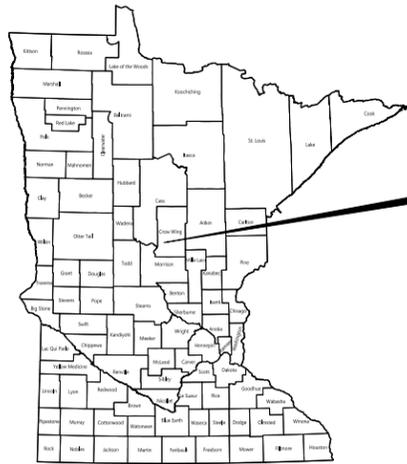


FIVE BELOW - BAXTER

BAXTER, MINNESOTA
FEBRUARY, 2026



PROJECT LOCATION
BAXTER, CROW WING COUNTY

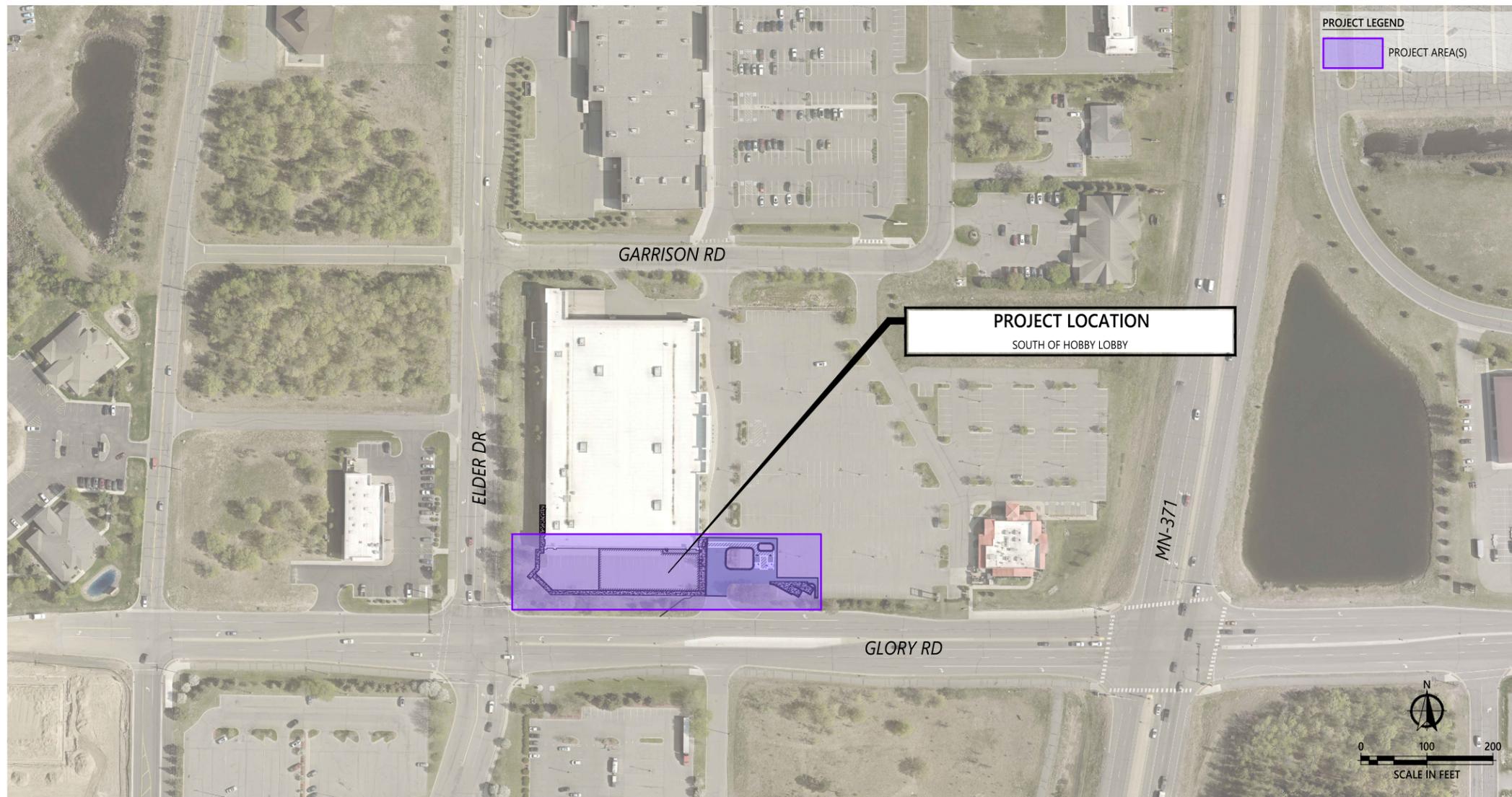
GOVERNING SPECIFICATIONS

THE 2025 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" AND THE 2025 SCHEDULE OF "MATERIALS CONTROL FOR 2025 STANDARD SPECIFICATIONS" SHALL GOVERN.

SHEET INDEX TABLE

SHEET NUMBERS	SHEET TITLE
C1.00	SHEET TITLE
C1.01	CIVIL LEGEND
C2.01-C2.18	CIVIL DETAILS
C3.01	EXISTING CONDITIONS & REMOVALS PLAN
C4.01	SITE PLAN
C5.01-C5.02	UTILITY PLAN
C6.01	SWPPP NARRATIVE
C6.02	SWPPP MAPS
C7.01-C7.03	GRADING PLAN
C8.01	EROSION CONTROL PLAN

THIS PLAN CONTAINS 30 SHEETS



MAP DESCRIPTION: PARTIAL MAP OF BAXTER, CROW WING COUNTY, MINNESOTA

THIS PLAN SET CONTAINS COLOR GRAPHICS. BLACK AND WHITE PRINTS ARE NOT OFFICIAL SIGNED DOCUMENTS.



DRAWN BY:
MJM
CHECKED BY:
ALW

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Thomas M. Rients
THOMAS M. RIENTS
LIC. NO.: 63111 DATE: 02/09/2026

DATE	REV#	REVISIONS DESCRIPTION

FIVE BELOW - BAXTER
TITLE SHEET

C1.00
PROJECT #: 2025-12133

SURVEY MONUMENTS - FOUND

- FOUND CAST IRON MONUMENT
- FOUND CONTROL POINT
- FOUND IRON PIPE
- FOUND JUDICIAL LAND MONUMENT
- FOUND LATH

SURVEY MONUMENTS - STAKED

- BENCHMARK
- STAKED CAST IRON MONUMENT
- STAKED CONTROL POINT
- STAKED IRON PIPE
- STAKED JUDICIAL LAND MONUMENT

PROPERTY LINEWORK

- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
- RIGHT-OF-WAY LINE
- EASEMENT LINE
- DESCRIPTION LINE
- DEEDED LOT LINE
- PLATTED LOT LINE
- BOUNDARY LINE
- SUBDIVISION LINE
- SETBACK LINE

TOPOGRAPHIC INFORMATION - EXISTING

- 1230 MAJOR CONTOURS
- 1234 MINOR CONTOURS
- 1234.56 SPOT ELEVATION

TOPOGRAPHIC INFORMATION - PROPOSED

- 1230 MAJOR CONTOURS (WITH LABEL)
- 1234 MINOR CONTOURS (WITH LABEL)
- 1234.56 SPOT ELEVATION
- +1.23% SLOPE - PERCENT
- 1:2 SLOPE - RISE : RUN
- TBC: 1234.56
FLW: 1234.06 SPOT ELEVATIONS @ CURB (TBC: TOP BACK CURB / FLW: CURB FLOWLINE)
- +1.23, -3.21 SPOT ELEVATIONS, CUT/FILL (GREEN: FILL DEPTH / RED: CUT DEPTH)

VEGETATION SYMBOLS - EXISTING

- SHRUB
- STUMP
- TREE - CONIFEROUS
- TREE - DECIDUOUS
- WETLAND

VEGETATION LINEWORK - EXISTING

- EDGE OF WOODS
- EDGE OF WETLANDS

VEGETATION SYMBOLS - PROPOSED

- SHRUB
- TREE - CONIFER
- TREE - DECIDUOUS

VEGETATION LINEWORK - PROPOSED

- EDGE OF WOODS

ROADWAY/PAVEMENT LINEWORK - EXISTING

- BITUMINOUS SURFACE
- CONCRETE SURFACE
- CURB & GUTTER
- GRAVEL SURFACE
- ROADWAY/DRIVE/TRAIL CENTERLINE

ROADWAY/PAVEMENT LINEWORK - PROPOSED

- BITUMINOUS SURFACE
- CONCRETE SURFACE
- CURB & GUTTER
- GRAVEL SURFACE
- ROADWAY/DRIVE/TRAIL CENTERLINE

SITE SYMBOLS - EXISTING

- FENCE POST
- FLAG POLE
- GUARD POST (BOLLARD)
- GUY POLE
- GUY WIRE ANCHOR
- ACCESSIBLE PARKING MARKING
- MAILBOX
- SIGN - DOUBLE POST
- SIGN - SINGLE
- SIGN - FIRE #

SITE LINEWORK - EXISTING

- BUILDING
- BARB WIRE FENCE
- CHAINLINK FENCE
- WOOD FENCE
- RAILROAD
- RETAINING WALL

SITE SYMBOLS - PROPOSED

- FENCE POST
- FLAG POLE
- GUARD POST (BOLLARD)
- GUY POLE
- GUY WIRE ANCHOR
- MAILBOX
- SIGN - DOUBLE POST
- SIGN - FIRE #
- SIGN - SINGLE

SITE LINEWORK - PROPOSED

- BUILDING
- FENCE - BARB WIRE
- FENCE - CHAINLINK
- FENCE - WOOD
- RETAINING WALL

COMMUNICATION/TELECOM UTILITY SYMBOLS - EXISTING

- TELECOM HANDHOLE
- TELECOM MANHOLE
- TELECOM PEDESTAL
- TELECOM POLE
- TV HANDHOLE
- TV PEDESTAL
- SATELLITE DISH

COMMUNICATION/TELECOM UTILITY LINEWORK - EXISTING

- UNDERGROUND FIBER OPTIC LINE
- OVERHEAD TELEPHONE LINE
- UNDERGROUND TELEPHONE LINE
- OVERHEAD TV LINE
- UNDERGROUND TV LINE

COMMUNICATION/TELECOM UTILITY SYMBOLS - PROPOSED

- TELECOM HANDHOLE
- TELECOM MANHOLE
- TELECOM PEDESTAL
- TELECOM POLE
- TV HANDHOLE
- TV PEDESTAL
- SATELLITE DISH

COMMUNICATION/TELECOM UTILITY LINEWORK - PROPOSED

- UNDERGROUND FIBER OPTIC LINE
- UNDERGROUND TELEPHONE LINE
- OVERHEAD TV LINE
- UNDERGROUND TV LINE

ELECTRICAL UTILITY SYMBOLS - EXISTING

- AC UNIT
- GENERATOR
- GROUND LIGHT (FLOODLIGHT)
- ELECTRIC HANDHOLE
- LIGHT POLE
- ELECTRIC MANHOLE
- ELECTRIC METER
- ELECTRIC PEDESTAL
- ELECTRIC POLE
- TRAFFIC SIGNAL
- TRANSFORMER
- YARD LIGHT

ELECTRICAL UTILITY LINEWORK - EXISTING

- OVERHEAD ELECTRIC LINE
- UNDERGROUND ELECTRIC LINE

ELECTRICAL UTILITY SYMBOLS - PROPOSED

- AC UNIT
- GENERATOR
- GROUND LIGHT (FLOODLIGHT)
- ELECTRIC HANDHOLE
- LIGHT POLE
- ELECTRIC MANHOLE
- ELECTRIC METER
- ELECTRIC PEDESTAL
- ELECTRIC POLE
- TRAFFIC SIGNAL
- TRANSFORMER
- YARD LIGHT

ELECTRICAL UTILITY LINEWORK - PROPOSED

- OVERHEAD ELECTRIC LINE
- UNDERGROUND ELECTRIC LINE

NATURAL GAS UTILITY SYMBOLS - EXISTING

- NATURAL GAS METER
- NATURAL GAS VALVE
- PROPANE TANK

NATURAL GAS UTILITY LINEWORK - EXISTING

- NATURAL GAS LINE

NATURAL GAS UTILITY SYMBOLS - PROPOSED

- NATURAL GAS METER
- NATURAL GAS VALVE
- PROPANE TANK

NATURAL GAS UTILITY LINEWORK - PROPOSED

- NATURAL GAS LINE

SANITARY SEWER SYMBOLS - EXISTING

- SANITARY AIR RELEASE MANHOLE
- SANITARY CLEANOUT
- LIFT STATION WET WELL
- LIFT STATION VALVE VAULT
- SANITARY MANHOLE
- SANITARY VALVE (FORCEMAIN)

SANITARY SEWER LINEWORK - EXISTING

- SANITARY FORCE MAIN
- SANITARY GRAVITY MAIN
- SANITARY SERVICE LINE

SANITARY SEWER SYMBOLS - PROPOSED

- SANITARY AIR RELEASE MANHOLE
- SANITARY CLEANOUT
- LIFT STATION WET WELL
- LIFT STATION VALVE VAULT
- SANITARY MANHOLE
- SANITARY VALVE (FORCEMAIN)

SANITARY SEWER LINEWORK - PROPOSED

- SANITARY FORCE MAIN
- SANITARY GRAVITY MAIN
- SANITARY SERVICE LINE

STORM SEWER SYMBOLS - EXISTING

- STORM SEWER CATCH BASIN
- STORM SEWER CLEANOUT
- CULVERT APRON
- STORM SEWER INLET STRUCTURE
- STORM SEWER MANHOLE

STORM SEWER LINEWORK - EXISTING

- STORM SEWER GRAVITY MAIN
- STORM SEWER DRAINTILE

STORM SEWER SYMBOLS - PROPOSED

- STORM SEWER CATCH BASIN
- STORM SEWER CLEANOUT
- STORM SEWER INLET STRUCTURE
- STORM SEWER MANHOLE

STORM SEWER LINEWORK - PROPOSED

- STORM SEWER GRAVITY MAIN
- STORM SEWER DRAINTILE

WATER SYMBOLS - EXISTING

- CURB STOP
- WATER HANDHOLE
- FIRE HYDRANT
- YARD HYDRANT
- WATER MANHOLE
- WATER METER
- WATER MAIN VALVE
- WATER WELL

WATER LINEWORK - EXISTING

- WATER MAIN LINE
- WATER SERVICE LINE

WATER SYMBOLS - PROPOSED

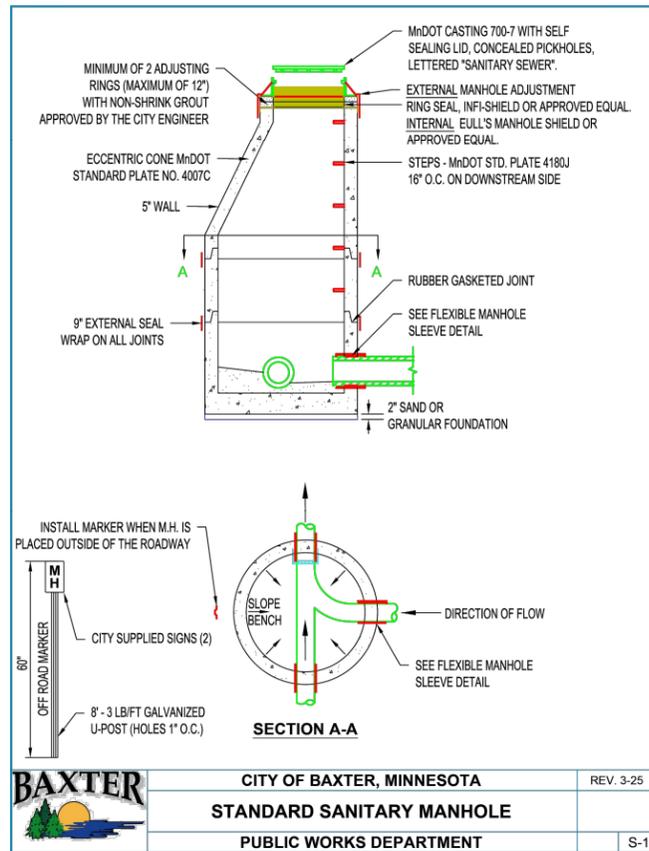
- CURB STOP
- HANDHOLE
- HYDRANT
- YARD HYDRANT
- MANHOLE
- WATER METER
- WATER MAIN VALVE
- WATER WELL

WATER LINEWORK - PROPOSED

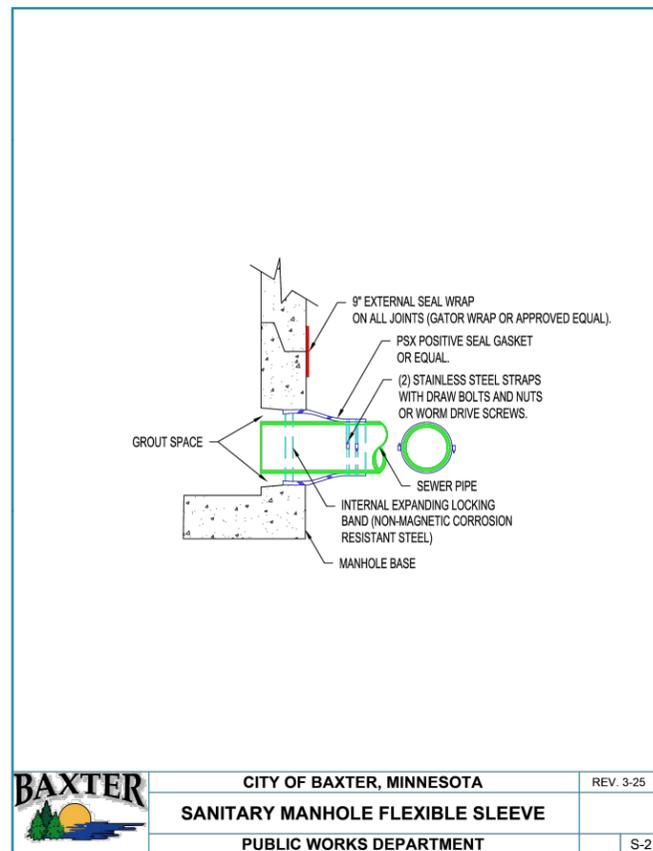
- WATER MAIN PIPE
- WATER SERVICE PIPE

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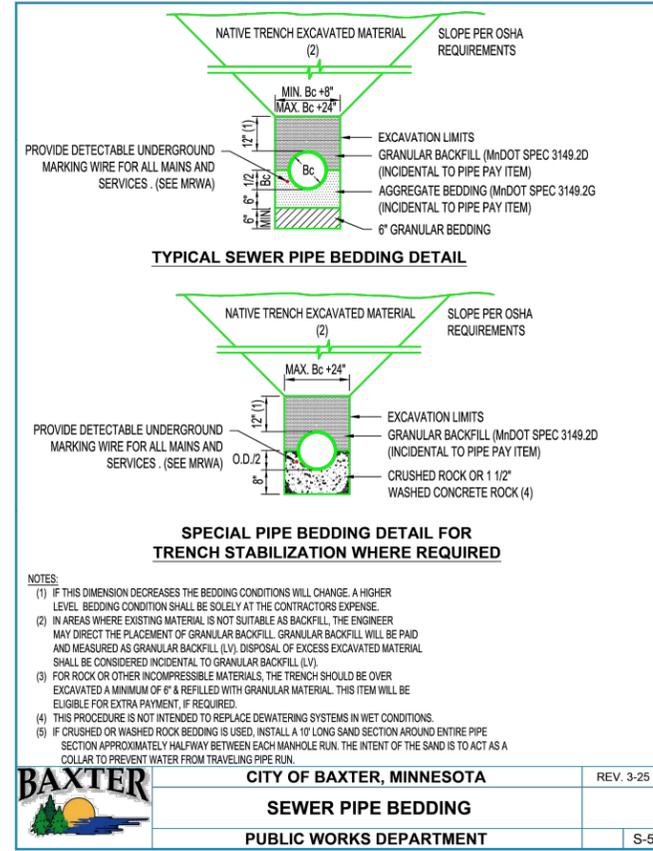
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	CHECKED BY: ALW						



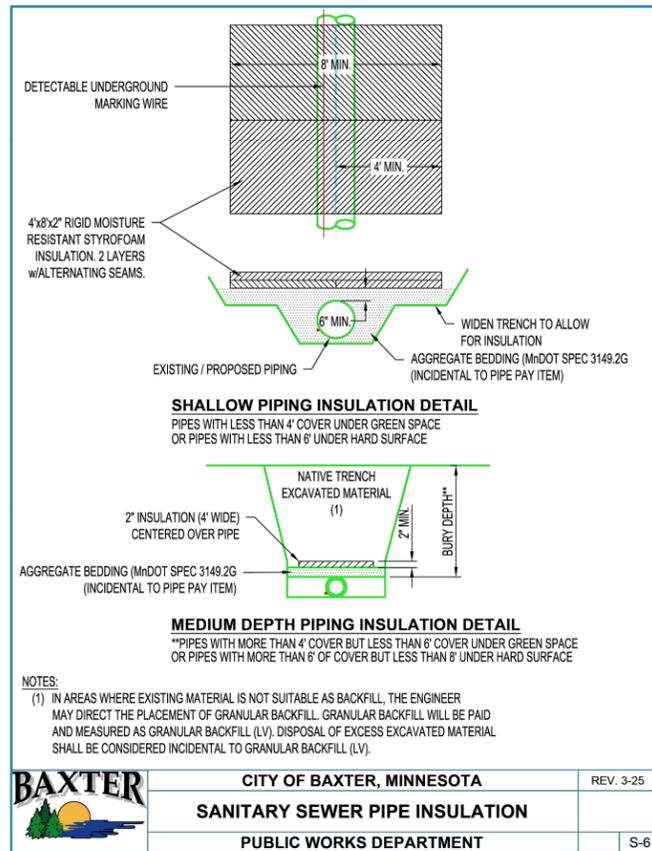
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	STANDARD SANITARY MANHOLE	
	PUBLIC WORKS DEPARTMENT	S-1



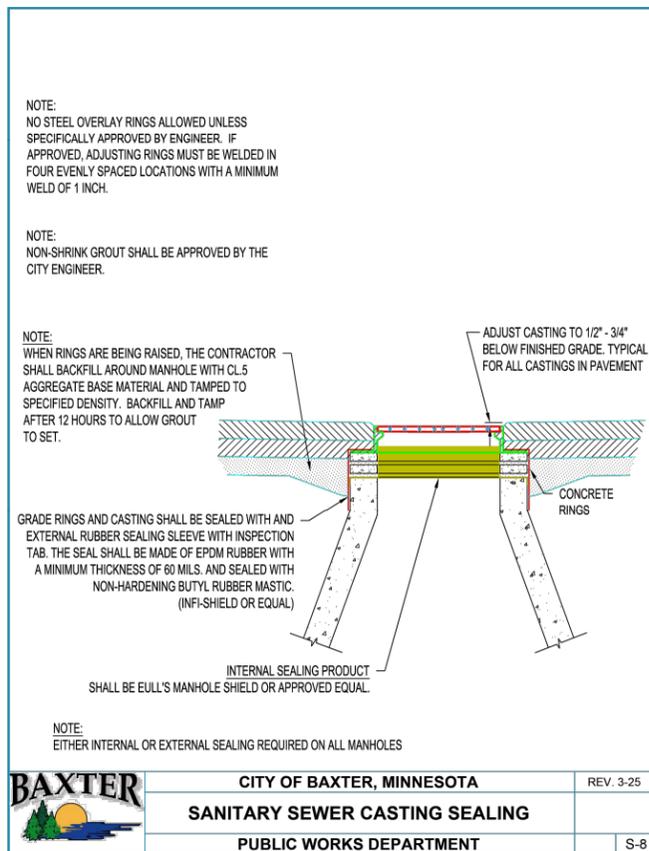
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	SANITARY MANHOLE FLEXIBLE SLEEVE	
	PUBLIC WORKS DEPARTMENT	S-2



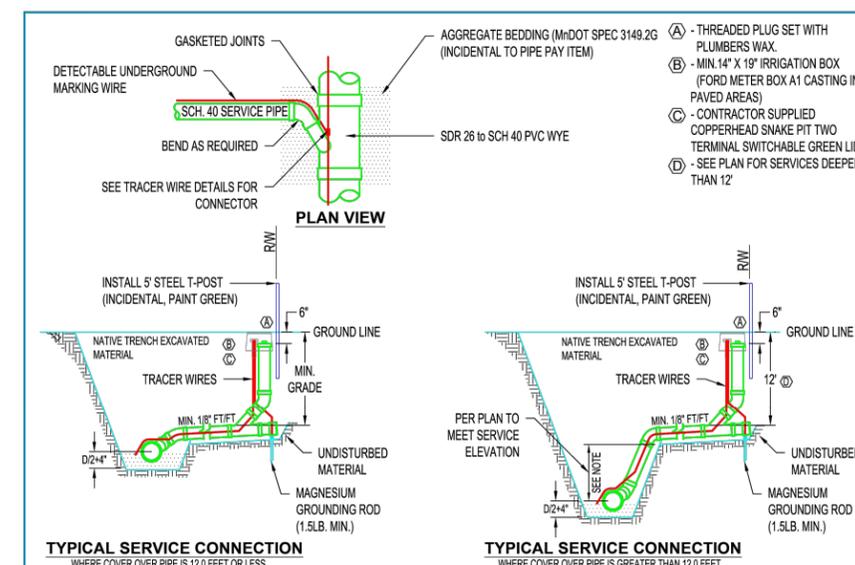
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	SEWER PIPE BEDDING	
	PUBLIC WORKS DEPARTMENT	S-5



	CITY OF BAXTER, MINNESOTA	REV. 3-25
	SANITARY SEWER PIPE INSULATION	
	PUBLIC WORKS DEPARTMENT	S-6

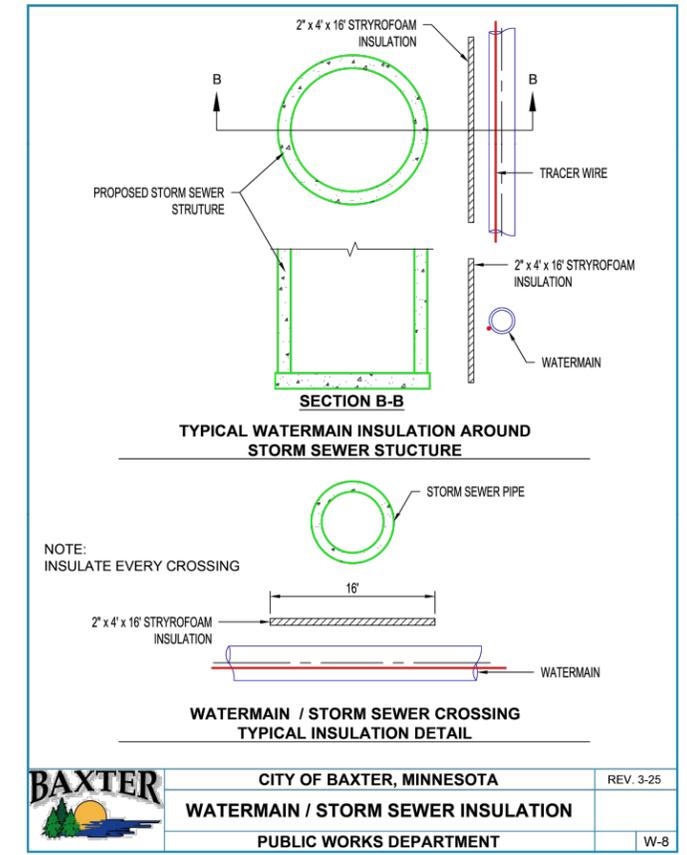
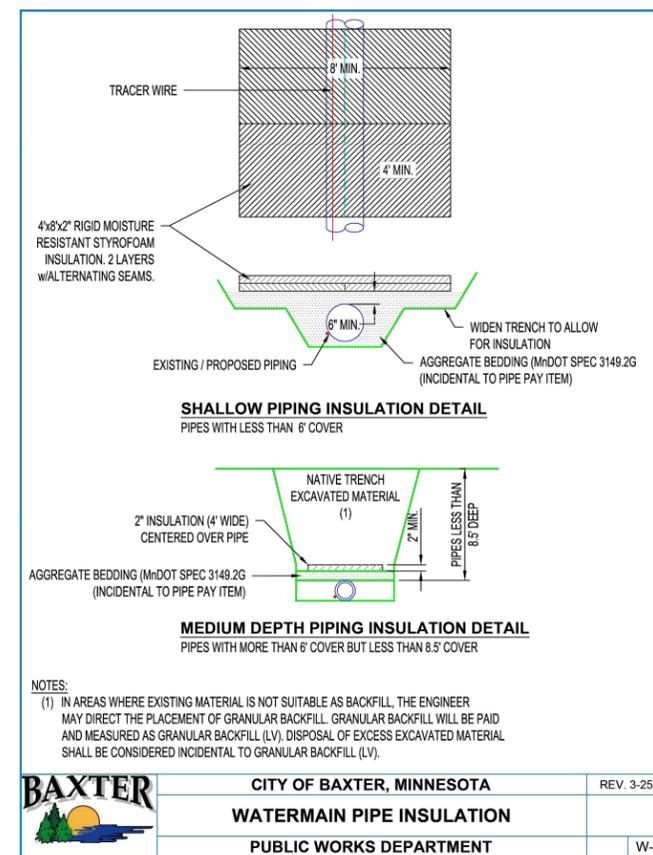
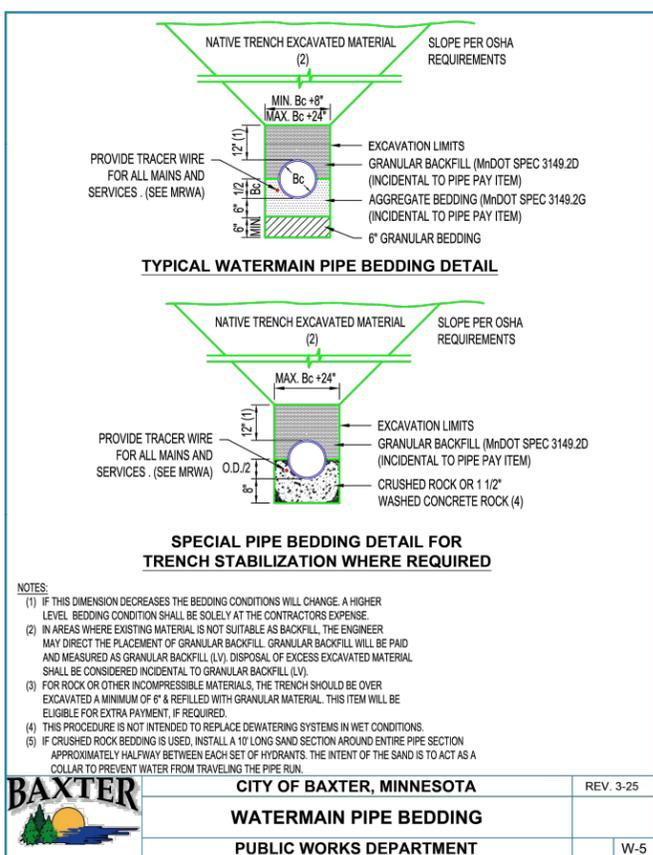
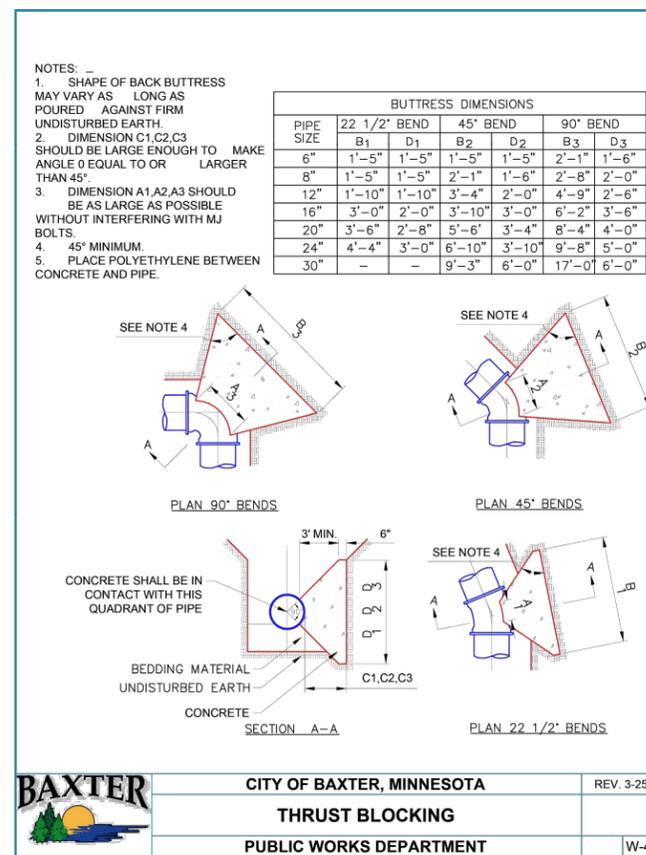
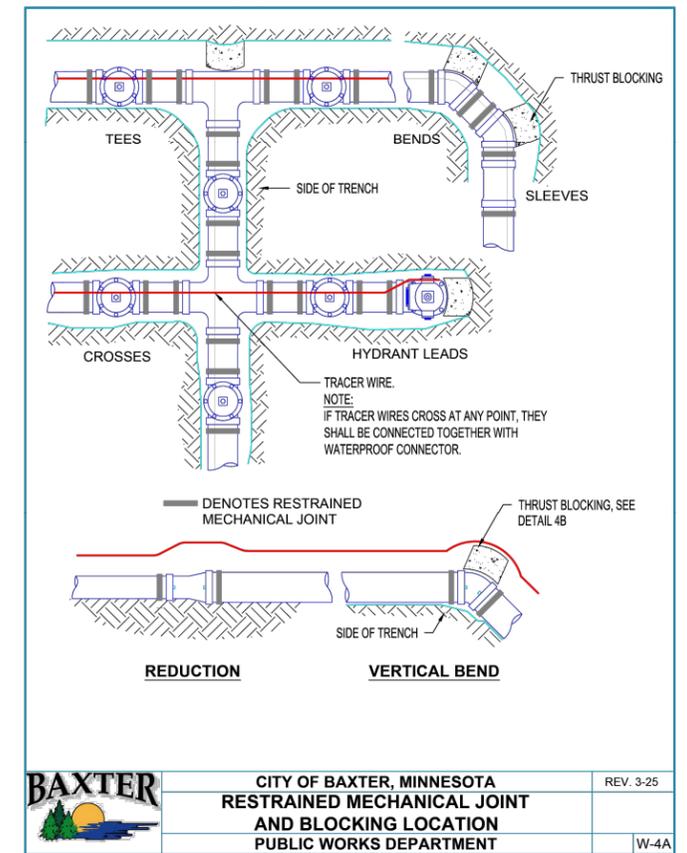
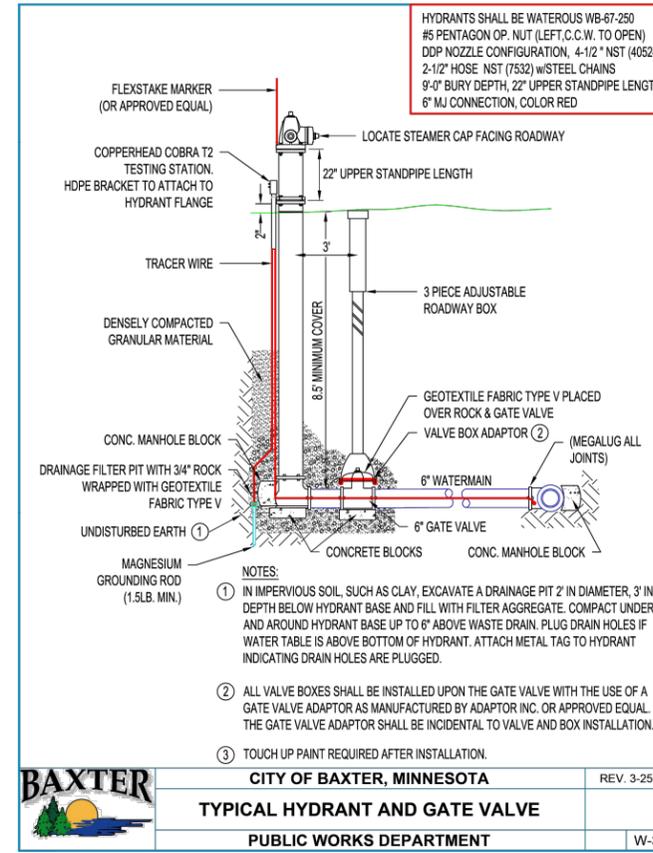
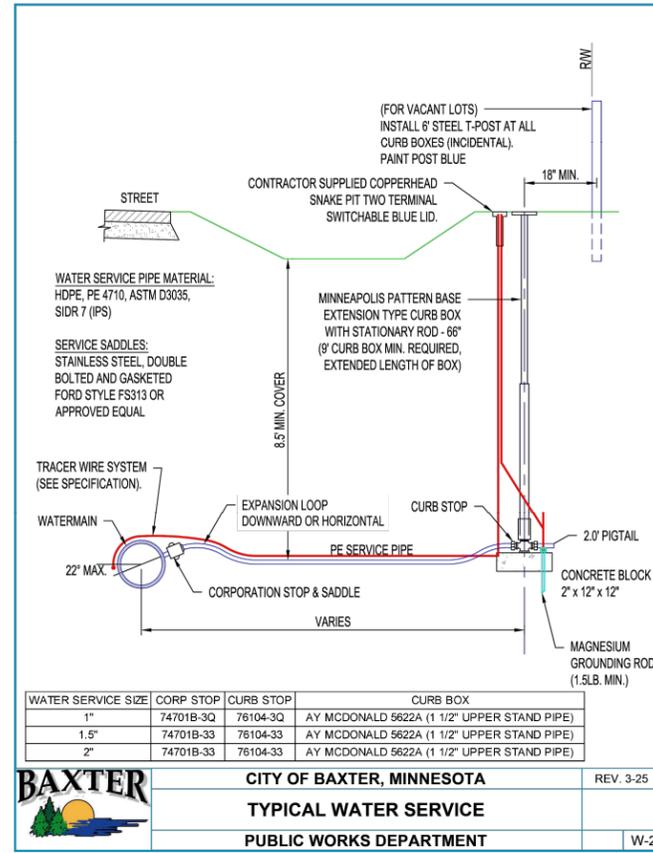
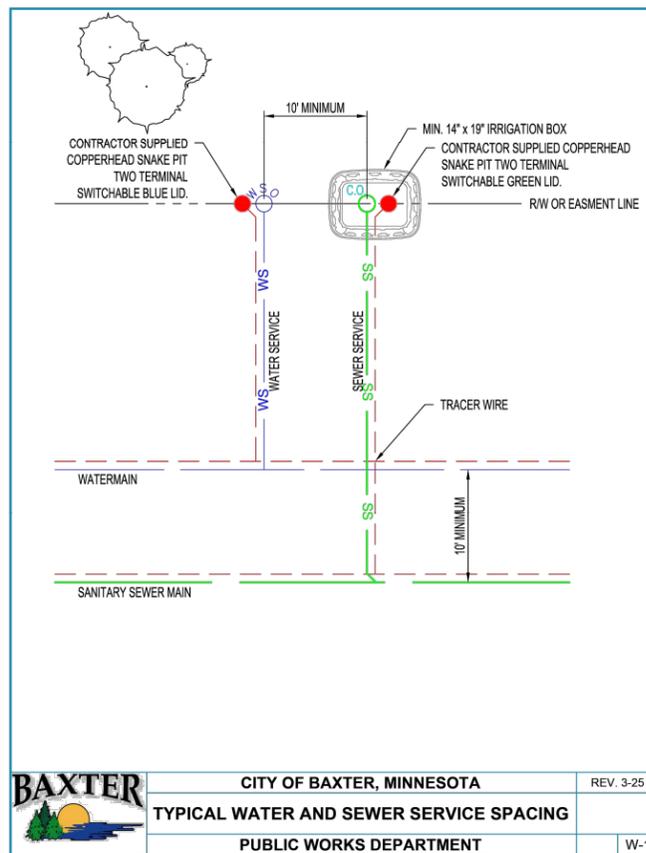


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	SANITARY SEWER CASTING SEALING	
	PUBLIC WORKS DEPARTMENT	S-8



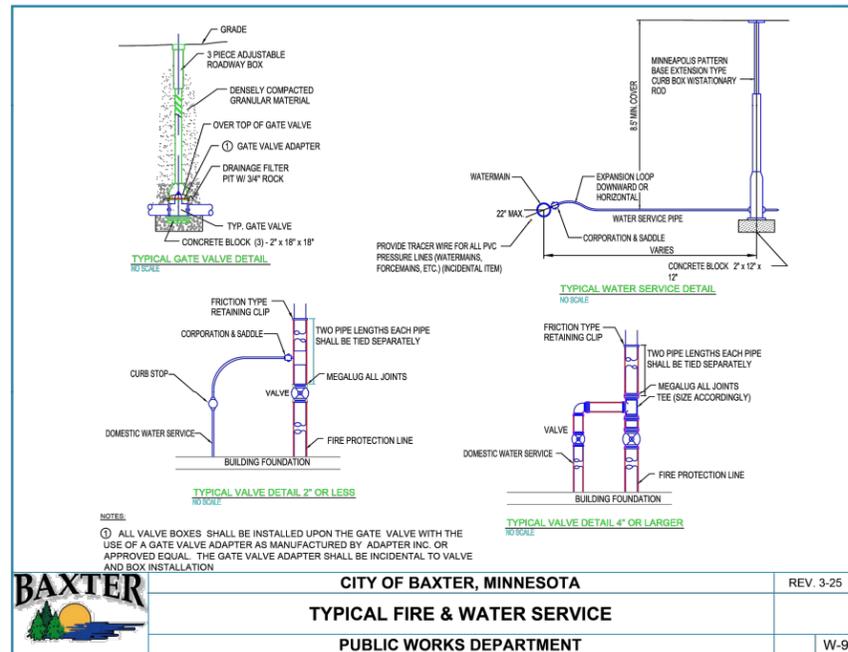
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	SANITARY SEWER SERVICE CONNECTION	
	PUBLIC WORKS DEPARTMENT	S-7

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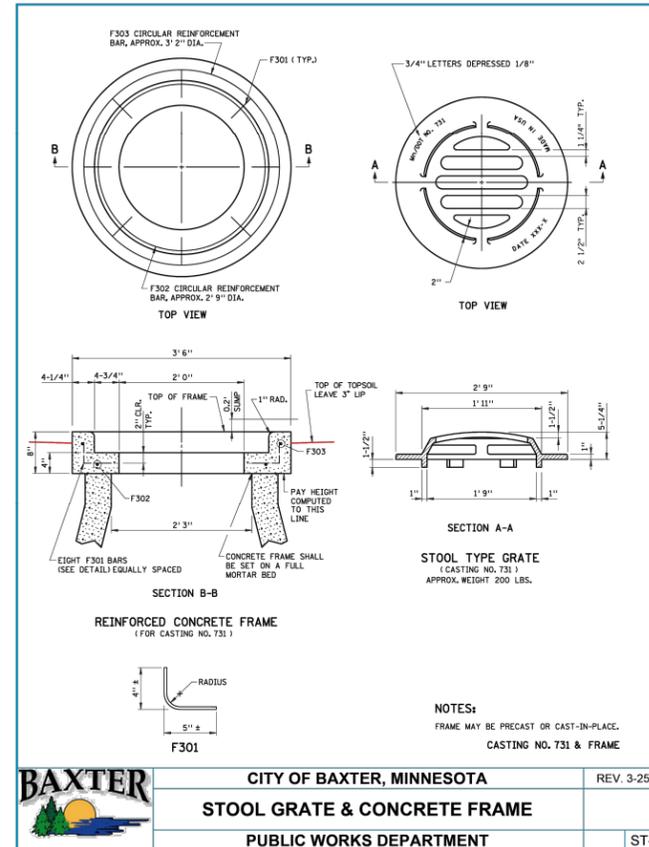


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	CITY OF BAXTER, MINNESOTA	REV. 3-25
	TYPICAL FIRE & WATER SERVICE	
	PUBLIC WORKS DEPARTMENT	W-9



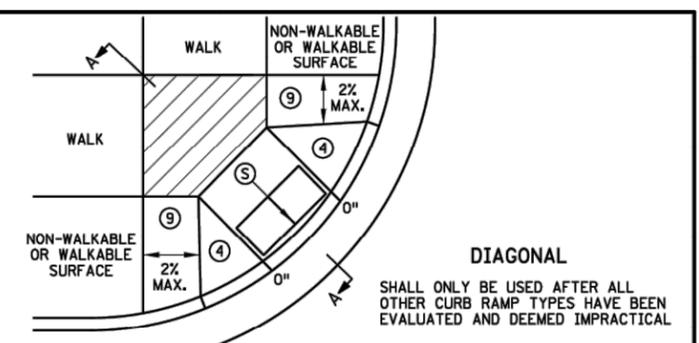
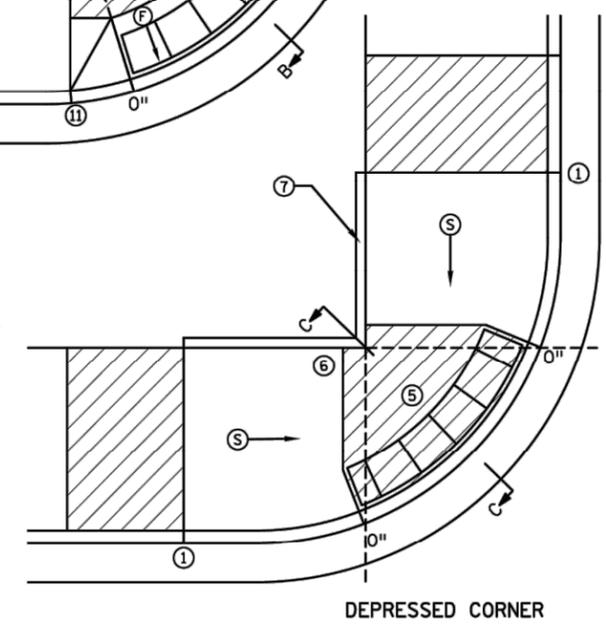
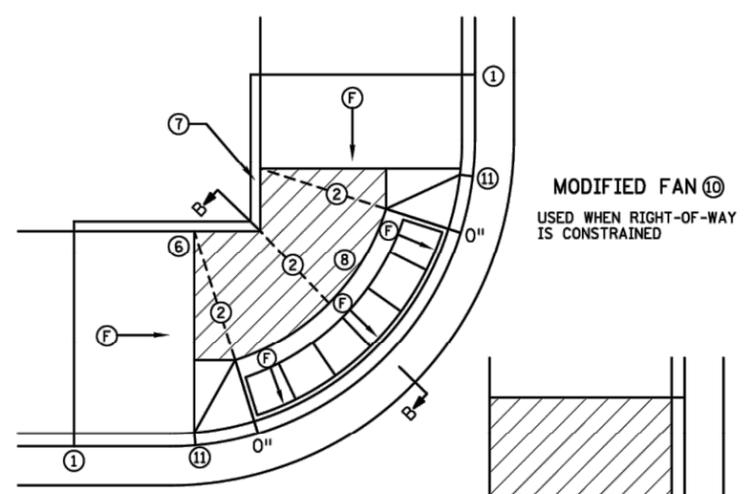
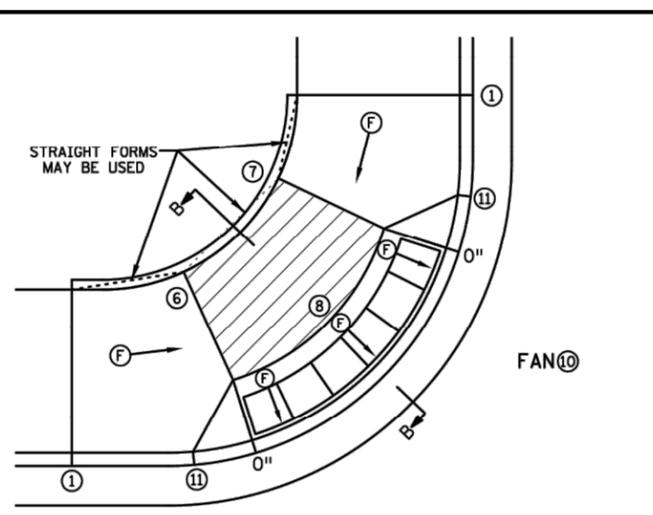
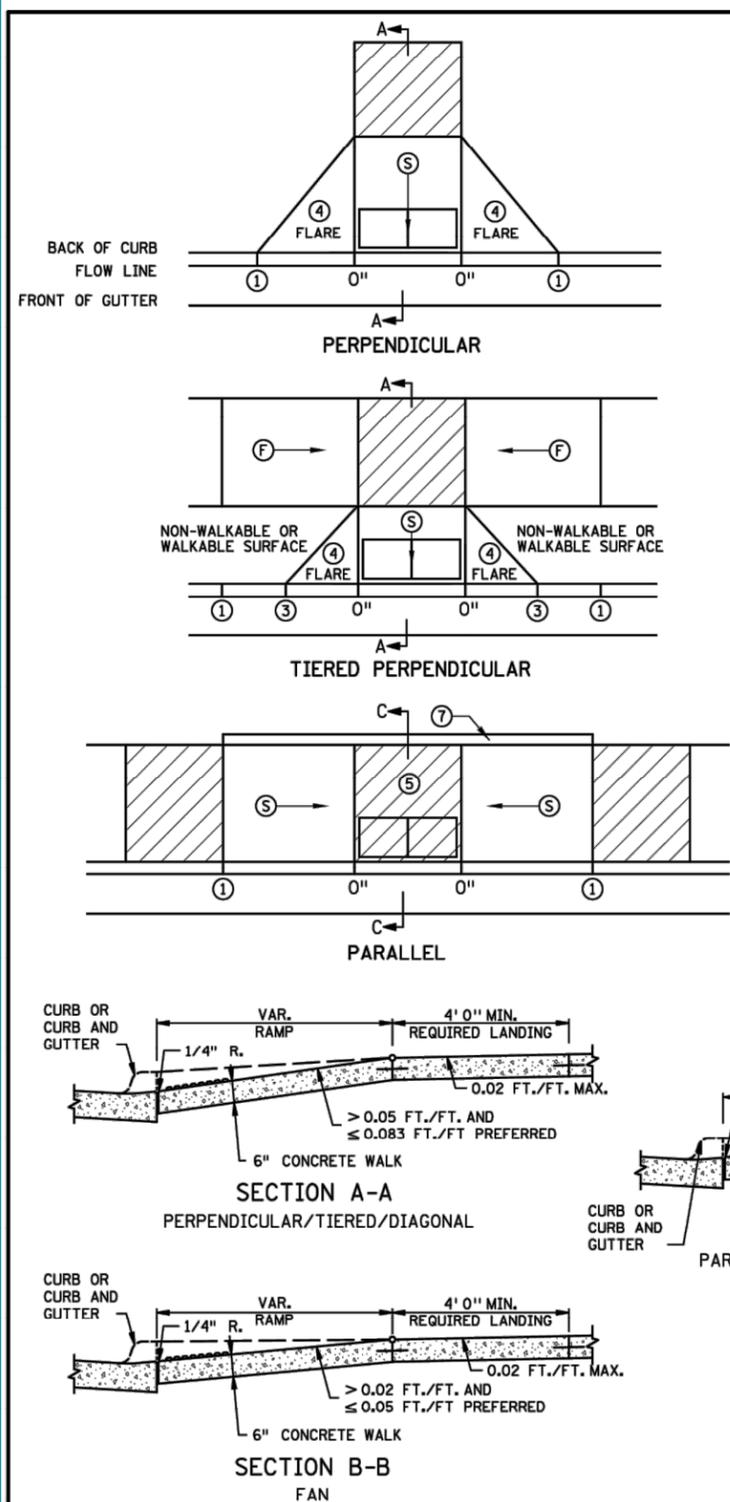
	CITY OF BAXTER, MINNESOTA	REV. 3-25
	STOOL GRATE & CONCRETE FRAME	
	PUBLIC WORKS DEPARTMENT	ST-3

THE FOLLOWING STANDARD PLATES (MNDOT) SHALL APPLY FOR CITY OF BAXTER PROJECTS		
PLATE	DESCRIPTION	NOTES:
3000M	REINFORCED CONCRETE PIPE (6 SHEETS)	
3006H	GASKET JOINT FOR R.C. PIPE (2 SHEETS)	
3007F	SHEAR REINFORCEMENT FOR PRECAST DRAINAGE STRUCTURES	
3022C	PRECAST CONCRETE SAFETY APRON (2 SHEETS)	
3100G	CONCRETE APRON FOR REINFORCED CONCRETE PIPE	
3145G	CONCRETE PIPE OR PRECAST BOX CULVERT TIES	
4006L	MANHOLE OR CATCH BASIN PRECAST DESIGNS G AND H	
4007C	PRECAST MECHANICAL JOINT SEWER MANHOLE	
4010I	CONCRETE ADJUSTING RINGS	
4011E	PRECAST CONCRETE BASE	
4020I	MANHOLE OR CATCH BASIN (FOR USE WITH OR WITHOUT TRAFFIC LOADS) (2 SHEETS)	
4108F	ADJUSTING RINGS FOR CATCH BASINS AND MANHOLES	WELD IF USED
4101D	RING CASTING FOR MANHOLE OR CATCH BASIN	CASTING 700-7
4180I	MANHOLE OR CATCH BASIN STEP	
7038A	DETECTABLE WARNING SURFACE TRUNCATED DOMES	EPOXY COATED RED
7100H	CONCRETE CURB AND GUTTER	B624
7111J	INSTALLATION OF CATCH BASIN CASTINGS	

	CITY OF BAXTER, MINNESOTA	REV. 3-25
	LIST OF MNDOT STANDARD PLATES	
	PUBLIC WORKS DEPARTMENT	ST-4

REV. 3-25
RD-1

CITY OF BAXTER, MINNESOTA
PEDESTRIAN CURB RAMP DETAILS
PUBLIC WORKS DEPARTMENT



NOTES:

LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE (PAR) CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE GREATER THAN 2%.

INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE, ONLY APPLICABLE WHEN THE INITIAL RAMP RUNNING SLOPE IS OVER 5.0%.

SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30' OF VERTICAL RISE WHEN THE LONGITUDINAL RUNNING SLOPE IS GREATER THAN 5.0%.

CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS WITHIN THE PAR. 1/4" DEEP VISUAL JOINTS SHALL BE USED AT THE TOPS OF CONCRETE FLARES ADJACENT TO WALKABLE SURFACES.

ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL, THUS BOTH SIDES OF A SLOPED WALKING SURFACE MUST BE EQUAL LENGTH, EXCEPT AS STATED IN (6) BELOW.

TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, ALL INITIAL LANDINGS AT A TOP OF A RAMPED SURFACE (RUNNING SLOPE GREATER THAN 2%) SHALL BE FORMED AND PLACED SEPARATELY IN AN INDEPENDENT CONCRETE POUR. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 6 OF 6 FOR ALL SEPARATELY POURED INITIAL LANDINGS.

WHEN SIDEWALK IS AT BACK OF CURB, TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE. MAINTAIN POSITIVE BOULEVARD DRAINAGE TO TOP OF CURB.

ALL RAMP TYPES SHOULD HAVE A MINIMUM 3' LONG RAMP LENGTH.

4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MIN. OF 24" IN THE PATH OF TRAVEL. DETECTABLE WARNING TO COVER THE ENTIRE PAR WIDTH OF SHARED-USE PATHS AND THE ENTIRE PAR WIDTH OF THE WALK WITH THE EXCEPTION OF 3" MAXIMUM ON EACH OUTSIDE EDGE WHICH ENSURES THE DETECTABLE WARNINGS ARE ENCASED IN CONCRETE WHEN ADJACENT TO TURF. WHEN ADJACENT TO CONCRETE FLARES 0" - 3" OFFSET IS ALLOWED.

WHEN DESIGNING OR ORDERING RECTANGULAR DETECTABLE WARNING SURFACES SHOULD BE 6" LESS THAN THE INCOMING PAR. ARC LENGTH OF THE RADIAL DETECTABLE WARNINGS SHOULD NOT BE GREATER THAN 20 FEET.

RECTANGULAR DETECTABLE WARNINGS SHALL BE SETBACK 3" FROM THE BACK OF CURB. RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB.

① MATCH FULL HEIGHT CURB.
② 4' MINIMUM DEPTH LANDING REQUIRED ACROSS TOP OF RAMP.
③ 3" HIGH CURB WHEN USING A 3' LONG RAMP, 4" HIGH CURB WHEN USING A 4' LONG RAMP.
④ SEE SHEET 4 OF 6, TYPICAL SIDE TREATMENT OPTIONS, FOR DETAILS ON FLARES AND RETURNED CURBS.
⑤ DETECTABLE WARNINGS MAY BE PART OF THE 4' X 4' MIN. LANDING AREA IF IT IS NOT FEASIBLE TO CONSTRUCT THE LANDING OUTSIDE OF THE DETECTABLE WARNING AREA.
⑥ THE GRADE BREAK SHALL BE PERPENDICULAR TO THE BACK OF WALK, THIS WILL ENSURE THAT THE GRADE BREAK IS PERPENDICULAR TO THE DIRECTION OF TRAVEL. (TYPICAL FOR ALL)
⑦ WHEN ADJACENT TO GRASS, GRADING SHALL ALWAYS BE USED WHEN FEASIBLE. V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS. WHEN ADJACENT TO PARKING LOTS, CONCRETE OR BITUMINOUS TAPERS LESS THAN 5% RUNNING SLOPE SHOULD BE USED OVER V CURB TO REDUCE TRIPPING HAZARDS AND FACILITATE SNOW & ICE REMOVAL.
⑧ A 7' MIN TOP RADIUS GRADE BREAK IS REQUIRED TO BE CONSTRUCTIBLE.
⑨ PAVE FULL WALK WIDTH.
⑩ "S" SLOPES ON FANS SHALL ONLY BE USED WHEN ALL OTHER FEASIBLE OPTIONS HAVE BEEN EVALUATED AND DEEMED IMPRACTICAL.
⑪ INTERMEDIATE CURB HEIGHTS TAPER SHALL RISE AT 8-10% TO A MINIMUM 3" CURB HEIGHT. REDUCE INTERMEDIATE CURB HEIGHT TO 2+ INCHES IF NECESSARY TO MATCH ADJACENT BOULEVARD OR SIDEWALK GRADES.

LEGEND

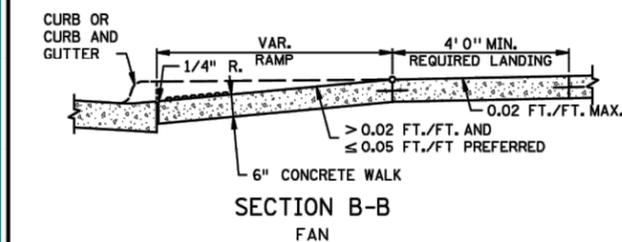
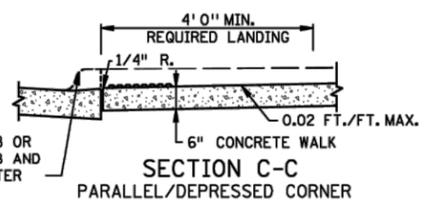
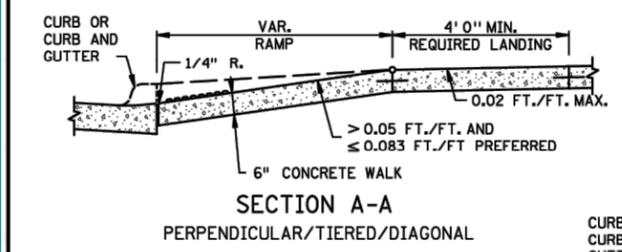
THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.

⑤ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.

⑥ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.

⑦ LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PAR.

X" CURB HEIGHT



REVISIONS:

APPROVED: 11-04-2021

Jeff J. Pal

OPERATIONS DIVISION

MINNESOTA STANDARD PLAN 5-297.250 1 OF 6

DEPARTMENT OF TRANSPORTATION

APPROVED: 11-04-2021

REVISOR:

STATE PROJ. NO. (TH) SHEET NO. OF SHEETS

PEDESTRIAN CURB RAMP DETAILS

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DRAWN BY: MJM

CHECKED BY: ALW

THOMAS M. RIENTS LIC. NO.: 63111 DATE: 02/09/2026

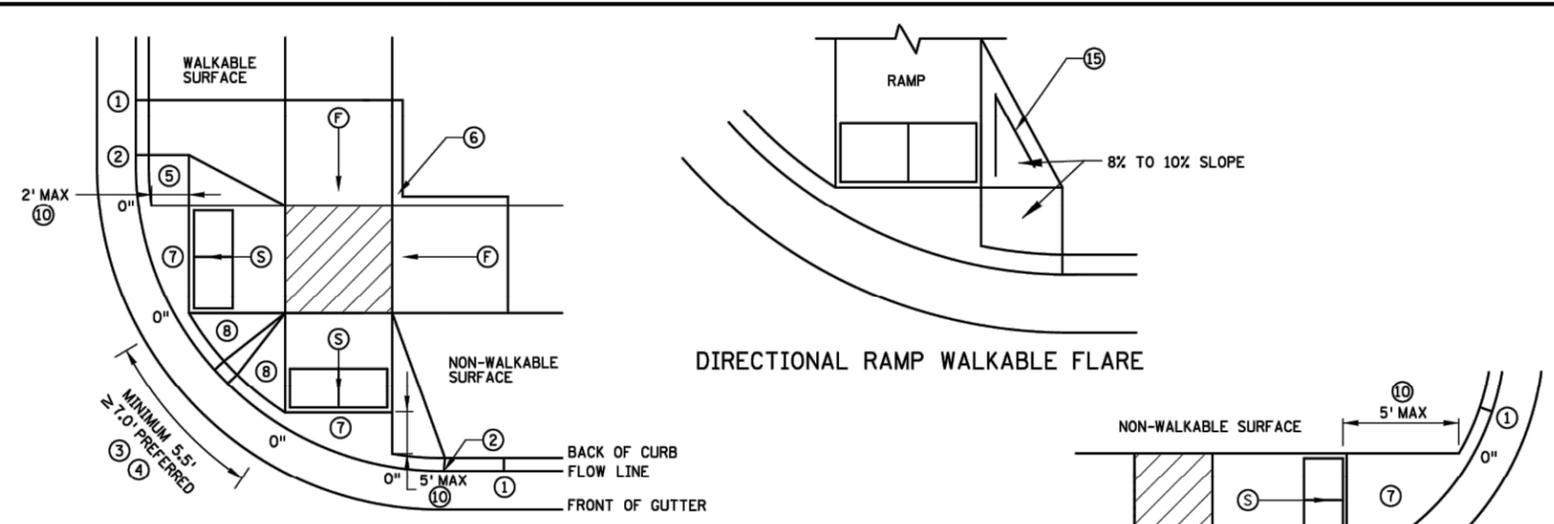
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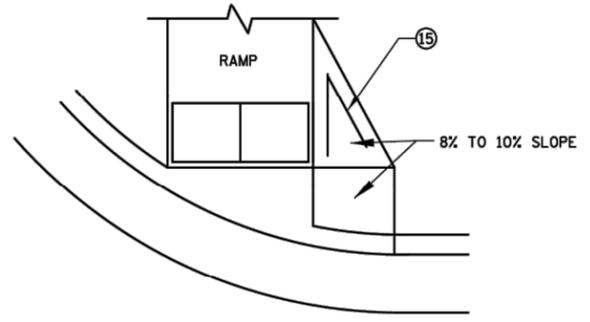
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PROJECT #: 2025-12133

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RD-2

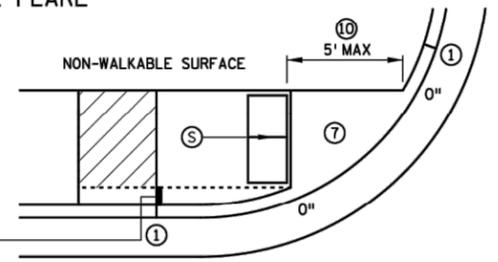
CITY OF BAXTER, MINNESOTA
PEDESTRIAN CURB RAMP DETAILS
PUBLIC WORKS DEPARTMENT



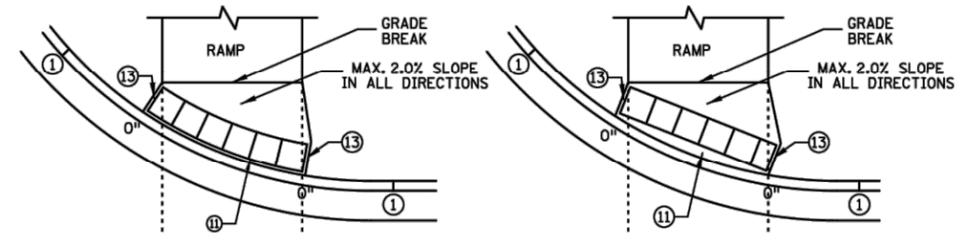
COMBINED DIRECTIONAL



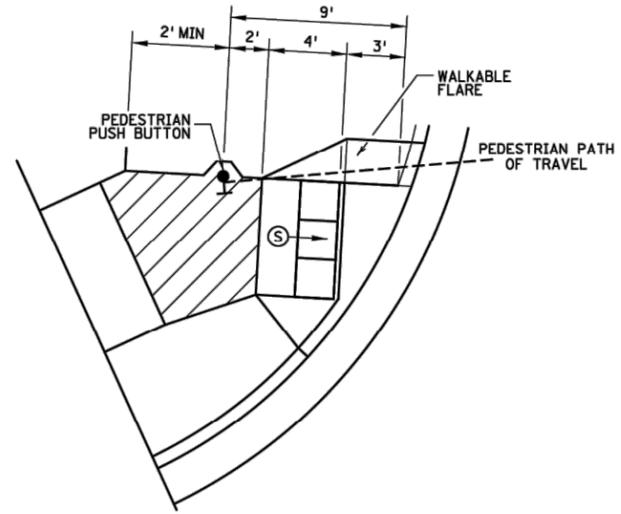
DIRECTIONAL RAMP WALKABLE FLARE



STANDARD ONE-WAY DIRECTIONAL ⑩

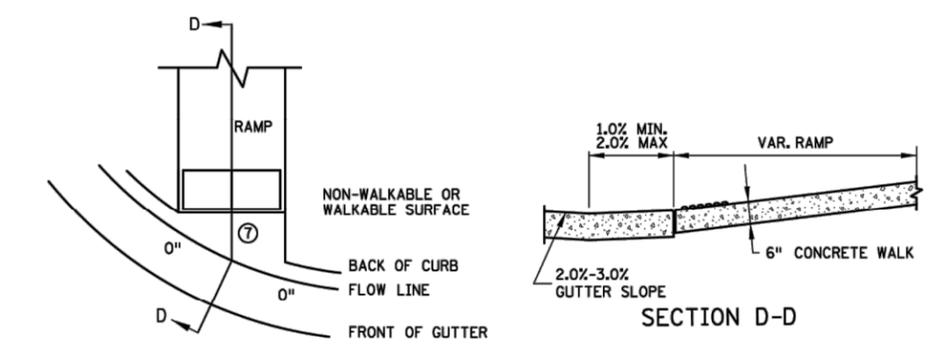


ONE-WAY DIRECTIONAL WITH DETECTABLE WARNING AT BACK OF CURB



SEMI-DIRECTIONAL RAMP ③④⑨

3' DOME SETBACK, 4' LONG RAMP AND PUSH BUTTON 9' FROM THE BACK OF CURB
PRIMARYLY USED FOR APS APPLICATIONS WHERE THE PAR DOES NOT CONTINUE PAST THE PUSH BUTTON (DEAD-END SIDEWALK)



CURB FOR DIRECTIONAL RAMPS ⑦

- NOTES:**
- LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE (PAR) CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE.
 - INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE, ONLY APPLICABLE WHEN THE INITIAL RAMP RUNNING SLOPE IS OVER 5.0%.
 - SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30" OF VERTICAL RISE WHEN THE LONGITUDINAL SLOPE IS GREATER THAN 5.0%.
 - CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS WITHIN THE PAR. 1/4" DEEP VISUAL JOINTS SHALL BE USED AT THE TOP GRADE BREAK OF CONCRETE FLARES ADJACENT TO WALKABLE SURFACES.
 - ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL. THUS BOTH SIDES OF A SLOPED WALKING SURFACE MUST BE EQUAL LENGTH.
 - TO ENSURE INITIAL RAMPS AND INITIAL LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS SHALL BE CAST SEPARATELY, FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 6 AND THE ADA SPECIAL PROVISION (PROSECUTION OF WORK).
 - TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE.
 - WHEN THE BOULEVARD IS 4' WIDE OR LESS, THE TOP OF CURB TAPER SHALL MATCH THE RAMP SLOPES TO REDUCE NEGATIVE BOULEVARD SLOPES FROM THE TOP BACK OF CURB TO THE PAR.
 - ALL RAMP TYPES SHOULD HAVE A MINIMUM 3' LONG RAMP LENGTH.
 - 4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MIN. OF 24" IN THE PATH OF TRAVEL. DETECTABLE WARNING TO COVER THE ENTIRE PAR WIDTH OF SHARED-USE PATHS AND THE ENTIRE PAR WIDTH OF THE WALK WITH THE EXCEPTION OF 3" MAXIMUM ON EACH OUTSIDE EDGE WHICH ENSURES THE DETECTABLE WARNINGS ARE ENCASED IN CONCRETE WHEN ADJACENT TO TURF. WHEN ADJACENT TO CONCRETE FLARES 0" - 3" OFFSET IS ALLOWED.
 - WHEN DESIGNING OR ORDERING RECTANGULAR DETECTABLE WARNING SURFACES SHOULD BE 6" LESS THAN THE INCOMING PAR. ARC LENGTH OF THE RADIAL DETECTABLE WARNINGS SHOULD NOT BE GREATER THAN 20 FEET.
 - RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB. SEE NOTES ⑩ & ⑪ FOR INFORMATION REGARDING RECTANGULAR DETECTABLE WARNING PLACEMENT.
- MATCH FULL CURB HEIGHT.
 - 3" HIGH CURB WHEN USING A 3' LONG RAMP
4" HIGH CURB WHEN USING A 4' LONG RAMP.
 - 3" MINIMUM CURB HEIGHT (5.5' MIN. DISTANCE REQUIRED BETWEEN DOMES)
4" PREFERRED (7' MIN. DISTANCE REQUIRED BETWEEN DOMES).
 - THE "BUMP" IN BETWEEN THE RAMPS SHOULD NOT BE IN THE PATH OF TRAVEL FOR COMBINED DIRECTIONAL RAMPS. IF THIS OCCURS MODIFY THE RAMP LOCATION OR SWITCH RAMP TO A FAN/DEPRESSED CORNER.
 - WHEN USING CONCRETE PAVED FLARES ON THE OUTSIDE OF DIRECTIONAL RAMPS, AND ADJACENT TO A WALKABLE SURFACE, DIRECTIONAL RAMP FLARES SHALL BE USED. SEE THE DETAIL ON THIS SHEET.
 - GRADING SHALL ALWAYS BE USED WHEN FEASIBLE. V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS. WHEN ADJACENT TO PARKING LOTS, CONCRETE OR BITUMINOUS TAPERS SHOULD BE USED OVER V CURB TO REDUCE TRIPPING HAZARDS AND FACILITATE SNOW & ICE REMOVAL.
 - MAX. 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK AND DRAIN TO FLOW LINE. SHALL BE CONSTRUCTED INTEGRAL WITH CURB AND GUTTER.
 - 8% TO 10% WALKABLE FLARE.
 - PLACE DOMES AT THE BACK OF CURB WHEN ALLOWABLE SETBACK CRITERIA IS EXCEEDED.
 - FRONT EDGE OF DETECTABLE WARNING SHALL BE SET BACK 2' MAXIMUM WHEN ADJACENT TO WALKABLE SURFACE, AND 5' MAXIMUM WHEN ADJACENT TO NON-WALKABLE SURFACE WITH ONE CORNER SET 3" FROM BACK OF CURB. A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP WITHOUT RAISED OBSTACLES THAT COULD MISTAKENLY BE TRAVERSED BY A USER WHO IS VISUALLY IMPAIRED.
 - RECTANGULAR DETECTABLE WARNINGS MAY BE SETBACK UP TO 9" FROM THE BACK OF CURB WITH CORNERS SET 3" FROM BACK OF CURB. IF 9" SETBACK IS EXCEEDED USE RADIAL DETECTABLE WARNINGS.
 - FOR DIRECTIONAL RAMPS WITH THE DETECTABLE WARNINGS PLACED AT THE BACK OF CURB, THE DETECTABLE WARNINGS SHALL COVER THE ENTIRE WIDTH OF THE WALK/PATH, THIS ENSURES A DETECTABLE EDGE AND HELPS ELIMINATE THE CURB TAPER OBSTRUCTING THE PATH OF PEDESTRIAN TRAVEL.
 - THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE BACK OF CURB. MAINTAIN 3" BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
 - TO BE USED FOR ALL DIRECTIONAL RAMPS, EXCEPT WHERE DOMES ARE PLACED ALONG THE BACK OF CURB.
 - PLACE 2 NO. 4 BARS 4 INCHES FROM SIDE OF FORMS WITH A MINIMUM 2 INCHES OF CONCRETE COVER ALONG EACH SIDE OF FLARE (INCIDENTAL).

LEGEND

THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.

- ⑤ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.
- ⑥ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.
- ⑦ LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PAR.
- X" CURB HEIGHT

REVISION:
APPROVED: 11-04-2021
Jeff J. Pel...
JEFF PERKINS
OPERATIONS DIVISION



STANDARD PLAN 5-297.250 2 OF 6
APPROVED: 11-04-2021
REVISOR:
THOMAS M. RIENTS
STATE DESIGN ENGINEER

PEDESTRIAN CURB RAMP DETAILS

STATE PROJ. NO. (T.H.) SHEET NO. OF SHEETS



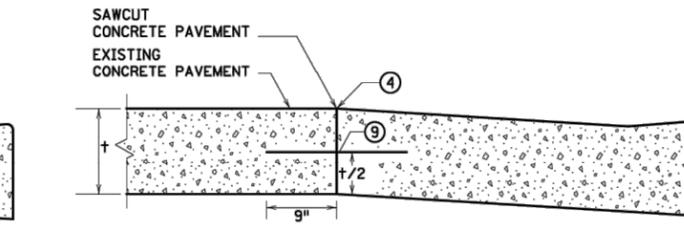
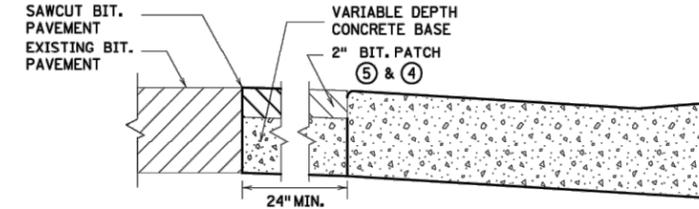
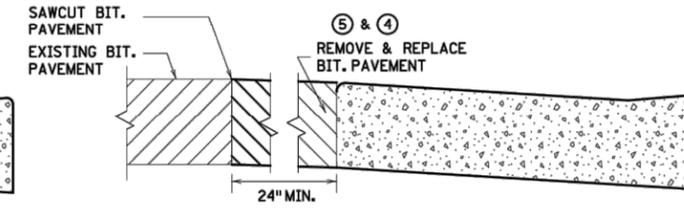
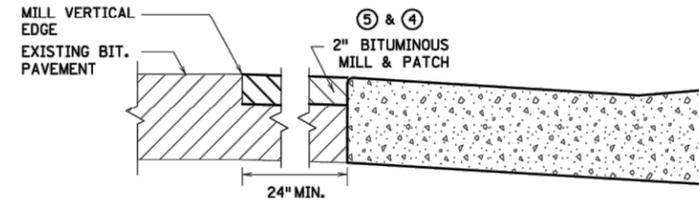
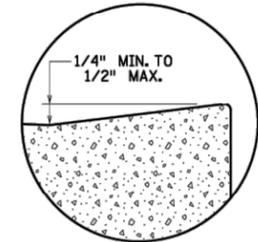
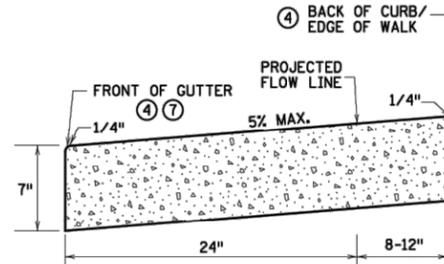
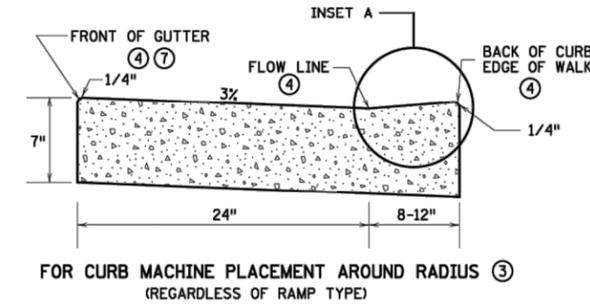
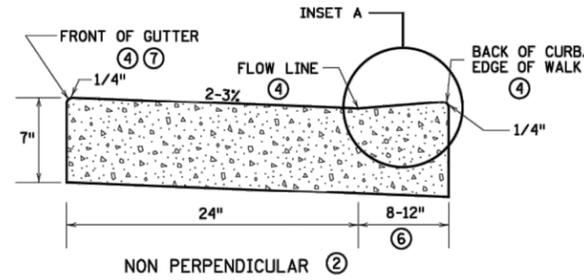
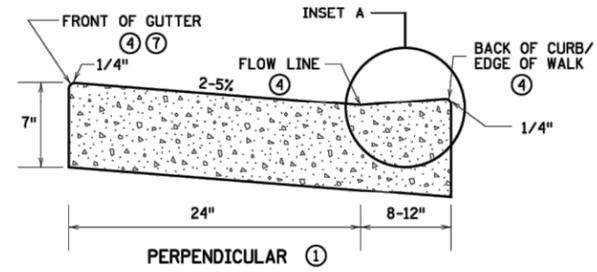
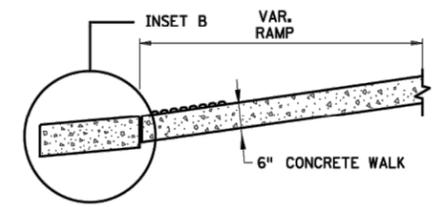
DRAWN BY: MJM
CHECKED BY: ALW
THOMAS M. RIENTS
LIC. NO.: 63111 DATE: 02/09/2026

DATE	REV#	REVISIONS DESCRIPTION

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CIVIL DETAILS

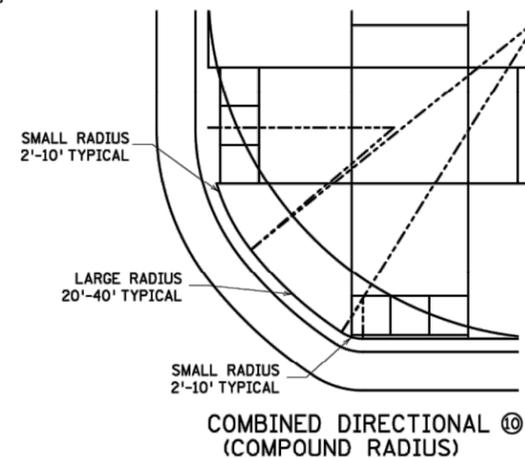
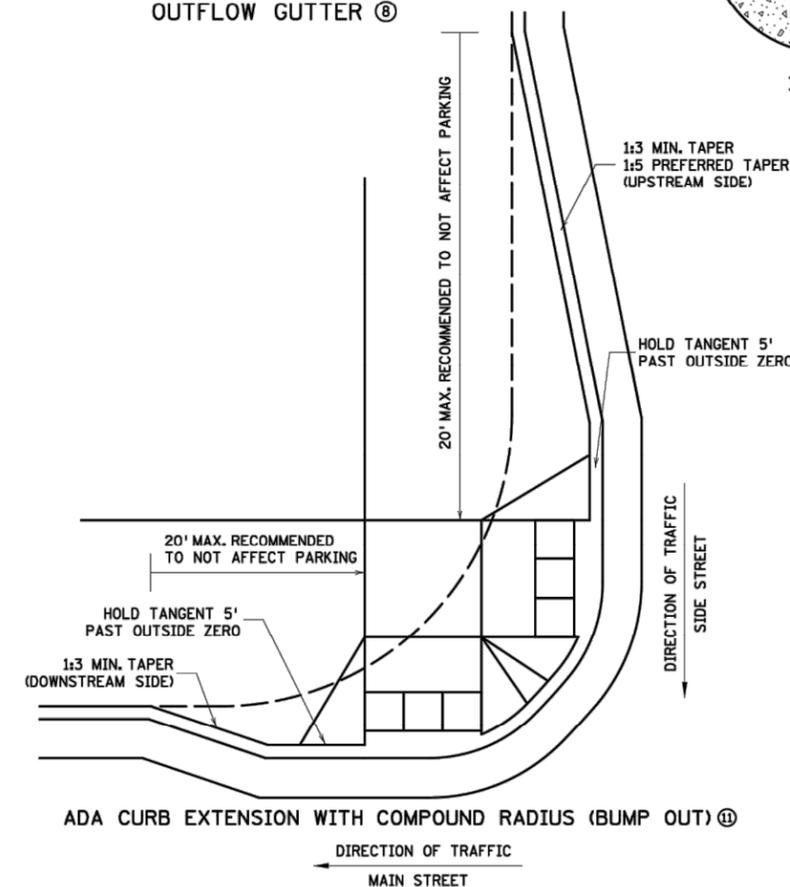
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ONLY ALLOWED PER ENGINEER'S APPROVAL

PAVEMENT TREATMENT OPTIONS IN FRONT OF CURB & GUTTER FOR USE ON CURB RAMP RETROFITS



NOTES:

- POSITIVE FLOW LINE DRAINAGE SHALL BE MAINTAINED THROUGH THE PEDESTRIAN ACCESS ROUTE (PAR) AT A 2% MAXIMUM. NO PONDING SHALL BE PRESENT IN THE PAR.
- ANY VERTICAL LIP THAT OCCURS AT THE FLOW LINE SHALL NOT BE GREATER THAN 1/4 INCH.
- ① FOR USE AT CURB CUTS WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS ASSUMED PERPENDICULAR TO THE GUTTER FLOW LINE. RAMP TYPES INCLUDE: PERPENDICULAR, TIERED PERPENDICULAR, PARALLEL, AND DIAGONAL RAMPS.
- ② FOR USE AT CURB RAMPS WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS ASSUMED NON PERPENDICULAR TO THE GUTTER FLOW LINE. RAMP TYPES INCLUDE: FANS & DEPRESSED CORNERS.
- ③ BEGIN GUTTER SLOPE TRANSITION 10' OUTSIDE OF ALL CURB RAMPS.
- ④ THERE SHALL BE NO VERTICAL DISCONTINUITIES GREATER THAN 1/4".
- ⑤ ELEVATION CHANGE TAKES PLACE FROM THE EXISTING TO NEW FRONT OF GUTTER. PATCH IS USED TO MATCH THE NEW GUTTER FACE INTO THE EXISTING ROADWAY.
- ⑥ VARIABLE WIDTH FOR DIRECTIONAL CURB APPLICATIONS. SEE SHEET 2 FOR DIRECTIONAL CURB SLOPE REQUIREMENTS.
- ⑦ TOP FRONT OF GUTTER SHALL BE CONSTRUCTED FLUSH WITH PROPOSED ADJACENT PAVEMENT ELEVATION. TOP 1.5" OF THE GUTTER FACE MUST BE A FORMED EDGE. PAR GUTTER SHALL NOT BE OVERLAID.
- ⑧ SHOULD BE USED AT VERTICALLY CONSTRAINED AREAS WHEN AT A DRAINAGE HIGH POINT OR SUPER ELEVATED ROADWAY SEGMENTS.
- ⑨ DRILL AND GROUT NO. 4 EPOXY-COATED 18" LONG TIE BARS AT 30" CENTER TO CENTER INTO EXISTING CONCRETE PAVEMENT 1' MINIMUM FROM ALL JOINTS.
- ⑩ HELPS PROVIDE TWO SEPARATE RAMPS, REDUCES THE DOME SETBACK LENGTH AND MINIMIZES DIRECTIONAL CURB. THIS RADIUS DESIGN CLOSELY FOLLOWS THE TURNING VEHICLE PATH WHILE OPTIMIZING CURB RAMP LENGTH.
- ⑪ CURB EXTENSIONS SHOULD BE USED IN VERTICALLY CONSTRAINED AREAS, USUALLY IN DOWNTOWN ROADWAY SEGMENTS WHERE ON-STREET PARKING IS AVAILABLE. CURB EXTENSIONS SHOULD BE CONSIDERED FOR APS INTERSECTIONS WHERE SPACE IS LIMITED. PUSH BUTTONS MUST MEET APS CRITERIA AS DESCRIBED IN THE PUSH BUTTON LOCATION DETAIL SHEET.

REVISION:
APPROVED: 11-04-2021
<i>Jeff A. Perkins</i>
JEFFREY PERKINS OPERATIONS DIVISION



STANDARD PLAN 5-297.250	3 OF 6
<i>Tom Styrzbecki</i>	APPROVED: 11-04-2021
THOMAS STYRZBECKI STATE DESIGN ENGINEER	REVISED:

PEDESTRIAN CURB RAMP DETAILS

STATE PROJ. NO.	(TH)	SHEET NO.	OF	SHEETS
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REV. 3-25
RD-3

CITY OF BAXTER, MINNESOTA
PEDESTRIAN CURB RAMP DETAILS
PUBLIC WORKS DEPARTMENT



DRAWN BY: MJM	THOMAS M. RIENS	LIC. NO.: 63111	DATE: 02/09/2026
CHECKED BY: ALW			

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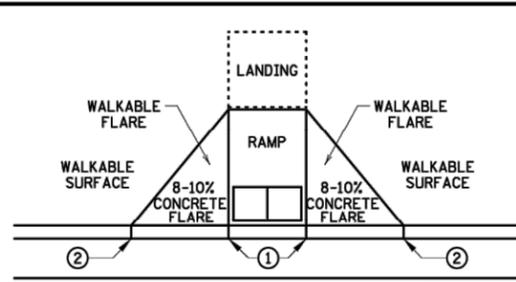
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PROJECT #: 2025-12133

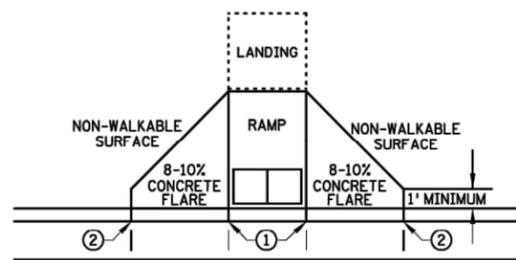
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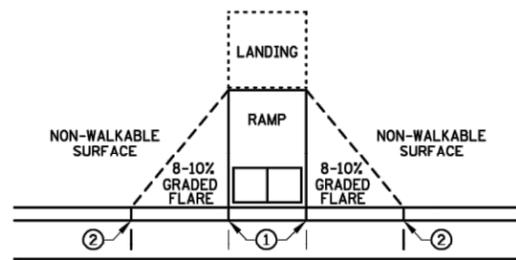
CITY OF BAXTER, MINNESOTA
PEDESTRIAN CURB RAMP DETAILS
 PUBLIC WORKS DEPARTMENT



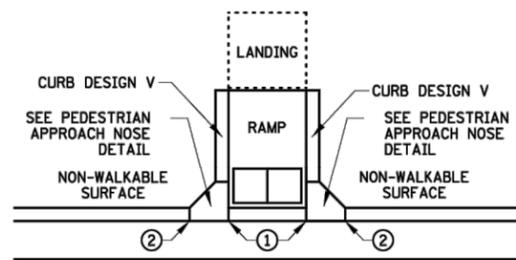
PAVED FLARES
 ADJACENT TO WALKABLE SURFACE



PAVED FLARES
 ADJACENT TO NON-WALKABLE SURFACE

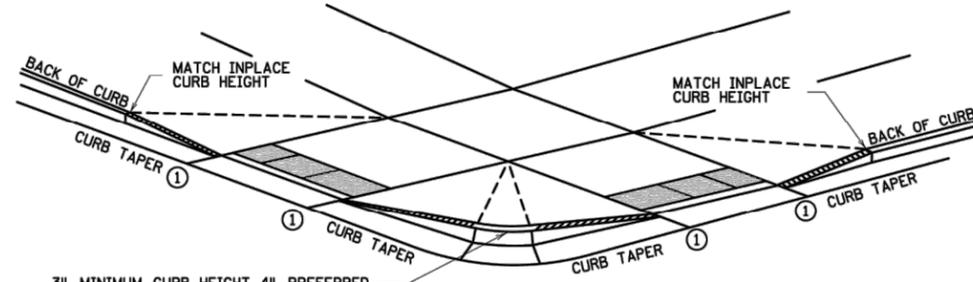


GRADED FLARES



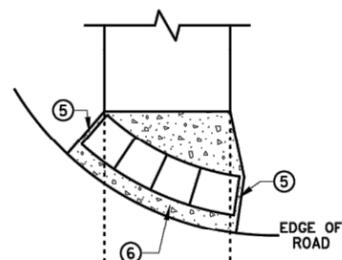
RETURNED CURB ④

TYPICAL SIDE TREATMENT OPTIONS ③ ⑩

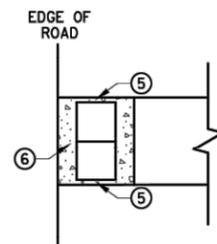


3" MINIMUM CURB HEIGHT, 4" PREFERRED
 (MEASURED AT FRONT FACE OF CURB)
 FOR A MIN. 6" LENGTH (MEASURED ALONG FLOW LINE)

DETECTABLE EDGE WITH ⑦
 CURB AND GUTTER

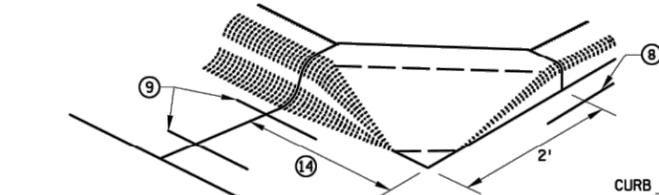


RADIAL DETECTABLE WARNING

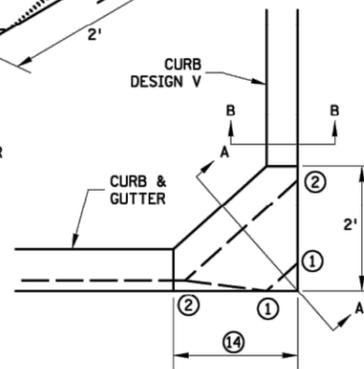


RECTANGULAR DETECTABLE WARNING

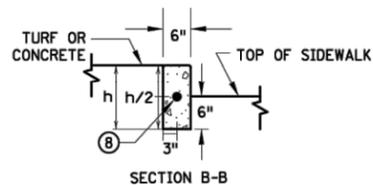
DETECTABLE EDGE WITHOUT CURB AND GUTTER



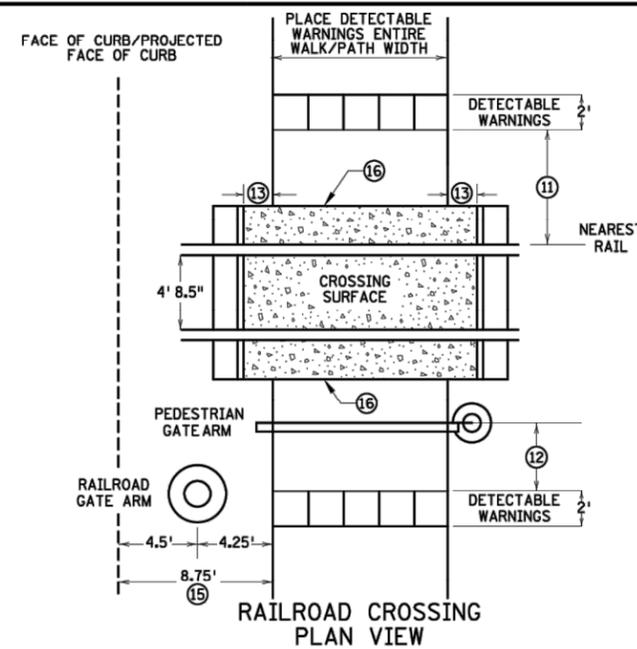
SECTION A-A



PEDESTRIAN APPROACH
 NOSE DETAIL
 (FOR RETURNED CURB
 SIDE TREATMENT)



SECTION B-B



RAILROAD CROSSING
 PLAN VIEW

NOTES:

- 1 INTERMEDIATE CURB HEIGHTS TAPER SHALL RISE AT 8-10% TO A MINIMUM 3 INCH CURB HEIGHT. INCREASE CURB TAPER LENGTH AT LESS THAN 8% OR REDUCE INTERMEDIATE CURB HEIGHT TO 2+ INCHES IF NECESSARY TO MATCH ADJACENT BOULEVARD OR SIDEWALK GRADES.
- 2 SEE STANDARD PLATE 7038 AND THIS SHEET FOR ADDITIONAL DETAILS ON DETECTABLE WARNING.
- 3 A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP WITHOUT RAISED OBSTACLES THAT COULD MISTAKENLY BE TRAVERSED BY A USER WHO IS VISUALLY IMPAIRED. CONCRETE FLARE LENGTHS ADJACENT TO NON-WALKABLE SURFACES SHOULD BE LESS THAN 8' LONG MEASURED ALONG THE RAMPS FROM THE BACK OF CURB.
- 4 0" CURB HEIGHT. SEE INSET A ON SHEET 3 OF 6.
- 5 FULL CURB HEIGHT.
- 6 SIDE TREATMENTS ARE APPLICABLE TO ALL RAMP TYPES AND SHOULD BE IMPLEMENTED AS NEEDED AS FIELD CONDITIONS DICTATE. THE ENGINEER SHALL DETERMINE THE RAMP SIDE TREATMENTS BASED ON MAINTENANCE OF BOTH ROADWAY AND SIDEWALK, ADJACENT PROPERTY CONSIDERATIONS, AND MITIGATING CONSTRUCTION IMPACTS.
- 7 TYPICALLY USED FOR MEDIANS AND ISLANDS.
- 8 WHEN NO CONCRETE FLARES ARE PROPOSED, THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE EDGE OF ROADWAY. MAINTAIN 3" MAX. BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
- 9 IF NO CURB AND GUTTER IS PLACED IN RURAL SECTIONS, DETECTABLE WARNINGS SHALL BE PLACED 1' FROM THE EDGE OF BITUMINOUS ROADWAY AND/OR BITUMINOUS SHARED-USE PATH TO PROVIDE VISUAL CONTRAST.
- 10 ALL CONSTRUCTED CURBS MUST HAVE A CONTINUOUS DETECTABLE EDGE FOR THE VISUALLY IMPAIRED. THIS DETECTABLE EDGE REQUIRES DETECTABLE WARNINGS WHEREVER THERE IS ZERO-INCH HIGH CURB. CURB TAPERS ARE CONSIDERED A DETECTABLE EDGE WHEN THE TAPER STARTS WITHIN 3" OF THE EDGE OF THE DETECTABLE WARNINGS, AND UNIFORMLY RISES TO A 3-INCH MINIMUM CURB HEIGHT. ANY CURB NOT PART OF A CURB TAPER AND LESS THAN 3 INCHES IN HEIGHT IS NOT CONSIDERED A DETECTABLE EDGE AND THEREFORE IS NOT COMPLIANT WITH ACCESSIBILITY STANDARDS.
- 11 DRILL AND GROUT 1 - NO. 4 12" LONG REINFORCEMENT BAR (EPOXY COATED) WITH 3" MIN. COVER. REINFORCEMENT BARS ARE NOT NEEDED IF THE APPROACH NOSE IS POURED INTEGRAL WITH THE V CURB.
- 12 DRILL AND GROUT 2 - NO. 4 12" LONG REINFORCEMENT BARS (EPOXY COATED) WITH 3" MIN. COVER. REINFORCEMENT BARS ARE NOT NEEDED IF THE APPROACH NOSE IS POURED INTEGRAL WITH THE CURB AND GUTTER.
- 13 SIDE TREATMENT EXAMPLES SHOWN ARE WHEN THE INITIAL LANDING IS APPROXIMATELY LEVEL WITH THE FULL HEIGHT CURB (I.E. 6" LONG RAMP FOR 6" HIGH CURB). WHEN THE INITIAL LANDING IS MORE THAN 1" BELOW FULL HEIGHT CURB REFER TO SHEETS 1 & 2 TO MODIFY THE CURB HEIGHT TAPERS AND MAINTAIN POSITIVE BOULEVARD DRAINAGE. CONSTRUCT THESE TAPERS AT 0"-3" AT 8-10%, THEN LESS THAN 5% FROM 3" CURB TO FULL CURB HEIGHT.
- 14 NEAREST EDGE OF DETECTABLE WARNING SURFACES SHALL BE PLACED 12' MINIMUM TO 15' MAXIMUM FROM THE NEAREST RAIL. FOR SKEWED RAILWAYS IN NO INSTANCE SHALL THE DETECTABLE WARNING BE CLOSER THAN 12' MEASURED PERPENDICULAR TO THE NEAREST RAIL.
- 15 WHEN PEDESTRIAN GATES ARE PROVIDED, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE SIDE OF THE GATES OPPOSITE THE RAIL, 2' FROM THE APPROACHING SIDE OF THE GATE ARM. THIS CRITERIA GOVERNS OVER NOTE ⑪.
- 16 CROSSING SURFACE SHALL EXTEND 2' MINIMUM PAST THE OUTSIDE EDGE OF WALK OR SHARED-USE PATH.
- 17 3' FOR MEDIANS AND SPLITTER ISLANDS. NOSE CAN BE REDUCED TO 2' ON FREE RIGHT ISLANDS.
- 18 SIDEWALK TO BE PLACED 8.75' MIN. FROM THE FACE OF CURB/PROJECTED FACE OF CURB. THIS ENSURES MIN. CLEARANCE BETWEEN THE SIDEWALK AND GATE ARM COUNTERWEIGHT SUPPORTS.
- 19 CONSTRUCT WITH EXPANSION MATERIAL PER MNDOT SPECIFICATION 3702 TYPES A-E. EXPANSION MATERIAL SHALL MATCH FULL HEIGHT OF ADJACENT CONCRETE.

REVISIONS:
APPROVED: 11-04-2021
<i>Jeff J. Perkins</i>
JEFFREY PERKINS OPERATIONS DIVISION



STANDARD PLAN 5-297.250	4 OF 6
APPROVED: 11-04-2021	REVISOR:
STATE PROJ. NO.	(TH) SHEET NO. OF SHEETS

PEDESTRIAN CURB RAMP DETAILS	
STATE PROJ. NO.	(TH) SHEET NO. OF SHEETS



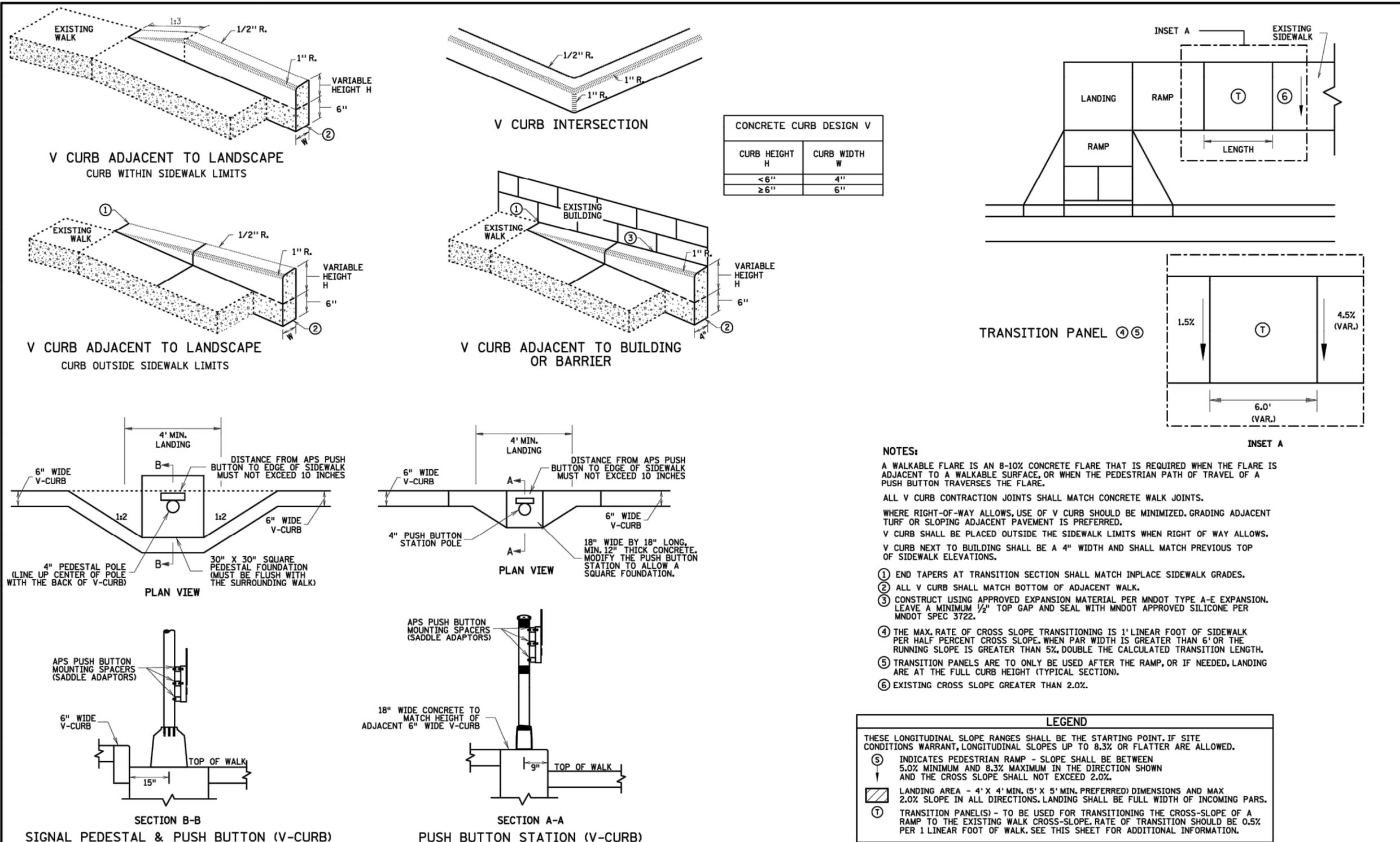
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THOMAS M. RIENTS		LIC. NO.: 63111	

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FIVE BELOW - BAXTER
 CIVIL DETAILS

C2.08
 PROJECT #: 2025-12133

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NOTES:

A WALKABLE FLARE IS AN 8-10% CONCRETE FLARE THAT IS REQUIRED WHEN THE FLARE IS ADJACENT TO A WALKABLE SURFACE, OR WHEN THE PEDESTRIAN PATH OF TRAVEL OF A PUSH BUTTON TRAVERSES THE FLARE.

ALL V CURB CONTRACTION JOINTS SHALL MATCH CONCRETE WALK JOINTS.

WHERE RIGHT-OF-WAY ALLOWS, USE OF V CURB SHOULD BE MINIMIZED. GRADING ADJACENT TURF OR SLOPING ADJACENT PAVEMENT IS PREFERRED.

V CURB SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS.

V CURB NEXT TO BUILDING SHALL BE A 4" WIDTH AND SHALL MATCH PREVIOUS TOP OF SIDEWALK ELEVATIONS.

- END TAPERS AT TRANSITION SECTION SHALL MATCH INPLACE SIDEWALK GRADES.
- ALL V CURB SHALL MATCH BOTTOM OF ADJACENT WALK.
- CONSTRUCT USING APPROVED EXPANSION MATERIAL PER MNDOT TYPE A-E EXPANSION. LEAVE A MINIMUM 1/2" TOP GAP AND SEAL WITH MNDOT APPROVED SILICONE PER MNDOT SPEC 3722.
- THE MAX. RATE OF CROSS SLOPE TRANSITIONING IS 1' LINEAR FOOT OF SIDEWALK PER HALF PERCENT CROSS SLOPE. WHEN PAR WIDTH IS GREATER THAN 6' OR THE RUNNING SLOPE IS GREATER THAN 5%, DOUBLE THE CALCULATED TRANSITION LENGTH.
- TRANSITION PANELS ARE TO ONLY BE USED AFTER THE RAMP, OR IF NEEDED, LANDING ARE AT THE FULL CURB HEIGHT (TYPICAL SECTION).
- EXISTING CROSS SLOPE GREATER THAN 2.0%.

LEGEND

THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.

⑤ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.

④ LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PARS.

① TRANSITION PANEL(S) - TO BE USED FOR TRANSITIONING THE CROSS-SLOPE OF A RAMP TO THE EXISTING WALK CROSS-SLOPE. RATE OF TRANSITION SHOULD BE 0.5% PER 1 LINEAR FOOT OF WALK. SEE THIS SHEET FOR ADDITIONAL INFORMATION.

REVISION:

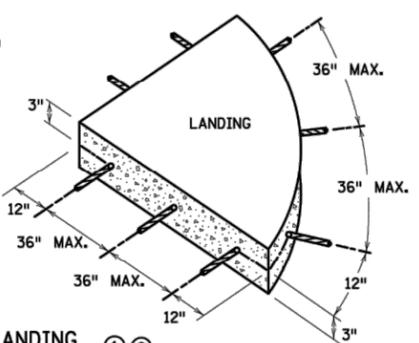
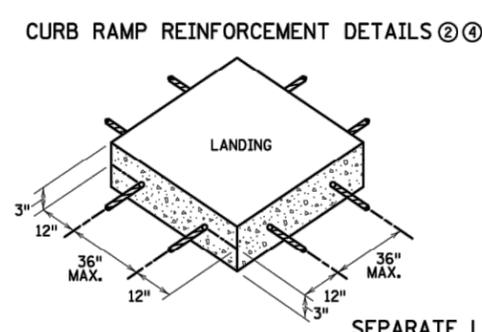
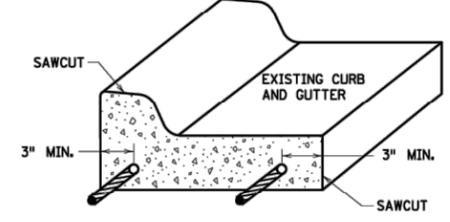
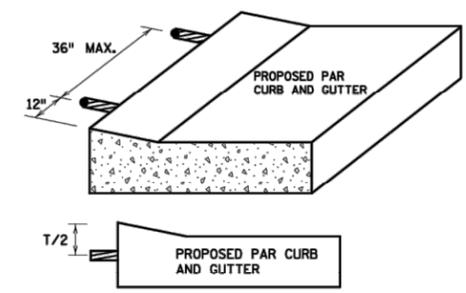
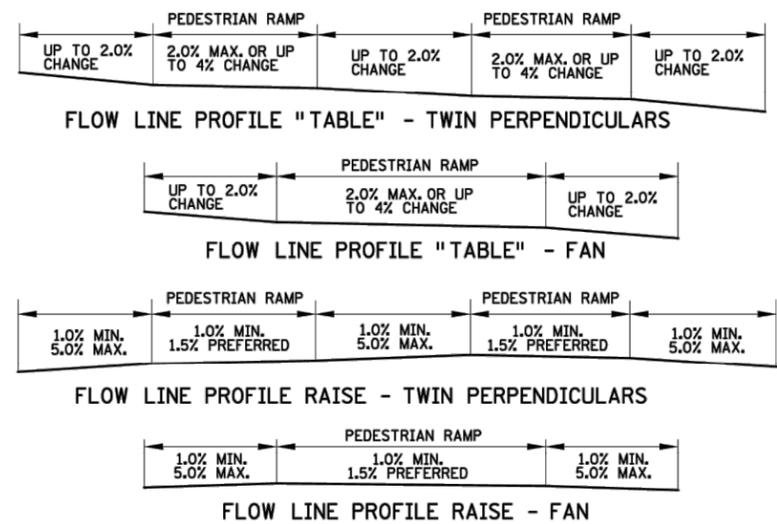
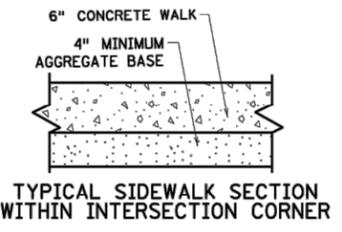
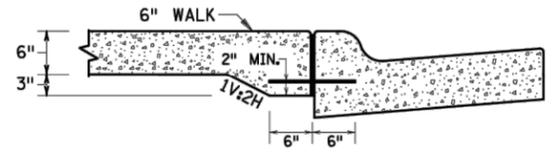
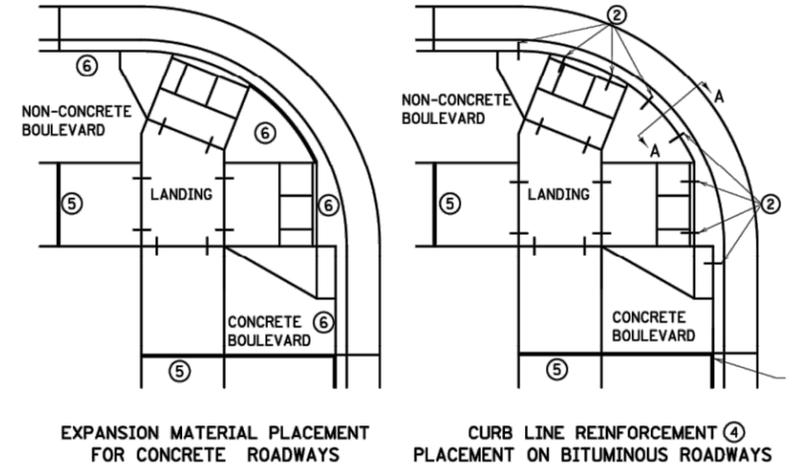
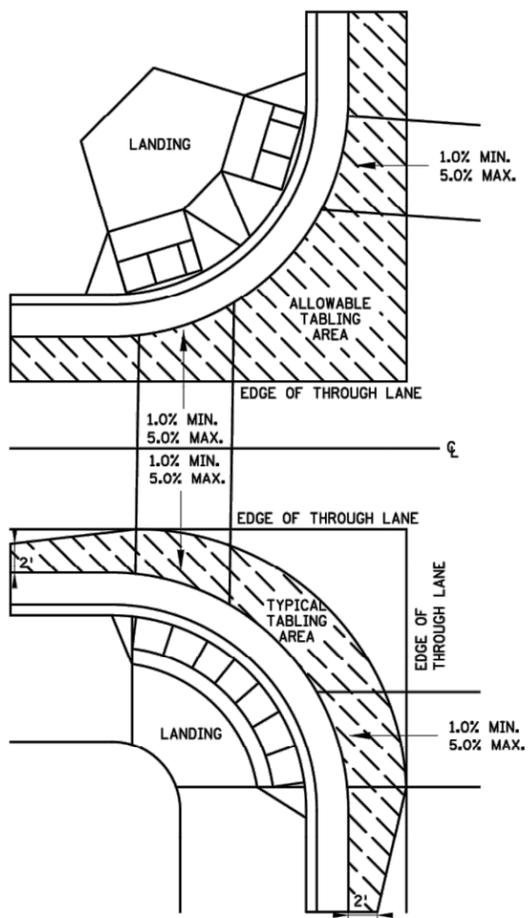
APPROVED: 11-04-2021

Jeff J. Perkins
JEFF PERKINS
OPERATIONS DIVISION

	STANDARD PLAN 5-297.250	5 OF 6	PEDESTRIAN CURB RAMP DETAILS
	 THOMAS M. RIENTS STATE DESIGN ENGINEER	APPROVED: 11-04-2021 REVISED:	

REV. 3-25	CITY OF BAXTER, MINNESOTA PEDESTRIAN CURB RAMP DETAILS PUBLIC WORKS DEPARTMENT	RD-5
		C2.09 PROJECT #: 2025-12133

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GENERAL NOTES:

"TABLING" OF CROSSWALKS MEANS MAINTAINING LESS THAN 2% CROSS SLOPE WITHIN A CROSSWALK, IS REQUIRED WHEN A ROADWAY IS IN A STOP OR YIELD CONDITION AND THE PROJECT SCOPE ALLOWS.

RECONSTRUCTION PROJECTS: ON FULL PAVEMENT REPLACEMENT PROJECTS "TABLING" OF ENTIRE CROSSWALK SHALL OCCUR WHEN FEASIBLE.

MILL & OVERLAY PROJECTS: "TABLING" OF FLOW LINES, IN FRONT OF THE PEDESTRIAN RAMP, IS REQUIRED WHEN THE EXISTING FLOW LINE IS GREATER THAN 2%. WARPING OF THE BITUMINOUS PAVEMENT CAN NOT EXTEND INTO THE THROUGH LANE. TABLE THE FLOW LINE TO 2% OR AS MUCH AS POSSIBLE WHILE ADHERING TO THE FOLLOWING CRITERIA:
 1) 1.0% MIN. CROSS-SLOPE OF THE ROAD
 2) 5.0% MAX. CROSS-SLOPE OF THE ROAD
 3) "TABLE" FLOW LINE UP TO 4% CHANGE FROM EXISTING SLOPE IN FRONT OF PEDESTRIAN RAMP
 4) UP TO 2% CHANGE IN FLOW LINE FROM EXISTING SLOPE BEYOND THE PEDESTRIAN CURB RAMP

STAND-ALONE ADA RETROFITS: FOLLOW MILL & OVERLAY CRITERIA ABOVE HOWEVER ALL PAVEMENT WARPING IS DONE WITH BITUMINOUS PATCHING ON BITUMINOUS ROADWAYS AND FULL-DEPTH APRON REPLACEMENT ON CONCRETE ROADWAYS.

RAISING OF CURB LINES SHOULD OCCUR IN VERTICALLY CONSTRAINED AREAS. RAISE THE CURB LINES ENOUGH TO ALLOW COMPLIANT RAMPS OR AS MUCH AS POSSIBLE WHILE ADHERING TO THE FOLLOWING CRITERIA:
 1) 1.0% MIN. AND 5.0% MAXIMUM CROSS-SLOPE OF THE ROAD
 2) 1.0% MIN. FLOW LINE (ON EITHER SIDE OF PEDESTRIAN RAMP) TO MAINTAIN POSITIVE DRAINAGE
 3) 5.0% RECOMMENDED MAX. FLOW LINE
 4) LONGITUDINAL THROUGH LANE ROADWAY TAPERS SHOULD BE 1" VERTICAL PER 15' HORIZONTAL

NOTES:

- TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, ALL INITIAL LANDINGS AT A TOP OF A RAMPED SURFACE (RUNNING SLOPE GREATER THAN 2%) SHALL BE FORMED AND PLACED SEPARATELY IN AN INDEPENDENT CONCRETE POUR. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON THIS SHEET FOR ALL SEPARATELY POURED INITIAL LANDINGS.
- DRILL AND GROUT NO. 4 12" LONG REINFORCEMENT BARS (EPOXY COATED) AT 36" MAXIMUM CENTER TO CENTER MINIMUM 12" SPACING FROM CONSTRUCTION JOINTS. BARS TO BE ADJUSTED TO MATCH RAMP GRADE. BARS TO BE PAID BY EACH.
- DRILL AND GROUT 2 - NO. 4 X 12" LONG (6" EMBEDDED) REINFORCEMENT BARS (EPOXY COATED). REINFORCEMENT REQUIRED FOR ALL CONSTRUCTION JOINTS. BARS TO BE PAID BY EACH.
- THIS CURB LINE REINFORCEMENT DETAIL SHALL BE USED ON BITUMINOUS ROADWAYS. FOR CONCRETE ROADWAYS, SEE NOTE 6.
- CONSTRUCT WITH EXPANSION MATERIAL PER MNDOT SPECIFICATION 3702 TYPES A-E. EXPANSION MATERIAL SHALL MATCH FULL HEIGHT OF ADJACENT CONCRETE.
- USE AN APPROVED TYPE F (1/4 INCH THICK) SEPARATION MATERIAL. SEPARATION MATERIAL SHALL MATCH FULL HEIGHT DIMENSION OF ADJACENT CONCRETE.

REVISIONS:
 APPROVED: 11-04-2021
 Jeff A. Perkins
 OPERATIONS DIVISION



STANDARD PLAN 5-297.250 6 OF 6
 APPROVED: 11-04-2021
 REVISIONS:
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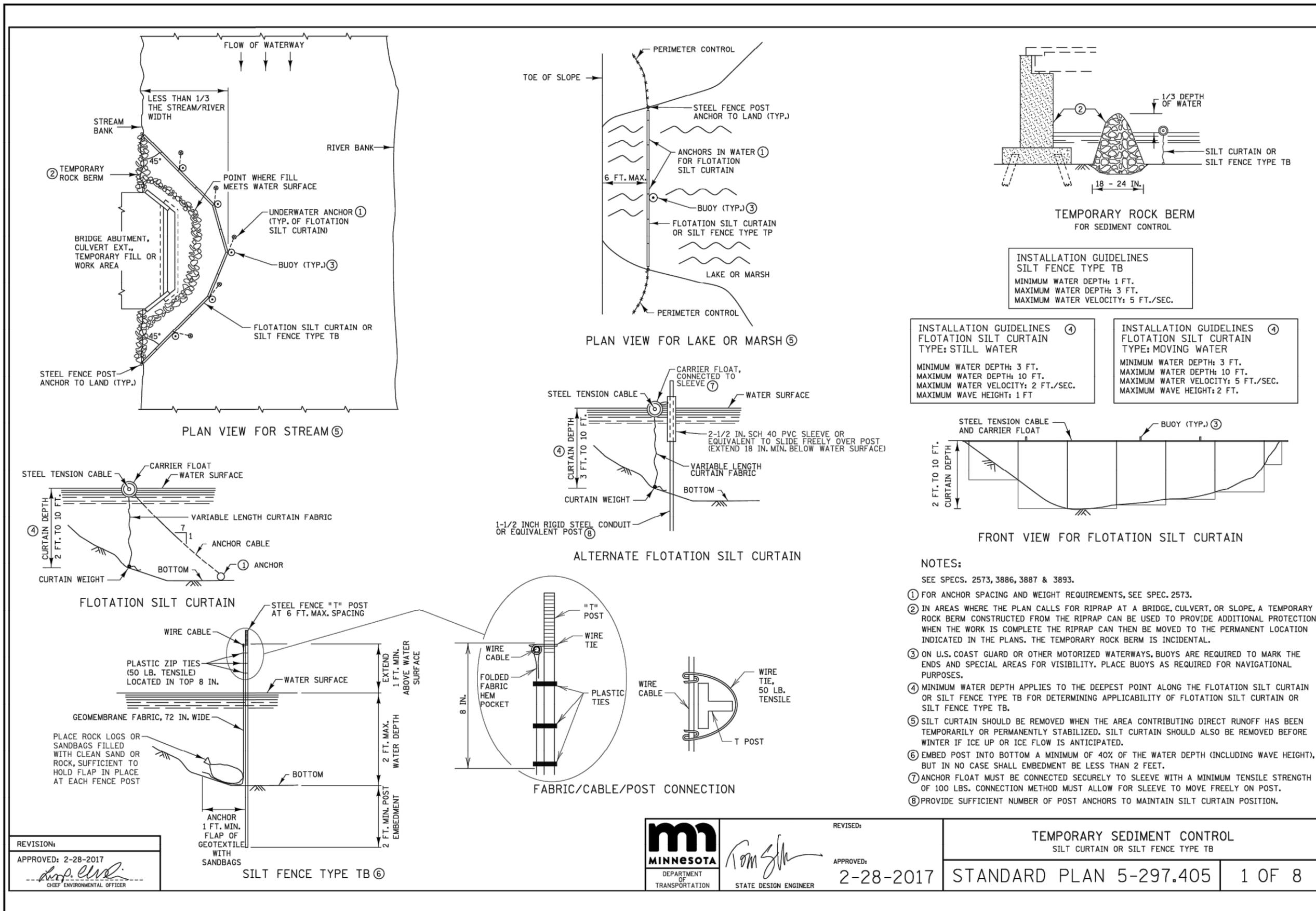
PEDESTRIAN CURB RAMP DETAILS



DRAWN BY: MJM
 CHECKED BY: ALW
 THOMAS M. RIENIS LIC. NO.: 63111 DATE: 02/09/2026

DATE	REV#	REVISIONS DESCRIPTION

FIVE BELOW - BAXTER
 CIVIL DETAILS



- NOTES:**
- SEE SPECS. 2573, 3886, 3887 & 3893.
- FOR ANCHOR SPACING AND WEIGHT REQUIREMENTS, SEE SPEC. 2573.
 - IN AREAS WHERE THE PLAN CALLS FOR RIPRAP AT A BRIDGE, CULVERT, OR SLOPE, A TEMPORARY ROCK BERM CONSTRUCTED FROM THE RIPRAP CAN BE USED TO PROVIDE ADDITIONAL PROTECTION. WHEN THE WORK IS COMPLETE THE RIPRAP CAN THEN BE MOVED TO THE PERMANENT LOCATION INDICATED IN THE PLANS. THE TEMPORARY ROCK BERM IS INCIDENTAL.
 - ON U.S. COAST GUARD OR OTHER MOTORIZED WATERWAYS, BUOYS ARE REQUIRED TO MARK THE ENDS AND SPECIAL AREAS FOR VISIBILITY. PLACE BUOYS AS REQUIRED FOR NAVIGATIONAL PURPOSES.
 - MINIMUM WATER DEPTH APPLIES TO THE DEEPEST POINT ALONG THE FLOTATION SILT CURTAIN OR SILT FENCE TYPE TB FOR DETERMINING APPLICABILITY OF FLOTATION SILT CURTAIN OR SILT FENCE TYPE TB.
 - SILT CURTAIN SHOULD BE REMOVED WHEN THE AREA CONTRIBUTING DIRECT RUNOFF HAS BEEN TEMPORARILY OR PERMANENTLY STABILIZED. SILT CURTAIN SHOULD ALSO BE REMOVED BEFORE WINTER IF ICE UP OR ICE FLOW IS ANTICIPATED.
 - EMBED POST INTO BOTTOM A MINIMUM OF 40% OF THE WATER DEPTH (INCLUDING WAVE HEIGHT), BUT IN NO CASE SHALL EMBEDMENT BE LESS THAN 2 FEET.
 - ANCHOR FLOAT MUST BE CONNECTED SECURELY TO SLEEVE WITH A MINIMUM TENSILE STRENGTH OF 100 LBS. CONNECTION METHOD MUST ALLOW FOR SLEEVE TO MOVE FREELY ON POST.
 - PROVIDE SUFFICIENT NUMBER OF POST ANCHORS TO MAINTAIN SILT CURTAIN POSITION.

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APPROVED: 2-28-2017
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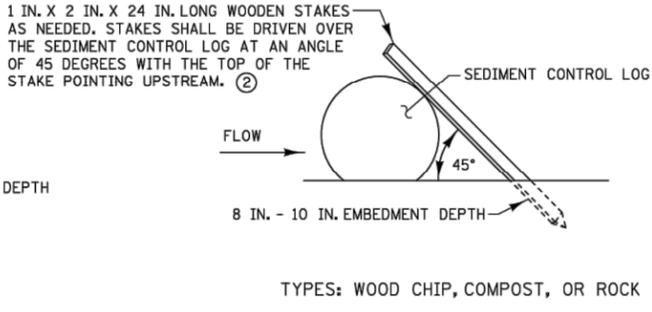
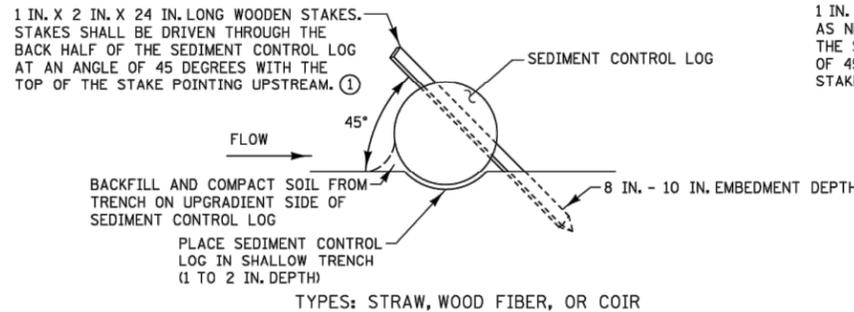
REVISOR:
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APPROVED: 2-28-2017
STATE DESIGN ENGINEER

TEMPORARY SEDIMENT CONTROL
SILT CURTAIN OR SILT FENCE TYPE TB
STANDARD PLAN 5-297.405 1 OF 8

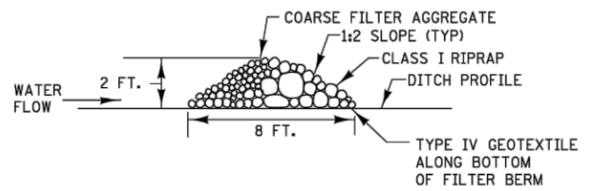
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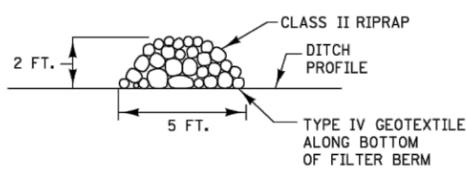
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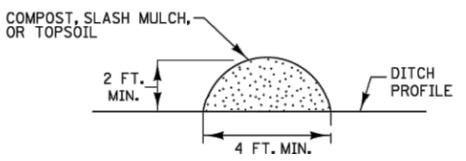
SEDIMENT CONTROL LOGS



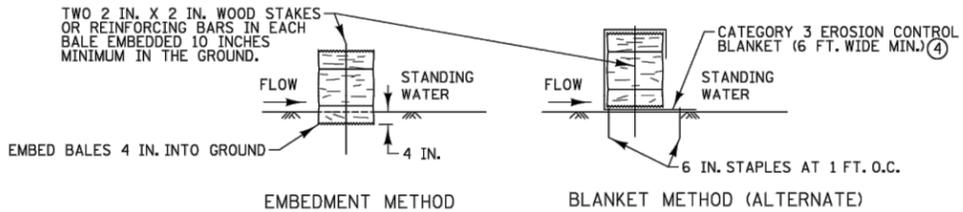
TYPE 3 (ROCK WEEPER)



TYPE 5 (ROCK) FILTER BERMS



TYPE 1 (COMPOST), TYPE 2 (SLASH MULCH), OR TYPE 4 (TOPSOIL)



BALE BARRIERS ③

- NOTES:
- SEE SPECS. 2573, 3149, 3874, 3882, 3886, & 3897.
 - ① SPACE BETWEEN STAKES SHALL BE A MAXIMUM OF 1 FOOT FOR DITCH CHECKS OR 2 FEET FOR OTHER APPLICATIONS.
 - ② PLACE STAKES AS NEEDED TO PREVENT MOVEMENT OF SEDIMENT CONTROL LOGS PLACED ON SLOPES OR AS NEEDED DUE TO OTHER FACTORS. STAKES SHALL BE INCIDENTAL.
 - ③ TO BE USED FOR CRITICAL PERIMETER CONTROL AREAS WHERE STANDING WATER OCCURS (6 INCH MAX. DEPTH). BALES SHALL CONSIST OF TYPE 1 MULCH OF APPROXIMATELY 14 IN. X 18 IN. X 36 IN. LONG. BALES SHALL BE PLACED ON EDGE AND BUTTED TIGHT TO ADJACENT BALES.
 - ④ INSTEAD OF TRENCHING, PLACE BALE ON THE BLANKET AND WRAP BLANKET AROUND THE BALE. PLACE STAKE THROUGH BALE AND BLANKET.

REVISIONS:
APPROVED: 2-28-2017
Chief Environmental Officer
CHIEF ENVIRONMENTAL OFFICER

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MINNESOTA
DEPARTMENT
OF
TRANSPORTATION

REVISED:
Tom S...
APPROVED:
2-28-2017
STATE DESIGN ENGINEER

TEMPORARY SEDIMENT CONTROL
FILTER BERMS, SEDIMENT CONTROL LOGS, AND BALE BARRIERS
STANDARD PLAN 5-297.405 | 2 OF 8



REV. 3-25
 CITY OF BAXTER, MINNESOTA
 FILTER BERMS, SEDIMENT CONTROL LOGS AND
 BALE BARRIERS
 PUBLIC WORKS DEPARTMENT
 EC-2

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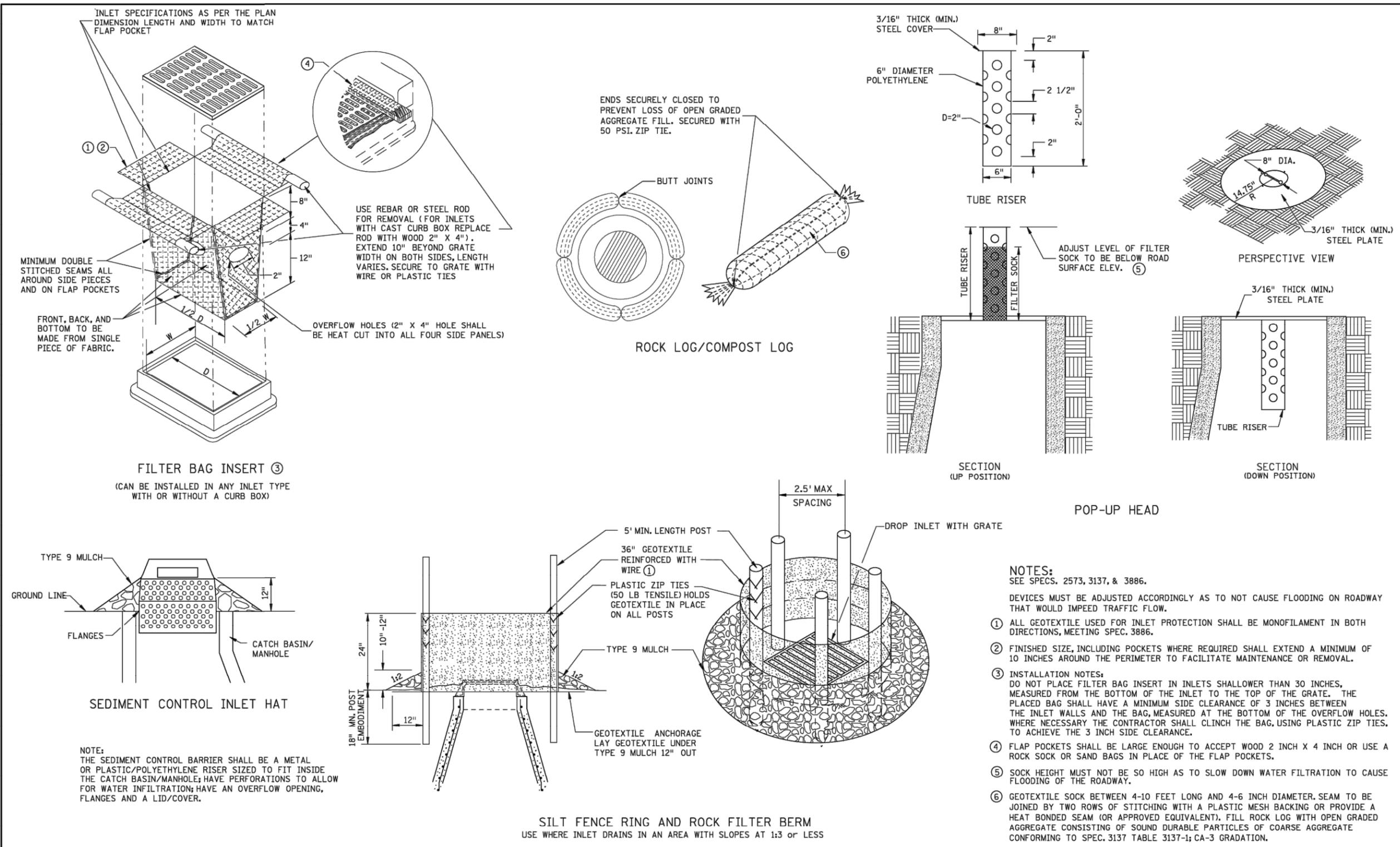
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THOMAS M. RIENTS
LIC. NO.: 63111 DATE: 02/09/2026

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CIVIL DETAILS

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PROJECT #: 2025-12133

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- NOTES:**
SEE SPECS. 2573, 3137, & 3886.
- DEVICES MUST BE ADJUSTED ACCORDINGLY AS TO NOT CAUSE FLOODING ON ROADWAY THAT WOULD IMPEDE TRAFFIC FLOW.
- ALL GEOTEXTILE USED FOR INLET PROTECTION SHALL BE MONOFILAMENT IN BOTH DIRECTIONS, MEETING SPEC. 3886.
 - FINISHED SIZE, INCLUDING POCKETS WHERE REQUIRED SHALL EXTEND A MINIMUM OF 10 INCHES AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
 - INSTALLATION NOTES:**
DO NOT PLACE FILTER BAG INSERT IN INLETS SHALLOWER THAN 30 INCHES, MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE. THE PLACED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE OF 3 INCHES BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES. WHERE NECESSARY THE CONTRACTOR SHALL CLINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3 INCH SIDE CLEARANCE.
 - FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2 INCH X 4 INCH OR USE A ROCK SOCK OR SAND BAGS IN PLACE OF THE FLAP POCKETS.
 - SOCK HEIGHT MUST NOT BE SO HIGH AS TO SLOW DOWN WATER FILTRATION TO CAUSE FLOODING OF THE ROADWAY.
 - GEOTEXTILE SOCK BETWEEN 4-10 FEET LONG AND 4-6 INCH DIAMETER. SEAM TO BE JOINED BY TWO ROWS OF STITCHING WITH A PLASTIC MESH BACKING OR PROVIDE A HEAT BONDED SEAM (OR APPROVED EQUIVALENT). FILL ROCK LOG WITH OPEN GRADED AGGREGATE CONSISTING OF SOUND DURABLE PARTICLES OF COARSE AGGREGATE CONFORMING TO SPEC. 3137 TABLE 3137-1; CA-3 GRADATION.

REVISION:
APPROVED: 2-28-2017
[Signature]
CHIEF ENVIRONMENTAL OFFICER

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MINNESOTA
DEPARTMENT OF TRANSPORTATION
[Signature]
STATE DESIGN ENGINEER
APPROVED:
2-28-2017

TEMPORARY SEDIMENT CONTROL
STORM DRAIN INLET PROTECTION
STANDARD PLAN 5-297.405 4 OF 8

REV. 3-25

CITY OF BAXTER, MINNESOTA
STORM DRAIN INLET PROTECTION
PUBLIC WORKS DEPARTMENT

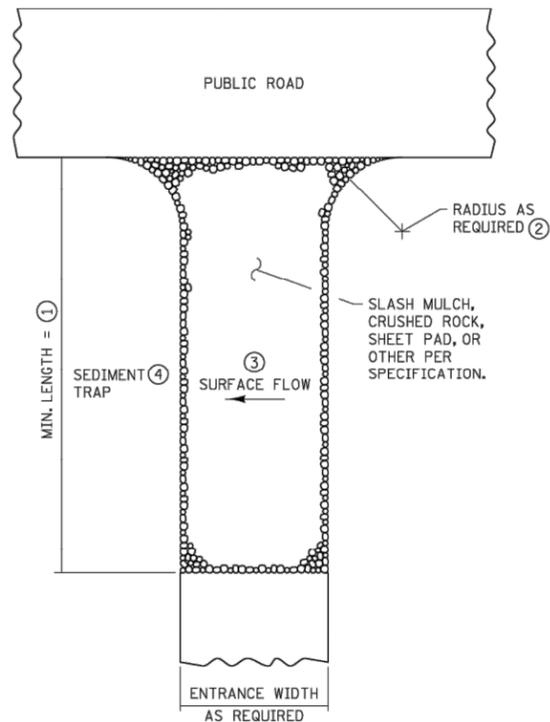


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THOMAS M. RIENTS
LIC. NO.: 63111 DATE: 02/09/2026

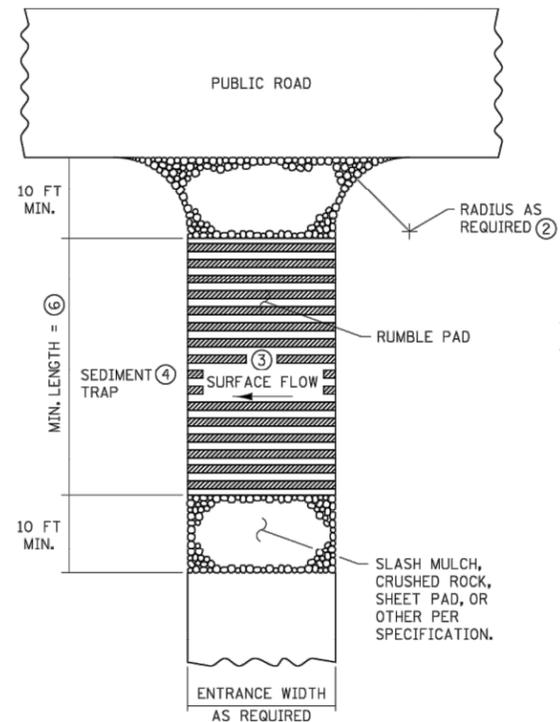
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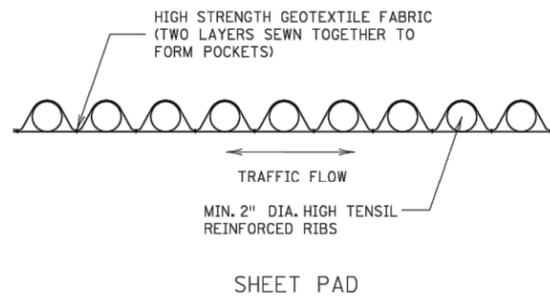
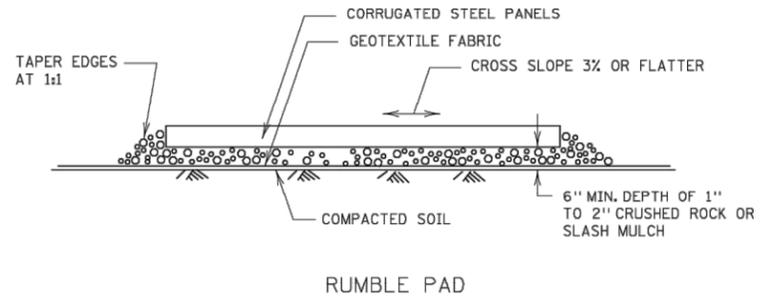
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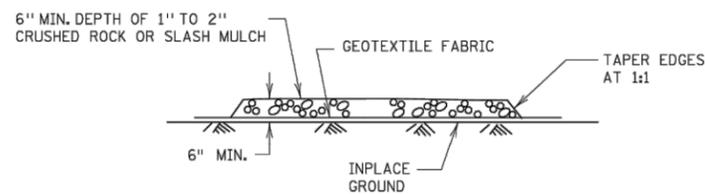
SLASH MULCH, CRUSHED ROCK, OR SHEET PAD CONSTRUCTION EXIT (5) (7)



RUMBLE PAD CONSTRUCTION EXIT (5) (7)



SHEET PAD



SLASH MULCH OR CRUSHED ROCK

NOTES:

SEE SPECS. 2573 & 3882.

- ① MINIMUM LENGTH SHALL BE THE GREATER OF 50 FEET OR A LENGTH SUFFICIENT TO ALLOW A MINIMUM OF 5 TIRE ROTATIONS ON THE PROVIDED PAD. MINIMUM LENGTH SHALL BE CALCULATED USING THE LARGEST TIRE WHICH WILL BE USED IN TYPICAL OPERATIONS.
- ② PROVIDE RADIUS OR WIDEN PAD SUFFICIENTLY TO PREVENT VEHICLE TIRES FROM TRACKING OFF OF PAD WHEN LEAVING SITE.
- ③ IF RUNOFF FROM DISTURBED AREAS FLOWS TOWARD CONSTRUCTION EXITS, PREVENT RUNOFF FROM DRAINING DIRECTLY TO PUBLIC ROAD OVER CONSTRUCTION EXIT BY CROWNING THE EXIT OR SLOPING TO ONE SIDE. IF SURFACE GRADING IS INSUFFICIENT, PROVIDE OTHER MEANS OF INTERCEPTING RUNOFF.
- ④ IF RUNOFF FROM CONSTRUCTION EXITS WILL DRAIN OFF OF PROJECT SITE, PROVIDE SEDIMENT TRAP WITH STABILIZED OVERFLOW.
- ⑤ IF A TIRE WASH OFF IS REQUIRED THE CONSTRUCTION EXITS SHALL BE GRADED TO DRAIN THE WASH WATER TO A SEDIMENT TRAP.
- ⑥ MINIMUM LENGTH OF RUMBLE PAD SHALL BE 20 FEET, OR AS REQUIRED TO REMOVE SEDIMENT FROM TIRES. IF SIGNIFICANT SEDIMENT IS TRACKED FROM THE SITE, THE RUMBLE PAD SHALL BE LENGTHENED OR THE DESIGN MODIFIED TO PROVIDE ADDITIONAL VIBRATION. WASH-OFF LENGTH SHALL BE AS REQUIRED TO EFFECTIVELY REMOVE CONSTRUCTION SEDIMENT FROM VEHICLE TIRES.
- ⑦ MAINTENANCE OF CONSTRUCTION EXITS SHALL OCCUR WHEN THE EFFECTIVENESS OF SEDIMENT REMOVAL HAS BEEN REDUCED. MAINTENANCE SHALL CONSIST OF REMOVING SEDIMENT AND CLEANING THE MATERIALS OR PLACING ADDITIONAL MATERIAL (SLASH MULCH OR CRUSHED ROCK) OVER SEDIMENT FILLED MATERIAL TO RESTORE EFFECTIVENESS.

REVISIONS:
 APPROVED: 2-28-2017

 CHIEF ENVIRONMENTAL OFFICER

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 MINNESOTA
 DEPARTMENT
 OF
 TRANSPORTATION

REVISOR:

 STATE DESIGN ENGINEER

APPROVED:
 2-28-2017

TEMPORARY SEDIMENT CONTROL
 STABILIZED CONSTRUCTION EXIT
 STANDARD PLAN 5-297.405 5 OF 8



CITY OF BAXTER, MINNESOTA
 STABILIZED CONSTRUCTION EXIT
 PUBLIC WORKS DEPARTMENT

REV. 3-25

EC-5

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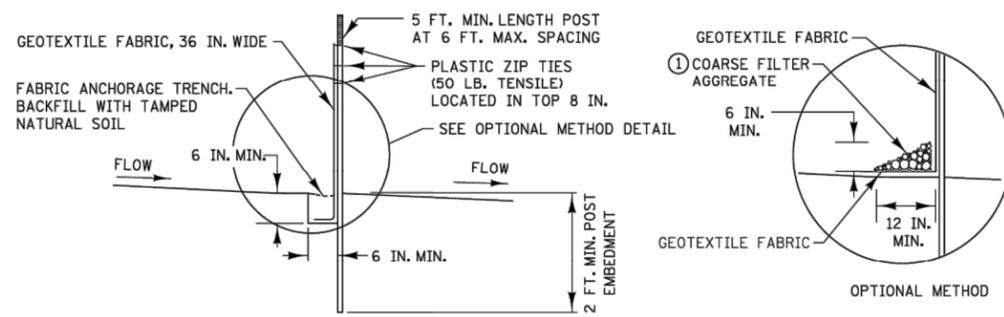
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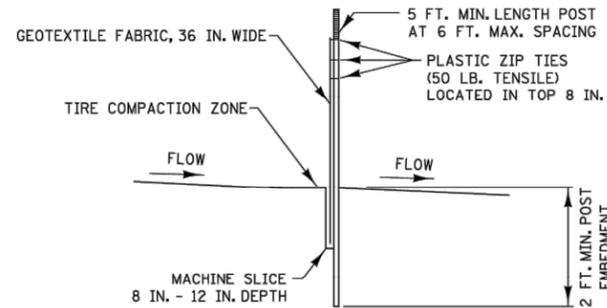
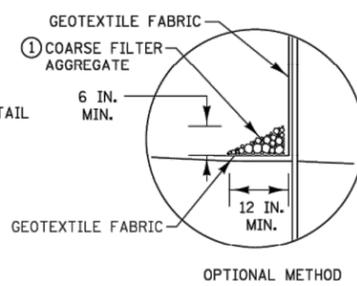
CITY OF BAXTER, MINNESOTA

SILT FENCE

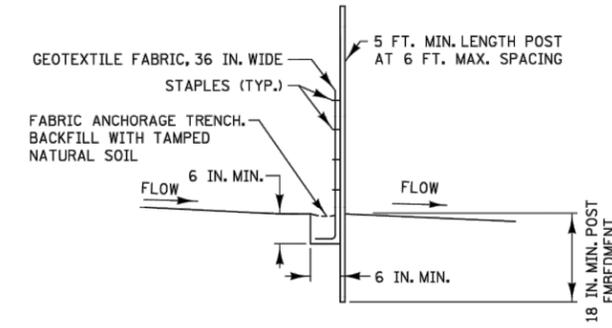
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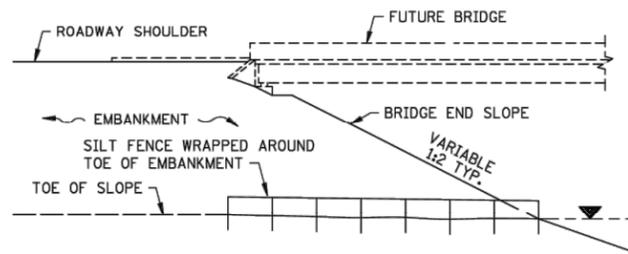
SILT FENCE TYPE HI ② (HAND INSTALLED)



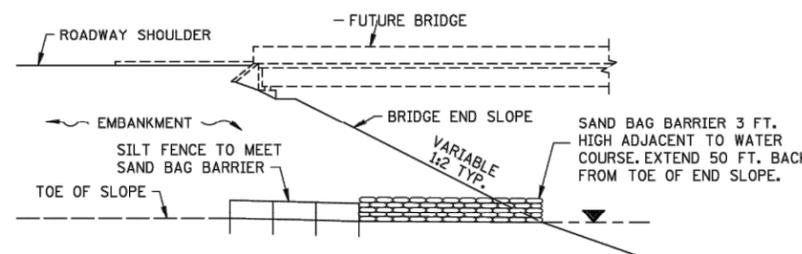
SILT FENCE TYPE MS ② (MACHINE SLICED)



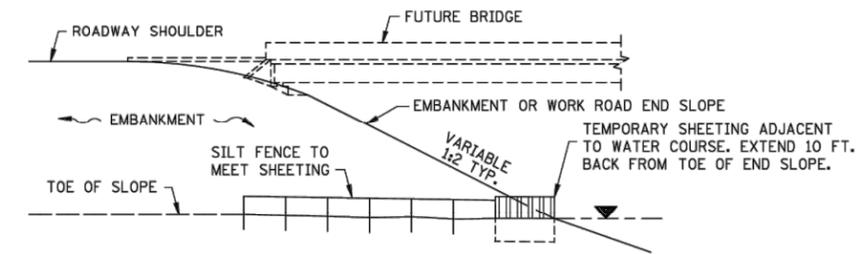
SILT FENCE TYPE PA ③ (PREASSEMBLED)



SILT FENCE ONLY ④

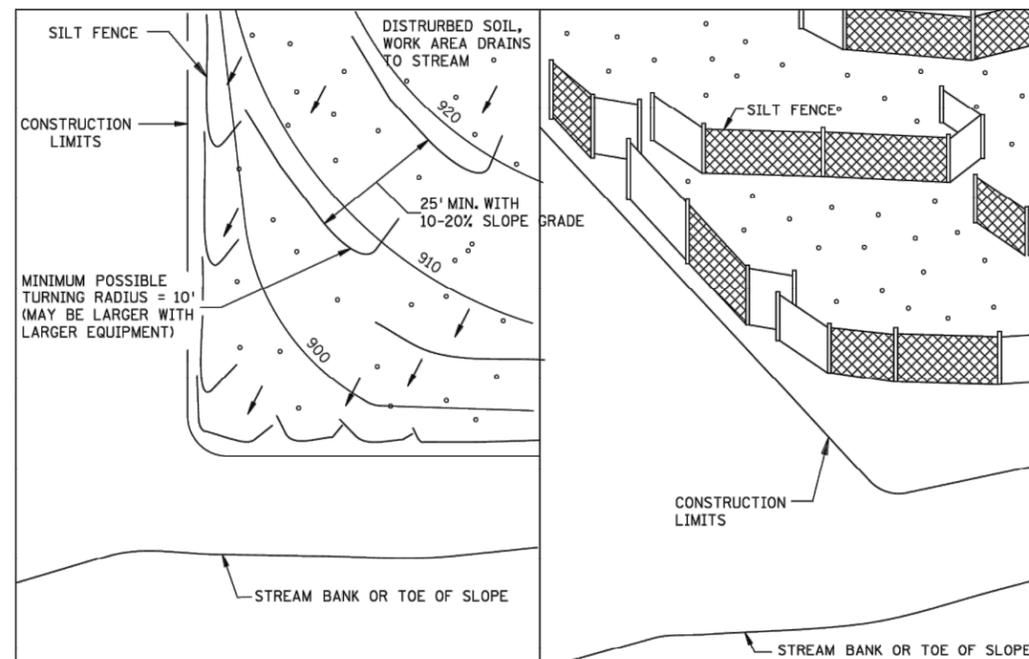


SILT FENCE WITH SAND BAGS ⑤



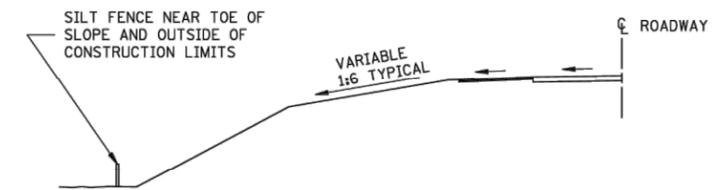
SILT FENCE WITH SHEETING ⑥

INSTALLATION AT BRIDGE EMBANKMENT ADJACENT TO WATER



PLAN VIEW

PERSPECTIVE VIEW



LOCATION AT TOE OF ROADWAY EMBANKMENT

NOTES:

- SEE SPECS. 2573, 3149 & 3886.
- ① COARSE FILTER AGGREGATE (SPEC. 3149) SHALL BE INCIDENTAL.
- ② TO PROTECT AREAS FROM SHEET FLOW. MAXIMUM CONTRIBUTING AREA: 1 ACRE.
- ③ TO PROTECT AREAS FROM SHEET FLOW. MAXIMUM CONTRIBUTING AREA: 0.25 ACRE.
- ④ WATER COURSE FLOW VELOCITY: STANDING. CONTRIBUTING SLOPE AREA: 1/2 ACRE.
- ⑤ WATER COURSE FLOW VELOCITY: 1 TO 7 FT./SEC. CONTRIBUTING SLOPE AREA: 1 ACRE.
- ⑥ WATER COURSE FLOW VELOCITY: 8 TO 15 FT./SEC. CONTRIBUTING SLOPE AREA: 3 ACRES.

REVISION:
APPROVED: 2-28-2017
Chief Environmental Officer



REVISOR:
APPROVED: 2-28-2017
STATE DESIGN ENGINEER

TEMPORARY SEDIMENT CONTROL
SILT FENCE
STANDARD PLAN 5-297.405 6 OF 8

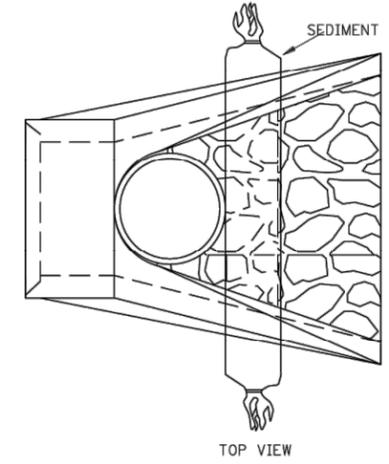
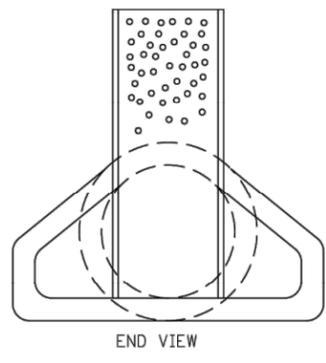
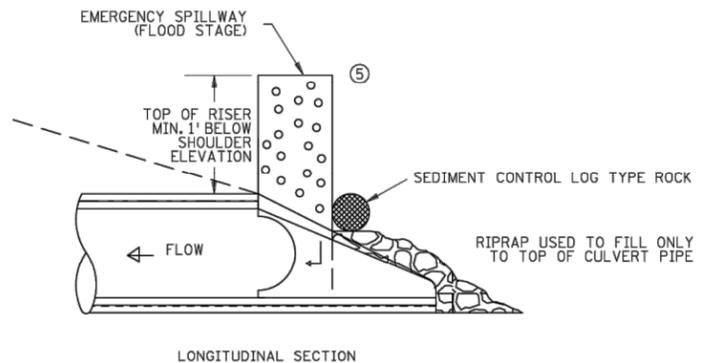
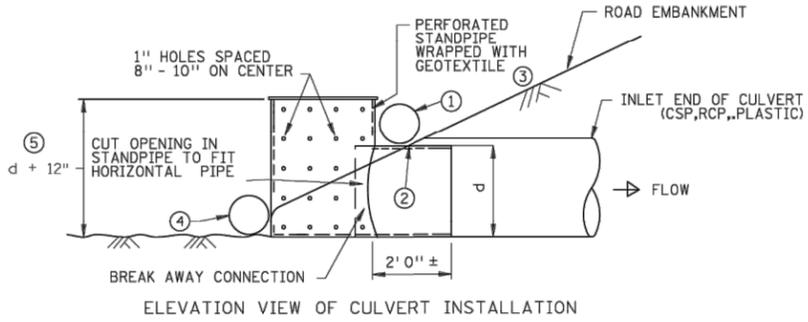
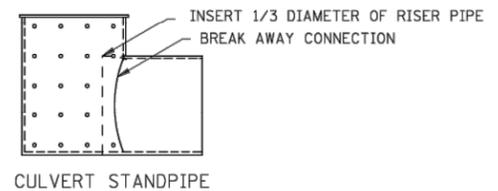
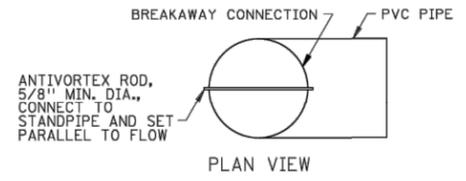


DRAWN BY: MJM
CHECKED BY: ALW
THOMAS M. RIENS
LIC. NO.: 63111
DATE: 02/09/2026

DATE	REV#	REVISIONS DESCRIPTION

FIVE BELOW - BAXTER
CIVIL DETAILS

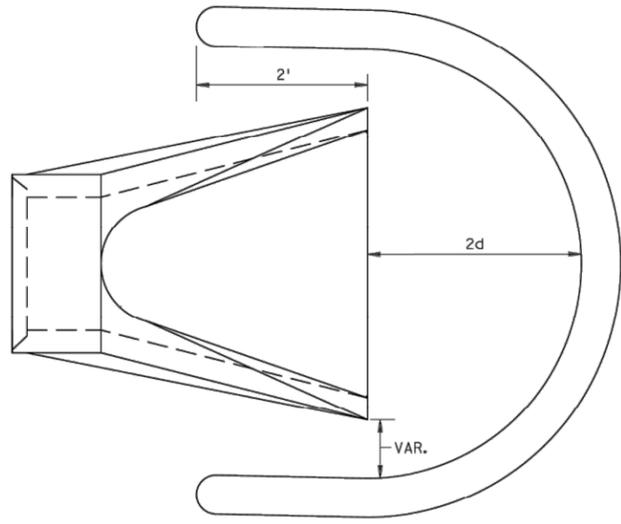
C2.15
PROJECT #: 2025-12133



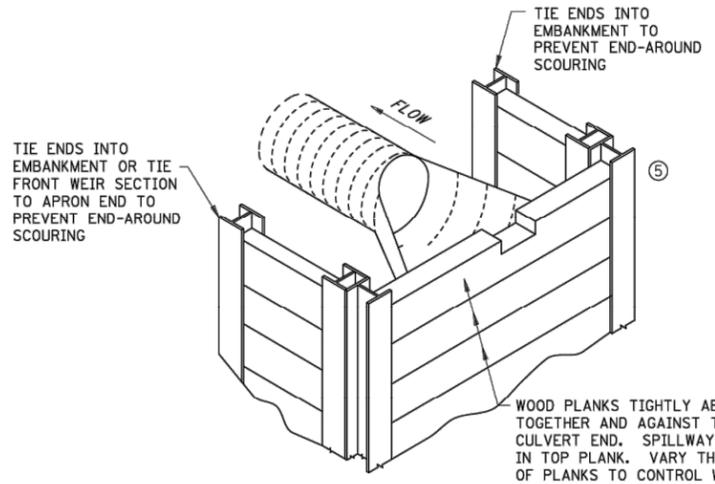
NOTE: SEDIMENT CONTROL LOG TYPE ROCK MAY BE WRAPPED AROUND RISER

CULVERT STANDPIPE INSERT (D-RISER)
 d = CULVERT SIZE: 12" - 36"

CULVERT STANDPIPE INSERT (D-RISER)



SEDIMENT CONTROL LOG WEIR (COMPOST, WOOD CHIP, OR ROCK)
 d = CULVERT SIZE: 12" - 36"



WOOD PLANK WEIR

NOTES:

- SEE SPECS. 2573, 3891 & 3893.
- FOR USE WHEN TEMPORARY PONDING IS NEEDED IN DITCH SECTIONS FOR SEDIMENT CONTROL.
- MANUFACTURED ALTERNATIVES LISTED ON MnDOT'S APPROVED PRODUCTS LIST MAY BE SUBSTITUTED AT NO ADDITIONAL COST.
- ① ROCK LOG OR SANDBAG TO HOLD STANDPIPE AND ACT AS A SEAL BETWEEN RISER PIPE AND CULVERT.
- ② PLACE CULVERT APRON AND SLIDE TEMPORARY STANDPIPE INTO CSP OR RCP CULVERT.
- ③ ALL GEOTEXTILE USED FOR CULVERT PROTECTION SHALL BE MONOFILAMENT IN BOTH DIRECTIONS, MEETING SPEC. 3886 FOR MACHINE SLICED.
- ④ ROCK LOG OR RIP RAP TO HOLD STANDPIPE AND ACT AS A FILTER BETWEEN RISER PIPE AND CULVERT.
- ⑤ HEIGHT OVERFLOW NOT TO CAUSE FLOODING OF ROAD OR ADJACENT PROPERTIES.

REVISION:
 APPROVED: 2-28-2017

 CHIEF ENVIRONMENTAL OFFICER



REVISOR:

 STATE DESIGN ENGINEER
 APPROVED:
 2-28-2017

TEMPORARY SEDIMENT CONTROL
 CULVERT END CONTROLS
 STANDARD PLAN 5-297.405 8 OF 8

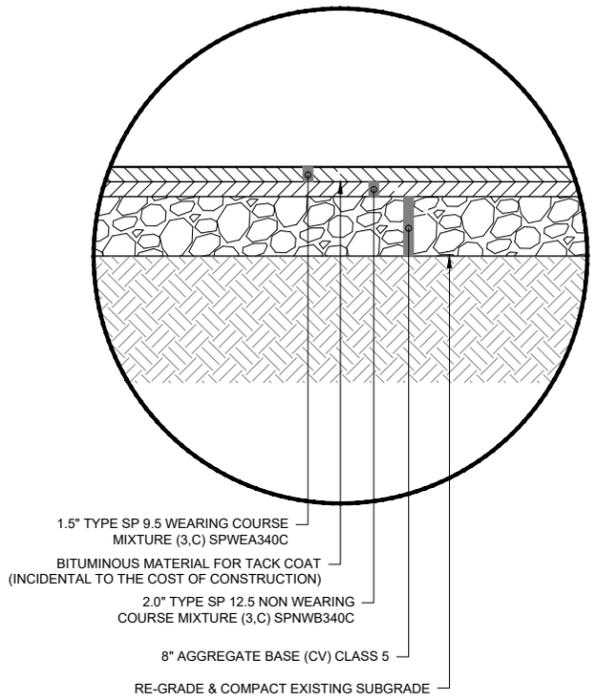


DRAWN BY: MJM
 CHECKED BY: ALW
 THOMAS M. RIENTS
 LIC. NO.: 63111 DATE: 02/09/2026

DATE	REV#	REVISIONS DESCRIPTION

FIVE BELOW - BAXTER
 CIVIL DETAILS

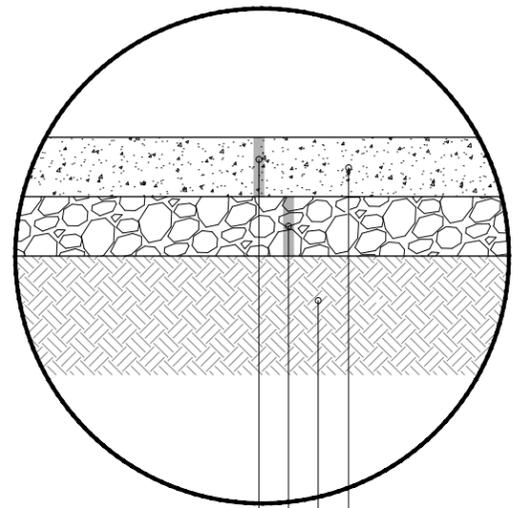
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1.5" TYPE SP 9.5 WEARING COURSE MIXTURE (3.C) SPWEA340C
 BITUMINOUS MATERIAL FOR TACK COAT (INCIDENTAL TO THE COST OF CONSTRUCTION)
 2.0" TYPE SP 12.5 NON WEARING COURSE MIXTURE (3.C) SPNWB340C
 8" AGGREGATE BASE (CV) CLASS 5
 RE-GRADE & COMPACT EXISTING SUBGRADE

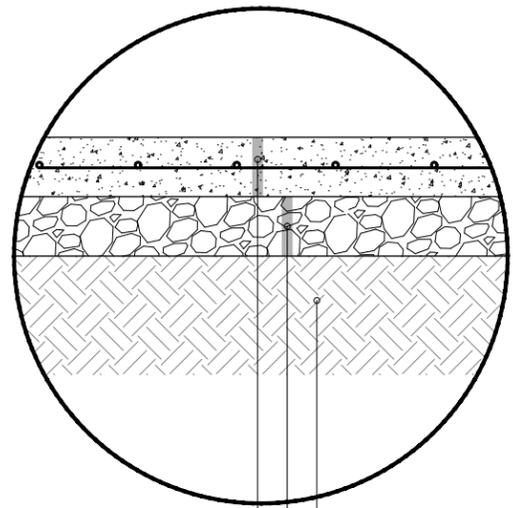
NOTE:
 VERIFY EXISTING PAVEMENT SECTION.
 NOTIFY ENGINEER IF DIFFERENT FROM SHOWN

PROPOSED BITUMINOUS PAVING SECTION - FULL DEPTH AREAS
 SCALE: NONE



6" CONCRETE WALK
 4" AGGREGATE BASE (CV) CLASS 5
 SCARIFY & COMPACT SUBGRADE
 WHERE NOTED ON PLANS FOR SNOWMELT, CONCRETE WALK TO INCLUDE W4X4 MESH PLACED 1.5'-2" BELOW FINISHED GRADE AND COORDINATED WITH ELECTRICAL

CONCRETE SIDEWALK SECTION
 SCALE: NONE



8" CONCRETE WITH #5 REBAR AT 16" O.C. BOTH WAYS
 6" AGGREGATE BASE (CV) CLASS 5
 SCARIFY & COMPACT SUBGRADE

REINFORCED CONCRETE PAVEMENT SECTION
 SCALE: NONE



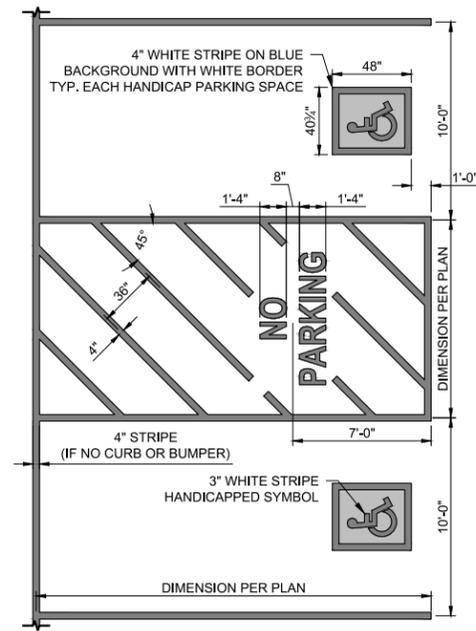
DRAWN BY: MJM
 CHECKED BY: ALW
 THOMAS M. RIENTS
 LIC. NO.: 63111 DATE: 02/09/2026

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

DATE	REV#	REVISIONS DESCRIPTION

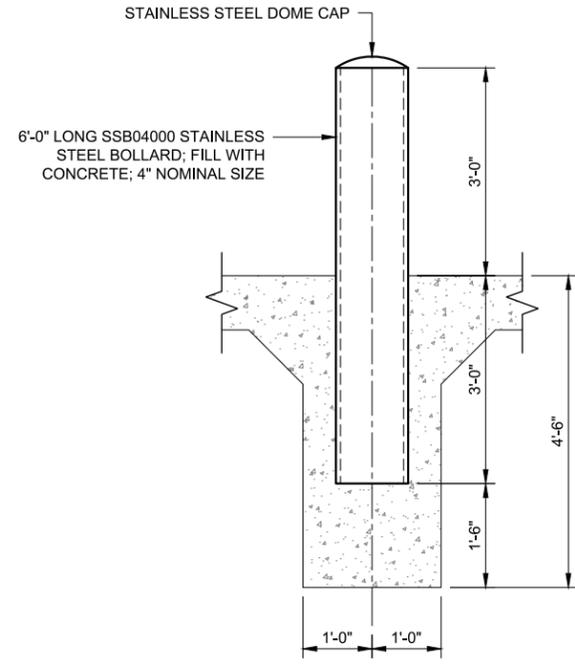
FIVE BELOW - BAXTER
 CIVIL DETAILS

C2.17
 PROJECT #: 2025-12133

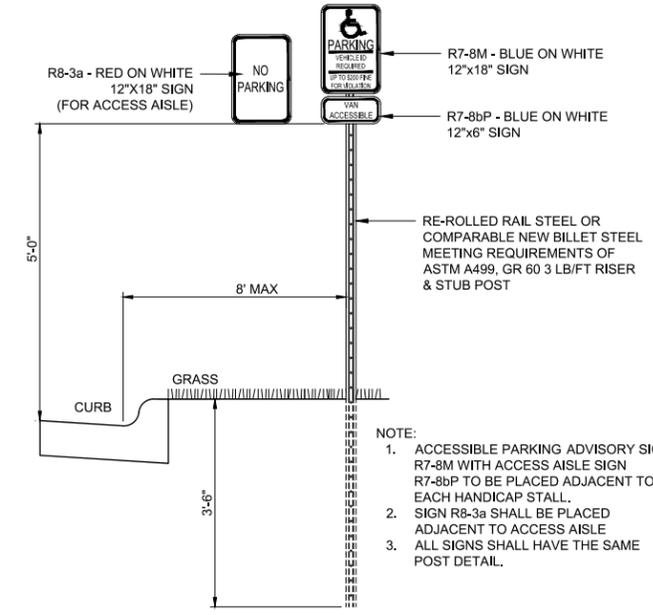


NOTE:
 1. ALL PAINT USED FOR STRIPING SHALL CONFORM TO STANDARD MNDOT REQUIREMENTS AS OUTLINED IN MNDOT SPECIFICATION No. 3591. COLOR SHALL BE BRIGHT WHITE.
 2. ALL PLAN DIMENSIONS MEASURED TO CENTER OF STRIPES OR FACE OF CURB.

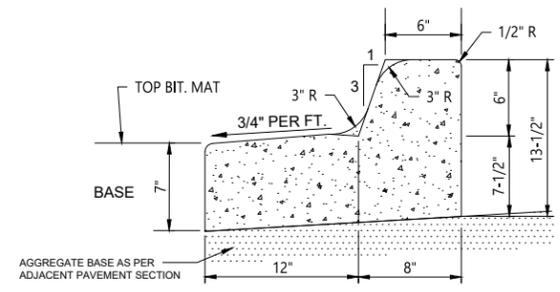
ACCESSIBLE STALL STRIPING DETAIL
 SCALE: NONE



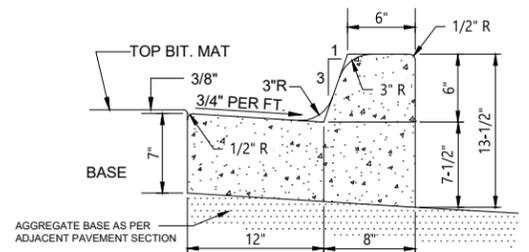
CAST IN PLACE EXTERIOR BOLLARD DETAIL
 SCALE: NONE



TYPICAL VAN ACCESSIBLE PARKING ADVISORY SIGN WITH POST DETAIL
 SCALE: NONE



OUTFALL GUTTER

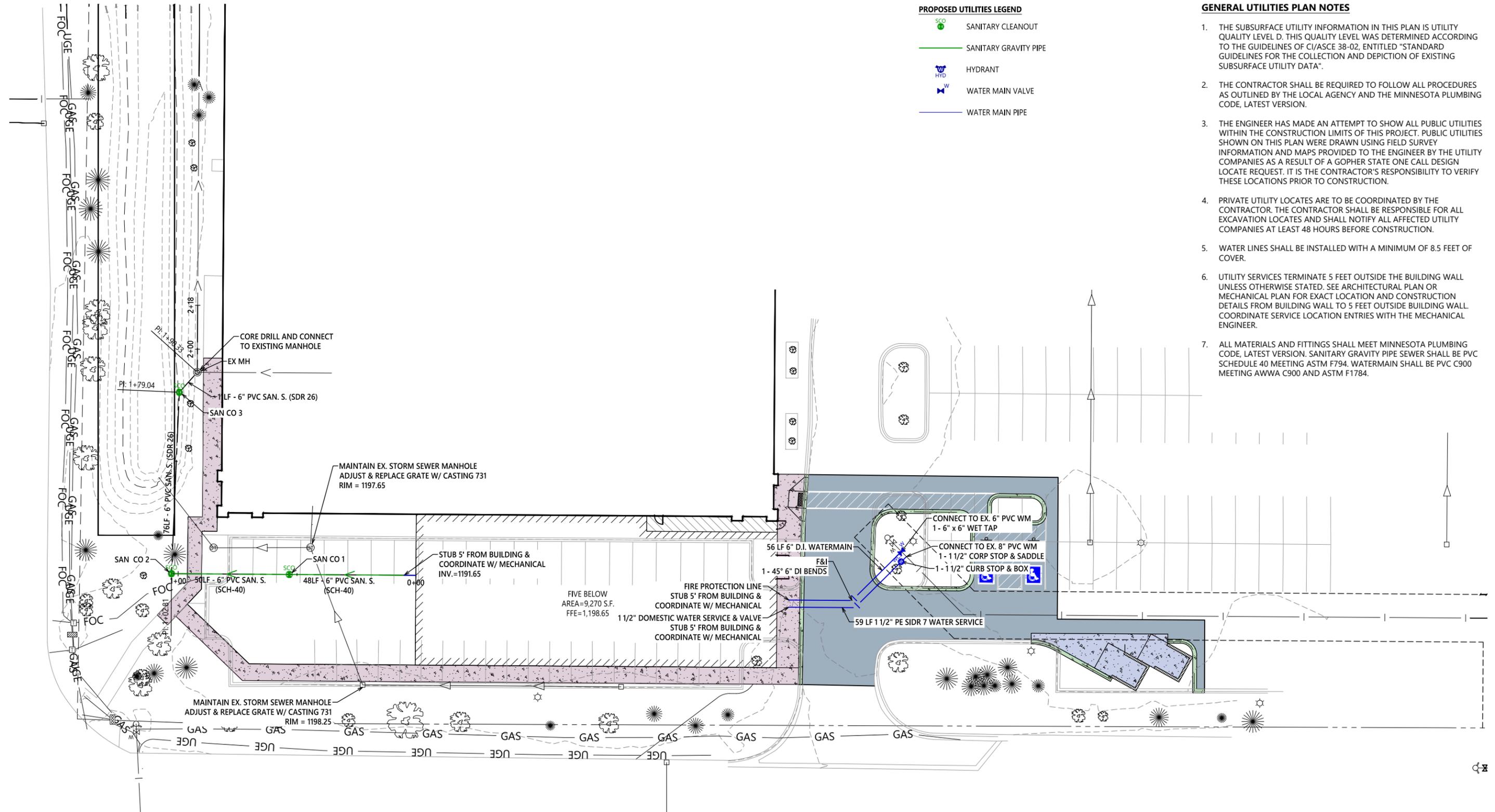


STANDARD INFLOW GUTTER

CONCRETE CURB & GUTTER B612
 SCALE: NONE

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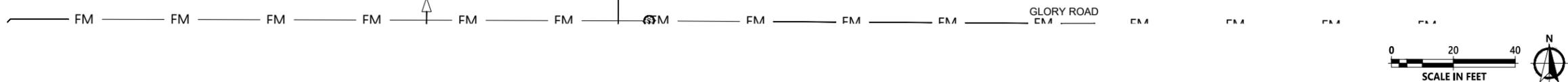


PROPOSED UTILITIES LEGEND

- SANITARY CLEANOUT
- SANITARY GRAVITY PIPE
- HYDRANT
- W WATER MAIN VALVE
- WATER MAIN PIPE

- GENERAL UTILITIES PLAN NOTES**
1. THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA".
 2. THE CONTRACTOR SHALL BE REQUIRED TO FOLLOW ALL PROCEDURES AS OUTLINED BY THE LOCAL AGENCY AND THE MINNESOTA PLUMBING CODE, LATEST VERSION.
 3. THE ENGINEER HAS MADE AN ATTEMPT TO SHOW ALL PUBLIC UTILITIES WITHIN THE CONSTRUCTION LIMITS OF THIS PROJECT. PUBLIC UTILITIES SHOWN ON THIS PLAN WERE DRAWN USING FIELD SURVEY INFORMATION AND MAPS PROVIDED TO THE ENGINEER BY THE UTILITY COMPANIES AS A RESULT OF A GOPHER STATE ONE CALL DESIGN LOCATE REQUEST. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THESE LOCATIONS PRIOR TO CONSTRUCTION.
 4. PRIVATE UTILITY LOCATES ARE TO BE COORDINATED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL EXCAVATION LOCATES AND SHALL NOTIFY ALL AFFECTED UTILITY COMPANIES AT LEAST 48 HOURS BEFORE CONSTRUCTION.
 5. WATER LINES SHALL BE INSTALLED WITH A MINIMUM OF 8.5 FEET OF COVER.
 6. UTILITY SERVICES TERMINATE 5 FEET OUTSIDE THE BUILDING WALL UNLESS OTHERWISE STATED. SEE ARCHITECTURAL PLAN OR MECHANICAL PLAN FOR EXACT LOCATION AND CONSTRUCTION DETAILS FROM BUILDING WALL TO 5 FEET OUTSIDE BUILDING WALL. COORDINATE SERVICE LOCATION ENTRIES WITH THE MECHANICAL ENGINEER.
 7. ALL MATERIALS AND FITTINGS SHALL MEET MINNESOTA PLUMBING CODE, LATEST VERSION. SANITARY GRAVITY PIPE SEWER SHALL BE PVC SCHEDULE 40 MEETING ASTM F794. WATERMAIN SHALL BE PVC C900 MEETING AWWA C900 AND ASTM F1784.

MINNESOTA DEPARTMENT OF TRANSPORTATION



WIDSETH
ARCHITECTS + ENGINEERS + SCIENTISTS + SURVEYORS

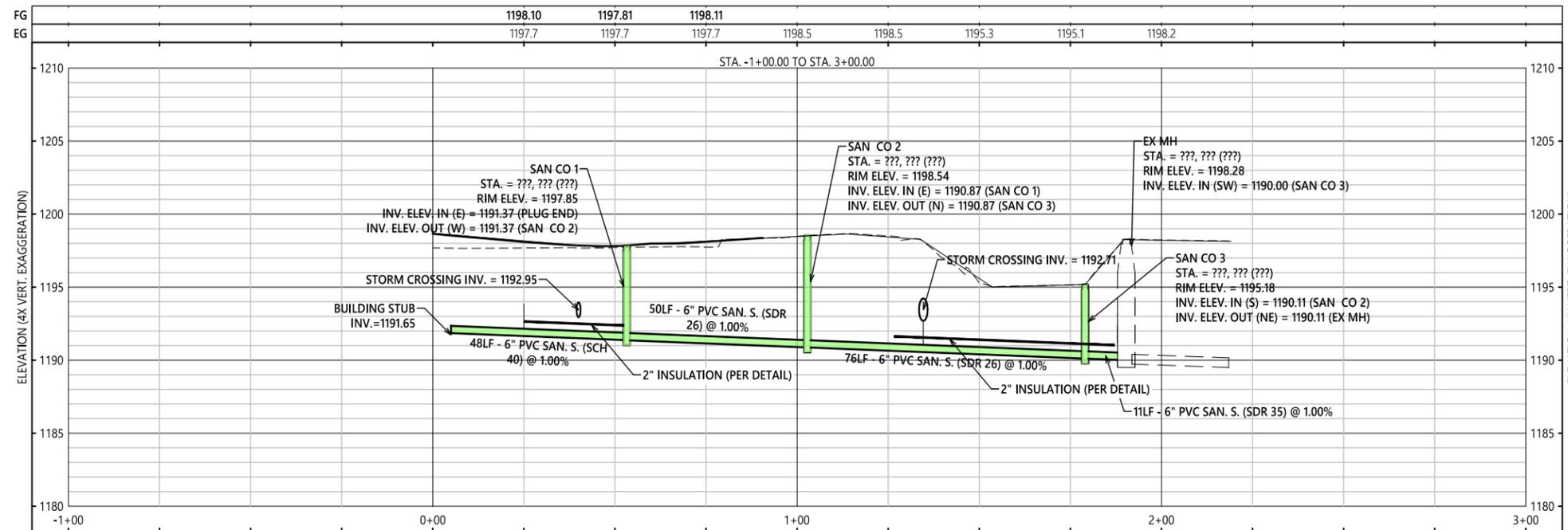
DRAWN BY: MJM
CHECKED BY: ALW
THOMAS M. RIENTS
LIC. NO.: 63111
DATE: 02/09/2026

DATE	REV#	REVISIONS DESCRIPTION

FIVE BELOW - BAXTER
UTILITY PLAN

C5.01
PROJECT #: 2025-12133

SANITARY SEWER CENTERLINE PROFILE



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DRAWN BY: MJM
 CHECKED BY: ALW
 THOMAS M. RIENTS LIC. NO.: 63111 DATE: 02/09/2026

DATE	REV#	REVISIONS DESCRIPTION

FIVE BELOW - BAXTER
 UTILITY PROFILES

C5.02
 PROJECT #: 2025-12133

GENERAL CONSTRUCTION ACTIVITY

Project Name: **FIVE BELOW - BAXTER**

Project Location:
7352 Glory Rs S, Baxter, MN 56425
Crow Wing County
46.344689, -94.247747

Project Description (type of construction, phases, timelines, potential for sediment/pollutant discharge):

This project includes the redevelopment of an existing site for a proposed Five Below Commercial store. The existing site includes a 44-space separate parking lot, regional parking lot, and adjacent storm water basins. The proposed project will include construction of the Five Below building as well as parking lot improvements.

The overall grading and storm water runoff will remain relatively unchanged. Runoff will be directed to existing storm water basins and eventually adjacent wetlands and municipal storm sewer. The total impervious of the site is reduced.

Seeding, mulch, and fertilizer will be used for temporary and permanent stabilization. Flocculants and other chemicals are not anticipated to be used on this project.

Total acres of disturbed area = 0.75 acres
Pre-construction acres of impervious surface = 0.59 acres
Post-construction acres of impervious surface = 0.46 acres
Total new acres of impervious surface = -0.13 acres (net decrease)

RECEIVING WATERS

This project does not include any impaired or special waters within one mile of the project area. However, Mississippi River, Perch Lake, and adjacent wetlands have the potential to receive storm water runoff. This project will not require a permanent storm water management system but will include temporary and permanent methods to minimize erosion and sedimentation.

Wetland areas will be protected with double row silt protection, which may include silt fence, bioroll, or earthen berms. Drainage ditches and construction exits will be established where needed. All disturbed areas will be covered with turf (seed, mulch, and fertilizer).

PROJECT CONTACTS

Owner:
Five Below
Contact Name:
Address:
Phone:
Email:

Site Manager / Contractor's Erosion Control Supervisor:
Name:
Training Dates:
Instructor(s):
Content/Hours:

Contractor:
To be determined
Contact Name:
Address:
Phone:
Email:

BMP Installer:
Name:
Training Dates:
Instructor(s):
Content/Hours:

SWPPP Designer:
Widseth, Inc.
Michael Mordal-Huebsch, PE
13076 1st Street
Becker, MN 55308
763-270-6152
m.mordal-huebsch@widseth.com
(UMN Erosion and Stormwater Management Design of Construction SWPPP certification - expires May 31, 2029)

Other:
MPCA
Dale Sova
218-316-3934
dale.сова@state.mn.us

State Duty Officer:
800-422-0798
651-649-5451

SOILS INFORMATION:

According to the NRCS Web Soil Survey, soils on site primary consist of D70A Barber-Urban Land Complex (0-3% slopes).

SEDIMENT AND EROSION CONTROL PRACTICES:

The contractor/ECS is responsible for the sediment and erosion control practices contained in the NPDES permit. Sediment control practices must be installed on all down gradient perimeters before any up gradient land disturbing activities begin. These practices must remain in place until Permit Termination Conditions have been established.

The timing of installation of sediment control practices may be adjusted to accommodate short-term activities, such as clearing and grubbing or passage of vehicles. Short-term activities must be completed as quickly as possible, and practices must be installed immediately after the activity is completed. However, these practices must be installed before the next precipitation event even if the activity is not complete.

Temporary sediment control devices for this project will primarily include the following:

- Silt fence for primary perimeter control
- Biorolls for secondary perimeter control
- Storm drain inlet protection
- Stabilized construction exits

The contractor/ECS must plan for, and implement, appropriate construction phasing, vegetative buffer strips, horizontal slope grading, and other construction practices that minimize erosion. The location of areas not to be disturbed must be delineated (marked) on site prior to construction.

All disturbed/exposed soil areas must be stabilized as soon as possible to limit soil erosion but in no case later than fourteen days after the construction activity in that portion of the site has temporarily or permanently ceased.

Temporary erosion control shall consist of the following:

- MnDOT seed mixture Oats (O) at 100 lbs per acre
- Fertilizer Type 1 (10-10-20) at 200 lbs per acre
- Mulch type 3 at 2 tons per acre

All storm drain inlets must be protected by appropriate BMPs during construction until all sources with potential for discharging to the inlet have been stabilized. Inlet protection may be removed if a specific safety concern has been identified and the procedure as described in the NPDES permit is followed.

Temporary soil stockpiles must have silt fence or other effective sediment controls and cannot be placed in surface waters, including storm water conveyances such as curb and gutter systems or ditches.

Vehicle tracking of sediment from the construction site must be minimized by BMPs such as stone or wood chip pads, concrete or steel wash racks, or equivalent systems. Street sweeping with collection must be used if such BMPs are not adequate to prevent sediment tracking.

Dewatering related to the construction activity must comply with the NPDES permit. Dewatering discharge that may have turbid or sediment laden discharge must be discharged to a temporary or permanent sedimentation basin on the project site whenever possible, and BMPs must be implemented to prevent water containing sediment or other pollutants from being discharged to surface waters or downstream properties.

Contractor may construct temporary sedimentation basins in accordance with the NPDES permit.

The normal wetted perimeter of any temporary or permanent drainage ditch or swale that drains water from any portion of the construction site, or diverts water around the construction site, must be stabilized within 200 lineal feet from the property edge or from the discharge into any surface water. Stabilization must be completed within 24 hours after connecting to surface water.

Pipe outlets must be provided with temporary or permanent energy dissipation within 24 hours after connecting to a surface water.

Permanent erosion control shall consist of the following:

- Seed mixture Northern Boulevard (formerly 25-131) at 220 lbs per acre
- Fertilizer type 1 (20-10-20) at 350 lbs per acre
- Mulch type 1 at 2 tons per acre

See landscape plans for additional permanent ground treatments.

EROSION CONTROL SUPERVISOR REQUIREMENTS:

The contractor must identify an Erosion Control Supervisor (ECS) who is knowledgeable and experienced in the application of erosion and sediment control Best Management Practices (BMPs). The ECS must work with the contractor to oversee and implement the SWPPP and the installation, inspection, and maintenance of erosion and sediment control BMPs before, during, and after construction.

The contractor/ECS is required to comply with all applicable training requirements of the NPDES permit. The permittee(s) shall ensure that employees are properly trained with certification proof. The contractor/ECS shall develop a chain of responsibility with all operators on the site to ensure that the SWPPP will be implemented and stay in effect until the project site has undergone permit termination.

The contractor/ECS must routinely inspect the entire construction site at least once every seven days during active construction and within 24 hours after a rainfall event greater than 0.5 inch in 24 hours. The contractor/ECS shall take immediate action to eliminate any deficiencies found during these inspections. Inspections, maintenance, and documentation must be in accordance with the NPDES permit. Copies of the inspection records must be submitted to the engineer.

The contractor/ECS must amend the SWPPP as necessary to include additional requirements, such as additional or modified BMPs, designed to correct problems or address situations in accordance with the NPDES permit. The contractor shall have a petroleum release plan and have all necessary materials on hand to implement the plan. All employees shall be trained in implementation of the plan. The MPCA must be informed of any petroleum spills greater than five gallons.

TMDL IMPLEMENTATION PLANS CONTAINING STORM WATER REQUIREMENTS:

No TMDL Implementation Plans are currently available for the project's receiving waters.

LONG TERM MAINTENANCE:

Long term maintenance of the permanent storm water management system will be performed by the owner. Sedimentation basins shall be inspected and maintained annually and cleaned and restored to design grade after one half the storage volume has been filled with sediment. Inlets and outlets shall be monitored and repaired for any erosion or defects that may develop.

CONSTRUCTION PRACTICES TO MINIMIZE STORM WATER AND OTHER POLLUTANT CONTAMINATION:

- Each contractor on site is individually responsible for maintaining a clean and safe work environment.
- Stockpiles should be constructed away from slopes and natural drainage ways and have sediment controls at the base.
- Collected solid waste, sediment, asphalt and concrete millings, floating debris, paper, plastic, fabric, construction demolition debris, and other wastes must be disposed of properly and must comply with MPCA disposal requirements.
- No construction materials can be buried on site.
- Licensed sanitary waste management handler must dispose of sanitary waste.
- Fertilizers must be stored in covered locations.
- Restricted access to chemical storage areas must be provided to prevent vandalism.
- All chemicals must be stored in locked containers when not in use.
- Oil, gasoline, paint, and other hazardous substances must be properly stored, including secondary containment, to prevent spills, leaks, or other discharges.
- Storage and disposal of hazardous waste must be in compliance with MPCA regulations.
- Vehicles must be monitored for leaks and preventative maintenance scheduled.
- Spill kits must be available during equipment fueling and maintenance operations.
- External washing of construction vehicles must be limited to a defined area of the site. Runoff must be contained and waste properly disposed of. No engine degreasing is allowed on site.
- Asphalt substances must be applied according to manufacturers recommendations.
- Spray guns must be cleaned on removable surfaces such as tarpaulins.
- Contractor/ECS must make a spill response plan before the application of any chemical that may be harmful to the environment.
- All spills must be reported immediately. Spill clean-up materials must be available on site. Material shall include but not limited to brooms, mops, rags, gloves, absorbent material, sand, plastic and metal containers. Spills that reach storm sewer conveyance systems connected to public waters must be immediately reported to the State Duty Officer.
- Contractor must control weeds on the entire project site.
- Form release oil must be applied over a pallet covered with absorbent material to collect excess fluid. The absorbent material shall be replaced when saturated.
- Dust control must be provided as conditions warrant.
- If this project is not stabilized before winter conditions, it shall be the contractor's responsibility to ensure sediment does not reach public waters. A written plan of this activity shall be presented to the engineer one month prior before expected project shut-down for the season occurs. This plan shall include dates of BMP employment, duration of BMPs employed, and schedule of subsequent BMPs employed.
- All liquid and solid wastes generated by concrete washout operations must be contained in a leak-proof containment facility or impermeable liner. The liquid and solid wastes must not contact the ground and not runoff from the concrete washout operations area. A sign must be installed adjacent to the washout facility to inform concrete equipment operators to utilize the proper facilities.

SWPPP AMENDMENTS:

1. _____
2. _____
3. _____
4. _____
5. _____

SUMMARY OF OTHER SWPPP REQUIREMENTS IN PROJECT PLANS		
DESCRIPTION	SHEET NAME	SHEET NUMBER
Construction limits and project phasing	GRADING PLAN	C6.01-C6.03
Existing and final grades, direction of flow	EROSION CONTROL PLAN	C7.01
Locations of impervious surfaces	SITE PLAN	C4.01
Standard erosion control construction details	CIVIL DETAILS	C2.11-C2.16
Locations and types of all temp. and perm. erosion prevention and sediment control BMPs	EROSION CONTROL PLAN	C7.01
Estimated quantities of erosion control items	EROSION CONTROL PLAN	C7.01



DRAWN BY: ###
CHECKED BY: ###
THOMAS M. RIENTS
LIC. NO.: 6311
DATE: 02/09/2026

DATE	REV#	REVISIONS DESCRIPTION

FIVE BELOW - BAXTER
SWPPP NARRATIVE

C6.01
PROJECT #: 2025-12133

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SPECIAL WATERS SEARCH

WEB SOIL SURVEY

Custom Soil Resource Report
Soil Map

Soil Map may not be valid at this scale.

Map Scale: 1:1,490 if printed on A landscape (11" x 8.5") sheet.

Map projection: Web Mercator Corner coordinates: WGS84 Edge tics: UTM Zone 15N WGS84

Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
D70A	Barber-Urban land complex, 0 to 3 percent slopes	8.0	100.0%
Totals for Area of Interest		8.0	100.0%

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GENERAL GRADING PLAN NOTES

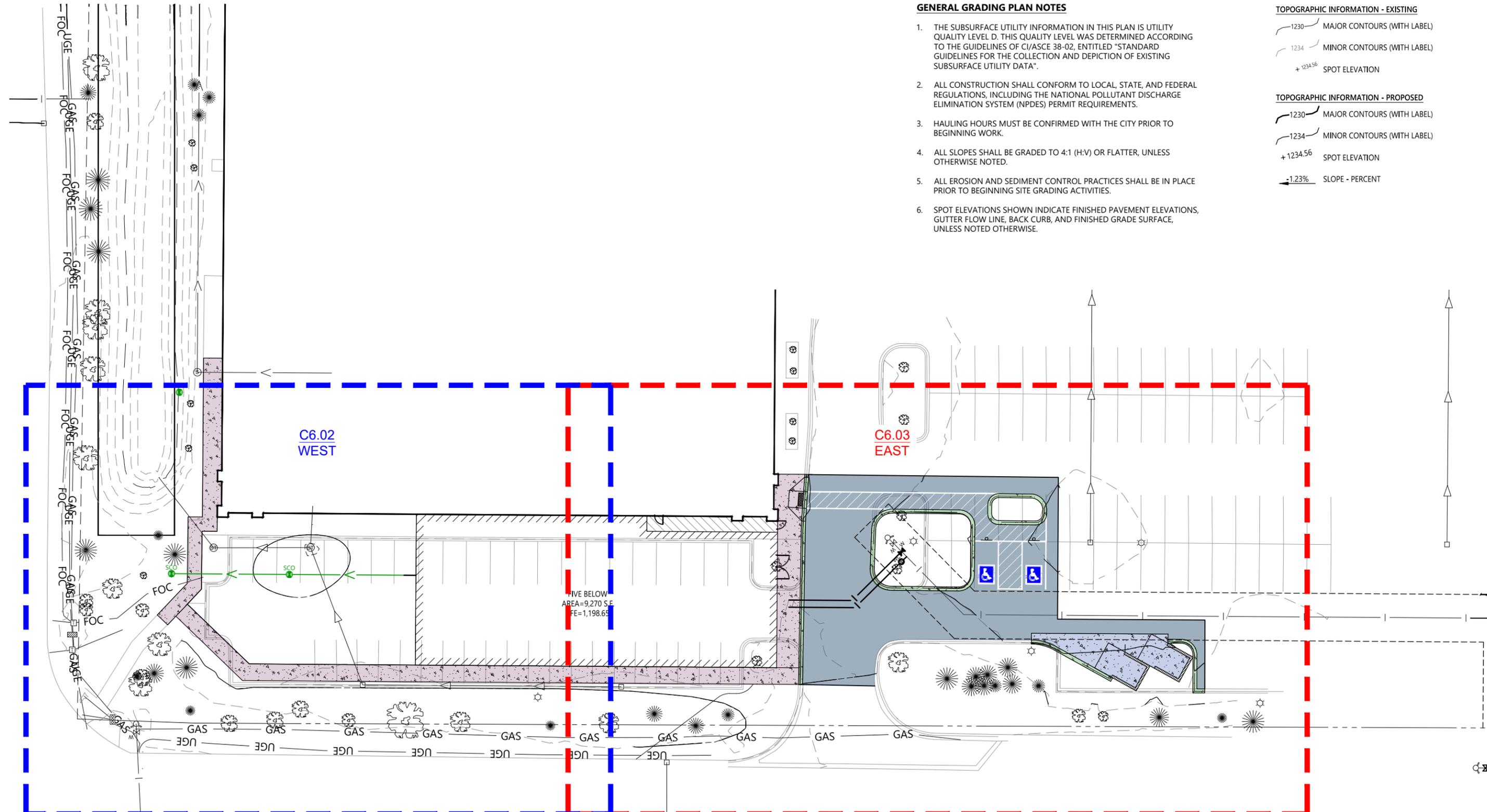
1. THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA".
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3. HAULING HOURS MUST BE CONFIRMED WITH THE CITY PRIOR TO BEGINNING WORK.
4. ALL SLOPES SHALL BE GRADED TO 4:1 (H:V) OR FLATTER, UNLESS OTHERWISE NOTED.
5. ALL EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE IN PLACE PRIOR TO BEGINNING SITE GRADING ACTIVITIES.
6. SPOT ELEVATIONS SHOWN INDICATE FINISHED PAVEMENT ELEVATIONS, GUTTER FLOW LINE, BACK CURB, AND FINISHED GRADE SURFACE, UNLESS NOTED OTHERWISE.

TOPOGRAPHIC INFORMATION - EXISTING

- 1230 MAJOR CONTOURS (WITH LABEL)
- 1234 MINOR CONTOURS (WITH LABEL)
- + 1234.56 SPOT ELEVATION

TOPOGRAPHIC INFORMATION - PROPOSED

- 1230 MAJOR CONTOURS (WITH LABEL)
- 1234 MINOR CONTOURS (WITH LABEL)
- + 1234.56 SPOT ELEVATION
- 1.23% SLOPE - PERCENT



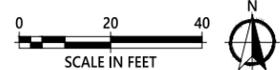
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 CHECKED BY: ALW
 THOMAS M. RIENTS LIC. NO.: 63111 DATE: 02/09/2026

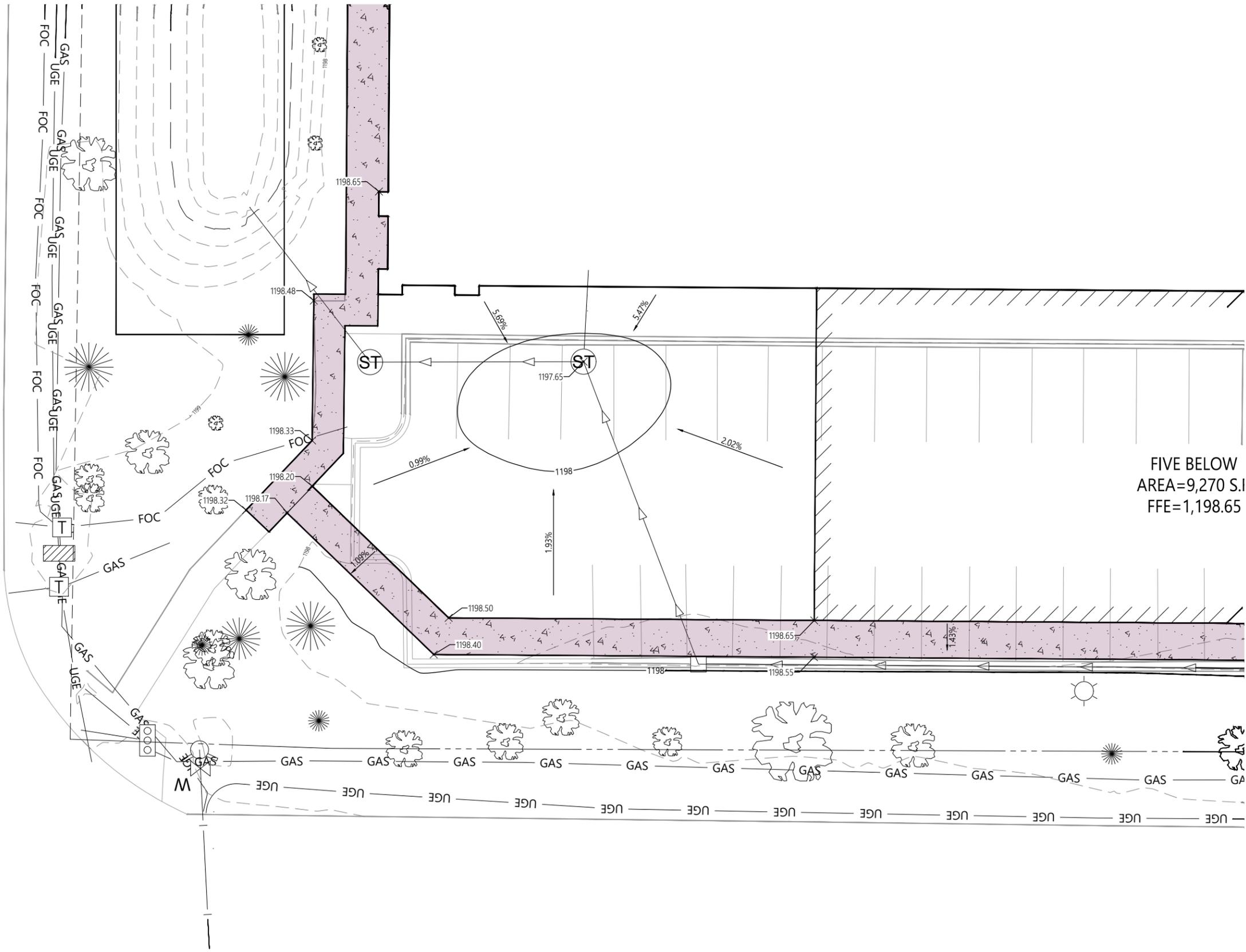
DATE	REV#	REVISIONS DESCRIPTION

FIVE BELOW - BAXTER
 GRADING PLAN OVERALL



C7.01
 PROJECT #: 2025-12133

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FIVE BELOW
AREA=9,270 S.I
FFE=1,198.65

TOPOGRAPHIC INFORMATION - EXISTING

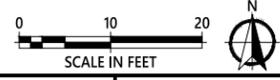
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DATE	REV#	REVISIONS DESCRIPTION

FIVE BELOW - BAXTER
 GRADING PLAN WEST

C7.02
 PROJECT #: 2025-12133

GENERAL EROSION CONTROL PLAN NOTES

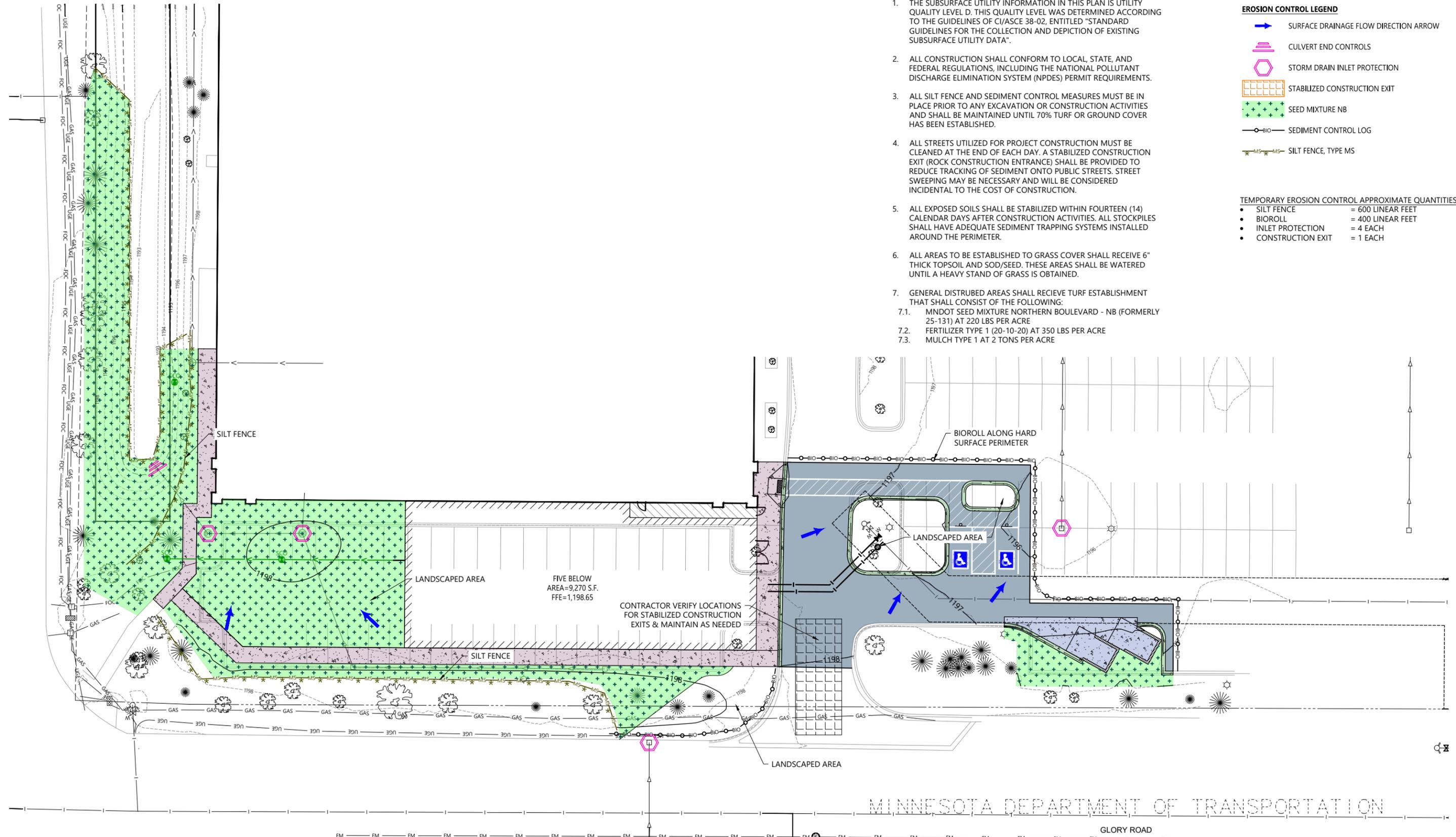
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2. ALL CONSTRUCTION SHALL CONFORM TO LOCAL, STATE, AND FEDERAL REGULATIONS, INCLUDING THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS.
3. ALL SILT FENCE AND SEDIMENT CONTROL MEASURES MUST BE IN PLACE PRIOR TO ANY EXCAVATION OR CONSTRUCTION ACTIVITIES AND SHALL BE MAINTAINED UNTIL 70% TURF OR GROUND COVER HAS BEEN ESTABLISHED.
4. ALL STREETS UTILIZED FOR PROJECT CONSTRUCTION MUST BE CLEANED AT THE END OF EACH DAY. A STABILIZED CONSTRUCTION EXIT (ROCK CONSTRUCTION ENTRANCE) SHALL BE PROVIDED TO REDUCE TRACKING OF SEDIMENT ONTO PUBLIC STREETS. STREET SWEEPING MAY BE NECESSARY AND WILL BE CONSIDERED INCIDENTAL TO THE COST OF CONSTRUCTION.
5. ALL EXPOSED SOILS SHALL BE STABILIZED WITHIN FOURTEEN (14) CALENDAR DAYS AFTER CONSTRUCTION ACTIVITIES. ALL STOCKPILES SHALL HAVE ADEQUATE SEDIMENT TRAPPING SYSTEMS INSTALLED AROUND THE PERIMETER.
6. ALL AREAS TO BE ESTABLISHED TO GRASS COVER SHALL RECEIVE 6" THICK TOPSOIL AND SOD/SEED. THESE AREAS SHALL BE WATERED UNTIL A HEAVY STAND OF GRASS IS OBTAINED.
7. GENERAL DISTURBED AREAS SHALL RECEIVE TURF ESTABLISHMENT THAT SHALL CONSIST OF THE FOLLOWING:
 - 7.1. MNDOT SEED MIXTURE NORTHERN BOULEVARD - NB (FORMERLY 25-131) AT 220 LBS PER ACRE
 - 7.2. FERTILIZER TYPE 1 (20-10-20) AT 350 LBS PER ACRE
 - 7.3. MULCH TYPE 1 AT 2 TONS PER ACRE

EROSION CONTROL LEGEND

-  SURFACE DRAINAGE FLOW DIRECTION ARROW
-  CULVERT END CONTROLS
-  STORM DRAIN INLET PROTECTION
-  STABILIZED CONSTRUCTION EXIT
-  SEED MIXTURE NB
-  SEDIMENT CONTROL LOG
-  SILT FENCE, TYPE MS

TEMPORARY EROSION CONTROL APPROXIMATE QUANTITIES:

- SILT FENCE = 600 LINEAR FEET
- BIOROLL = 400 LINEAR FEET
- INLET PROTECTION = 4 EACH
- CONSTRUCTION EXIT = 1 EACH



MINNESOTA DEPARTMENT OF TRANSPORTATION

GLORY ROAD



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DRAWN BY: ####
 CHECKED BY: *Thomas Rients*
 THOMAS M. RIENTS LIC. NO.: 63111 DATE: 02/09/2026

DATE	REV#	REVISIONS DESCRIPTION

FIVE BELOW - BAXTER
 EROSION CONTROL PLAN

C8.01
 PROJECT #: 2025-12133