
  - Length of work space should be based on the ability of flaggers to communicate.
  - If the work space is located near a horizontal or vertical curve, the buffer distances should be increased in order to maintain adequate stopping sight distance to the flagger and a queue of stopped vehicles (see table above).
  - Channelizing devices on the center-line may be omitted when a pilot car is leading traffic and approved by the Engineer.
  - Flaggers should use 24" STOP/SLOW paddles to control traffic. Flags should be limited to emergency situations.

Texas Department of Transportation  
Traffic Operations Division Standard

**TRAFFIC CONTROL PLAN**  
**ONE-LANE TWO-WAY**  
**TRAFFIC CONTROL**

**TCP (1-2) - 18**

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1-97 2-18				
	DIST	COUNTY	SHEET NO.	

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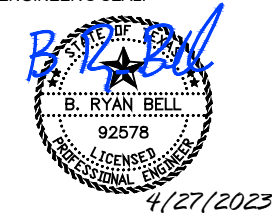
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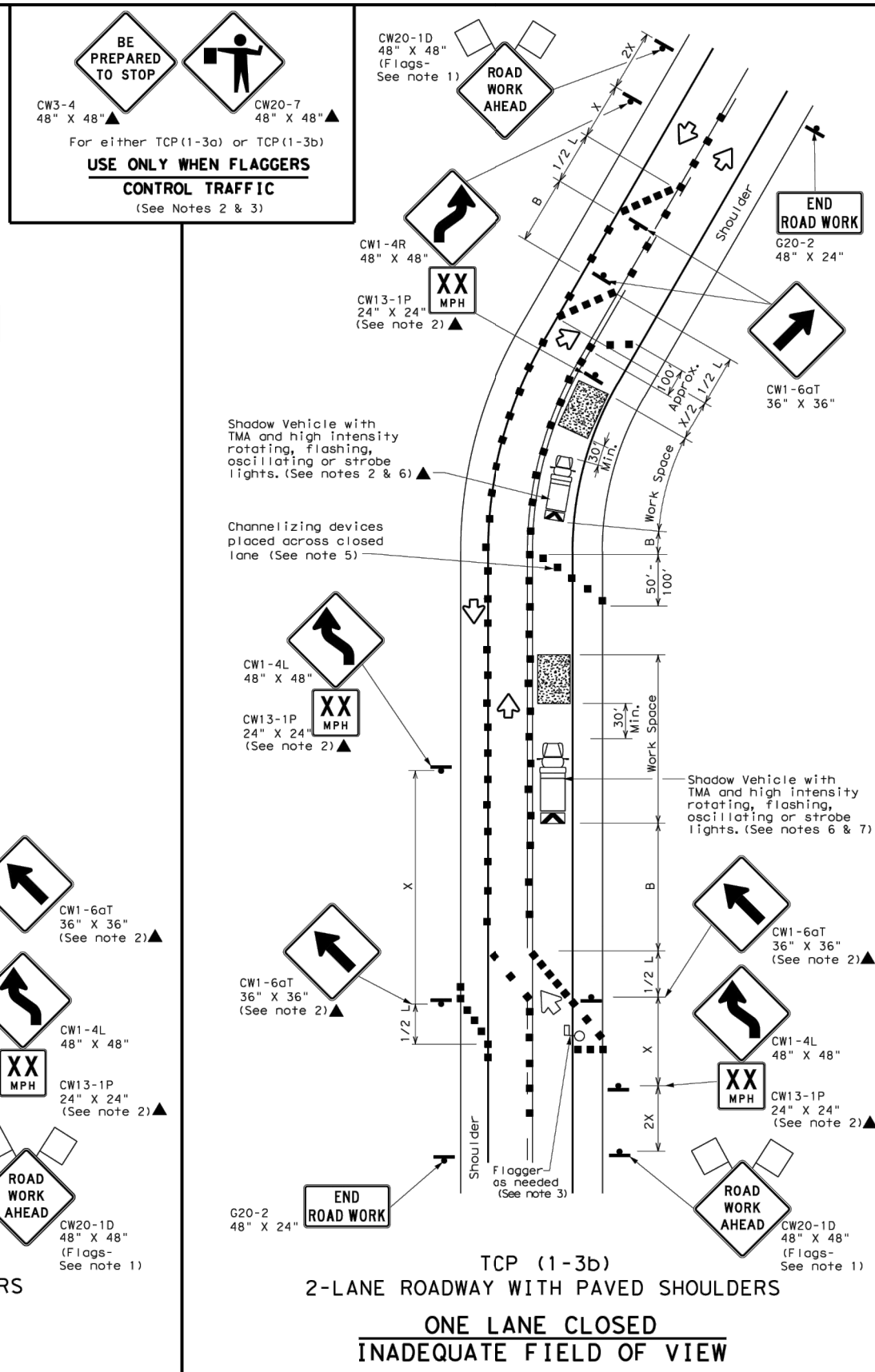
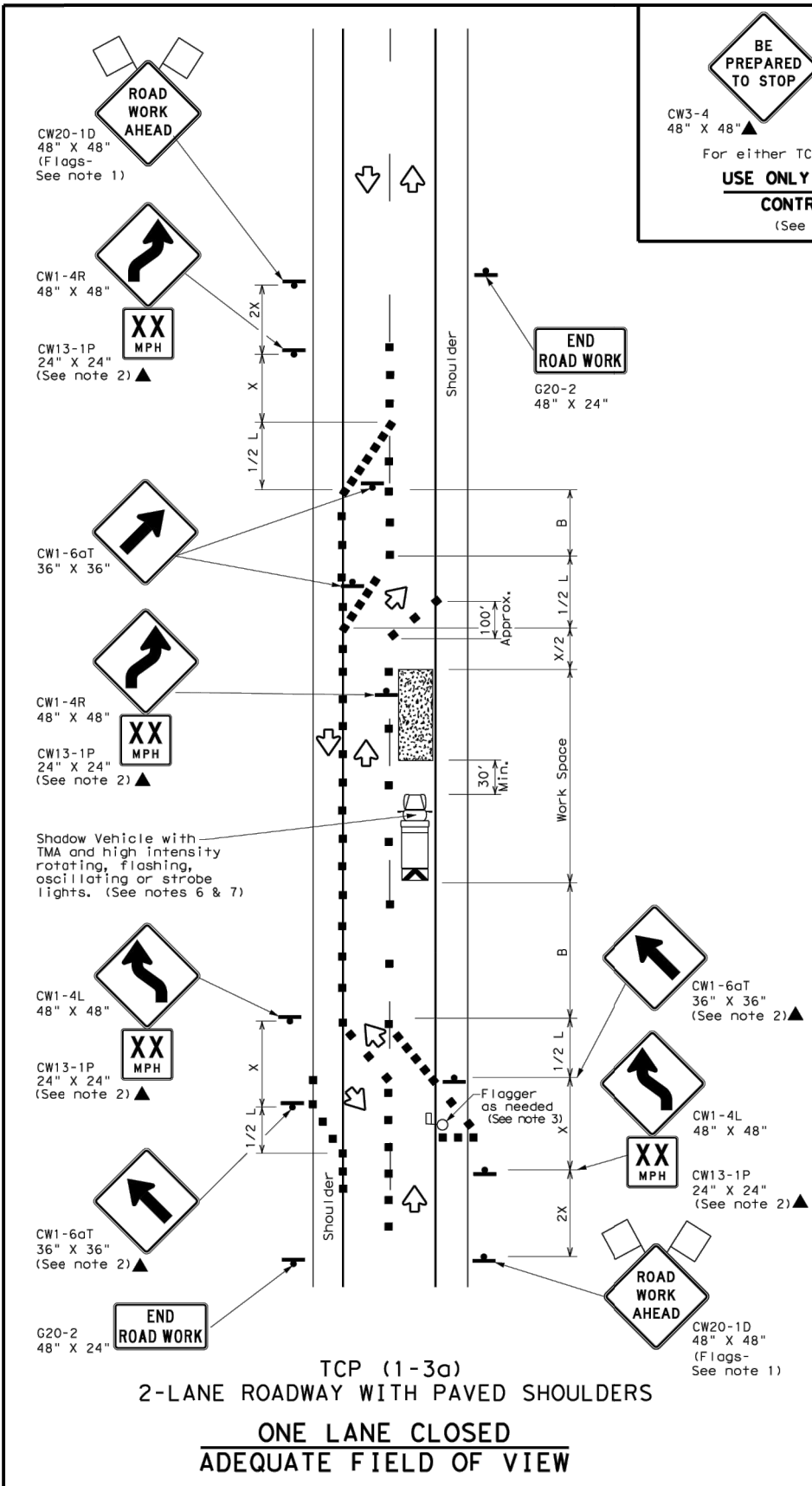
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**LEGEND**

	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "x"	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	L = WS <sup>2</sup> / 60	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40	L = WS	265'	295'	320'	40'	80'	240'	155'
45		450'	495'	540'	45'	90'	320'	195'
50	L = WS	500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60	L = WS	600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70	L = WS	700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

\* Conventional Roads Only  
\*\* Taper lengths have been rounded off.  
L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

**TYPICAL USAGE**

MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓		

- GENERAL NOTES**
- Flags attached to signs where shown are REQUIRED.
  - All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
  - Flagger control should NOT be used unless roadway conditions or heavy traffic volume require additional emphasis to safely control traffic. Additional flaggers may be positioned in advance of traffic queues to alert traffic to reduce speed.
  - DO NOT PASS, PASS WITH CARE and construction regulatory speed zone signs may be installed downstream of the ROAD WORK AHEAD signs.
  - When the work zone is made up of several work spaces, channelizing devices should be placed laterally across the closed lane to re-emphasize closure. Laterally placed channelizing devices should be repeated every 500 to 1000 feet in urban areas and every 1/4 to 1/2 mile in rural areas.
  - A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
  - Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect wider work spaces.
  - Where traffic is directed over a yellow centerline, channelizing devices which separate two-way traffic should be spaced on tapers at 20', or 15' if posted speed are 35 mph or slower, and for tangent sections, at 1/2S where S is the speed in mph. This tighter device spacing is intended for the area of conflicting markings not the entire work zone.

Texas Department of Transportation  
Traffic Operations Division Standard

**TRAFFIC CONTROL PLAN**  
**TRAFFIC SHIFTS ON**  
**TWO LANE ROADS**  
**TCP (1-3) - 18**

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