

Preliminary Plan Enclosures VOLUME I

- Preliminary Plan Application and Checklist
- Application Fee – Check #11289
- Abutters List Labels
- Local and State Review Comments
- Property Deeds Title Commitment, and Owner Affidavit Letter
- Lighting Layouts and Fixtures
- Landscaping Zoning Figures
- Traffic Impact Study, Robin Rug Mixed-Use Development, 8/9/22



Preliminary Plan Application and Checklist

**APPLICATION FORM AND SUBMISSION CHECKLIST FOR
MAJOR LAND DEVELOPMENTS AND MAJOR SUBDIVISIONS**

Major Subdivisions - A plan for a **residential** subdivision of land **consisting of more than five lots**; or, a plan for a **nonresidential** subdivision of land.

Major Land Development - A plan for a residential or nonresidential land development project as defined in Article VIII of the Zoning Ordinance.

The completed application form together with the appropriate materials from Items A, B, C, D, and E for either a Master Plan, Preliminary or Final application as indicated on the checklist shall be submitted to the Administrative Officer at least three weeks prior to the regularly scheduled meeting of the Planning Board.

Date Submission Due: _____ Planning Board Meeting Date: _____

APPLICATION FORM

Type of Application: Please check one:

☐ **Master Plan** - An overall plan for a proposed project site outlining general, rather than detailed, development intentions. *NOTE:* The entire property under the applicant's ownership shall be shown on the Master Plan with all proposed development indicated, including future phases. Failure to show the entire property and all proposed development may result in forfeiture of future development rights on the property.

☒ **Preliminary** - The stage, following Master Plan, at which time all required detailed engineering drawings and all required State and Federal Permits shall be submitted.

Indicate date of Master Plan Approval: May 12, 2022

Indicate dates of all extensions: _____

☐ **Final** - The stage, following Preliminary, which is the last stage of review.

Indicate date of Preliminary Plan Approval: _____

Indicate dates of all extensions: _____

APPLICATION FORM, CONT.

1. Name of proposed subdivision: Bristol Yarn Mill

2. Name, address and telephone number of property owner (if the owner of record is a corporation, the name and address of the president and secretary):

Russ-Russ Realty Co.: AP 10, Lots 41, 42, 44, 60, 61, 62, 68, 73
Russel Karian: AP 10, Lots 49, 50
Karian Realty Co.: AP 10, Lots 43, 74, 76
Sentier Realty, LLC: AP 10, Lot 71

3. Name and address and telephone number of applicant, if different from owner: (A written, notarized confirmation from property owner authorizing the applicant to make the submission shall also be submitted):

Brady Sullivan Properties, LLC
670 North Commercial Street, Manchester, NH 03101
603-622-6223
Contact: Shane Brady

4. Plat and lot number(s) of the parcel being subdivided: AP 10, lots 41, 42, 43, 44, 48, 49, 60, 61, 62, 68, 73, 74 & 76

5. Area of the subdivision parcel(s): 4.8 +/- acres

6. Zoning District: Waterfront PUD, Waterfront, Downtown

7. Names of existing streets within and immediately adjacent to the parcel being subdivided with notation as to whether Town, State or Private. Include right-of-way and pavement widths:

Thames St (Town) 49.5'/28'+/-

Hope St (State) variable (est. >60')/32'+/-

Constitution St (Town) variable (est. >60')/39'+/-

8. Attach a list of the names and current mailing addresses (and on mailing labels) of property owners within notice area radius (See Section 8.5A) from the current real estate and assessment records of the Town, including plat and lot numbers

Signed by Owner/Applicant: [Signature] Date: 11-3-2022

Notarized:

Subscribed and sworn to before me this 3 day of November 20 22

Denise Brees
NOTARY PUBLIC



SUBMISSION CHECKLIST

MAJOR LAND DEVELOPMENTS AND MAJOR SUBDIVISIONS

M = Master Plan

P = Preliminary

F = Final

	M	P	F	Applicant ✓	Date & Town Initials
NOTE: All plan sheets must include Title Block as well as items B 1-8 below.	•	•	•	✓	
A. Plan of Existing Conditions - Five (5) blue-line or photocopies of a plan at a scale of 1" = 40' (minimum size of 8 1/2" x 11", maximum size of 18" x 24") showing the entire tract under the applicant's ownership as it currently exists, including location and dimension of existing lots, easements and rights-of-way, and all natural and man-made features;	•				
B. Subdivision/Land Development Drawing(s) - Master Plan and Preliminary Plans - Five (5) blue-line or photocopies shall be submitted for distribution to and review by the Technical Review Committee. Each sheet shall be a minimum size of 8 1/2" x 11" and a maximum size of 24" x 36" at a scale of 1" = 40', unless determined otherwise at the Pre-Application Conference, with a sufficient number of sheets to clearly show all of the information required. Sheets shall be numbered sequentially (e.g. sheet 1 of 3, 2 of 3, etc.)	•	•		✓	
Final Plans - Five (5) blue-lines or photocopies shall be submitted for distribution to and review by the Technical Review Committee. Upon approval, one (1) reproducible mylar*, six (6) blue-line or photocopies, and one electronic AutoCAD file of all plans shall be submitted. Each sheet shall be a maximum size of 24" x 36" at a scale of 1" = 40' [unless otherwise specified by the Planning Board]; two of these sheets shall include the assessor's numbers, signed by the Tax Assessor. The subdivision plans shall contain the following:			•		
1. Name of the proposed subdivision; including phase numbers, if any	•	•	•	✓	
2. Name and address of property owner or applicant;	•	•	•	✓	
3. Name, address and telephone number of person or firm preparing the plan;	•	•	•	✓	
4. Date of plan preparation, with revision date(s) if any;	•	•	•	✓	
5. Graphic scale (1"=40' or larger) and north arrow;	•	•	•	✓	
6. Plat and lot number(s) of the land being subdivided ;	•	•	•	✓	
7. Legend showing all symbols;	•	•	•	✓	
8. Zoning districts(s) of the land being subdivided and the abutting and adjacent properties. If more than one district, zoning boundary lines must be shown;	•	•	•	✓	
9. Perimeter boundary lines of the subdivision , drawn so as to distinguish them from other property lines. These shall be marked in the field by survey stakes to identify the limits of the property;	•	•	•	✓	

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10. Names of abutting property owners and property owners across any adjacent streets;	•	•	•	✓	
11. Area of the entire proposed development parcel(s);	•	•	•	✓	
12. Location and number of all proposed lots, with accurate areas, dimensions and angles indicated; including a zoning data table showing calculations necessary to determine conformance to zoning regulations;	•	•		✓	
13. Location, names, right-of-way and pavement widths of existing and proposed streets within and immediately adjacent to the subdivision parcel;	•	•	•	✓	
14. Location, size and proposed use of existing buildings and structures, including historic designation, if any;	•	•	•	✓	
15. Location of existing utilities including wells and individual sewage disposal systems (abandoned utilities must be shown and noted as such);	•	•	•	✓	
16. Provisions for collecting and discharging stormwater;	•	•		✓	
17. Location, scale, massing, height, (including relationship to existing and proposed grades) and dimensions of proposed structures, if any; including; floor area ratios, lot coverage and total building square footage; this information may be provided on a separate sheet, if necessary;	•	•	•	✓	
18. Location of proposed permanent bounds (documentation that all proposed bounds have been set shall be provided prior to Final plan recording);		•	•	✓	
19. Location of existing environmental features including general soil types, rock outcrops, surface water, wetland areas, wooded areas and major trees twelve (12) inch caliper or larger, and any other significant environmental features, if any;	•	•		✓	
20. Existing contours with minimum intervals of two (2) feet; where any changes in contours are proposed, finished grades must be shown as solid lines. Spot elevations must also be shown with at least two (2) benchmarks referenced;	•	•		✓	
21. Location of any wetlands, watercourses or their buffers (perimeter wetlands) present on the subdivision parcel, as determined by a RIDEM qualified wetlands biologist {in accordance with RIDEM Wetland Regulations};	•				
22. Notation as to flood zone of the property including base flood elevation data;	•	•		✓	
23. Areas of agricultural use, (if any);	•	•		N/A	
24. Location of historic cemeteries on or immediately adjacent to the subdivision (if any);	•	•		N/A	
25. Location of any unique natural and/or historic features, including stone walls;	•	•		✓	
26. Location, dimension, and area of any land proposed to be set aside as open space, recreation, or drainage or conveyed to the Town for public purposes;	•	•	•	✓	
27. Location and notation of type of proposed easement(s) or existing easement(s) to remain (if any) with accurate dimensions and areas indicated;		•	•	✓	

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28. Notation of special conditions of approval imposed by the Planning Board (if any);		•	•	✓	
29. Notation of any permits and agreements with State and Federal Reviewing agencies (if any);		•	•	✓	
30. A place for signatures of the Planning Board Chair or Designee must be provided on all plans and/or documents to be signed by the Planning Board;	•	•	•	✓	
31. Certification by a Registered Land Surveyor that all interior and perimeter lot lines and street lines of the land being subdivided have been designed to conform to <u>Procedural and Technical Standards for the Practice of Land Surveying in the State of Rhode Island and Providence Plantations</u> , effective April 1, 1994, as amended. Measurement standards for the surveys shall meet the minimum standards for Class I Surveys.		•	•	✓	
C. Construction Drawings - Six (6) blue-line or photocopies of preliminary and final construction plans of street and drainage structures drawn to a minimum scale of 1 inch to 40 feet (1"=40') for referral to the Technical Review Committee. Each sheet shall be no larger than 24 " by 36", and a sufficient number of sheets shall be included to clearly show all of the information required. Sheets shall be numbered sequentially (e.g., sheet 1 of 3, 2 of 3, etc). All construction plans shall be certified as correct (stamped and signed) by a Registered Professional Engineer:		•	•	✓	
1. <u>If street creation or extension is proposed:</u>					
a. <i>Streets Plan and Profile:</i> (minimum scale of 1"=40' horizontal) with profiles (minimum 1"=4' vertical) indicating the street center line, curblines, and the existing elevations of the ground at the street center line, location size, rim and invert elevations of proposed sewer lines, water lines and other underground utilities; and, street cross-sections showing width of right-of-way, roadway; and, if required location and width of sidewalks.					
b. Street plans shall also include all vehicular access to and from the site onto public streets including the size and location of curb cuts, driveways, parking and loading areas and other off-site traffic improvements necessary to ensure public safety; and, stubs for future connections to future streets in adjacent vacant lands. NOTE: The Planning Board may make any referrals and require of the applicant any studies it deems necessary to evaluate traffic and circulation plans.					

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C. Construction Drawings, cont.		•	•		
2. For phased projects, as-built drawings for the previous phase;		•	•	N/A	
3. Drainage and Utility Plans to show the existing and proposed drainage structures, drainage basin areas, drainage flow paths, and outfalls with water quality measurements, including all utilities such as sewer, water, electric, gas, fire alarm cable, telephone and cable TV, manholes, valves, hydrants. Also included shall be a report summarizing drainage calculations; drainage plans and drainage calculations shall be prepared by a Registered Professional Engineer;		•	•	✓	
4. Additional plans with appropriate construction details and construction notes for street pavement, sidewalk, drainage structures, street lighting, exterior site lighting/photometrics plan (Major Land Developments only), sanitary structures, water line and water line structures, trench excavation, and steep slope excavation, if applicable;		•	•	✓	
5. Grading plans to show proposed contours at two-foot intervals for all grading proposed for on and off-site street construction, drainage facilities, and upon individual lots if part of proposed subdivision improvements;		•	•	✓	
6. Soil erosion and sediment control plan in conformance with Appendix F, if required;		•	•	✓	
7. Landscaping plan as detailed in Appendix F;		•	•	✓	
8. Plans showing street names and hydrant locations approved by the Fire Chief;		•	•	✓	
9. Construction Schedule identifying expected start and finish times for major construction tasks;		•		✓	
10. Temporary improvements, if any, in accordance with Appendix F;	•	•		N/A	
11. A traffic flow plan showing circulation patterns within the development;		•		✓	
12. If any streets, areas, or facilities are to be dedicated to the Town of Bristol, a statement shall be added to all final plat plans stating that such dedication shall not impose any duty or responsibility upon the Town of Bristol to maintain or improve any dedicated streets, areas, or facilities until specifically authorized by the Town Council per Section 6.7 of these regulations.			•		
D. Supporting Materials:	•	•	•		
1. 12 reduced copies of all plans required in Items A, B, and C above (minimum 8 1/2" x 11" and maximum 11" x 17");	•	•	•	✓	
2. One (1) copy of a narrative report providing the general description of the proposed development and phasing if any, including the proposed financing and present ownership of all of the land included within the development; if the applicant does not own all the land, then explain how it will be assembled or how site control will be achieved;	•				

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3. One (1) copy of a current deed and an Attorney's certificate certifying title, including encumbrances, easements, and/or temporary or permanent restrictions on the property;		•		Final Plan	
4. A list of consultants (i.e. engineering, planning, environmental) to be used on the project;		•		✓	
5. A vicinity map, (locus map) drawn to a minimum scale of 1" = 400' or as necessary to show the area within one-half mile of the subdivision parcel showing the location of all streets, existing lot lines, and zoning district boundaries. Schools, parks, fire stations and other significant public facilities shall be indicated on the locus map by shading and labeling the specific use;	•				
6. A list of the names and mailing addresses as shown on the current real estate and assessment records of the Town for the property owners within the notice area; (See Section 8.5) this information shall also be on mailing labels;	•	•		✓	
7. Low Impact Development (LID) Site Planning and Design strategies and Checklist from the Rhode Island Stormwater Design and Installation Standards Manual. Stormwater designs should meet RI Stormwater Manual Minimum Standards.	•				
8. A radius map, showing the property within the notice area (See Section 8.5) including: a. the shape, dimension and area of the property; b. the location of all zoning use district boundary lines; c. the assessor's plat and lot numbers; and, d. the general location, shape, use; and if any, historic designation of all existing buildings and structures and improvements; The above information may be shown on an 11" x 17" plan at a scale as appropriate;	•				
9. Architectural elevations (with measurements as needed for each interpretation) and of all exterior facades of proposed or existing buildings, structures and equipment including type and color of materials to be used; elevations shall be at an appropriate scale as determined at the preapplication stage;	•				
10. Legal documents describing the property, including proposed easements and rights-of-way, offer to convey public streets, creating Homeowners Association (if appropriate) deed transferring open space, dedications, restrictions, or other required legal documents:					
a. Two draft copies for review by the Planning Board Solicitor;	•				
b. Two signed final copies;		•		Final Plan	
11. Written confirmation from the RI Department of Environmental Management that the plans of the proposed subdivision, including any required off-site construction, have been reviewed and indicating that the Wetlands Act either does not apply to the proposed site alteration or that approval has been granted for the proposed site alteration;		•		No	

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12. In lieu of item 10 above, an affidavit signed by a qualified professional (having minimum qualifications as described by the RIDEM Department of Freshwater Wetlands) stating that there are no freshwater wetlands or their buffers (perimeter wetlands) present on the property being subdivided {in accordance with RIDEM Wetland Regulations};		•		N/A	
13. Preliminary determination from the Coastal Resources Management Council if the parcel to be subdivided is subject to the jurisdiction of this agency;		•		✓	
14. A Physical Alteration Permit (PAP) issued by the State Department of Transportation for any connection to or construction work within a State highway or State right-of-way (if applicable);		•		N/A	
15. Water Service (if proposed):					
a. Written confirmation that the Bristol County Water Authority has reviewed the plan and is able to provide water service;		•		Final Plan	
b. A copy of the water contract covering the installation of water service or written confirmation that a contract has been executed or will be executed upon approval by the Planning Board (a copy of the executed contract must be submitted prior to endorsement by the Planning Board;			•		
16. Sewer:					
a. Written confirmation from the Bristol Water Pollution Control Facility certifying that the subdivision can tie into the Bristol Sewer Service; or,		•		Final Plan	
b. If Individual Sewage Disposal Systems are proposed, either a preliminary subdivision suitability report or a water table verification from the Rhode Island Department of Environmental Management indicating that the soil and water table within the proposed subdivision are suitable for the safe and proper operation of individual sewage disposal systems;		•		N/A	
17. Certification from the Tax Collector that all property taxes are current and that sewer assessments and sewer use fees are paid;			•		
18. A letter stating it is the intent to complete the required improvements prior to Planning Board endorsement of the final plat or a letter requesting that a security sufficient to cover the cost of required improvements be set by the Planning Board, along with a construction cost estimate for improvements;			•	N/A	
19. Application form;	•	•	•	✓	

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<p>20. Initial written comments on the Master Plan and final written comments on Preliminary from the following agencies:</p> <p>a. _____ Planning Board Engineer Date: _____</p> <p>b. _____ Water Authority Date: _____</p> <p>c. <u>X</u> Sewer Department Date: <u>12/24/21</u></p> <p>d. _____ Building Inspector Date: _____</p> <p>e. <u>X</u> Planning Department Date: <u>5/12/22</u></p> <p>f. _____ Department of Public Works Date: _____</p> <p>g. _____ Fire and Police Date: _____</p> <p>h. _____ Conservation Commission Date: _____</p> <p>i. _____ Other Local Agencies, Specify: _____ Date: _____</p> <p>j. _____ Adjacent Community (Warren) Date: _____</p> <p>k. _____ RI Dept. of Environmental Management Date: _____</p> <p>l. <u>N/A</u> RI Dept. of Transportation Date: _____</p> <p>m. <u>X</u> Coastal Resources Date: <u>7/27/22</u></p> <p>n. _____ Other State Agencies, Specify: _____ Date: _____</p> <p>o. <u>N/A</u> FEMA Date: _____</p> <p>p. _____ Other Federal Agencies, Specify: _____ Date: _____</p> <p>NOTE: REFERRALS TO THE ABOVE AGENCIES WILL BE MADE BY PLANNING DEPARTMENT STAFF.</p>	•	•		✓	
21. Report and recommendations from the Technical Review Committee;	•	•		✓	
22.Approvals from other Town Boards and commissions, as appropriate;		•	•	In Progress	
23.Approvals from other State Agencies, as appropriate;		•		Final Plan	
24.Such other information as may be required to show that the details of the proposal are in accordance with this section and all other applicable requirements and standards of these Regulations.	•	•	•	N/A	
E. Payment of Required Fees - Payment of the following fees or posting of financial guarantees, if required, to be prior to endorsement by the Planning Board:					
1. Application fee;	•	•	•	✓	
2. Final plat recording fee;			•		
3. Engineer Review and Inspection Fee;	•	•	•	✓	
4. Performance guarantee or other financial guarantees (if applicable);			•		
5. Fees in-lieu of land dedication (if applicable);			•		
6. Maintenance guarantee for acceptance of public improvements (if applicable);			•		
7. Receipt that all other fees to outside agencies have been paid by applicant, if any;			•		
8. Mapping fees shall be paid to the Tax Assessor prior to recording plan.			•		

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Other Requirements: The following items may be required based on the presence of certain site conditions as indicated at the Pre-application or Master Plan stage of review. The applicant shall have any required items prepared by a qualified party.					
1. Fiscal impact statement (see Section 6.6);		•		Separate Cover	
2. Completed Environmental Impact Assessment statement for the proposed development (see Section 6.6);		•		N/A	
3. Archaeological Study;		•		TBD	
4. Traffic Impact Study,		•		✓	
5. Phase 1 Environmental Site Assessment (ESA), conducted by a qualified professional consultant, as may be requested at the pre-application stage. If the Phase 1 indicates suspect environmental site conditions, then a Phase 2 ESA shall be conducted by a qualified professional consultant prior to preliminary plan approval;	•	•		✓	
6. Perspective drawings, view shed maps, line of site diagrams, sketches, renderings, photographs or scale models as needed to illustrate the visual impact on the community;	•				
7. Flood plain compensation calculations;		•		N/A	
8. Historic District Commission Certificate of Appropriateness, if the project requires Historic District Commission review and approval.		•		In Progress	

** Mylar plans shall be on polyester film, single matte with a thickness of 3 mils (.003 inches), and must have opacity so as to allow consistent computer scanning. All plans shall be prepared using a compatible ink with excellent cohesiveness which will produce a permanent bond and result in a plan with long term durability. All signatures must be in black India ink or its equal.*



Application Fee

Check #11289

PMD, Inc.670 North Commercial Street, Suite 303
Manchester, NH 03101
(603)622-6223M & T Bank
One M & T Plaza
Buffalo, NY
14203
022000046**11289**

**** THIRTY TWO THOUSAND TWO HUNDRED FIFTY AND 00/100 DOLLARS

11/03/2022

\$32,250.00**

TO THE
ORDER OFTown of Bristol
10 Court Street
Bristol, RI 02809

⑈0000011289⑈ ⑆022000046⑆⑈6500831114⑈

DATE:11/03/2022 CK#:11289 TOTAL:\$32,250.00** BANK:PMD, Inc. (3530ckg)
PAYEE:Town of Bristol(v0002811)

Property	Account	Invoice - Date	Description	Amount
zpmddpayr	1501-00	Bristol RI 11/3/22 - 11	Bristol, RI	32,250.00
				<hr/> 32,250.00



Abutters List Labels

Town of Bristol 10 Court St. Bristol, RI 02809	10-22	St. Michael's Church PO Box 414 319 Hope Street Bristol, RI 02809	10-28	Deborah Van Allen Appleyard 15 Church Street Bristol, RI 02809	10-29
Dean A. & Mary C. Nadalin 9 Church Street Bristol, RI 02809	10-30	162 Thames St, LLC 99 Tupelo Street Bristol, RI 02809	10-31	Russ-Russ Realty Co. PO Box 656 Bristol, RI 02809	10-32
Town of Bristol 10 Court St. Bristol, RI 02809	10-33	St. Albans Association, LLC c/o Rory Hanmer 125 Sunrise Drive Bristol, RI 02809	10-34	Mary N. Remieres Life Estate c/o Susan A. Donovan 353 Hope Street Bristol, RI 02809	10-35
Beth A. & Torey Engell 18 Church Street Bristol, RI 02809	10-36	Barbara L. Pyle, Trustee Barbara L. Pyle Living Trust 12 Church Street Bristol, RI 02809	10-37	Debra A. Deveau & Steven M. JT Bramwell 8 Church Street Bristol, RI 02809	
John S. & Gwenda J. McQuilkin 90 Robin Road Portsmouth, RI 02871	10-39	Garry & Catherine Holmstrom Garry & Catherine Holmstrom Trust 341 Hope Street Bristol, RI 02809	10-40	Russ-Russ Realty Co. PO Box 656 Bristol, RI 02809	10-41
Russ-Russ Realty Co. PO Box 656 Bristol, RI 02809	10-42	Karian Realty Co. PO Box 656 Bristol, RI 02809	10-43	Russ-Russ Realty Co. PO Box 656 Bristol, RI 02809	10-44
Eric John Zufelt 82 Thames Street Bristol, RI 02809	10-45	Bankard F. & Rayne G. Baer 40 Constitution Street Bristol, RI 02809	10-46	Gordon Stewart & Benita Maija 281 Hope Street Bristol, RI 02809	10-47
Thomas A. & JoAnn Pasqual Trustees 221 Hope Street, Unit 9 Bristol, RI 02809	10-48	Karian Russell 4 Owen Lane Barrington, RI 02806	10-49	Karian Russell 4 Owen Lane Barrington, RI 02806	10-50
Michael A. & Paula Ramos 289 Hope Street, #1 Bristol, RI 02809	10-51	Richard W. De Nathan & Ilsi Arreaza, TE c/o Washington Trust 23 Broad Street Westerly, RI 02891	10-52		

Richmond N. & Nancy E. Chace 21 Constitution Street Bristol, RI 02809	10-53	Harold W. Demopulos/ Trust Agreement 17 Constitution Street Bristol, RI 02809	10-54	Calm Realty, LLC 11 Constitution Street Bristol, RI 02809	10-55
John Hartley Paula Arsenault Hartley 9 Constitution Street Bristol, RI 02809	10-56	James S. & Jane L. Lavender 340 Porter Street Melrose, MA 02176	10-57	Peter M. Salcone 509 Clarks Row Bristol, RI 02809	10-58
Victor G. Cabral, Jr. 122 Mt. Hope Avenue Bristol, RI 02809	10-59	Russ-Russ Realty Co. PO Box 656 Bristol, RI 02809	10-60	Russ-Russ Realty Co. PO Box 656 Bristol, RI 02809	10-61
Russ-Russ Realty Co. PO Box 656 Bristol, RI 02809	10-62	Bristol County Lodge 1860 Benevolet & Protective 1 Constitution Street PO Box 226 Bristol, RI 02809	10-63	Town of Bristol (Vacant Land) 10 Court Street Bristol, RI 02809	10-70
Sentier Realty, LLC 125 Thames Street Bristol, RI 02809	10-71	Russ-Russ Realty Co. PO Box 656 Bristol, RI 02809	10-73	Karian Realty Co. PO Box 656 Bristol, RI 02809	10-74
Patricia J. Booth 316 Hope Street Bristol, RI 02809	10-75	Karian Realty Co. PO Box 656 Bristol, RI 02809	10-76	301 Hope Street, LLC 301 Hope Street PO Box 903 Bristol, RI 02809	10-78
Kurt R. & Chandra W. Hammond, TE 9.5 Church Street Bristol, RI 02809	10-83	Town of Bristol 10 Court Street Bristol, RI 02809	10-84	Robert C. & Diane M. Holt, TE 12 Constitution St. Bristol, RI 02809	11-6
RDH Realty, LLC 12 Constitution St. Bristol, RI 02809	11-7	Robin K. Allister 37 Factory Pond Circle Smithfield, RI 02828	11-8	Gordon Karian Irrevocable Trust/Robin Karian Allister, Trustee 37 Factory Pond Circle Greenville, RI 02828	11-9
Robin Karian 37 Factory Pond Circle Greenville, RI 02828	11-10	USCG Finance Center PO Box 4109 Chesapeake, VA 23327	11-22	USCG Finance Center PO Box 4109 Chesapeake, VA 23327	11-23
Monahan-Bell Living Trust 11 Byfield Street Bristol, RI 02809	14-69	Leonard Place, LLC 385 High Street Bristol, RI 02809	14-70	Joan D. Johnson, Trustee 344 Hope Street Bristol, RI 02809	14-71

<p>Martin B. & Linda M. Reynolds 328 Hope Street Bristol, RI 02809</p>	<p>Safe Way Realty, LLC c/o Stephen Coelho PO Box 210 Bristol, RI 02809</p>	<p>Wirsa Bristol, LLC 2 Meadow Circle Barrington, RI 02806</p>
14-86	14-87	14-88
<p>Gregory A. & Alison L. Fox 290 Hope Street Bristol, RI 02809</p>	<p>Halsey Properties, LLC PO Box 687 Bristol, RI 02809</p>	<p>Gregory A. & Alison L. Fox 290 Hope Street Bristol, RI 02809</p>
14-102	14-103	14-116
<p>Mathew, Steven, & Ann Gardner, Trustees-Gardner Trust 12 Byfield Street Bristol, RI 02809</p>		
14-118		



Local and State Review Comments



Town of Bristol, RI
WATER POLLUTION CONTROL DEPARTMENT
2 PLANT AVENUE
BRISTOL, RI 02809-3015
(401) 253-8877 fax: (401) 253-2910

TOWN HALL
10 COURT STREET
BRISTOL, RI 02809
(401) 253-7000

Jose' J. Da Silva, Superintendent

12/24/2021


To: Planning Board

RE: Bristol Yarn Mill Redevelopment
AP 10 Lot 41, 42, 43, 44, 49, 50, 60, 61, 62, 68, 71, 73, 74, and 76

Just some points of concern/items, we require of the developer, while looking at the plans of the redevelopment of the Yarn Mill:

1. The number of residential units in the proposed development has grown more than 50% from 98 residential units to 151. The developer needs to provide a detailed analysis of wastewater contribution.
2. Requirement to install new service lateral/laterals between the main and the building. Letter to Diane Williamson p.2, second pa. states that "New utility services will be provided to existing public utility infrastructure ..." This requirement is confirmation of their intention.
3. That back flow preventers be installed.
4. Department wants signed certifications from Architect/Engineer that extraneous water sources have been physically inspected and verified to discharge to a location other than the sanitary sewer.
Certification to include the following at a minimum
 - Ground water (i.e. Floor drains, French drains, sump pumps, leaking joints) and
 - Stormwater (i.e. roof drains, yard drains, and catch basins) have been verified and inspected and verified that they have been removed.
5. Confirm that commercial facilities will incorporate grease traps as required. Best that they contact the department's pretreatment coordinator for details.
6. Engineer/Architect to provide estimate of peak flow discharge from new development and evaluation of impact on pump station capacity.

Respectfully,


Jose' J. Da Silva
Superintendent
Bristol Water Pollution Control Facility



State of Rhode Island
Coastal Resources Management Council
Oliver H. Stedman Government Center
4808 Tower Hill Road, Suite 3
Wakefield, RI 02879-1900

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**RHODE ISLAND COASTAL RESOURCES MANAGEMENT COUNCIL
REPORT OF FINDINGS -- PRELIMINARY DETERMINATION**

STATEMENT OF LIMITATIONS

The contents of this staff determination report shall be valid only for the period on and preceding the date of this report. This report is neither an approval nor denial of the subject proposal. It is an evaluation of CRMC regulations in effect as of 27 July 2022 as they pertain to the below stated proposal, including preliminary staff recommendations.

Modifications to the below stated proposal may, upon the discretion of the CRMC, render this determination null and void.

APPLICANT INFORMATION

NAME: Russ Russ Realty **CRMC FILE NO.** D2022-05-126

LOCATION/POLE: 125 Thames Street

CITY/TOWN: Bristol

PLAT: 10

LOT:

41,42,43,44,49,50,60,61,62,68,

CONTACT PERSON(S) & ADDRESS:

Also send to:

Russ Russ Realty
P.O Box 656
Bristol, RI 02809

Brady Sullivan Properties
670 North Commercial St
Manchester NH 03101

PRELIMINARY REVIEW INFORMATION

PROPOSAL: Redevelopment of historic structure, including offsite parking

PLAN(S) REVIEWED: "Brady Sullivan Properties, LLC, Master Plan, Bristol Yarn Mill, Alternative Parking Plan, 125 Thames Street, Bristol..." dated April 13, 2021 and unstamped by Fuss & O'Neill.

INVESTIGATOR: T. Silvia

DATE/TIME: 6/14/22 PM

MEASUREMENTS & OBSERVATIONS: Confirmed existing conditions, noted offsite parking areas

PREVIOUS CRMC ACTIONS FOR SITE: 76-11-12 Town (cxld), 79-9-26 revetment/fill; 85-7-52 addition; 03-5-49 mtce; 10-8-49 demo/restoration; 19-11-78 Town MPL/upland work

Preliminary Buffer and Setback Requirements:

SETBACK (ref. Section 1.1.7 Red Book): 50'

BUFFER (ref. Section 1.1.9 Red Book): varies, Urban Coastal Greenway (UCG)

NAME: **Russ Russ Realty**
CRMC FILE NUMBER: **D 2022-05-126**

Note: **Setbacks** apply to "construction related activities" including filling, removing, and grading (ref: Section 1.3.1(B) Red Book). The coastal program requires a minimum setback of either 50', or the buffer zone width plus 25' (whichever is greater). Work within this minimum setback will require a variance per Section 1.1.5 of the Red Book. All variances must be requested in writing. No construction or construction related work shall occur within the required setback (exemptions include structural shoreline protection, outfalls and water dependant uses). Work within the required setback may require a Category "B" review (public notice and decision by the full coastal council) and would likely result in adverse CRMC staff recommendations to the Coastal Council during the review process.

Buffer zones are areas that must be retained in, or allowed to revert to, "an undisturbed natural condition." All structures (excluding accessory structures) should be setback a minimum of 25' from the buffer zone to allow for access, fire protection and maintenance without infringement into the buffer.

If applicable, the plan must show "area of land within 50 feet" in accordance with Rule 5.04 of The Rules and Regulations Governing the Protection and Management of Freshwater Wetlands in the Vicinity of the Coast (the Rules), and label this area as a "buffer zone" in accordance with Rule 5.14. In addition, no activities (such as: drainage, grading, filling, etc.) may affect the freshwater wetland or the buffer zone. Where such alterations occur, or are proposed, an application shall be submitted in accordance with CRMC's Freshwater Wetland Rules.

Coastal Hazard: In accordance with Section 1.1.10, the applicant is encouraged to utilize CRMC's "STORMTOOLS" mapping feature to better understand the impact of current and future Sea Level Rise and Storms on the subject property. Also, in accordance with Section 1.1.6(I), the applicant is required to complete a "Coastal Hazards Worksheet" to further understand the impact of climate change on a proposal (<http://www.crmc.ri.gov/coastalhazardapp.html>). While the RICRMP does not yet require structures to be designed for SLR scenarios, the applicant should consider SLR, Climate Change, and design life expectations in design planning.

Coastal feature verification shall be valid for one-year from the date of this Determination or until an erosion event (e.g., due to storm event, landslide, man-induced alteration, etc.) occurs that alters the coastal feature.

SUMMARY OF FINDINGS

CRMC JURISDICTION: Yes **TYPE WATER:** 5; **Comm/Recreational Harbors, Bristol Harbor**
For the purpose of this review the coastal feature(s) shall be the manmade shoreline and the inland edge of coastal(s) feature shall be the top of the riprap revetment and/or edge of outfall headwall.

Applicability of Red Book and SAM Plans (as amended):

Red Book Sections: 1.1.7, 1.1.9, 1.1.10, 1.1.11, 1.2.1(E), 1.2.2(F), 1.3.1 (A), 1.3.1(B), 1.3.1(C), 1.3.1(D), 1.3.1(F), 1.3.1(G), 1.3.5, 1.3.6, 1.8

SAMP: N/A, however Metro Bay SAMP UCG guidelines recommended for the site

STAFF CONCERNS/COMMENTS/INFORMATION REQUIREMENTS:

- 1) Staff is familiar with the project site from past permit reviews. The parcel consists of a historic mill building running down Thames Street, bordered by the Bristol County Elks, US Coast Guard Station and Town of Bristol on its other sides. The existing development is setback less than 20' from the coastal feature in some locations and existing lawn/debris is located in the area, which is fenced to the north separating it from the Town's marina facilities.
- 2) The current proposal is to redevelop the mill into a mixed use residential (127 units) and commercial structure, with offsite parking/stormwater/landscaping improvements located on the east side of Thames St, in addition to interior parking facilities located below the Base Flood Elevation (BFE). All equipment and livable space will be located above BFE. The applicant is advised that a Coastal Hazard Analysis (CHA) is required for this project and should use its results in its designs moving forward.
- 3) Stormwater runoff from existing roofed areas will receive treatment and all new driveway and

NAME: **Russ Russ Realty**
CRMC FILE NUMBER: **D 2022-05-126**

parking surfaces will require treatment. Bioretention is proposed seaward of the existing building. All stormwater management shall meet the requirements of the RI Stormwater Installation and Design Standards Manual and supporting drainage details and calculations are required. An Operations and Management (O&M) plan is required as part of the Assent application.

4) All new work (including stormwater BMPs, filling/removing/grading and exterior buildings) are subject to the 50' construction setback. Variances are required where this cannot be met (ref. Section 1.1.7).

5) For commercial development, buffer zones are based on the residential guidance of the Redbook Section 1.1.11, however in this locale, it is clear the full buffer zone cannot be accommodated. Due to its historic nature and existing location, staff concurs with the applicant's request to use the guidance contained within the Metro Bay SAMP's UCG for designing a restorative buffer zone for the site.

6) Additionally, both the UCG and Section 1.3.6 require a public access plan for new subdivisions. The proposed design is supported by staff and could include further amenities to offset variance requests, such as signage and public parking as well as working with the Town to connect along the Lot 70 border, linking much of the remaining shoreline along this side of the downtown harbor for continuous public access. A public access deed restriction will be required as part of this project as well as a conservation easement and future ownership (HOA?) documentation.

7) A local building official signoff (including Town zoning as applicable) is required for this project and it is staff's understanding there may be forth coming objection to this project during CRMC's required 30day public notice period, based on the comments received during local review. The applicant shall provide a written response to any comments received during CRMC notice period.

8) Future development of the waterside of this parcel will be subject to separate CRMC PD application process as well as the requirements of Section 1.3.1(D) and US ACOE review.

9) The RIHPHC provides comment on CRMC projects and has submitted a response indicated that they will need additional information/review of the project during future Assent submittal unless they are reviewing the project under a separate federal tax credit process. Please continue to work directly with the RIPHC during formulation of this project.

10) Overall, provided the stormwater management, buffer zone and public access components are addressed, the project appears likely to receive favorable staff recommendations. A public notice and possible public hearing may be required for this project.

SIGNATURE: _____



STAFF BIOLOGIST



Town of Bristol, Rhode Island

Planning Board

10 Court Street
Bristol, RI 02809
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401-253-7000

DECISION OF BRISTOL PLANNING BOARD

Bristol Yarn Mill – Major Land Development

Master Plan

OWNERS: Russ-Realty Co., Russell Karian, Sentier Realty, and Karian Realty, Co.

APPLICANT: Brady Sullivan Properties, LLC

PROPERTY ADDRESS: 125 Thames Street

PLAT 10 LOTS 41, 42, 43, 44, 49, 50, 60, 61, 62, 68, 71, 73, 74, and 76

Motion:

“The Bristol Planning Board hereby acknowledges the applicant’s agreement to an extension of time frame on the action on the Master Plan for the Bristol Yarn Mill (a/k/a Robin Rug) for an additional 60 days to bring the deadline for action to June 21, 2022, and grants conditional approval to the Master Plan as revised April 13, 2022 submitted on April 14, 2022, and recommends to the Town Council that the 2008 Change of Zone conditions and ordinance text also be amended to allow a density of 127 residential units and 6,300 square foot of commercial space along with parking spaces being 9’ wide in lieu of the required 10’ wide and to allow single – striped parking spaces in the interior parking lot (see plan entitled “Master Plan Bristol Yarn Mill – Alternative Parking Plan”) on the east side of Thames Street on Plat 10 Lots 41, 43, 44, 49, 50, 68, 71, 73, 74, and 76.”

Approval is based upon the following findings of fact and conclusions of law.

I. Procedural History

1. In 2008, the Town Council approved a conditional Change of Zoning Map to apply the Waterfront – Urban Rehab Land Development “Urban Rehab Land Development” (a/k/a Waterfront Planned Unit Development) zone to the mill property at 125 Thames Street, Plat 10, Lots 42, 60, 61, and 62. Along with the Change of Zoning Map in 2008, the Town Council revised the Zoning Ordinance (Section 28-284 (d)(2) to provide a mix of residential and commercial uses with a residential density of 1 unit per 2,250 square feet of gross floor area. According to the Town of Bristol Tax Assessor’s records, the subject mill building contains 296,717 square feet of gross floor area (not including the basement

and the concrete building being demolished) which allows a residential density of 131 dwelling units. However, when the Town Council applied the Zone to the property, they conditioned the approval on a maximum density of 98 units, along with eight (8) other development conditions relative to public access to the waterfront, affordable housing, commercial space, water-related uses, traffic study, infrastructure mitigation, off-site parking, and existing historic buildings.

2. The 2008 Town Council adopted changes to the Zoning Ordinance Section 28-284 (d)(2) specific to this property and also granted a Change of Zoning Map with conditions. These original zone change conditions run with the land and apply to this proposal.
3. In June 2010, the Planning Board granted Master Plan approval with conditions to the original proposal; however, that approval has expired.
4. In May 2021, a concept review application was submitted by Brady Sullivan Properties, LLC as the applicant. A public Site Visit was held on June 8, 2021 and a concept review meeting with the Planning Board was held on June 10, 2021.
5. On October 15, 2021, Brady Sullivan Properties, LLC, as the applicant, submitted a Master Plan application, along with a petition to change certain conditions of the 2008 zone map change. The Master Plan application proposed a residential density of 151 units and a commercial use square footage of 6,300 in the mill. A petition to change the zone map from Waterfront and Downtown on the surface parking lot property, located opposite the mill on the east side of Thames Street, to the Waterfront Planned Unit Development was also submitted and later withdrawn.
6. The application was re-submitted on November 19, 2021 to address missing requisites and the application was certified complete on December 22, 2021. Planning Board action was needed by March 22, 2022 unless an extension was mutually agreed upon. Two extensions were mutually agreed upon bringing the deadline for Planning Board action to June 21, 2022.
7. The applicant requested a waiver of the architectural renderings which were then submitted on March 3, 2022. The applicant also requested a waiver of the draft legal documents which will be submitted at the preliminary phase; therefore, a waiver is not applicable.
8. A Technical Review Committee meeting on the Master Plan application was held on December 22, 2021.
9. A duly advertised public information meeting was held on January 13, 2022 and continued to February 10, 2022 and March 10, 2022.
10. One member of the Planning Board, Charles Millard, recused himself from consideration of the application. First Alternate Planning Board Member Brian Clark participated in lieu of Mr. Millard.
11. Following the March 10, 2022 meeting, the applicant revised the plans as to the residential density from 151 to 130 units and made revisions to the surface parking plan.
12. The Planning Board held a special meeting on March 16, 2022. The result of this meeting was a 5-0 vote of the Planning Board to direct the Solicitor and the Planner to draft a motion to deny, unless the applicant agreed to a continuance in order to revise its plans. Following this meeting, the applicant revised the plans for a density of 127 units and made

- further revisions to the surface parking lot plan to eliminate any small car parking spaces and increase the buffer to the neighboring properties.
13. The public information meeting was then re-advertised and re-opened on April 14, 2022.
 14. The Director of Community Development recommended approval of the revised plans with the density of 127 units subject to several conditions, including that the applicant convey the parking lot on the northeast corner of Church and Thames Streets to the Town for public parking to satisfy the 10% requirement for land area to be allocated to public or institutional use in Section 28-284 (g).
 15. Upon receipt of this recommendation, the applicant revised the surface parking plan to create additional tandem parking spaces and 9' wide small car spaces, while keeping a buffer greater than the minimum required along the abutting properties; and, agreed that the parking on the corner of Church and Thames Street (Plat 10, Lot 32) be would be deeded to the Town. The Director of Community Development recommended approval of the alternate parking plan.
 16. John McCoy, Esq. and John Rego, Esq. appeared as legal counsel on behalf of the Applicant. Several witnesses were presented in support of the application.
 17. Members of the public were also present at each of the public informational meetings and provided testimony in support and in opposition of the proposed development for the Board's consideration. Written comments in support and in opposition to the proposed development were also received and entered into the record.
 18. At the Planning Board's April 14, 2022 meeting, a motion was passed on a 3-2 vote to direct the Director of Community Development and the Town Solicitor to draft a motion for approval along with recommendations to the Town Council for changes to the conditions of the 2008 zone change.

II. Project Description

1. The proposed project is described on the plans prepared by Fuss and O'Neill entitled Bristol Yarn Mill Master Plan, Shawn Martin, P.E. Sheets G1.01- G1.02, C1.01-C1.03, revised November 19, 2021 as noted on the cover sheet G.1.01, and as further amended on a plan entitled Bristol Yarn Mill Master Plan Alternative Parking Plan, dated April 13, 2021 [*sic – the correct date is April 13, 2022*]. Said Alternative Parking Plan depicts 151 offsite parking spaces 9' wide by 18' long and a total parking count of 299 spaces for the proposed development. Included with the Master Plan are plans entitled Topographic and Boundary Survey prepared by Charles E. Lent, Registered Professional Surveyor of Control Point Associates, Sheets 1-3, dated October 1, 2021.
2. The subject Mill building property is located at 125 Thames Street and includes surface parking lot parcels which extend from Thames Street to Hope Street on : Plat 10, Lots 41, 42, 43, 44, 49, 50, 60, 61, 62, 68, 71, 73, 74, and 76.
3. The mill buildings will be renovated for the proposed 127 residential units and 6,300 square feet of commercial spaces. The rehabilitation and reuse of the buildings will be in accordance with the guidelines of the RI Historical Preservation and Heritage Commission and the Bristol Historic District Commission. None of the historic buildings are intended to be removed and no additions or significant modifications to the existing buildings are

proposed. Only the non-contributing concrete masonry block building located near the Bristol Maritime Center is planned to be razed for the project.

4. The property includes residential dwellings on Thames Street (a duplex and a single family) as well as 2 commercial buildings on Hope Street and 2 apartments over one of the commercial buildings. The uses of these buildings are proposed to remain as existing. The residential dwellings on Thames Street will be dedicated as off-site affordable housing units.
5. The project includes 11 parking spaces on the north side of the mill for the commercial uses, 137 parking spaces within the mill building and 151 parking spaces in the surface parking lot on the east side of Thames Street for the residential units. In the Waterfront Zoning District, the Zoning Ordinance allows for 50% of the required parking to be small car parking spaces. However, all of the parking spaces in the surface parking lot are proposed to be 9' wide versus the required 10' width. The interior parking spaces are varying in width due to the columns with the standard spaces 9' wide and the small car spaces 8' wide. The exterior surface parking lot will have double striped parking spaces; however, the interior parking spaces will not be double striped.
6. The subject Mill building property is located on the east side of Bristol Harbor adjacent to the Town's Maritime Center.
7. The property is also within the Downtown Historic District and subject to the jurisdiction of the Coastal Resources Management Council.
8. The project contains a segment of the Town's harbor walk that will extend from the Maritime Center on the north to the property of the Elk's Lodge on the South. The applicant has agreed to partner with the Town in the Town's negotiations to extend the walkway to Constitution Street.
9. The proposal for 127 units is consistent with the residential density of the zoning ordinance at 1 unit per 2,336 which is greater than the 2,250 square feet of gross floor area required; however, this density would still require an amendment to the original change of zone conditions by the Town Council.
10. Along with the density modification to the original change of zone from 98 units to 127 units, the applicant is requesting that the minimum commercial space be modified from the required 22,000 square feet of area to 6,300 square feet of area and that the standard parking spaces be 9' wide in lieu of the required 10' width and the interior parking lot small car spaces be 8' wide in lieu of the required 9' wide and none of the interior parking spaces to be double striped

III. Findings of Fact and Conclusions of Law

The Board approves this application for the following reasons:

1. The proposed development is consistent with the purpose and objectives of the Urban Rehab Land Development, as set forth in Section 28-284 "Land Development Projects – Urban Rehab Land Development Project" because it rehabilitates an under-utilized and inefficient historic structure and it rehabilitates the urban waterfront. It encourages a design that is

friendly to pedestrians, protects the existing built environment and character which imparts a sense of place to the community while allowing beneficial new uses and rejuvenation, safeguards the physical fabric of the community from neglect and decay and prevents incongruous re-development, provides an appropriate change of use that is compatible with the architecture of the buildings on site and the surrounding neighborhood, and it enhances public access to the waterfront.

2. The proposed development is consistent with the approved 2016 Comprehensive Plan which includes the following references:
 - a. In the Economic Development Element, Action Item #ED-C-3 states that the Town should continue to work with the owner of Robin Rug property to encourage and facilitate the completion of the development proposal for a mixed use development-residential and commercial.
 - b. Land Use Element – Section 3 references this property and the proposed conversion of the property into a mixed use development with residential uses on the upper floors with commercial and parking on the first floors. (Page 45)
 - c. Land Use Element – Section 3 states that the Urban Rehab and Waterfront PUD which is intended to encourage the rehabilitation of and reuse of deteriorated, underutilized, and inefficient historic and/or nonconforming structures of conservation concern to the Town
 - d. Future Land Use Map designates this property as “Waterfront PUD – waterfront mixed use commercial and residential”
 - e. The Housing Element references the proposed Adaptive Re-use of the Robin Rug and the proposed affordable housing with the statement “While these units have not yet been built, they were a condition of the zone change for the re-use of the mill.”
3. The proposal complies with the density requirements in the Zoning Ordinance of Section 28-284 (d)(2)).
4. The proposal complies with the conditions of the Town Council Zone Change of 2008 other than to the density and the amount of commercial space which will need further Town Council action to amend both the zone map and the ordinance text. The density of the proposed development is not a substantive increase in density, complies with the Zoning Ordinance, and is appropriate for the site.
5. The Town Council Zone Change of 2008 set the requirements for affordable housing to be provided with a minimum of 10% and a maximum of 20% of the units by either off-site, on-site, or fee-in-lieu. Twenty units (15% of 127 units) are to be for Low-Moderate Income Housing with three of the units to be in existing dwellings on Lot 49 (existing 2 family dwelling) and Lot 50 (existing 1 family dwelling) and the balance to be fee-in-lieu (17 x \$40,000 = \$680,000) to be paid to the Town and placed in an affordable housing trust fund.
6. The proposal provides more parking than required by the Zoning Ordinance. The required parking is 144 parking spaces and the proposal provides a total of 299 spaces.
7. The proposal provides a greater buffer between the surface parking area and the abutting residential properties than required by the Zoning Ordinance. The Zoning Ordinance requires

a minimum buffer width of 3' (Section 28-251 (10)), and the proposal provides buffers that range from 5 to 25' wide.

8. The proposal includes the dedication of the existing parking lot at the corner of Church and Thames to the Town of Bristol for public parking to partially satisfy the 10% requirement for Public/Institutional uses per Section 28-284(g). Additional public space is a public walkway from Hope Street and the public access easement along the waterfront where a walkway is proposed.
9. Off-site parking is permitted in accordance with Section 28-255 and will be deed restricted to be connected with the mill building. The off-site parking design, lighting, drainage, fencing buffering and landscaping details, will be reviewed by the Planning Board as part of the Major Land Development preliminary application. The requirement of this Section for a separate TRC review is deemed met by the Planning Board review.
10. The proposal is consistent with the provisions of the Zoning Ordinance, including Section 28-284 d (2). As stated in this section of the Zoning, the Planning Board may allow the provision of otherwise allowed nonresidential uses in alternate locations within the building in lieu of the required retail and restaurant uses on the first floor within 50' of the front lot line. The Planning Board may also reduce the gross floor area of such required nonresidential uses by 20 %. When the change of zone was granted in 2008, the Town Council required 22,000 square feet of commercial space. The current proposal is 6,300 square feet of commercial. The Planning Board may reduce the amount by 20%; however, this reduction is greater and will require an amendment to the 2008 Change of Zone conditions.
11. There will be no significant negative environmental impacts from the proposed development as shown on the plan with all required conditions for approval including requirements for permits from the Coastal Resources Management Council, FEMA compliance for flood proofing and flood mitigation, compliance with the review by the Water Pollution and Control Facility (see letter of 12/24/21), maintenance of the proposed drainage by the owner, and compliance with any requirements of the Phase 1 and Phase 2 Environmental Site Assessments.
12. Sufficient evidence was presented to satisfy the required criteria for Master Plan approval including a Phase 1 Environmental Site Assessment, agreement for compliance with the requirements of the Bristol Water Pollution Control Facility that all stormwater currently going into the Town sewer will be removed, engineered plans that provide drainage mitigation to demonstrate that there will be no significant negative impacts on the health and safety of current or future residents of the community, subject to the conditions.
13. The proposed development will not result in the creation of individual lots which such physical constraints to development that building on those lots according to pertinent regulations and building standards would be impracticable because no new lots are being created.

14. The proposed development will have adequate and permanent physical access to a public street (Thames Street).
15. Section 28-284 (d)(1) of the Zoning Ordinance is not applicable, since the section of the ordinance that controls this proposal is Section 28-284(d)(2).
16. The proposed development is consistent with each of the general purposes of Article 1 of the Bristol Subdivision and Development Review Regulations as follows:
 - a. It was processed in accordance with the process set forth in the Regulations which provide for the orderly, thorough, and expeditious review of land developments;
 - b. It promotes high quality and appropriate design and construction of land development projects because it is a redevelopment and rehabilitation of an existing historic waterfront mill such that the building is preserved and restored;
 - c. It promotes the protection of the existing natural and built environment and the mitigation of all significant negative impacts on the existing environment, with the conditions of approval, because it is a redevelopment of the historic waterfront mill; there will not be any new buildings; it includes water quality measures for the drainage; it eliminates existing infiltration into the sewer system; and it provides enhanced buffering and screening from the existing parking lot to the abutting properties;
 - d. It promotes a land development that is well integrated into the surrounding neighborhood with regard to natural and built features and it concentrates the development in areas which can best support intensive use because of natural characteristics and existing infrastructure because it is a re-development project in an existing mill building with improvements to an existing surface parking lot with existing utilities that have the capacity for the re-development;
 - e. It reflects the intent of the Bristol Comprehensive Plan with regard to the physical character of the various neighborhood and planning areas of the Town because it is a redevelopment of an existing mill building that actually establishes the existing character of the neighborhood area;
 - f. The proposal was reviewed by the Planning Board's Technical Review Committee and the various Town department heads and local agencies including the Bristol County Water Authority;
 - g. The proposal dedicates public land, impact mitigation, and payment in lieu thereof that is based on clear documentation of needs because it complies with the required set aside of public land area including walkways, a harbor walk, as identified in the Comprehensive Plan, and properties for public parking.
 - h. The project sites improvements to allow for maximum protection of critical landscapes and resources as they relate to Bristol's historic and cultural values because it is the re-development of an existing historic mill building in the National Register Historic District and the improvement of the existing surface parking lot rather than new "greenfield" development.
 - i. The project continues the Town's historic policy of providing public access to the water because it includes a public harbor walk along the Bristol Harbor shoreline

- along the west side of the mill property from the Bristol Maritime Center to the Bristol Elks Lodge;
- j. It was processed in accordance with the regulations which set forth the procedures for review and approval;
 - k. The project promotes sustainable development practices because it re-develops an existing historic mill, located in the downtown, provides pedestrian connectivity and also provides opportunities for alternative transportation use including public bus, bicycle and boat.
17. Subject to the following Conditions:
- A. Change of conditions of the 2008 Town Council zoning map approval and ordinance text including residential and commercial density uses as well as modification to the width of the parking spaces and the double striping on the interior parking spaces.
 - B. Dedication by deed of the parking lot on the northeast corner of Church and Thames Street, Plat 10, Lot 32 to the Town of Bristol for public parking as required by the zoning that there be 10% of the land area to be set aside for public institutional uses as required by Section 28-284 (g).
 - C. Connection of the public walkway to the Maritime Center.
 - D. A Traffic Study to include counts to be taken in the Summer months (June, July, August, or September), and be subject to the peer review of the Planning Board's consultant engineer, with the review fee to be reimbursed by the applicant in accordance with the regulations;
 - E. A revised Fiscal Impact Study subject to the peer review of a consultant selected by the Planning Board with review fee to be reimbursed by the applicant in accordance with the regulations;
 - F. Deed restrictions on both the mill building property and the surface parking lot across from the mill so that the parking is connected to the mill and cannot be separately conveyed;
 - G. Deed restrictions, running to the Town of Bristol, on the surface parking lot that no structures be built on this property;
 - H. Parking spaces in the surface parking lot to be double striped.
 - I. All services to the residential units will be private including recycling and garbage pick-up, snow plowing, driveway and drainage maintenance. This shall be a deed covenant in Land Evidence Records.

Motion by Boardmember Anthony Murgo, Seconded by Boardmember Armand Bilotti.

Voting in Favor: Boardmembers Squatrito, Murgo, Bilotti

Voting Against: Boardmember Katz and Alternate Boardmember Clark.

Motion passes.

Being a decision of the Bristol Planning Board on May 12, 2022.

By: Diane M. Williamson, Diane M. Williamson, Administrative Officer

Date: May 16, 2022



STATE OF RHODE ISLAND

HISTORICAL PRESERVATION & HERITAGE COMMISSION

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July 6, 2022

Via email: lturmer@crmc.ri.gov & jabbruzzese@crmc.ri.gov, CRMC

Raymond Coia, Chair
Coastal Resources Management Council
Oliver H. Stedman Government Center
4808 Tower Hill Road
Wakefield, Rhode Island 02879

Re: CRMC File Number 2022-05-126
Applicant: Brady Sullivan Properties
125 Thames Street
Bristol, Rhode Island

Dear Mr. Coia:

The Rhode Island Historical Preservation and Heritage Commission (RIHPHC) staff has reviewed the information that your office provided for the above-referenced application. Brady Sullivan Properties is proposing to improve the existing building and surrounding landscape at 125 Thames Street in Bristol.

The building, historically known as the Cranston Worsted Mills, is a contributing resource in the Bristol Waterfront Historic District, which is listed in the National Register of Historic Places. We understand that the applicant intends to pursue federal historic rehabilitation tax credits for the project, which will be reviewed by the RIHPHC for its compliance with the Secretary of the Interior's *Standards for the Treatment of Historic Properties*. Provided the project continues through the end of the tax credit process and the RIHPHC is afforded the opportunity to review plans for the project, no further consultation under *650-RICR-20-00-1.2.3 Areas of Historic and Archaeological Significance* is necessary. If the applicant/owner decides not to pursue historic tax credits, the RIHPHC must still review the project through CRMC.

These comments are provided in accordance with *650-RICR-20-00-1.2.3 Areas of Historic and Archaeological Significance* of the Coastal Resources Management Council. If you have any questions, please contact Elizabeth Totten, Project Review Coordinator, of this office.

Sincerely,

Elizabeth Totten
FOR

Jeffrey Emidy
Interim Executive Director
Interim State Historic Preservation Officer



Town of Bristol, Rhode Island

Department of Community Development

10 Court Street
Bristol, RI 02809
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TECHNICAL REVIEW COMMITTEE MEETING BRISTOL YARN MILL REDEVELOPMENT MASTER PLAN APPLICATION

The Technical Review Committee held a meeting for the Bristol Yarn Mill Redevelopment. The meeting was held on **December 22, 2021** at 10:00 a.m. on the 2nd floor of the Burnside Building 400 Hope Street.

Attending:

Diane Williamson - Administrative Officer
Edward Tanner – Zoning Officer
Armand Bilotti – Planning Board Duty Member
Ory Lima – Historic District Commission Chair
Greg Marsili – Bristol Harbor Master
Susan Rabideau – BCWA
Rob Ferguson – BFD
Jose DaSilva – BWPCF
Shealyn Davey – BWPCF
Bob Sykes – Pare Corporation
Andrew Teitz – Asst. Town Solicitor

John G. Rego – Applicant's Attorney
John McCoy – Applicant's Attorney
Shawn Martin – Fuss & O'Neill
Chris Reynold – Brady Sullivan - Developers

Diane Williamson reviewed application process and summarized process and TRC purpose.

Introductions from applicant's team. Reviewed summary of project. Proposed redevelopment of the Robin Rug complex into 151 residential apartment units and 6,000 s/f of commercial space. A decade plus ago the Town approved a plan for this property with 98 residential units. Applicant is going back to the Town Council to amend the previous zone change with conditions to fit the new design and permit the proposed increased density. Design features were summarized. Existing roof drains from building that discharge to sanitary sewers will be removed. Parking lot across street will be improved; new paving, landscaping & storm water treatment. CRMC review is required. A public access walkway is proposed along the harbor shoreline. Walkway will connect from the Town's Maritime Center south to the Elk's Club property.

Pare Corporation is the Town's consulting engineer for this application. Pare has submitted a two page memorandum with comments.

Harbor Master (Marsili) – No comments at this time.

Fire Dept. (Ferguson) – No comments now until plans are developed.

Bristol County Water Authority (BCWA) (Rabideau) – Discussed domestic and fire service needs for water. There are several water lines entering this property now. BCWA would like to see one new service line for entire building.

Water Pollution Control Dept. (DaSilva & Davey) – There are existing problems in this area with inflow and infiltration (I&I) into the Town's sanitary sewer lines. Will need a plan for disconnections. Best to have new sanitary sewer service lines for the property to eliminate any inflow and infiltration. Backflow preventers will also be required on the first floor. Peak sanitary flow rate calculations will be needed. Commercial uses may require grease traps and/or other pre-treatment.

The TRC and applicant discussed other issues that have been raised or otherwise need to be addressed as follows:

Base flood elevation – some units may be below this elevation. Town will need elevation certificates. Floors are at different elevations as portions of the mill buildings were built over time. Applicant says no residential units will be below base flood elevation. More info will be coming with design details.

Surface parking lot on east side of Thames – buffering from neighbors, lighting, space allocation. Applicant stated that each unit lessee will have a parking pass but spaces within surface parking area will be first come-first served, not assigned parking. Commercial space will have access during business hours. No gate will be installed for entrance-exit. Parking lot lighting was discussed. Security and safety lighting will be installed, but will not be lighting up the property too much. Shields and light cutoffs will be installed to direct light down. Lighting will be low level pedestrian scale. Existing vegetation buffer will be maintained and be enhanced. New shade trees will be planted within parking areas.

Dumpsters for the property proposed in parking lot area. One central location. TRC would like to re-visit the location and consider other options.

There are four buildings located within the parking lot properties east of Thames Street. Residential and commercial uses will remain as is.

The application for a zoning change includes re-zoning the parking lot and existing buildings into the Rehab Waterfront Planned Unit Development (WPUD) zoning district. This may not be necessary as it may complicate future use of existing structures on the east side of Thames Street. May require deed restrictions to keep uses as is.

A traffic study was conducted in 2007-2008 era as part of the previous redevelopment proposal. TRC will require a revised/new traffic study. This is a requirement for Preliminary Plan Application, not Master Plan, but Planning Board may want it sooner given the increased density that is proposed. May want to wait until Preliminary and warm weather to conduct the study.

There is no proposal to install any new utilities on the parking lot parcels. Existing utilities may be in conflict. Storm water BMP's may conflict with existing underground utilities. Locations will be investigated further.

Plan from environmental site assessment included with application shows property lot line through a portion of a neighbor's building. Survey plan shows most accurate lot line locations.

Existing driveway curb-cut at Hope St. will be maintained and improved with new pavement. Will be an 'in' only from Hope Street and will not be used as an exit. This driveway is only 11 feet wide, and could provide pedestrian access through the site. TRC would prefer that this driveway be used as a pedestrian access rather than vehicular. TRC will also want to see a pedestrian circulation plan and path through the parking lot.

Commercial space proposed within this development is less than what was proposed in the previous development plan. Commercial space is envisioned as more professional space, not as much retail, as there is not much of a street presence. Existing commercial building on Hope Street is appropriate for retail.

Parking lot design within the mill property on the west side of Thames Street was discussed. TRC discussed parking space configuration, aisle width and circulation concerns. Some dimensional variances will be required from the zoning board, mostly within the proposed parking area on the mill property, as many of the spaces would be within lower level of the building. There is also another outdoor surface lot proposed on the mill property. TRC engineer requested more information on the parking lot designs. Interior spaces beneath the building will be assigned to individual residents so more familiarity for users. Applicant will provide more details. Exterior parking will all be impervious.

TRC discussed density that was approved by zone change in 2008. At that time the Town approved approximately 100 units, with the final plan designed for 98. The density was calculated based upon gross floor area (G.F.A.) of the mill buildings, not on the size of the lots. This calculation is used to determine the number of units in the building, but not the size of individual units. The existing building is several hundred thousand square feet – many times larger than the lot. Going forward the TRC will want to see that infrastructure can support the number of units proposed. That is the most important aspect to consider (water, sewer, parking, etc.). Any approval of the zone change application that will be presented to the Town Council will dictate the number of units and overall density.

Electrical transformer located in parking lot was discussed.

Proposed storm water management in parking lot on east side of Thames Street was discussed. Stormwater will be collected, treated and discharged into a new drain pipe system to be installed in Thames Street to a new outfall at the end of Constitution Street into Bristol Harbor. Stormwater management will focus on water quality treatment. Not much volume will be infiltrated, but a soil survey investigation will be conducted. Sand filter beds will be used to filter stormwater.

Roof drain storm water runoff from the mill buildings will go to the west side of the mill lot to bio-retention areas and may also need to be piped to Bristol Harbor. Design for roof drains is not yet complete. Storm water will also need to be mitigated to avoid conflict with fire department access on west side of the building.

The applicant has no proposal for “Green Roofs” or solar panels on roofs of the buildings.

North side of mill lot survey shows the property line located within the access driveway to the Town’s Maritime Center and marina. Town will need an access easement to formalize existing public access to Town facilities.

TRC discussed public access corridor and walkway along the waterfront. There is a potential route shown on the plans, but will need more details where public walkway connects to Town property at the Maritime Center and also at the southern end near the Elks property. An existing fire access easement across the Elks property may be an opportunity to negotiate public access from Constitution Street. Applicant will work with the Elks on public access issues. The applicant will submit additional details to help TRC visualize connections along with new marina.

Proposed dog walk/run area shown on plans near the pedestrian waterfront walkway may be perceived as a conflict. Pet amenities are something that the applicant needs for building residents but the design is not complete yet.

Will need an agreement (similar to existing Stone Harbour Agreement) to detail access and maintenance of the public boardwalk. No gates are proposed on the boardwalk.

The applicant’s fiscal impact study was discussed. Questions on how the numbers were derived. Applicant needs to explain calculations to the Town. Existing report shows negative impacts to the Town from this development.

Coastal Resources Management Council (CRMC) permits will be required, and the applicant will need to evaluate impacts from sea level rise and incorporate adaptation strategies into the design. CRMC process is public, an Assent will be required. Public comments will be accepted. State Historic Preservation Office will review the application also during CRMC review process.

Applicant stated that there are no modifications to the exterior of existing mill buildings proposed. The only demolition proposed would be to the existing block building located at north end of property near the Maritime Center. Exterior of the mill buildings will be restored to historical style. ADA access will need to blend in as best they can to meet access requirements. Applicant may also propose some awnings and the like over doors. Existing loading doors will remain.

Affordable Housing requirements were discussed. The applicant is proposing a payment to the Town in lieu of building affordable housing units for the 10% of units proposed. This payment calculation per the 2008 agreement with original zone change. Formulas for determining amount of fee in lieu for affordable housing are different now. TRC noted that this development is not vested to the old agreement if they are building more than the 98 units previously approved. Bristol’s current zoning ordinance does not allow payment in lieu for affordable housing. This property may have vested rights to a payment in lieu if developing 98 units, but since 151 units are now proposed this payment may not be vested, and units would need to be dedicated affordable either within the property or off-site. It was noted that the property contains several existing residential structures on the easterly side of Thames Street that could be made available for affordable housing. This is a policy issue that needs to be worked out with Planning Board and Town Council.

The property will be developed with the benefit of State Historic Preservation Tax Credits, so State Historic Preservation Office will review project and may be more stringent than the local Bristol Historic District Commission (HDC) requirements.

Next step for this proposal is to the Planning Board when applicant is ready. TRC is ok with that along with the concurrent application to the Town Council for a zoning change. Planning Board Master Plan decision will have conditions and recommendations to the Town Council on the zone change application. Next Planning Board meeting is January 13, 2022. There will be public notice for the Planning Board public hearing, including advertising, posting requirements, and signage on the property.

The Planning Board application should include a written response to comments from the TRC for Planning Board to review.

Motion to send to Planning Board:

- 1st Ory Lima
- 2nd Armand Bilotti
- All In Favor

Meeting adjourned at 11:32 am

Notes by Ed Tanner



Property Deeds, Title Commitment, and Owner Affidavit Letter

QUIT-CLAIM DEED

#51776

BRISTOL DEVELOPMENT CO. (a Rhode Island Corporation), for consideration paid, grants to RUSS-RUSS REALTY CO. (a Rhode Island Corporation), with QUIT-CLAIM COVENANTS

All those certain parcels of land, with all the buildings and improvements thereon, on the easterly and westerly sides and at the southerly end of Thames Street, on the southerly side of John Street, and on the northerly sides of Church and Constitution Streets, in the Town and County of Bristol and State of Rhode Island, more fully described as follows:

PARCEL 1: That certain lot or parcel of land with all the buildings and improvements thereon, situated on the westerly side of Thames Street and on the northerly line of Constitution Street, bounded and described as follows: Beginning at a point in the westerly line of Thames Street at its intersection with the northerly line of Constitution Street as now used and occupied as a public highway, said point of beginning being eighteen and 27/100 (18.27) feet southerly, measured along said westerly line of Thames Street from the intersection of said westerly line of Thames Street with the Schubarth line, so-called, and said point of beginning being the southeasterly corner hereof; thence westerly bounding southerly on said Church Street as now used and occupied as a public highway one hundred forty- nine and 25/100 (149.25) feet, more or less, to land now or lately of the Bristol County Lodge #1860 Order of Elks; thence northerly bounding westerly on said last named land eighty-two and 6/10 (82.6) feet to the northeasterly corner of said last named land; thence westerly bounding southerly on said last named land to the Harbor line; thence northerly on said Harbor line to land now or lately of the Town of Bristol, being the parcel of land conveyed by Cranston Worsted Mills to the State of Rhode Island and Providence Plantations by deed dated June 29, 1894 and recorded in Book 54 at page 1; thence easterly bounding northerly on said last named land about eighteen and 5/10 (18.5) feet to the southeasterly corner thereof; thence northerly bounding westerly on said last named land eighty-five (85) feet, more or less, to the southerly line of the Long Wharf or Long Wharf Estate; thence easterly bounding northerly on said last named land two hundred one and 5/10 (201.5) feet to said Thames Street; thence southerly bounding easterly on said Thames Street four hundred eighty-five (485) feet, more or less, to said northerly line of Constitution Street as now used and occupied as a public highway at the point of beginning; together with all right, title, and interest of the grantor in and to the Long Wharf and the Long Wharf Estate, so-called.

-2-

However otherwise bounded and described, meaning to convey and hereby conveying the same premises described as Parcels I to V inclusive in the deed from Collins & Aikman Corporation to this grantor dated February 10, 1956 and recorded on February 15, 1956 in Book 128 at page 291.

PARCEL II: That certain lot or parcel of land with all the buildings and improvements thereon, situated in said Town of Bristol, County of Bristol, and State of Rhode Island, on the easterly side of Thames Street, bounded and described as follows:

Beginning at a point on the easterly side of Thames Street distant one hundred seventeen and 58/100 (117.58) feet, more or less, from the southeasterly corner of Thames Street and Church Street, said point of beginning being the southwesterly corner of land now or lately of James D. Rielly et ux; thence easterly along a fence and along the southerly boundary of said Rielly land, along the southerly boundary of land now or lately of Theresa Bramwell, and along the southerly boundary of land now or lately of Mary P. Brogan et al to land now or lately of Alfred J. Shepherd one hundred seventy-two and 96/100 (172.96) feet, more or less; thence southerly, along the westerly boundary of said Shepherd land and the westerly boundary of land now or lately of Ralph F. Kinder et al to land now or lately of John B. Zufelt et ux one hundred thirty-nine and 5/10 (139.5) feet, more or less; thence westerly along the northerly boundary of said Zufelt land one hundred seventy-two (172) feet, more or less, to the easterly side of Thames Street; thence northerly along Thames Street one hundred forty-seven (147) feet, more or less, to the point or place of beginning.

Said parcel is set forth as Parcel VI in the deed referred to in Parcel I hereof.

PARCEL III: That certain lot or parcel of land with all the buildings and improvements thereon, if any, situated in the said Town of Bristol and State of Rhode Island, bounding southerly on Church Street on which it measures about one hundred (100) feet and holding that width extending back therefrom about eighty-seven (87) feet to and bounding northerly on land now or lately of Residential Rooms, Inc., bounding easterly on land now or lately of John T. Carpenter et ux in part and in part on land now or lately of Geoffrey B. Davis et al, and westerly on Thames Street.

Said parcel is set forth as Parcel VII in the deed referred to in Parcel I hereof.

PARCEL IV: That certain lot or parcel of land with all the buildings and improvements thereon, situated at the southeasterly corner of Thames Street and John Street, in the Town of Bristol and State of Rhode Island, bounded and described as follows:

Beginning at the southeasterly corner of Thames Street and John Street; thence easterly bounding northerly on John Street about one hundred two (102) feet to land now or lately of Alfred R. Rego et al; thence southerly bounding easterly on said Rego land to land now or lately of Residential Rooms, Inc.; thence westerly bounding southerly on said last named land to Thames Street; thence northerly bounding westerly on Thames Street to John Street at the point and place of beginning.

Being the same premises described in two deeds to this grantor, both recorded on March 31, 1966, one from Milton L. Bassing and wife in Book 162 at page 32, and the other from Rudolph M. Bassing and wife in Book 162 at page 34.

PARCEL V: That certain lot or parcel of land with all the buildings and improvements thereon, situated on the easterly side and at the southerly end of Thames Street, bounded and described as follows:

Beginning at a point in the easterly line of Thames Street at the southwesterly corner of land now or lately of O. Reid LeClair et ux et al, said point of beginning being the northwesterly corner hereof; thence easterly bounding northerly on said LeClair land in part and in part on land now or lately of Hezekiah W. Church et al, in all about eighty-six (86) feet to other land now or lately of Hezekiah W. Church; thence southerly bounding easterly on said Church land about seventy (70) feet to land now or lately of Andrew DelToro; thence southwesterly bounding southeasterly on said DelToro land one hundred twenty-eight and 33/100 (128.33) feet to a drill hole at the shore; thence turning an interior angle of 112° 39' and running northwesterly bounding southeasterly by the shore in part and in part by land now or lately of the United States of America eleven and 06/100 (11.06) feet to the southwesterly corner of Thames Street; thence turning an interior angle of 62° 37' and running easterly bounding northerly on the southerly end of said Thames Street about forty-seven (47) feet to the southeasterly end of said Thames Street; thence northerly bounding westerly on said Thames Street seventy (70) feet to said LeClair land at the point of beginning.

-4-

Together with the perpetual right to use in common with others, for pleasure purposes, the seashore which lies westerly of said premises, below high water mark.

Meaning and intending to convey and hereby conveying the same premises conveyed to this grantor by deed from James D. Rielly and wife dated and recorded on May 13, 1960 in Book 137 at page 361.

This conveyance is executed subject to any restrictions, rights, easements, reservations, limitations, provisions, exceptions and encumbrances etc. set forth in title deeds.

WITNESS this 16th day of OCTOBER, 1975, said corporation has caused these Presents to be executed and its corporate seal to be hereunto affixed by its Officer(s) duly authorized.

BRISTOL DEVELOPMENT CO. S.S. 51

BY Irving Solomon

BY _____

STATE OF RHODE ISLAND
COUNTY OF PROVIDENCE

In Providence on the 16th day of October, 1975 before me personally appeared Irving Solomon, President
and
of BRISTOL DEVELOPMENT CO. to me known and known by me to be the party executing the foregoing instrument for and on behalf of said corporation, and he acknowledged said instrument, by him executed, to be his free act and deed in his said capacity and the free act and deed of said corporation.

James H. [Signature]
Notary Public

Recorded OCT 23 1975 at 10:35 A.M.
Arland Bickens Town Clerk



00036

003711

QUITCLAIM DEED

Ralph F. Kinder of the Town of Bristol, County of Bristol and State of Rhode Island, and Fleet National Bank, a national banking institution, in its capacity as trustee under the Joseph C. Kinder Marital Trust, for consideration paid, grant to Karlan Realty Company a Rhode Island corporation with its principal offices located in the Town of Bristol, County of Bristol and State of Rhode Island, with QUIT-CLAIM COVENANTS:

That certain tract or parcel of land, together with all buildings and improvements thereon, located westerly of Hope Street, in the Town of Bristol, County of Bristol, State of Rhode Island, being bounded and described in Exhibit A attached hereto:

Subject to taxes assessed December 31, 1989, and all easements and rights of way of record.

WITNESS our hands this 7th day of Sept., 1990.

Ralph F. Kinder
Ralph F. Kinder

Fleet National Bank, in its capacity as trustee under the Joseph C. Kinder Marital Trust

By [Signature]

By Sharon M. Clark T.C.

STATE OF RHODE ISLAND
COUNTY OF BRISTOL

In Bristol, on the 7th day of Dec., 1990, before me personally appeared Ralph F. Kinder, to me known and known by me to be the party executing the foregoing instrument, and he acknowledged said instrument by him executed to be his free act and deed.

Joseph G. Kinder
Notary Public
My Commission Expires: 6/30/91

Joseph G. Kinder

STATE OF RHODE ISLAND
COUNTY OF BRISTOL

In PROV Bristol, on the 6TH day of Dec, 1990, before me personally appeared Laura M. Fiske, ASSIST. VICE PRES of Fleet

448.00
12-10-1990
17008

000809

NEW HAMPSHIRE TAX

National Bank, to me known and known by me to be the party executing the foregoing instrument and she acknowledged said instrument by her executed to be her free act and deed and the free act and deed of said corporation.

Thomas E. Johnston
Notary Public

My Commission Expires: 6/30/9

STATE OF RHODE ISLAND
COUNTY OF BRISTOL PROV

In PROV Bristol, on the 6TH day of Dec, 1990, before me personally appeared Spitta M. Moore Trust Officer of Fleet National Bank, to me known and known by me to be the party executing the foregoing instrument and she acknowledged said instrument by her executed to be her free act and deed and the free act and deed of said corporation.

Thomas E. Johnston
Notary Public

My Commission Expires: 6/30/9

EXHIBIT A

Beginning at a point in the westerly street line of Hope Street, said point being the most southeasterly corner of land now or formerly of Alfred Brazil, Jr.;

thence running in a southerly direction, bounding on the aforementioned westerly street line of Hope Street, a distance of twenty-seven and 44/100 (27.44) feet to a corner, said corner being the most northeasterly corner of land now or formerly of Emanuel and Harriet K. Iacoletti;

thence turning an interior angle of 91°-18'-00" and running in a westerly direction, bounding on the aforementioned Iacoletti land, a distance of sixty-five and 73/100 (65.73) feet to a point;

thence turning an interior angle of 178°-33'-00" and continuing in a westerly direction, bounding on the aforementioned Iacoletti land, a distance of thirty-four and 40/100 (34.40) feet to a corner, said corner being the most northwesterly corner of said Iacoletti land;

thence turning an interior angle of 271°-20'-00" and running in a southerly direction, bounding on the aforementioned Iacoletti land and land now or formerly of Manuel Furtado, Jr., a distance of one hundred twenty-four and 88/100 (124.88) feet to a corner, said corner being the most southeasterly corner of the herein described parcel;

thence turning an interior angle of 87°-45'-30" and running in a westerly direction, bounding, in part, on land now or formerly of Stephen M. and Joyce A. Grabert and in part on land now or formerly of Harold W. and Frances S. Demopoulos, a distance of seventy-six and 12/100 (76.12) feet to a corner, said corner being the most southwesterly corner of the herein described parcel;

thence turning an interior angle of 91°-21'-00" and running in a northerly direction, bounding on land now or formerly of Russell Karian, land now or formerly of John B. and

Sadie Zufelt, and land now or formerly of Russ-Russ Realty Co., a distance of two hundred eleven and 03/100 (211.03) feet to a corner, said corner being the most northwesterly corner of the herein described parcel;

thence turning an interior angle of 90°-20'-40" and running in an easterly direction, bounding on land now or formerly of Alfred J. Shepherd, a distance of one hundred fifteen and 64/100 (115.64) feet to a corner, said corner being the most northeasterly corner of the herein described parcel;

thence turning an interior angle of $89^{\circ}-25'-50''$ and running in a southerly direction, bounding on the previously mentioned Brazil land, a distance of sixty-one and $39/100$ (61.39) feet to a corner;

thence turning an interior angle of $271^{\circ}-18'-00''$ and running in an easterly direction, bounding on the aforementioned Brazil land, a distance of fifty-nine and $04/100$ (59.04) feet to the point and place of beginning.

The last course making an angle of $88^{\circ}-38'-00''$ with the first herein described course.

Containing, by calculation, 21,132 square feet of land.

Subject to and benefitting from a right-of-way of record.

DEC 10 1990

Recorded 10:23 at A. M.

Arline C. Fredericks Town Clerk

May 26, 2010

RECIPROCAL EASEMENT AGREEMENT

This Reciprocal Easement Agreement ("Easement") is being entered into on this 11th day of AUGUST, 2010 by and between Robin Industries, Inc., a Rhode Island corporation, with a business address of 125 Thames Street, Bristol, Rhode Island 02809 ("Robin Rug") and BRISTOL COUNTY LODGE #1860 of the BENEVOLENT and PROTECTIVE ORDER OF ELKS of the UNITED STATES OF AMERICA, a Rhode Island non-profit corporation, with a business address of 1 Constitution Street, Bristol, Rhode Island 02809 (the "Elks").

P R E A M B L E:

WHEREAS, Robin Rug is the owner of that certain lot or parcel of land commonly known as 125 Thames Street, Town of Bristol, State of Rhode Island 02809 and identified on the Town's Assessors' Maps as Plat 10, Lots 42, 60, 61, 62 and 73 (the "Robin Rug Property"), and reflected on Exhibit A attached hereto and made a part hereof (the "Plan");

WHEREAS, the Elks are the owners of that certain lot or parcel of land commonly known as 1 Constitution Street, Town of Bristol, State of Rhode Island 02809 and identified on the Town's Assessor's Maps as Plat 10, Lot 63 (the "Elks Property") and reflected on the Plan;

WHEREAS, Robin Rug and the Elks desire to set forth the terms and conditions pursuant to which the Elks shall allow pedestrian and vehicular access across a portion of the Elks Property, identified as the "Construction/Maintenance Easement" area on the Plan for the purpose of construction, and after project completion, external maintenance, repair and remodeling, to the rear of any structures on the Robin Rug Property, pursuant to the terms and conditions contained herein;

WHEREAS, the Elks desire to set forth the terms and conditions pursuant to which the Elks shall allow pedestrian and vehicular access to the Town of Bristol and to Robin Rug across the area identified on the Plan as "20' Wide Fire Lane Access Easement" for the purpose of allowing emergency personnel and vehicles ingress and egress to and from the rear of the Robin Rug Property during emergency situations and for the purpose of allowing Robin Rug and its successors and assigns access to the rear of the Robin Rug Property;

WHEREAS, the Elks and Robin Rug desire to set forth the terms and conditions, pursuant to which Robin Rug shall allow vehicular access and parking to the Elks on a portion of the Robin Rug Property identified on the Plan as the "Parking Easement" for the purpose of parking registered passenger vehicles pursuant to the terms and conditions contained herein;

WHEREAS, the 20' Wide Fire Lane Access Easement area, the Construction/Maintenance Easement area, and the Parking Easement area hereinafter sometimes referred to individually as an "Easement Area," and collectively sometimes referred to as the "Easement Areas"; and

NOW, THEREFORE, for good and adequate consideration between and among the parties, the receipt and sufficiency of which are hereby acknowledged, the undersigned hereby declare and encumber the Easement Areas with the easements set forth herein in perpetuity as follows:

1. Easements.

(a) ***The Fire Access Easement.*** The Elks hereby grant, convey and declare to the Town of Bristol for **public safety purposes only** and Robin Rug, and their successors and assigns, for their use and the use of their customers, patrons, invitees, licensees, vendors, maintenance personnel, tenants, subtenants, contractors and employees (collectively, the "Robin Rug Permitted Parties"), the non-exclusive, perpetual right, license and easement over through and across the 20' Wide Fire Lane Access Easement Area for the purpose of allowing emergency personnel and vehicles ingress and egress to and from the rear of the Robin Rug Property during emergency situations, and generally to allow Robin Rug Permitted Parties access to the Robin Rug Property. The Robin Rug Permitted Parties **shall not include** members of the general public, and the **Fire Access Easement shall not be deemed a public right of way.**

(b) ***The Construction and Maintenance Easement.*** The Elks hereby grant, convey and declare to the Robin Rug Permitted parties, the non-exclusive, perpetual right, license and easement over through, across and on the Construction/Maintenance Easement Area for pedestrian and vehicular access, staging, construction materials, workmen, equipment and all related development materials during periods of construction, and after project completion, for periodic maintenance to the side, roof, structure and windows of buildings or structures located on the Robin Rug Property. Robin Rug shall have the right to cut and remove the tree located on the Construction/Maintenance Easement Area bordering the Robin Rug Property upon the execution of this Easement.

The Construction/Maintenance Easement Area shall be subject to the following reservations as well as all other provisions set forth herein:

- (i) Between Memorial Day and Columbus Day of each year, Robin Rug Permitted parties shall be allowed access to the Construction/Maintenance Easement Area for the purposes defined herein only on Mondays through Fridays between the hours of 7:00 a.m. and 4:00 p.m., and unless otherwise mutually agreed, any and all construction equipment shall be removed from the Construction/Maintenance Easement Area at the conclusion of each workday **so as not to interfere in any way with Elk's parking** in that area. During other periods of the year, Robin Rug Permitted Parties shall be allowed access to the Construction/Maintenance Easement during all reasonable days and hours.

(ii) During all times of the year, Robin Rug Permitted Parties shall provide the Elks with forty-eight (72) hour notice of its intention to utilize the Construction/Maintenance Easement Area.

(iii) In the event the Elks have a function scheduled at their facility during the noticed maintenance period, **Robin Rug shall not interfere with such function.**

(c) **Parking Easement.** Robin Rug hereby grants, conveys and declares to the Elks, for the use only by its members, guests, club house patrons and employees (the "Elks Permitted Parties"), the non-exclusive (to the extend Robin Rug Permitted Parties have access rights hereunder), perpetual right, license and easement over and on the Parking Easement Area for pedestrian and vehicular access and parking of registered passenger vehicles. Trucks (except for normal deliveries), trailers, boats, campers and other commercial vehicles in excess of 14,000 pounds of gross vehicle weight shall be expressly prohibited from the Parking Easement Area.

The Parking Easement Area shall be subject to the following reservations as well as all other provisions set forth herein:

(i) Robin Rug shall asphalt pave the Parking Easement Area (provided it is legally permissible), at its' expense, at the completion of the residential project planned by Robin Rug on its' Property. Alternatively, the Elks may pave the Parking Easement Area at their cost and expense at any time. In the event asphalt pavement is not allowed by State and Local authorities Robin Rug will install stamped pavers, permeable pavers, crushed stone or gravel at its' expense.

(ii) The Elks permitted parties shall utilize the Parking Easement Area for member, guest and patron parking only during operating hours of the Elks Property and not for overnight, commercial or long-term parking.

(iii) Nothing herein shall prevent Robin Rug from landscaping, fencing or otherwise screening the Robin Rug Property from the Parking Easement Area; provided such landscaping, fencing or screening does not materially interfere with the Elks' use of such Easement.

(e) Reservations, Conditions and Restrictions. The easements, rights and licenses granted hereunder are subject to the following reservations, conditions and restrictions:

(i) Except as expressly otherwise provided, no fence or obstruction or change of grade or other barrier shall be erected or permitted, nor shall any act be committed which would prevent, obstruct or interfere with the Robin Rug Permitted parties', Elks Permitted parties' or Town's (collectively, the "Permitted parties") use of the Easement Areas as set forth herein without prior written approval of both Robin Rug and Elks.

(ii) The Elks shall relocate their dumpster to a location west of the Fire Access Easement. Robin Rug at its' expense shall provide an enclosure for the relocated dumpster and shrubbery, (fencing materials and shrubbery to be chosen by the Elks.)

(iii) A party exercising its rights hereunder for its behalf or on the behalf of any of the Permitted parties shall assure that no activities occur in the Easement Areas which results in obnoxious odors, loud noises or other nuisances; provided, however, that outdoor charitable events of the kind and nature historically engaged in by the Elks upon its property shall not be prohibited hereby.

(iv) The parties hereto agree to negotiate in good faith the expansion, adjustment, limitation, amendment or termination of the Easements set forth herein in order to continue to fulfill and achieve the mutually agreed upon goals and objectives of the parties as reflected herein.

2. **Maintenance of Easement Areas.** The Elks agree to maintain and repair at their expense the *Parking Easement Area* in a good and commercially reasonable manner, such maintenance and repair including, without limitation, maintaining paving, curbing and lighting of the *Parking Easement Area* (provided, however, that the installation of any lighting in the *Parking Easement Area* shall first be approved by Robin Rug); restriping and maintaining the surface in level and evenly covered condition of the type of surfacing generally installed and maintained for commercial use; removing all ice, snow and other accumulations, mud and sand, debris and refuse and thoroughly sweeping the *Parking Easement Area* to the extent reasonably necessary to keep the *Parking Easement Area* clean and in orderly condition to allow the exercise of the rights granted hereunder. Robin Rug agrees to maintain and repair at their expense the *Fire Access Easement Area* in a good and commercially reasonable manner, such maintenance and repair including, without limitation, maintaining paving, curbing and lighting of the *Fire Access Easement Area*; restriping and maintaining the surface in level and evenly covered condition of the type of surfacing generally installed and maintained for commercial use to allow the exercise of the rights granted hereunder; provided, however, that any damage caused to any of the *Fire Access Easement Areas* as a result of the Elk's use and enjoyment of such *Fire Access Easement Area* shall be repaired at the Elks' expense.

3. **Insurance.** Each of the parties on behalf of themselves and their respective successors and assigns, agrees to maintain with respect to the Easement Areas owned by such party, a policy or policies of public liability insurance in amounts reasonably commercially acceptable and appropriate for properties in similar locations and used for similar purposes, such insurance to be in such amounts and with respect to injuries to any one person, injuries suffered in any one accident, or damage to property, or any combined single limit policies as deemed necessary, desirable and appropriate by standard customary practices in the State of Rhode Island.

4. Taxes and Assessments. Each of the parties agrees to pay, as and when due, any and all municipal, state or federal real property taxes and assessments, charges, fees and liens, if any, due and owing with respect to the Easement Area owned by them.

5. Not a Public Dedication. Except for the purposes of public safety and waterfront access, nothing herein shall be deemed to be a gift or dedication of any portion of the Easement Areas, Plan or any land therein to the general public or for any other public use or purpose whatsoever. Except as herein specifically provided, no right, privileges or immunities of any party hereto shall inure to the benefit of any third party nor should any third party be deemed to be a beneficiary of any of the provisions contained herein except as otherwise specifically stated herein to the contrary.

6. Covenants Running With the Land. Each of the foregoing provisions, terms and obligations shall constitute covenants running with the land, and each such provision, term or obligation shall run in perpetuity; and any violation or breach of any such provisions, terms, obligations or covenants may be enjoined, abated or remedied by appropriate proceedings at the instance of either party or their successors or assigns and the costs of such proceedings, together with damages and attorney's fees, all as fixed by the court, shall be borne and paid by the party or parties causing or permitting the continuance of any such breach or violation.

7. Mutual Indemnity. Each party (the "Indemnifying party") shall indemnify and hold the other party, its officers, directors, employees, agents, tenants, subtenants, successors and assigns (collectively the "Indemnified parties"), harmless from and against any losses, costs, damages, liabilities or expenses (including, without limitation, reasonable attorney's fees) suffered or incurred by any of the Indemnified parties caused by, as a result of or arising out of the gross negligence or willful conduct of the Indemnifying party or any of the Indemnifying party's Permitted parties in connection with the exercise of a party's rights under this Easement or the performance or non-performance of a party's obligations hereunder.

8. Arbitration. Any controversy or claim arising out of or relating to this Easement shall be settled by arbitration in accordance with the following provisions:

(a) Disputes Covered. The agreement of the parties to arbitrate covers all disputes of every kind relating to or arising out of this Easement. Disputes include actions for breach of contract with respect to this Easement, as well as any claim based upon tort or any other causes of action relating to the Easement. In addition, the arbitrators selected according to procedures set forth below shall determine the arbitrability of any matter brought to them, and their decision shall be final and binding on the parties.

(b) Forum. The forum for the arbitration shall be Providence, Rhode Island.

(c) Selection. There shall be three arbitrators, unless the parties are able to agree on a single arbitrator. In the absence of such agreement within ten (10) days after the initiation of an arbitration proceeding, Robin Rug shall select one arbitrator and the Elks shall select one arbitrator, and those two arbitrators shall then select, within ten (10) days, a third arbitrator. If those two arbitrators are unable to select a third arbitrator within such ten (10)-day period, a third arbitrator shall be appointed by the commercial panel of the American Arbitration Association. The decision in writing of at least two of the three arbitrators shall be final and binding upon the parties.

(d) Administration. The arbitration shall be administered by the American Arbitration Association.

(e) Rules. The rules of arbitration shall be the Commercial Arbitration Rules of the American Arbitration Association, as modified by any other instructions that the parties may agree upon at the time, except that each party shall have the right to conduct discovery in any manner and to the extent authorized by the Federal Rules of Civil Procedure as interpreted by the federal courts. If there is any conflict between those rules and the provisions of this section, the provisions of this section shall prevail.

(f) Substantive Law. The arbitrators shall be bound by and shall strictly enforce the terms of this Easement and may not limit, expand or otherwise modify its terms. The arbitrators shall make a good faith effort to apply substantive applicable law, but an arbitration decision shall not be subject to review because of errors of law. The arbitrators shall be bound to honor claims of privilege or work-product doctrine recognized at law, but the arbitrators shall have the discretion to determine whether any such claim of privilege or work product doctrine applies.

(g) Decision. The arbitrators' decision shall provide a reasoned basis for the resolution of each dispute and for any award. The arbitrators shall not have power to award damages in connection with any dispute in excess of actual compensatory damages and shall not multiply actual damages or award consequential or punitive damages.

(h) Remedies; Award. The arbitrators shall have power and authority to award any remedy or judgment that could be awarded by a court of law in Rhode Island. The award rendered by arbitration shall be final and binding upon the parties, and judgment upon the award may be entered in any court of competent jurisdiction in the United States.

9. Default. Any party failing to fulfill its obligations pursuant to the terms and conditions herein shall be in default of such obligations if it fails to cure such failure within thirty (30) days after receiving notice from the other party thereof, and if such failure is not cured within such period of time, then, in addition to any other rights or remedies available to the non-defaulting party at law or in equity, the non-defaulting party may perform or correct such default on the defaulting party's behalf and be entitled to reimbursement for such costs and expenses, including costs of arbitration, if any, and attorneys' fees, incurred in order to so perform or correct such default. The parties agree that certain instances of default shall not be adequately remedied at law, and the non defaulting party shall be entitled to equitable remedies including injunctive relief.

10. Binding Effect. The terms of this Easement and all rights and easements granted herein shall constitute covenants running with and be appurtenant to and run with the land effected thereby. All terms, rights and easements herein shall inure to the benefit of and be binding upon the parties, their respective successors and assigns.

11. Waiver. The failure of any party hereto at any time or times hereafter to exercise any right, power, privilege or remedy hereunder or to require strict performance by the other or another party of any of the provisions, terms or conditions contained in this Easement shall not waive, affect, or diminish any right, power, privilege or remedy of such party at any time or times thereafter to demand strict performance thereof; and no rights of any party hereto shall be deemed to have been waived by any act or knowledge of such party, or any of its Permitted parties, unless such waiver is contained in an instrument in writing, signed by such party. No waiver by any party hereto of any of its rights on any one occasion shall operate as a waiver of any of its other rights or any of its rights on a future occasion.

12. Entire Understanding. This Easement sets forth the entire agreement and understanding between the parties with respect to the subject matter hereof and merges any and all discussions, negotiations, letters of intent or agreements in principle between them. Neither of the parties shall be bound by any conditions or understandings with respect to such subject matter other than as expressly provided herein, or as duly set forth on or subsequent to the date hereof in writing and signed by a duly authorized officer of the party to be bound thereby.


13. Governing Law. This Easement is and shall be deemed to be a contract entered into and made pursuant to the laws of the State of Rhode Island and shall in all respects be governed, construed, applied and enforced in accordance with the laws of said State, without reference to its conflict of laws principles.

14. Good Will. This agreement is intended to facilitate both parties endeavors to be "good neighbors" and to beautify their properties and the Bristol Water front area. The community recognizes the Elks Lodge efforts therefore as part of this agreement Robin Rug volunteers to contribute the monthly rental compensation from the signing of this agreement back to September 1,2009 to the Elks Lodge to be used for charitable purposes which they deem appropriate.

[Signatures Appear on the Following Pages]

IN WITNESS WHEREOF, the parties have executed this Easement, as of the date first written above.

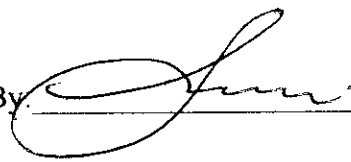
ROBIN INDUSTRIES, INC.

By: 
Name: RUSSELL KARIAN
Title: PRESIDENT

BRISTOL COUNTY LODGE #1860 of the
BENEVOLENT and PROTECTIVE

ORDER
AMERICA

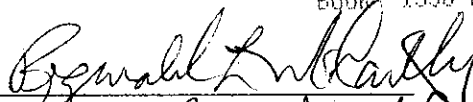
OF ELKS of the UNITED STATES OF

By: 
Name: Charles Francis
Title: Trustee Chairman

David P. Ciolek emulated Ruler.
David P. Ciolek PRESIDENT

STATE OF RHODE ISLAND
COUNTY OF BRISTOL

In said County and State on the 11TH day of AUGUST, 2010 before me personally appeared Charles Francis, David Ciolek, RUSSELL KARIAN of Robin Industries, Inc., to me known and known by me to be the party executing the foregoing instrument and he acknowledged said instrument by him so executed to be his free act and deed and the free act and deed of Robin Industries, Inc.


Notary Public: Reginald L. McCarthy
Commission Expires on: 7/24/14

STATE OF RHODE ISLAND
COUNTY OF BRISTOL

In said County and State on the 11th day of AUGUST, 2010 before me personally appeared Charles Francis, DAVID CIOK of BRISTOL COUNTY LODGE #1860 of the BENEVOLENT and PROTECTIVE ORDER OF ELKS of the UNITED STATES OF AMERICA, to me known and known by me to be the party executing the foregoing instrument and he acknowledged said instrument by him so executed to be his free act and deed and the free act and deed of BRISTOL COUNTY LODGE #1860 of the BENEVOLENT and PROTECTIVE ORDER OF ELKS of the UNITED STATES OF AMERICA.

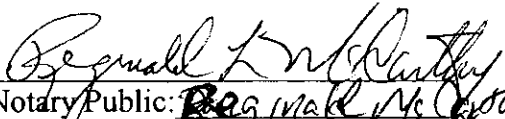

Notary Public: Reginald L. McEntee
Commission Expires on: 7/24/14

EXHIBIT A

WATERMAN ENGINEERING PLAN

432632_1

Charles Francis, Trustee, Chairman

David P. Coe
Exalted Ruler, President

Russell Karian, President
RUSS- RUSS REALTY INC.

BRISTOL HARBOR
(TIDAL WATERS)

AUGUST 11TH 2010
Revised L. K. Coe
NOTARY PUBLIC

NF
A.P. 10, LOT 83
BRISTOL COUNTY LODGE #1880 OF THE
BENEVOLENT AND PROTECTIVE ORDER OF ELKS
OF THE UNITED STATES OF AMERICA
VOLUME 128 PAGE 321

2 STORY
WOOD
STRUCTURE

MAG. NAIL
(SET)

MAG. NAIL
(SET)

PARKING /
ACCESS EASEMENT
AREA:
3,821 S.F.±

N 00°48'52" W 431.89'
SURVEY CLOSING LINE

RIP-RAP SEAWALL

20' WIDE FIRE LANE
ACCESS EASEMENT
AREA:
2,878 S.F.±

CONSTITUTION STREET
(PUBLIC WIDTH - VARIES)

CONSTRUCTION /
MAINTENANCE EASEMENT
AREA:
1,897 S.F.±

TANK

Plan intended
to illustrate the
easement
locations only
and is not a
subdivision.

David L. Willison
Administrative Officer

August 16, 2010
Date

4 STORY
BRICK STRUCTURE
NF
A.P. 10, LOTS 42, 60-62 & 73
RUSS- RUSS REALTY CO.
VOLUME 203, PAGE 18

PROJECT NO.	03-043	PROJECT TITLE:	EASEMENT PLAN CONSTITUTION STREET BRISTOL, RHODE ISLAND
SCALE:	1" = 30'		
DATE:	12/19/07		
DRAWN BY:	BJT	PREPARED FOR:	RUSS- RUSS REALTY COMPANY c/o ROBIN RUO COMPANY 125 THAMES STREET - BRISTOL, R.I. 02809
CHECKED BY:	RSL		
FILE NAME:	03-043_EASEMENT...		
DRAWING #:	EPI		

WATERMAN ENGINEERING CO.
CIVIL ENGINEERS / SURVEYORS - EST. 1894
46 SUTTON AVENUE
EAST PROVIDENCE, RHODE ISLAND 02914
PH. - (401) 438-5775 FAX - (401) 438-5773



I. GORDON KARIAN, of the Town of Bristol, Rhode Island

~~002084~~

for consideration paid, grant to RUSSELL KARIAN of 125 Thames Street,
Bristol, Rhode Island

xx _____ with WARRANTY COVENANTS

(Description, and Incumbrances, if any)

That certain lot or parcel of land with all buildings and improvements thereon, situated on the easterly side of Thames Street, in the Town and County of Bristol, State of Rhode Island, bounded and described as follows:

Beginning at a point on the easterly side of Thames Street, at the northwesterly corner of the premises herein described, which said point is the southwesterly corner of land now or formerly of Joseph Perry and wife, Almerinda C. Perry; thence running easterly, bounding northerly on said Perry land, a distance of Eighty-three (83') feet more or less, to a point for a corner; thence turning and running northerly, bounding westerly on said Perry land, a distance of Forty-three (43') feet, more or less, to land now or formerly of Lucia S. Turillo; thence turning and running easterly bounding northerly on said Turillo land, a distance of Ninety (90') feet to land now or formerly of Grace F. Kinder; thence turning and running southerly, bounding easterly on said Kinder land, a distance of One Hundred Eleven (111') feet, more or less, to land now or formerly of Meta A. Williston Estate, Elizabeth W. Pettine and William R. Williston; thence turning and running westerly, bounding southerly, in part on said Williston Estate, et als. land, in part on land now or formerly of Ida L. Hall, and in part on land now or formerly of Jesse King Sylvia and wife, Rose K. Sylvia, a distance of Eighty-seven (87') feet, more or less, to land now or formerly of James D. Rielly and wife, Adleen D. Rielly; thence turning and running northerly to a point for a corner, bounding westerly on said Rielly land, a distance of Twenty-seven and 5/10 (27.5') feet; thence turning and running westerly, bounding southerly on said Rielly land, a distance of Eighty-five (85') feet, more or less, to said easterly side of Thames Street; thence turning and running northerly, bounding westerly on the easterly line of said Thames Street, a distance of Forty and 5/10 (40.5') feet to the first above-mentioned Perry land and the point or place of beginning, or however otherwise the same may be bounded and described.

Recorded

JUN 27 1989
at 10:57 AM

Aimee C Mederos Town Clerk

~~chuslandx~~ xof x lex grm orbo
 xvixex

~~He~~ He my hand this

27th day of April, 19 89

GORDON KARIAN

(PRINT OR TYPE: NAME OF GRANTOR)

State of Rhode Island, Etc.
COUNTY OF BRISTOL

(PRINT OR TYPE NAME OF GRANTOR)

In Bastrop on the 27th day of June, 1989
before me personally appeared Gordon Karian

to me known and known by me to be the part y executing the foregoing instrument, and he acknowledged said instrument, by him executed, to be his free act and deed.

LEO R. MURPHY JR.
NOTARY PUBLIC

Russell Karian

125 Thames St., Bristol, R.I.

(PRINT OR TYPE: NAME AND ADDRESS OF GRANTEE)

Jul 19, 2022 11:36A
Receipt Number: 110338
FOR DEPOSIT ONLY
TOWN OF BRISTOL, RI
TOWN CLERKS OFFICE

Your receipt

PURCHASE RECEIPT

Bristol Town Clerk's Office Town of Bristol

10 Court Street
Bristol RI 02809
(401)253-7000
OTC Local Ref ID: 72453874
7/19/2022 11:34 AM

Status: **APPROVED**
Customer Name: Gerry Holdright
Type: Visa
Credit Card Number: **** * 3133

Items	Location	Quantity	TPE Order ID	Total Amount
Office of Bristol Town Clerk - CC Payment	Town of Bristol	1	37740113	\$1.50
Total remitted to the Bristol Town Clerk's Office				\$1.50
RI total amount charged				\$2.53

Signature

Approved over phone

Samantha

Your receipt

PURCHASE RECEIPT

May 17, 2021

Town of Bristol
Department of Community Development
9 Court Street
Bristol, RI 02809

RE: Authorization for Third-Party Submission of Development Application

To Whom it May Concern,

This letter serves to confirm that I, Russell Karian, am the rightful owner of Plat 10, Lots 42, 68, 41, 44, 62, 61, 73, 60, 43, 49, 74, 76 in Bristol, Rhode Island.

The aforementioned lots are owned by the following entities: Russ-Russ Realty Co., P.O. Box 656, Bristol, RI; Karian Realty Co., P.O. Box 656, Bristol, RI; and, Russell Karian, individually.

As owner of all three entities, I authorize Brady Sullivan Properties, LLC of 670 N. Commercial Street, Ste 303, Manchester, NH, to propose and submit redevelopment applications in connection with these properties to the Town of Bristol.

Russell Karian
Russell Karian

Date: 5-17-2021

State of Rhode Island
County of Bristol

On this 17th day of May, 2021, before me, the undersigned notary public, personally appeared RUSSELL KARIAN and proved to me through satisfactory evidence of identification to be the person whose name is signed on the preceding or attached document, and acknowledged that they signed it voluntarily for its stated purpose.

Notary Signature: Stephanie Bernardo, Notary Public
Printed Name: Stephanie Bernardo
ID Number: 750840
My Commission expires: 9-11-2021



**COMMITMENT FOR TITLE INSURANCE
ISSUED BY
CHICAGO TITLE INSURANCE COMPANY****NOTICE**

IMPORTANT—READ CAREFULLY: THIS COMMITMENT IS AN OFFER TO ISSUE ONE OR MORE TITLE INSURANCE POLICIES. ALL CLAIMS OR REMEDIES SOUGHT AGAINST THE COMPANY INVOLVING THE CONTENT OF THIS COMMITMENT OR THE POLICY MUST BE BASED SOLELY IN CONTRACT.

THIS COMMITMENT IS NOT AN ABSTRACT OF TITLE, REPORT OF THE CONDITION OF TITLE, LEGAL OPINION, OPINION OF TITLE, OR OTHER REPRESENTATION OF THE STATUS OF TITLE. THE PROCEDURES USED BY THE COMPANY TO DETERMINE INSURABILITY OF THE TITLE, INCLUDING ANY SEARCH AND EXAMINATION, ARE PROPRIETARY TO THE COMPANY, WERE PERFORMED SOLELY FOR THE BENEFIT OF THE COMPANY, AND CREATE NO EXTRACONTRACTUAL LIABILITY TO ANY PERSON, INCLUDING A PROPOSED INSURED.

THE COMPANY'S OBLIGATION UNDER THIS COMMITMENT IS TO ISSUE A POLICY TO A PROPOSED INSURED IDENTIFIED IN SCHEDULE A IN ACCORDANCE WITH THE TERMS AND PROVISIONS OF THIS COMMITMENT. THE COMPANY HAS NO LIABILITY OR OBLIGATION INVOLVING THE CONTENT OF THIS COMMITMENT TO ANY OTHER PERSON.

COMMITMENT TO ISSUE POLICY

Subject to the Notice; Schedule B, Part I—Requirements; Schedule B, Part II—Exceptions; and the Commitment Conditions, Chicago Title Insurance Company (the "Company"), commits to issue the Policy according to the terms and provisions of this Commitment. This Commitment is effective as of the Commitment Date shown in Schedule A for each Policy described in Schedule A, only when the Company has entered in Schedule A both the specified dollar amount as the Proposed Policy Amount and the name of the Proposed Insured.

If all of the Schedule B, Part I—Requirements have not been met within after the Commitment Date, this Commitment terminates and the Company's liability and obligation end.

COMMITMENT CONDITIONS**1. DEFINITIONS**

- (a) "Knowledge" or "Known": Actual or imputed knowledge, but not constructive notice imparted by the Public Records.
- (b) "Land": The land described in Schedule A and affixed improvements that by law constitute real property. The term "Land" does not include any property beyond the lines of the area described in Schedule A, nor any right, title, interest, estate, or easement in abutting streets, roads, avenues, alleys, lanes, ways, or waterways, but this does not modify or limit the extent that a right of access to and from the Land is to be insured by the Policy.

This page is only a part of a 2016 ALTA® Commitment for Title Insurance issued by Chicago Title Insurance Company. This Commitment is not valid without the Notice; the Commitment to Issue Policy; the Commitment Conditions; Schedule A; Schedule B, Part I—Requirements; Schedule B, Part II—Exceptions; and a counter-signature by the Company or its issuing agent that may be in electronic form.

- (c) "Mortgage": A mortgage, deed of trust, or other security instrument, including one evidenced by electronic means authorized by law.
 - (d) "Policy": Each contract of title insurance, in a form adopted by the American Land Title Association, issued or to be issued by the Company pursuant to this Commitment.
 - (e) "Proposed Insured": Each person identified in Schedule A as the Proposed Insured of each Policy to be issued pursuant to this Commitment.
 - (f) "Proposed Policy Amount": Each dollar amount specified in Schedule A as the Proposed Policy Amount of each Policy to be issued pursuant to this Commitment.
 - (g) "Public Records": Records established under state statutes at the Commitment Date for the purpose of imparting constructive notice of matters relating to real property to purchasers for value and without Knowledge.
 - (h) "Title": The estate or interest described in Schedule A.
2. If all of the Schedule B, Part I—Requirements have not been met within the time period specified in the Commitment to Issue Policy, this Commitment terminates and the Company's liability and obligation end.
3. The Company's liability and obligation is limited by and this Commitment is not valid without:
- (a) the Notice;
 - (b) the Commitment to Issue Policy;
 - (c) the Commitment Conditions;
 - (d) Schedule A;
 - (e) Schedule B, Part I—Requirements; [and]
 - (f) Schedule B, Part II—Exceptions; and
 - (g) a counter-signature by the Company or its issuing agent that may be in electronic form].
4. **COMPANY'S RIGHT TO AMEND**
The Company may amend this Commitment at any time. If the Company amends this Commitment to add a defect, lien, encumbrance, adverse claim, or other matter recorded in the Public Records prior to the Commitment Date, any liability of the Company is limited by Commitment Condition 5. The Company shall not be liable for any other amendment to this Commitment.
5. **LIMITATIONS OF LIABILITY**
- (a) The Company's liability under Commitment Condition 4 is limited to the Proposed Insured's actual expense incurred in the interval between the Company's delivery to the Proposed Insured of the Commitment and the delivery of the amended Commitment, resulting from the Proposed Insured's good faith reliance to:
 - (i) comply with the Schedule B, Part I—Requirements;
 - (ii) eliminate, with the Company's written consent, any Schedule B, Part II—Exceptions; or
 - (iii) acquire the Title or create the Mortgage covered by this Commitment.
 - (b) The Company shall not be liable under Commitment Condition 5(a) if the Proposed Insured requested the amendment or had Knowledge of the matter and did not notify the Company about it in writing.
 - (c) The Company will only have liability under Commitment Condition 4 if the Proposed Insured would not have incurred the expense had the Commitment included the added matter when the Commitment was first delivered to the Proposed Insured.

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- (d) The Company's liability shall not exceed the lesser of the Proposed Insured's actual expense incurred in good faith and described in Commitment Conditions 5(a)(i) through 5(a)(iii) or the Proposed Policy Amount.
- (e) The Company shall not be liable for the content of the Transaction Identification Data, if any.
- (f) In no event shall the Company be obligated to issue the Policy referred to in this Commitment unless all of the Schedule B, Part I—Requirements have been met to the satisfaction of the Company.
- (g) In any event, the Company's liability is limited by the terms and provisions of the Policy.

6. LIABILITY OF THE COMPANY MUST BE BASED ON THIS COMMITMENT

- (a) Only a Proposed Insured identified in Schedule A, and no other person, may make a claim under this Commitment.
- (b) Any claim must be based in contract and must be restricted solely to the terms and provisions of this Commitment.
- (c) Until the Policy is issued, this Commitment, as last revised, is the exclusive and entire agreement between the parties with respect to the subject matter of this Commitment and supersedes all prior commitment negotiations, representations, and proposals of any kind, whether written or oral, express or implied, relating to the subject matter of this Commitment.
- (d) The deletion or modification of any Schedule B, Part II—Exception does not constitute an agreement or obligation to provide coverage beyond the terms and provisions of this Commitment or the Policy.
- (e) Any amendment or endorsement to this Commitment must be in writing [and authenticated by a person authorized by the Company].
- (f) When the Policy is issued, all liability and obligation under this Commitment will end and the Company's only liability will be under the Policy.

7. IF THIS COMMITMENT HAS BEEN ISSUED BY AN ISSUING AGENT

The issuing agent is the Company's agent only for the limited purpose of issuing title insurance commitments and policies. The issuing agent is not the Company's agent for the purpose of providing closing or settlement services.

8. PRO-FORMA POLICY

The Company may provide, at the request of a Proposed Insured, a pro-forma policy illustrating the coverage that the Company may provide. A pro-forma policy neither reflects the status of Title at the time that the pro-forma policy is delivered to a Proposed Insured, nor is it a commitment to insure.

9. ARBITRATION

The Policy contains an arbitration clause. All arbitrable matters when the Proposed Policy Amount is \$2,000,000 or less shall be arbitrated at the option of either the Company or the Proposed Insured as the exclusive remedy of the parties. A Proposed Insured may review a copy of the arbitration rules at <http://www.alta.org/arbitration>.

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Transaction Identification Data for reference only:Issuing Agent: **Accurate Title**Issuing Office: **170 S. River Rd, Bedford NH 03110**Issuing Office's ALTA® Registry ID: **1034284**

Loan ID Number:

Commitment Number:

Issuing Office File Number: **2021-383**Property Address: **60 and 70 Thames Street, Bristol, RI 02809****SCHEDULE A**

1. Commitment Date: **February 24, 2021 at 4:00 AM**
2. Policy to be issued:
 - (a) 2006 ALTA® Owner's Policy
Proposed Insured: **Brady Sullivan Properties, LLC**
Proposed Policy Amount: **\$4,200,000.00**
 - (b) 2006 ALTA® Loan Policy
Proposed Insured: **Cash**
Proposed Policy Amount:
3. The estate or interest in the Land described or referred to in this Commitment is **Fee Simple**.
4. The Title is, at the Commitment Date, vested in:

Parcel 1:

Russ-Russ Realty Co. by virtue of a Quit Claim Deed from Bristol Development Co. dated October 16, 1975 and recorded with the Town of Bristol Land Evidence Records on October 23, 1975 in Book 203 at Page 18. (Parcel 1 in said Deed)

Parcel 2:

Russ-Russ Realty Co. by virtue of a Quit Claim Deed from Bristol Development Co. dated October 16, 1975 and recorded with the Town of Bristol Land Evidence Records on October 23, 1975 in Book 203 at Page 18. (Parcel 2 in said Deed)

Parcel 3:

Russ-Russ Realty Co. by virtue of a Quit Claim Deed from Bristol Development Co. dated October 16, 1975 and recorded with the Town of Bristol Land Evidence Records on October 23, 1975 in Book 203 at Page 18. (Parcel 3 in said Deed)

Parcel 4:

Karian Realty Company by virtue of a Quit Claim Deed from Ralph F. Kinder and Fleet National Bank in its capacity as Trustee under the Joseph C. Kinder Marital Trust said

This page is only a part of a 2016 ALTA® Commitment for Title Insurance issued by Chicago Title Insurance Company. This Commitment is not valid without the Notice; the Commitment to Issue Policy; the Commitment Conditions; Schedule A; Schedule B, Part I—Requirements; Schedule B, Part II—Exceptions; and a counter-signature by the Company or its issuing agent that may be in electronic form.

deed is dated December 7, 1990 and recorded with the Town of Bristol Land Evidence Records on December 10, 1990 in Book 392 at Page 36.

Parcel 5:

Russell Karian by virtue of a Warranty Deed from Jeffrey J. Graham and Geraldine Marsden dated August 29, 2003 and recorded with the Town of Bristol Land Evidence Records on August 29, 2003 in Book 1047 at Page 90.

NOTE: The Street referenced in the legal description in this Deed is incorrectly listed as "Thomas Street", it should be "Thames Street". I have corrected that in our legal description in this Commitment.

Parcel 6:

Russell Karian by virtue of a Warranty Deed from Gordon Karian dated June 27, 1989 and recorded with the Town of Bristol Land Evidence Records on June 27, 1989 in Book 352 at Page 218.

5. The Land is described as follows: **See Exhibit "A" attached hereto and made a part hereof.**

CHICAGO TITLE INSURANCE COMPANY



By: _____

Authorized Signatory

**Accurate Title
170 S. River Rd
Bedford, NH 03110**

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**SCHEDULE B, PART I
Requirements**

All of the following Requirements must be met:

1. The Proposed Insured must notify the Company in writing of the name of any party not referred to in this Commitment who will obtain an interest in the Land or who will make a loan on the Land. The Company may then make additional Requirements or Exceptions.
2. Pay the agreed amount for the estate or interest to be insured.
3. Pay the premiums, fees, and charges for the Policy to the Company.
4. Documents satisfactory to the Company that convey the Title or create the Mortgage to be insured, or both, must be properly authorized, executed, delivered, and recorded in the Public Records.
 - a. Duly authorized and executed Deed from Russell Karian vesting Fee Simple title in Brady Sullivan Properties, LLC.

**SCHEDULE B, PART II
Exceptions**

THIS COMMITMENT DOES NOT REPUBLISH ANY COVENANT, CONDITION, RESTRICTION, OR LIMITATION CONTAINED IN ANY DOCUMENT REFERRED TO IN THIS COMMITMENT TO THE EXTENT THAT THE SPECIFIC COVENANT, CONDITION, RESTRICTION, OR LIMITATION VIOLATES STATE OR FEDERAL LAW BASED ON RACE, COLOR, RELIGION, SEX, SEXUAL ORIENTATION, GENDER IDENTITY, HANDICAP, FAMILIAL STATUS, OR NATIONAL ORIGIN.

The Policy will not insure against loss or damage resulting from the terms and provisions of any lease or easement identified in Schedule A, and will include the following Exceptions unless cleared to the satisfaction of the Company:

1. Any defect, lien, encumbrance, adverse claim, or other matter that appears for the first time in the Public Records or is created, attaches, or is disclosed between the Commitment Date and the date on which all of the Schedule B, Part I—Requirements are met.
2. **THE FOLLOWING ARE AS TO PARCEL 1:** Restrictions, rights and easements as set forth in Book 128 at Page 291. Right, title and interest in and to the Long Wharf Estate as set forth in Parcel 1 of the deed recorded in Book 203 at Page 18; Perpetual right to use in common with others, for pleasure purposes the seashore which lies westerly of said premises below high water mark as set forth in Book 203 at Page 18; Town of Bristol, Rhode Island Planning Board Decision as set forth in Book 1543 at Page 161; Bristol Historic District Commission Letter of Decision as set forth in Book 1544 at Page 60.

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3. **THE FOLLOWING ARE AS TO PARCEL 2:** Restrictions, rights and easements as set forth in Deed in Book 128 at Page 291; Town of Bristol, Rhode Island Planning Board Decision as set forth in Book 1543 at Page 161.
4. **THE FOLLOWING ARE AS TO PARCEL 3:** Restrictions and Easements as set forth in Book 128 at Page 291.
5. **THE FOLLOWING ARE AS TO PARCEL 4:** Ten foot right of way as set forth in Book 63 at Page 398; Easement and Transfer Agreement as set forth in Book 392 at Page 22; Town of Bristol Historic District Commission Letter of Decision recorded in Book 1540 at Page 187.
6. **THE FOLLOWING ARE AS TO PARCEL 5:** Bristol Historic District Decision recorded in Book 1087 at Page 291. Bristol Historic District Decision recorded in Book 1091 at Page 341;
7. **THE FOLLOWING ARE AS TO PARCEL 6:** Bristol Historic District Commission Letter recorded in Book 1544 at Page 61.

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EXHIBIT A
Legal Description

Parcel 1:

All those certain parcels of land, with all the buildings and improvements thereon, on the easterly and westerly sides and at the southerly end of Thames Street, on the southerly side of John Street, and on the northerly side of Church and Constitution Streets, in the Town and County of Bristol and State of Rhode Island, more fully described as follows:

PARCEL I: That certain lot or parcel of land with all the buildings and improvements thereon, situated on the westerly side of Thames Street and the northerly line of Constitution Street, bounded and described as follows:

Beginning at a point in the westerly line of Thames Street and its intersection with the northerly line of Constitution Street as now used and occupied as a public highway, said point of beginning being eighteen and 27/100 (18.27) feet southerly, measured along said westerly line of Thames Street from the intersection of said westerly line of Thames Street with the Schubarth line, so-called, and said point of beginning being the southeasterly corner hereof; thence westerly bounding southerly on said Church Street as now used and occupied as a public highway one hundred forty-nine and 25/100 (149.25) feet, more or less, to land now or lately of the Bristol County Lodge #1860 Order of the Elks; thence northerly bounding westerly on said last named land eighty-two and 6/10 (82.6) feet to the northeasterly corner of said last named land; thence westerly bounding southerly on said last named land to the Harbor line; thence northerly on said Harbor line to land now or lately of the Town of Bristol, being the parcel of land conveyed by Cranston Worsted Mills to the State of Rhode Island and Providence Plantations by deed dated June 29, 1894 and recorded in Book 54 at Page 1; thence easterly bounding northerly on said last named land about eighteen and 5/10 (18.5) feet to the southeasterly corner thereof; thence northerly bounding westerly on said last named land eighty-five (85) feet, more or less, to the southerly line of the Long Wharf or Long Wharf Estates; thence easterly bounding northerly on said last named land two hundred one and 5/10 (201.5) feet to said Thames Street; thence southerly bounding easterly on said Thames Street four hundred eighty-five (485) feet, more or less, to said northerly line of Constitution Street as now used and occupied as a public highway at the point of beginning; together with all right, title, and interest of the grantor in and to the Long Wharf and the Long Wharf Estate, so-called.

However otherwise bounded and described, meaning to convey and hereby conveying the same premises described as Parcels I to V inclusive in the deed from Collins & Aikman Corporation to this grantor dated February 10, 1956 and recorded on February 15, 1956 in Book 128 at Page 291.

125 Thames Street, Bristol
Plat 10 Lot 60

0 Thames Street, Bristol
Plat 10 Lot 42, 61

0 Constitution Street, Bristol
Plat 10 Lot 62, 73

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Parcel 2:

PARCEL II: That certain lot or parcel of land with all the buildings and improvements thereon, situated in said Town of Bristol, County of Bristol, and State of Rhode Island, on the easterly side of Thames Street, bounded and described as follows:

Beginning at a point on the easterly side of Thames Street distant one hundred seventeen and 58/100 (117.58) feet, more or less, from the southeasterly corner of Thames Street and Church Street, said point of beginning being the southwesterly corner of land now or lately of James D. Rielly et ux; thence easterly along a fence and along the southerly boundary of said Rielly land, along the southerly boundary of land now or lately of Theresa Bramwell, and along the southerly boundary of land now or lately of Mary P. Brogan et al to land now or lately of Alfred J. Shepherd one hundred seventy-two and 96/100 (172.96) feet, more or less; thence southerly, along the westerly boundary of said Shepherd land and the westerly boundary of land now or lately of Ralph F. Kinder et al to land now or lately of John B. Zufelt et ux one hundred thirty-nine and 5/10 (139.5) feet, more or less, thence westerly along the northerly boundary of said Zufelt land one hundred seventy-two (172) feet, more or less, to the easterly side of Thames Street; thence northerly along Thames Street one hundred forty-seven (147) feet, more or less, to the point and place of beginning.

Said parcel is set forth as Parcel VI in the deed referred to in Parcel I hereof.

0 Thames Street, Bristol
Plat 10 Lots 41, 44, 68

Parcel 3:

That certain lot or parcel of land with all the buildings and improvements thereon, if any, situated in the Town of Bristol, County of Bristol and State of Rhode Island, bounding southerly on Church Street on which it measures about one hundred (100) feet and holding that width extending back therefrom about eighty-seven (87) feet to and bounding northerly on land now or lately of Residential Rooms, Inc., bounding easterly on land now or lately of John T. Carpenter et ux in part and in part on land now or lately of Geoffrey B. Davis et al, and westerly on Thames Street.

0 Thames Street, Bristol
Plat 10 Lot 32

Parcel 4:

That certain lot or parcel of land with all the buildings and improvements thereon situated in the Town and County of Bristol, State of Rhode Island and bounded and described as follows:

Beginning at a point in the westerly street line of Hope Street, said point being the most southeasterly corner of land now or formerly of Alfred Brazil, Jr.;

thence running in a southerly direction, bounding on the aforementioned westerly street line of Hope Street, a distance of twenty-seven and 44/100 (27.44) feet to a corner, said corner being the most northeasterly corner of land now or formerly of Emanuel and Harrier K. Iacoletti;

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thence turning an interior angle of 91° 18' 00" and running in a westerly direction, bounding on the aforementioned Iacoletti land, a distance of sixty-five and 73/100 (65.73) feet to a point;

thence turning an interior angle of 178° 33' 00" and continuing in a westerly direction, bounding on the aforementioned Iacoletti land, a distance of thirty-four and 40/100 (34.40) feet to a corner, said corner being the most northwesterly corner of said Iacoletti land;

thence turning an interior angle of 271° 20' 00" and running in a southerly direction, bounding on the aforementioned Iacoletti land and land now or formerly of Manuel Furtado, Jr., a distance of one hundred twenty-four and 88/100 (124.88) feet to a corner of the herein described parcel;

thence turning an interior angle of 87° 45' 30" and running in a westerly direction, bounding, in part, on land now or formerly of Stephen M. and Joyce A. Grabert and in part on land now or formerly of Harold W. and Frances S. Demopoulos, a distance of seventy-six and 12/100 (76.12) feet to a corner, said corner being the most southwesterly corner of the herein described parcel;

thence turning an interior angle of 91° 21' 00" and running in a northerly direction, bounding on land now or formerly of Russell Karian, land now or formerly of John B. and Sadie Zufelt, and land now or formerly of Russ-Russ Realty Co., a distance of two hundred eleven and 03/100 (211.03) feet to a corner, said corner being the most northwesterly corner of the herein described parcel;

thence turning an interior angle of 90° 20' 40" running in an easterly direction, bounding on land now or formerly of Alfred J. Shepherd, a distance of one hundred fifteen and 64/100 (115.64) feet to a corner, said corner being the most northeasterly corner of the herein described parcel;

thence turning an interior angle of 89° 25' 50" and running in a southerly direction, bounding on the previously mentioned Brazil land, a distance of sixty-one and 39/100 (61.39) feet to a corner;

thence turning an interior angle of 271° 18' 00" and running in an easterly direction, bounding on the aforementioned Brazil land, a distance of fifty-nine and 04/100 (59.04) feet to the point and place of beginning.

The last course making an angle of 88° 38' 00" with the first herein described course.

Containing, by calculation, 21,132 square feet of land.

317 Hope Street, Bristol
Plat 10 Lots 74, 76, 43

Parcel 5:

That certain real estate situated in the Town of Bristol, County of Bristol and State of Rhode Island, and described as follows:

That certain lot or parcel of land, with all buildings and improvements thereon, situated on the easterly side of Thames Street, in the Town and County of Bristol and State of Rhode Island, forty (40) feet southerly from the northwesterly corner of the lot of land known as The Homestead Estate of the later Francis Bourne, bounding westerly on Thames Street, twenty seven (27) feet, five (5) inches, and holding

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that width, extending easterly eighty-five (85) feet, more or less; bounding northerly or easterly on land now or lately of John P. Sousa and wife, and southerly in part on land now or lately of Rose K. Sylvia and in part on land now or lately of Mary Gibbemeyer et al.

Being the same premises conveyed to Jeffrey J. Graham and Geraldine Marsden by Warranty Deed from Mary Alice Graham dated July 1, 1994 and recorded in the Land Evidence Records of the Town of Bristol on April 17, 2000 in Book 737 at Page 151.

60 Thames Street, Bristol
Plat 10 Lot 50

Parcel 6:

That certain lot or parcel of land with all buildings and improvements thereon, situated on the easterly side of Thames Street, in the Town and County of Bristol, State of Rhode Island, bounded and described as follows:

Beginning at the point on the easterly side of Thames Street, at the northwesterly corner of the premises herein described, which said point is the southwest corner of land now or formerly of Joseph Perry and wife, Almerinda C. Perry; thence running easterly, bounding northerly on said Perry land, a distance of Eighty-three (83') feet, more or less, to a point for a corner; thence turning and running northerly, bounding westerly on said Perry land, a distance of Forty-three (43') feet, more or less, to land now or formerly of Lucia S. Turillo; thence turning and running easterly bounding northerly on said Turillo land, a distance of Ninety (90') feet to land now or formerly of Grace F. Kinder; thence turning and running southerly, bounding easterly on said Kinder land, a distance of One Hundred Eleven (111') feet, more or less, to land now or formerly of Meta A. Williston Estate, Elizabeth W. Pattine and William R. Williston; thence turning and running westerly, bounding southerly, in part on said Williston Estate, et als. land, in part on land now or formerly of Ida L. Hall, and in part on land now or formerly of Jesse King Sylvia and wife Rose K. Sylvia, a distance of Eighty-seven (87') feet, more or less, to land now or formerly of James D. Reilly and wife, Adleen D. Rielly; thence turning and running northerly to a point for a corner, bounding westerly on said Rielly land, a distance of Twenty-seven and 5/10 (27.5') feet; thence turning and running westerly; bounding southerly on said Rielly land, a distance of Eighty-five (85') feet, more or less, to said easterly side of Thames Street; thence turning and running northerly, bounding westerly on the easterly line of said Thames Street, a distance of Forty and 5/10 (40.5') feet to the first above-mentioned Perry land and the point or place of beginning, or however otherwise the same may be bounded and described.

70 Thmes Street, Bristol
Plat 10 Lot 49

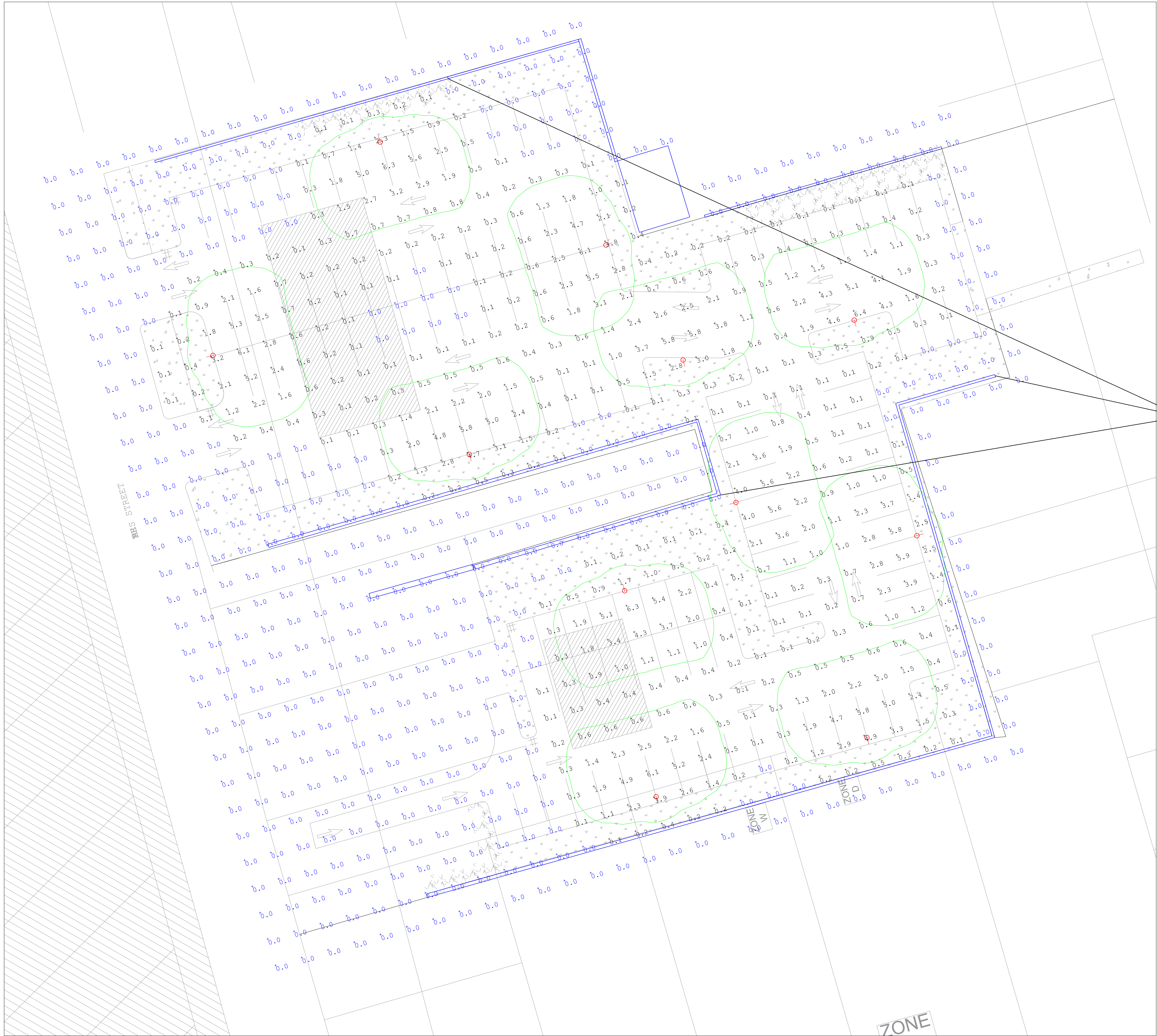
Property Address:
125 Thames Street, Bristol, RI 02809
Plat: 10 Lot(s): 42, 60, 61, 62 and 73 - "Mill"
0 Thames Street, Bristol, RI 02809
Plat: 10 Lot(s): 41, 44 and 68 - Mill Parking Lot"
0 Thames Street, Bristol, RI 02809
Plat: 10 Lot(s): 32 - "Church and Thames Parking Lot"
317 Hope Street, Bristol, RI 02809

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Plat: 10 Lot(s): 74, 76 and 43 - Gravel Parking Lot"
60 Thames Street, Bristol, RI 02809
Plat: 10 Lot(s): 50 - "60 Thames Street Property"
70 Thames Street, Bristol, RI 02809
Plat: 10 Lot(s): 49 - "70 Thames Street Property"

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Lighting Layouts and Fixtures



6' stockade fence included in calculation.

Luminaire Schedule					
Qty	Label	Watts	Arrangement	LLF	Description
11	SL4	79.6	Single	0.900	Sternberg 1521LED-16L40T4-MDL014-FG-BLOC-x-x-x

Calculation Summary						
Label	Units	Avg	Max	Min	Avg/Min	Max/Min
CalcPts_1	Fc	0.62	6.4	0.0	N.A.	N.A.
Beyond Prop	Fc	0.00	0.0	0.0	N.A.	N.A.

Project:
125 Thames Street

Contact:
Liza Tuttle
Specification Sales
(860) 751-4388
ltuttle@illuminate.com

illuminate
44 Sixth Road
Woburn, MA 01801
(781) 935-8500
333 Pleasant Valley Road
South Windsor, CT 06074
(860) 282-0597

Detail: Photometric Calculation
Date: 4/13/2022
Revision: A
Scale: 1" = 20'-0"
Drawn By: AJS

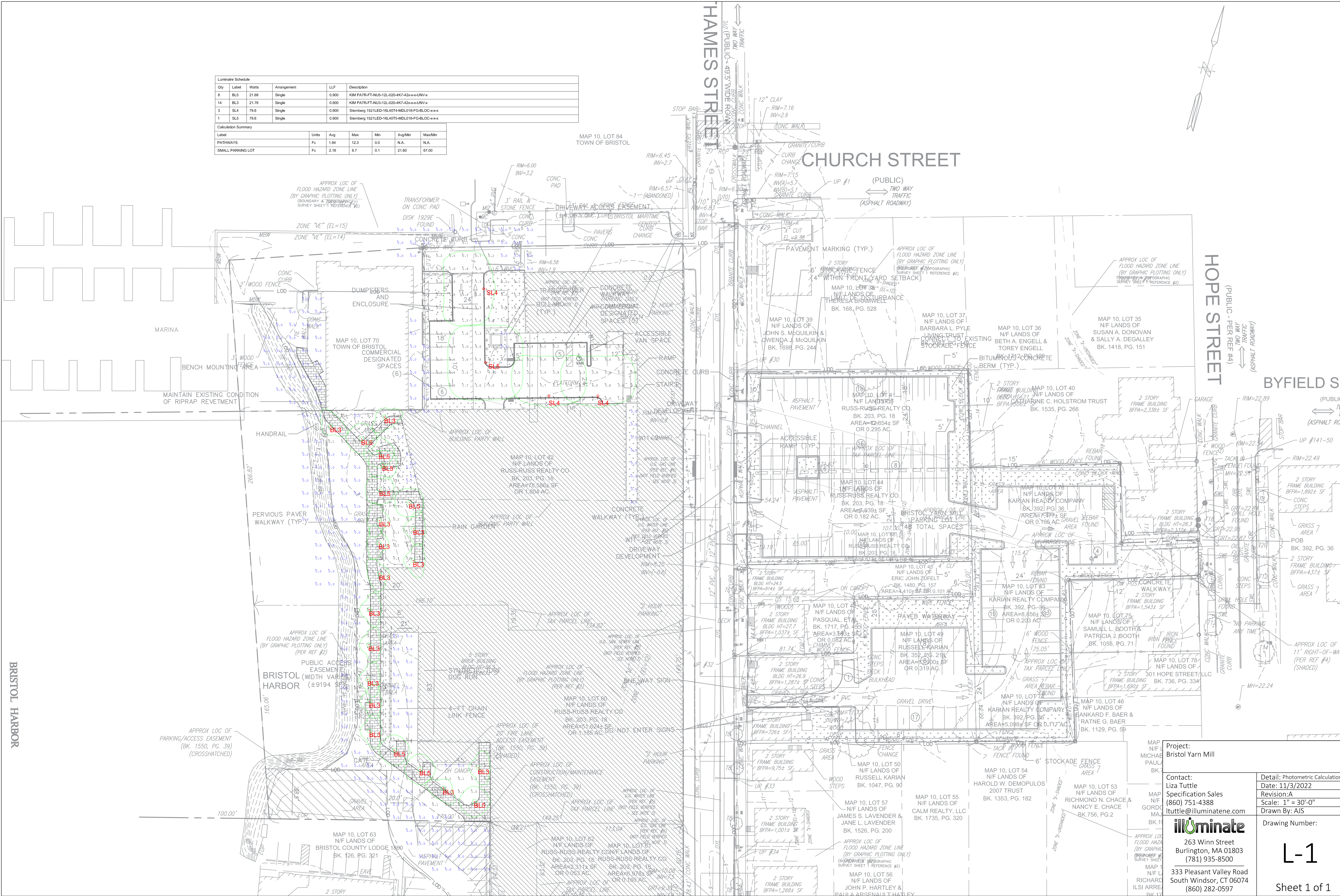
Drawing Number:

L-1

Sheet 1 of 1

Luminaire Schedule					
Qty	Label	Watts	Arrangement	LLF	Description
8	BL5	21.68	Single	0.900	KIM PATR-FT-NU5-12L-020-4K7-42xxx-UNV-x
14	BL3	21.76	Single	0.900	KIM PATR-FT-NU3-12L-020-4K7-42xxx-UNV-x
3	SL4	79.6	Single	0.900	Sternberg 1521LED-16L40T4-MDL018-FG-BLOC-xxx
1	SL5	79.6	Single	0.900	Sternberg 1521LED-16L40T5-MDL018-FG-BLOC-xxx

Calculation Summary						
Label	Units	Avg	Max	Min	Avg/Min	Max/Min
PATHWAYS	Fc	1.94	12.3	0.0	N.A.	N.A.
SMALL PARKING LOT	Fc	2.16	6.7	0.1	21.60	67.00



Project:
Bristol Yarn Mill

Contact:
Liza Tuttle
Specification Sales
(860) 751-4388
ltuttle@illuminate.com

Detail: Photometric Calculation
Date: 11/3/2022
Revision: A
Scale: 1" = 30'-0"
Drawn By: AUS

illuminate

263 Winn Street
Burlington, MA 01803
(781) 935-8500

333 Pleasant Valley Road
South Windsor, CT 06074
(860) 282-0597

L-1
Sheet 1 of 1



550 CONCORD SERIES

ARCHITECTURAL POLE

DIMENSIONS
12" Ø
4" TALL



JOB NAME _____

FIXTURE TYPE _____

MEMO _____

BUILD A PART NUMBER

ORDERING EXAMPLE: 550FP5-14-.188-BCC-GFILPIUC-SH/BKT

Base Model	Shaft	Height	Wall Thickness	Post Center Cap	Option Burial	Option Photocell	Option Receptacle	Option Flag Pole Holder	Option Banner Arms	Option Planter Arms	Option Sign Arms	Option Speaker Hub	Option Sign Bracket	Option Steel Wreath Hook	Finish

Model

• 550

Shaft

- T5: 5"-3" Tapered Smooth
- T54: 5"-4" Tapered Smooth
- P5: 5" Straight Smooth
- FP5: 5" Straight Fluted

¹ Not available in .125 wall.

Height (in feet)

- | | | | |
|------|------|------|------|
| • 8 | • 10 | • 12 | • 14 |
| • 16 | • 18 | • 20 | |

Wall Thickness

- .125: 1/8" Wall Thickness
- .188: 3/16" Wall Thickness
- .250: 1/4" Wall Thickness

Post Center Cap (if required)

(Click here to view post cap sheet)

- | | | | |
|--------|-------|-------|--------|
| • BCC | • FCC | • SCC | • TFCC |
| • SSCC | • RCC | • PCC | |

Options (Click here to view accessories sheet)

- DB4 Direct Burial mounting style pole, with 4" direct burial section (or advise other length)
- HXB Helix Base mounting style pole
- PCD Electronic Button Photocontrol, mounted on an access door (120v-277v)
- PCD4 Electronic Button Photocontrol, mounted on an access door (480v)
- GFI LPIUC 15 Amp duplex GFCI receptacles with a low-profile in-use cover
- GFI IUC 15 Amp duplex GFCI receptacles with a standard in-use cover

- USB LPIUC 15 Amp duplex USB/Receptacle combo with a low-profile in-use cover (NON-GFI)
- USB IUC 15 Amp duplex USB/Receptacle combo with a standard in-use cover (NON-GFI)
- FH Cast Aluminum flag holder mount, for use with 1" diameter flag pole
- SBA Single Banner Arm, "PM" style mount
- DBA Double Banner Arms, "PM" style mount
- SBAR Single Banner Arm and Ring, for triangle banners, "PM" style mount
- HSBA Single Banner Arm, HUB mount style mount
- HDBA Double Banner Arms, HUB mount style mount
- BDBA6 Double Banner Arms, Break-Away style, to break with 60MPH wind gust
- BDBA9 Double Banner Arms, Break-Away style, to break with 90MPH wind gust
- C5SBA Single Banner Arm, Clamp-Style mount, for 5" diameter poles
- C5DBA Double Banner Arms at 180°, Clamp-Style mount, for 5" diameter poles
- DHPA Double Hooked Planter Arm
- SHPA Single Hooked Planter Arm
- DSPA Double Stepped Planter Arm
- SSPA Single Stepped Planter Arm
- PA478 Cast aluminum decorative planter arm
- SA78 Small cast aluminum decorative sign arm, with 24" long channel for blade sign by others
- SA478 Large cast aluminum decorative sign arm, with 24" long channel for blade sign by others
- SABA Banner arm style sign arm, with 24" long channel for blade sign by others
- SH Female threaded speaker hub, advise thread size

- SB Sign Bracket, vertically mounted on pole shaft
- WHK Steel wreath hook

Finish

Standard Finishes² (Click here to view paint finish sheet)

- BKT Black Textured
- WHT White Textured
- PGT Park Green Textured
- ABZT Architectural Medium Bronze Textured
- DBT Dark Bronze Textured

² Smooth finishes are available upon request.

Sternberg Select Finishes

- VG Verde Green
- SI Swedish Iron
- OWGT Old World Gray Textured

Standard Urban Finishes (Click here to view paint finish sheet)

- UGMT Gun Metal Textured
- UGM Gun Metal Matte
- UBT Urban Bronze Textured
- UB Urban Bronze Matte
- USLT Urban Silver Textured
- USL Urban Silver Matte
- UWHT Urban White Textured
- UWH Urban White Matte

Custom Finishes³

- CM Custom Match
- OI Old Iron
- RT Rust
- WBR Weathered Brown
- CD Cedar
- WBK Weathered Black
- TT Two Tone

³ Custom colors require upcharge.



SternbergLighting

ESTABLISHED 1923

800-621-3376
555 Lawrence Ave., Roselle, IL 60172
contactus@sternberglighting.com
www.sternberglighting.com

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Specifications

Construction

The base shall be made of heavy wall, 356 alloy cast aluminum. It shall have a 3/4" thick floor cast as an integral part of the base. It shall have a cast anchor bolt cover designed with a curved relief. The shaft shall be double circumferentially welded internally and externally to the base for added strength.

Tenon

Standard tenon size to be 3" OD x 3" tall. Consult factory for additional sizes.

Shaft

The straight smooth shaft shall be made of ASTM 6063 extruded aluminum and tempered to a T6 condition.

The tapered smooth shaft shall be made of ASTM 6063 extruded aluminum and tempered to a T6 condition.

The straight fluted shaft will have a 12 Flat Flute profile and shall be made of ASTM 6061 extruded aluminum and tempered to a T6 condition.

Installation

Anchor Base: Slotted base with a bolt circle pattern between 8-1/4" (MIN) and 9-1/4" (MAX) diameter. Base accommodates four 3/4" diameter, hot-dipped galvanized "L" type anchor bolts that shall be provided with the post for anchorage. A 12" diameter cast slip base cover shall be provided for anchor bolt access. Above the base cover, a hand-hole with door shall be provided for wiring access, secured with stainless steel hardware. Post will be provided with a grounding stud mounted inside the shaft, opposite the door.

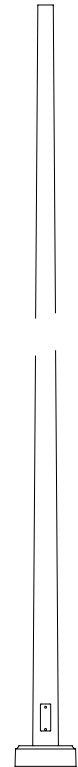
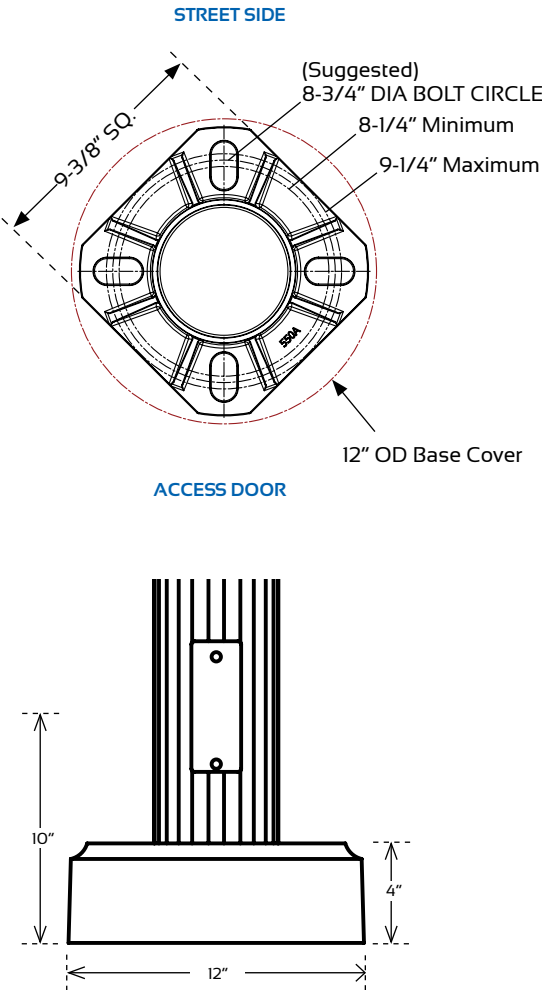
Direct Burial: 4' direct burial portion of pole shaft extending below pole base. Includes "Slipping" base style which has set screws at the neck to tighten the base down to the pole shaft. Includes anti-rotational rod and (2) wire-way holes on direct burial portion.

Warranty

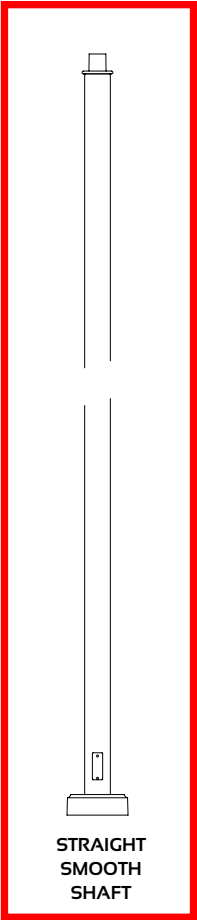
Five-year limited warranty. See product and finish warranty guide for details.

Finish

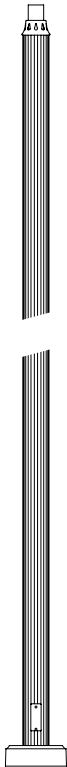
Refer to website for details.



TAPERED SMOOTH SHAFT



STRAIGHT SMOOTH SHAFT



STRAIGHT FLUTED SHAFT



550 CONCORD SERIES

ARCHITECTURAL POLE

EPA Chart - AASHTO 2009

POLE HEIGHT	90 MPH	100 MPH	110 MPH	120 MPH	130 MPH	140 MPH	150 MPH
FP5 5" Straight Fluted Pole - 0.188" Wall Thickness							
8	17.30	13.76	11.15	9.15	7.60	6.37	5.38
10	13.44	10.56	8.43	6.82	5.56	4.56	3.75
12	10.68	8.25	6.46	5.10	4.04	3.20	2.52
14	8.56	6.47	4.92	3.75	2.83	2.10	1.52
16	6.87	5.03	3.67	2.63	1.82	1.18	0.67
FP5 5" Straight Fluted Pole - 0.25" Wall Thickness							
12	14.25	11.15	8.86	7.11	5.75	4.67	3.80
14	11.65	8.97	6.99	5.48	4.31	3.38	2.63
16	9.59	7.23	5.49	4.16	3.13	2.31	1.65
18	7.88	5.78	4.22	3.04	2.12	1.38	0.79
20	6.44	4.54	3.13	2.06	1.23	0.57	
P5 5" Straight Smooth Shaft - 0.125" Wall Thickness							
8	11.88	9.56	7.85	6.56	5.55	4.76	4.12
10	9.18	7.36	6.02	5.01	4.22	3.60	3.11
12	7.25	5.78	4.70	3.88	3.26	2.76	2.37
14	5.77	4.56	3.68	3.02	2.51	2.11	1.79
16	4.58	3.58	2.85	2.31	1.90	1.58	1.32
P5 5" Straight Smooth Shaft - 0.188" Wall Thickness							
10	13.86	11.15	9.15	7.64	6.47	5.54	4.79
12	11.20	8.97	7.34	6.10	5.15	4.39	3.79
14	9.18	7.32	5.96	4.93	4.14	3.52	3.02
16	7.58	6.01	4.86	4.00	3.33	2.81	2.40
18	6.26	4.92	3.95	3.22	2.66	2.23	1.88
P5 5" Straight Smooth Shaft - 0.25" Wall Thickness							
12	14.77	11.87	9.73	8.11	6.86	5.87	5.08
14	12.27	9.82	8.03	6.67	5.62	4.79	4.13
16	10.29	8.21	6.68	5.53	4.64	3.94	3.38
18	8.68	6.89	5.57	4.58	3.83	3.23	2.76
20	7.33	5.77	4.64	3.79	3.14	2.63	2.23
T5-3 5" - 3" Smooth Tapered Shaft - 0.125" Wall Thickness							
8	6.65	5.25	4.29	3.56	3.00	2.55	2.20
10	4.95	3.83	3.10	2.55	2.13	1.79	1.53
12	3.70	2.78	2.21	1.79	1.47	1.22	1.02
14	2.71	1.94	1.51	1.19	0.95	0.76	0.61
T5-3 5" - 3" Smooth Tapered Shaft - 0.188" Wall Thickness							
10	7.72	6.08	4.96	4.11	3.46	2.94	2.53
12	6.03	4.67	3.78	3.11	2.59	2.19	1.86
14	4.73	3.58	2.86	2.32	1.91	1.59	1.34
16	3.67	2.68	2.10	1.68	1.35	1.10	0.90
T5-3 5" - 3" Smooth Tapered Shaft - 0.25" Wall Thickness							
12	8.15	6.39	5.20	4.30	3.61	3.06	2.63
14	6.56	5.06	4.08	3.35	2.79	2.35	2.00
16	5.28	3.99	3.18	2.58	2.12	1.77	1.48
18	4.22	3.09	2.42	1.93	1.56	1.27	1.05

* Consult factory for other heights.

** All EPA values expressed in square feet.



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550 CONCORD SERIES

ARCHITECTURAL POLE

EPA Chart - AASHTO 2009

POLE HEIGHT	90 MPH	100 MPH	110 MPH	120 MPH	130 MPH	140 MPH	150 MPH
T5-4 5" - 4" Smooth Tapered Shaft - 0.125" Wall Thickness							
8	6.60	5.28	4.31	3.58	3.02	2.57	2.22
10	4.88	3.87	3.14	2.58	2.16	1.82	1.55
12	3.62	2.83	2.26	1.83	1.51	1.25	1.05
14	2.61	2.00	1.56	1.23	0.99	0.80	0.65
T5-4 5" - 4" Smooth Tapered Shaft - 0.188" Wall Thickness							
10	7.66	6.12	4.99	4.14	3.48	2.97	2.55
12	5.95	4.72	3.82	3.15	2.63	2.22	1.89
14	4.63	3.64	2.91	2.37	1.95	1.63	1.38
16	3.56	2.75	2.16	1.73	1.40	1.14	0.94
18	2.66	2.00	1.53	1.18	0.93	0.73	0.57
T5-4 5" - 4" Smooth Tapered Shaft - 0.25" Wall Thickness							
12	8.07	6.44	5.24	4.34	3.64	3.10	2.66
14	6.46	5.12	4.14	3.40	2.83	2.39	2.03
16	5.17	4.05	3.24	2.64	2.17	1.81	1.52
18	4.09	3.16	2.49	1.99	1.61	1.32	1.09
20	3.17	2.40	1.84	1.44	1.13	0.90	0.71

* Consult factory for other heights.

** All EPA values expressed in square feet.



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1521LED OMEGA SERIES

LED

EPA
.72 (ft²)
WEIGHT
27 LBS

7 YEAR
WARRANTY

LUMEN
RANGE
2,640 to
10,090

LIFE SPAN
L70
MINIMUM
100,000
HOURS

UL
LISTED

CLICK
FOR FAQ's

RATED
IP66

JOB NAME

FIXTURE TYPE

MEMO

BUILD A PART NUMBER

ORDERING EXAMPLE: **2A-1521LED-R-12L40T3-MDLO18-SV2-HSHB/CA4/4212P4/RCC/BKT**

Mounting Config.	Fixture	Shade Edge	LED	CCT	Distribution Type	Driver	Lens	Optional Control Receptacle	Optional Control	Option Fuse	Option Hang-straight	Option Term. Block	Option House Side Shield	Arm See Arm Spec Sheets	Pole See Pole Spec Sheets	Finish

Mounting Configuration

(Click here to link to mounting configuration specification page)

- 1W • 2A90 • 4A • SH44¹
- 1A • 3A • 1AM • CH44¹
- 2A • 3A90 • 2AM • CAT

W = Wall Mount A = Arm Mount AM = Arm Mid-Mount
SH = Stem Hung CH = Chain Hung CAT = Catenary

¹Include overall drop length in inches after designation for Stem/Chain application (IE: CH44-48")

Fixture

- 1521LED • 1521LED SM

Shade Edge

- R (Round Edge) • F (Flared Edge)

LED

- 24L • 16L • 12L

CCT - Color Temperature (K)

- 27(00) • 30(00) • 35(00)
- 40(00) • 50(00)

Distribution Type

- T2 • T3 • T4 • T5

Driver

- MDLO18² (120V-277V, 180mA)
- MDHO18² (347V-480V, 180mA)
- MDLO14³ (120V-277V, 140mA)
- MDHO14³ (347V-480V, 140mA)
- MDLO08⁴ (120V-277V, 80mA)
- MDHO08⁴ (347V-480V, 80mA)

²16L or 12L system only

³24L or 12L system only

⁴12L system only

Lens

- FG (Flat Glass)
- SG (Sag Glass)
- FSG (Frosted Sag Glass)
- SV1 (Flat Soft Vue Light Diffused Acrylic)
- SV2 (Flat Soft Vue Moderate Diffused Acrylic)
- SV4 (Flat Soft Vue Maximum Diffused Acrylic)

Options (Click here to view accessories sheet)

- R7⁵ 7-Pin control receptacle only
- PE^{5,6} Twist-Lock Photocontrol (120V-277V)

- PE^{5,6} Twist-Lock Photocontrol (347V)
- PE^{4,5,6} Twist-Lock Photocontrol (480V)
- SC^{5,6} Shorting Cap
- PEC Electronic Button Photocontrol (120V-277V)
- PEC4 Electronic Button Photocontrol (480V)
- FHD⁷ Double Fuse and Holder
- HSHS⁸ Standard Horizontal Hangstraight, Spike Finial
- HSHN⁸ Standard Horizontal Hangstraight, No Finial
- HSHB⁸ Standard Horizontal Hangstraight, Ball Finial
- EZ⁸ Vertical Hangstraight, Large, "EZ" Mount
- TB Terminal Block
- HSS 120° House Side Shield
- BLOC Back Light Optical Control

⁵ Only available with HSH_{...} & SM.

⁶ Requires R7 control receptacle.

⁷ Ships loose for installation in base.

⁸ Not for use with 1521LED SM.

Arm (Click here to link to arm specification page)

See Arms & Wall Brackets specification sheets.

- CA • CSA • FFA • CAS
- DAG • R2⁹ • RA

⁹ Luminaires above grade height to be 2' higher than pole height, REQUIRES "EZ" hangstraight.

Pole (Click here to link to pole specification page)

See Pole specification sheets.

Finish

Standard Urban Finishes (Click here to view paint finish sheet)

- UGMT Gun Metal Textured
- UGM Gun Metal Matte
- UBT Urban Bronze Textured
- UB Urban Bronze Matte
- USLT Urban Silver Textured
- USL Urban Silver Matte
- UWHT Urban White Textured
- UWH Urban White Matte
- BKT Black Textured

Custom Urban Finishes¹⁰

- CM Custom Match

¹⁰ Smooth finishes are available upon request.

Specifications

Fixture

The 1521LED Omega series is small scale, decorative downlight fixture with a spun aluminum bell styled dome. The dome is available with two types of shades: round edge (R) and flared edge (F) styles. The luminaire measures 21" outside diameter and 17" overall height. The luminaire is U.L. listed in U.S. and Canada.

LEDs

The luminaire shall use high output, high brightness LEDs. They shall be mounted in arrays, on printed circuit boards designed to maximize heat transfer to the heat sink surface. The arrays shall be roof mounted to minimize up-light. The LEDs and printed circuit boards shall be 100% recyclable; they shall also be protected from moisture and corrosion by a conformal coating. They shall not contain lead, mercury or any other hazardous substances and shall be RoHS compliant. The LED life rating data shall be determined in accordance with IESNA LM-80. The High Performance white LEDs will have a life expectancy of approximately 100,000 hours with not less than 70% of original brightness (lumen maintenance), rated at 25°C. The High Brightness, High Output LEDs shall be 5000K (4500K, 3000K, 3500K or 2700K option) color temperature with a minimum CRI of 70. Consult factory for custom color CCT. The luminaire shall have a minimum _____ (see table) delivered initial lumen rating when operated at steady state with an average ambient temperature of 25°C (77°F).

Optics

The luminaire shall be provided with refractor type optics applied to each LED array. The luminaire shall provide Type ____ (2, 3, 4 or 5) light distribution per the IESNA classifications. Testing shall be done in accordance with IESNA LM-79.

BLOC Optic: An optional "Back Light Optical Control" shield can be provided at the factory. This is an internal optic level "House Side Shield" offering significantly reduced backlight

See next page



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1521LED OMEGA SERIES

LED

and glare while maintaining the original design aesthetics of the luminaire.

Electronic Drivers

The LED driver shall be U.L. Recognized. It shall be securely mounted inside the fixture, for optimized performance and longevity. It shall be supplied with a quick-disconnect electrical connector on the power supply, providing easy power connections and fixture installation. It shall have overload, overheat and short circuit protection, and have a DC voltage output, constant current design, 50/60HZ. It shall be supplied with line-ground, line-neutral and neutral-ground electrical surge protection in accordance with IEEE/ANSI C62.41.2 guidelines. It shall be a high efficiency driver with a THD less than 20% and a high power factor greater than .9. It shall be dimming capable using a 0-10v signal, consult factory for more information.

Photocontrols

Button Style: The photocontrol shall be mounted on the fixture and pre-wired to driver. The electronic button type photocontrol is instant on with a 5-10 second turn off, and shall turn on at 1.5 footcandles with a turn-off at 2-3 footcandles. Photocontrol is 120-277 volt and warranted for 6 years. This option removes the current IP rating. See pole spec sheet for pole mounted version.

Twist-Lock Style: The photocontrol shall be mounted externally on the fixture (1521LEDSM), or mounted on the hang-straight, and pre-wired to driver. The twist lock type photocontrol is instant on with a 3-6 second turn off, and shall turn on at 1.5 footcandles with a turn-off at 2-3 footcandles. Photocontrol is 120-277 volt and warranted for 6 years.

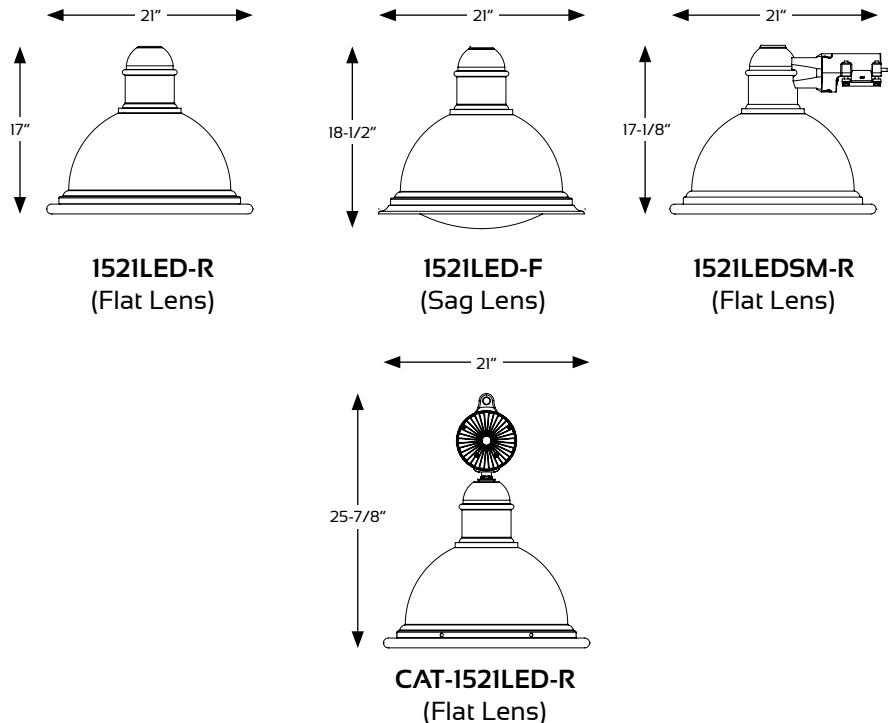
Warranty

Seven-year limited warranty. See product and finish warranty guide for details.

Finish

Refer to website for details.

Fixtures



Options

R7-PE



Twist-lock
receptacle
with photocell

PEC



Electronic
button
photocell

FHD Fuse holder



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1521LED OMEGA SERIES

LED

Performance (Based on FG Lens)

MODEL #	T2 LUMENS	BUG	EFFICACY (LPW)	T3 LUMENS	BUG	EFFICACY (LPW)	T4 LUMENS	BUG	EFFICACY (LPW)	T5 LUMENS	BUG	EFFICACY (LPW)	WATTS
24L40T_-MDL014	10735	B2U0G2	119.3	10800	B2U0G2	120.0	9945	B2U0G2	110.5	10680	B3U0G1	118.7	90
24L30T_-MDL014	10235	B2U0G2	113.7	10295	B2U0G2	114.4	9480	B2U0G2	105.3	10185	B3U0G1	113.2	90
24L27T_-MDL014	9255	B2U0G2	102.8	9310	B2U0G2	103.4	8575	B2U0G2	95.3	9205	B3U0G1	102.3	90
16L40T_-MDL018	8880	B2U0G2	111.0	8900	B2U0G2	111.3	8285	B2U0G2	103.6	8885	B3U0G1	111.1	80
16L30T_-MDL018	8465	B2U0G2	105.8	8485	B2U0G2	106.1	7900	B2U0G2	98.8	8470	B3U0G1	105.9	80
16L27T_-MDL018	7655	B2U0G2	95.7	7670	B2U0G2	95.9	7140	B1U0G1	89.3	7660	B3U0G1	95.8	80
12L40T_-MDL018	6655	B1U0G1	110.9	6705	B1U0G1	111.8	6180	B1U0G1	103.0	6625	B3U0G1	110.4	60
12L30T_-MDL018	6345	B1U0G1	105.8	6395	B1U0G1	106.6	5890	B1U0G1	98.2	6315	B3U0G1	105.3	60
12L27T_-MDL018	5735	B1U0G1	95.6	5780	B1U0G1	96.3	5330	B1U0G1	88.8	5710	B2U0G1	95.2	60
12L40T_-MDL014	5430	B1U0G1	118.0	5465	B1U0G1	118.8	5030	B1U0G1	109.3	5410	B2U0G1	117.6	46
12L30T_-MDL014	5175	B1U0G1	112.5	5210	B1U0G1	113.3	4795	B1U0G1	104.2	5160	B2U0G1	112.2	46
12L27T_-MDL014	4680	B1U0G1	101.7	4710	B1U0G1	102.4	4335	B1U0G1	94.2	4665	B2U0G1	101.4	46
12L40T_-MDL008	3285	B1U0G1	121.7	3300	B1U0G1	122.2	3040	B1U0G1	112.6	3260	B2U0G0	120.7	27
12L30T_-MDL008	3130	B1U0G1	115.9	3145	B1U0G1	116.5	2900	B1U0G1	107.4	3110	B2U0G0	115.2	27
12L27T_-MDL008	2830	B1U0G1	104.8	2845	B1U0G1	105.4	2620	B1U0G1	97.0	2810	B2U0G0	104.1	27



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arms



SternbergLighting

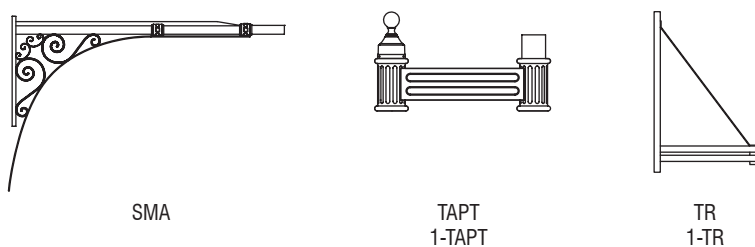
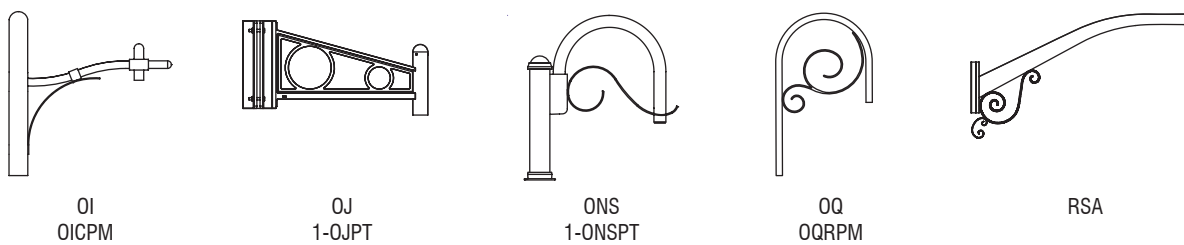
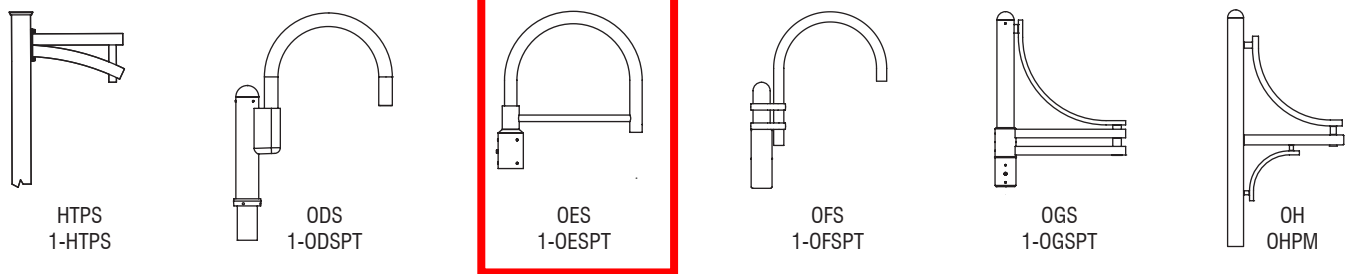
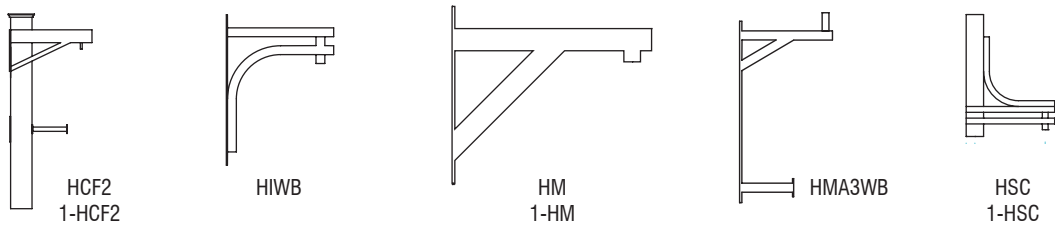
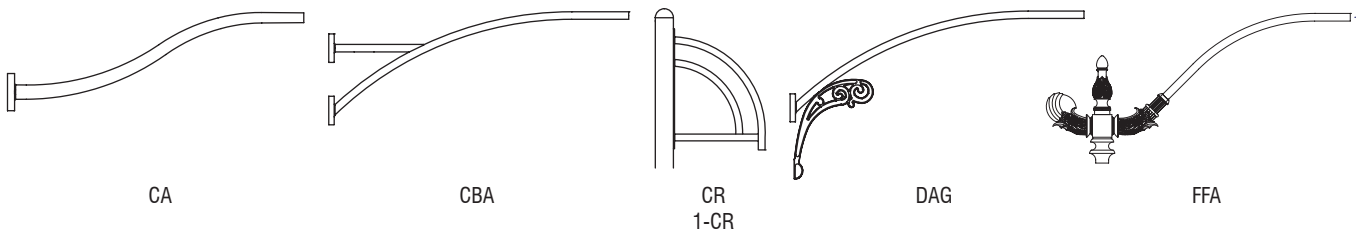
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arm matrix

Arms not shown to scale





ARM	DIMENSIONS		EPA ft ²	WT. lbs
	Width	Height		
CA	4'	2'	1.25	12
CA	6'	2'	1.75	16
CA	8'	3'	2.4	21
CBA	4'	2'	1.4	15
CBA	6'	2'-6"	1.9	19
CBA	8'	3'	2.3	22
CR / 1-CR	22-1/4"	26-3/4"	0.48	12
DAG	4'	3'-6"	2	18
DAG	6'	4'	2.6	26
FFA	4'	3'	3.7	51
FFA	6'	4'	4	54
FFA	8'	4'	4.4	56
HCF2 / 1-HCF2	19-9/16"	27-7/8"	1.2	10
HIWB	24-1/4"	34"	1.55	12
HM / 1-HM	18"	16"	0.68	22
HMA3WB	10-1/4"	20-15/16"	0.27	4
HSC / 1-HSC	30-1/2"	32-1/32"	2.24	21

ARM	DIMENSIONS		EPA ft ²	WT. lbs
	Width	Height		
HTPS / 1-HTPS	24-11/16"	14-1/16"	1.12	12
ODS / 1-ODSPT	27-3/8"	39-3/4"	0.97	20
OES / 1-OESPT	27-1/2"	30-1/4"	0.75	17
OFS / 1-OFSPT	29-15/16"	41-3/16"	0.8	18
OGS / 1-OGSPT	28"	40-3/4"	2.19	23
OH / OHPM	29-1/8"	47-7/8"	3.1	42
OI / OICPM	40-1/4"	37-1/2"	1.32	32
OJ / 1-OJPT	28-1/16"	18"	2.73	19
ONS / 1-ONSPT	27-3/8"	32-11/16"	1.31	12
OQ / OQRPM	30-3/8"	51-5/16"	1.13	18
RSA	4'	2'	3.85	9.1
RSA	6'	2'-2"	4.8	13.1
RSA	8'	3'-4"	5.5	17.1
SMA	4'	3'-6"	1.4	22
SMA	6'	3'-6"	1.85	25
TAPT / 1-TAPT	22-1/4"	12-1/2"	0.86	12
TR / 1-TR	34"	52"	2.61	11

PM = Post Mount WB = Wall Bracket PT = Post Top



SternbergLighting

ESTABLISHED 1923



LOCATED IN ROSELLE, IL

Engineered, Tested and Assembled in the USA!

Sternberg Lighting has created a legacy of old world craftsmanship that dates back to the company's inception in 1923. The work ethic and product innovations that made the early Sternberg company successful are still being practiced by our employees today. Our dedicated staff, attention to detail, and quality production processes are what make Sternberg a world class company.

Sternberg Lighting serves the municipal, landscape, higher education and commercial markets providing efficient and cost effective lighting solutions to the outdoor market.

See our complete catalog online at:

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JOB _____

TYPE _____

NOTES _____

APPROVALS _____

FEATURES

- Traditional or Performance Optics
- Bluetooth enabled RGBW accent
- Integral NEMA 3R Enclosure
- Dual receptacle power panel
- PA System capability
- IP66 optical system

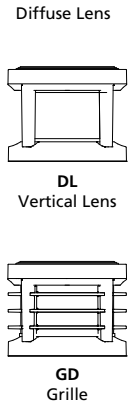
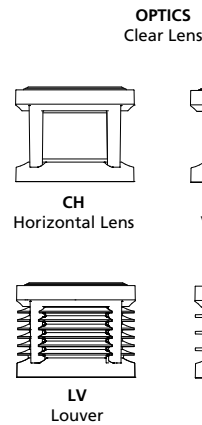
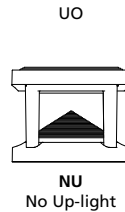
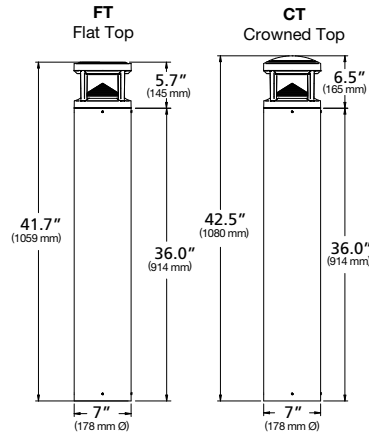
CERTIFICATIONS



IK10

NU configurations in 3000K and warmer CCTs only

SPECIFICATIONS



ORDERING CODE

Model	Top	Optics	Distribution	Light Engine
PA7R Pavilion 7" Ø Round	FT Flat Top CT ¹ Crowned Top	NU No Up-light CH Clear Horizontal Lens CL ² Clear Vertical Lens DL ^{2,3} Diffuse Vertical Lens LV Louvers GC Grille with clear vertical lens GD ³ Grille with diffuse vertical lens	1 Type I 2 Type II 3 Type III 3HS Type III + House side shield 4 Type IV 5 Type V	12L-010-5K7 14W (1000 nominal lm), 5000K, 70 CRI 12L-010-4K7 14W (1000 nominal lm), 4000K, 70 CRI 12L-010-3K7 14W (1000 nominal lm), 3000K, 70 CRI 12L-010-AMB 14W, Amber 560 nm Monochromatic 12L-020-5K7 22W (2000 nominal lm), 5000K, 70 CRI 12L-020-4K7 22W (2000 nominal lm), 4000K, 70 CRI 12L-020-3K7 22W (2000 nominal lm), 3000K, 70 CRI 12L-020-AMB 22W, Amber 560 nm Monochromatic
Body	Fixture Finish	Controls	Voltage	Options
24A 24" OAH, Aluminum 42A 42" OAH, Aluminum 42BR-C 42" OAH, Brown Concrete 42NG-C 42" OAH, Natural Gray Concrete 42A-ROP 42" OAH, Aluminum + Dual Receptacle Outlet Panel and Cover 42A-ROP-L 42" OAH, Aluminum + Dual Receptacle Outlet Panel and Locking Cover 42A-2GEB 42" OAH, Aluminum + Integral Recessed 2 Gang Electrical Box 42A-SG3 42" OAH, Aluminum + Speaker Grille Enclosure for 3" Ø speaker	BL Black DB Dark Bronze LG Light Gray GT Graphite PS Platinum Silver TT Titanium WH White RAL RAL Color CC Custom Color*	SWP ^{1,5} SiteSync pre-commission MW ⁴ Motion sensing (50% dim, 100% output upon detection) SiteSync Accessories† SWUSB SiteSync Software on USB SWTAB SiteSync Windows® Tablet SWBRG SiteSync Wireless Bridge Node	UNV 120-277V 120 ⁶ 120V 277 ⁶ 208-277V 347 ⁶ 347V 480 ⁶ 480V	EM ⁷ Battery Backup LR ⁸ Luminous Accent SF ⁹ Single Fuse DF ⁹ Double Fuse

†PRECOMMISSIONED SITESYNC ORDERING INFORMATION:
When ordering a fixture with the SiteSync lighting control option, additional information will be required to complete the order. The SiteSync Commissioning Form or alternate schedule information must be completed. This form includes Project location, Group information, and Operating schedules. For more detailed information please visit www.HubbellLighting.com/products/sitesync or contact Hubbell Lighting tech support at (800) 345-4928.

Examples:
PA7R/FT/NU3/12L-020-3K7/42A/TT/SWP/UNV/LR

- Adds .6 / 15mm to OAH (over all height).
- CL and DL configurations shall be IK04
- Only Available with 1 Type I or 5 Type V distributions only.
- 24" Ø typical coverage area, not Available with CH.
- Specify group and zone at time of order. See www.hubbelllighting.com/sitesync for more details. Order at least one SiteSync interface accessory SWUSB or SWTAB. Each option contains SiteSync License, GUI, and Bridge Node.
- Dedicated input voltage, required for MW Motions sensing.
- 20°C min starting temperature, 90+ minute run time, output equivalent to 12L-010-#K7
- Adds +5 watts and 1" / 254mm to overall height.
- SF for 120, 277 and 347 input voltage, DF for 208, 240 and 480 input voltage.

Microsoft, Encarta, MSN, and Windows are either registered trademarks or trademarks of Microsoft Corporation in the United States and/or other countries.

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LUMINAIRE PERFORMANCE

LED #	Nominal Lumen Package	Nominal Wattage	Lens Options	Distribution	3000K					4000K					5000K				
					Lumen	BUG Rating			lm/w	Lumen	BUG Rating			lm/w	Lumen	BUG Rating			lm/w
						B	U	G			B	U	G			B	U	G	
12L	1,000	14	NU UO Optics	1	749	0	0	0	54	815	0	0	0	59	835	0	0	0	60
				2	860	0	0	0	62	936	0	0	0	67	958	0	0	0	69
				3	809	0	0	0	58	881	0	0	0	63	902	0	0	0	65
				3HS	684	0	0	0	49	744	0	0	0	53	762	0	0	0	55
				4	977	0	0	0	70	1063	0	0	0	76	1089	0	0	0	78
				5	908	1	0	0	65	988	1	0	0	71	1011	1	0	0	73
			CH Clear Horizontal Lens	1	1184	0	3	1	85	1288	0	3	1	92	1319	0	3	1	95
				2	1139	0	3	1	82	1239	0	3	1	89	1269	0	3	1	91
				3	1094	0	3	1	79	1190	0	3	1	85	1219	0	3	1	87
				3HS	960	0	3	1	69	1045	0	3	1	75	1070	0	3	1	77
				4	1152	0	3	1	83	1254	0	3	1	90	1284	0	3	1	92
				5	1225	1	3	1	88	1333	1	3	1	96	1365	1	3	1	98
			CL Clear Vertical Lens	1	1146	0	3	1	82	1247	0	3	1	90	1277	0	3	1	92
				2	1228	0	3	1	88	1336	1	3	1	96	1368	1	3	1	98
				3	1276	0	3	1	92	1389	1	3	1	100	1422	1	3	1	102
				3HS	1030	0	3	1	74	1121	0	3	1	80	1148	0	3	1	82
				4	1272	0	3	1	91	1384	0	3	1	99	1417	0	3	1	102
				5	1305	1	3	1	94	1420	1	3	1	102	1454	1	3	1	104
			DL Diffused Vertical Lens	1	1086	0	3	1	78	1182	0	3	1	85	1210	0	3	1	87
				5	1141	1	3	1	82	1241	1	3	1	89	1271	1	3	1	91
			LV External Louvers	1	489	0	3	1	35	533	0	3	1	38	545	0	3	1	39
				2	534	0	3	1	38	581	0	3	1	42	595	0	3	1	43
				3	550	0	3	1	40	599	0	3	1	43	613	0	3	1	44
				3HS	397	0	3	1	29	432	0	3	1	31	442	0	3	1	32
				4	577	0	3	1	41	628	0	3	1	45	643	0	3	1	46
				5	583	1	3	1	42	634	1	3	1	46	649	1	3	1	47
			GC Grille with Clear Lens	1	843	0	3	1	61	917	0	3	1	66	939	0	3	1	67
				2	829	0	3	1	60	903	0	3	1	65	924	0	3	1	66
				3	831	0	3	1	60	905	0	3	1	65	926	0	3	1	67
				3HS	694	0	3	1	50	755	0	3	1	54	773	0	3	1	56
				4	901	0	3	1	65	980	0	3	1	70	1004	0	3	1	72
				5	842	1	3	1	60	916	1	3	1	66	938	1	3	1	67
			GD Grille with Diffused Lens		728	0	3	1	52	792	0	3	1	57	811	0	3	1	58
					782	1	3	1	56	851	1	3	1	61	872	1	3	1	63

Kim Lighting reserves the right to change specifications without notice.

LUMINAIRE PERFORMANCE

LED #	Nominal Lumen Package	Nominal Wattage	Lens Options	Distribution	3000K					4000K					5000K				
					Lumen	BUG Rating			lm/w	Lumen	BUG Rating			lm/w	Lumen	BUG Rating			lm/w
						B	U	G			B	U	G			B	U	G	
12L	2,000	22	NU UO Optics	1	1044	0	0	0	48	1136	0	0	0	52	1164	0	0	0	54
				2	1199	0	0	0	55	1305	0	0	0	60	1336	0	0	0	62
				3	1128	0	0	1	52	1228	0	0	1	57	1257	0	0	1	58
				3HS	953	0	0	0	44	1037	0	0	0	48	1062	0	0	1	49
				4	1362	0	0	0	63	1482	0	0	1	68	1518	0	0	1	70
				5	1265	1	0	0	58	1377	1	0	0	63	1410	1	0	0	65
			CH Clear Horizontal Lens	1	1778	0	3	1	82	1935	0	3	1	89	1981	0	3	1	91
				2	1711	1	3	1	79	1862	1	3	1	86	1906	1	3	1	88
				3	1643	1	3	1	76	1788	1	3	1	82	1831	1	3	1	84
				3HS	1443	0	3	1	66	1570	0	3	1	72	1608	0	3	1	74
				4	1731	0	3	1	80	1884	0	3	1	87	1929	0	3	1	89
				5	1841	1	3	1	85	2003	1	3	1	92	2051	1	3	1	95
			CL Clear Vertical Lens	1	1852	0	4	1	85	2016	1	4	1	93	2064	1	4	1	95
				2	1984	1	3	1	91	2159	1	3	1	99	2211	1	3	1	102
				3	2062	1	3	1	95	2244	1	3	1	103	2298	1	3	1	106
				3HS	1665	0	3	1	77	1811	0	3	1	83	1855	0	3	1	85
				4	2055	0	3	1	95	2236	1	3	1	103	2290	1	3	1	106
				5	2109	1	3	1	97	2295	1	3	1	106	2350	1	3	1	108
			DL Diffused Vertical Lens	1	1639	1	3	2	76	1783	1	3	2	82	1826	1	3	2	84
				5	1721	1	3	2	79	1873	1	3	2	86	1918	1	3	2	88
			LV External Louvers	1	746	0	3	1	34	811	1	3	1	37	831	1	3	1	38
				2	814	1	3	1	37	885	1	3	1	41	907	1	3	1	42
				3	838	1	3	1	39	912	1	3	1	42	934	1	3	1	43
				3HS	605	0	3	1	28	658	0	3	1	30	674	0	3	1	31
				4	879	0	3	1	41	956	1	3	1	44	979	1	3	1	45
				5	888	1	3	1	41	966	1	3	1	45	989	1	3	1	46
			GC Grille with Clear Lens	1	1038	0	3	1	48	1130	0	3	1	52	1157	0	3	1	53
				2	1021	0	3	1	47	1111	1	3	1	51	1138	1	3	1	52
				3	1024	0	3	1	47	1114	1	3	1	51	1141	1	3	1	53
				3HS	854	0	3	1	39	930	0	3	1	43	952	0	3	1	44
				4	1109	0	3	1	51	1207	0	3	1	56	1236	0	3	1	57
				5	1037	1	3	1	48	1128	1	3	1	52	1155	1	3	1	53
			GD Grille with Diffused Lens		1036	0	3	1	48	1127	1	3	2	52	1154	1	3	2	53
					953	1	3	1	44	1037	1	3	1	48	1062	1	3	1	49

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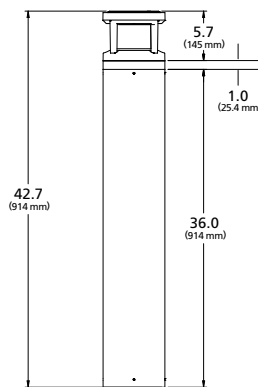
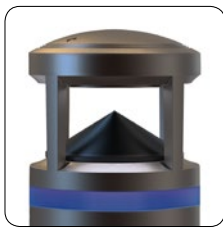
LUMINAIRE PERFORMANCE

Electrical Characteristics												Dimming				
Current	System Watts	Line Voltage		Amps AC						Min. Power Factor	Max THD (%)	Dimming Range	Source current out		Absolute voltage	
		VAC	Hz	120	208	240	277	347	480				Min	Max	Min	Max
350mA	14	120-480	50/60	0.18	0.11	0.09	0.08	0.06	0.05	>0.9	20	10% to 100%	0mA	1mA	0V	10V
500mA	22			0.12	0.07	0.06	0.05	0.04	0.03							

TM-21 LIFETIME CALCULATION

Operating Hours						
Ambient Temp.	0	25,000	TM-21-11 36,000	50,000	100,000	Calculated L ₇₀ (hours)
25°C / 77°F	100%	98%	97%	95%	90%	'60khrs
40°C / 104°F	100%	98%	97%	95%	90%	

LUMINOUS ACCENT



The Luminous Accent option adds an additional 1" / 25.4mm to the overall fixture height and may be controlled via wired DMX RDM or Bluetooth wireless. The Luminous Accent shall be IK08.

HUBBELL LIGHTING RGBW REMOTE APP

The Hubbell Lighting RGBW Remote application may be downloaded free of charge from the Apple App Store or Google Play.

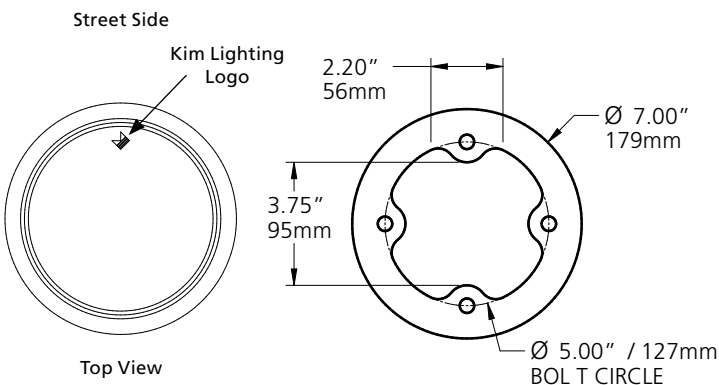
- Color selection and adjustment.
- Camera function for color matching.
- Intensity slider for dimming/ramping up.
- Save and rename up to 10 presets.
- Group and rename fixtures.
- Fixture is password protected, refer to instructions to set unique password.



MOUNTING

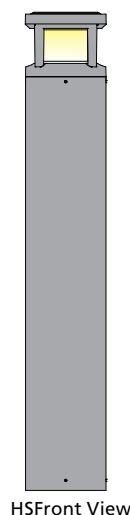
Aluminum Body

Once attached to base mounting plate, fixture may be rotated 20° in either direction and secured with set screws at base of the bollard body. KIM Lighting logo indicates 'street side' output.



SHIELDING

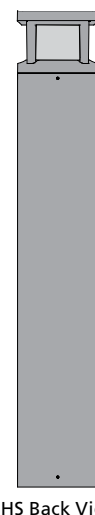
HS configurations feature factory installed 180° shield(s) that may also be installed in the field for any Optic configuration.



HS Front View



HS Side View

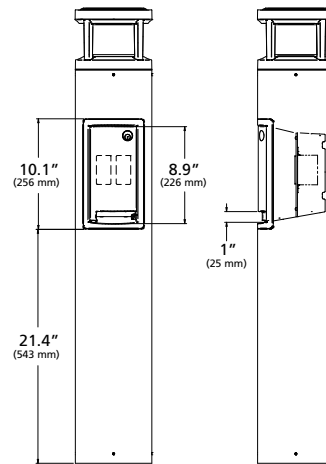


HS Back View

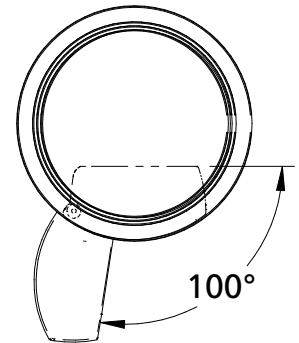
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RECEPTACLE OUTLET PANEL

The Receptacle outlet panel shall be NEMA 3R rated for wet location(s) while in use and shall be compatible with any single receptacle outlet device with standard mounting holes. Door shall be self-closing. Tamper resistant lock must be specified at time of order. Devices and device wiring by others.



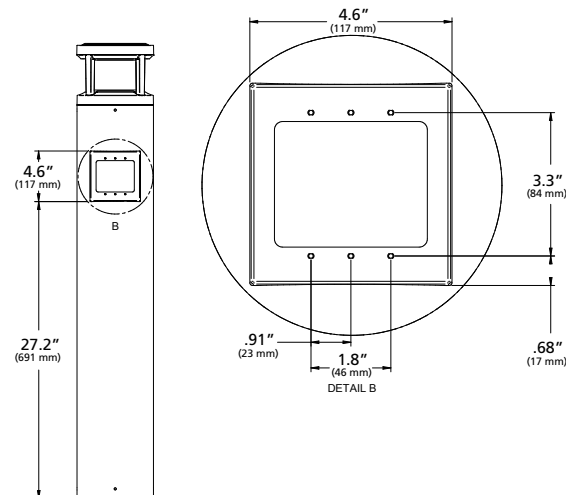
Range of Motion for access door



HUBBELL WIRING DEVICES USB DEVICES: <http://ecatalog.hubbell-wiring.com/productinformation/viewcatalog.aspx?Dest=hubbell-wiring.com/press/catalog/A.pdf&Page=18>

INTEGRAL ELECTRICAL BOX

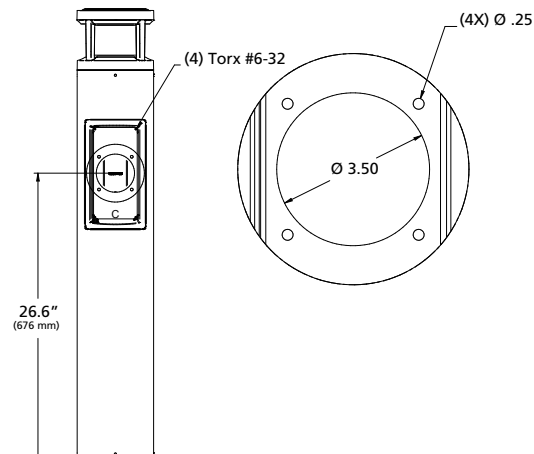
The integral 2 Gang electrical box shall be # deep and have standard mounting holes for installing either a single receptacle outlet device or a pair of single receptacle outlet device. Devices, device wiring, device hardware and bezel by others.



HUBBELL WIRING DEVICES GFCI DEVICES: <http://ecatalog.hubbell-wiring.com/productinformation/viewcatalog.aspx?Dest=hubbell-wiring.com/press/catalog/K.pdf>

SPEAKER GRILLE ENCLOSURE

The speaker grille enclosure shall accommodate a 3"Ø marine grade speaker rated for outdoor use. Grille shall be secured with (4) Torx # screws for accessibility. Mounting provisions as shown. Speaker, mounting hardware and wiring by others.



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SPECIFICATIONS

Housing:

- Castings shall be low copper aluminum alloy die-cast.
- Gaskets shall be molded silicone to prevent harmful ingress to the lamp and driver compartments.
- Optical system shall be IP66 rated.

Shaft:

- Aluminum shaft(s) shall be .125" thick extruded aluminum 6061 alloy.
- Concrete shaft(s) shall conform to current specifications for "Portland Cement." ASTM C150, Type I or II. Aggregates shall meet current requirements of "Specifications for Concrete Aggregates," ASTM C33. Water shall be clean and free from deleterious amounts of silt, oil, acids, alkalies or organic materials. Wire for reinforcement shall conform to ASTM A185. Steel for lugs and plates shall conform to ASTM A36, or A283 grade D.
- Concrete shaft(s) shall be medium sand-blasted with anti-graffiti sealer and material color shall be integral to the concrete mix.
- Concrete shaft(s) shall be cured to allow for completion of the hydration process, and result in a 28 day compressive strength of not less than 4,500 psi.
- Concrete shaft(s) shall be cast from fiberglass molds used to insure uniform parts. Mold parting lines maybe slightly visible in finished parts.

Optics:

- U0 configurations shall have an optically clear flat tempered glass lens, all other configurations shall have either an optically clear or high transmission diffused acrylic lens.
- Maximum surge current shall be 20KA and shall fail open at end of life to prevent harm to the electrical components from uncontrolled or excess incoming power
- Driver shall be rated for -40°C environment

Bluetooth:

- The Integral module shall enable the adjustment of the Luminous Accent to dim or change color to the desired setting when paired with Hubbell Lighting RGBW Remote App via cellular/tablet device.
- The integral module shall be compatible with Bluetooth Low Energy (BLE) or Bluetooth Smart mobile devices operating on iOS8 or Android Gingerbread operating systems or newer.

DMX:

- 6 wires: Red (DMX+), Yellow (DMX-), Brown (DMX Ground), Black (Voltage), White (common), and Green (Ground)
- Single DMX universe with six slots/addresses of virtual control which are pre-programmed at the factory:

DMX slot/address 1 = red

DMX slot/address 2 = blue

DMX slot/address 3 = green

DMX slot/address 4 = white

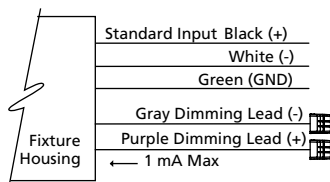
DMX slot/address 5 = X tilt, a 127 means do nothing, a value between 1-63 means left, a value between 192-254 means right

DMX slot/address 6 = Y tilt, a 127 means do nothing, a value between 1-63 means up, a value between 192-254 means down

- Fully DMX RDM compatible.

Controls

- Standard fixtures dimming range shall be from 10% to 100% and be compatible with 0-10V, user-defined, control devices. (Wiring illustration)
- Optional motion sensor shall be capable of detecting motion 360° around the bollard. When no motion is detected for the specified time, the sensor wattage to factory preset level, reducing the light level accordingly. When motion is detected by the sensor, the bollard shall return to full wattage and full light output. Please contact KIM Lighting if project requirements vary from standard configuration.
- Pavilion may be specified with SiteSync™ wireless control system for reduction in energy and maintenance costs while optimizing light quality 24/7. For more details, see ordering information or visit: www.hubbellighting.com/sitesync



Electrical:

- High temperature fuse holders factory installed inside the fixture housing. Fuse is included.
- Battery Backup shall have an operating range of -20°C to 50°C with a typical recharge time of 32 hours.

Installation

- Aluminum shaft configurations shall have four 3/8" x 10" x 2" zinc plated L-hook anchor bolts

shall to be installed with an included template. Nuts and washers shall be provided to level and secure the mounting plate to the anchor bolts.

- Aluminum shaft configurations shall have a mounting plate shall be able to be rotated 20° in either direction during installation for aiming adjustment.
- Concrete shaft configurations shall have four steel mounting tabs for installation on four 1/2" x 10" + 2" zinc electroplated L-hook anchor bolts. Each anchor bolt is supplied with two nuts, two washers, and a rigid pressed board template.
- Concrete shaft configurations shall be palletized with adequate hold-downs to prevent load movement in transit.

Finish:

- Fade and abrasion resistant, electrostatically applied, thermally cured, triglycidal isocyanurate (TGIC) polyester powder coat.
- Standard colors include (BL) Black, (DB) Dark Bronze, (GT) Graphite, (PS) Platinum Silver, (LG) Light Gray, (TT) Titanium, (WH) White, and (CC) Custom Color (Include RAL#).

Certifications and Listings:

- UL 1598 Standard for wet locations for Luminaires.
- UL 8750 Standard for Safety for Light Emitting Diode (LED) Equipment for use in Lighting Products.
- CSA C22.2#250.0 Luminaires.
- RoHS compliant.
- IP66 rated.
- IDA approved, 3000K and warmer CCTs only.

Caution:

- Fixtures must be grounded in accordance with national, state and/or local electrical codes, Failure to do so may result in serious personal injury.

Warranty:

- For full warranty see <http://www.hubbellighting.com/resources/warranty>

Bluetooth® DMX



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Landscaping Zoning Figures

BRISTOL RHODE ISLAND ZONING REGULATIONS
ARTICLE VIII SECTION 28-251(11)A:

ALONG THE STREET FRONTAGE, A
THREE-FOOT PLANTED STRIP WITH ONE
SHADE TREE FOR EVERY 40 FEET OF
FRONTAGE SHALL BE PROVIDED.

THE TOWN OF BRISTOL SUBDIVISION AND
DEVELOPMENT REVIEW REGULATIONS
RECOMMENDED TREE LIST INCLUDES BUR OAK
AND JAPANESE ZELKOVA.

LEGEND:

- PROPERTY LINE
- PROPOSED LANDSCAPE AREA

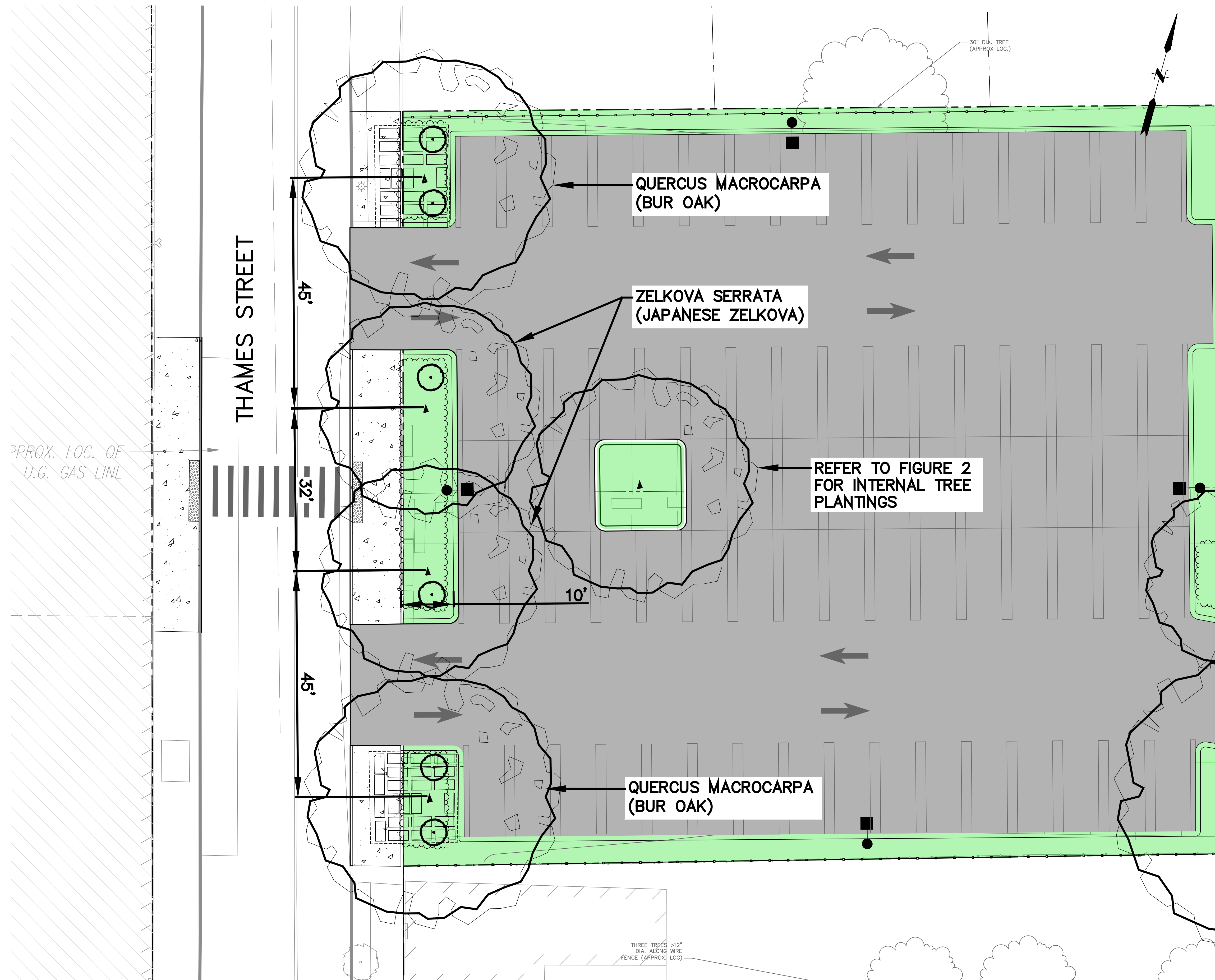


FIGURE 1: STREET FRONTAGE PLANTINGS

ALONG INTERIOR LOT LINES, A THREE-FOOT PLANTED STRIP WITH ONE SHADE TREE FOR EVERY 50 FEET OF INTERIOR LOT LINES SHALL BE PROVIDED. A HEDGE OF COMPACT EVERGREENS OR OTHER SUITABLE PLANTINGS MAY BE SUBSTITUTED FOR THE PLANTED STRIP.

THE TOWN OF BRISTOL SUBDIVISION AND DEVELOPMENT REVIEW REGULATIONS RECOMMENDED TREE LIST INCLUDES SCARLET OAK, KATSURA TREE, JAPANESE PAGODA TREE, TULIP TREE, GREEN ASH, AND WHITE ASH.

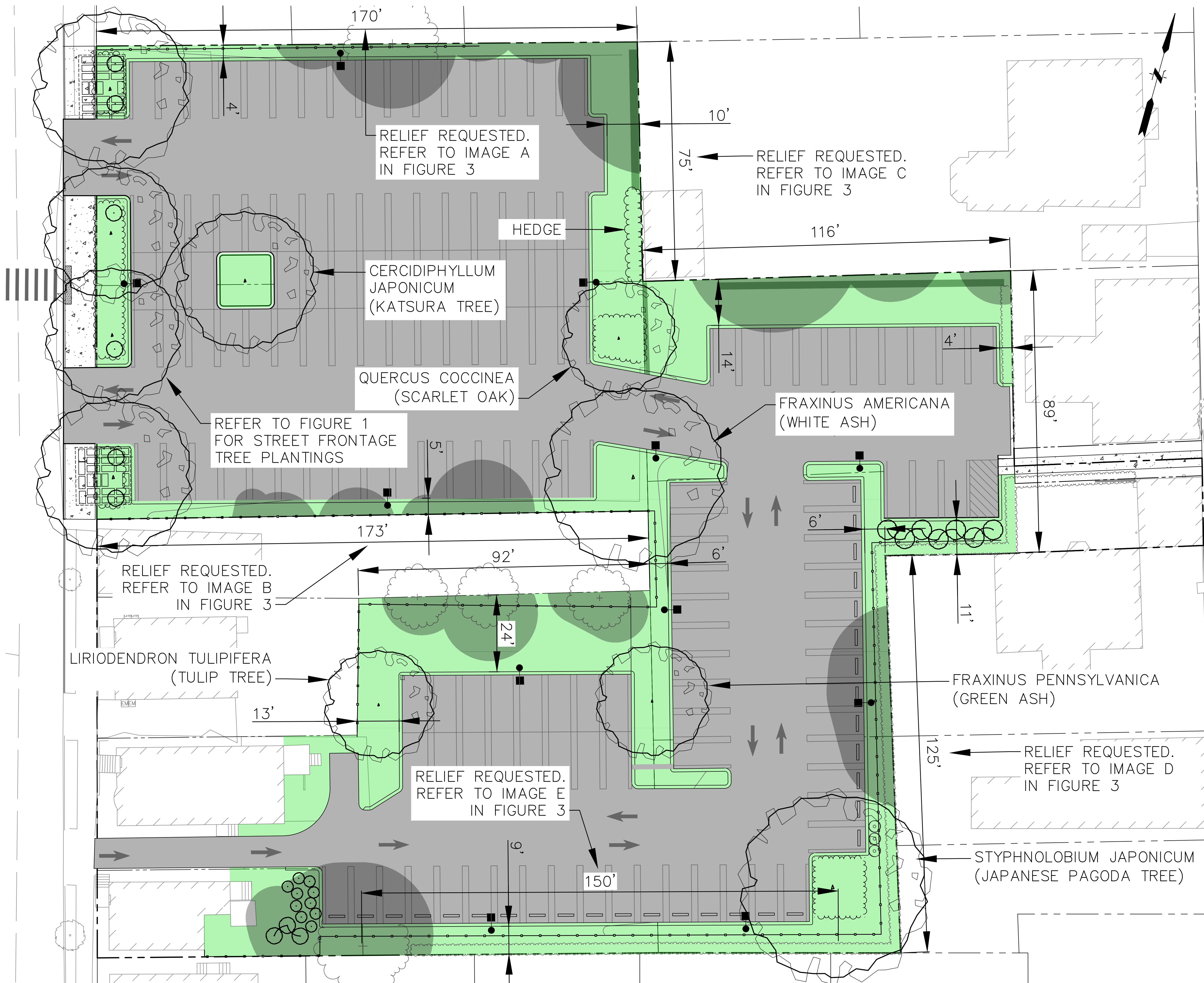


FIGURE 2: INTERIOR LOT LINE PLANTINGS

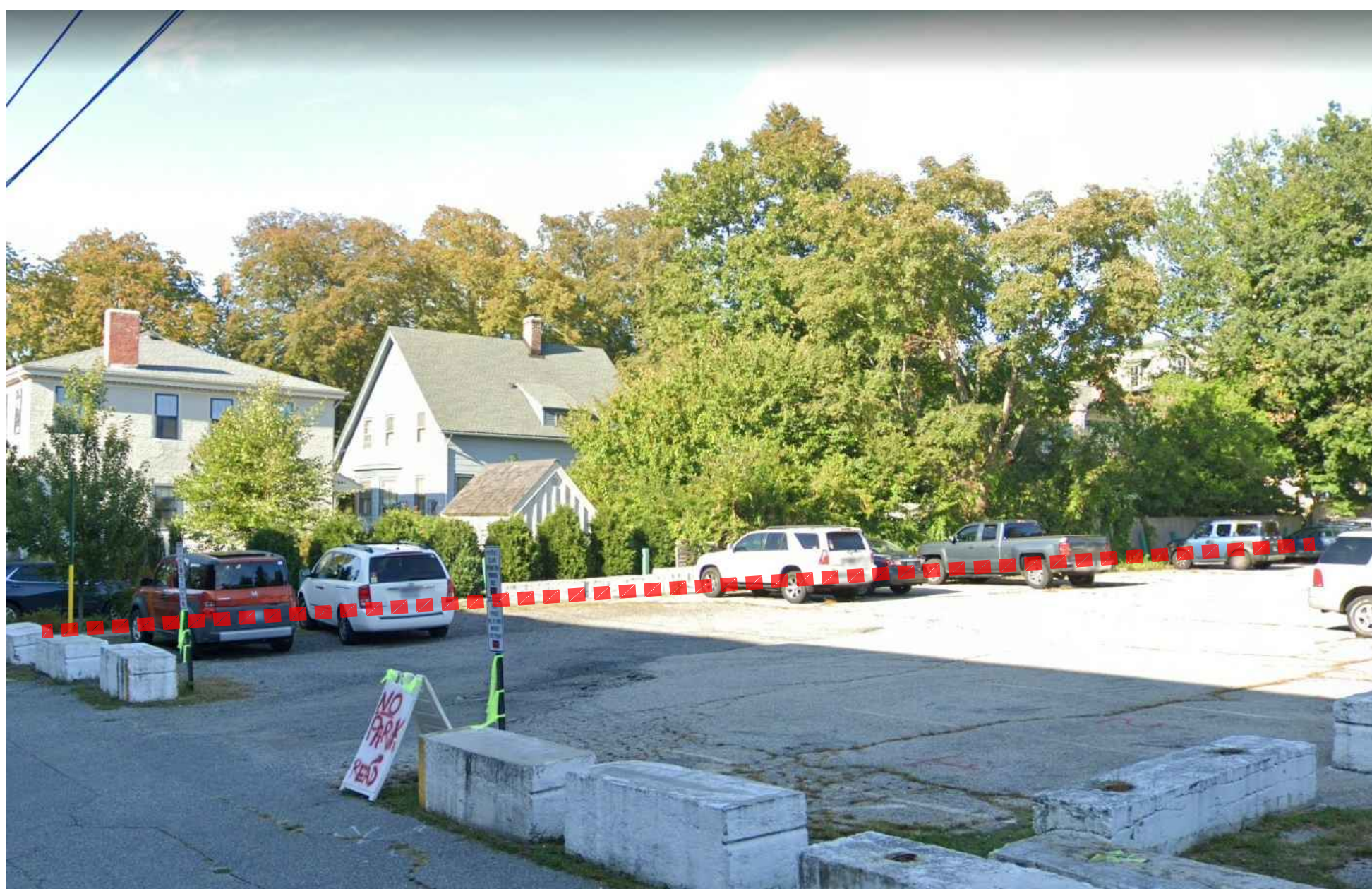


IMAGE A: PROPERTY LINE NORTH OF EXISTING PAVED AREA (ADJACENT TO LOTS 37–39)



IMAGE B: PROPERTY LINE SOUTH OF EXISTING PAVED AREA (ADJACENT TO LOT 45)

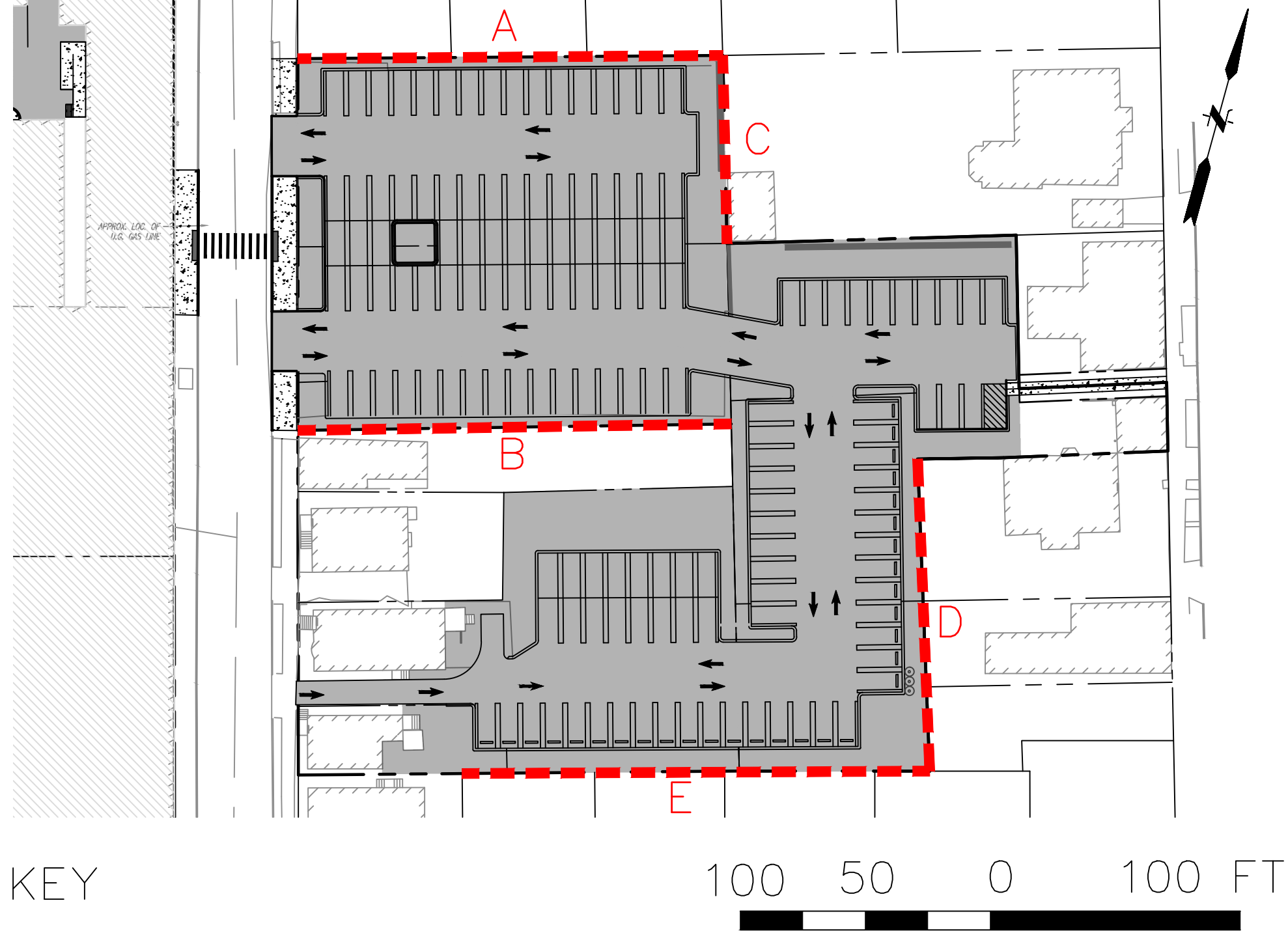


IMAGE C: PROPERTY LINE EAST OF EXISTING PAVED AREA (ADJACENT TO LOT 40)



IMAGE D: EASTERN PROPERTY LINE (ADJACENT TO LOTS 46, 75, AND 78)



IMAGE E: SOUTHERN PROPERTY LINE (ADJACENT TO LOTS 53–56)



BRISTOL RHODE ISLAND ZONING REGULATIONS ARTICLE VIII SECTION 28-251(11)C:

20% OF THE PARKING AREA SHALL BE SHADED BY DECIDUOUS TREES.

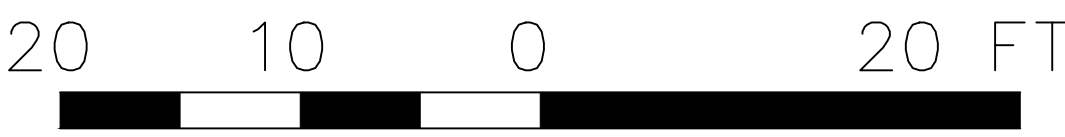
TREE CANOPY SUMMARY:

	LOT COVERAGE		PARKING AREA COVERAGE	
	(SF)	(%)	(SF)	(%)
LOT	61,135	—	—	—
PAVED PARKING AREA	41,480	67.8%	—	—
EXISTING TREE CANOPY (OFF-SITE)	3,870	6.3%	1,170	2.7%
EXISTING TREE CANOPY (ON-SITE)	3,210	5.3%	1,010	2.4%
PROPOSED TREE CANOPY (ASSUMING 75% CANOPY GROWTH)	12,850	18.4%	7,435	18.0%
PROPOSED TREE CANOPY (ASSUMING 15-FT CANOPY RADIUS)	5,980	9.8%	2,160	5.2%
TOTAL TREE CANOPY (75% GROWTH)	19,840	32.5%	9,615	23.1%
TOTAL ON-SITE TREE CANOPY (75% GROWTH, NO OFF-SITE)	16,060	26.3%	8,445	20.4%
TOTAL ON-SITE TREE CANOPY (15-FT RADIUS, NO OFF-SITE)	9,190	15.1%	3,170	7.6%

LEGEND:

- LOT BOUNDARY
- PROPOSED PAVED PARKING AREA
- PROPOSED TREE (15-FOOT RADIUS)
- PROPOSED TREE CANOPY (75% GROWTH)
- EXISTING TREE CANOPY (ON-SITE)
- EXISTING TREE CANOPY (OFF-SITE)
- TREE CANOPY OVER PARKING AREA

FIGURE 4: PARKING LOT SHADING



BRISTOL RHODE ISLAND ZONING REGULATIONS
ARTICLE VIII SECTION 28-251(11)C:

THE INTERIOR OF PARKING AREAS SHALL
HAVE A MINIMUM OF TEN SQUARE FEET OF
LANDSCAPING FOR EACH PARKING SPACE.

PARKING AND LANDSCAPING SUMMARY:

PARKING SPACES	148
REQUIRED LANDSCAPE AREA	1,480 SF
PROVIDED LANDSCAPE AREA	16,410 SF
LANDSCAPING PER PARKING SPACE	111 SF

LEGEND:

- PROPERTY LINE
- PROPOSED LANDSCAPE AREA

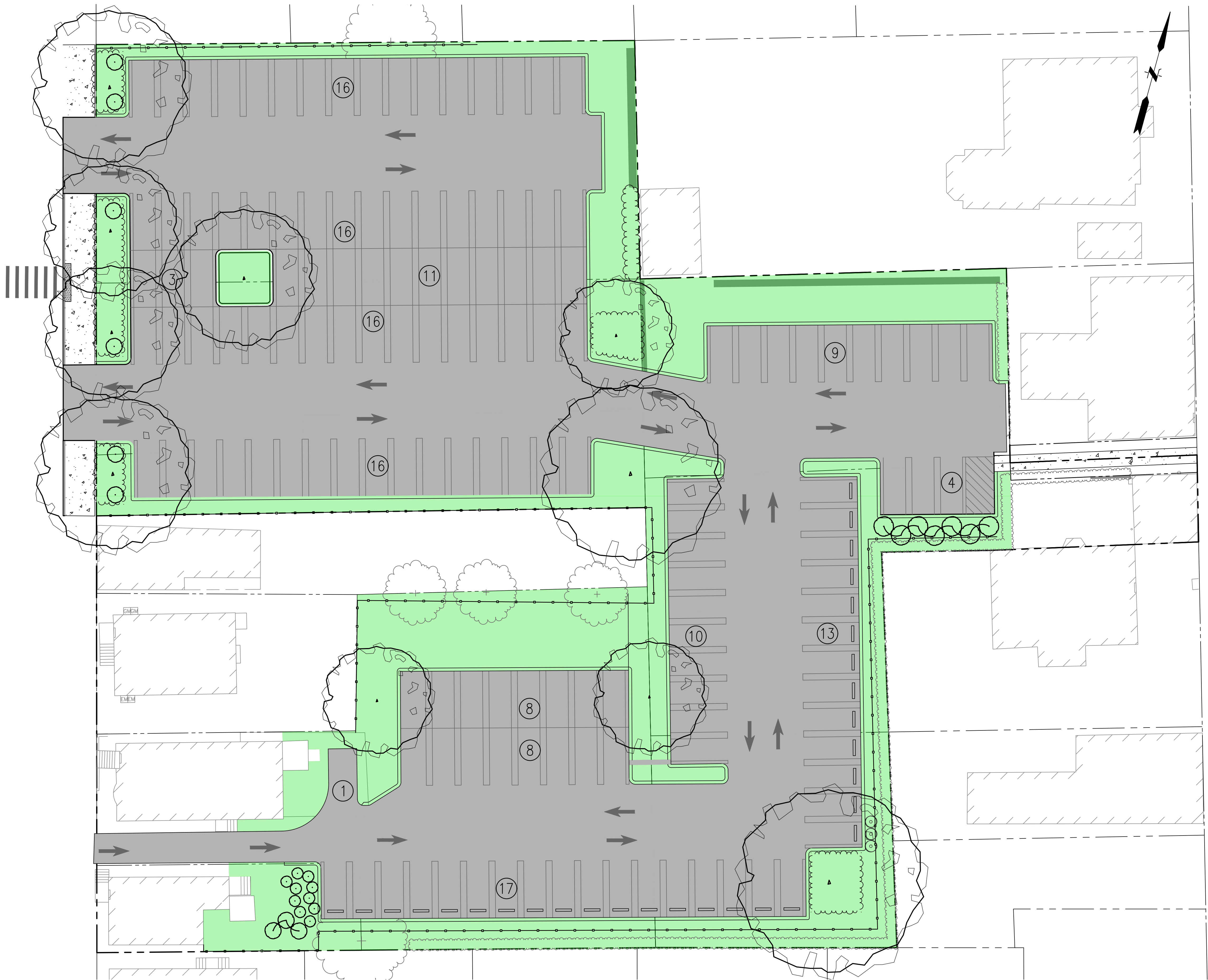


FIGURE 5: PARKING TO LANDSCAPING RATIO



Traffic Impact Study, Robin Rug Mixed-Use Development – 8/9/22



FUSS & O'NEILL

August 9, 2022

Chris Reynolds, PE
Senior Project Manager
Brady Sullivan Properties, LLC
670 North Commercial Street, Suite 303
Manchester, NH 03101

Re: Traffic Impact Study
Bristol Yarn Mill Redevelopment
Assessors Plat 10 Lots 41, 42, 43, 44, 49, 60, 61, 62, 68, 73, 74, 76, and 125 Thames Street
Bristol, RI

Dear Mr. Reynolds:

Brady Sullivan Properties (Brady Sullivan) proposes to redevelop the existing historic Bristol Yarn Mill complex at 125 Thames Street in Bristol, Rhode Island. The redevelopment will include approximately 150 residential apartment units and approximately 6,300 square feet of commercial space with both on-site and off-site parking spaces. The off-site parking is planned for lots 41, 42, 43, 44, 49, 60, 61, 62, 68, 73, 74, and 76, which jointly have frontage on Thames Street and Hope Street. The development is expected to open in 2024.

Brady Sullivan has retained Fuss & O'Neill to study the impact of the proposed development on traffic conditions throughout the adjacent roadway network. This letter has been prepared to document the findings of the study in support of the project's land use applications with the Town of Bristol.

Existing Project Site Conditions

The total project site is approximately three acres in Bristol's Waterfront Planned Unit Development District and is partially occupied by an approximately 300,000 square-foot brick building, formerly the Bristol Yarn Corporation and Robin Rug carpet store. The site is bound by Thames Street to the east, Constitution Street to the south, Bristol Fireman's Memorial Park to the north, and Bristol Harbor to the west. Access to the site is currently provided by two full access driveways: one on Constitution Street west of its intersection with Thames Street, and one that is shared with Bristol Maritime Welcome Center on the northern portion of the site.

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Corres.

Mr. Chris Reynolds, PE

August 9, 2022

Page 2

Adjacent Roadway Network

Thames Street is a north/south roadway that extends approximately 0.67 miles south from Hope Street to its terminus at Constitution Street. The roadway is classified by Rhode Island Department of Transportation (RIDOT) as a major collector roadway under local jurisdiction. The roadway is approximately 30 feet wide and provides one travel lane in each direction, on-street parking on the west side of the roadway, and sidewalks on both sides of the roadway. Dedicated bicycle facilities are not provided; however, the East Bay Bike Path is accessible from Thames Street approximately one-half mile north of the site.

Hope Street (Route 114) is a north/south roadway classified by RIDOT as a principal arterial under State jurisdiction. The roadway is approximately 30 feet wide in the vicinity of the site providing one travel lane in each direction, and on-street parking on the west side of the roadway south of Church Street. Sidewalks are located on both sides of the roadway, and dedicated bicycle facilities are not provided. Additionally, a bus stop that services the Rhode Island Public Transit Authority (RIPTA) Route 60 Bus is located approximately one-quarter mile north of the site at the intersection of Hope Street and Church Street.

Constitution Street is an east/west roadway that extends approximately 0.4 miles east from Bristol Harbor to its terminus at Wood Street. The roadway is classified by RIDOT as a major collector under local jurisdiction. The roadway is approximately 40 feet wide and carries two-way traffic. On-street parking and sidewalks are provided on both sides of the roadway, and dedicated bicycle facilities are not provided.

Study Area Intersections

The intersection of Hope Street and Constitution Street was reviewed as part of this study. This intersection is a four-legged unsignalized intersection, with Hope Street providing the northbound and southbound approaches and Constitution Street providing the eastbound and westbound approaches. All four approaches provide one travel lane shared for all movements. Constitution Street is controlled by stop signs on both approaches. Sidewalks are present on all four approaches, and crosswalks and pedestrian ramps are present on the southern, eastern, and western legs of the intersection. Dedicated bike facilities are not present at this intersection.

Traffic Volumes and Counts

The potential for traffic impact on the roadway network by the proposed development is highest during the morning and afternoon peak hours when commuter related trips are at their highest. Representatives of Fuss & O'Neill conducted morning and afternoon turning movement counts (TMC) on Thursday, July 21, 2022 at the intersection of Constitution Street and Hope Street. The

Mr. Chris Reynolds, PE

August 9, 2022

Page 3

traffic count data collected indicates that the morning peak hour of traffic is from 8:00 am to 9:00 am, and the afternoon peak hour is from 4:00 pm to 5:00 pm. These peak hours were subsequently analyzed for impacts and the existing traffic volumes for these peak hours are shown in the attached *Figure No. 1*.

Fuss & O'Neill also conducted a 48-hour automatic traffic recorder (ATR) count on Thursday, July 21, 2022 and Friday, July 22, 2022 to collect volumes and speeds. The ATR was located on Thames Street approximately 250 feet south of Church Street. Copies of the TMC and ATR traffic data are attached to this letter.

The 85th percentile speed on Thames Street was recorded at 26 miles per hour for northbound vehicles and 27 miles per hour for southbound vehicles. The 24-hour traffic volume on this roadway (total for both directions) was recorded to be 1,640 vehicles.

An annual growth rate of 0.5 percent was applied to existing traffic volumes to develop the 2032 No-Build condition, depicted in the attached *Figure No. 2*.

Proposed Conditions

The existing building on the project site is proposed to be redeveloped with approximately 150 residential apartment units and approximately 6,300 square feet of first floor commercial space. A total of 148 parking spaces are proposed on-site, and 151 parking spaces are proposed off-site. Vehicle access to the building site will be provided by a full access driveway on Thames Street and a full-access driveway on Constitution Street. Vehicle access to the off-site parking will be provided on Thames Street by two full-access driveways and one entrance only driveway.

Pedestrians may access the site from the off-site parking by sidewalks on Thames Street and a proposed midblock crosswalk, located approximately 200 feet south of Church Street.

Trip Distribution

The distribution of new vehicle traffic generated by the project that will enter or exit the project site was applied to the road network based on the existing regional traffic distributions and the layout of the adjacent roadway network. During the peak hours, the following arrival distributions of new traffic are anticipated:

- 60 percent from Thames Street north of the site
- 40 percent from Hope Street south of the site

An arrival/departure distribution for the site is depicted in the attached *Figure No. 3*.

Mr. Chris Reynolds, PE

August 9, 2022

Page 4

Trip Generation

The expected site generated vehicle traffic for the morning and afternoon peak hours was calculated from existing empirical data from the Institute of Transportation Engineers (ITE) publication Trip Generation, 11th edition, 2021. This publication is an industry-accepted resource for determining trip generation.

Trip generation for this development has been modeled using two different land use codes (LUC), one to represent the residential portion and another to represent the commercial portion of the development.

LUC 220 “Multi-Family Housing (Low-Rise)” was used to model the residential portion of the development. Based on 150 units, the residential portion of the development is expected to generate 60 vehicle trips (14 entering, 46 exiting) during the morning peak hour, and 77 vehicle trips (48 entering, 29 exiting) during the afternoon peak hour.

LUC 822 “Strip Retail Plaza (<40k)” was used to model the proposed commercial space. Although the specific land uses of this space have not yet been determined, this LUC is intended to represent a variety of possible commercial uses. Based on 6,300 square feet, the commercial portion of the development is expected to generate 15 vehicle trips (9 entering, 6 exiting) during the morning peak hour, and 42 vehicle trips (21 entering, 21 exiting) during the afternoon peak hour.

Therefore, the development may be expected to generate 75 total vehicle trips (23 entering, 52 exiting) during the morning peak hour, and 119 total vehicle trips (69 entering, 50 exiting) during the afternoon peak hour.

Mr. Chris Reynolds, PE

August 9, 2022

Page 5

Anticipated trip generation is summarized in *Table 1* below.

Table 1 – Trip Generation

	Residential (LUC 220)	Commercial (LUC 822)	TOTAL
AM Peak Hour	60	15	75
<i>Entering</i>	14	9	23
<i>Exiting</i>	46	6	52
PM Peak Hour	77	42	119
<i>Entering</i>	48	21	69
<i>Exiting</i>	29	21	50

This trip generation estimate is thought to be conservative. Based on 150 units LUC 230 “Low-Rise Residential with Ground-Floor Commercial”, 66 vehicle trips (15 entering, 51 exiting) would be expected during the morning peak hour, and 54 vehicle trips (38 entering, 16 exiting) would be expected during the afternoon peak hour. Nonetheless, to provide a conservative assessment of potential impacts, the trip generation estimate based on LUC 220 and LUC 822 was used for analysis in this study.

The total trips generated by the proposed development, depicted in the attached *Figure No. 4*, were added to the 2032 No-Build volumes to yield the 2032 Build volumes, shown in the attached *Figure No. 5*.

Traffic Capacity Analysis

Intersection Capacity Analysis

Capacity analysis for the study area intersection was conducted using Synchro Professional Software, version 11.0.

For intersection capacity analysis results, Level of Service (LOS) is used to describe the operating condition of the road or intersection. LOS is a measure of the delay experienced by stopped vehicles at an intersection. LOS is rated on a scale from A to F, with A describing a condition of very low delay (less than 10 seconds per vehicle), and F describing a condition where delays will exceed 50 seconds per vehicle for unsignalized intersections and 80 seconds per vehicle for signalized intersections. Delay is described as a measure of driver discomfort, frustration, fuel consumption, and lost travel time. Therefore, intersections with longer delay times are less acceptable to most drivers.

Mr. Chris Reynolds, PE

August 9, 2022

Page 6

For two-way stop controlled intersection capacity analyses, LOS is used to provide a description of the delay and operational characteristics of the turns from the minor street (stop sign controlled) to the major street and turns from the major street to the minor street. Through vehicles are not delayed by the minor street and do not experience delay, therefore they are not rated with a level of service.

The definition for LOS, as well as the methodology for conducting unsignalized intersection capacity analyses, are taken from the 2000 Highway Capacity Manual published by the Transportation Research Board.

The determination of the traffic impact from the proposed development is made through a comparison of the No-Build condition LOS (without the proposed development) versus the Build condition LOS (with the proposed development).

Using the above referenced methodologies, weekday morning and weekday afternoon peak hour capacity analyses were conducted at the unsignalized intersection of Hope Street and Constitution Street, as well as the off-site parking full-access driveways on Thames Street.

At the intersection of Hope Street and Constitution Street, all critical movements operate at LOS B or better during the morning peak hour under No-Build and Build conditions. During the afternoon peak hour, all critical movements operate at LOS B or better under No-Build and Build conditions, with the exception of the westbound approach, which operates at LOS C under the Build condition. Although this approach experiences a degradation in LOS, vehicles experience an increase in average delay of less than one second per vehicle.

The unsignalized driveways accessing the off-site parking on Thames Street are expected to operate at LOS A in both the morning and afternoon peak hours.

Table 2 presents a summary of the levels of service and delay under No-Build and Build conditions. Copies of the analysis worksheets for the No-Build and Build conditions of morning and afternoon peak hours have been included as attachments to this letter.

Mr. Chris Reynolds, PE

August 9, 2022

Page 7

Table 2 – Unsignalized Intersection Level of Service

Hope Street at Constitution Street				
Critical Movement	Morning Peak Hour		Afternoon Peak Hour	
	No-Build	Build	No-Build	Build
Eastbound Approach	LOS B	LOS B	LOS B	LOS B
Westbound Approach	LOS B	LOS B	LOS B	LOS C
Northbound Left	LOS A	LOS A	LOS A	LOS A
Southbound Left	LOS A	LOS A	LOS A	LOS A
Thames Street at Northern Driveway				
Critical Movement	Morning Peak Hour		Afternoon Peak Hour	
	No-Build	Build	No-Build	Build
Westbound Approach	-	LOS A	-	LOS A
Southbound Left	-	LOS A	-	LOS A
Thames Street at Southern Driveway				
Critical Movement	Morning Peak Hour		Afternoon Peak Hour	
	No-Build	Build	No-Build	Build
Westbound Approach	-	LOS A	-	LOS A
Southbound Left	-	LOS A	-	LOS A

Queue Analysis

No-Build and Build condition 95th percentile queue lengths were reviewed at the study intersection and the proposed full access driveways for the proposed development. The 95th percentile queue lengths represent the maximum queue lengths that can be expected at each of the critical approach lanes of the study area intersections. The queue lengths are provided in the attached analysis worksheets.

During the morning and afternoon peak hours, the 95th percentile queue lengths at the intersection of Hope Street and Constitution Street are not expected to exceed one vehicle length under the Build condition. Queueing is not anticipated at the site driveways. *Table 3* presents a summary of the 95th percentile queue lengths for the No-Build and Build conditions during the morning and afternoon peak hours.

Mr. Chris Reynolds, PE

August 9, 2022

Page 8

Table 3 - Peak Hour Queue Length Summary

Hope Street at Constitution Street				
Critical Movements	Morning Peak Hour		Afternoon Peak Hour	
	No-Build	Build	No-Build	Build
Eastbound Approach	5* ft	10 ft	20 ft	25 ft
Westbound Approach	5 ft	10 ft	5 ft	5 ft
Thames Street at Northern Driveway				
Critical Movements	Morning Peak Hour		Afternoon Peak Hour	
	No-Build	Build	No-Build	Build
Westbound Approach	-	0 ft	-	0 ft
Thames Street at Southern Driveway				
Critical Movements	Morning Peak Hour		Afternoon Peak Hour	
	No-Build	Build	No-Build	Build
Westbound Approach	-	0 ft	-	0 ft

*Queue lengths reported have been rounded to the nearest five feet

Traffic Safety Analysis

Intersection Sight Distance Analysis

Intersection sight distances were measured at the project site driveway locations in accordance with criteria set forth in the 2008 Rhode Island Department of Transportation (RIDOT) Highway Design Manual. This sight distance is measured from a point 15 feet back from the edge of travel-way at a height of 3.5 feet, the standard height of a driver's eye.

The speed limit posted on Constitution Street and Thames Street is 25 miles per hour. In accordance with criteria set forth in the 2008 RIDOT Highway Design Manual, 280 feet of intersection sight distance is required for a passenger car turning left out of the project site, and 240 feet of intersection sight distance is required for a passenger car turning right out of the project site.

At each of the four site egress locations, in both directions, the sight distance triangle is not obstructed and does not encroach upon abutting landowner property. The sight distances are clear to the nearest intersection and exceed RIDOT criteria for safe egress from the site.

Mr. Chris Reynolds, PE

August 9, 2022

Page 9

Crash Analysis

Crash data was obtained from the Bristol Police Department for the following areas in the vicinity of the site:

- Thames Street between Church Street and Constitution Street
- Constitution Street between Thames Street and Hope Street

Thames Street between Church Street and Constitution Street experienced eight collisions during the five-year study period, averaging approximately two crashes per year. Six of the collisions occurred with parked vehicles, one collision was an angle collision between motor vehicles, and the manner of one collision is unknown. Of the eight collisions, three resulted in property damage only and the severity of the other five collisions is unknown.

Constitution Street between Thames Street and Hope Street also experienced eight collisions during the five-year study period, averaging approximately two crashes per year. Five of the collisions occurred with parked vehicles, two were angle collisions, and one was a sideswipe collision. None of the collisions resulted in injury.

Pedestrian Safety

The crash records obtained from the Bristol Police Department do not indicate a history of pedestrian or bicycle collisions in the study area. Additionally, the measured 85th percentile vehicle speed on Thames Street is under 30 miles per hour in both directions, greatly reducing the risk of serious pedestrian injury.

The proposed development includes a painted midblock crosswalk across Thames Street located approximately 200 feet south of Church Street. This proposed crosswalk is intended to accommodate the anticipated increase in foot traffic crossing Thames Street to travel between the building and the off-site parking.

Therefore, the proposed development is not expected to adversely impact pedestrian safety.

Conclusion

The purpose of preparing a traffic impact study is to identify the impact of the proposed development's site generated traffic. The study efforts have indicated the proposed facility is expected to generate 75 vehicle trips (23 entering, 52 exiting) during the morning peak hour and 119 vehicle trips (69 entering, 50 exiting) during the afternoon peak hour.



Mr. Chris Reynolds, PE

August 9, 2022

Page 10

Results of capacity analysis indicate the most significant increase in delay experienced on any approach at the intersection of Constitution Street and Hope Street is less than one second per vehicle. No significant increases in queue length are expected between the No-Build and Build conditions throughout the study area.

A review of crash data provided by the Bristol Police Department did not indicate any abnormal crash patterns or frequencies. The development is not expected to exacerbate crash patterns or frequencies.

Sight distances at the proposed driveway locations exceed criteria set forth in the RIDOT Highway Design Manual for safe egress for the site.

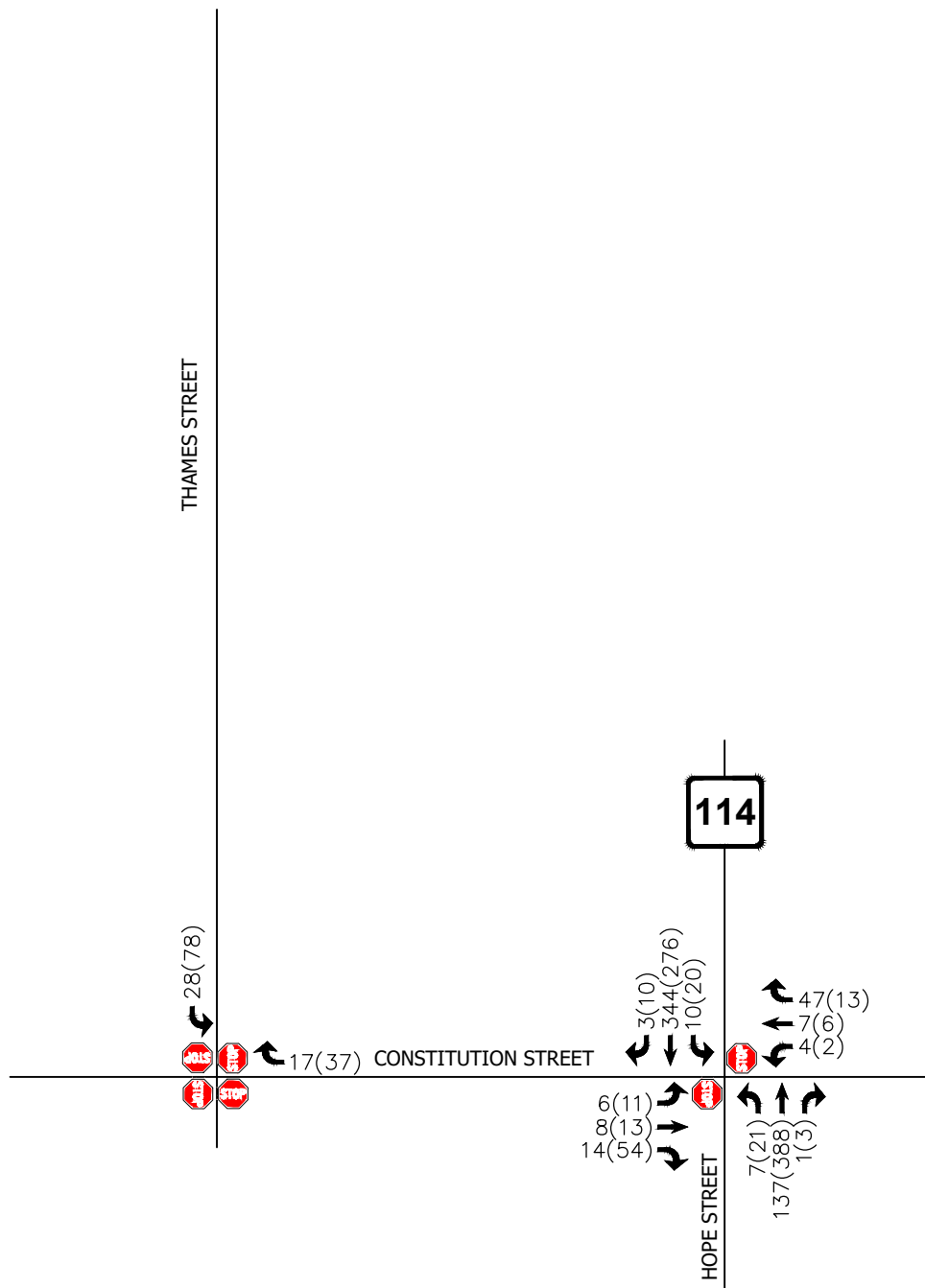
Based on the results of the foregoing analysis, it is the professional opinion of Fuss & O'Neill, Inc. that the proposed development will not have a significant impact to traffic operations within the study area. We trust that this information will be sufficient for your review; however, if you should require additional information or have any questions, please contact us.

Sincerely,

Katherine Patch
Transportation Engineer

Shawn M. Martin, PE
Senior Vice President

Enclosures: Traffic Volume Figures
 Capacity Analysis Worksheets
 Traffic Counts



xxx(xxx) = WEEKDAY MORNING PEAK HOUR (WEEKDAY PM PEAK HOUR)



FUSS & O'NEILL

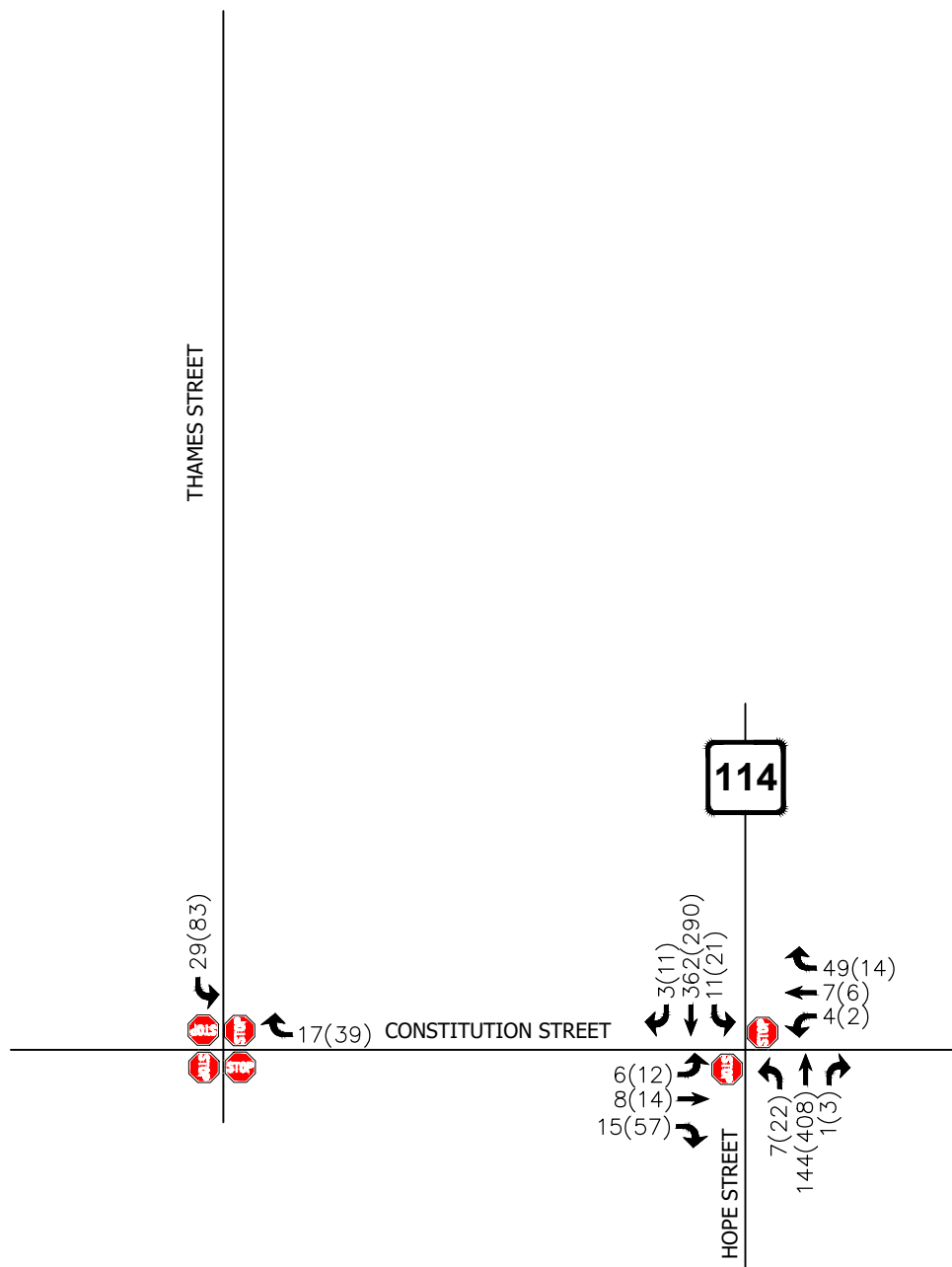
317 IRON HORSE WAY, SUITE 204
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FIGURE 1: 2022 EXISTING CONDITIONS

PROJ. NO: 20061150.A22

ROBIN RUG REDEVELOPMENT

JULY 2022



xxx(xxx) = WEEKDAY MORNING PEAK HOUR (WEEKDAY PM PEAK HOUR)



FUSS & O'NEILL

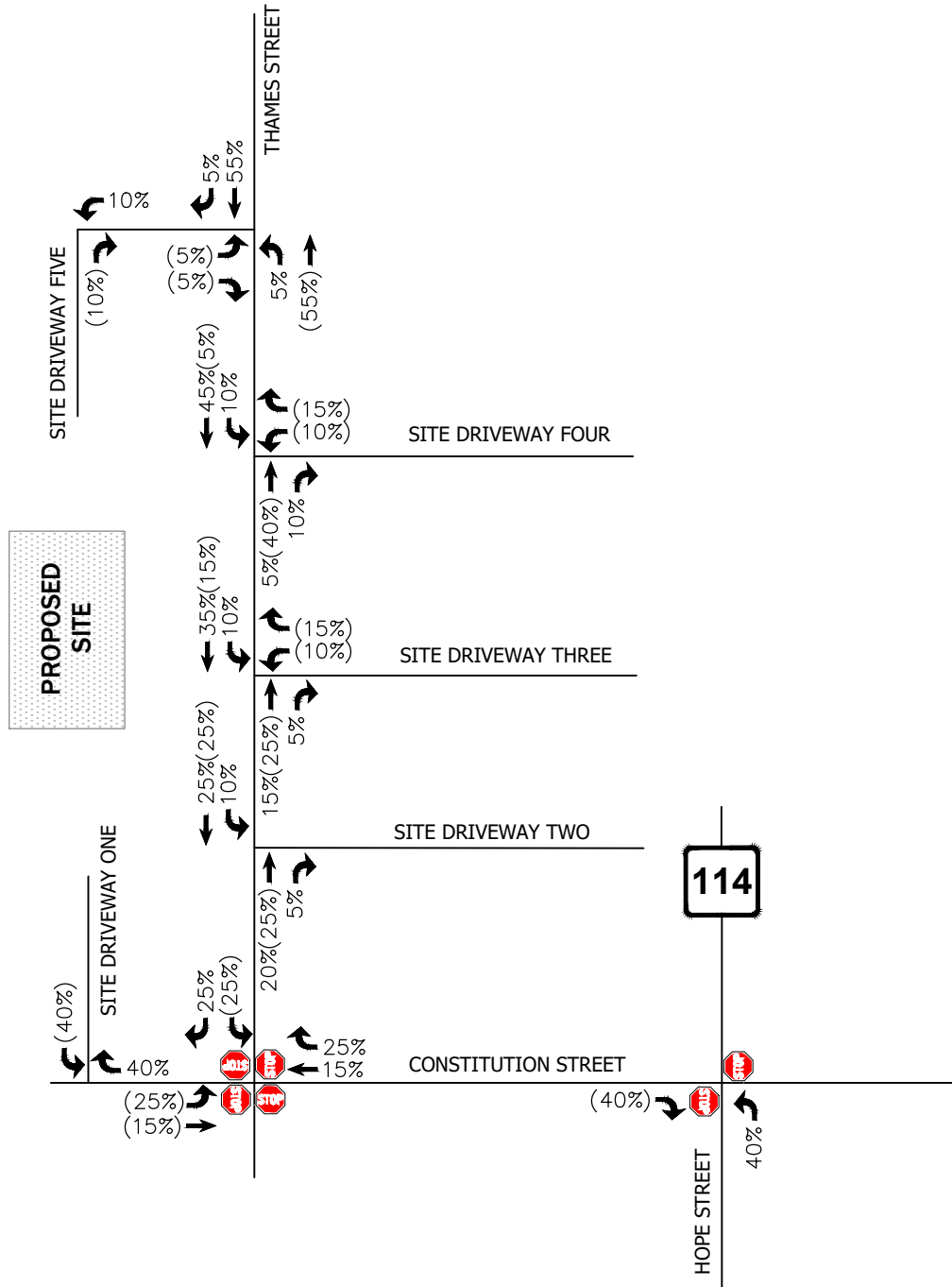
317 IRON HORSE WAY, SUITE 204
PROVIDENCE, RI 02908
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www.fando.com

FIGURE 2: 2032 NO BUILD CONDITIONS

PROJ. NO: 20061150.A22

ROBIN RUG REDEVELOPMENT

JULY 2022



XXX(XXX) = ENTERING TRAFFIC (EXITING TRAFFIC)



FUSS & O'NEILL

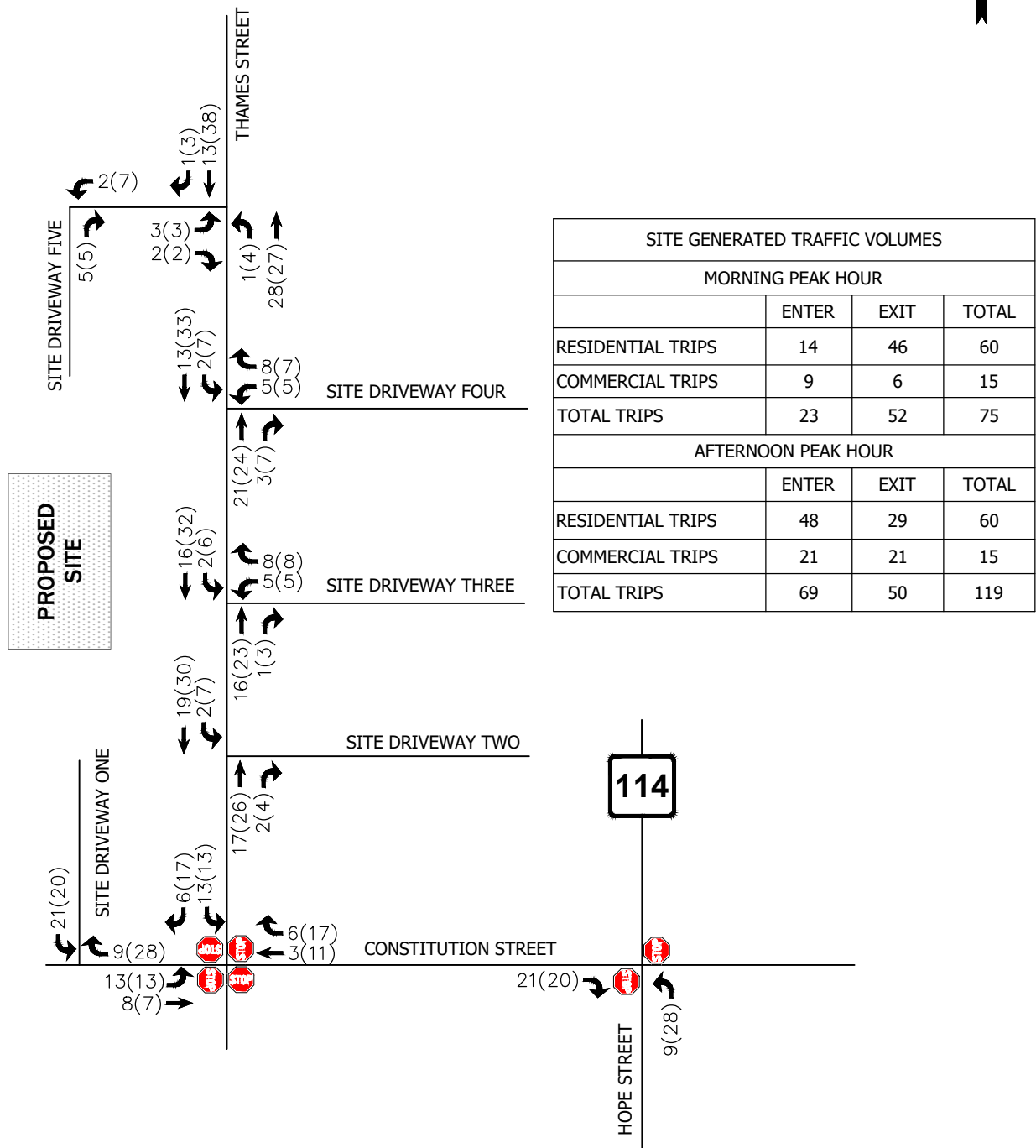
317 IRON HORSE WAY, SUITE 204
PROVIDENCE, RI 02908
401.861.3070
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FIGURE 3: TRIP DISTRIBUTION

PROJ. NO: 20061150.A22

ROBIN RUG REDEVELOPMENT

JULY 2022



xxx(XXX) = WEEKDAY MORNING PEAK HOUR (WEEKDAY PM PEAK HOUR)



FUSS & O'NEILL

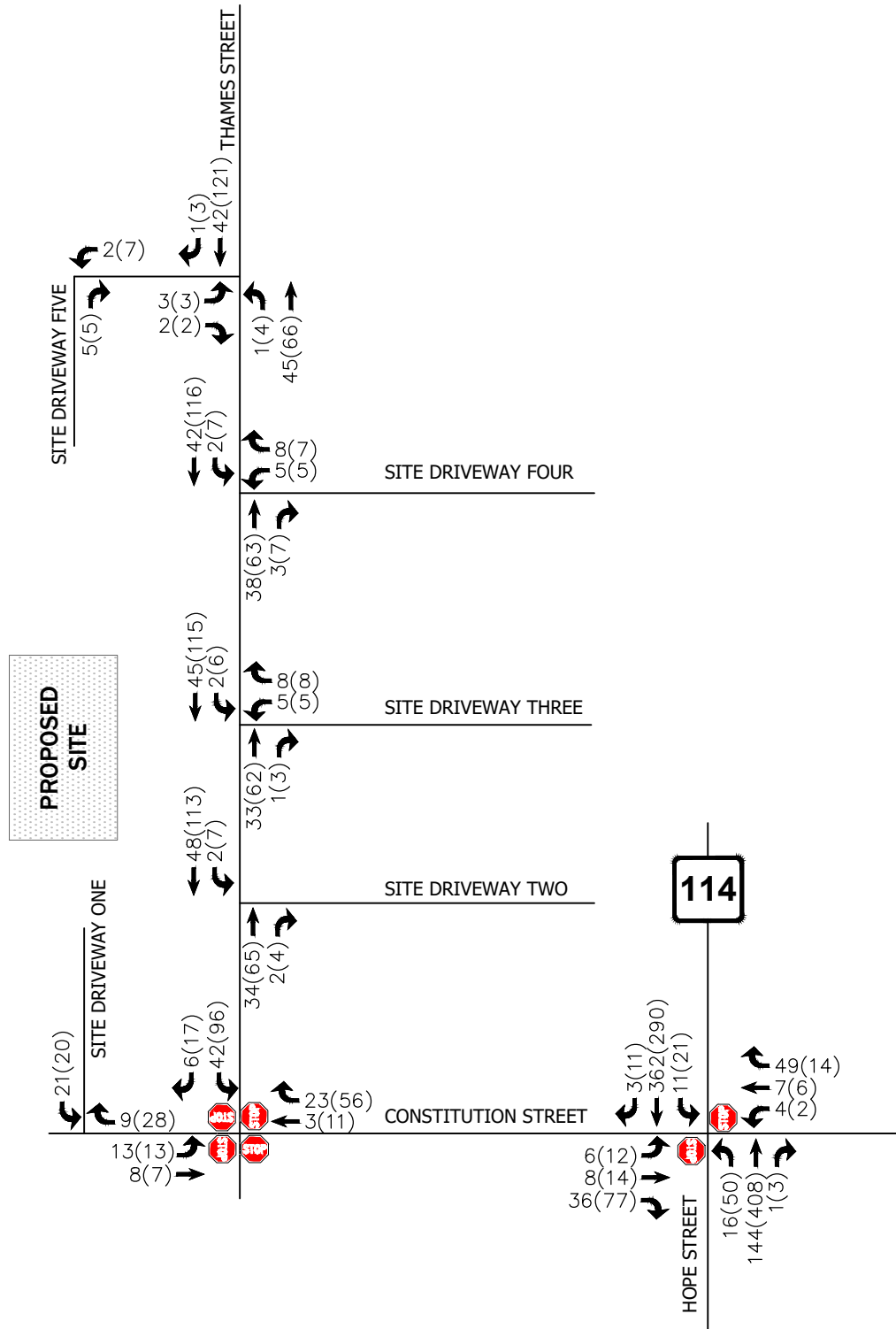
317 IRON HORSE WAY, SUITE 204
PROVIDENCE, RI 02908
401.861.3070
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FIGURE 4: TRIP GENERATION

PROJ. NO: 20061150.A22

ROBIN RUG REDEVELOPMENT

JULY 2022



xxx(yyy) = WEEKDAY MORNING PEAK HOUR (WEEKDAY PM PEAK HOUR)



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FIGURE 5: 2032 BUILD CONDITIONS

PROJ. NO: 20061150.A22

















ROBIN RUG REDEVELOPMENT

JULY 2022

HCM Unsignalized Intersection Capacity Analysis

1: Constitution Street & Hope Street

















2032 NO BUILD
AM PEAK HOUR

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	6	8	15	4	7	49	7	144	1	11	362	3
Future Volume (Veh/h)	6	8	15	4	7	49	7	144	1	11	362	3
Sign Control	Stop				Stop				Free		Free	
Grade	0%				0%				0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	7	9	16	4	8	53	8	157	1	12	393	3
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type							None			None		
Median storage veh												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	649	592	394	612	594	158	396				158	
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	649	592	394	612	594	158	396				158	
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1				4.1	
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2				2.2	
p0 queue free %	98	98	98	99	98	94	99				99	
cM capacity (veh/h)	351	412	655	384	412	888	1163				1422	
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	32	65	166	408								
Volume Left	7	4	8	12								
Volume Right	16	53	1	3								
cSH	483	726	1163	1422								
Volume to Capacity	0.07	0.09	0.01	0.01								
Queue Length 95th (ft)	5	7	1	1								
Control Delay (s)	13.0	10.4	0.4	0.3								
Lane LOS	B	B	A	A								
Approach Delay (s)	13.0	10.4	0.4	0.3								
Approach LOS	B	B										
Intersection Summary												
Average Delay			1.9									
Intersection Capacity Utilization			33.6%	ICU Level of Service				A				
Analysis Period (min)			15									

HCM Unsignalized Intersection Capacity Analysis

1: Constitution Street & Hope Street

















2032 NO BUILD
PM PEAK HOUR

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	12	14	57	2	6	14	22	408	3	21	290	11
Future Volume (Veh/h)	12	14	57	2	6	14	22	408	3	21	290	11
Sign Control	Stop			Stop			Free			Free		
Grade	0%			0%			0%			0%		
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	13	15	62	2	7	15	24	443	3	23	315	12
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type							None			None		
Median storage veh												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	878	861	321	929	866	444	327				446	
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	878	861	321	929	866	444	327				446	
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1				4.1	
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2				2.2	
p0 queue free %	95	95	91	99	97	98	98				98	
cM capacity (veh/h)	249	282	720	211	280	614	1233				1114	
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	90	24	470	350								
Volume Left	13	2	24	23								
Volume Right	62	15	3	12								
cSH	470	407	1233	1114								
Volume to Capacity	0.19	0.06	0.02	0.02								
Queue Length 95th (ft)	18	5	1	2								
Control Delay (s)	14.5	14.4	0.6	0.7								
Lane LOS	B	B	A	A								
Approach Delay (s)	14.5	14.4	0.6	0.7								
Approach LOS	B	B										
Intersection Summary												
Average Delay				2.3								
Intersection Capacity Utilization				42.2%	ICU Level of Service				A			
Analysis Period (min)				15								

HCM Unsignalized Intersection Capacity Analysis

1: Constitution Street & Hope Street

















2032 BUILD
AM PEAK HOUR

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	6	8	36	4	7	49	16	144	1	11	362	3
Future Volume (Veh/h)	6	8	36	4	7	49	16	144	1	11	362	3
Sign Control	Stop				Stop				Free		Free	
Grade	0%				0%				0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	7	9	39	4	8	53	17	157	1	12	393	3
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type							None			None		
Median storage (veh)												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	667	610	394	654	612	158	396				158	
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	667	610	394	654	612	158	396				158	
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1				4.1	
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2				2.2	
p0 queue free %	98	98	94	99	98	94	99				99	
cM capacity (veh/h)	339	400	655	345	399	888	1163				1422	
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	55	65	175	408								
Volume Left	7	4	17	12								
Volume Right	39	53	1	3								
cSH	535	712	1163	1422								
Volume to Capacity	0.10	0.09	0.01	0.01								
Queue Length 95th (ft)	9	8	1	1								
Control Delay (s)	12.5	10.6	0.9	0.3								
Lane LOS	B	B	A	A								
Approach Delay (s)	12.5	10.6	0.9	0.3								
Approach LOS	B	B										
Intersection Summary												
Average Delay			2.4									
Intersection Capacity Utilization			32.8%	ICU Level of Service				A				
Analysis Period (min)			15									

HCM Unsignalized Intersection Capacity Analysis

2: Thames Street & Constitution Street










2032 BUILD
AM PEAK HOUR

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Sign Control		Stop			Stop			Stop			Stop	
Traffic Volume (vph)	13	8	0	0	3	23	0	0	0	42	0	6
Future Volume (vph)	13	8	0	0	3	23	0	0	0	42	0	6
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	14	9	0	0	3	25	0	0	0	46	0	7
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total (vph)	23	28	0	53								
Volume Left (vph)	14	0	0	46								
Volume Right (vph)	0	25	0	7								
Hadj (s)	0.16	-0.50	0.00	0.13								
Departure Headway (s)	4.2	3.5	4.1	4.1								
Degree Utilization, x	0.03	0.03	0.00	0.06								
Capacity (veh/h)	840	996	881	857								
Control Delay (s)	7.3	6.6	7.1	7.4								
Approach Delay (s)	7.3	6.6	0.0	7.4								
Approach LOS	A	A	A	A								
Intersection Summary												
Delay				7.2								
Level of Service				A								
Intersection Capacity Utilization				17.9%	ICU Level of Service	A						
Analysis Period (min)				15								

HCM Unsignalized Intersection Capacity Analysis

4: Thames Street & Site Driveway Three










2032 BUILD
AM PEAK HOUR

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	5	8	33	1	2	45
Future Volume (Veh/h)	5	8	33	1	2	45
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	9	36	1	2	49
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage veh						
Upstream signal (ft)			120			
pX, platoon unblocked						
vC, conflicting volume	90	36			37	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	90	36			37	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	99	99			100	
cM capacity (veh/h)	910	1036			1574	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	14	37	51			
Volume Left	5	0	2			
Volume Right	9	1	0			
cSH	987	1700	1574			
Volume to Capacity	0.01	0.02	0.00			
Queue Length 95th (ft)	1	0	0			
Control Delay (s)	8.7	0.0	0.3			
Lane LOS	A		A			
Approach Delay (s)	8.7	0.0	0.3			
Approach LOS	A					
Intersection Summary						
Average Delay			1.3			
Intersection Capacity Utilization			14.0%	ICU Level of Service		A
Analysis Period (min)			15			

HCM Unsignalized Intersection Capacity Analysis

5: Site Driveway Four & Thames Street

















2032 BUILD
AM PEAK HOUR

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	5	8	38	3	2	42
Future Volume (Veh/h)	5	8	38	3	2	42
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	9	41	3	2	46
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage veh						
Upstream signal (ft)			317			
pX, platoon unblocked						
vC, conflicting volume	92	42			44	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	92	42			44	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	99	99			100	
cM capacity (veh/h)	906	1028			1564	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	14	44	48			
Volume Left	5	0	2			
Volume Right	9	3	0			
cSH	981	1700	1564			
Volume to Capacity	0.01	0.03	0.00			
Queue Length 95th (ft)	1	0	0			
Control Delay (s)	8.7	0.0	0.3			
Lane LOS	A		A			
Approach Delay (s)	8.7	0.0	0.3			
Approach LOS	A					
Intersection Summary						
Average Delay			1.3			
Intersection Capacity Utilization			13.8%	ICU Level of Service		A
Analysis Period (min)			15			

HCM Unsignalized Intersection Capacity Analysis

1: Constitution Street & Hope Street

















2032 BUILD
PM PEAK HOUR

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	12	14	77	2	6	14	50	408	3	21	290	11
Future Volume (Veh/h)	12	14	77	2	6	14	50	408	3	21	290	11
Sign Control	Stop			Stop			Free			Free		
Grade	0%			0%			0%			0%		
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	13	15	84	2	7	15	54	443	3	23	315	12
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type							None			None		
Median storage veh												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	938	921	321	1011	926	444	327				446	
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	938	921	321	1011	926	444	327				446	
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1				4.1	
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2				2.2	
p0 queue free %	94	94	88	99	97	98	96				98	
cM capacity (veh/h)	222	253	720	175	252	614	1233				1114	
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	112	24	500	350								
Volume Left	13	2	54	23								
Volume Right	84	15	3	12								
cSH	478	377	1233	1114								
Volume to Capacity	0.23	0.06	0.04	0.02								
Queue Length 95th (ft)	23	5	3	2								
Control Delay (s)	14.8	15.2	1.3	0.7								
Lane LOS	B	C	A	A								
Approach Delay (s)	14.8	15.2	1.3	0.7								
Approach LOS	B	C										
Intersection Summary												
Average Delay				3.0								
Intersection Capacity Utilization				52.3%	ICU Level of Service				A			
Analysis Period (min)				15								

HCM Unsignalized Intersection Capacity Analysis

2: Thames Street & Constitution Street










2032 BUILD
PM PEAK HOUR

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Sign Control		Stop			Stop			Stop			Stop	
Traffic Volume (vph)	13	7	0	0	11	56	0	0	0	96	0	17
Future Volume (vph)	13	7	0	0	11	56	0	0	0	96	0	17
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	14	8	0	0	12	61	0	0	0	104	0	18
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total (vph)	22	73	0	122								
Volume Left (vph)	14	0	0	104								
Volume Right (vph)	0	61	0	18								
Hadj (s)	0.16	-0.47	0.00	0.12								
Departure Headway (s)	4.4	3.7	4.2	4.2								
Degree Utilization, x	0.03	0.08	0.00	0.14								
Capacity (veh/h)	787	930	833	835								
Control Delay (s)	7.5	7.0	7.2	7.9								
Approach Delay (s)	7.5	7.0	0.0	7.9								
Approach LOS	A	A	A	A								
Intersection Summary												
Delay				7.6								
Level of Service				A								
Intersection Capacity Utilization				21.6%	ICU Level of Service		A					
Analysis Period (min)				15								

HCM Unsignalized Intersection Capacity Analysis

4: Thames Street & Site Driveway Three










2032 BUILD
PM PEAK HOUR

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	5	8	62	3	6	115
Future Volume (Veh/h)	5	8	62	3	6	115
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	9	67	3	7	125
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage veh						
Upstream signal (ft)			124			
pX, platoon unblocked						
vC, conflicting volume	208	68			70	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	208	68			70	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	99	99			100	
cM capacity (veh/h)	777	995			1531	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	14	70	132			
Volume Left	5	0	7			
Volume Right	9	3	0			
cSH	904	1700	1531			
Volume to Capacity	0.02	0.04	0.00			
Queue Length 95th (ft)	1	0	0			
Control Delay (s)	9.0	0.0	0.4			
Lane LOS	A		A			
Approach Delay (s)	9.0	0.0	0.4			
Approach LOS	A					
Intersection Summary						
Average Delay		0.8				
Intersection Capacity Utilization		20.9%		ICU Level of Service		A
Analysis Period (min)		15				

HCM Unsignalized Intersection Capacity Analysis

5: Site Driveway Four & Thames Street

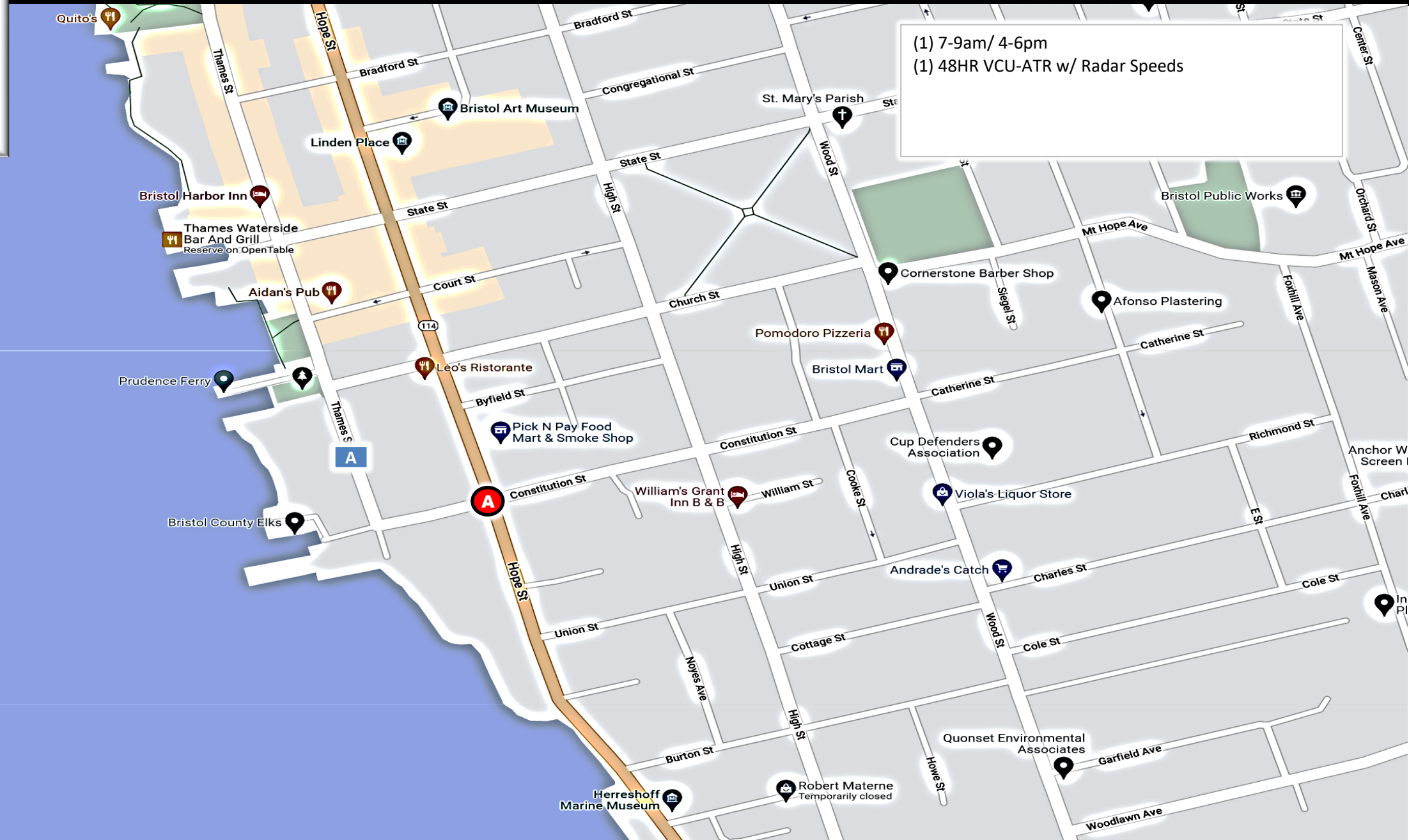
2032 BUILD
PM PEAK HOUR

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	5	7	63	7	7	116
Future Volume (Veh/h)	5	7	63	7	7	116
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	8	68	8	8	126
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage veh						
Upstream signal (ft)			321			
pX, platoon unblocked						
vC, conflicting volume	214	72			76	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	214	72			76	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	99	99			99	
cM capacity (veh/h)	770	990			1523	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	13	76	134			
Volume Left	5	0	8			
Volume Right	8	8	0			
cSH	892	1700	1523			
Volume to Capacity	0.01	0.04	0.01			
Queue Length 95th (ft)	1	0	0			
Control Delay (s)	9.1	0.0	0.5			
Lane LOS	A		A			
Approach Delay (s)	9.1	0.0	0.5			
Approach LOS	A					
Intersection Summary						
Average Delay		0.8				
Intersection Capacity Utilization		21.8%		ICU Level of Service		A
Analysis Period (min)		15				



Location Map: 228763 Bristol, RI

Precision Data Industries, LLC 157 Washington Street, Suite 2, Hudson, MA 01749 ph: 508-875-0100 email: datarequests@pdillc.com



Client:
Fuss & O'Neill

Engineer:
K. Patch

Site Code:

Date:
Thurs 7/21 thru Fri 7/22/2022

PDI Job #
228763

City, State:
Bristol, RI

Thames Street
south of Church Street
City, State: Bristol, RI
Client: F&O/K. Patch
Site Code: TBD



PDI File #: 228763 ATR A

Count Date: Thursday, July 21, 2022
Direction: NB

AM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 AM	0	1	1	0	0	0	2
12:15 AM	0	0	0	0	0	0	0
12:30 AM	0	0	0	0	0	0	0
12:45 AM	0	0	1	0	0	0	1
1:00 AM	0	0	0	0	0	0	0
1:15 AM	0	0	1	0	0	0	1
1:30 AM	0	0	0	0	0	0	0
1:45 AM	0	0	0	0	0	0	0
2:00 AM	0	0	0	0	0	0	0
2:15 AM	0	0	0	0	0	0	0
2:30 AM	0	0	0	0	0	0	0
2:45 AM	0	0	0	0	0	0	0
3:00 AM	0	0	0	0	0	0	0
3:15 AM	0	0	0	0	0	0	0
3:30 AM	0	0	0	0	0	0	0
3:45 AM	0	0	0	0	0	0	0
4:00 AM	0	0	0	0	0	0	0
4:15 AM	0	0	0	0	0	0	0
4:30 AM	0	0	0	0	0	0	0
4:45 AM	0	0	1	0	0	0	1
5:00 AM	0	0	0	0	0	0	0
5:15 AM	0	0	0	0	0	0	0
5:30 AM	0	0	1	0	0	0	1
5:45 AM	0	0	1	0	0	0	1
6:00 AM	0	0	2	0	0	0	2
6:15 AM	1	0	0	0	0	0	1
6:30 AM	0	1	2	0	0	0	3
6:45 AM	1	0	5	0	0	0	6
7:00 AM	2	0	3	0	0	0	5
7:15 AM	0	0	5	0	1	0	6
7:30 AM	0	0	0	0	0	0	0
7:45 AM	0	0	5	0	0	0	5
8:00 AM	0	0	4	0	0	0	4
8:15 AM	0	0	9	0	0	0	9
8:30 AM	0	0	4	0	0	0	4
8:45 AM	0	0	5	0	0	0	5
9:00 AM	2	0	4	0	0	0	6
9:15 AM	0	0	6	0	0	0	6
9:30 AM	2	0	5	0	0	0	7
9:45 AM	0	0	6	0	0	0	6
10:00 AM	0	0	1	0	2	0	3
10:15 AM	0	0	4	0	0	0	4
10:30 AM	1	0	3	0	0	0	4
10:45 AM	0	0	3	0	1	0	4
11:00 AM	1	0	8	0	1	0	10
11:15 AM	1	0	4	0	0	0	5
11:30 AM	0	0	7	0	0	0	7
11:45 AM	0	0	4	0	1	0	5

AM Total	11	2	105	0	6	0	124
Percentage	8.87%	1.61%	84.68%	0.00%	4.84%	0.00%	

AM Peak Volume	6:15 AM	12:00 AM	11:00 AM	12:00 AM	10:00 AM	12:00 AM	11:00 AM
	4	1	23	0	3	0	27

PM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 PM	0	0	4	0	1	0	5
12:15 PM	0	0	9	0	1	0	10
12:30 PM	0	2	8	0	1	0	11
12:45 PM	0	0	6	0	0	0	6
1:00 PM	0	0	13	0	0	0	13
1:15 PM	1	0	8	0	0	0	9
1:30 PM	0	0	6	0	0	0	6
1:45 PM	0	1	9	0	2	0	12
2:00 PM	1	1	9	0	0	0	11
2:15 PM	0	0	5	0	0	0	5
2:30 PM	0	1	3	0	0	0	4
2:45 PM	0	0	7	0	0	1	8
3:00 PM	0	0	13	0	2	0	15
3:15 PM	0	0	6	0	0	0	6
3:30 PM	1	0	8	0	1	0	10
3:45 PM	0	0	2	0	1	0	3
4:00 PM	1	0	5	0	0	0	6
4:15 PM	0	0	7	0	0	0	7
4:30 PM	3	0	9	0	0	0	12
4:45 PM	0	0	9	0	0	0	9
5:00 PM	0	0	10	0	0	0	10
5:15 PM	0	0	3	0	0	1	4
5:30 PM	0	0	6	0	0	0	6
5:45 PM	0	0	4	0	0	0	4
6:00 PM	0	0	14	0	0	0	14
6:15 PM	0	0	8	0	0	0	8
6:30 PM	0	0	10	0	0	0	10
6:45 PM	0	0	8	0	0	0	8
7:00 PM	0	0	3	0	0	0	3
7:15 PM	0	0	3	0	0	0	3
7:30 PM	0	0	5	0	0	0	5
7:45 PM	0	0	5	0	0	0	5
8:00 PM	1	0	7	0	0	0	8
8:15 PM	0	0	4	0	0	0	4
8:30 PM	0	0	3	0	0	0	3
8:45 PM	0	0	2	0	1	0	3
9:00 PM	0	0	1	0	0	0	1
9:15 PM	1	0	3	0	0	0	4
9:30 PM	0	2	1	0	0	0	3
9:45 PM	0	0	3	0	0	0	3
10:00 PM	0	0	0	0	0	0	0
10:15 PM	0	0	2	0	0	0	2
10:30 PM	0	0	1	0	0	0	1
10:45 PM	0	0	0	0	0	0	0
11:00 PM	0	0	2	0	0	0	2
11:15 PM	0	0	0	0	0	0	0
11:30 PM	0	0	2	0	0	0	2
11:45 PM	0	0	0	0	0	0	0

PM Total	9	7	256	0	10	2	284
Percentage	3.17%	2.46%	90.14%	0.00%	3.52%	0.70%	

PM Peak Volume	3:45 PM	1:45 PM	6:00 PM	12:00 PM	3:00 PM	2:00 PM	12:15 PM
	4	3	40	0	4	1	40

Day Total	20	9	361	0	16	2	408
Percentage	4.90%	2.21%	88.48%	0.00%	3.92%	0.49%	

Thames Street
south of Church Street
City, State: Bristol, RI
Client: F&O/K. Patch
Site Code: TBD



PDI File #: 228763 ATR A

Count Date: Friday, July 22, 2022
Direction: NB

AM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 AM	0	0	0	0	0	0	0
12:15 AM	0	0	1	0	0	0	1
12:30 AM	0	0	0	0	0	0	0
12:45 AM	0	0	1	0	0	0	1
1:00 AM	0	0	1	0	0	0	1
1:15 AM	0	0	1	0	0	0	1
1:30 AM	0	0	0	0	0	0	0
1:45 AM	0	0	0	0	0	0	0
2:00 AM	0	0	0	0	0	0	0
2:15 AM	0	0	1	0	0	0	1
2:30 AM	0	0	0	0	0	0	0
2:45 AM	0	0	0	0	0	0	0
3:00 AM	0	0	0	0	0	0	0
3:15 AM	0	0	0	0	0	0	0
3:30 AM	0	0	0	0	0	0	0
3:45 AM	0	0	0	0	0	0	0
4:00 AM	0	0	0	0	0	0	0
4:15 AM	0	0	0	0	0	0	0
4:30 AM	0	0	2	0	0	0	2
4:45 AM	0	0	0	0	0	0	0
5:00 AM	1	0	0	0	0	0	1
5:15 AM	0	0	0	0	0	0	0
5:30 AM	0	0	0	0	0	0	0
5:45 AM	0	0	0	0	0	0	0
6:00 AM	0	0	1	0	0	0	1
6:15 AM	1	0	2	0	0	0	3
6:30 AM	0	0	1	0	0	0	1
6:45 AM	0	0	6	0	0	0	6
7:00 AM	0	0	3	0	0	0	3
7:15 AM	1	0	4	0	0	0	5
7:30 AM	0	0	2	0	1	0	3
7:45 AM	0	0	5	0	1	0	6
8:00 AM	1	0	2	0	1	0	4
8:15 AM	3	0	3	0	0	0	6
8:30 AM	1	0	8	0	0	0	9
8:45 AM	1	0	8	0	0	0	9
9:00 AM	1	0	6	0	0	0	7
9:15 AM	0	0	7	0	0	0	7
9:30 AM	1	0	5	0	0	0	6
9:45 AM	1	0	8	0	0	0	9
10:00 AM	4	0	5	0	0	0	9
10:15 AM	1	0	5	0	0	0	6
10:30 AM	0	1	1	0	0	0	2
10:45 AM	0	0	7	0	0	0	7
11:00 AM	0	0	10	0	0	0	10
11:15 AM	0	0	9	0	0	0	9
11:30 AM	0	0	5	0	0	0	5
11:45 AM	0	0	7	0	0	0	7

AM Total 17 1 127 0 3 0 148
Percentage 11.49% 0.68% 85.81% 0.00% 2.03% 0.00%

AM Peak 9:30 AM 9:45 AM 10:45 AM 12:00 AM 7:15 AM 12:00 AM 8:30 AM
Volume 7 1 31 0 3 0 32

PM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 PM	0	0	11	0	1	0	12
12:15 PM	2	1	8	0	0	0	11
12:30 PM	0	0	11	0	0	0	11
12:45 PM	2	0	9	0	1	0	12
1:00 PM	1	0	7	1	0	0	9
1:15 PM	0	0	14	0	0	0	14
1:30 PM	0	0	5	0	0	0	5
1:45 PM	0	0	6	0	0	0	6
2:00 PM	0	0	3	0	0	0	3
2:15 PM	0	0	8	0	0	0	8
2:30 PM	0	1	7	0	1	0	9
2:45 PM	1	0	8	0	1	0	10
3:00 PM	0	0	12	0	0	0	12
3:15 PM	0	0	10	0	0	0	10
3:30 PM	3	0	7	0	0	0	10
3:45 PM	1	0	12	0	2	0	15
4:00 PM	0	0	11	0	0	0	11
4:15 PM	1	0	17	0	0	0	18
4:30 PM	0	0	12	0	0	1	13
4:45 PM	1	0	10	0	0	0	11
5:00 PM	1	0	18	0	0	0	19
5:15 PM	0	0	8	0	0	0	8
5:30 PM	0	0	9	0	0	0	9
5:45 PM	0	0	11	0	0	0	11
6:00 PM	0	0	9	0	0	0	9
6:15 PM	0	0	7	0	0	0	7
6:30 PM	0	0	7	0	0	0	7
6:45 PM	0	0	15	0	0	0	15
7:00 PM	1	0	15	0	0	0	16
7:15 PM	0	1	6	0	0	0	7
7:30 PM	0	0	9	0	0	0	9
7:45 PM	3	0	7	0	0	0	10
8:00 PM	1	2	8	0	0	0	11
8:15 PM	0	1	17	0	0	0	18
8:30 PM	0	0	3	0	0	0	3
8:45 PM	1	0	5	0	0	0	6
9:00 PM	1	0	3	0	0	0	4
9:15 PM	0	1	6	0	0	0	7
9:30 PM	0	0	3	0	0	0	3
9:45 PM	0	0	4	0	0	0	4
10:00 PM	0	0	5	0	0	0	5
10:15 PM	0	1	0	0	0	0	1
10:30 PM	0	0	3	0	0	0	3
10:45 PM	0	0	3	0	0	0	3
11:00 PM	0	0	1	0	0	0	1
11:15 PM	0	0	0	0	0	0	0
11:30 PM	0	0	5	0	0	0	5
11:45 PM	0	0	4	0	0	0	4

PM Total 20 8 379 1 6 1 415
Percentage 4.82% 1.93% 91.33% 0.24% 1.45% 0.24%

PM Peak 12:15 PM 7:15 PM 4:15 PM 12:15 PM 12:00 PM 3:45 PM 4:15 PM
Volume 5 3 57 1 2 1 61

Day Total 37 9 506 1 9 1 563
Percentage 6.57% 1.60% 89.88% 0.18% 1.60% 0.18%

Thames Street
south of Church Street
City, State: Bristol, RI
Client: F&O/K. Patch
Site Code: TBD



PDI File #: 228763 ATR A

Count Date: Thursday, July 21, 2022
Direction: SB

AM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 AM	0	0	2	0	0	0	2
12:15 AM	0	0	2	0	0	0	2
12:30 AM	0	0	2	0	0	0	2
12:45 AM	0	0	1	0	0	0	1
1:00 AM	0	0	3	0	0	0	3
1:15 AM	0	0	0	0	0	0	0
1:30 AM	0	0	0	0	0	0	0
1:45 AM	0	0	0	0	0	0	0
2:00 AM	0	0	0	0	0	0	0
2:15 AM	0	0	0	0	0	0	0
2:30 AM	0	0	0	0	0	0	0
2:45 AM	0	0	0	0	0	0	0
3:00 AM	0	0	0	0	0	0	0
3:15 AM	0	0	0	0	0	0	0
3:30 AM	0	0	0	0	0	0	0
3:45 AM	0	0	0	0	0	0	0
4:00 AM	0	0	0	0	0	0	0
4:15 AM	0	0	0	0	0	0	0
4:30 AM	0	0	0	0	0	0	0
4:45 AM	1	0	1	0	0	0	2
5:00 AM	0	0	0	0	0	0	0
5:15 AM	0	0	0	0	0	0	0
5:30 AM	0	0	0	0	0	0	0
5:45 AM	0	0	1	0	0	0	1
6:00 AM	0	0	1	0	0	0	1
6:15 AM	1	0	1	0	0	0	2
6:30 AM	0	0	6	0	0	0	6
6:45 AM	0	0	9	0	0	0	9
7:00 AM	1	0	5	0	0	0	6
7:15 AM	0	1	8	0	0	0	9
7:30 AM	0	0	7	0	1	0	8
7:45 AM	1	0	5	0	0	0	6
8:00 AM	0	0	6	0	0	0	6
8:15 AM	0	0	7	0	0	0	7
8:30 AM	0	0	4	0	1	0	5
8:45 AM	2	0	13	0	0	0	15
9:00 AM	3	0	11	0	0	0	14
9:15 AM	2	0	10	0	0	0	12
9:30 AM	2	0	3	0	0	0	5
9:45 AM	0	1	4	0	0	0	5
10:00 AM	0	0	16	0	0	0	16
10:15 AM	1	0	16	0	0	0	17
10:30 AM	0	0	14	0	1	0	15
10:45 AM	2	0	6	0	0	0	8
11:00 AM	0	0	14	0	0	0	14
11:15 AM	1	0	13	0	0	0	14
11:30 AM	1	0	12	0	0	0	13
11:45 AM	1	0	16	0	0	0	17

AM Total	19	2	219	0	3	0	243
Percentage	7.82%	0.82%	90.12%	0.00%	1.23%	0.00%	
AM Peak Volume	8:45 AM 9	6:30 AM 1	11:00 AM 55	12:00 AM 0	6:45 AM 1	12:00 AM 0	11:00 AM 58

PM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 PM	0	0	13	0	0	0	13
12:15 PM	0	0	14	0	1	0	15
12:30 PM	0	0	13	0	0	0	13
12:45 PM	0	0	9	0	0	0	9
1:00 PM	1	0	14	0	1	0	16
1:15 PM	2	0	14	0	1	0	17
1:30 PM	0	0	8	0	0	0	8
1:45 PM	0	0	14	0	0	0	14
2:00 PM	0	0	19	0	0	0	19
2:15 PM	2	0	15	0	0	0	17
2:30 PM	0	0	18	0	0	0	18
2:45 PM	1	0	15	0	0	0	16
3:00 PM	1	0	19	0	2	0	22
3:15 PM	0	1	17	0	0	0	18
3:30 PM	0	1	16	0	0	0	17
3:45 PM	0	0	12	0	1	0	13
4:00 PM	0	0	18	0	0	0	18
4:15 PM	0	0	16	0	0	0	16
4:30 PM	3	0	27	0	0	0	30
4:45 PM	0	0	21	0	1	0	22
5:00 PM	0	1	17	0	0	0	18
5:15 PM	1	0	19	0	0	0	20
5:30 PM	0	0	18	0	0	0	18
5:45 PM	0	0	9	0	0	0	9
6:00 PM	2	0	17	0	0	0	19
6:15 PM	0	0	16	0	0	0	16
6:30 PM	0	0	13	0	0	0	13
6:45 PM	1	0	16	0	0	0	17
7:00 PM	0	0	13	0	0	0	13
7:15 PM	0	0	15	0	0	0	15
7:30 PM	1	0	15	0	0	0	16
7:45 PM	0	0	7	0	0	0	7
8:00 PM	0	0	10	0	0	0	10
8:15 PM	3	0	8	0	0	0	11
8:30 PM	0	2	9	0	0	0	11
8:45 PM	0	0	12	0	0	0	12
9:00 PM	0	0	7	0	0	0	7
9:15 PM	0	0	9	0	0	0	9
9:30 PM	0	0	3	0	0	0	3
9:45 PM	0	0	5	0	0	0	5
10:00 PM	0	0	5	0	0	0	5
10:15 PM	0	0	4	0	0	0	4
10:30 PM	0	0	3	0	0	0	3
10:45 PM	0	0	1	0	0	0	1
11:00 PM	0	0	1	0	0	0	1
11:15 PM	0	0	0	0	0	0	0
11:30 PM	0	0	2	0	0	0	2
11:45 PM	0	0	0	0	0	0	0

PM Total	18	5	566	0	7	0	596
Percentage	3.02%	0.84%	94.97%	0.00%	1.17%	0.00%	
PM Peak Volume	2:15 PM 4	2:45 PM 2	4:30 PM 84	12:00 PM 0	3:00 PM 3	12:00 PM 0	4:30 PM 90

Day Total	37	7	785	0	10	0	839
Percentage	4.41%	0.83%	93.56%	0.00%	1.19%	0.00%	

Thames Street
south of Church Street
City, State: Bristol, RI
Client: F&O/K. Patch
Site Code: TBD



PDI File #: 228763 ATR A

Count Date: Friday, July 22, 2022
Direction: SB

AM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 AM	0	0	0	0	0	0	0
12:15 AM	0	0	1	0	0	0	1
12:30 AM	0	0	0	0	0	0	0
12:45 AM	0	0	0	0	0	0	0
1:00 AM	0	0	0	0	0	0	0
1:15 AM	0	0	0	0	0	0	0
1:30 AM	0	0	0	0	0	0	0
1:45 AM	0	0	0	0	0	0	0
2:00 AM	0	0	0	0	0	0	0
2:15 AM	0	0	0	0	0	0	0
2:30 AM	0	0	0	0	0	0	0
2:45 AM	0	0	0	0	0	0	0
3:00 AM	0	0	0	0	0	0	0
3:15 AM	0	0	0	0	0	0	0
3:30 AM	0	0	0	0	0	0	0
3:45 AM	0	0	0	0	0	0	0
4:00 AM	0	0	0	0	0	0	0
4:15 AM	0	0	0	0	0	0	0
4:30 AM	0	0	1	0	0	0	1
4:45 AM	0	0	2	0	0	0	2
5:00 AM	0	0	0	0	0	0	0
5:15 AM	0	0	0	0	0	0	0
5:30 AM	0	0	0	0	0	0	0
5:45 AM	0	0	1	0	0	0	1
6:00 AM	0	0	2	0	0	0	2
6:15 AM	1	0	3	0	0	0	4
6:30 AM	0	0	1	0	0	0	1
6:45 AM	2	0	10	0	0	0	12
7:00 AM	0	0	4	0	0	0	4
7:15 AM	0	0	5	0	0	0	5
7:30 AM	0	0	4	0	1	0	5
7:45 AM	0	0	4	0	0	0	4
8:00 AM	2	0	4	0	0	0	6
8:15 AM	1	0	6	0	1	0	8
8:30 AM	0	0	9	0	0	0	9
8:45 AM	0	0	9	0	0	0	9
9:00 AM	0	0	12	0	0	0	12
9:15 AM	1	0	23	0	0	0	24
9:30 AM	0	0	11	0	1	0	12
9:45 AM	1	0	14	0	0	0	15
10:00 AM	4	0	18	0	1	0	23
10:15 AM	5	0	10	0	1	0	16
10:30 AM	0	0	14	0	1	0	15
10:45 AM	0	0	16	0	0	0	16
11:00 AM	2	0	18	0	0	0	20
11:15 AM	1	0	14	0	0	0	15
11:30 AM	0	0	24	0	0	0	24
11:45 AM	0	0	12	0	0	0	12

AM Total	20	0	252	0	6	0	278
Percentage	7.19%	0.00%	90.65%	0.00%	2.16%	0.00%	

AM Peak Volume	9:30 AM	12:00 AM	10:45 AM	12:00 AM	9:30 AM	12:00 AM	10:45 AM
	10	0	72	0	3	0	75

PM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 PM	0	0	10	0	0	0	10
12:15 PM	0	2	14	0	0	0	16
12:30 PM	2	0	12	0	0	0	14
12:45 PM	3	0	16	0	0	0	19
1:00 PM	4	2	20	0	0	0	26
1:15 PM	0	0	23	0	1	0	24
1:30 PM	1	0	18	0	1	0	20
1:45 PM	1	0	13	0	0	0	14
2:00 PM	0	1	15	0	0	0	16
2:15 PM	0	0	16	0	0	0	16
2:30 PM	2	0	21	0	1	0	24
2:45 PM	0	0	11	0	0	0	11
3:00 PM	0	1	15	0	0	0	16
3:15 PM	0	0	15	0	0	0	15
3:30 PM	4	0	15	0	0	0	19
3:45 PM	0	0	15	0	1	0	16
4:00 PM	2	0	20	0	0	0	22
4:15 PM	2	1	22	0	0	0	25
4:30 PM	0	0	23	1	0	0	24
4:45 PM	2	0	18	0	0	0	20
5:00 PM	1	0	14	0	0	0	15
5:15 PM	1	0	26	0	0	0	27
5:30 PM	1	2	24	0	0	0	27
5:45 PM	0	0	17	0	0	0	17
6:00 PM	1	1	18	0	0	0	20
6:15 PM	0	0	15	0	0	0	15
6:30 PM	0	0	16	0	0	0	16
6:45 PM	0	0	25	0	0	0	25
7:00 PM	1	1	32	0	0	0	34
7:15 PM	0	1	21	0	0	0	22
7:30 PM	1	2	11	0	0	0	14
7:45 PM	0	1	13	0	0	0	14
8:00 PM	0	1	18	0	0	0	19
8:15 PM	0	1	28	0	0	0	29
8:30 PM	0	1	23	0	0	0	24
8:45 PM	0	1	17	0	0	0	18
9:00 PM	3	1	20	0	0	0	24
9:15 PM	0	1	14	0	0	0	15
9:30 PM	0	1	4	0	0	0	5
9:45 PM	0	0	8	0	0	0	8
10:00 PM	0	0	11	0	0	0	11
10:15 PM	0	0	5	0	0	0	5
10:30 PM	0	0	9	0	0	0	9
10:45 PM	0	0	4	0	0	0	4
11:00 PM	0	2	3	0	0	0	5
11:15 PM	0	0	4	0	0	0	4
11:30 PM	0	0	4	0	0	0	4
11:45 PM	0	0	2	0	0	0	2

PM Total	32	24	738	1	4	0	799
Percentage	4.01%	3.00%	92.37%	0.13%	0.50%	0.00%	

PM Peak Volume	12:15 PM	7:00 PM	6:30 PM	3:45 PM	12:45 PM	12:00 PM	6:30 PM
	9	5	94	1	2	0	97

Day Total	52	24	990	1	10	0	1077
Percentage	4.83%	2.23%	91.92%	0.09%	0.93%	0.00%	



PRECISION
DATA
INDUSTRIES, LLC

157 Washington Street, Suite 2
Hudson, MA 01749
Office: 508-875-0100 Fax: 508-875-0118

Weekly Report

Direction: NB

Day Date			Thursday 07/21/22		Friday 07/22/22										Week Ave			
	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM		
12:00	2	5	0	12	0	0	0	0	0	0	0	0	0	0	1	9		
12:15	0	10	1	11	0	0	0	0	0	0	0	0	0	0	1	11		
12:30	0	11	0	11	0	0	0	0	0	0	0	0	0	0	0	11		
12:45	1	6	1	12	0	0	0	0	0	0	0	0	0	0	1	9		
1:00	0	13	1	9	0	0	0	0	0	0	0	0	0	0	1	11		
1:15	1	9	1	14	0	0	0	0	0	0	0	0	0	0	1	12		
1:30	0	6	0	5	0	0	0	0	0	0	0	0	0	0	0	6		
1:45	0	12	0	6	0	0	0	0	0	0	0	0	0	0	0	9		
2:00	0	11	0	3	0	0	0	0	0	0	0	0	0	0	0	7		
2:15	0	5	1	8	0	0	0	0	0	0	0	0	0	0	1	7		
2:30	0	4	0	9	0	0	0	0	0	0	0	0	0	0	0	7		
2:45	0	8	0	10	0	0	0	0	0	0	0	0	0	0	0	9		
3:00	0	15	0	12	0	0	0	0	0	0	0	0	0	0	0	14		
3:15	0	6	0	10	0	0	0	0	0	0	0	0	0	0	0	8		
3:30	0	10	0	10	0	0	0	0	0	0	0	0	0	0	0	10		
3:45	0	3	0	15	0	0	0	0	0	0	0	0	0	0	0	9		
4:00	0	6	0	11	0	0	0	0	0	0	0	0	0	0	0	9		
4:15	0	7	0	18	0	0	0	0	0	0	0	0	0	0	0	13		
4:30	0	12	2	13	0	0	0	0	0	0	0	0	0	0	1	13		
4:45	1	9	0	11	0	0	0	0	0	0	0	0	0	0	1	10		
5:00	0	10	1	19	0	0	0	0	0	0	0	0	0	0	1	15		
5:15	0	4	0	8	0	0	0	0	0	0	0	0	0	0	0	6		
5:30	1	6	0	9	0	0	0	0	0	0	0	0	0	0	1	8		
5:45	1	4	0	11	0	0	0	0	0	0	0	0	0	0	1	8		
6:00	2	14	1	9	0	0	0	0	0	0	0	0	0	0	2	12		
6:15	1	8	3	7	0	0	0	0	0	0	0	0	0	0	2	8		
6:30	3	10	1	7	0	0	0	0	0	0	0	0	0	0	2	9		
6:45	6	8	6	15	0	0	0	0	0	0	0	0	0	0	6	12		
7:00	5	3	3	16	0	0	0	0	0	0	0	0	0	0	4	10		
7:15	6	3	5	7	0	0	0	0	0	0	0	0	0	0	6	5		
7:30	0	5	3	9	0	0	0	0	0	0	0	0	0	0	2	7		
7:45	5	5	6	10	0	0	0	0	0	0	0	0	0	0	6	8		
8:00	4	8	4	11	0	0	0	0	0	0	0	0	0	0	4	10		
8:15	9	4	6	18	0	0	0	0	0	0	0	0	0	0	8	11		
8:30	4	3	9	3	0	0	0	0	0	0	0	0	0	0	7	3		
8:45	5	3	9	6	0	0	0	0	0	0	0	0	0	0	7	5		
9:00	6	1	7	4	0	0	0	0	0	0	0	0	0	0	7	3		
9:15	6	4	7	7	0	0	0	0	0	0	0	0	0	0	7	6		
9:30	7	3	6	3	0	0	0	0	0	0	0	0	0	0	7	3		
9:45	6	3	9	4	0	0	0	0	0	0	0	0	0	0	8	4		
10:00	3	0	9	5	0	0	0	0	0	0	0	0	0	0	6	3		
10:15	4	2	6	1	0	0	0	0	0	0	0	0	0	0	5	2		
10:30	4	1	2	3	0	0	0	0	0	0	0	0	0	0	3	2		
10:45	4	0	7	3	0	0	0	0	0	0	0	0	0	0	6	2		
11:00	10	2	10	1	0	0	0	0	0	0	0	0	0	0	10	2		
11:15	5	0	9	0	0	0	0	0	0	0	0	0	0	0	7	0		
11:30	7	2	5	5	0	0	0	0	0	0	0	0	0	0	6	4		
11:45	5	0	7	4	0	0	0	0	0	0	0	0	0	0	6	2		
Total	124	284	148	415	0	0	0	0	0	0	0	0	0	0	136	350		
Day Total	408		563		0		0		0		0		0		486			
Peak HR	11:00 AM	12:15 PM	8:30 AM	4:15 PM													11:00 AM	4:15 PM
Volume	27	40	32	61													29	50

**Thames Street
south of Church Street
City, State: Bristol, RI
Client: F&O/K. Patch
Site Code: TBD**



PRECISION
DATA
INDUSTRIES, LLC

157 Washington Street, Suite 2
Hudson, MA 01749
Office: 508-875-0100 Fax: 508-875-0118

Weekly Report

Direction: SB

PDI File # 228763 ATR A

[illegible]

Thames Street
south of Church Street
City, State: Bristol, RI
Client: F&O/ K. Patch
Site Code: TBA



PDI File #: 228763 ATR-A (Speed)

Count Date
Thursday, July 21, 2022

Speed (60-minute)

NB																
Start Time:	1 to 14	15 to 19	20 to 24	25 to 29	30 to 34	35 to 39	40 to 44	45 to 49	50 to 54	55 to 59	60 to 64	65 to 69	70+	Total	85th %ile	Ave Speed
12:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	3	23.4	21.7
1:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	1	20.0	20.0
2:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0
3:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0
4:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17.0	17.0
5:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	1	20.0	20.0
6:00 AM	1	6	4	0	0	0	0	0	0	0	0	0	0	11	23.0	18.3
7:00 AM	1	1	9	3	0	0	0	0	0	0	0	0	0	14	25.0	22.0
8:00 AM	2	4	11	4	2	0	0	0	0	0	0	0	0	23	25.0	21.6
9:00 AM	4	2	12	5	0	0	0	0	0	0	0	0	0	23	26.0	20.5
10:00 AM	2	5	8	2	0	0	0	0	0	0	0	0	0	17	23.6	19.5
11:00 AM	6	8	12	2	0	0	0	0	0	0	0	0	0	28	23.0	18.1
12:00 PM	3	3	17	9	0	0	0	0	0	0	0	0	0	32	25.4	22.0
1:00 PM	2	7	19	8	3	0	0	0	0	0	0	0	0	39	26.0	22.4
2:00 PM	1	12	13	6	0	0	0	0	0	0	0	0	0	32	25.0	20.7
3:00 PM	6	12	18	2	0	0	0	0	0	0	0	0	0	38	23.0	19.2
4:00 PM	1	3	21	8	0	0	0	0	0	0	0	0	0	33	25.0	22.4
5:00 PM	0	6	11	8	0	0	0	0	0	0	0	0	0	25	25.8	22.1
6:00 PM	0	11	21	8	0	1	0	0	0	0	0	0	0	41	25.0	22.1
7:00 PM	1	9	6	2	0	0	0	0	0	0	0	0	0	18	23.5	19.7
8:00 PM	0	7	8	3	0	0	0	0	0	0	0	0	0	18	24.4	20.8
9:00 PM	0	4	3	1	0	1	0	0	0	0	0	0	0	9	25.4	22.4
10:00 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	3	23.4	21.3
11:00 PM	0	1	1	1	1	0	0	0	0	0	0	0	0	4	29.2	24.0
Total	30	104	200	72	6	2	0	0	0	0	0	0	0	414	25.0	21.0
Percent	7.25%	25.12%	48.31%	17.39%	1.45%	0.48%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%			

AM Peak 11:00 AM 11:00 AM 9:00 AM 9:00 AM 8:00 AM 11:00 AM
Volume 6 8 12 5 2 0 0 0 0 0 0 0 0 28

PM Peak 3:00 PM 2:00 PM 4:00 PM 12:00 PM 1:00 PM 6:00 PM 6:00 PM
Volume 6 12 21 9 3 1 0 0 0 0 0 0 0 41

15th Percentile:	17.0 MPH	Average Speed:	21.0 MPH	Posted Speed Limit:	25 MPH
50th Percentile:	21.0 MPH	10 MPH Pace:	16 to 25 MPH	Number of Vehicles > 25 MPH:	49
85th Percentile:	25.0 MPH	Number in Pace:	325	Percent of Vehicles > 25 MPH:	11.8%
95th Percentile:	27.0 MPH	Percent in Pace:	78.5%		

Thames Street
south of Church Street
City, State: Bristol, RI
Client: F&O/ K. Patch
Site Code: TBA



PDI File #: 228763 ATR-A (Speed)

Count Date
Thursday, July 21, 2022

Speed (60-minute)

SB																
Start Time:	1 to 14	15 to 19	20 to 24	25 to 29	30 to 34	35 to 39	40 to 44	45 to 49	50 to 54	55 to 59	60 to 64	65 to 69	70+	Total	85th %ile	Ave Speed
12:00 AM	0	2	3	1	1	0	1	0	0	0	0	0	0	8	30.8	25.3
1:00 AM	1	1	1	0	0	0	0	0	0	0	0	0	0	3	19.2	15.7
2:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0
3:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0
4:00 AM	1	0	0	1	0	0	0	0	0	0	0	0	0	2	24.6	19.0
5:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0
6:00 AM	2	9	9	3	0	1	0	0	0	0	0	0	0	24	24.6	20.9
7:00 AM	2	6	15	4	2	0	3	0	0	0	0	0	0	32	30.1	23.9
8:00 AM	2	4	15	5	3	1	2	0	0	0	0	0	0	32	32.1	24.5
9:00 AM	0	10	15	4	4	0	0	0	0	0	0	0	0	33	27.4	22.6
10:00 AM	7	12	20	15	0	2	0	0	0	0	0	0	0	56	26.0	21.6
11:00 AM	6	12	24	9	6	1	0	0	0	0	0	0	0	58	27.0	22.0
12:00 PM	1	14	20	11	3	1	0	0	0	0	0	0	0	50	27.0	22.7
1:00 PM	3	16	18	14	4	0	0	1	1	0	0	0	0	57	27.0	23.0
2:00 PM	2	13	28	19	3	0	0	0	0	0	1	0	0	66	27.3	23.5
3:00 PM	3	17	37	11	5	2	0	0	0	0	0	0	0	75	27.0	22.5
4:00 PM	2	19	36	19	7	0	1	0	0	0	0	0	0	84	27.0	23.0
5:00 PM	0	17	25	21	2	0	1	0	0	0	0	0	0	66	27.0	22.6
6:00 PM	2	13	28	17	2	1	0	0	0	0	0	0	0	63	26.7	22.7
7:00 PM	0	14	29	8	1	0	0	0	0	0	0	0	0	52	25.0	21.4
8:00 PM	0	12	21	6	3	0	0	0	0	0	0	0	0	42	26.9	22.2
9:00 PM	0	8	10	4	2	0	0	0	0	0	0	0	0	24	25.6	21.7
10:00 PM	0	3	6	4	1	0	0	0	0	0	0	0	0	14	28.1	23.3
11:00 PM	0	0	1	2	0	0	0	0	0	0	0	0	0	3	27.8	25.3
Total	34	202	361	178	49	9	8	1	1	0	1	0	0	844	27.0	22.6
Percent	4.03%	23.93%	42.77%	21.09%	5.81%	1.07%	0.95%	0.12%	0.12%	0.00%	0.12%	0.00%	0.00%			

AM Peak	10:00 AM	10:00 AM	11:00 AM	10:00 AM	11:00 AM	10:00 AM	7:00 AM								11:00 AM
Volume	7	12	24	15	6	2	3	0	0	0	0	0	0	0	58

PM Peak	1:00 PM	4:00 PM	3:00 PM	5:00 PM	4:00 PM	3:00 PM	4:00 PM	1:00 PM	1:00 PM		2:00 PM				4:00 PM
Volume	3	19	37	21	7	2	1	1	1	0	1	0	0	0	84

15th Percentile:	18.0 MPH	Average Speed:	22.6 MPH	Posted Speed Limit:	25 MPH
50th Percentile:	22.0 MPH	10 MPH Pace:	18 to 27 MPH	Number of Vehicles > 25 MPH:	200
85th Percentile:	27.0 MPH	Number in Pace:	617	Percent of Vehicles > 25 MPH:	23.7%
95th Percentile:	32.0 MPH	Percent in Pace:	73.1%		

Thames Street
south of Church Street
City, State: Bristol, RI
Client: F&O/ K. Patch
Site Code: TBA



PDI File #: 228763 ATR-A (Speed)

Count Date
Thursday, July 21, 2022

Speed (60-minute)

Combined NB and SB

Start Time:	1 to 14	15 to 19	20 to 24	25 to 29	30 to 34	35 to 39	40 to 44	45 to 49	50 to 54	55 to 59	60 to 64	65 to 69	70+	Total	85th %ile	Ave Speed
12:00 AM	0	3	5	1	1	0	1	0	0	0	0	0	0	11	28.5	24.3
1:00 AM	1	1	2	0	0	0	0	0	0	0	0	0	0	4	20.6	16.8
2:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0
3:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0
4:00 AM	1	1	0	1	0	0	0	0	0	0	0	0	0	3	24.0	18.3
5:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	1	20.0	20.0
6:00 AM	3	15	13	3	0	1	0	0	0	0	0	0	0	35	24.0	20.1
7:00 AM	3	7	24	7	2	0	3	0	0	0	0	0	0	46	26.5	23.3
8:00 AM	4	8	26	9	5	1	2	0	0	0	0	0	0	55	27.9	23.3
9:00 AM	4	12	27	9	4	0	0	0	0	0	0	0	0	56	26.0	21.8
10:00 AM	9	17	28	17	0	2	0	0	0	0	0	0	0	73	26.0	21.1
11:00 AM	12	20	36	11	6	1	0	0	0	0	0	0	0	86	26.0	20.7
12:00 PM	4	17	37	20	3	1	0	0	0	0	0	0	0	82	27.0	22.4
1:00 PM	5	23	37	22	7	0	0	1	1	0	0	0	0	96	27.0	22.7
2:00 PM	3	25	41	25	3	0	0	0	0	0	1	0	0	98	27.0	22.6
3:00 PM	9	29	55	13	5	2	0	0	0	0	0	0	0	113	25.2	21.4
4:00 PM	3	22	57	27	7	0	1	0	0	0	0	0	0	117	27.0	22.8
5:00 PM	0	23	36	29	2	0	1	0	0	0	0	0	0	91	27.0	22.4
6:00 PM	2	24	49	25	2	2	0	0	0	0	0	0	0	104	26.0	22.4
7:00 PM	1	23	35	10	1	0	0	0	0	0	0	0	0	70	24.7	21.0
8:00 PM	0	19	29	9	3	0	0	0	0	0	0	0	0	60	26.0	21.8
9:00 PM	0	12	13	5	2	1	0	0	0	0	0	0	0	33	26.0	21.9
10:00 PM	0	4	8	4	1	0	0	0	0	0	0	0	0	17	27.2	22.9
11:00 PM	0	1	2	3	1	0	0	0	0	0	0	0	0	7	29.2	24.6
Total	64	306	561	250	55	11	8	1	1	0	1	0	0	1258	27.0	22.1
Percent	5.09%	24.32%	44.59%	19.87%	4.37%	0.87%	0.64%	0.08%	0.08%	0.00%	0.08%	0.00%	0.00%			

AM Peak 11:00 AM 11:00 AM 11:00 AM 10:00 AM 11:00 AM 10:00 AM 7:00 AM 11:00 AM
Volume 12 20 36 17 6 2 3 0 0 0 0 0 0 86

PM Peak 3:00 PM 3:00 PM 4:00 PM 5:00 PM 1:00 PM 3:00 PM 4:00 PM 1:00 PM 1:00 PM 2:00 PM 4:00 PM
Volume 9 29 57 29 7 2 1 1 1 0 1 0 0 117

15th Percentile:	18.0 MPH	Average Speed:	22.1 MPH	Posted Speed Limit:	25 MPH
50th Percentile:	22.0 MPH	10 MPH Pace:	18 to 27 MPH	Number of Vehicles > 25 MPH:	249
85th Percentile:	27.0 MPH	Number in Pace:	936	Percent of Vehicles > 25 MPH:	19.8%
95th Percentile:	31.0 MPH	Percent in Pace:	74.4%		

Thames Street
south of Church Street
City, State: Bristol, RI
Client: F&O/ K. Patch
Site Code: TBA



PDI File #: 228763 ATR-A (Speed)

Count Date
Friday, July 22, 2022

Speed (60-minute)

NB																
Start Time:	1 to 14	15 to 19	20 to 24	25 to 29	30 to 34	35 to 39	40 to 44	45 to 49	50 to 54	55 to 59	60 to 64	65 to 69	70+	Total	85th %ile	Ave Speed
12:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	2	22.4	21.0
1:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	2	22.9	22.5
2:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	1	24.0	24.0
3:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0
4:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	2	18.6	17.5
5:00 AM	1	0	0	0	0	0	0	0	0	0	0	0	0	1	12.0	12.0
6:00 AM	2	6	1	1	0	0	0	0	0	0	0	0	0	10	19.3	16.9
7:00 AM	5	7	5	3	0	0	0	0	0	0	0	0	0	20	24.2	18.9
8:00 AM	6	8	6	5	0	0	0	0	0	0	0	0	0	25	25.4	19.2
9:00 AM	4	10	8	8	0	0	0	0	0	0	0	0	0	30	27.7	20.8
10:00 AM	4	5	4	8	0	0	0	0	0	0	0	0	0	21	27.0	20.5
11:00 AM	2	8	14	5	1	0	0	0	0	0	0	0	0	30	25.7	21.2
12:00 PM	2	9	24	10	2	0	0	0	0	0	0	0	0	47	26.1	22.3
1:00 PM	1	6	17	9	1	0	0	0	0	0	0	0	0	34	26.0	22.4
2:00 PM	1	8	9	11	1	0	0	0	0	0	0	0	0	30	27.7	22.5
3:00 PM	4	11	18	11	0	0	0	0	0	0	0	0	0	44	26.0	21.2
4:00 PM	5	16	20	13	1	0	0	0	0	0	0	0	0	55	26.0	21.0
5:00 PM	3	10	27	8	0	0	0	0	0	0	0	0	0	48	25.0	21.1
6:00 PM	0	11	23	6	0	1	0	0	0	0	0	0	0	41	25.0	21.6
7:00 PM	7	11	18	5	0	0	0	0	0	0	0	0	0	41	24.0	19.3
8:00 PM	8	10	15	4	0	0	0	0	0	0	0	0	0	37	23.0	18.8
9:00 PM	1	6	4	5	1	0	0	0	0	0	0	0	0	17	27.0	21.5
10:00 PM	0	3	6	2	2	0	0	0	0	0	0	0	0	13	28.4	23.2
11:00 PM	0	3	2	3	2	0	0	0	0	0	0	0	0	10	29.3	23.7
Total	56	151	225	117	11	1	0	0	0	0	0	0	0	561	26.0	20.9
Percent	9.98%	26.92%	40.11%	20.86%	1.96%	0.18%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%			

AM Peak	8:00 AM	9:00 AM	11:00 AM	9:00 AM	11:00 AM									9:00 AM
Volume	6	10	14	8	1	0	0	0	0	0	0	0	0	30

PM Peak	8:00 PM	4:00 PM	5:00 PM	4:00 PM	12:00 PM	6:00 PM								4:00 PM
Volume	8	16	27	13	2	1	0	0	0	0	0	0	0	55

15th Percentile:	16.0 MPH	Average Speed:	20.9 MPH	Posted Speed Limit:	25 MPH
50th Percentile:	21.0 MPH	10 MPH Pace:	18 to 27 MPH	Number of Vehicles > 25 MPH:	103
85th Percentile:	26.0 MPH	Number in Pace:	400	Percent of Vehicles > 25 MPH:	18.4%
95th Percentile:	28.0 MPH	Percent in Pace:	71.3%		

Thames Street
south of Church Street
City, State: Bristol, RI
Client: F&O/ K. Patch
Site Code: TBA



PDI File #: 228763 ATR-A (Speed)

Count Date
Friday, July 22, 2022

Speed (60-minute)

SB																
Start Time:	1 to 14	15 to 19	20 to 24	25 to 29	30 to 34	35 to 39	40 to 44	45 to 49	50 to 54	55 to 59	60 to 64	65 to 69	70+	Total	85th %ile	Ave Speed
12:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	1	19.0	19.0
1:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0
2:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0
3:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0
4:00 AM	0	0	1	2	0	0	0	0	0	0	0	0	0	3	27.4	25.7
5:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21.0	21.0
6:00 AM	1	9	5	5	1	0	0	0	0	0	0	0	0	21	26.0	20.9
7:00 AM	0	6	9	2	2	1	0	0	0	0	0	0	0	20	27.6	23.2
8:00 AM	2	3	19	6	1	1	1	0	0	0	0	0	0	33	28.0	23.3
9:00 AM	1	17	35	7	6	4	1	0	0	0	0	0	0	71	29.0	23.4
10:00 AM	5	15	26	13	2	2	0	0	0	0	0	0	0	63	27.0	21.9
11:00 AM	3	21	31	13	4	1	0	0	0	0	0	0	0	73	26.0	22.0
12:00 PM	4	9	26	9	2	3	0	0	0	0	0	0	0	53	27.0	22.5
1:00 PM	1	23	38	13	3	1	0	0	0	0	0	0	0	79	26.0	21.6
2:00 PM	0	18	34	11	4	0	2	0	0	0	0	0	0	69	26.0	22.6
3:00 PM	4	19	33	6	0	1	0	0	0	0	0	0	0	63	24.0	21.0
4:00 PM	3	21	46	13	1	3	0	0	0	0	0	0	0	87	25.0	21.6
5:00 PM	3	25	40	9	2	5	0	0	0	0	0	0	0	84	26.0	21.5
6:00 PM	2	26	37	7	6	0	0	0	0	0	0	0	0	78	25.0	21.2
7:00 PM	4	36	29	10	1	1	0	0	0	0	0	0	0	81	24.0	20.1
8:00 PM	6	35	36	12	3	1	0	0	0	0	0	0	0	93	25.0	20.5
9:00 PM	1	16	23	7	3	2	0	0	0	0	0	0	0	52	26.0	22.1
10:00 PM	3	10	13	2	3	0	0	0	0	0	0	0	0	31	25.5	20.9
11:00 PM	0	4	5	3	1	0	0	0	0	0	0	0	0	13	25.4	22.8
Total	43	314	487	150	45	26	4	0	0	0	0	0	0	1069	26.0	21.7
Percent	4.02%	29.37%	45.56%	14.03%	4.21%	2.43%	0.37%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%			

AM Peak	10:00 AM	11:00 AM	9:00 AM	10:00 AM	9:00 AM	9:00 AM	8:00 AM								11:00 AM	
Volume	5	21	35	13	6	4	1	0	0	0	0	0	0	0	73	
PM Peak	8:00 PM	7:00 PM	4:00 PM	1:00 PM	6:00 PM	5:00 PM	2:00 PM								8:00 PM	
Volume	6	36	46	13	6	5	2	0	0	0	0	0	0	0	93	

15th Percentile:	17.0 MPH	Average Speed:	21.7 MPH	Posted Speed Limit:	25 MPH
50th Percentile:	21.0 MPH	10 MPH Pace:	16 to 25 MPH	Number of Vehicles > 25 MPH:	179
85th Percentile:	26.0 MPH	Number in Pace:	813	Percent of Vehicles > 25 MPH:	16.7%
95th Percentile:	31.0 MPH	Percent in Pace:	76.1%		

Thames Street
south of Church Street
City, State: Bristol, RI
Client: F&O/ K. Patch
Site Code: TBA



PDI File #: 228763 ATR-A (Speed)

Count Date
Friday, July 22, 2022

Speed (60-minute)

Combined NB and SB

Start Time:	1 to 14	15 to 19	20 to 24	25 to 29	30 to 34	35 to 39	40 to 44	45 to 49	50 to 54	55 to 59	60 to 64	65 to 69	70+	Total	85th %ile	Ave Speed
12:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	3	21.8	20.3
1:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	2	22.9	22.5
2:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	1	24.0	24.0
3:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0
4:00 AM	0	2	1	2	0	0	0	0	0	0	0	0	0	5	26.8	22.4
5:00 AM	1	0	1	0	0	0	0	0	0	0	0	0	0	2	19.7	16.5
6:00 AM	3	15	6	6	1	0	0	0	0	0	0	0	0	31	25.0	19.6
7:00 AM	5	13	14	5	2	1	0	0	0	0	0	0	0	40	25.3	21.0
8:00 AM	8	11	25	11	1	1	1	0	0	0	0	0	0	58	27.0	21.5
9:00 AM	5	27	43	15	6	4	1	0	0	0	0	0	0	101	28.0	22.6
10:00 AM	9	20	30	21	2	2	0	0	0	0	0	0	0	84	27.0	21.6
11:00 AM	5	29	45	18	5	1	0	0	0	0	0	0	0	103	26.0	21.7
12:00 PM	6	18	50	19	4	3	0	0	0	0	0	0	0	100	27.0	22.4
1:00 PM	2	29	55	22	4	1	0	0	0	0	0	0	0	113	26.0	21.8
2:00 PM	1	26	43	22	5	0	2	0	0	0	0	0	0	99	27.0	22.6
3:00 PM	8	30	51	17	0	1	0	0	0	0	0	0	0	107	25.0	21.1
4:00 PM	8	37	66	26	2	3	0	0	0	0	0	0	0	142	25.0	21.4
5:00 PM	6	35	67	17	2	5	0	0	0	0	0	0	0	132	25.4	21.3
6:00 PM	2	37	60	13	6	1	0	0	0	0	0	0	0	119	25.0	21.3
7:00 PM	11	47	47	15	1	1	0	0	0	0	0	0	0	122	24.0	19.8
8:00 PM	14	45	51	16	3	1	0	0	0	0	0	0	0	130	24.7	20.0
9:00 PM	2	22	27	12	4	2	0	0	0	0	0	0	0	69	27.0	21.9
10:00 PM	3	13	19	4	5	0	0	0	0	0	0	0	0	44	27.0	21.6
11:00 PM	0	7	7	6	3	0	0	0	0	0	0	0	0	23	27.7	23.2
Total	99	465	712	267	56	27	4	0	0	0	0	0	0	1630	26.0	21.4
Percent	6.07%	28.53%	43.68%	16.38%	3.44%	1.66%	0.25%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%			

AM Peak 10:00 AM 11:00 AM 11:00 AM 10:00 AM 9:00 AM 9:00 AM 8:00 AM 11:00 AM
Volume 9 29 45 21 6 4 1 0 0 0 0 0 0 103

PM Peak 8:00 PM 7:00 PM 5:00 PM 4:00 PM 6:00 PM 5:00 PM 2:00 PM 4:00 PM
Volume 14 47 67 26 6 5 2 0 0 0 0 0 0 142

15th Percentile:	17.0 MPH	Average Speed:	21.4 MPH	Posted Speed Limit:	25 MPH
50th Percentile:	21.0 MPH	10 MPH Pace:	16 to 25 MPH	Number of Vehicles > 25 MPH:	282
85th Percentile:	26.0 MPH	Number in Pace:	1195	Percent of Vehicles > 25 MPH:	17.3%
95th Percentile:	30.0 MPH	Percent in Pace:	73.3%		

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Cars and Heavy Vehicles (Combined)

	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	3	58	2	0	63	7	1	1	0	9	0	35	3	0	38	3	1	2	0	6	116
7:15 AM	0	77	1	0	78	9	2	1	0	12	0	16	3	0	19	4	1	1	0	6	115
7:30 AM	0	79	2	0	81	6	3	0	0	9	0	24	1	0	25	6	4	1	0	11	126
7:45 AM	2	75	1	0	78	12	1	0	0	13	0	34	1	0	35	4	1	0	0	5	131
Total	5	289	6	0	300	34	7	2	0	43	0	109	8	0	117	17	7	4	0	28	488
8:00 AM	1	74	2	0	77	12	2	2	0	16	0	28	1	0	29	1	2	1	0	4	126
8:15 AM	2	93	2	0	97	9	2	1	0	12	0	26	1	0	27	2	3	3	0	8	144
8:30 AM	0	93	1	0	94	13	3	0	0	16	0	42	1	0	43	6	0	1	0	7	160
8:45 AM	0	84	5	0	89	13	0	1	0	14	1	41	4	0	46	5	3	1	0	9	158
Total	3	344	10	0	357	47	7	4	0	58	1	137	7	0	145	14	8	6	0	28	588
Grand Total	8	633	16	0	657	81	14	6	0	101	1	246	15	0	262	31	15	10	0	56	1076
Approach %	1.2	96.3	2.4	0.0		80.2	13.9	5.9	0.0		0.4	93.9	5.7	0.0		55.4	26.8	17.9	0.0		
Total %	0.7	58.8	1.5	0.0	61.1	7.5	1.3	0.6	0.0	9.4	0.1	22.9	1.4	0.0	24.3	2.9	1.4	0.9	0.0	5.2	
Exiting Leg Total	337					32					670					37					1076
Cars	8	612	15	0	635	80	13	4	0	97	1	233	15	0	249	31	14	9	0	54	1035
% Cars	100.0	96.7	93.8	0.0	96.7	98.8	92.9	66.7	0.0	96.0	100.0	94.7	100.0	0.0	95.0	100.0	93.3	90.0	0.0	96.4	96.2
Exiting Leg Total	322					30					647					36					1035
Heavy Vehicles	0	21	1	0	22	1	1	2	0	4	0	13	0	0	13	0	1	1	0	2	41
% Heavy Vehicles	0.0	3.3	6.3	0.0	3.3	1.2	7.1	33.3	0.0	4.0	0.0	5.3	0.0	0.0	5.0	0.0	6.7	10.0	0.0	3.6	3.8
Exiting Leg Total	15					2					23					1					41

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

8:00 AM		Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
		from North					from East					from South					from West					
		Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
8:00 AM		1	74	2	0	77	12	2	2	0	16	0	28	1	0	29	1	2	1	0	4	126
8:15 AM		2	93	2	0	97	9	2	1	0	12	0	26	1	0	27	2	3	3	0	8	144
8:30 AM		0	93	1	0	94	13	3	0	0	16	0	42	1	0	43	6	0	1	0	7	160
8:45 AM		0	84	5	0	89	13	0	1	0	14	1	41	4	0	46	5	3	1	0	9	158
Total Volume		3	344	10	0	357	47	7	4	0	58	1	137	7	0	145	14	8	6	0	28	588
% Approach Total		0.8	96.4	2.8	0.0		81.0	12.1	6.9	0.0		0.7	94.5	4.8	0.0		50.0	28.6	21.4	0.0		
PHF		0.375	0.925	0.500	0.000	0.920	0.904	0.583	0.500	0.000	0.906	0.250	0.815	0.438	0.000	0.788	0.583	0.667	0.500	0.000	0.778	0.919
Cars		3	334	9	0	346	47	7	2	0	56	1	130	7	0	138	14	8	5	0	27	567
Cars %		100.0	97.1	90.0	0.0	96.9	100.0	100.0	50.0	0.0	96.6	100.0	94.9	100.0	0.0	95.2	100.0	100.0	83.3	0.0	96.4	96.4
Heavy Vehicles		0	10	1	0	11	0	0	2	0	2	0	7	0	0	7	0	0	1	0	1	21
Heavy Vehicles %		0.0	2.9	10.0	0.0	3.1	0.0	0.0	50.0	0.0	3.4	0.0	5.1	0.0	0.0	4.8	0.0	0.0	16.7	0.0	3.6	3.6
Cars Enter Leg		3	334	9	0	346	47	7	2	0	56	1	130	7	0	138	14	8	5	0	27	567
Heavy Enter Leg		0	10	1	0	11	0	0	2	0	2	0	7	0	0	7	0	0	1	0	1	21
Total Entering Leg		3	344	10	0	357	47	7	4	0	58	1	137	7	0	145	14	8	6	0	28	588
Cars Exiting Leg		182					18					350					17					567
Heavy Exiting Leg		8					1					12					0					21
Total Exiting Leg		190					19					362					17					588

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Cars

	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	3	56	2	0	61	7	1	1	0	9	0	34	3	0	37	3	1	2	0	6	113
7:15 AM	0	72	1	0	73	8	1	1	0	10	0	16	3	0	19	4	1	1	0	6	108
7:30 AM	0	78	2	0	80	6	3	0	0	9	0	22	1	0	23	6	3	1	0	10	122
7:45 AM	2	72	1	0	75	12	1	0	0	13	0	31	1	0	32	4	1	0	0	5	125
Total	5	278	6	0	289	33	6	2	0	41	0	103	8	0	111	17	6	4	0	27	468
8:00 AM	1	72	1	0	74	12	2	1	0	15	0	25	1	0	26	1	2	1	0	4	119
8:15 AM	2	88	2	0	92	9	2	1	0	12	0	25	1	0	26	2	3	3	0	8	138
8:30 AM	0	93	1	0	94	13	3	0	0	16	0	41	1	0	42	6	0	1	0	7	159
8:45 AM	0	81	5	0	86	13	0	0	0	13	1	39	4	0	44	5	3	0	0	8	151
Total	3	334	9	0	346	47	7	2	0	56	1	130	7	0	138	14	8	5	0	27	567
Grand Total	8	612	15	0	635	80	13	4	0	97	1	233	15	0	249	31	14	9	0	54	1035
Approach %	1.3	96.4	2.4	0.0		82.5	13.4	4.1	0.0		0.4	93.6	6.0	0.0		57.4	25.9	16.7	0.0		
Total %	0.8	59.1	1.4	0.0	61.4	7.7	1.3	0.4	0.0	9.4	0.1	22.5	1.4	0.0	24.1	3.0	1.4	0.9	0.0	5.2	
Exiting Leg Total	322					30					647					36					1035

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

8:00 AM	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
8:00 AM	1	72	1	0	74	12	2	1	0	15	0	25	1	0	26	1	2	1	0	4	119
8:15 AM	2	88	2	0	92	9	2	1	0	12	0	25	1	0	26	2	3	3	0	8	138
8:30 AM	0	93	1	0	94	13	3	0	0	16	0	41	1	0	42	6	0	1	0	7	159
8:45 AM	0	81	5	0	86	13	0	0	0	13	1	39	4	0	44	5	3	0	0	8	151
Total Volume	3	334	9	0	346	47	7	2	0	56	1	130	7	0	138	14	8	5	0	27	567
% Approach Total	0.9	96.5	2.6	0.0		83.9	12.5	3.6	0.0		0.7	94.2	5.1	0.0		51.9	29.6	18.5	0.0		
PHF	0.375	0.898	0.450	0.000	0.920	0.904	0.583	0.500	0.000	0.875	0.250	0.793	0.438	0.000	0.784	0.583	0.667	0.417	0.000	0.844	0.892
Entering Leg	3	334	9	0	346	47	7	2	0	56	1	130	7	0	138	14	8	5	0	27	567
Exiting Leg					182					18					350					17	567
Total					528					74					488					44	1134

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class: **Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)**



	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
7:15 AM	0	5	0	0	5	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	7
7:30 AM	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	0	1	0	0	1	4
7:45 AM	0	3	0	0	3	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	6
Total	0	11	0	0	11	1	1	0	0	2	0	6	0	0	6	0	1	0	0	1	20
8:00 AM	0	2	1	0	3	0	0	1	0	1	0	3	0	0	3	0	0	0	0	0	7
8:15 AM	0	5	0	0	5	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	6
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
8:45 AM	0	3	0	0	3	0	0	1	0	1	0	2	0	0	2	0	0	1	0	1	7
Total	0	10	1	0	11	0	0	2	0	2	0	7	0	0	7	0	0	1	0	1	21
Grand Total	0	21	1	0	22	1	1	2	0	4	0	13	0	0	13	0	1	1	0	2	41
Approach %	0.0	95.5	4.5	0.0		25.0	25.0	50.0	0.0		0.0	100.0	0.0	0.0		0.0	50.0	50.0	0.0		
Total %	0.0	51.2	2.4	0.0	53.7	2.4	2.4	4.9	0.0	9.8	0.0	31.7	0.0	0.0	31.7	0.0	2.4	2.4	0.0	4.9	
Exiting Leg Total	15					2					23					1					41
Buses	0	7	0	0	7	1	0	0	0	1	0	7	0	0	7	0	0	0	0	0	15
% Buses	0.0	33.3	0.0	0.0	31.8	100.0	0.0	0.0	0.0	25.0	0.0	53.8	0.0	0.0	53.8	0.0	0.0	0.0	0.0	0.0	36.6
Exiting Leg Total	8					0					7					0					15
Single-Unit Trucks	0	11	1	0	12	0	1	2	0	3	0	5	0	0	5	0	1	1	0	2	22
% Single-Unit	0.0	52.4	100.0	0.0	54.5	0.0	100.0	100.0	0.0	75.0	0.0	38.5	0.0	0.0	38.5	0.0	100.0	100.0	0.0	100.0	53.7
Exiting Leg Total	6					2					13					1					22
Articulated Trucks	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	4
% Articulated	0.0	14.3	0.0	0.0	13.6	0.0	0.0	0.0	0.0	0.0	0.0	7.7	0.0	0.0	7.7	0.0	0.0	0.0	0.0	0.0	9.8
Exiting Leg Total	1					0					3					0					4

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

7:15 AM	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
7:15 AM	0	5	0	0	5	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	7
7:30 AM	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	0	1	0	0	0	1	4
7:45 AM	0	3	0	0	3	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	6
8:00 AM	0	2	1	0	3	0	0	1	0	1	0	3	0	0	3	0	0	0	0	0	0	7
Total Volume	0	11	1	0	12	1	1	1	0	3	0	8	0	0	8	0	1	0	0	0	1	24
% Approach Total	0.0	91.7	8.3	0.0		33.3	33.3	33.3	0.0		0.0	100.0	0.0	0.0		0.0	100.0	0.0	0.0			
PHF	0.000	0.550	0.250	0.000	0.600	0.250	0.250	0.250	0.000	0.375	0.000	0.667	0.000	0.000	0.667	0.000	0.250	0.000	0.000	0.250	0.857	
Buses	0	4	0	0	4	1	0	0	0	1	0	3	0	0	3	0	0	0	0	0	0	8
Buses %	0.0	36.4	0.0	0.0	33.3	100.0	0.0	0.0	0.0	33.3	0.0	37.5	0.0	0.0	37.5	0.0	0.0	0.0	0.0	0.0	33.3	
Single-Unit Trucks	0	4	1	0	5	0	1	1	0	2	0	4	0	0	4	0	1	0	0	0	1	12
Single-Unit %	0.0	36.4	100.0	0.0	41.7	0.0	100.0	100.0	0.0	66.7	0.0	50.0	0.0	0.0	50.0	0.0	100.0	0.0	0.0	100.0	50.0	
Articulated Trucks	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4
Articulated %	0.0	27.3	0.0	0.0	25.0	0.0	0.0	0.0	0.0	0.0	0.0	12.5	0.0	0.0	12.5	0.0	0.0	0.0	0.0	0.0	16.7	
Buses	0	4	0	0	4	1	0	0	0	1	0	3	0	0	3	0	0	0	0	0	0	8
Single-Unit Trucks	0	4	1	0	5	0	1	1	0	2	0	4	0	0	4	0	1	0	0	0	1	12
Articulated Trucks	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4
Total Entering Leg	0	11	1	0	12	1	1	1	0	3	0	8	0	0	8	0	1	0	0	0	1	24
Buses	4					0					4					0					8	
Single-Unit Trucks	4					2					5					1					12	
Articulated Trucks	1					0					3					0					4	
Total Exiting Leg	9					2					12					1					24	

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Buses

	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	2
7:15 AM	0	1	0	0	1	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	2
7:30 AM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
7:45 AM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
Total	0	4	0	0	4	1	0	0	0	1	0	3	0	0	3	0	0	0	0	0	8
8:00 AM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
8:15 AM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	3
Total	0	3	0	0	3	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	7
Grand Total	0	7	0	0	7	1	0	0	0	1	0	7	0	0	7	0	0	0	0	0	15
Approach %	0.0	100.0	0.0	0.0		100.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	46.7	0.0	0.0	46.7	6.7	0.0	0.0	0.0	6.7	0.0	46.7	0.0	0.0	46.7	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	8					0					7					0					15

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

7:00 AM	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	2
7:15 AM	0	1	0	0	1	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	2
7:30 AM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
7:45 AM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
Total Volume	0	4	0	0	4	1	0	0	0	1	0	3	0	0	3	0	0	0	0	0	8
% Approach Total	0.0	100.0	0.0	0.0		100.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	1.000	0.000	0.000	1.000	0.250	0.000	0.000	0.000	0.250	0.000	0.750	0.000	0.000	0.750	0.000	0.000	0.000	0.000	0.000	1.000
Entering Leg	0	4	0	0	4	1	0	0	0	1	0	3	0	0	3	0	0	0	0	0	8
Exiting Leg	4					0					4					0					8
Total	8					1					7					0					16

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Single-Unit Trucks

	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
7:15 AM	0	1	0	0	1	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	1	0	0	1	2
7:45 AM	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	4
Total	0	4	0	0	4	0	1	0	0	1	0	3	0	0	3	0	1	0	0	1	9
8:00 AM	0	1	1	0	2	0	0	1	0	1	0	1	0	0	1	0	0	0	0	0	4
8:15 AM	0	4	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
8:45 AM	0	2	0	0	2	0	0	1	0	1	0	0	0	0	0	0	0	1	0	1	4
Total	0	7	1	0	8	0	0	2	0	2	0	2	0	0	2	0	0	1	0	1	13
Grand Total	0	11	1	0	12	0	1	2	0	3	0	5	0	0	5	0	1	1	0	2	22
Approach %	0.0	91.7	8.3	0.0		0.0	33.3	66.7	0.0		0.0	100.0	0.0	0.0		0.0	50.0	50.0	0.0		
Total %	0.0	50.0	4.5	0.0	54.5	0.0	4.5	9.1	0.0	13.6	0.0	22.7	0.0	0.0	22.7	0.0	4.5	4.5	0.0	9.1	
Exiting Leg Total	6					2					13					1					22

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

7:30 AM	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	1	0	0	1	2
7:45 AM	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	4
8:00 AM	0	1	1	0	2	0	0	1	0	1	0	1	0	0	1	0	0	0	0	0	4
8:15 AM	0	4	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
Total Volume	0	7	1	0	8	0	0	1	0	1	0	4	0	0	4	0	1	0	0	1	14
% Approach Total	0.0	87.5	12.5	0.0		0.0	0.0	100.0	0.0		0.0	100.0	0.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.438	0.250	0.000	0.500	0.000	0.000	0.250	0.000	0.250	0.000	0.500	0.000	0.000	0.500	0.000	0.250	0.000	0.000	0.250	0.875
Entering Leg	0	7	1	0	8	0	0	1	0	1	0	4	0	0	4	0	1	0	0	1	14
Exiting Leg					4					2		8			8					0	14
Total	12					3					12					1					28

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Articulated Trucks

	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
Grand Total	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	4
Approach %	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	75.0	0.0	0.0	75.0	0.0	0.0	0.0	0.0	0.0	0.0	25.0	0.0	0.0	25.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	1					0					3					0					4

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

7:15 AM	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:15 AM	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
Total Volume	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	4
% Approach Total	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.333
Entering Leg	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	4
Exiting Leg	1					0					3					0					4
Total	4					0					4					0					8

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**



Bicycles (on Roadway and Crosswalks)

	Hope Street (Route 114)							Constitution Street							Hope Street (Route 114)							Constitution Street							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
7:00 AM	0	0	0	0	0	0	0	0	2	0	0	0	0	2	0	1	0	0	0	0	1	1	0	0	0	0	0	1	4
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	1
Total	0	0	0	0	0	0	0	0	2	0	0	0	0	2	0	1	0	0	0	0	1	2	0	0	0	0	0	2	5
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	2	0	0	0	0	0	2	3
Total	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	2	1	0	0	0	0	3	4
Grand Total	0	0	0	0	0	0	0	0	2	0	0	0	1	3	0	1	0	0	0	0	1	4	1	0	0	0	0	5	9
Approach %	0.0	0.0	0.0	0.0	0.0	0.0		0.0	66.7	0.0	0.0	0.0	33.3		0.0	100.0	0.0	0.0	0.0	0.0		80.0	20.0	0.0	0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	22.2	0.0	0.0	0.0	11.1	33.3	0.0	11.1	0.0	0.0	0.0	0.0	11.1	44.4	11.1	0.0	0.0	0.0	0.0	55.6	
Exiting Leg Total	1							2							4							2							9

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

7:00 AM	Hope Street (Route 114)							Constitution Street							Hope Street (Route 114)							Constitution Street							Total	
	from North							from East							from South							from West								
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
7:00 AM	0	0	0	0	0	0	0	0	2	0	0	0	0	0	2	0	1	0	0	0	0	1	1	0	0	0	0	0	1	4
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	1
Total Volume	0	0	0	0	0	0	0	0	2	0	0	0	0	0	2	0	1	0	0	0	0	1	2	0	0	0	0	0	2	5
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.000	0.250	0.500	0.000	0.000	0.000	0.000	0.000	0.500	0.313
Entering Leg	0	0	0	0	0	0	0	0	2	0	2	0	0	0	2	0	1	0	0	0	0	1	2	0	0	0	0	0	2	5
Exiting Leg	1							0							2							2							5	
Total	1							2							3							4							10	

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Pedestrians

	Hope Street (Route 114)							Constitution Street							Hope Street (Route 114)							Constitution Street							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	1	0	1	0	0	0	0	1	0	1	3
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	4	1	5	0	0	0	0	3	1	4	0	0	0	0	2	1	3	12
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	3	5	8	0	0	0	0	4	1	5	0	0	0	0	2	1	3	16
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	4	3	7	0	0	0	0	1	5	6	0	0	0	0	0	2	2	15
Total	0	0	0	0	0	0	0	0	0	0	0	11	10	21	0	0	0	0	9	7	16	0	0	0	0	5	4	9	46
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	1	3	4	0	0	0	0	1	1	2	0	0	0	0	1	0	1	7
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	4	4	0	0	0	0	0	0	0	0	0	0	0	4	0	4	8
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	2	2	4	0	0	0	0	0	7	7	0	0	0	0	0	2	2	13
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0	1	2	3	0	0	0	0	0	3	3	8
Total	0	0	0	0	0	0	0	0	0	0	0	4	10	14	0	0	0	0	2	10	12	0	0	0	0	5	5	10	36
Grand Total	0	0	0	0	0	0	0	0	0	0	0	15	20	35	0	0	0	0	11	17	28	0	0	0	0	10	9	19	82
Approach %	0	0	0	0	0	0	0	0	0	0	0	42.9	57.1		0	0	0	0	39.3	60.7		0	0	0	0	52.6	47.4		
Total %	0	0	0	0	0	0	0	0	0	0	0	18.3	24.4	42.7	0	0	0	0	13.4	20.7	34.1	0	0	0	0	12.2	11	23.2	
Exiting Leg Total	0							35							28							19							82

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

7:15 AM	Hope Street (Route 114)							Constitution Street							Hope Street (Route 114)							Constitution Street							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	4	1	5	0	0	0	0	3	1	4	0	0	0	0	2	1	3	12
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	3	5	8	0	0	0	0	4	1	5	0	0	0	0	2	1	3	16
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	4	3	7	0	0	0	0	1	5	6	0	0	0	0	0	2	2	15
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	1	3	4	0	0	0	0	1	1	2	0	0	0	0	1	0	1	7
Total Volume	0	0	0	0	0	0	0	0	0	0	0	12	12	24	0	0	0	0	9	8	17	0	0	0	0	5	4	9	50
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	50.0	50.0		0.0	0.0	0.0	0.0	52.9	47.1		0.0	0.0	0.0	0.0	55.6	44.4		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.750	0.600	0.750	0.000	0.000	0.000	0.000	0.563	0.400	0.708	0.000	0.000	0.000	0.000	0.625	0.500	0.750	0.781
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	12	12	24	0	0	0	0	9	8	17	0	0	0	0	5	4	9	50
Exiting Leg	0							24							17							9							50
Total	0							48							34							18							100

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars and Heavy Vehicles (Combined)

	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	2	69	5	0	76	5	1	2	0	8	2	101	5	0	108	14	3	2	0	19	211
4:15 PM	2	85	6	0	93	1	2	0	0	3	1	98	5	0	104	12	3	1	0	16	216
4:30 PM	3	48	6	0	57	4	1	0	0	5	0	92	4	0	96	12	6	5	0	23	181
4:45 PM	3	74	3	0	80	3	2	0	0	5	0	97	7	0	104	16	1	3	0	20	209
Total	10	276	20	0	306	13	6	2	0	21	3	388	21	0	412	54	13	11	0	78	817
5:00 PM	2	78	10	0	90	2	1	1	0	4	1	90	6	0	97	9	2	2	0	13	204
5:15 PM	2	70	5	0	77	6	2	0	0	8	1	83	2	0	86	12	1	1	0	14	185
5:30 PM	3	59	5	0	67	5	4	0	0	9	0	85	3	0	88	13	2	4	0	19	183
5:45 PM	0	51	2	0	53	5	2	1	0	8	1	95	4	0	100	7	2	2	0	11	172
Total	7	258	22	0	287	18	9	2	0	29	3	353	15	0	371	41	7	9	0	57	744
Grand Total	17	534	42	0	593	31	15	4	0	50	6	741	36	0	783	95	20	20	0	135	1561
Approach %	2.9	90.1	7.1	0.0		62.0	30.0	8.0	0.0		0.8	94.6	4.6	0.0		70.4	14.8	14.8	0.0		
Total %	1.1	34.2	2.7	0.0	38.0	2.0	1.0	0.3	0.0	3.2	0.4	47.5	2.3	0.0	50.2	6.1	1.3	1.3	0.0	8.6	
Exiting Leg Total	792					68					633					68					1561
Cars	16	521	42	0	579	31	15	4	0	50	6	728	36	0	770	95	19	20	0	134	1533
% Cars	94.1	97.6	100.0	0.0	97.6	100.0	100.0	100.0	0.0	100.0	100.0	98.2	100.0	0.0	98.3	100.0	95.0	100.0	0.0	99.3	98.2
Exiting Leg Total	779					67					620					67					1533
Heavy Vehicles	1	13	0	0	14	0	0	0	0	0	0	13	0	0	13	0	1	0	0	1	28
% Heavy Vehicles	5.9	2.4	0.0	0.0	2.4	0.0	0.0	0.0	0.0	0.0	0.0	1.8	0.0	0.0	1.7	0.0	5.0	0.0	0.0	0.7	1.8
Exiting Leg Total	13					1					13					1					28

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	2	69	5	0	76	5	1	2	0	8	2	101	5	0	108	14	3	2	0	19	211
4:15 PM	2	85	6	0	93	1	2	0	0	3	1	98	5	0	104	12	3	1	0	16	216
4:30 PM	3	48	6	0	57	4	1	0	0	5	0	92	4	0	96	12	6	5	0	23	181
4:45 PM	3	74	3	0	80	3	2	0	0	5	0	97	7	0	104	16	1	3	0	20	209
Total Volume	10	276	20	0	306	13	6	2	0	21	3	388	21	0	412	54	13	11	0	78	817
% Approach Total	3.3	90.2	6.5	0.0		61.9	28.6	9.5	0.0		0.7	94.2	5.1	0.0		69.2	16.7	14.1	0.0		
PHF	0.833	0.812	0.833	0.000	0.823	0.650	0.750	0.250	0.000	0.656	0.375	0.960	0.750	0.000	0.954	0.844	0.542	0.550	0.000	0.848	0.946
Cars	9	270	20	0	299	13	6	2	0	21	3	379	21	0	403	54	12	11	0	77	800
Cars %	90.0	97.8	100.0	0.0	97.7	100.0	100.0	100.0	0.0	100.0	100.0	97.7	100.0	0.0	97.8	100.0	92.3	100.0	0.0	98.7	97.9
Heavy Vehicles	1	6	0	0	7	0	0	0	0	0	0	9	0	0	9	0	1	0	0	1	17
Heavy Vehicles %	10.0	2.2	0.0	0.0	2.3	0.0	0.0	0.0	0.0	0.0	0.0	2.3	0.0	0.0	2.2	0.0	7.7	0.0	0.0	1.3	2.1
Cars Enter Leg	9	270	20	0	299	13	6	2	0	21	3	379	21	0	403	54	12	11	0	77	800
Heavy Enter Leg	1	6	0	0	7	0	0	0	0	0	0	9	0	0	9	0	1	0	0	1	17
Total Entering Leg	10	276	20	0	306	13	6	2	0	21	3	388	21	0	412	54	13	11	0	78	817
Cars Exiting Leg	403					35					326					36					800
Heavy Exiting Leg	9					1					6					1					17
Total Exiting Leg	412					36					332					37					817

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars

	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	2	68	5	0	75	5	1	2	0	8	2	99	5	0	106	14	3	2	0	19	208
4:15 PM	2	81	6	0	89	1	2	0	0	3	1	96	5	0	102	12	3	1	0	16	210
4:30 PM	3	47	6	0	56	4	1	0	0	5	0	89	4	0	93	12	6	5	0	23	177
4:45 PM	2	74	3	0	79	3	2	0	0	5	0	95	7	0	102	16	0	3	0	19	205
Total	9	270	20	0	299	13	6	2	0	21	3	379	21	0	403	54	12	11	0	77	800
5:00 PM	2	77	10	0	89	2	1	1	0	4	1	89	6	0	96	9	2	2	0	13	202
5:15 PM	2	68	5	0	75	6	2	0	0	8	1	81	2	0	84	12	1	1	0	14	181
5:30 PM	3	56	5	0	64	5	4	0	0	9	0	85	3	0	88	13	2	4	0	19	180
5:45 PM	0	50	2	0	52	5	2	1	0	8	1	94	4	0	99	7	2	2	0	11	170
Total	7	251	22	0	280	18	9	2	0	29	3	349	15	0	367	41	7	9	0	57	733
Grand Total	16	521	42	0	579	31	15	4	0	50	6	728	36	0	770	95	19	20	0	134	1533
Approach %	2.8	90.0	7.3	0.0		62.0	30.0	8.0	0.0		0.8	94.5	4.7	0.0		70.9	14.2	14.9	0.0		
Total %	1.0	34.0	2.7	0.0	37.8	2.0	1.0	0.3	0.0	3.3	0.4	47.5	2.3	0.0	50.2	6.2	1.2	1.3	0.0	8.7	
Exiting Leg Total	779					67					620					67					1533

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	2	68	5	0	75	5	1	2	0	8	2	99	5	0	106	14	3	2	0	19	208
4:15 PM	2	81	6	0	89	1	2	0	0	3	1	96	5	0	102	12	3	1	0	16	210
4:30 PM	3	47	6	0	56	4	1	0	0	5	0	89	4	0	93	12	6	5	0	23	177
4:45 PM	2	74	3	0	79	3	2	0	0	5	0	95	7	0	102	16	0	3	0	19	205
Total Volume	9	270	20	0	299	13	6	2	0	21	3	379	21	0	403	54	12	11	0	77	800
% Approach Total	3.0	90.3	6.7	0.0		61.9	28.6	9.5	0.0		0.7	94.0	5.2	0.0		70.1	15.6	14.3	0.0		
PHF	0.750	0.833	0.833	0.000	0.840	0.650	0.750	0.250	0.000	0.656	0.375	0.957	0.750	0.000	0.950	0.844	0.500	0.550	0.000	0.837	0.952
Entering Leg	9	270	20	0	299	13	6	2	0	21	3	379	21	0	403	54	12	11	0	77	800
Exiting Leg					403					35					326					36	800
Total					702					56					729					113	1600

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class: **Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)**



	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	3
4:15 PM	0	4	0	0	4	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	6
4:30 PM	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	4
4:45 PM	1	0	0	0	1	0	0	0	0	0	0	2	0	0	2	0	1	0	0	1	4
Total	1	6	0	0	7	0	0	0	0	0	0	9	0	0	9	0	1	0	0	1	17
5:00 PM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
5:15 PM	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	4
5:30 PM	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
5:45 PM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
Total	0	7	0	0	7	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	11
Grand Total	1	13	0	0	14	0	0	0	0	0	0	13	0	0	13	0	1	0	0	1	28
Approach %	7.1	92.9	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	100.0	0.0	0.0		
Total %	3.6	46.4	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	46.4	0.0	0.0	46.4	0.0	3.6	0.0	0.0	3.6	
Exiting Leg Total	13					1					13					1					28
Buses	0	9	0	0	9	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	18
% Buses	0.0	69.2	0.0	0.0	64.3	0.0	0.0	0.0	0.0	0.0	0.0	69.2	0.0	0.0	69.2	0.0	0.0	0.0	0.0	0.0	64.3
Exiting Leg Total	9					0					9					0					18
Single-Unit Trucks	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	0	1	0	0	1	7
% Single-Unit	0.0	15.4	0.0	0.0	14.3	0.0	0.0	0.0	0.0	0.0	0.0	30.8	0.0	0.0	30.8	0.0	100.0	0.0	0.0	100.0	25.0
Exiting Leg Total	4					1					2					0					7
Articulated Trucks	1	2	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
% Articulated	100.0	15.4	0.0	0.0	21.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	10.7
Exiting Leg Total	0					0					2					1					3

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	3
4:15 PM	0	4	0	0	4	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	6
4:30 PM	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	4
4:45 PM	1	0	0	0	1	0	0	0	0	0	0	2	0	0	2	0	1	0	0	1	4
Total Volume	1	6	0	0	7	0	0	0	0	0	0	9	0	0	9	0	1	0	0	1	17
% Approach Total	14.3	85.7	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.250	0.375	0.000	0.000	0.438	0.000	0.000	0.000	0.000	0.000	0.000	0.750	0.000	0.000	0.750	0.000	0.250	0.000	0.000	0.250	0.708
Buses	0	4	0	0	4	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	10
Buses %	0.0	66.7	0.0	0.0	57.1	0.0	0.0	0.0	0.0	0.0	0.0	66.7	0.0	0.0	66.7	0.0	0.0	0.0	0.0	0.0	58.8
Single-Unit Trucks	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	0	1	0	0	1	5
Single-Unit %	0.0	16.7	0.0	0.0	14.3	0.0	0.0	0.0	0.0	0.0	0.0	33.3	0.0	0.0	33.3	0.0	100.0	0.0	0.0	100.0	29.4
Articulated Trucks	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
Articulated %	100.0	16.7	0.0	0.0	28.6	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	11.8
Buses	0	4	0	0	4	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	10
Single-Unit Trucks	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	0	1	0	0	1	5
Articulated Trucks	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
Total Entering Leg	1	6	0	0	7	0	0	0	0	0	0	9	0	0	9	0	1	0	0	1	17
Buses	6					0					4					0					10
Single-Unit Trucks	3					1					1					0					5
Articulated Trucks	0					0					1					1					2
Total Exiting Leg	9					1					6					1					17

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Buses

	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	3
4:15 PM	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
4:30 PM	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	3
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Total	0	4	0	0	4	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	10
5:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
5:15 PM	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	3
5:30 PM	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
5:45 PM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
Total	0	5	0	0	5	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	8
Grand Total	0	9	0	0	9	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	18
Approach %	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	9					0					9					0					18

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	3
4:15 PM	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
4:30 PM	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	3
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Total Volume	0	4	0	0	4	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	10
% Approach Total	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.750	0.000	0.000	0.750	0.000	0.000	0.000	0.000	0.000	0.833
Entering Leg	0	4	0	0	4	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	10
Exiting Leg					6					0		4			4					0	10
Total	10					0					10					0					20

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Single-Unit Trucks

	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
Total	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	0	1	0	0	1	5
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
Grand Total	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	0	1	0	0	1	7
Approach %	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	100.0	0.0	0.0		
Total %	0.0	28.6	0.0	0.0	28.6	0.0	0.0	0.0	0.0	0.0	0.0	57.1	0.0	0.0	57.1	0.0	14.3	0.0	0.0	14.3	
Exiting Leg Total	4					1					2					0					7

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:15 PM	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Total
4:15 PM	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
Total Volume	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	0	1	0	0	1	6
% Approach Total	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.500	0.000	0.250	0.000	0.000	0.250	0.500
Entering Leg	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	0	1	0	0	1	6
Exiting Leg					4					1					1					0	6
Total					5					1					5					1	12

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Articulated Trucks

	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Grand Total	1	2	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
Approach %	33.3	66.7	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	33.3	66.7	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	0					0					2					1					3

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Hope Street (Route 114)					Constitution Street					Hope Street (Route 114)					Constitution Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total Volume	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
% Approach Total	50.0	50.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.250	0.250	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.500
Entering Leg	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
Exiting Leg	0					0					1					1					2
Total	2					0					1					1					4

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



Bicycles (on Roadway and Crosswalks)

	Hope Street (Route 114)							Constitution Street							Hope Street (Route 114)							Constitution Street							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	2	0	0	0	0	0	0	0	2
4:15 PM	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	0	1	0	2	0	0	0	0	0	0	3	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:15 PM	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	1	0	0	0	1	3	
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	1	0	0	0	1	3	
Grand Total	0	1	0	0	0	0	1	0	0	0	2	0	2	0	0	1	0	1	0	2	0	1	0	0	0	0	1	6	
Approach %	0.0	100.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	100.0	0.0		0.0	0.0	50.0	0.0	50.0	0.0		0.0	100.0	0.0	0.0	0.0	0.0		
Total %	0.0	16.7	0.0	0.0	0.0	0.0	16.7	0.0	0.0	0.0	0.0	33.3	0.0	33.3	0.0	0.0	16.7	0.0	16.7	0.0	33.3	0.0	16.7	0.0	0.0	0.0	16.7		
Exiting Leg Total	0							3							2							1							6

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Hope Street (Route 114)							Constitution Street							Hope Street (Route 114)							Constitution Street							Total	
	from North							from East							from South							from West								
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	2	0	0	0	0	0	0	0	2
4:15 PM	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total Volume	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	1	0	2	0	0	0	0	0	0	0	3
% Approach Total	0.0	100.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	50.0	0.0	50.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0		
PHF	0.000	0.250	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.375		
Entering Leg	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	1	0	2	0	0	0	0	0	0	3	
Exiting Leg	0							0							2							1							3	
Total	1							0							4							1							6	

PDI File #: **228763 A**
 Location: **N: Hope Street (Route 114) S: Hope Street (Route 114)**
 Location: **E: Constitution Street W: Constitution Street**
 City, State: **Bristol, RI**
 Client: **F&O/K. Patch**
 Site Code: **TBD**
 Count Date: **Thursday, July 21, 2022**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



Pedestrians

	Hope Street (Route 114)							Constitution Street							Hope Street (Route 114)							Constitution Street							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	0	1	0	1	4
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1	0	1	0	0	0	0	2	2	4	6
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	2
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	4	0	0	0	0	0	1	1	5
Total	0	0	0	0	0	0	0	0	0	0	0	4	0	4	0	0	0	0	5	2	7	0	0	0	0	3	3	6	17
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	2	2	4	0	0	0	0	2	1	3	0	0	0	0	1	2	3	10
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	2	3	5	0	0	0	0	1	3	4	0	0	0	0	0	3	3	12
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	3	5	0	0	0	0	1	0	1	0	0	0	0	2	1	3	9
Total	0	0	0	0	0	0	0	0	0	0	0	6	8	14	0	0	0	0	4	5	9	0	0	0	0	3	6	9	32
Grand Total	0	0	0	0	0	0	0	0	0	0	0	10	8	18	0	0	0	0	9	7	16	0	0	0	0	6	9	15	49
Approach %	0	0	0	0	0	0	0	0	0	0	0	55.6	44.4		0	0	0	0	56.3	43.8		0	0	0	0	40	60		
Total %	0	0	0	0	0	0	0	0	0	0	0	20.4	16.3	36.7	0	0	0	0	18.4	14.3	32.7	0	0	0	0	12.2	18.4	30.6	
Exiting Leg Total	0							18							16							15							49

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Hope Street (Route 114)							Constitution Street							Hope Street (Route 114)							Constitution Street							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	2	2	4	0	0	0	0	2	1	3	0	0	0	0	1	2	3	10
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	2	3	5	0	0	0	0	1	3	4	0	0	0	0	0	3	3	12
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	3	5	0	0	0	0	1	0	1	0	0	0	0	2	1	3	9
Total Volume	0	0	0	0	0	0	0	0	0	0	0	6	8	14	0	0	0	0	4	5	9	0	0	0	0	3	6	9	32
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	42.9	57.1		0.0	0.0	0.0	0.0	44.4	55.6		0.0	0.0	0.0	0.0	33.3	66.7		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.750	0.667	0.700	0.000	0.000	0.000	0.000	0.500	0.417	0.563	0.000	0.000	0.000	0.000	0.375	0.500	0.750	0.667
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	6	8	14	0	0	0	0	4	5	9	0	0	0	0	3	6	9	32
Exiting Leg	0							14							9							9							32
Total	0							28							18							18							64