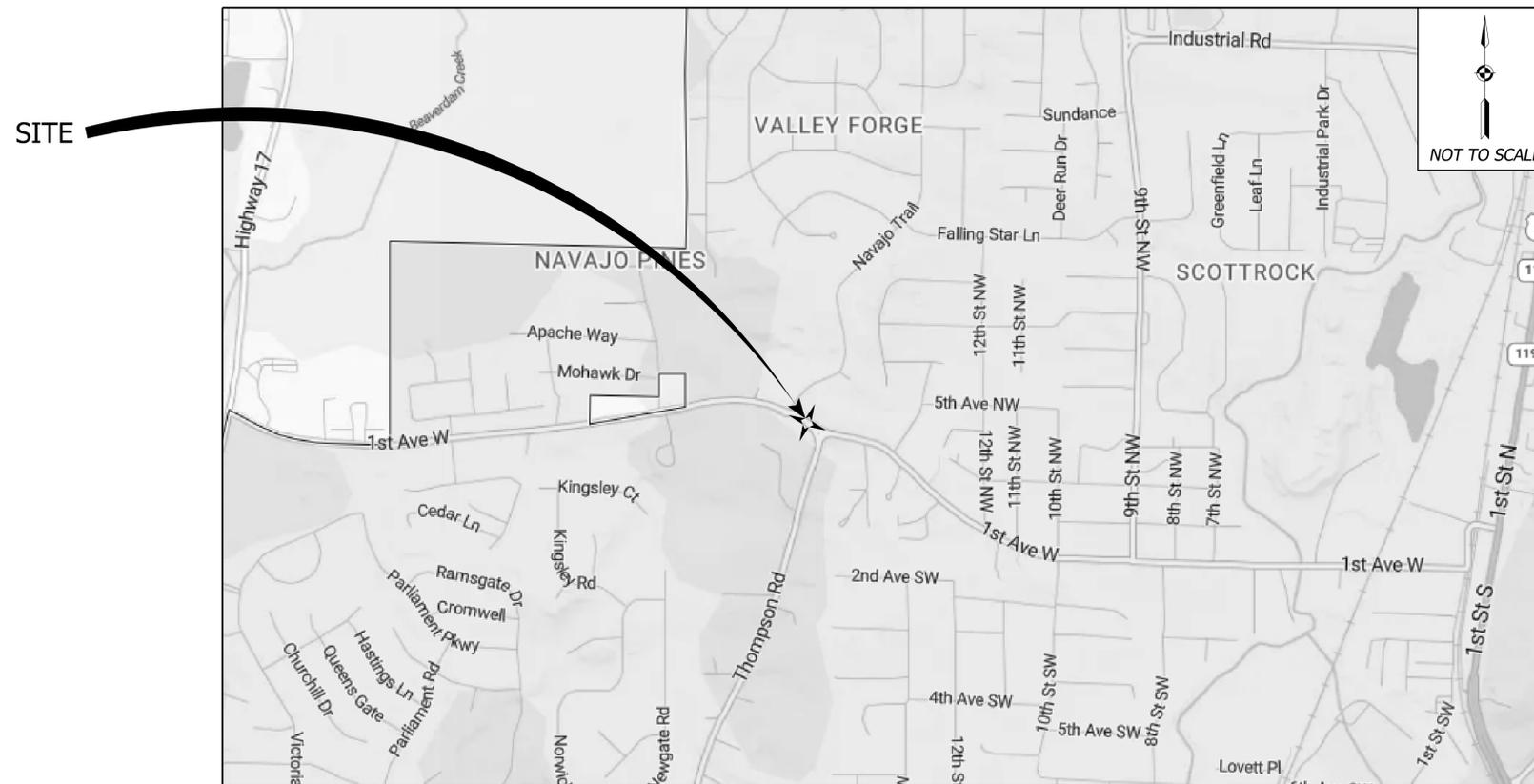


TRAFFIC SIGNAL MODIFICATION PLANS

FOR

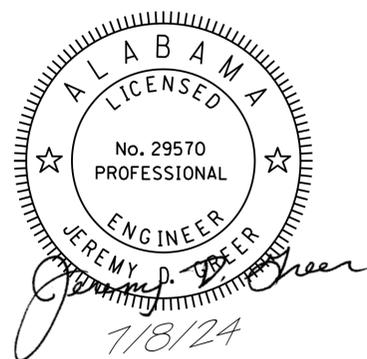
1ST AVE W AT THOMPSON ROAD/NAVAHO TRAIL

ALABASTER, ALABAMA



VICINITY MAP
NOT TO SCALE

FOR REVIEW
NOT FOR CONSTRUCTION



INDEX TO SHEETS

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TRAFFIC CONTROL PLAN.....	TS8

PREPARED BY:



2 PERIMETER PARK SOUTH
SUITE 500 EAST
BIRMINGHAM, ALABAMA 35243
PHONE: 205-940-6420
SA# 23-0362

PREPARED FOR
CITY OF ALABASTER
1953 MUNICIPAL WAY
ALABASTER, ALABAMA 35007

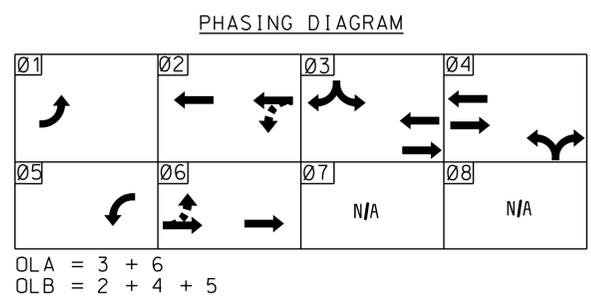
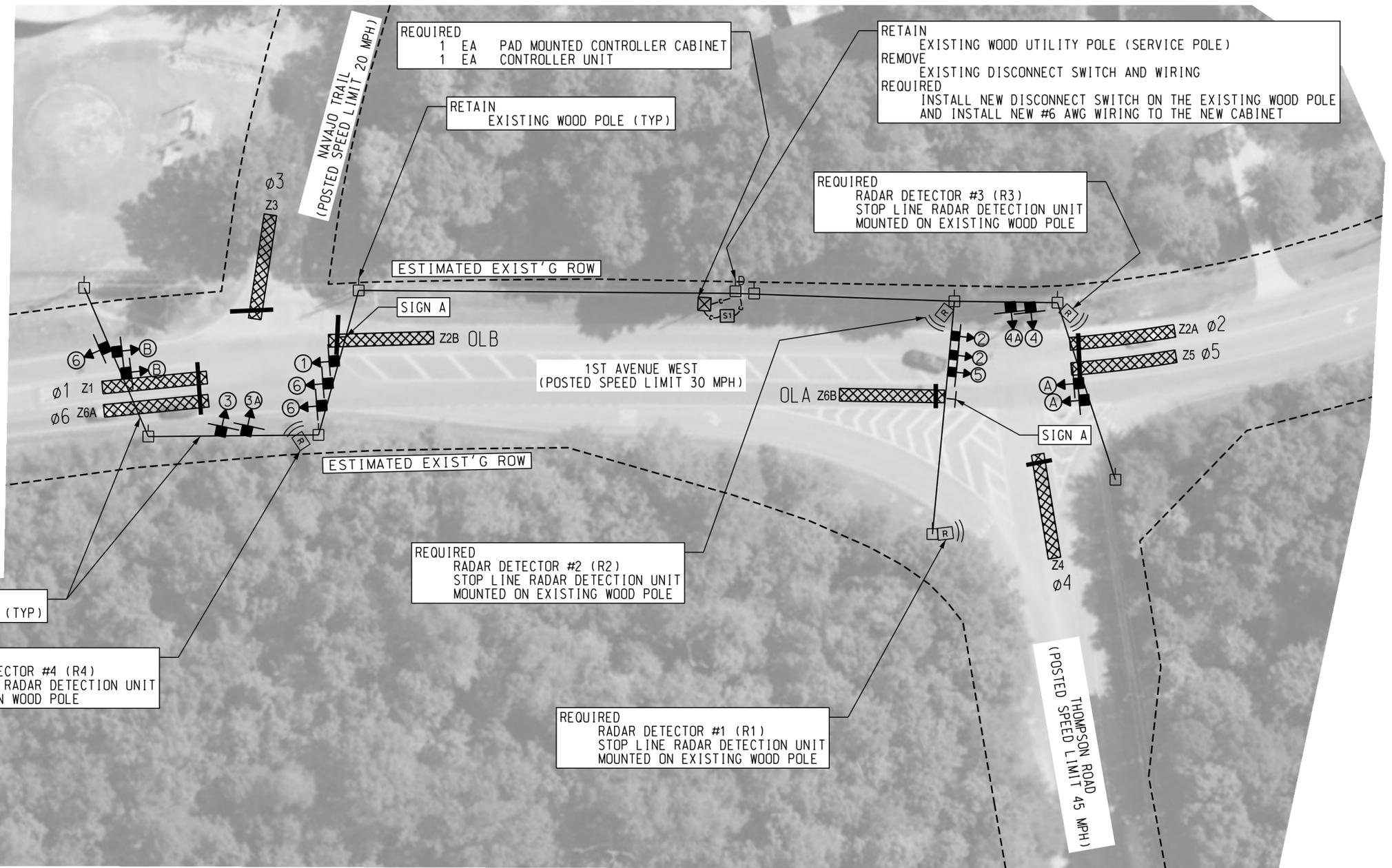
NOTES:

- PAVEMENT MARKINGS ARE FOR ILLUSTRATIVE PURPOSES ONLY, UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE FINAL LOCATION OF THE POWER SOURCE.
- THE CONTRACTOR SHALL VERIFY THERE ARE NO OVERHEAD OR UNDERGROUND UTILITIES CONFLICTING WITH ALL PROPOSED DESIGN ELEMENTS INCLUDING BUT NOT LIMITED TO THE LOCATION OF THE CONTROLLER/CABINET AND POWER SOURCE POLE. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONTACT EACH OF THE UTILITY OWNERS AND MAKE THE NECESSARY ARRANGEMENTS TO HAVE EACH UTILITY SPOTTED IN THE FIELD BY THE OWNERS BEFORE ANY EXCAVATION.
- ALL SIGNAL HEAD BACKPLATES SHALL HAVE A 2 INCH YELLOW RETROREFLECTIVE STRIP AROUND THE BORDER.
- VEHICULAR SIGNAL HEADS SHALL BE LED AND BLACK IN COLOR WITH VISORS.
- THE NAVAJO TRAIL INTERSECTION SHALL INCLUDE A SINGLE WAVETRONIX XP21 SENSOR, OR EQUIVALENT, FOR VEHICLE DETECTION.
- THE THOMPSON ROAD INTERSECTION SHALL INCLUDE THREE (3) WAVETRONIX MATRIX SENSORS, OR EQUIVALENT, FOR VEHICLE DETECTION. THESE SENSORS SHALL BE MOUNTED ON THE EXISTING WOOD POLES.
- THE CONTRACTOR SHALL CONTACT AND COORDINATE TO HAVE WAVETRONIX TECHNICIANS ON-SITE TO ASSIST IN ATTACHING THE DETECTOR UNIT TO THE WOOD POLES.
- CONTRACTOR SHALL COORDINATE A FIELD MEETING TO INCLUDE CITY OF ALABASTER AND SAIN ASSOCIATES TO DEMONSTRATE DETECTION SYSTEM AND MAKE NECESSARY ADJUSTMENTS.

**PRELIMINARY
NOT FOR CONSTRUCTION**

TRAFFIC SIGNAL LAYOUT

1ST AVENUE WEST AT THOMPSON ROAD/NAVAHO TRAIL

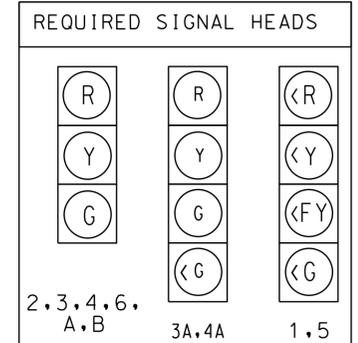
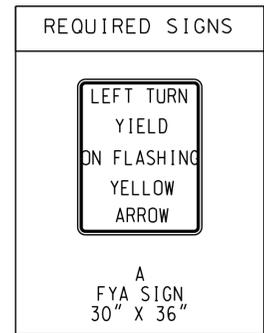


ESTIMATED EQUIPMENT AND MATERIAL SCHEDULE (LUMP SUM)

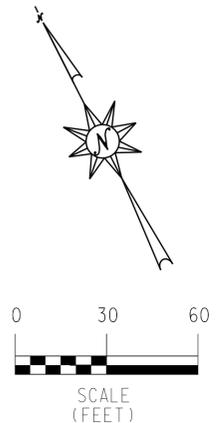
DESCRIPTION
3/8 in. MESSENGER CABLE
#14 SIGNAL CABLE IMSA 20-1
BACKPLATES WITH HARDWARE
MISCELLANEOUS HARDWARE
FYA SIGN W/ HARDWARE

RADAR DETECTOR CHART

ZONE	PHASE	DETECTOR	SIZE	FROM STOP BAR	ZONE TYPE	DELAY
1	1	R4	6 X 50	-	-	-
2A	2	R3	6 X 50	-	-	-
2B	OLB	R4	6 X 50	-	-	-
3	3	R4	6 X 50	-	-	-
4	4	R1	6 X 50	-	-	-
5	5	R3	6 X 50	-	-	-
6A	6	R4	6 X 50	-	-	-
6B	OLA	R2	6 X 50	-	-	-



**FOR REVIEW
NOT FOR CONSTRUCTION**



NO.	REVISIONS DESCRIPTION	BY	CHK	DATE

Two Perimeter Park South
Suite 500 East
Birmingham, Alabama 35243
Phone: (205) 940-6420
Website: www.sain.com

SAIN ASSOCIATES

TRAFFIC SIGNAL LAYOUT
1ST AVE W @ THOMPSON RD
FOR
CITY OF ALABASTER
ALABASTER, ALABAMA

DRN. BY	JOB NO.
CCD. BY	SCALE
PROJ. MGR.	DATE
JDS	7-8-2024

SHEET NO.
TS3

TRAFFIC SIGNAL DETAILS

ESTIMATED QUANTITIES **			
ITEM #	DESCRIPTION	UNITS	QTY
730A-000	Removal of Existing Traffic Control Unit	Lump Sum	1
730C-000	Furnishing and Installing Traffic Control Unit	Lump Sum	1
730K-001	Traffic Signal Junction Box, Type S1	Each	1
730L-002	1", Metallic, Conduit	Linear Foot	50
730L-005	2", Non-Metallic, Conduit	Linear Foot	150
730P-022	Vehicular Signal Head, 12 Inch, 3 Section, Type LED	Each	11
730P-023	Vehicular Signal Head, 12 Inch, 4 Section, Type LED	Each	4
730R-041	Controller Assembly, Type NEMA, 16 Channels	Each	1
730R-162	Controller Cabinet, Type Nema, 16 Channels	Each	1
730T-010	Electrical Power Service Assembly with Wood Pole	Each	1
*730U-400	Radar Detection System	Lump Sum	1

* SEE SHEET TS3 NOTES 6,7,8, AND 9.

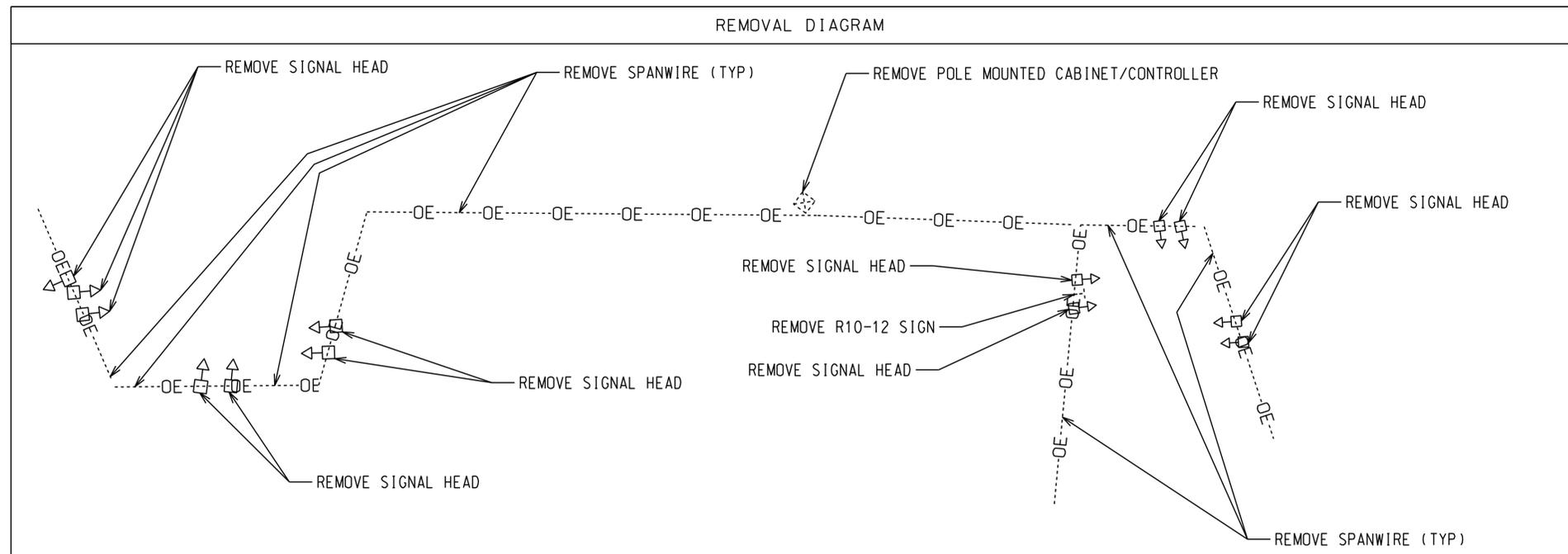
** QUANTITIES SHOWN ARE ESTIMATES ONLY. THE CONTRACTOR IS RESPONSIBLE FOR CALCULATING/ESTIMATING THEIR OWN QUANTITIES.

LEGEND	
A	- STD. DWG. NO. T.S.D.-730-1
B	- STD. DWG. NO. T.S.D.-730-2
C	- STD. DWG. NO. T.S.D.-730-2A
D	- STD. DWG. NO. T.S.D.-730-6
E	- STD. DWG. NO. T.S.D.-730-10
F	- STD. DWG. NO. T.S.D.-730-13
G	- STD. DWG. NO. T.S.D.-730-17
H	- STD. DWG. NO. T.S.D.-730-18

CONDUIT AND CONDUCTOR SCHEDULE				
CONDUIT TYPE	CONDUCTOR	FROM	TO	
3 - 2"	PVC	CONTROLLER	JUNCTION BOX #1	
3 - 2"	PVC	JUNCTION BOX #1	SIGNAL POLE	
1-1"	RGS	3#6-1c 120V	DISCONNECT SWITCH	
-	-	RADAR POWER/DATA CABLE	CONTROLLER	EACH DETECTOR UNIT
-	-	#14-10c IMSA 20-1	CONTROLLER	SIGNAL HEAD 1,5
-	-	#14-4c IMSA 20-1	SIGNAL HEAD 1	SIGNAL HEAD 6
-	-	#14-4c IMSA 20-1	SIGNAL HEAD 5	SIGNAL HEAD 2
-	-	#14-7c IMSA 20-1	CONTROLLER	SIGNAL HEAD 3A,4A
-	-	#14-4c IMSA 20-1	SIGNAL HEAD 3A	SIGNAL HEAD 3
-	-	#14-4c IMSA 20-1	SIGNAL HEAD 4A	SIGNAL HEAD 4
-	-	#14-4c IMSA 20-1	CONTROLLER	SIGNAL HEAD B
-	-	#14-4c IMSA 20-1	CONTROLLER	SIGNAL HEAD A

SUGGESTED CONTROLLER TIMINGS										
PHASE	MIN GREEN	PASSAGE	YELLOW	ALL RED	MAX 1	MAX 2	PED WALK	PED DONT WALK	RECALL	NON LOCK
1	5	3.0	2.5	3.0	25	25	-	-	-	-
2	30	3.0	4.7	1.7	45	45	-	-	MIN	-
3	5	3.0	2.5	2.7	15	15	-	-	-	-
4	5	3.0	2.5	3.0	30	30	-	-	-	-
5	5	3.0	2.5	2.7	35	40	-	-	-	-
6	20	3.0	4.0	1.7	35	40	-	-	MIN	-
OLA	6*	-	4.0	2.0	-	-	-	-	-	-
OLB	6*	-	4.0	2.0	-	-	-	-	-	-

OLA = 3+6
 OLB = 2+4+5
 *TRAILING GREEN



NOTES:

- REMOVE:
- EXISTING TRAFFIC SIGNAL HEADS (13)
- EXISTING SPAN MOUNTED R10-12 TRAFFIC SIGN (1)
- EXISTING POLE MOUNTED SIGNAL CABINET/CONTROLLER (1)

- RETAIN:
- ALL EXISTING WOOD POLES (9)

NO	REVISIONS DESCRIPTION	BY	CHK	DATE

SAIN ASSOCIATES
 Two Perimeter Park South
 Suite 500 East
 Birmingham, Alabama 35243
 Phone: (205) 940-6420
 Website: www.sain.com



TRAFFIC SIGNAL DETAILS
 1ST AVE W @ THOMPSON RD
 FOR
 CITY OF ALABASTER
 ALABASTER, ALABAMA

DRN. BY	JOB NO.
CKD. BY	SCALE
CC	NOT TO SCALE
PROJ. MGR.	DATE
JDS	7-8-2024

SHEET NO.
TS4

FOR REVIEW
 NOT FOR CONSTRUCTION

GENERAL TRAFFIC CONTROL PLAN NOTES

REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO

○ DENOTES NOTES THAT APPLY TO THIS PROJECT

- 700 THE TRAFFIC CONTROL PLAN IS DEVELOPED IN CONFORMANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES PART 6, 2009 EDITION. THE TRAFFIC CONTROL DEVICES INDICATED REPRESENT CONDITIONS KNOWN DURING PLAN DEVELOPMENT. IN THE EVENT ACTUAL PHYSICAL CONDITIONS WARRANT ADDITIONAL TRAFFIC CONTROL DEVICES, THEY SHALL BE INSTALLED IN CONFORMANCE WITH THE M.U.T.C.D. PART 6 AS DIRECTED BY THE ENGINEER. COST SHALL BE PAID FOR UNDER THE APPROPRIATE PAY ITEM.
- 701 ALL BLACK ON ORANGE CONSTRUCTION SIGNS SHALL BE FABRICATED USING TYPE XI FLUORESCENT ORANGE REFLECTIVE SHEETING MATERIAL FOR THE SIGN BACKGROUND.
- 702 DURING NON-WORKING HOURS NO EQUIPMENT OR MATERIAL SHALL BE PARKED OR STORED CLOSER THAN 30 FEET TO THE EDGE OF ANY ROADWAY CARRYING TRAFFIC. WHEN THIS IS NOT PRACTICAL, IT SHALL BE PLACED IN AN AREA APPROVED BY THE ENGINEER AND DELINEATED BY REFLECTORIZED DRUMS. THIS INCLUDES STORAGE OF TRAFFIC CONTROL DEVICES SUCH AS TRAILER MOUNTED OR OTHER TEMPORARY SIGNS, BARRICADES, DRUMS, ETC., WHICH ARE NOT IN USE DURING NON-WORKING HOURS. TO BE FURNISHED BY THE CONTRACTOR WITHOUT COST TO THE ALDOT. (SEE SKETCH ON SHEET)
- 703 WHERE THE LOCATION OF A REQUIRED SIGN FALLS IN A DRIVEWAY, SIDEWALK, BRIDGE, ETC., OR WHERE THE VISIBILITY OF A SIGN IS LIMITED TO THE TRAVELING PUBLIC, THE LOCATION SHALL BE ADJUSTED AS DIRECTED BY THE ENGINEER.
- 704 THE CONTRACTOR IS TO REMOVE, RELOCATE OR COVER DURING CONSTRUCTION AND THEN RESET OR UNCOVER UPON COMPLETION OF A PARTICULAR SECTION ANY CONFLICTING IN-PLACE ROADWAY SIGNS AND DELINEATORS, AS DIRECTED BY THE ENGINEER. SIGNS REQUIRING REMOVAL SHALL BE STOCKPILED AS DIRECTED BY THE ENGINEER AND SHALL BECOME PROPERTY OF THE ALDOT. COST SHALL BE A SUBSIDIARY OBLIGATION OF ITEM 740B.
- 705 DURING ALL PHASES OF WORK, NON-APPLICABLE PAVEMENT STRIPING OR MARKINGS SHALL BE REMOVED AND APPROPRIATE PAVEMENT STRIPING OR MARKINGS SHALL BE PLACED AS EXPEDITIOUSLY AS PRACTICAL, BUT IN ALL CASES, SHALL BE IN PLACE BY NIGHTFALL ON ANY ROADWAY CARRYING TRAFFIC, EXCEPT ON SHORT TERM OPERATIONS WHERE IT IS DETERMINED BY THE ENGINEER, THAT SUCH REMOVAL AND REPLACEMENT IS MORE HAZARDOUS THAN LEAVING EXISTING MARKINGS IN PLACE. COST OF ANY REMOVAL SHALL BE PAID FOR UNDER ITEM 701D OR AS A SUBSIDIARY OBLIGATION OF ITEM 701C.
- 706 OMITTED
- 707 THE CONTRACTOR SHALL PLACE ALL ADVANCE WARNING SIGNS BEFORE PROCEEDING WITH HIS WORK. SIGNS SHALL BE PLACED IN ORDER, IN THE DIRECTION OF TRAFFIC AND REMOVED IN REVERSE ORDER.
- 708 ALL VEHICLES, EQUIPMENT, PERSONNEL (EXCEPT FLAGGERS), AND THEIR ACTIVITIES, ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
- 709 THE CONTRACTOR SHALL MAINTAIN AT LEAST ONE ACCESS TO BUSINESSES AND RESIDENCES DURING ALL PHASES OF CONSTRUCTION.
- 710 CONSTRUCTION SIGNS MOUNTED ON TEMPORARY SUPPORTS SHALL BE MOUNTED AT A MINIMUM HEIGHT OF 5 FEET.
- 711 FLAGGERS SHALL BE PROPERLY ATTIRED, EQUIPPED WITH STAFF MOUNTED STOP/SLOW PADDLES IN SIGHT OF EACH OTHER, OR HAVE DIRECT COMMUNICATION AT ALL TIMES. FLAGGER STATION LOCATION MAY BE VARIED FROM THOSE SHOWN BASED ON ROADWAY ALIGNMENT AND CONDITIONS AT THE TIME OF THE LANE CLOSURE.
- 712 FLAGGERS ARE TO BE USED WHEN DIRECTED BY THE ENGINEER. SIGNS SHALL BE PLACED AT THE APPROPRIATE TIME, AND SHALL BE COVERED OR REMOVED WHEN FLAGGERS ARE NOT ON DUTY AND DURING NON-WORKING HOURS.
- 713 FOR MOVING OPERATIONS, THE TRAFFIC CONES MAY BE DELETED IF THE FLAGGERS ARE IN SIGHT OF EACH OTHER, OR IF A PILOT CAR IS USED ON A TWO LANE ROADWAY.
- 714 OMITTED
- 715 ALL CONTRACTOR'S EMPLOYEES' PERSONAL VEHICLES, AND CONTRACTOR'S EQUIPMENT NOT IN OPERATION, SHALL BE PARKED A MINIMUM OF THIRTY (30) FEET FROM THE TRAVELED WAY DURING WORKING HOURS, AS NOT TO CREATE A HAZARD.
- 716 THE TRAFFIC CONTROL PLAN IS NOT ALL INCLUSIVE. THE TCP PROVIDES SEVERAL DETAILED DRAWINGS INDICATING THE TRAFFIC CONTROL NECESSARY FOR THE DIFFERENT CONSTRUCTION ACTIVITIES ANTICIPATED FOR THIS PROJECT. THE CONTRACTOR SHALL SELECT THE DETAILED DRAWING THAT BEST FITS THE ACTIVITY TO BE PERFORMED.
- 717 OMITTED
- 718 REQUIRED TEMPORARY ROUTE MARKER ASSEMBLIES THAT ARE TO BE LOCATED IN THE VICINITY OF EXISTING ROUTE MARKERS SHOULD BE PLACED ALONG SIDE OF THOSE ALREADY IN PLACE. SOME EXISTING ROUTE MARKERS MAY HAVE TO BE COVERED OR REMOVED, AS DIRECTED BY THE ENGINEER. COST SHALL BE A SUBSIDIARY OBLIGATION OF ITEM 740B.

- 719 RA-1 (REBUILD ALABAMA) SIGNS SHALL BE REQUIRED FOR EVERY PROJECT. RA-1 SIGNS SHALL BE PLACED AT THE BEGINNING OF THE WORK LIMITS OF THE SUBJECT PROJECT ROUTE. RA-1 SIGNS SHALL BE POSTED ON THE RIGHT-HAND SIDE OF THE ROADWAY ON THEIR OWN SUPPORT SYSTEM. THE RA-1 SIGNS SHALL BE REMOVED UPON COMPLETION OF THE PROJECT.
- 720 ALL TRAFFIC CONTROL DEVICES THAT ARE NOT APPLICABLE AT ANY SPECIFIC TIME SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.
- 721 OMITTED
- 722 OMITTED
- 723 THE CONTRACTOR SHALL MAKE PROVISIONS FOR THE SAFETY OF PEDESTRIAN TRAFFIC CROSSING THE WORK ZONES DURING CONSTRUCTION.
- 724 OMITTED
- 725 ALL SIGNS SHALL BE POST-MOUNTED IF THE WORK PERIOD EXCEEDS FOUR DAYS, EXCEPT FOR THOSE SIGNS WHICH ARE MOUNTED ON BARRICADES. FOR REPEATED DAY OPERATIONS, SIGNS MAY BE MOUNTED ON TEMPORARY SUPPORTS AND REMOVED AT THE COMPLETION OF THE DAY'S OPERATION.
- 726 W8 SIGNS INTENDED TO WARN MOTORISTS OF SURFACE CONDITIONS EXTENDING FOR GREATER THAN 1 MILE SHALL BE PLACED PRIOR TO THE BEGINNING OF SURFACE CONDITION AND AT 1 MILE INCREMENTS THEREAFTER, WITH THE EXCEPTION SPECIFIED IN NOTE 727.
- 727 DURING THE WIDENING OR RESURFACING OF ANY ROADWAY CARRYING TRAFFIC, THE CONTRACTOR SHALL ADVISE THE MOTORISTS OF ANY EDGE OF PAVEMENT DROP-OFFS 3 INCHES OR GREATER BY PLACING SHOULDER DROP-OFF SIGNS EVERY ½ MILE BEGINNING PRIOR TO THE WIDENING OR RESURFACING. REQUIRED SHOULDER WORK TO ELIMINATE THE DROP-OFFS SHALL BE PURSUED IN AN EXPEDITIOUS MANNER FOLLOWING THE WIDENING AND/OR RESURFACING.
- 728 A DIFFERENCE IN ELEVATION OF APPROXIMATELY 2 INCHES OR LESS AT THE CENTERLINE MAY BE ALLOWED DURING NON-WORKING HOURS WITHOUT ADDITIONAL TRAFFIC CONTROL. SPECIAL CONDITIONS MAY EXIST WHERE PROTECTION SHOULD BE PROVIDED WHERE THE DIFFERENCE IS 2 INCHES OR LESS.
- 729 SIGNS ON TEMPORARY SUPPORTS ARE TO BE REMOVED OR COVERED WHEN NO WORK IS BEING PERFORMED OR AT THE COMPLETION OF THE DAY'S OPERATION.
- 730 OMITTED
- 731 OMITTED
- 732 CHANNELIZING DRUMS SHOULD BE PLACED ON 10 FOOT INTERVALS IN RADII.
- 733 CHANNELIZING DRUMS PLACED TO PROTECT COMPLETED WORK NOT OPEN TO TRAFFIC, SHOULD BE SPACED AT 50 FOOT INTERVALS.
- 734 CHANNELIZING DRUMS PLACED IN THE EXCAVATED AREA AHEAD OF PAVING OPERATIONS, SHOULD BE SPACED AT 50 FOOT INTERVALS.
- 735 CHANNELIZING DRUMS PLACED ON PAVEMENT DURING WORKING HOURS SHALL BE SHIFTED TO THE EDGE OF SHOULDER DURING NON-WORKING HOURS AND DURING PEAK PERIODS.
- 736 CHANNELIZING DRUMS SHOULD BE PLACED ON 25 FOOT INTERVALS THROUGHOUT ALL TAPERS.
- 737 CHANNELIZING DEVICES SHALL EXTEND TO A POINT WHERE THEY ARE VISIBLE TO APPROACHING TRAFFIC.
- 738 OMITTED
- 739 OMITTED
- 740 OMITTED
- 741 FOR DIVIDED ROADWAYS, THE REQUIRED ADVANCE WARNING SIGNS SHALL BE POSTED ON BOTH THE RIGHT AND LEFT SIDE OF THE ROADWAY.
- 742 THE CONTRACTOR SHALL CLOSE THE LANE ADJACENT TO THE WORK AREA ANYTIME WORK OUTSIDE THE EXISTING TRAVEL LANES ENDOACHES WITHIN 2 FEET OF THE EXISTING EDGE OF PAVEMENT.
- 743 OMITTED
- 744 THE TRANSITION TAPER LENGTH (L) IS SHOWN IN TABLE 6C-4, AND THE BUFFER LENGTH IS SHOWN IN TABLE 6C-2 OF THE MUTCD, PART 6, 2009 EDITION.
- 745 OMITTED
- 746 UNEVEN LANES SIGNS SHALL BE COVERED OR REMOVED WHEN NO UNEVEN PAVEMENT CONDITIONS EXIST.
- 747 MOVING OPERATIONS SHALL BE CONFINED TO ONE LANE IN THE DIRECTION OF TRAFFIC.

- 748 R16-3 (WHEN WORKERS ARE PRESENT BEGIN HIGHER FINES) AND R16-3A (END HIGHER FINES) SIGNS SHALL BE REQUIRED FOR EVERY PROJECT ON STATE ROUTES AND INTERSTATE HIGHWAYS. THESE SIGNS SHALL BE POSTED AT THE BEGINNING AND END OF THE PROJECT WITH AN R2-1 (REGULATORY SPEED SIGN) ALWAYS FOLLOWING THE R16-3 SIGN. R16-3B (HIGHER FINES ZONE) AND R2-1 SIGNS SHALL BE POSTED AT MAXIMUM INTERVALS OF THREE MILES THROUGHOUT THE PROJECT LIMITS.
- 749 WHEN A CONSTRUCTION WORK ZONE SPEED LIMIT REDUCTION IS NOT REQUIRED AT THE END OF THE WORK DAY, THE CONTRACTOR SHALL COVER OR REMOVE THE REDUCED R2-1 (REGULATORY SPEED SIGNS) AND THE W3-5B (REDUCED SPEED AHEAD) SIGNS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- 750 DURING REPLACEMENT OF GUARDRAIL AND/OR GUARDRAIL END ANCHORS, A REFLECTORIZED DRUM SHALL BE PLACED BEFORE THE END OF ANY EXPOSED GUARDRAIL AT NIGHT WHERE THE GUARDRAIL END ANCHOR CANNOT BE REPLACED IN ONE DAY'S TIME.
- 751 CONSTRUCTION SIGNS MOUNTED ON A SINGULAR OR DUAL SQUARE TUBULAR OR U-CHANNEL POST SHALL BE INSTALLED AS SHOWN ON SPECIAL DRAWING NOS. IHS-710-21 AND IHS-710-23.
- 752 THE CONTRACTOR AND THE CONSTRUCTION ENGINEER SHALL DISCUSS AND PLAN FOR THE HANDLING OF TRAFFIC FOR ALL HOLIDAYS BEFORE ANY WORK BEGINS. UNLESS OTHERWISE PRE-APPROVED BY THE REGION ENGINEER, THE FOLLOWING SHALL HOLD:

 FOR CHRISTMAS AND NEW YEARS DAY:
 FROM 11:59 PM DECEMBER 23 THROUGH 6:00 AM JANUARY 2.

 FOR NATIONAL MEMORIAL DAY AND LABOR DAY:
 FROM 12:00 NOON THE FRIDAY BEFORE THE HOLIDAY THROUGH 11:59 PM THE DAY OF THE HOLIDAY.

 FOR INDEPENDENCE DAY (THE 4TH OF JULY):
 FROM 12:00 NOON THE DAY BEFORE THE HOLIDAY THROUGH 11:59 PM THE DAY OF THE HOLIDAY.

 FOR THANKSGIVING DAY:
 FROM 12:00 NOON THE WEDNESDAY BEFORE THANKSGIVING DAY THROUGH 11:59 PM THE SUNDAY FOLLOWING THANKSGIVING DAY.

 ANY OTHER STATE HOLIDAYS WILL BE HANDLED AS APPROVED BY THE PROJECT ENGINEER.

 THE CONTRACTOR SHALL COORDINATE WITH THE ENGINEER AND LOCAL GOVERNMENT ON TRAFFIC AND/OR WORK RESTRICTIONS FOR LOCAL HOLIDAYS OR EVENTS NOT LISTED ON ALDOT'S LIST OF OFFICIAL STATE HOLIDAYS.
- 753 THE CONTRACTOR SHALL NOT HAVE A LANE CLOSURE FROM 6:30AM THROUGH 8:30AM AND 3:00PM THROUGH 6:00PM, MONDAY THROUGH FRIDAY.

NO	DESCRIPTION	BY	CHK	DATE

SAIN ASSOCIATES

Two Perimeter Park South
Suite 500 East
Birmingham, Alabama 35243
Phone: (205) 940-6420
Website: www.sain.com



DRAWING NAME
TRAFFIC CONTROL PLAN

IST AVE W @ THOMPSON RD
FOR
CITY OF ALABASTER
ALABASTER, ALABAMA

DRN. BY	JOB NO.
JDG	23-0362
CKD. BY	SCALE
CC	NOT TO SCALE
PROJ. MGR.	DATE
JDS	7-8-2024
SHEET NO.	
T85	

NOT TO SCALE	--SPECIFICATIONS-- CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION
DESIGN BUREAU SPECIAL DRAWING	SPECIAL DRAWING NO
GENERAL TRAFFIC CONTROL PLAN NOTES	INDEX NO
SPECIAL PROJECT DETAIL	2000

FOR REVIEW
NOT FOR CONSTRUCTION



ALABAMA DEPARTMENT OF TRANSPORTATION
1409 COLLEEM BULEVARD
MONTGOMERY, AL 36130-3050

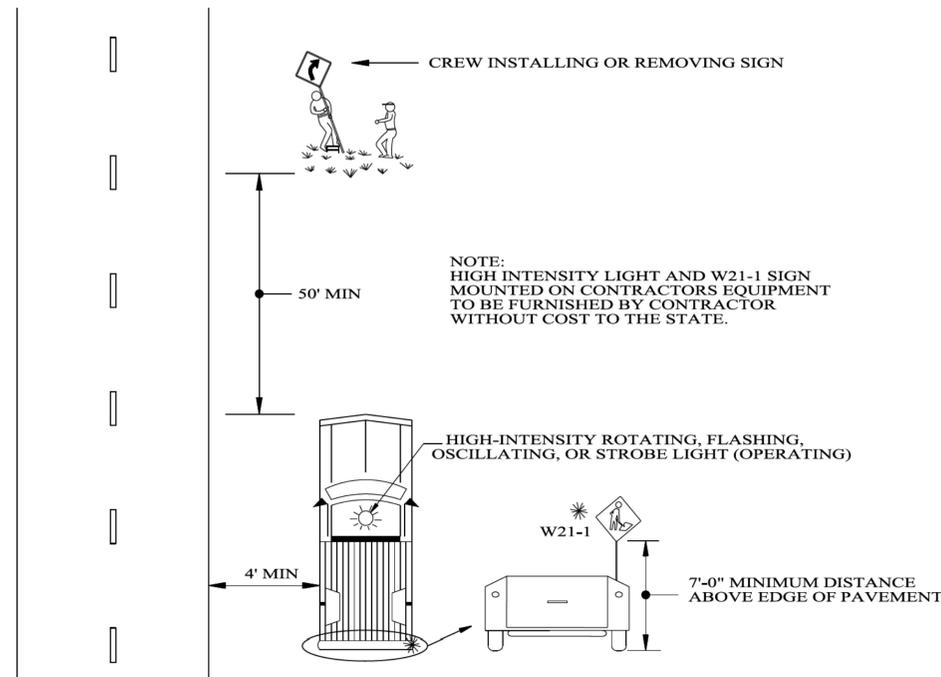
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REVISIONS:
1. Modified Note 748 for the addition of the R13-3b sign on 9-28-2021 by D.J.W.
2. Aligned text for Note 746 on 12-8-2021 by D.J.W.

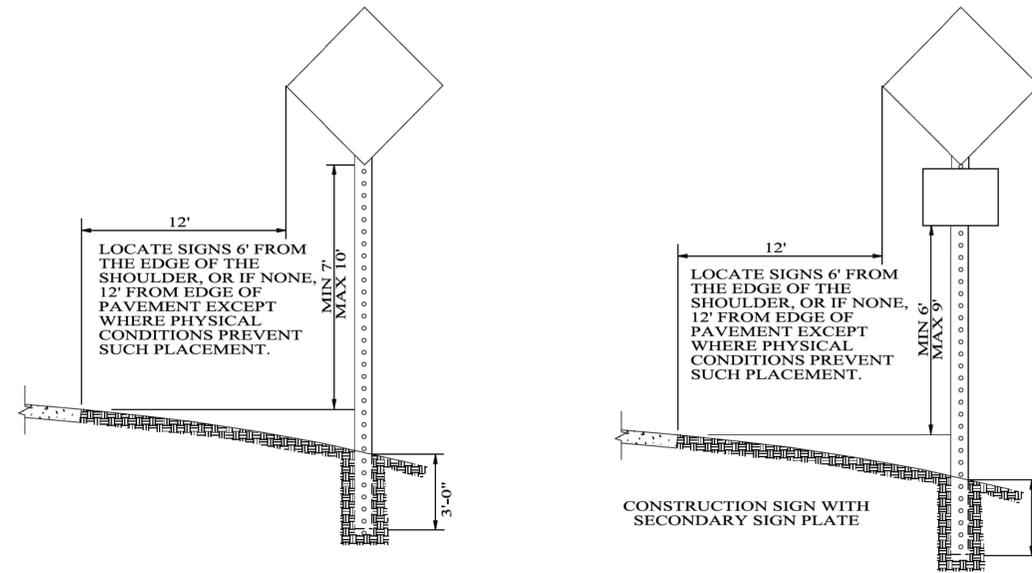
Bureau Std Engr: D.J.W.
DRAWN BY: _____
DATE DRAWN: _____
REVISED DATE: 12-8-2021

TEMPORARY TRAFFIC CONTROL PLAN SHEET

REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO

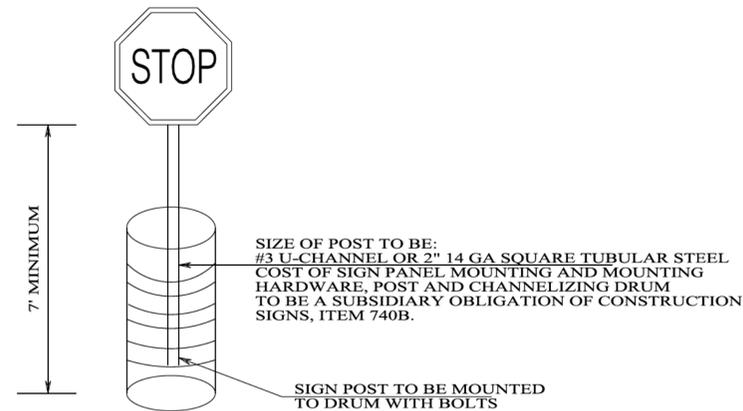


TYPICAL METHOD FOR INSTALLING OR REMOVING CONSTRUCTION SIGNS



NOTE:
IF THE CONTRACTOR CHOOSES TO SPLICE THE POSTS FOR THE REQUIRED POST MOUNTED CONSTRUCTION SIGNS, THEY SHALL BE SPLICED AS SHOWN ON DRAWING IHS-710-23.

HEIGHT AND LATERAL LOCATION OF POST MOUNTED CONSTRUCTION SIGNS



DETAIL FOR DRUM MOUNTED CONSTRUCTION SIGNS

NOT TO SCALE

--SPECIFICATIONS-- CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION	
SPECIAL DRAWING NO	INDEX NO
SPECIAL PROJECT DETAIL	2001A

ALABAMA DEPARTMENT OF TRANSPORTATION
1409 COLISEUM BOULEVARD
MONTGOMERY, AL 36130-3050

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REVISIONS:

1.

Bureau Std Engr: D.J.W.
DRAWN BY:
DATE DRAWN:
REVISED DATE: 5-12-2019

DESIGN BUREAU SPECIAL DRAWING
STANDARD DETAILS
FOR TRAFFIC CONTROL PLANS

NO	DESCRIPTION	BY	CHK	DATE

SAIN ASSOCIATES
Two Perimeter Park South
Suite 500 East
Birmingham, Alabama 35243
Phone: (205) 940-6420
Website: www.sain.com



TRAFFIC CONTROL PLAN
1ST AVE W @ THOMPSON RD
FOR
CITY OF ALABASTER
ALABASTER, ALABAMA

DRN. BY	JOB NO.
JDG	23-0362
CKD. BY	SCALE
CC	NOT TO SCALE
PROJ. MGR.	DATE
JDS	7-8-2024

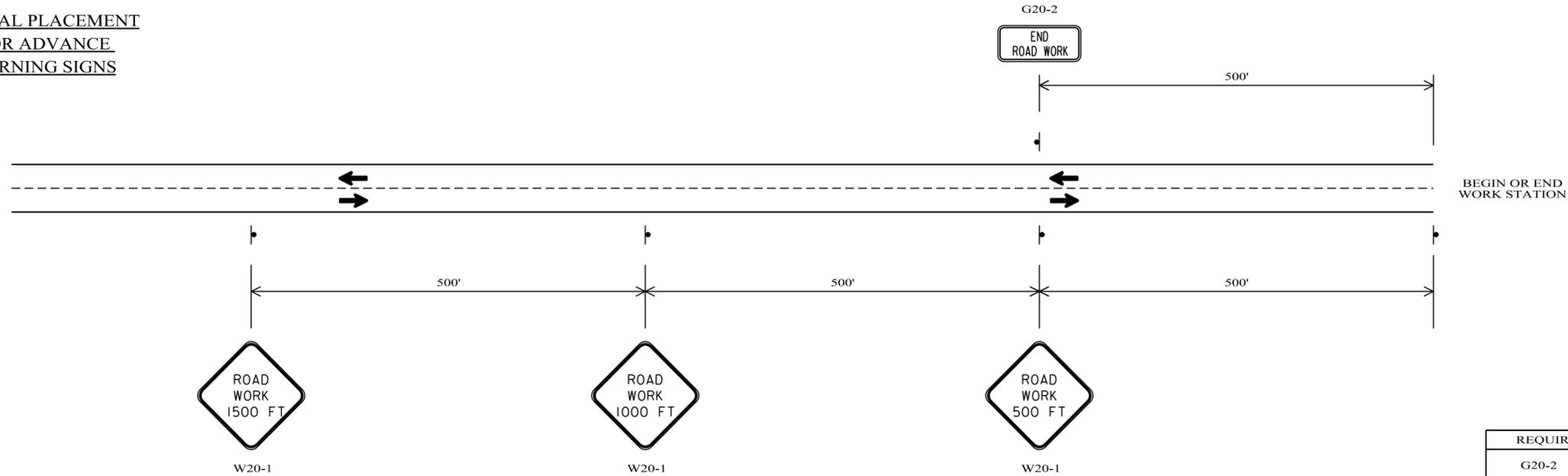
SHEET NO.
TS6

FOR REVIEW
NOT FOR CONSTRUCTION

TEMPORARY TRAFFIC CONTROL PLAN SHEET

REFERENCE PROJECT NO.	FISCAL YEAR	SHEET NO.

TYPICAL PLACEMENT FOR ADVANCE WARNING SIGNS



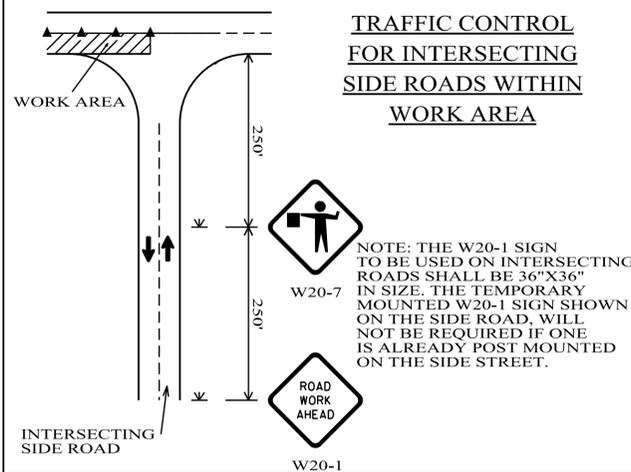
REQUIRED SIGN SIZES	
G20-2	48" X 24"
W20-1	48" X 48"
W20-4	48" X 48"
W20-7	48" X 48"

GENERAL NOTES

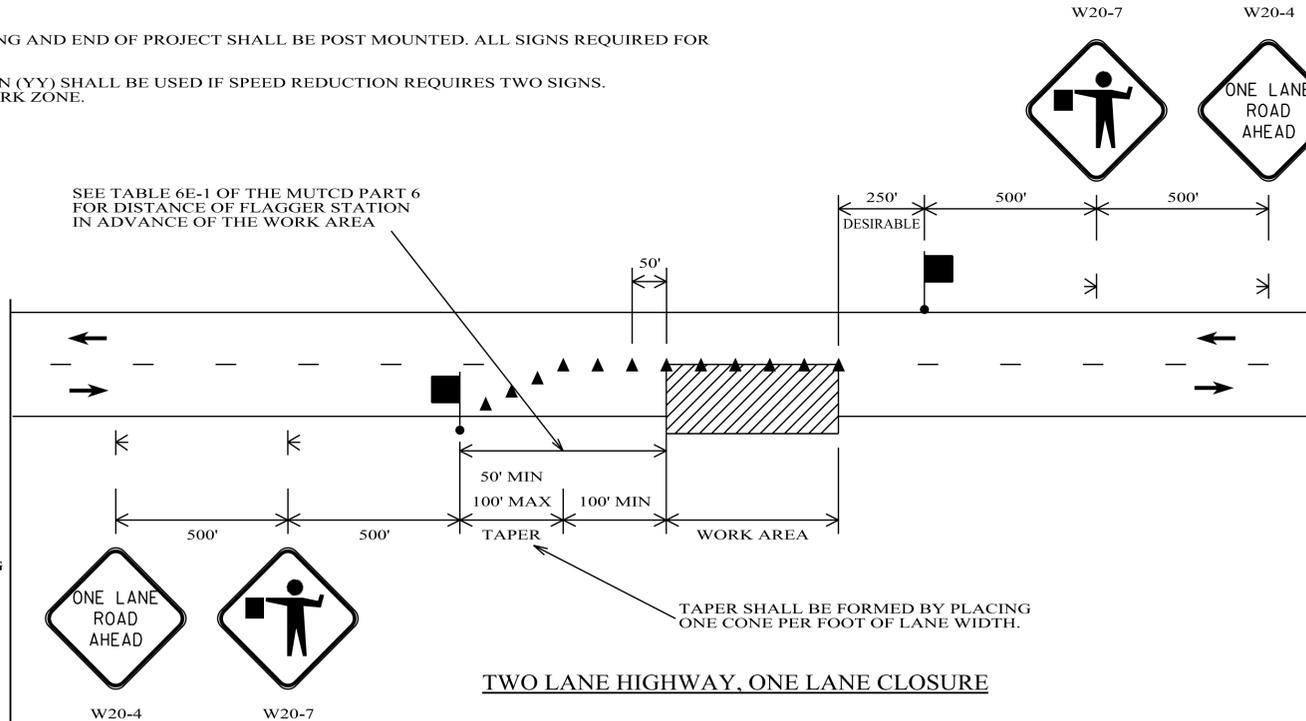
- ALL ADVANCE WARNING SIGNS REQUIRED AT THE BEGINNING AND END OF PROJECT SHALL BE POST MOUNTED. ALL SIGNS REQUIRED FOR THE LANE CLOSURE SHALL BE TEMPORARY MOUNTED.
- SIGN (XX) SHALL SHOW THE SPEED IN THE WORK ZONE. SIGN (YY) SHALL BE USED IF SPEED REDUCTION REQUIRES TWO SIGNS. SIGN(ZZ) SHALL SHOW THE POSTED SPEED OUTSIDE THE WORK ZONE.

THE SIGN SIZES SHOWN ON THIS SHEET SHALL SUPERCEDE THOSE SHOWN ON THE STANDARD HIGHWAY SIGNS DRAWINGS UNLESS OTHERWISE APPROVED BY THE ENGINEER.

TRAFFIC CONTROL FOR INTERSECTING SIDE ROADS WITHIN WORK AREA



SEE TABLE 6E-1 OF THE MUTCD PART 6 FOR DISTANCE OF FLAGGER STATION IN ADVANCE OF THE WORK AREA



LEGEND

- TEMPORARY MOUNTED SIGN
- POST MOUNTED SIGN
- CONES
- FLAGGER
- WORK AREA

NOT TO SCALE

-SPECIFICATIONS- CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION	
SPECIAL DRAWING NO.	INDEX NO.
SPECIAL PROJECT DETAIL	2002

ALABAMA DEPARTMENT OF TRANSPORTATION
1409 COLISEUM BOULEVARD
MONTGOMERY, AL 36130-3050

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REVISIONS:

NO.	DATE	DESCRIPTION

Bureau Std Engr: D.J.W.
DRAWN BY: _____
DATE DRAWN: _____
REVISED DATE: 3-24-2021

DESIGN BUREAU SPECIAL DRAWING
DETAILS FOR TRAFFIC CONTROL FOR TWO LANE HIGHWAYS

NO.	REVISIONS DESCRIPTION	BY	CD	DATE

SAIN ASSOCIATES
Two Perimeter Park South
Suite 500 East
Birmingham, Alabama 35243
Phone: (205) 940-6420
Website: www.sain.com



DRAWING NAME: **TRAFFIC CONTROL PLAN**
1ST AVE W @ THOMPSON RD
FOR
CITY OF ALABASTER ALABAMA

DRN. BY	JOB NO.
JDG	23-0362
CKD. BY	SCALE
CC	NOT TO SCALE
PROJ. MGR.	DATE
JDS	7-8-2024

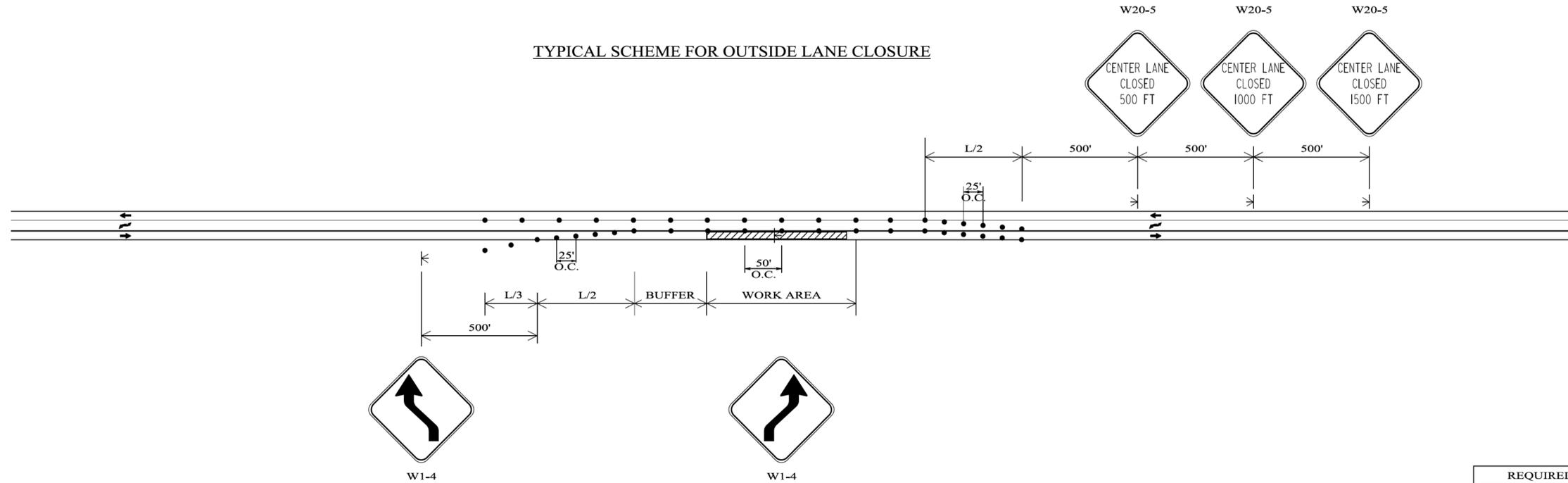
SHEET NO.
TS7

FOR REVIEW
NOT FOR CONSTRUCTION

TEMPORARY TRAFFIC CONTROL PLAN SHEET

REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO

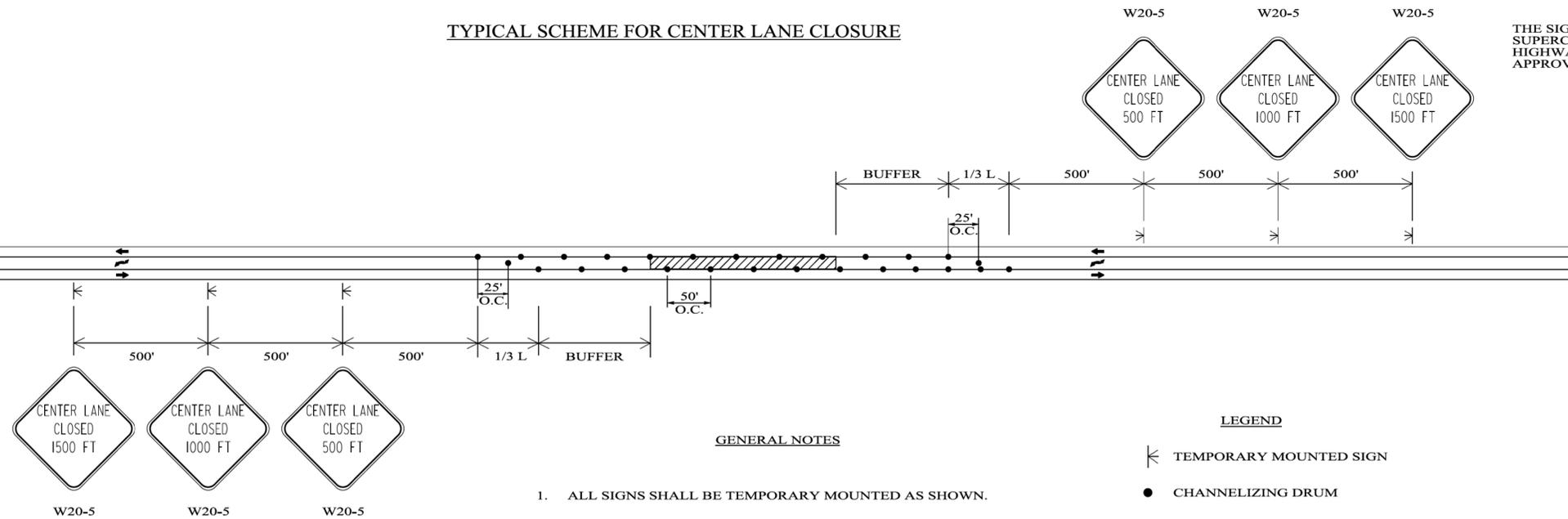
TYPICAL SCHEME FOR OUTSIDE LANE CLOSURE



REQUIRED SIGN SIZES	
W1-4	48" X 48"
W20-5	48" X 48"

THE SIGN SIZES SHOWN ON THIS SHEET SHALL SUPERCEDE THOSE SHOWN ON THE STANDARD HIGHWAY SIGNS DRAWINGS UNLESS OTHERWISE APPROVED BY THE ENGINEER.

TYPICAL SCHEME FOR CENTER LANE CLOSURE



GENERAL NOTES

- ALL SIGNS SHALL BE TEMPORARY MOUNTED AS SHOWN.

LEGEND

	TEMPORARY MOUNTED SIGN
	CHANNELIZING DRUM
	WORK AREA

NOT TO SCALE

--SPECIFICATIONS--	
CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION	
SPECIAL DRAWING NO	INDEX NO
SPECIAL PROJECT DETAIL	2021

ALABAMA DEPARTMENT OF TRANSPORTATION
1409 COLISEUM BOULEVARD
MONTGOMERY, AL 36130-3050

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REVISIONS:
1.

Bureau Std Engr: D.J.W.
DRAWN BY:
DATE DRAWN:
REVISED DATE: 5-12-2019

DESIGN BUREAU SPECIAL DRAWING
DETAILS FOR TRAFFIC CONTROL FOR 3-LANE HIGHWAY LANE CLOSURES

NO	REVISIONS DESCRIPTION	BY	CHK	DATE

SAIN ASSOCIATES
Two Perimeter Park South
Suite 500 East
Birmingham, Alabama 35243
Phone: (205) 940-6420
Website: www.sain.com



DRAWING NAME: TRAFFIC CONTROL PLAN
FOR
1ST AVE W @ THOMPSON RD
CITY OF ALABASTER
ALABASTER, ALABAMA

DRN. BY	JOB NO.
JDG	23-0362
CKD. BY	SCALE
CC	NOT TO SCALE
PROJ. MGR.	DATE
JDS	7-8-2024

SHEET NO.
TS8

FOR REVIEW
NOT FOR CONSTRUCTION