

**CHANGE ORDER AGREEMENT  
FOR PROFESSIONAL SERVICES**

Contractor: Robert E. Boyer Construction, Inc.  
Contract Date: April 11, 2023  
Project: Utica Park/Lightner Mine Expansion

Change Order No. 4(A)  
Date: November 19, 2024; REVISED January 7, 2025

Client's Name: City of Angels  
Client's Address: P.O. Box 667; 200 Monte Verda St., Bldg B Angels Camp, CA 95222

Attention: Steve Williams, Interim City Administrator  
Telephone: (209) 736-1346

**We hereby agree to make the following changes:**

Amend the agreement to include **Attachment A**.

Current Contract Amount:	\$2,275,000.00
Change Order Agreement #1:	\$193,888.00 (mine remediation – outside original scope)
Change Order Agreement # 2:	\$182,645.32 ( Add fire turnout road/hydrant)
Change Order Agreement #3:	\$132,303.62 (purchase gym equipment on behalf of City)
Change Order Agreement # 4:	\$9,146.40 (Speed bumps per neighboring landowner)
Revised Contract Amount:	\$2,792,983.34

Authorized Signature (Boyer) \_\_\_\_\_

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Accepted: The above estimated fees/costs relating to this Change Order are satisfactory and are hereby accepted. All services to be performed under the same terms and conditions as specified in the original contract.

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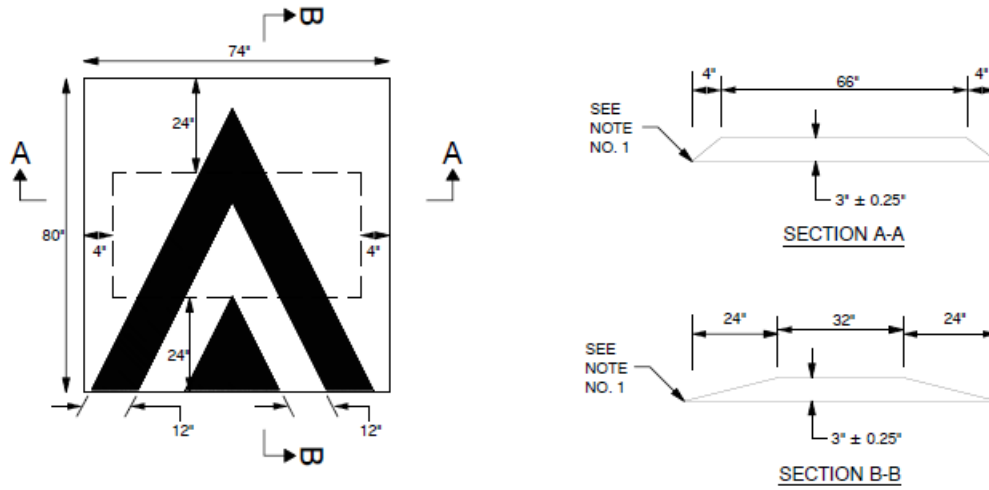
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Authorized Signature: \_\_\_\_\_  
Steve Williams, Interim City Administrator

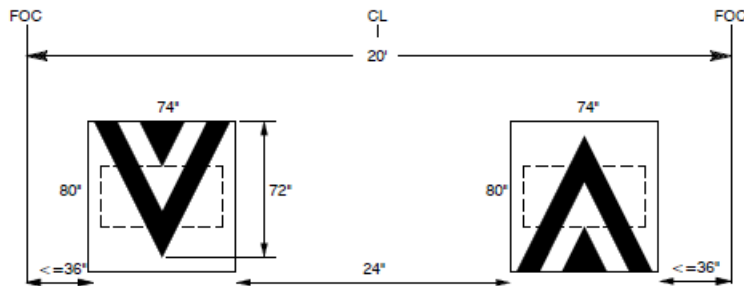
Date: \_\_\_\_\_

**Attachment A**  
**Speed Cushion Design 20' and 30' road**

## ASPHALT CONCRETE SPEED CUSHION DETAILS



## TYPICAL 20' ROAD LAYOUT - CURB TO CURB



**NOTES:**

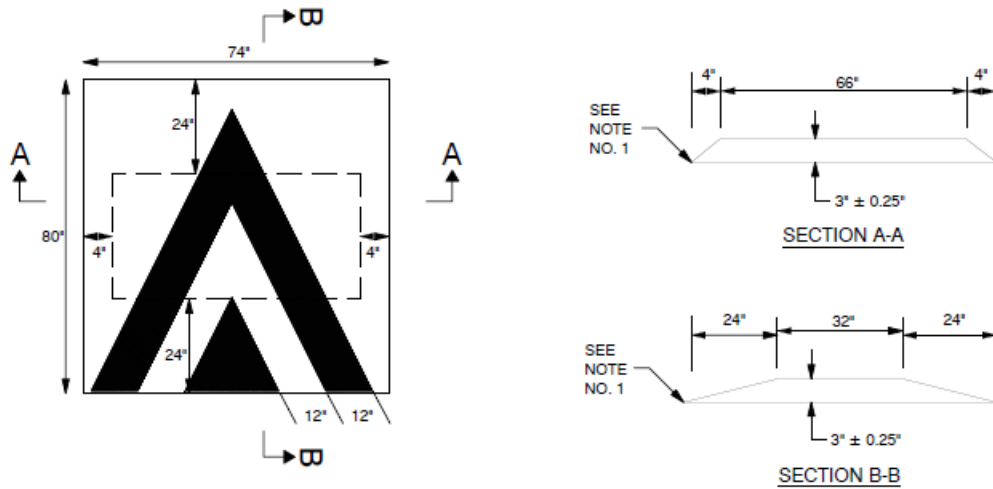
1. MAXIMUM STEP OF TRANSITION FROM PAVEMENT TO RAMP IS 0.5".
2. NUMBER OF SPEED CUSHION MODULES VARY BETWEEN 2 AND 5 BASED ON STREET WIDTH.
3. WIDTH BETWEEN SPEED CUSHION MODULES VARY BASED ON STREET WIDTH.
4. DISTANCE BETWEEN FACE OF CURB (FOC), GUTTER WIDTH AND LAST MODULE EDGE SHOULD NOT EXCEED 4'

N.T.S

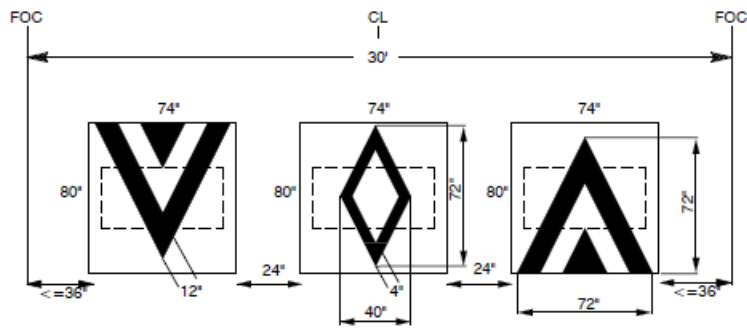
**SPEED CUSHION**

FIGURE A-9c

## ASPHALT CONCRETE SPEED CUSHION DETAILS



## TYPICAL 30' ROAD LAYOUT - CURB TO CURB



**NOTES:**

1. MAXIMUM STEP OF TRANSITION FROM PAVEMENT TO RAMP IS 0.5°.
2. NUMBER OF SPEED CUSHION MODULES VARY BETWEEN 2 AND 5 BASED ON STREET WIDTH.
3. WIDTH BETWEEN SPEED CUSHION MODULES VARY BASED ON STREET WIDTH.
4. DISTANCE BETWEEN FACE OF CURB (FOC), GUTTER WIDTH AND LAST MODULE EDGE SHOULD NOT EXCEED 4'

N.T.S

**SPEED CUSHION**  
FIGURE A-95

Figure 1: **Approximate** location Speed Cushions

