

**Motorized Transportation (in order of priority) – 0-10 Year Capital Improvements**

<b>Facility Name</b>	<b>Description</b>	<b>Benefits</b>	<b>Cost Estimate</b>
Foundry Lane Phase 1 & Phase 2; Round-about at intersection	Urban Expressway: New road north from intersection of Foundry Lane/SR 4 to SR 49 – Provides connection between SR 4 and SR 49 around the 4/49 high volume and congested intersection and opens 133± acres to new mixed-use development. Access from SR 4 (25 years in the making) scheduled before the California Transportation Commission June, 2025. Established per joint study and community effort in the <i>Angels Camp State Route 4 and State Route 49 Gateway Corridor Study, Angels Camp, CA [Caltrans, Calaveras Council of Governments, City of Angels] Jan. 2016</i>	Circulation Economic Development Safety (Emergency Evacuation)	\$8,741,500
Greenhorn Creek (GHC) to Finnegan Lane Connector	First portion of the Greenhorn Creek Road South Extension to SR 49. First phase is 500± feet – See Also Angels Creek Trail.	Safety (Emergency Evacuation) Economic Development Some circulation	\$350,000
Blair Mine Road in GHC to Stockton Road connector	Arterial Urban/Collector/Minor/Arterial Rural. Connections from Blair Mine Road outlet (between GHC Lots 443, 444) to Stockton Road northwest to GHC subdivision	Safety (Emergency Evacuation)	\$450,000
Stockton Road widening	Improve to collector standards (See also bike/pedestrian facilities proposed) from SR 49/Savemart Shopping Center to Angel Oaks/Greenhorn Creek Road	Circulation	\$2,704,170
Tryon Road to Greenstone Mine/SR 49 Connector	Connection from Tryon Road to SR 49 via on-site roadways from Tryon Road off Vallecito near Fire Station #1 to allow for emergency evacuation around SR 49 Bridge over Angels Creek and to SR 49 in the vicinity of the PG&E substation adjacent to SR 49 and south of the Annex during extreme flooding	Safety (Emergency Evacuation)	\$1,250,000
Murphys Grade Road and SR 49 Intersection Improvements	Improve intersection to allow for large truck turns at the intersection to allow for Business Attraction and Expansion (BAE) parcels development on Murphys Grade Road. Also improve visibility to east/west traffic	Economic Development (allows for truck turns) Circulation	\$952,500
Sierra Avenue (GHC) to Tuolumne Avenue Connector	Arterial Urban/Collector/Minor/Arterial Rural Angel Oaks/Greenhorn Creek Road at Gateway Park along dirt Sierra Avenue to Tuolumne Avenue	Safety (Emergency Evacuation)	\$250,000
Road “A”	Construct new, 2-lane roadway from near the terminus of Clifton Lane, around Country Lane Estates, to Dogtown Road.	Circulation	\$2,400,000
Return to Historic Downtown Loop (North 49)	City of Angels, Caltrans, Calaveras Council of Governments. May 2017. <i>Angels Camp Main Street Plan, Final</i>	Circulation Economic Development Safety	\$500,000

	Construct new street entrance to Utica Park through parcel purchased south of former Napa Parking Lot and make Sam's Way a one-way exit		
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**Motorized Transportation – 11-20 Year Capital Improvements**

Facility Name	Description	Benefits	Cost Estimate
(In order)			
1. GHC Southwest Connector	GHC to SR 49		\$20,043,000
2. Tryon Southeast Connector	South SR 49 to Vallecito Rd/East SR 4. Includes connection to Fairgrounds.		\$75,000,000
3. Demarest Extension	Connect Demarest to Stockton Road (behind Savemart Shopping Center)		\$619,900
4. Foundry Lane Phase III (Angel Oaks/Greenhorn Creek Road Extension)	Extend Angel Oaks/Greenhorn Creek Road across SR 4 and up to intersect with Foundry Lane. Will require revised encroachment from Caltrans.		\$12,213,000
(Not in order)			
Kurt Drive Extension	Stelte Park to Murphys Grade Road		\$5,473,500
Purdy Road Extension to Kurt Drive	Purdy Road to Kurt Drive Extension (See Kurt Drive Extension)		\$217,000
Rolleri Bypass /Murphys Grade Road Intersection Improvements	Improve intersection at Rolleri Bypass and Murphys Grade Road		\$709,500
Moxie Connector	Gold Cliff Road to McCauley Ranch Road at location of existing emergency access		\$1,206,000
Assess Booster Way Bridge	Assess Booster Way bridge for necessity to improve and/or widen. Cost includes widening Booster Way.		\$1,656,000
Murphys Grade Road widen, realign	Widen and realign from SR 49 to French Gulch Road and upgrade to minimum standards		\$15,000,000

**Non-Motorized Transportation (in order of priority) – 0-10 Year Capital Improvements**

<b>Facility Name</b>	<b>Description</b>	<b>Benefits</b>	<b>Cost Estimate</b>
Angels Creek Trail Phase 1 and Phase 2	<p>Phase 1 – Class III facility from SR 49 to Finnegan Lane at Finnegan Court. Class 1 or Class II facility from Finnegan Lane to Greenhorn Creek dead-end</p> <p>Phase 2 – SR 49 Crosswalk, Class 1 Facility from SR 49, to new pedestrian bridge over Angels Creek; through City Parking lot along Angels Creek through Tryon Park and continuing along Angels Creek to the Kurt Drive/Vallecito Road bridge and intersection</p>	<p>Circulation Safety Economic Development</p>	
Stockton Road (SR 49 to Angel Oaks/GHC Road)	Bike/Ped (combination Class I, II, III) along Stockton Road from SR 49 (Savemart Shopping Center) to Greenhorn Creek Road	<p>Circulation Safety</p>	
North Main Street Plan Improvements	<p>Calaveras Council of Governments, City of Angels, Caltrans. May, 2020. <i>Angels Camp North Main Street Plan, Final.</i></p> <p>Implement:</p> <ol style="list-style-type: none"> <li>1. Copello SR 49 Crosswalk</li> <li>2. Bicycle Pedestrian Path along SR 49</li> <li>3. Short-term projects</li> </ol>	<p>Safety (School) Circulation Economic Development</p>	
Stanislaus Avenue	Sidewalks from Twain Harte Elementary School to Gold Cliff Road (Complete Gap from School to Gold Cliff, one side)	Safety (School)	
San Joaquin Avenue	Sidewalks Bighorn Mobilehome Park to Twain Harte Elementary (one side)	Safety (School)	
Demarest	Sidewalks from Bighorn MHP to Stockton Road along existing dirt road/future road behind Savemart to Stockton Road	Circulation	
Angels Camp Main Street Plan Crosswalk/Intersection Improvements	<p>City of Angels, Caltrans, Calaveras Council of Governments. May 2017. <i>Angels Camp Main Street Plan, Final</i></p> <ol style="list-style-type: none"> <li>1. Implement intersection improvements at the Rasberry Intersection per the plan (bulbouts, pedestrian signalization, re-define crosswalks)</li> <li>2. Implement intersection improvements at the Vallecito SR 49, including gateway improvements</li> <li>3. Mark Twain Intersection Improvements (Crosswalks)</li> <li>4. Intersection improvements from Crusco's alley (1240 S. Main) to 1239 S. Main</li> </ol>	<p>Circulation Safety Economic Development</p>	
Monte Verda/SR 49 Intersection and crosswalks	Improve safety and pedestrian crossings over SR 49 and across Monte Verda at this intersection	Safety	

**PARKING Transportation – 0-10 Years**

<b>Facility Name</b>	<b>Description</b>	<b>Benefits</b>	<b>Cost Estimate</b>
Expand Raspberry (Slate Circle) Parking Lot	City of Angels, Caltrans, Calaveras Council of Governments. May 2017. <i>Angels Camp Main Street Plan, Final</i>		
Construct 2-story Napa Parking Lot			
Construct parking lot adjacent to new Utica Park ingress	City of Angels, Caltrans, Calaveras Council of Governments. May 2017. <i>Angels Camp Main Street Plan, Final</i>		
Increase parking lot safety	Add lighting (Vallecito City Lot)		

**Recreational Non-Motorized Transportation – 0-10 Years**

<b>Facility Name</b>	<b>Description</b>	<b>Benefits</b>	<b>Cost Estimate</b>
Angels Creek Trail Phase 3 and Phase 4	Phase 3: Finnegan Lane to City Wastewater Treatment Plant, including bridge across Angels Creek Phase 4: City Wastewater Treatment Plant to New Melones Reservoir		
Bike/Pedestrian Bridge Finnegan Lane to Annex	Bike/Pedestrian bridge across Angels Creek to Annex (vicinity of Mayo Street)		
Utica Park to Mark Twain	Class I or Class II facility from Utica Park through property adjacent to Utica/Lightner Park properties to Mark Twain		
Bush Street – Utica Park to Finnegan Lane	Class III from Utica Park, along Bush Street to Finnegan Lane		

**Transit Transportation – 0-10 Years**

<b>Facility Name</b>	<b>Description</b>	<b>Benefits</b>	<b>Cost Estimate</b>
Dial-A-Ride	Increase availability of dial-a-ride for Angels Camp seniors, disabled, without cars		
North Main Street Plan Shelter/Stop	Calaveras Council of Governments, City of Angels, Caltrans. May, 2020. <i>Angels Camp North Main Street Plan, Final.</i>  Copello Transit Stop (See Transit)		

Main Street Plan

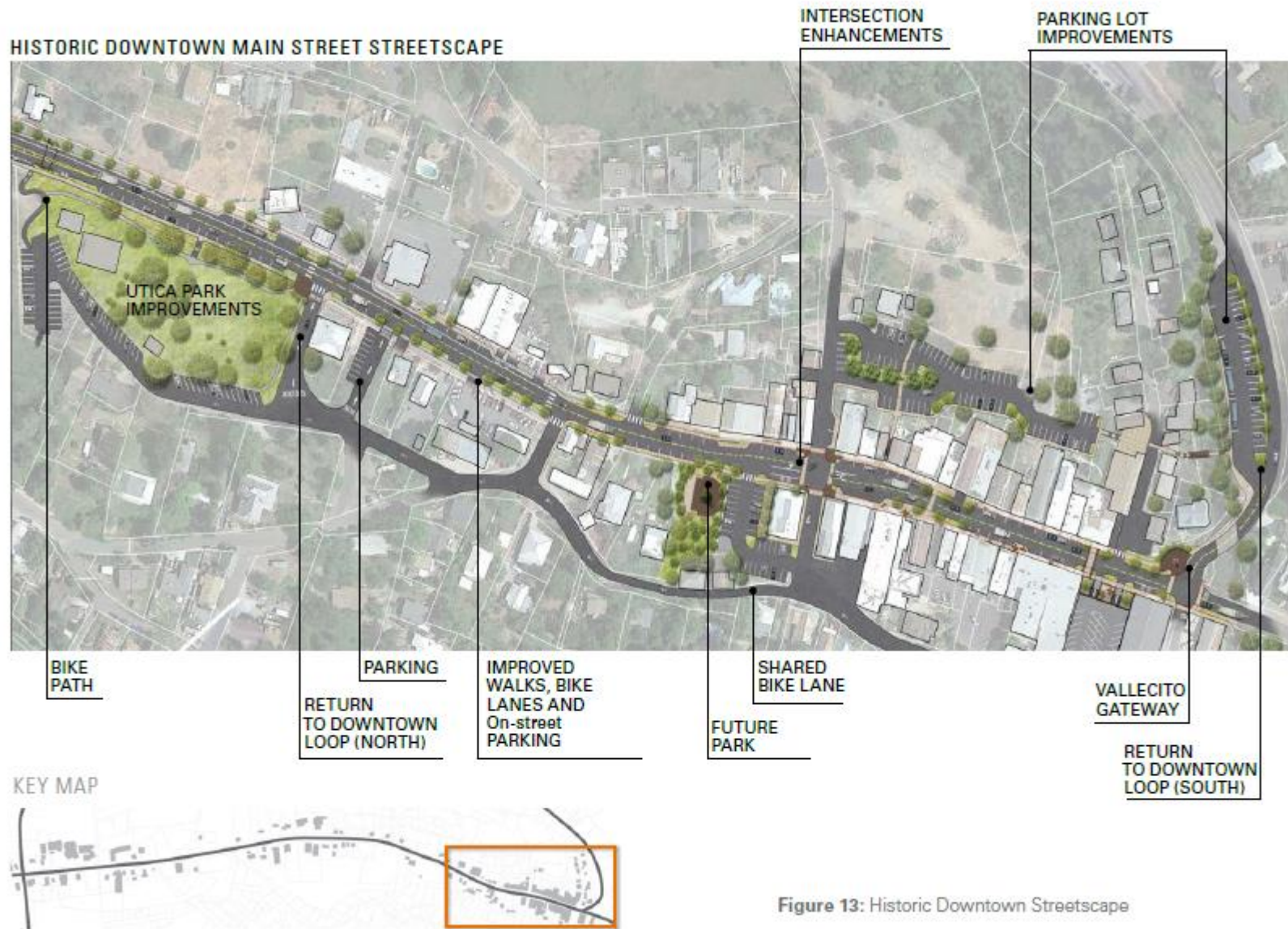


Figure 13: Historic Downtown Streetscape



# UTICA PARK

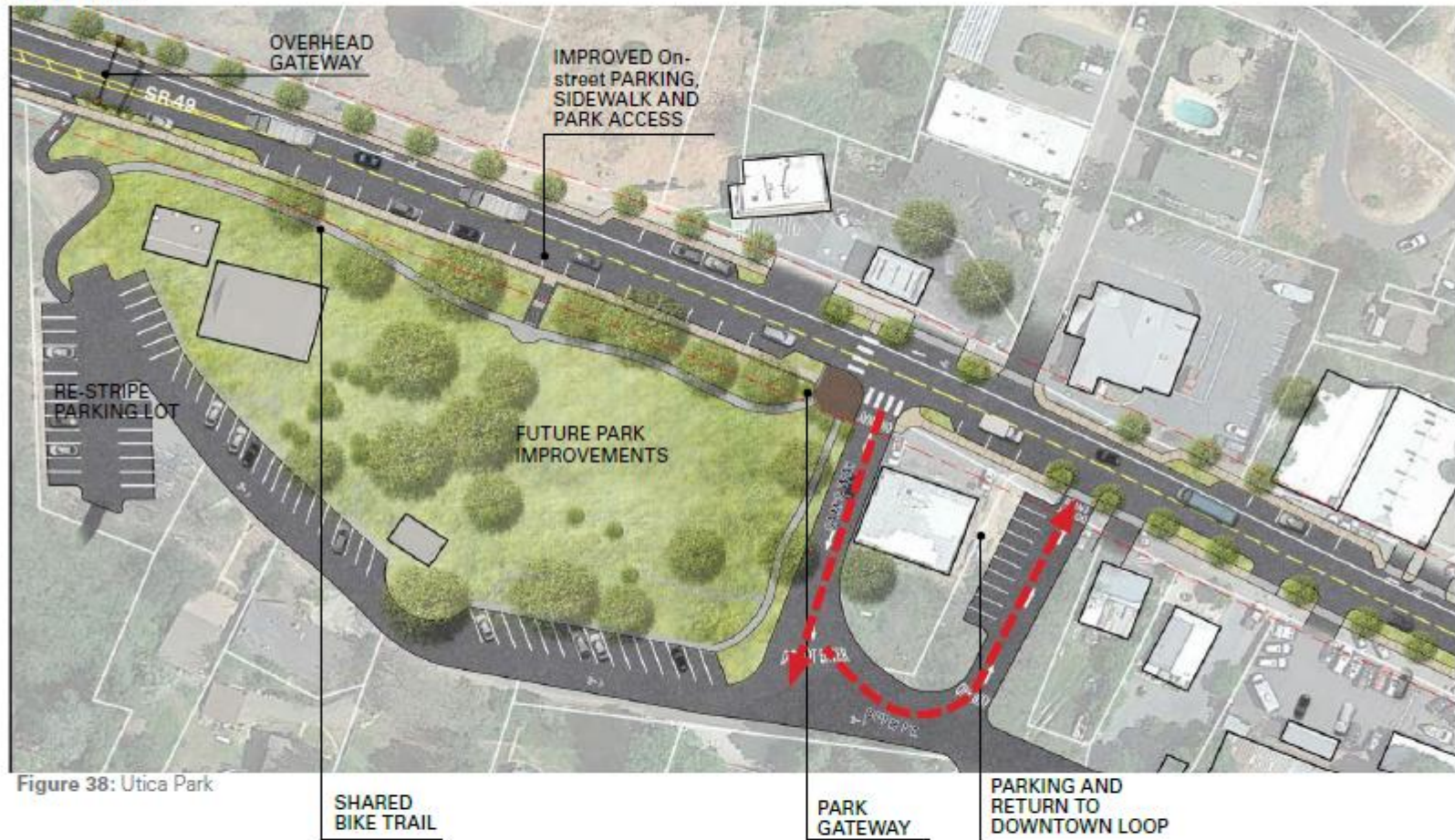


Figure 38: Utica Park





### **RASBERRY STREET INTERSECTION**

Improvements to Raspberry Intersection will include bump outs to create smaller intersections for pedestrian safety and traffic calming. The existing roadway and lanes will remain as currently aligned. Bump outs will extend into the space currently used for parking and should maintain the sight distance triangle. Visibility will be improved by bringing the car forward to increase the visibility of pedestrians and increase the view around buildings.



**Figure 19:** Raspberry Intersection Perspective





## STREET CONCEPTS

### 4.1.5 INTERSECTION ENHANCEMENTS: VALLECITO INTERSECTION

The City has conducted numerous studies on the intersection at Vallecito and SR 49. The Plan identifies alternatives for pedestrian and signage improvements at Vallecito to provide low- cost, short-term solutions. Long range planning will require detailed land use planning and transportation engineering analysis to address future development and transportation demands at the south end of the City.

The Plan looks to improve pedestrian conditions at the Vallecito Intersection in order to visually extend the downtown streetscape and improve the gateway at the south entry of Angels. The proposed pedestrian improvements, including increased pedestrian area in the northeast corner, intend to improve pedestrian safety conditions by shortening the crosswalk length. It would also have a traffic calming benefit by providing a space for landscaping or entry feature that would give drivers arriving from the south a stronger indication that they are entering an area with a high potential for pedestrian activity. The signage improvements, particularly in the southeast corner, would improve driver sight distance conditions for turns to/from Vallecito Road.

#### RECOMMENDATIONS

- **Provide Pedestrian Improvements to the Vallecito Intersection:** A reduction and reorganization of the Vallecito Intersection could improve the safety of this intersection for both vehicles and pedestrians.
- **Develop a Gateway Plaza:** Increase the available public space and create a nice entry to downtown by designing a plaza space with benches, interpretive signage, and a sculpture.
- **Extend Sidewalks from Downtown:** Ensure pedestrian connectivity and a positive walking experience by extending sidewalks, landscape and site furnishings from the Main Street.

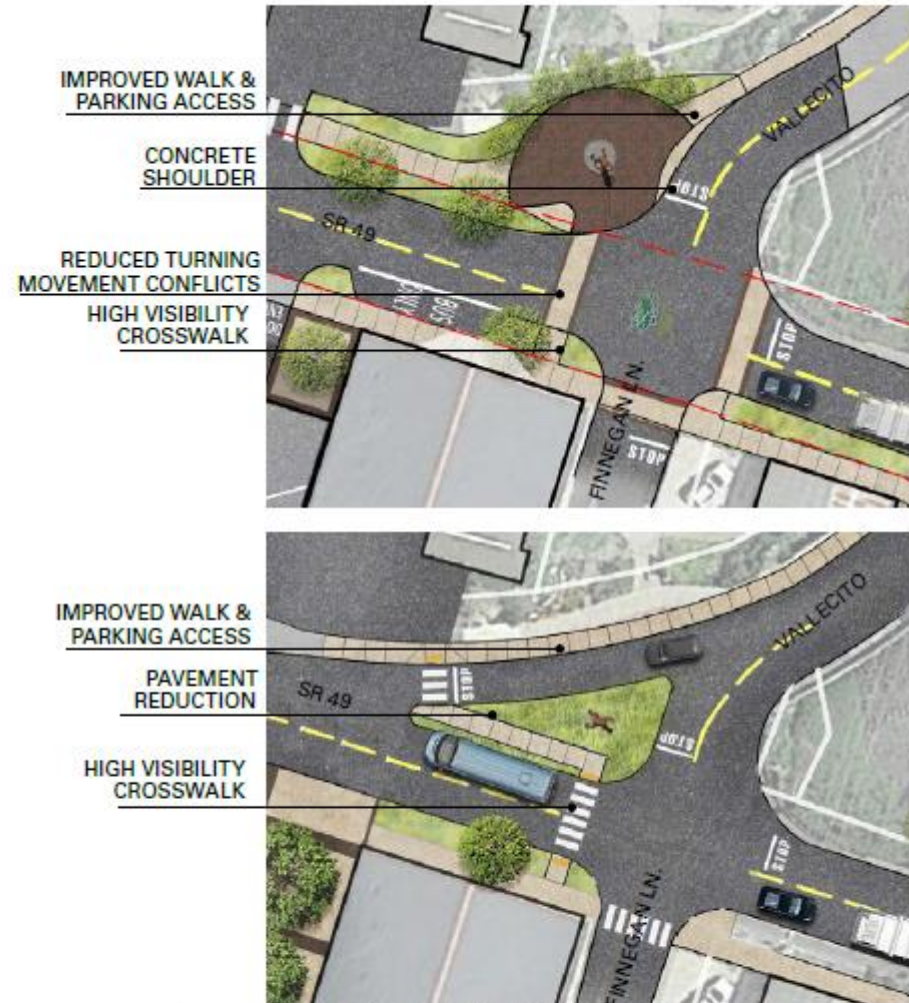
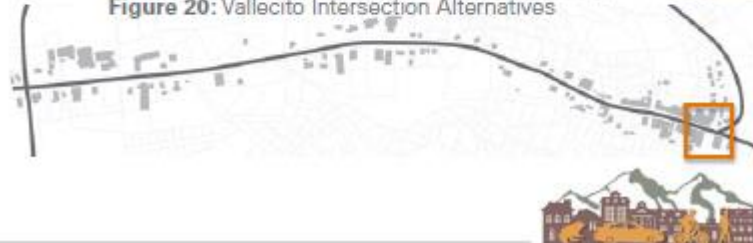


Figure 20: Vallecito Intersection Alternatives







**Figure 21:** Conceptual Illustrative Perspectives of the Vallecito Intersection Alternatives

### VALLECITO INTERSECTION ILLUSTRATIVE

The Vallecito intersection improvements reduce the amount of pavement and enhance pedestrian and traffic safety conditions. Improvements to the Vallecito intersection will influence other design alternative recommendations. The Vallecito Parking Area will have better pedestrian connections to downtown. It is suggested that improvements at this location are coordinated with planning for the Angels Creek Trail. Future improvements to Utica Hotel should coordinate their future parking entrance with existing intersection movement to minimize conflicts.

### VALLECITO INTERSECTION KEY FINDINGS

Key findings at the Vallecito intersection are as follows:

- Pedestrian improvements can be installed to improve pedestrian safety conditions (and provide an entry feature).
- There is an opportunity to “bulb out” the northeast corner to provide a large pedestrian plaza area and “T-up” the Vallecito approach, in order to improve pedestrian and traffic safety conditions and to provide a better experience for those entering Downtown from Vallecito Road. This option is evaluated in Appendix D. However, considering the constraints associated with accommodating large truck turns, detailed engineering analysis beyond the scope of this plan would need to be conducted to finalize the design of these improvements.
- The intersection stop control configuration could potentially be modified to improve traffic operations and safety conditions. Detailed engineering analysis beyond the scope of this plan would need to be conducted to finalize the design of these improvements.

For additional traffic data and truck turning studies regarding these options, refer to Appendix F.

## PARKING LOT SUMMARY



Figure 26: Parking Lot Summary



## PARKING CONCEPTS

### 4.3.2 RASBERRY PARKING LOT

The Raspberry Parking Lot is under-utilized, considering this area is less than a 2 minute walk to downtown. The parking area is challenged by steep grade changes and lack of visibility from Main Street, which results in petty crime and inefficient parking.

Future improvements should focus on improving visibility, safety and access as these efforts will make Raspberry a desirable parking area for Main Street. Raspberry Lot should be the primary parking area in downtown for visitor parking with increased wayfinding and signage from SR 49.

Potential future development of the vacant land east of the Raspberry Parking Area could provide a parking expansion area. This area could be temporary expansion of parking until the parcel is developed by constructing a gravel or asphalt lot that is leased by the City. This area could also provide long-term parking through a public/private partnership with the City, when the landowner is interested in developing the land. Future long-term parking could be surface parking or a parking garage.

### RECOMMENDATIONS

- **Restripe parking lot layout:** The existing paved areas can be realigned to increase the number of parking spaces.
- **Improve Pedestrian Access to Downtown:** Stair access from the upper lot, high visibility pedestrian crossings on the drive aisle, and improved stair connection between buildings will define a pedestrian path. Additional access could be provided at the south end of the parking area with a stair connection to Birds Way to improve access to the south side of Main Street and the Future Angels Creek Trail.
- **Improve Security by Implementing Crime Prevention Through Environmental Design (CEPTD) Strategies:** Security lighting and increased landscape along the backs of buildings will increase the sense of ownership and visibility to increase safety.

- **Improve Parking Signage Visibility from SR 49:** The Raspberry Parking Area should be clearly signed for Parking with the appropriate spacing for drivers to make a decision about turning onto Raspberry from SR 49.
- **Add Wayfinding Signage:** The Parking Area should include return to downtown signs, a downtown map and wayfinding kiosk to orient pedestrians to access downtown. These should be located at the existing stair access and potential Birds Way stair access.





## RASBERRY PARKING LOT ALTERNATIVES



ADD AND  
IMPROVE STAIR  
CONNECTION TO  
DOWNTOWN

MAP OF  
DOWNTOWN  
ENHANCE  
WALK, LIGHTING  
AND LANDSCAPE

**EXISTING PARKING:**  
56 Spaces

**PROPOSED PARKING:**  
66 Spaces

**NET PARKING:**  
+ 10 Spaces



POTENTIAL  
FUTURE  
DEVELOPMENT  
AND PARKING

**EXISTING PARKING:**  
56 Spaces

**PROPOSED PARKING:**  
119 Spaces

**NET PARKING:**  
+ 63 Spaces

Figure 28: Raspberry Parking Lot

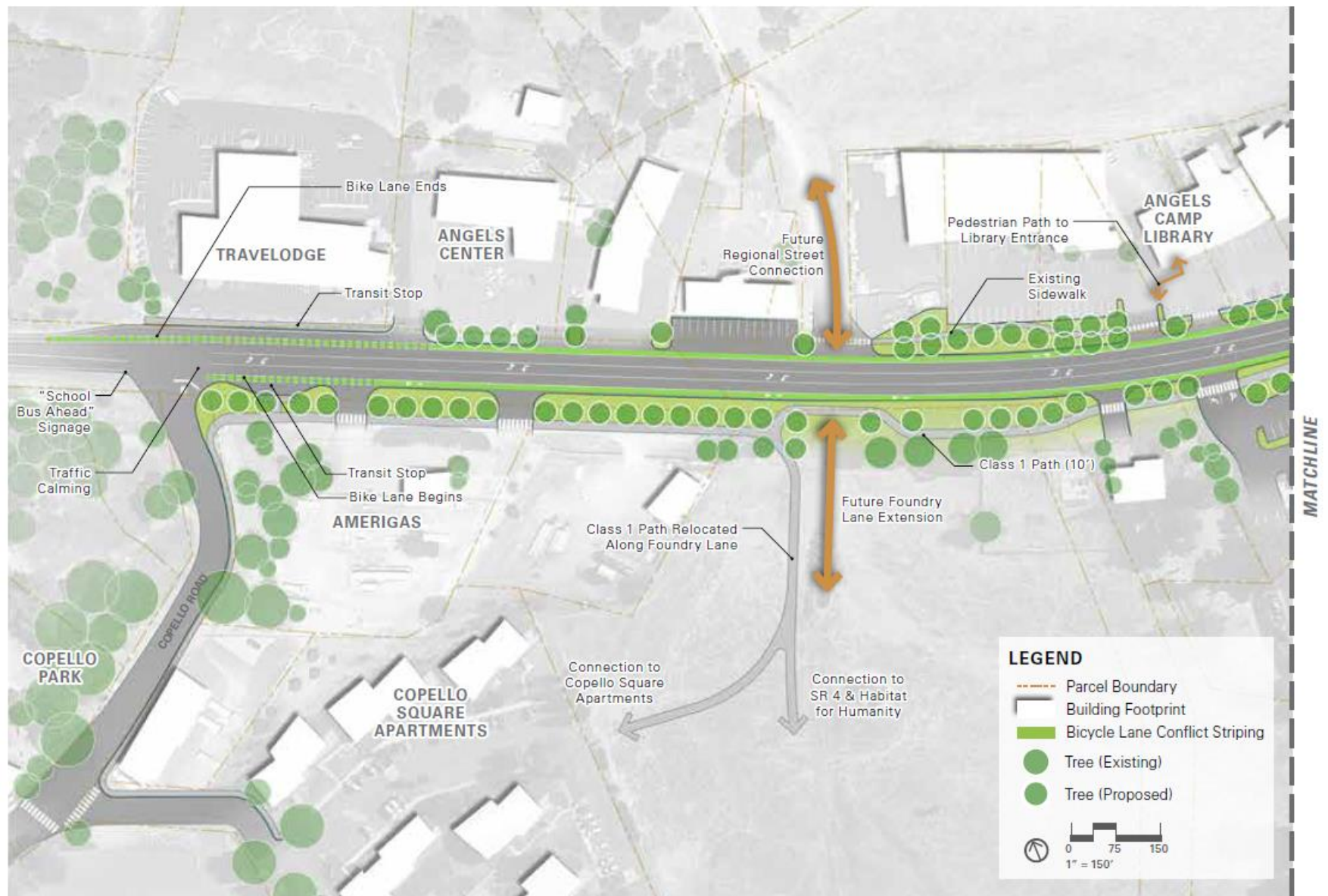
North Main Street Plan



# 5 –CONCEPTUAL DESIGN & COMPLETE STREET PLAN

## SHORT TERM ILLUSTRATIVE PLAN (1 OF 3)

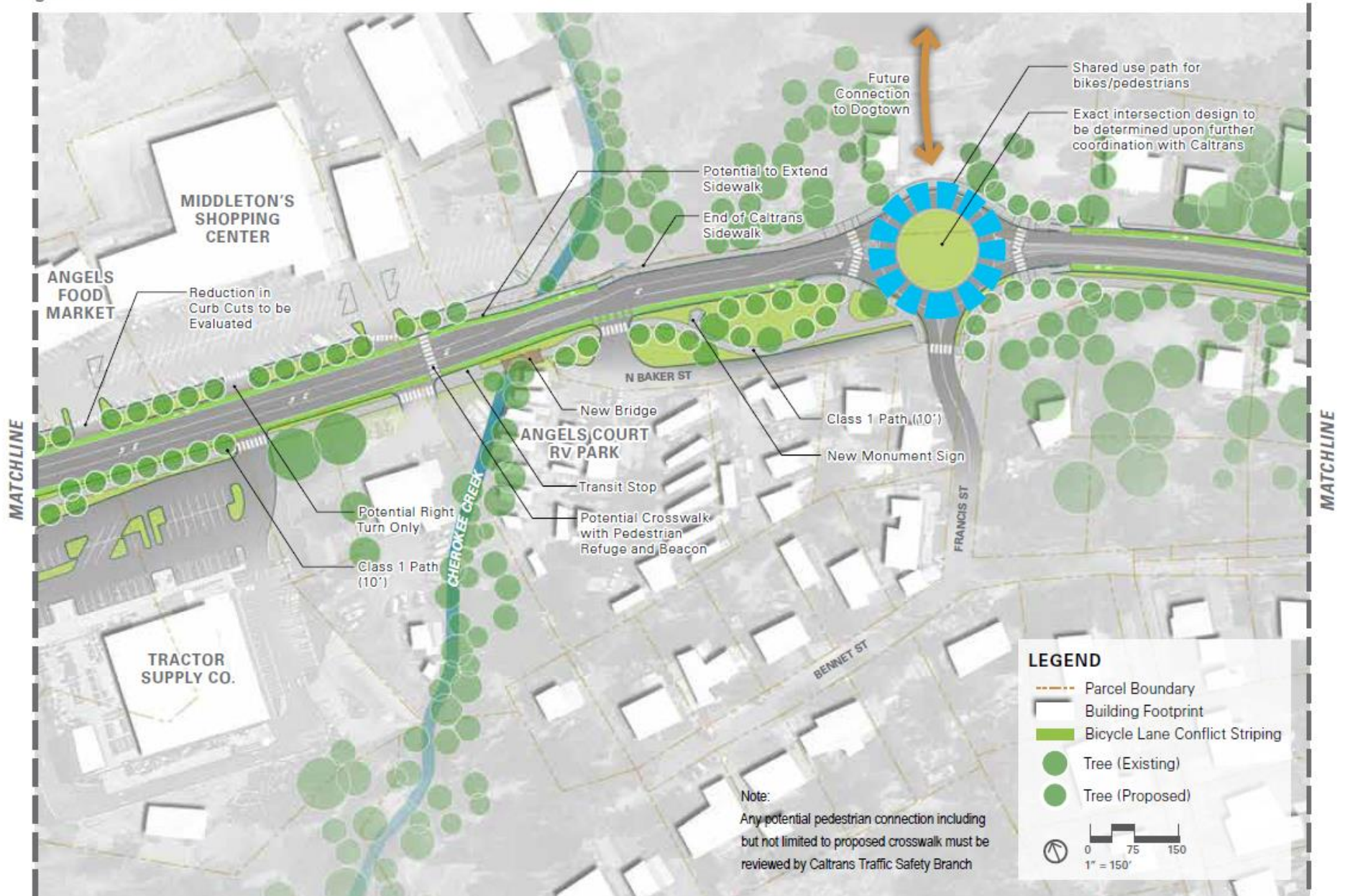
Figure 5.1: Short-Term Illustrative Plan (1 of 3)





## SHORT TERM ILLUSTRATIVE PLAN (2 OF 3)

Figure 5.2: Short-Term Illustrative Plan (2 of 3)





## SHORT TERM ILLUSTRATIVE PLAN (3 OF 3)

Figure 5.3: Short-Term Illustrative Plan (3 of 3)

