

CHANGE ORDER NO. 4

Owner: City of Baxter
Engineer: Bolton & Menk, Inc.
Contractor: Anderson Brothers Construction
Project: 202-23 TH 210 & Inglewood Drive Improvement Project (SP 1805-84, SP 230-107-003, SAP 230-121-001, STBG 1822 (215))
Contract Name: City Improvement No. 4121
Date Issued: May 6, 2024
Owner's Project No.: 4121
Engineer's Project No.: T42.120675
Contractor's Project No.:
Effective Date of Change Order: Upon Council Approval

The Contract is modified as follows upon execution of this Change Order:

Description:

- See description in attached MnDOT Change Order No. 4

Attachments:

- MnDOT Change Order No. 4 document and supporting documentation.

Change in Contract Price		Change in Contract Times	
Original Contract Price:		Original Contract Times:	
\$ 2,127,824.08		Substantial Completion:	August 18, 2023
		Ready for final payment:	November 1, 2023
Increase from previously approved Change Orders 1 to 3:		Increase from previously approved Change Orders:	
\$ 207,545.17		Substantial Completion:	None
		Ready for final payment:	None
Contract Price prior to this Change Order:		Contract Times prior to this Change Order:	
\$ 2,335,369.25		Substantial Completion:	August 18, 2023
		Ready for final payment:	November 1, 2023
Increase this Change Order No. 4:		Increase this Change Order No. 4:	
\$ 75,022.27		Substantial Completion:	None
		Ready for final payment:	June 30, 2024
Contract Price incorporating this Change Order:		Contract Times with all approved Change Orders:	
\$ 2,410,391.52		Substantial Completion:	August 18, 2023
		Ready for final payment:	June 30, 2024

Recommended by Engineer
By: Bryan S. Dean
Title: Project Engineer
Date: 5/13/2024

Accepted by Contractor
Scott W. Leman
Division Manager
5-13-2024

Recommended by Utilities Commission
By: _____
Title: _____
Date: _____

Accepted by Owner (Authorized Signature)



STATE AID FOR LOCAL TRANSPORTATION CHANGE ORDER

Rev. February 2018

SP/SAP(s)	230-107-003 230-121-001	MN Project No.:	SP 1805-84, STBG 1822(215)	Change Order No.	4
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Project Location	T.H. 210, 2400' E of Highland Scenic RD (CSAH 48) to 5300' W of T.H. 371		
Local Agency	City of Baxter	Local Project No.	4114
Contractor	Anderson Brothers	Contract No.	1049387
Address/City/State/Zip	13190 Memorywood Drive, Baxter, MN 56425		
Total Change Order Amount \$		\$75,022.27	

- *Constructing two additional catch basins at the intersection of TH 210/Inglewood Drive to address MnDOT's concerns of the slow runoff of this area during rain events. The two catch basins will be connected by 12" and 15" RCP pipe.*
- *A Traffic Control setup for roughly this 3-day duration will be required to complete this change order.*

Estimate Of Cost: (Include any increases or decreases in contract items, any negotiated or force account items.)

**Group/ funding Category	Item No.	Description	Unit	Unit Price	+ or - Quantity	+ or - Amount \$
3	2021.501	MOBILIZATION	LS	\$16,412.97	+1	+\$16,412.97
3	2104.503	SAWING BIT PAVEMENT (FULL DEPTH)	LIN FT	\$2.60	+173	+\$449.80
3	2104.503	REMOVE CURB & GUTTER	LIN FT	\$7.85	+37	\$290.45
3	2104.504	REMOVE BITUMINOUS PAVEMENT	SQ YD	\$8.12	+132	+\$1,071.84
3	2106.507	SELECT GRANULAR EMBANKMENT (CV)	CY	\$30.50	+43	+\$1,311.50
3	2211.507	AGGREGATE BASE (CV) CLASS 6	CY	\$45.00	+22	+\$990.00
3	2232.501	MILL BIT SURFACE 1.5"	SQ YD	\$8.09	+339	+\$2,742.51
3	2360.509	TYPE SP 12.5 WEARING COURSE MIXTURE (3,C)	TON	\$110.00	+44	+\$4,840.00
3	2501.502	15" RC PIPE APRON	EACH	\$1,874.89	+1	+\$1,874.89
3	2503.503	12" RC PIPE SEWER DESIGN 3006 CL V	LIN FT	\$92.84	+33	+\$3,063.72
3	2503.503	15" RC PIPE SEWER DESIGN 3006 CL V	LIN FT	\$100.59	+37	+\$3,721.83
3	2506.502	CASTING ASSEMBLY	EACH	\$1,673.47	+2	+\$3,346.94
3	2506.503	CONST DRAINAGE STRUC DES G	LIN FT	\$1,559.55	+2.6	+\$4,054.83
3	2506.503	CONST DRAINAGE STRUC DES SD-48	LIN FT	\$1,714.40	+2.8	+\$4,800.32
3	2511.507	RANDOM RIP RAP CLASS III	CY	\$154.85	+9.5	+\$1,471.08
3	2554.502	GUIDE POST, TYPE SPECIAL	EACH	\$165.61	+1	+\$165.61
3	2563.601	TRAFFIC CONTROL	LS	\$14,678.60	+1	+\$14,678.60
3	2575.605	TURF ESTABLISHMENT	LS	\$3,850.00	+1	+\$3,850.00
3	2575.504	ROLLED EROS PREVENTION CAT 80A	LIN FT	\$148.52	+12	+\$1,782.24
3	2582.503	24" SOLID LINE MULTI COMP	LIN FT	\$9.92	+14	+\$138.88
3	2582.503	4" SOLID LINE MULTI COMP GR IN (WR)	LIN FT	\$1.77	+83	+\$146.91
3	2582.503	6" SOLID LINE MULTI COMP GR IN (WR)	LIN FT	\$2.21	+83	+\$183.43
3	2582.518	PAVEMENT MESS PREFORM THERMO GR IN (WR)	SQ FT	\$36.47	+16	+\$583.52
3	2582.518	CROSSWALK PREFORM THERMO GR IN	SQ FT	\$25.42	+120	+\$3,050.40
Net Change this Change Order						\$75,022.27



STATE AID FOR LOCAL TRANSPORTATION
CHANGE ORDER

Rev. February 2018

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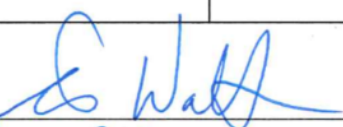
NOTES:


-Highlighted text equals negotiated price with contractor. See attachments for additional information and documentation. Non-Highlighted text is an overrun of an existing bid item.

****Group/funding category is required for federal aid projects**

- (1) Inglewood Drive/Knollwood Drive, S.P. 230-107-003, (A) 100% State Funds (Capped), Then 80% Federal/20% State Funds (Capped), Remainder 100% City of Baxter Funds
- (2) RCI – T.H. 210/Knollwood Drive (B) 100% State Funds
- (3) Inglewood Drive, S.P. 230-107-003, (C) 80% Federal/20% State Funds (Capped), Then 100% State Funds (Capped), Remainder 100% City of Baxter Funds
- (4) City of Baxter Foley Road, S.P. 230-121-001 (D) 80% Federal/20% City of Baxter Local Funds (Capped) Remainder 100% City of Baxter Local Funds
- (5) City of Baxter Knollwood Drive/Foley Road, S.P. 230-121-001 (E) 80% Federal/20% City of Baxter Funds (Capped) Remainder 100% City of Baxter Funds

Due to this change, the contract time: (check one)	
<input type="checkbox"/> Is NOT changed	<input checked="" type="checkbox"/> May be revised as provided in MnDOT Specification 1806
<input type="checkbox"/> Is Increased by _____ Working Days	<input checked="" type="checkbox"/> Is Increased by new completion date of June 30, 2024.
<input type="checkbox"/> Is Decreased by _____ Working Days	<input type="checkbox"/> Is Decreased by new completion date of _____.

Approved by Project Engineer:  Date: 3/14/24
Print Name: Trevor Waffer, PE Phone: 218-454-5110

Approved by Contractor:  Date: 3/14/24
Print Name: Scott W Hendrickson Phone: 218-820-9917

DSAE Portion: The State of Minnesota is not a participant in this contract. Signature by the District State Aid Engineer is for FUNDING PURPOSES ONLY and for compliance with State and Federal Aid Rules/Policy. Eligibility does not guarantee funds will be available.

This work is eligible for: ☒ Federal Funding ☒ State Aid Funding ☒ Local funds

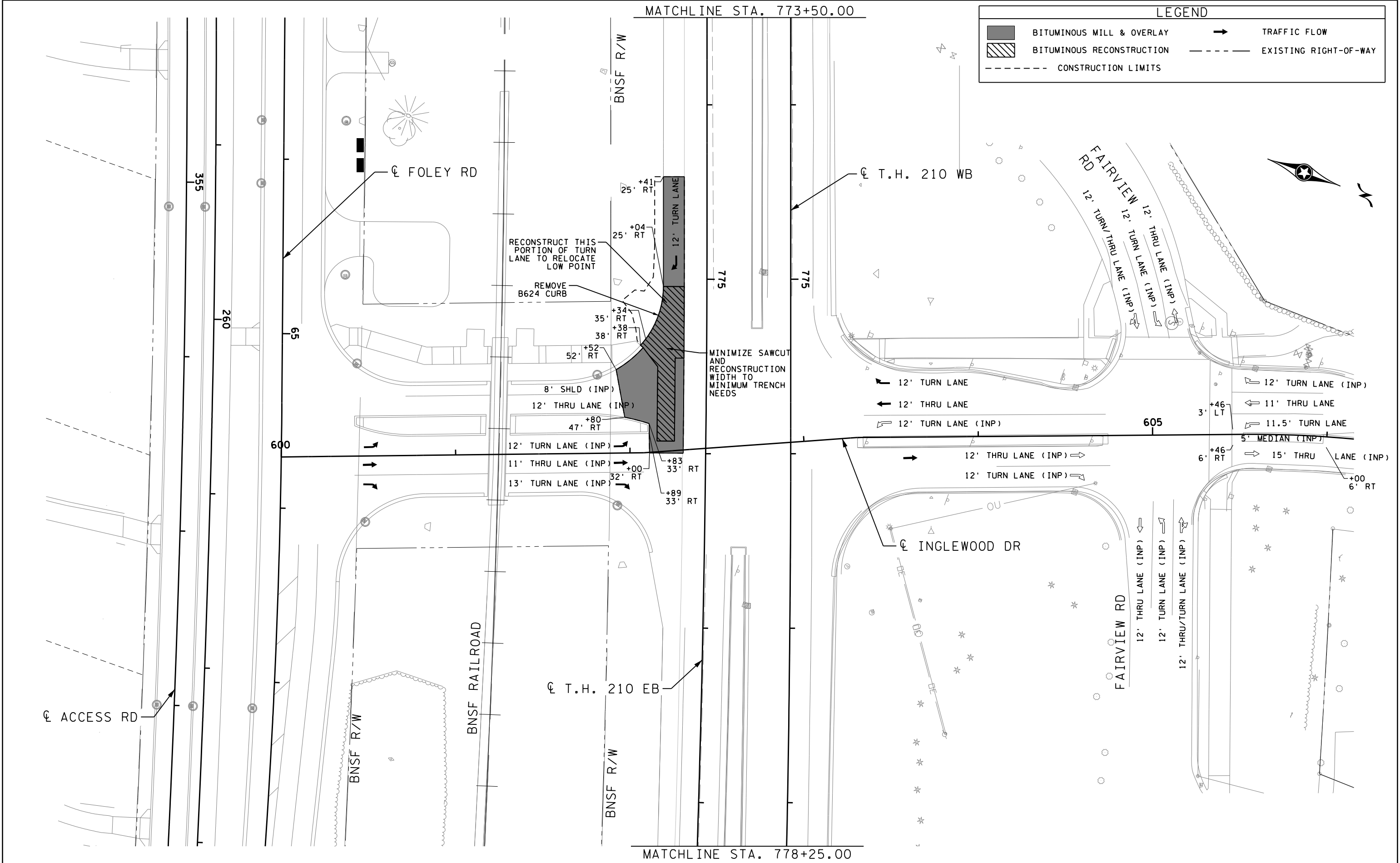
District State Aid Engineer: Andjela Tomovic Digitally signed by Andjela Tomovic
Date: 2024.05.06 10:37:41 -05'00' Date: _____

MnDOT D3 Representative: Kenneth Slama Digitally signed by Kenneth Slama
Date: 2024.05.06 09:32:40 -05'00' Date: _____

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alex.groff
12/14/2023



12224 NICOLLET AVENUE
BURNSVILLE, MINNESOTA 55337
Phone: (952) 890-0509
Email: BurnsVille@bolton-menk.com
www.bolton-menk.com

CHANGE ORDER #4

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
Derek J. Arens
DEREK J. ARENS
LIC. NO. 52673 DATE 02-15-2022

DESIGNED
AUG
DRAWN
SJP
CHECKED
DJA

S.P. 1805-84 (T.H. 210), S.P. 230-107-003, S.A.P. 230-121-001
INGLEWOOD DRIVE R/R CROSSING & ASSO. ROAD IMP.
CONSTRUCTION PLAN

SHEET
82
OF
170

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12/14/2023

LEGEND

X" CURB HEIGHT

CATCH BASINDRAINAGE STRUCTURE

DRAINAGE FLOW ARROW

XXX CONTROL POINTS AT GUTTER FLOW LINE

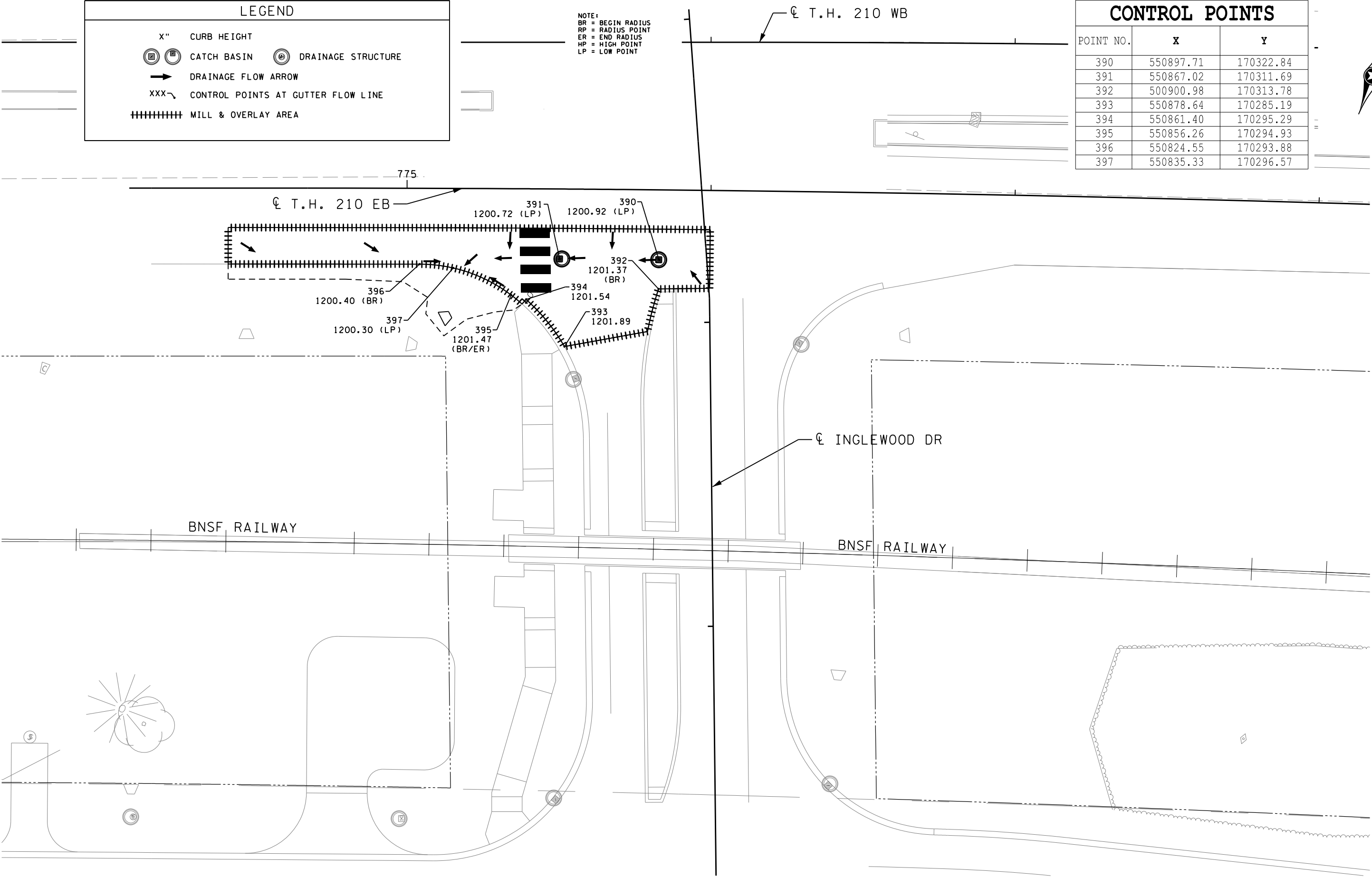
||||||| MILL & OVERLAY AREA

NOTE:
BR = BEGIN RADIUS
RP = RADIUS POINT
ER = END RADIUS
HP = HIGH POINT
LP = LOW POINT

CONTROL POINTS		
POINT NO.	X	Y
390	550897.71	170322.84
391	550867.02	170311.69
392	500900.98	170313.78
393	550878.64	170285.19
394	550861.40	170295.29
395	550856.26	170294.93
396	550824.55	170293.88
397	550835.33	170296.57



1



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CHANGE ORDER #4

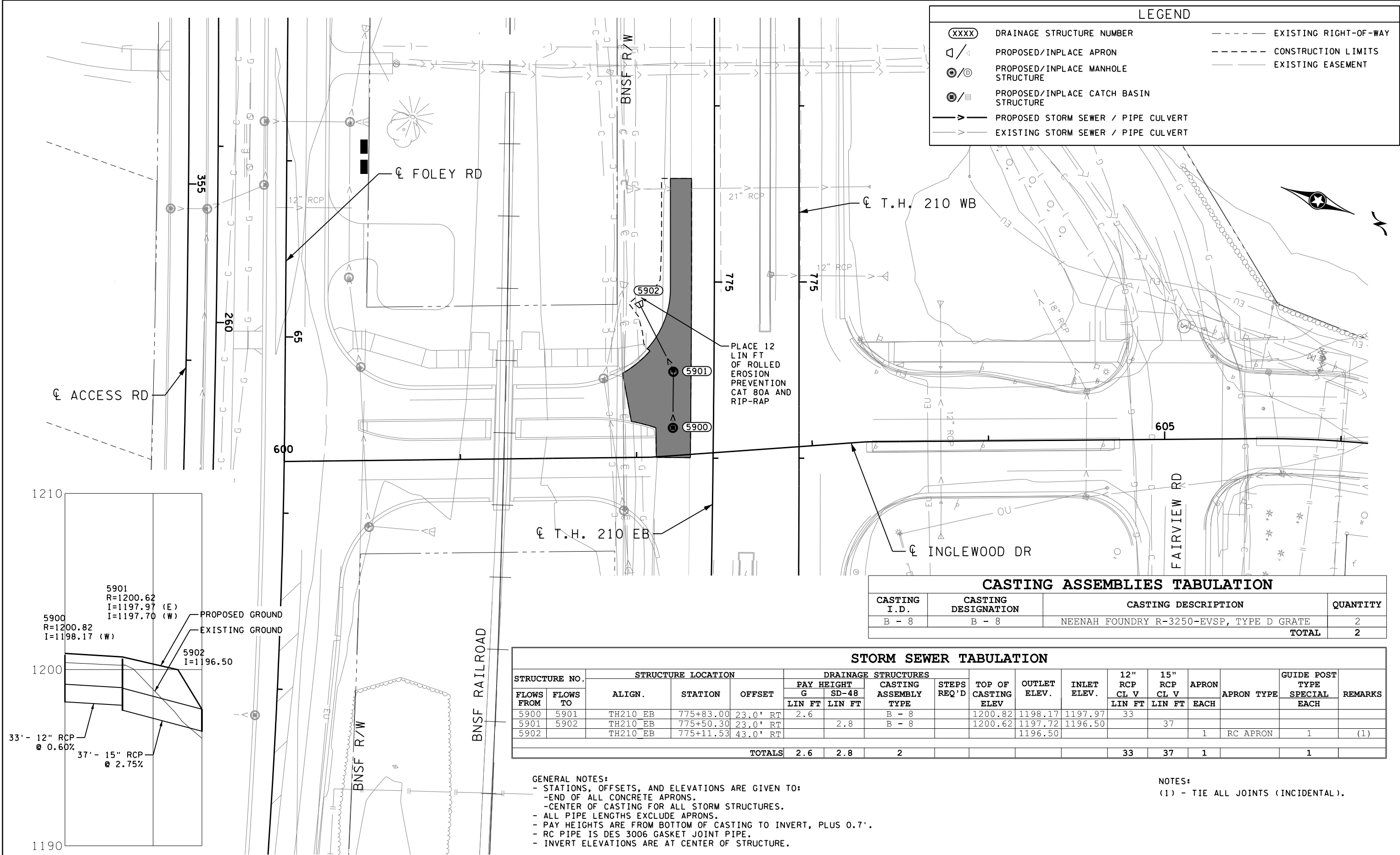
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
Derek J. Arens
DEREK J. ARENS
LIC. NO. 52673 DATE 02-15-2022

DESIGNED
AUG
DRAWN
SJP
CHECKED
DJA

S.P. 1805-84 (T.H. 210), S.P. 230-107-003, S.A.P. 230-121-001
INGLEWOOD DRIVE R/R CROSSING & ASSO. ROAD IMP.
INTERSECTION & ADA RAMP DETAILS

SHEET
87
OF
170

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12/14/2023



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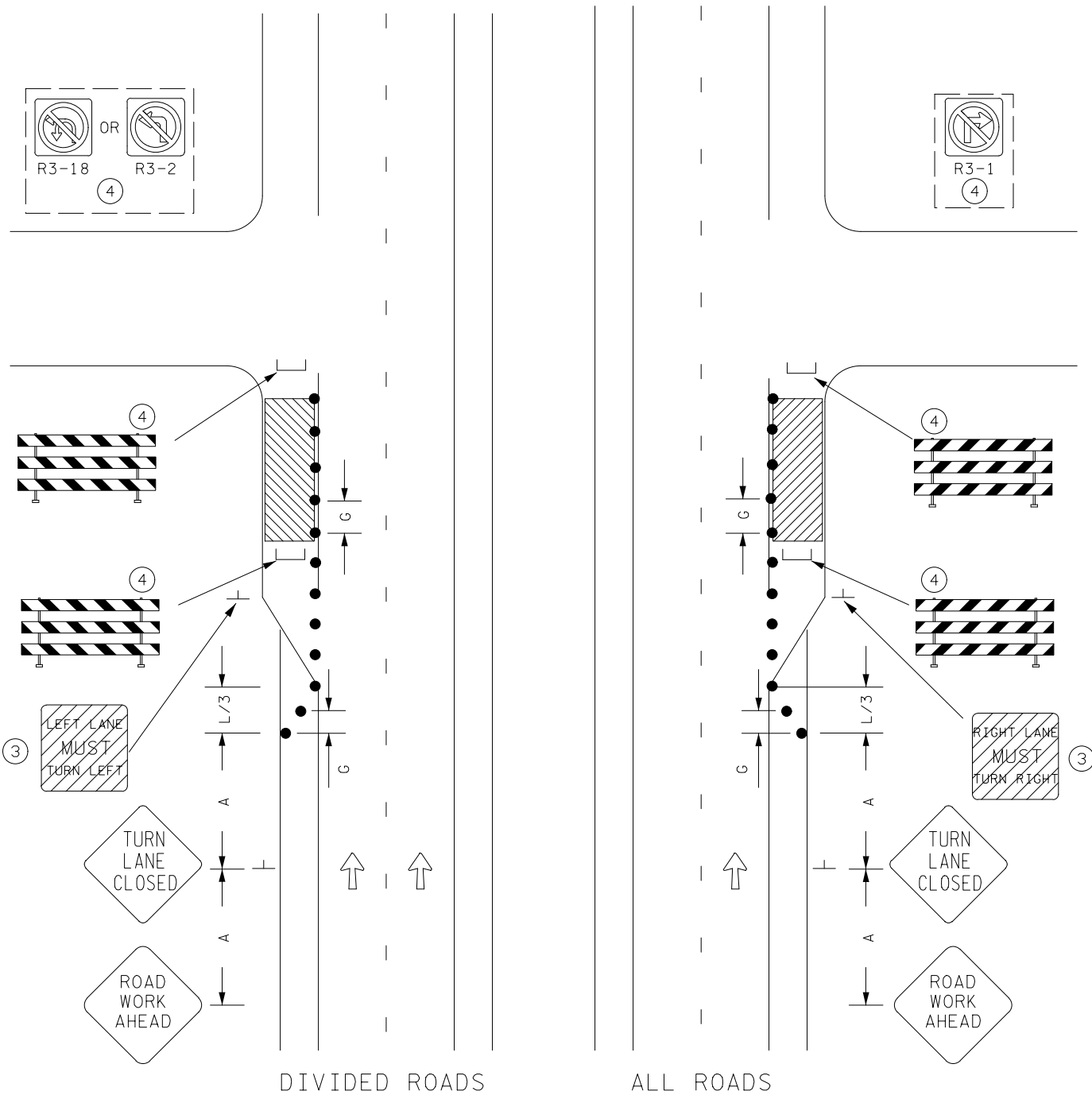
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

DEREK J. ARENS
LIC. NO. 52673 DATE 02-15-2022

DESIGNED
A.J.G.
DRAWN
S.J.P.
CHECKED
D.J.A.

S.P. 1805-84 (T.H. 210), S.P. 230-107-003, S.A.P. 230-121-001
INGLEWOOD DRIVE R/R CROSSING & ASSO. ROAD IMP.
DRAINAGE PLAN

SHEET
97
OF
170



TURN LANE CLOSURES

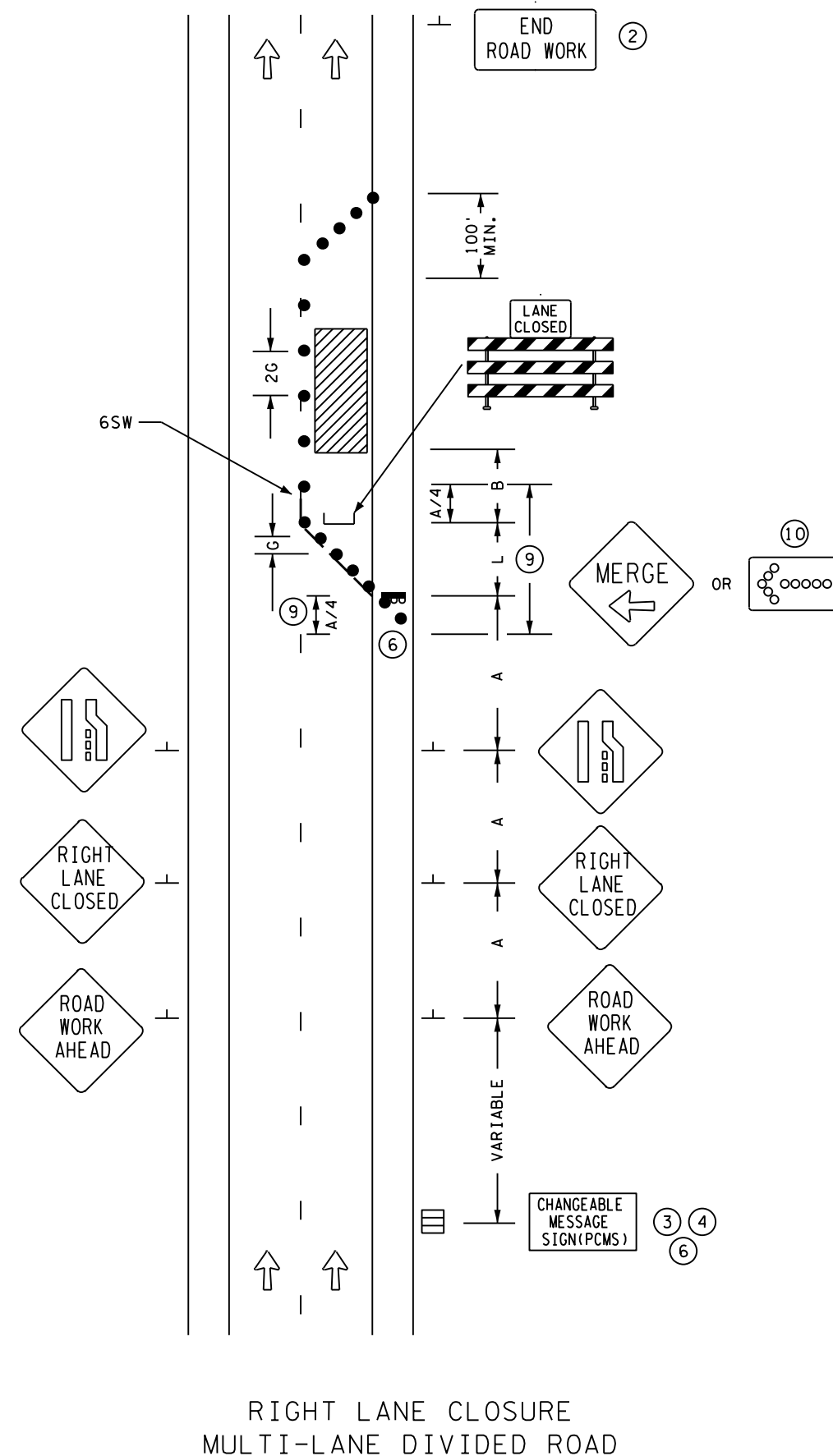
POSTED SPEED LIMIT PRIOR TO WORK STARTING	SPACING OF CHANNELIZING DEVICES (G)	SPACING OF ADVANCE WARNING SIGNS (A)	DECISION SIGHT DISTANCE	TAPER LENGTH (L)	SHIFTING TAPER (L/2)	TYPICAL SHOULDER TAPER (L/3)	BUFFER SPACE (B)
(MPH)	FEET	FEET	FEET	FEET	FEET	FEET	FEET
0 - 30	25	100	550	200	100	75	200
35 - 40		325	700	325	175	125	305
45 - 50		600	900	600	300	200	425
55	50	750	1200	700	350	250	500
60 - 65		1000	1400	800	400	275	650
70 - 75		1200	1600	900	450	300	820

ALL DISTANCES BASED ON 12 FT LANE WIDTHS.

- DESIGNER NOTES:
- TYPICAL APPLICATION FOR REFERENCE ONLY, NOT TO BE INSERTED INTO PLAN.
1. INSERT SPACING CHART DISTANCES INTO LAYOUTS.
 2. FOR ANY EXCAVATION OR DROP-OFF IN EXCESS OF 12 IN., SEE THE MINNESOTA DEPARTMENT OF TRANSPORTATION "TEMPORARY BARRIER GUIDANCE MANUAL".
 3. LIST ALL CONFLICTING INPLACE SIGNING, INCLUDING BUT NOT LIMITED TO OVERHEADS AND LANE DESIGNATION, THAT MUST BE MODIFIED, COVERED, AND/OR REMOVED. INCLUDE SIGN MODIFICATIONS, COVERS AND/OR REMOVALS IF THE TRAFFIC CONTROL PLAN.
 4. OPTIONAL R3-1, R3-2, OR R3-18 SIGNS MAY BE PLACED ON SIGN STANDS OR THE TOP OF BARRICADES ON SIDE CLOSEST TO TRAFFIC. SIGNS ARE REQUIRED IF TURNS ARE PROHIBITED.
 5. WHEN IMPACTING PEDESTRIAN FACILITIES, ALTERNATE PEDESTRIAN ACCESS ROUTES MUST BE PROVIDED. SEE LAYOUTS 6J-24 AND 6J-25 IN THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 6. CONTACT APPROPRIATE ROAD AUTHORITY FOR SIGNAL TIMING, SIGNAL HEAD LOCATION MODIFICATIONS, AND POSSIBLE TURN RESTRICTIONS BEFORE BEGINNING WORK AT OR NEAR ANY SIGNALIZED INTERSECTION.
 7. MASK OR REMOVE ALL CONFLICTING PAVEMENT MARKINGS. DETERMINE QUANTITY OF MARKINGS TO BE MASKED OR REMOVED FOR PAY ITEM QUANTITIES.
 8. ALL TEMPORARY PAVEMENT MARKINGS SHALL BE A WET REFLECTIVE MATERIAL.

● DRUMS

NOT TO SCALE



POSTED SPEED LIMIT PRIOR TO WORK STARTING	SPACING OF CHANNELIZING DEVICES (G)	SPACING OF ADVANCE WARNING SIGNS (A)	DECISION SIGHT DISTANCE	TAPER LENGTH (L)	SHIFTING TAPER (L/2)	TYPICAL SHOULDER TAPER (L/3)	BUFFER SPACE (B)
(MPH)	FEET	FEET	FEET	FEET	FEET	FEET	FEET
0 - 30	25	100	550	200	100	75	200
35 - 40		325	700	325	175	125	305
45 - 50	50	600	900	600	300	200	425
55		750	1200	700	350	250	500
60 - 65		1000	1400	800	400	275	650
70 - 75		1200	1600	900	450	300	820

ALL DISTANCES BASED ON 12 FT LANE WIDTHS.

DESIGNER NOTES:

TYPICAL APPLICATION FOR REFERENCE ONLY, NOT TO BE INSERTED INTO PLAN.

1. INSERT SPACING CHART DISTANCES INTO LAYOUTS.
- ② DETERMINE IF "END ROAD WORK" SIGNS ARE NEEDED.
- ③ CONSIDER THE INSTALLATION OF A PCMS AND/OR G20-X2 OR SPECIAL SIGN IF CONGESTION IS EXPECTED, IF ADVANCE SIGNING TO DIVERT TRAFFIC IS NEEDED, OR OTHER CONDITIONS DETERMINED BY THE ENGINEER.
- ④ CONSIDER THE INSTALLATION OF A PCMS AND/OR G20-X1 SIGN (MODIFIED) OR G20-X2 SIGN FOR SEVEN DAY ADVANCE WARNING OF RESTRICTION.
5. FOR ANY EXCAVATION OR DROP-OFF IN EXCESS OF 12 IN., SEE THE MINNESOTA DEPARTMENT OF TRANSPORTATION "TEMPORARY BARRIER GUIDANCE MANUAL".
- ⑥ SEE FIELD MANUAL LAYOUT *7 FOR PROPER SHOULDER DELINEATION FOR TRAILER MOUNTED TRAFFIC CONTROL DEVICES.
7. IF 48"x48" ADVANCE WARNING SIGNS WILL NOT FIT ON THE LEFT SIDE BECAUSE OF A NARROW MEDIAN (LESS THAN 6 FT.)
 - A. REDUCE THE LEFT SIDE SIGN SIZES TO 36"x36" OR
 - B. ELIMINATE THE LEFT SIDE SIGNING, USE AN ADDITIONAL "RIGHT LANE CLOSED" SIGN ON THE RIGHT, AND DISPLAY THE FLASHING ARROWBOARD ON THE SHOULDER.
8. IF BACKUPS ARE EXPECTED CONSIDER USE OF LAYOUT "LANE CLOSURE WITH ZIPPER MERGE".
- ⑨ MASK OR REMOVE ALL CONFLICTING PAVEMENT MARKINGS. DETERMINE QUANTITY OF MARKINGS TO BE MASKED OR REMOVED FOR PAY ITEM QUANTITIES.
- ⑩ THE FLASHING ARROW BOARD SHALL BE USED WHEN THE POSTED SPEED LIMIT IS 45 MPH OR GREATER, AND SHALL BE PLACED FULLY ON THE SHOULDER. IF THERE IS NO SHOULDER, OR THE SHOULDER IS TOO NARROW, PLACE THE FLASHING ARROW BOARD AT THE END OF THE TAPER IN LIEU OF THE TYPE III BARRICADE ASSEMBLY.
11. ALL TEMPORARY PAVEMENT MARKINGS SHALL BE A WET REFLECTIVE MATERIAL.
12. LIST ALL CONFLICTING INPLACE SIGNING THAT MUST BE MODIFIED, COVERED, AND/OR REMOVED. INCLUDE SIGN MODIFICATIONS, COVERS AND/OR REMOVALS IN THE TRAFFIC CONTROL PLAN.

6SW - 6 INCH SOLID LINE WHITE WET REFLECTIVE

● DRUMS

— — - TEMPORARY WET REFLECTIVE
PAVEMENT MARKING.