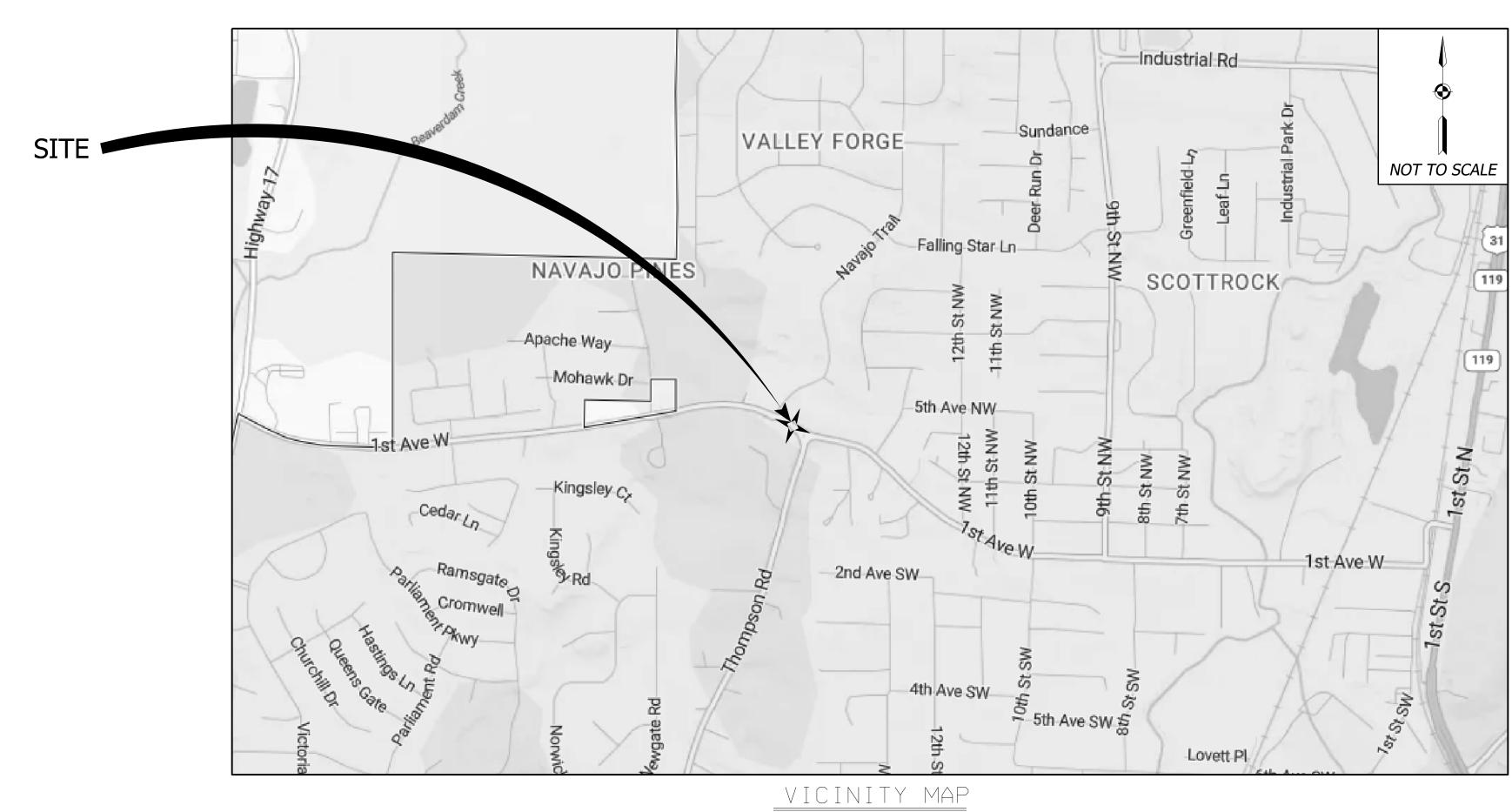
TRAFFIC SIGNAL MODIFICATION PLANS FOR 1ST AVE W AT THOMPSON ROAD/NAVAHO TRAIL

ALABASTER, ALABAMA



No. 29570
PROFESSIONAL

NO. 29570
PROFESSIONAL

NOT FOR CONSTRUCTION

PREPARED BY:



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SA# 23-0362

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PREPARED FOR

CITY OF ALABASTER
1953 MUNICIPAL WAY
ALABASTER, ALABAMA 35007

MISCELLANEOUS EQUIPMENT

	EXISTING	REQUI REC
TRAFFIC SIGNAL HEAD		#
TRAFFIC SIGNAL HEAD (3-SECTION) W/ BACKPLATE		#
TRAFFIC SIGNAL HEAD (4-SECTION) W/ BACKPLATE		#
TRAFFIC SIGNAL HEAD (5-SECTION) W/ BACKPLATE		
PEDESTRIAN SIGNAL HEAD		~
8 FOOT PEDESTAL POLE AND PEDESTRIAN SIGNAL HEAD		
PEDESTAL MOUNTED FLASHING WARNING SIGNAL WITH SIGN		
PEDESTAL MOUNTED ILLUMINATED SCHOOL ZONE SIGN	SCH	SCH
PUSH BUTTON ASSEMBLY	PB	PB
SPAN/MASTARM MOUNTED SIGN		\dashv
OMNI DIRECTIONAL ANTENNA		T
DIRECTIONAL ANTENNA	111	
EMERGENCY VEHICLE PREEMPTION SENSOR	EVP	EVP
BLANKOUT MESSAGE SIGN	(B)	$\langle \mathbb{B} \rangle$
TRAFFIC CONTROL CENTER		
HIGHWAY ADVISORY RADIO		
HUB BUILDING	H	H
DYNAMIC MESSAGE SIGN (OVERHEAD)	00	••
DYNAMIC MESSAGE SIGN (ROADSIDE)	·	•
DYNAMIC MESSAGE SIGN (CANTILEVER)	0	•
BROADBAND RADIO SYSTEM	((۱۱۰۲	(וויך
WIRELESS SENSOR	©	(
WIRELESS REPEATER		_
DIGITAL RADIO	DR	DR
(NOTE: # INDICATES SIGNAL H	EAD NUMBER)	

(NOTE: # INDICATES SIGNAL HEAD NUMBER)

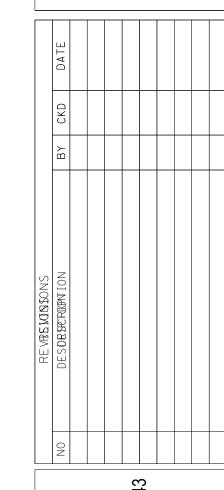
POLES

	EXI STI NG	REQUI RED
METAL POLE	\circ	
CONCRETE POLE	$\langle \mathbb{T} \rangle$	•
METAL MASTARM POLE	O	•
CLASS 3 WOOD SERVICE POLE WITH DISCONNECT	□D	ĎD
WOOD POLE	<u> </u>	■-
DOWN GUY		\longrightarrow
LUMINAIRE	<::>	

ABBREVIATIONS

AMERICAN WIRE GUAGE	AWG
CLOSED CIRCUIT TELEVISION	CCTV
CONDUIT	С
CURB AND GUTTER	C&G
DYNAMIC MESSAGE SIGN	DMS
EASTBOUND ROADWAY	EBR
EMERGENCY VEHICLE PREEMPTION	EVP
END ANCHOR	E/A
FLASHING BEACON	FB
GALVANIZED RIGID CONDUIT	GRC
GUARDRAIL	GR
HIGH DENSITY POLYETHYLENE	HDPE
HIGHWAY ADVISORY RADIO	HAR
INDIVIDUAL LOWERING DEVICE	ILD
INTELLIGENT TRANSPORTATION SYSTEM(S)	ITS
JUNCTION BOX	JB
LEFT	LT
LIGHT EMITTING DIODE	LED
LIGHT POLE	LP
LUMINAIRE	LUM
MILEPOST	MP
NORTHBOUND ROADWAY	NBR
NON-METALLIC CONDUIT	NMC
PAN TILT ZOOM	PTZ
PRIMARY FIBER DISTRIBUTION UNIT	PFDU
PROPERTY LINE	PL
PUBLIC ACCESS CAMERA	PAC
PULL BOX	РВ
RADIUS	R
RAILROAD	RR
RIGHT	RT
RIGHT OF WAY	ROW
ROADWAY	RDWY
SECONDARY FIBER DISTRIBUTION UNIT	SFDU
SCHOOL	SCH
SOUTHBOUND ROADWAY	SBR
STANDARD DRAWING	STD-DWG
STATION	STA
TRAFFIC CONTROL CENTER	TCC
TRAFFIC SIGNAL OPERATING PLAN	TSOP
TRANSPORTATION MANAGEMENT CENTER	TMC
TURNOUT	TO
VEHICLE DETECTION CAMERA	VDC
WESTBOUND ROADWAY	WBR

NOT FOR CONSTRUCTION





AVE W @ THOMPSON RD TRAFFIC SIGNAL AND ITS LEGEND

1ST 23-0362 NOT TO SCALE PROJ. MGR. 7-8-2024

JDS SHEET NO.

TS1

\$DATE\$

TRAFFIC SIGNAL NOTES

IN THE EVENT CONFLICTS OCCUR BETWEEN THE PROJECT TRAFFIC SIGNAL NOTES AND THE MUTCD, THE MUTCD WILL GOVERN.

NOTES THAT APPLY TO THIS PROJECT.

- WHEN THE CONTROLLER IS IN THE FLASHING MODE, THE VEHICULAR SIGNAL HEADS SHALL FLASH YELLOW ON 1ST AVENUE WEST, RED ON ALL CROSS STREETS, AND RED ON PROTECTED LEFT TURNS.
- 501. ALL EXISTING TRAFFIC CONTROL EQUIPMENT WHICH IS THE PROPERTY OF THE STATE INCLUDING SIGNAL HEADS, CONTROLLERS, POLES, AND MISCELLANEOUS HARDWARE SHALL BE REMOVED UPON COMPLETION OF THE NEW TRAFFIC CONTROL UNIT (TEMPORARY OR PERMANENT) AND STORED TO COMPLY WITH SECTION 730.03 OF THE STANDARD SPECIFICATIONS. THE SAME SHALL BE DELIVERED TO THE ALABAMA DEPARTMENT OF TRANSPORTATION AS DIRECTED BY THE ENGINEER.
- ALL EXISTING TRAFFIC CONTROL EQUIPMENT WHICH IS THE PROPERTY OF THE CITY OF ALABASTER INCLUDING SIGNAL HEADS, CONTROLLERS, POLES, AND MISCELLANEOUS HARDWARE SHALL BE REMOVED UPON COMPLETION OF THE NEW TRAFFIC CONTROL UNIT (TEMPORARY OR PERMANENT) AND STORED TO COMPLY WITH SECTION 730.03 OF THE STANDARD SPECIFICATIONS. THE SAME SHALL BE DELIVERED TO THE CITY OF ALABASTER AS DIRECTED BY THE ENGINEER.
- THE LOCATION OF THE POWER SOURCE AS SHOWN IN THE PLANS IS APPROXIMATE.

 THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF THE POWER SOURCE

 AND THE SHORTEST ROUTE TO SERVE THE TRAFFIC SIGNAL CONTROLLER CABINET

 AND LUMINAIRES.
- AS WORK BEGINS RELATED TO OR AFFECTING THE SIGNAL(S), WITHIN THE CONSTRUCTION LIMITS, THE CONTRACTOR SHALL ASSUME RESPONSIBILITY OF ALL EXISTING, TEMPORARY, AND REQUIRED SIGNAL(S). THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUAL OPERATION AND MAINTENANCE OF THE SIGNAL(S) UNTIL ALL SIGNAL WORK OR WORK AFFECTING THE SIGNAL(S) IS ACCEPTED BY THE CITY OF ALABASTER.
- THE CONTRACTOR SHALL INSTALL TEMPORARY SIGNAL(S) AS REQUIRED BY THE TEMPORARY TRAFFIC SIGNAL PLAN LAYOUT OR THE TRAFFIC CONTROL PLANS. WHEN TEMPORARY SIGNAL(S) ARE NOT REQUIRED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MODIFYING AND ADJUSTING THE EXISTING AND/OR REQUIRED SIGNAL(S) SO THAT ALL LANE SHIFTS. CLOSURES. AND ANY OTHER CHANGES TO THE ROADWAY DURING CONSTRUCTION ARE CONTROLLED BY THE EXISTING AND/OR REQUIRED SIGNAL(S).
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ALL UTILITY COMPANIES TO LOCATE ALL OVERHEAD AND UNDERGROUND UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT. DAMAGE TO UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE UTILITY COMPANY AND THE ENGINEER. THE CONTRACTOR SHALL BEAR ALL COST TO REPAIR ANY AND ALL DAMAGES TO THE UTILITIES CAUSED BY THE CONTRACTOR.
- THE CITY OF ALABASTER RESERVES THE RIGHT TO RESPOND
 TO TRAFFIC CONTROL UNIT(S) MALFUNCTIONS IN AN EMERGENCY OR NATURAL
 DISASTER. IN DOING SO THE CONTRACTOR'S LIABILITY AND RESPONSIBILITY
 RELATED TO MAINTAINING THE TRAFFIC UNIT(S) OR SYSTEM REMAINS IN EFFECT.
- THE CONTRACTOR SHALL HAVE THE APPROVAL OF THE ENGINEER PRIOR TO THE REMOVAL OF ANY EXISTING TRAFFIC CONTROL UNIT. THE CONTRACTOR SHALL NOT REMOVE AN EXISTING TRAFFIC CONTROL UNIT UNTIL THE REQUIRED TRAFFIC CONTROL UNIT IS INSTALLED AND COMPLETELY OPERATIONAL.

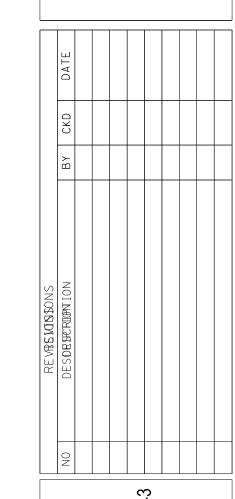
- 509. EACH REQUIRED TRAFFIC SIGNAL STRAIN POLE AND MAST ARM POLE MAY VARY IN LENGTH AND SIZE. THE CONTRACTOR SHALL ASCERTAIN THAT THE POLE HEIGHTS ARE SUFFICIENT TO PROVIDE THE REQUIRED VEHICULAR TRAFFIC SIGNAL CLEARANCE. EXTENSIONS FOR MOUNTING SIGNALS SHALL BE PROVIDED WHEN NECESSARY.
- 510. EACH MAST ARM MAY VARY IN LENGTH. THE CONTRACTOR SHALL ASCERTAIN THAT ALL ARM LENGTHS ARE SUFFICIENT SO THAT EACH VEHICULAR SIGNAL HEAD POSITION CONFORMS TO THE MUTCD.
- THE TRAFFIC SIGNAL POLE LOCATION(S) AS SHOWN IN THE PLANS IS(ARE)
 APPROXIMATE. THE CONTRACTOR SHALL COORDINATE THE POLE LOCATION(S) WITH
 THE ENGINEER. THE CONTRACTOR SHALL ASCERTAIN THAT THE FINAL POLE
 LOCATION(S) PROVIDE FOR THE VEHICULAR TRAFFIC SIGNAL HEADS TO MEET THE
 DISTANCE REQUIREMENTS TO THE STOP LINE AS REQUIRED BY THE MUTCD. WHEN
 PEDESTRIAN SIGNAL HEADS AND/OR PEDESTRIAN CROSSWALKS ARE INVOLVED THE
 SAME SAID POLE LOCATION(S) SHALL ALSO CONFORM TO THE RELATIVE SECTIONS OF
 THE MUTCD.
- THE CONTRACTOR SHALL LOCATE EACH REQUIRED AND RELOCATED VEHICULAR TRAFFIC SIGNAL HEAD ON THE SPAN WIRE OR MAST ARM SO THAT EACH HEAD IS LOCATED IN THE APPROACH LANE FOR WHICH IT APPLIES. LOCATION OF SIGNAL HEADS SHALL CONFORM TO THE MUTCD.
- 513. THE TRAFFIC SIGNAL STRAIN POLE LOCATION(S) AS SHOWN IN THE PLANS IS(ARE)
 APPROXIMATE. THE ENGINEER SHALL APPROVE ALL FOUNDATION LOCATIONS PRIOR
 TO THE CONTRACTOR EXCAVATING FOR EACH FOUNDATION.
- BALANCE ADJUSTERS SHALL BE INSTALLED ON TRAFFIC SIGNAL HEADS FOR PROPER AIM. THE CONTRACTOR SHALL ALIGN THE SIGNAL HEADS IN ACCORDANCE WITH THE MUTCD AND TO THE SATISFACTION OF THE ENGINEER.
- 515. A 12 INCH DRIP COIL WITH 3 LOOPS SHALL BE PROVIDED TO THE RIGHTS OF EACH VEHICULAR TRAFFIC SIGNAL HEAD. A DRIP LOOP SHALL BE FORMED SO THAT WATER CANNOT ENTER THE ENTRANCE CLAMP. THE WIRE SHALL ENTER THE CLAMP FROM THE BOTTOM OF THE DRIP LOOP.
- 516. WHEN PVC CONDUIT IS USED FROM THE CONTROLLER TO THE STEEL STRAIN POLE OR MAST ARM POLE. THE CONTRACTOR SHALL BOND THE CONTROLLER TO THE POLE WITH A #6-1C BONDING CABLE.
- 517. MARKING/WARNING TAPE SHALL BE BURIED OVER CONDUIT. THE TAPE SHALL BE
 4 INCH POLYETHYLENE, RED IN COLOR WITH BLACK LETTERING.
- 518. WHEN EXISTING LOOP WIRE AND VEHICLE LOOP DETECTORS ARE TO BE RETAINED AND REUSED, OR RELOCATED IN A NEW CONTROLLER CABINET, THE CONTRACTOR SHALL ASCERTAIN THE MANUFACTURER AND MODEL NUMBER OF EACH EXISTING DETECTOR AMPLIFIER AND PROVIDE A NEW WIRING HARNESS COMPLETELY WIRED IN THE CONTROLLER CABINET FOR EACH EXISTING DETECTOR AMPLIFIER.
- 519. WHEN SIGNAL TIMINGS ARE NOT INCLUDED IN THE PLANS FOR TIME BASE OR CLOSED LOOP SYSTEMS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HIRE A LICENSED PROFESSIONAL ENGINEER TO CALCULATE SIGNAL TIMINGS. THE COST OF CALCULATING SYSTEM TIMINGS SHALL BE A SUBSIDIARY OBLIGATION OF 730C.
- 520. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HIRE A LICENSED PROFESSIONAL ENGINEER TO INPUT THE TIMINGS AND FINE TUNE THE TIMINGS. THE COST OF INPUTTING AND FINE TUNING TIMINGS SHALL BE A SUBSIDIARY OBLIGATION OF 730C.

- 521.) SAIN ASSOCIATES SHALL BE RESPONSIBLE FOR INPUTTING AND FINE TUNING THE TIMINGS.
- 522. WHEN EXISTING SPAN WIRE THAT IS TO BE RETAINED HAS SAGGED. THE CONTRACTOR SHALL ADJUST THE SPAN WIRE SO THAT SIGNAL HEADS COMPLY WITH THE CLEARANCE SHOWN ON THE STANDARD DETAIL DRAWING.
- 523. A NOTICE OF INTENT FOR NPDES PERMIT COVERAGE HAS BEEN FILED WITH ADEM FOR THIS PROJECT. A COPY OF THE CONSTRUCTION BEST MANAGEMENT PRACTICES PLAN (CBMPP) IS AVAILABLE THROUGH OFFICE ENGINEER PRIOR TO BIDDING.
- THE CONTRACTOR SHALL PROVIDE A SET OF AS-BUILT PLANS TO THE CITY OF ALABASTER.

 525. THE CONTRACTOR SHALL INSTALL BACKPLATES WITH A 2 INCH FLUORESCENT YELLOW REFLECTIVE BORDER ON ALL EXISTING AND REQUIRED SIGNAL HEADS AS SHOWN ON PLANS. BACKPLATES ON REQUIRED SIGNAL HEADS SHALL BE PAID FOR AS A SUBSIDIARY OF 730P.
- 526. WHEN LUMINAIRES PAID FOR AS A SUBSIDIARY OF 730N ARE INCLUDED, THE RELATED PHOTODECTORS MAY BE MOUNTED ON THE TOP OR SIDE OF THE SIGNAL CABINET WHERE NO SHADOWS ARE EXPECTED TO BE CAST ON THE CABINET THAT WOULD IMPAIR THE FUNCTION OF THE PHOTOCELL WHEN COMPARED TO MOUNTING ON THE DISCONNECT POLE IN ITS TRADITIONAL LOCATION. WHERE THE EFFECT OF SHADOWS IS UNCERTAIN. THE CONTRACTOR SHALL MOUNT THE PHOTODECTOR IN ITS STANDARD LOCATION.

BACKPLATES ON EXISTING SIGNAL HEADS SHALL BE PAID FOR AS A SUBSIDIARY OF 7300.

WHEN IMSA SIGNAL CABLE 20-1 IS USED THAT IS 7C OR LARGER, CONTRACTOR SHALL CLEARLY TAG AND MARK IN THE CONTROLLER CABINET THE SIGNAL INDICATION EACH CONDUCTOR CONNECTS TO WHERE THE CONDUCTOR JACKET DOES NOT MATCH IN COLOR THE ASSOCIATED SIGNAL HEAD COLOR.



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TRAFFIC SIGNAL NOTES

AVE W @ THOMPSON RD

TRAFFIC

TO BY

CC

CITY OF

ALABAS

PROJ. MGR. DATE

JDS 7-8-2024

SHEET NO.

TS2

HOR REVIEW NOT FOR CONSTRUCTION

\$DATE\$

NOTES:

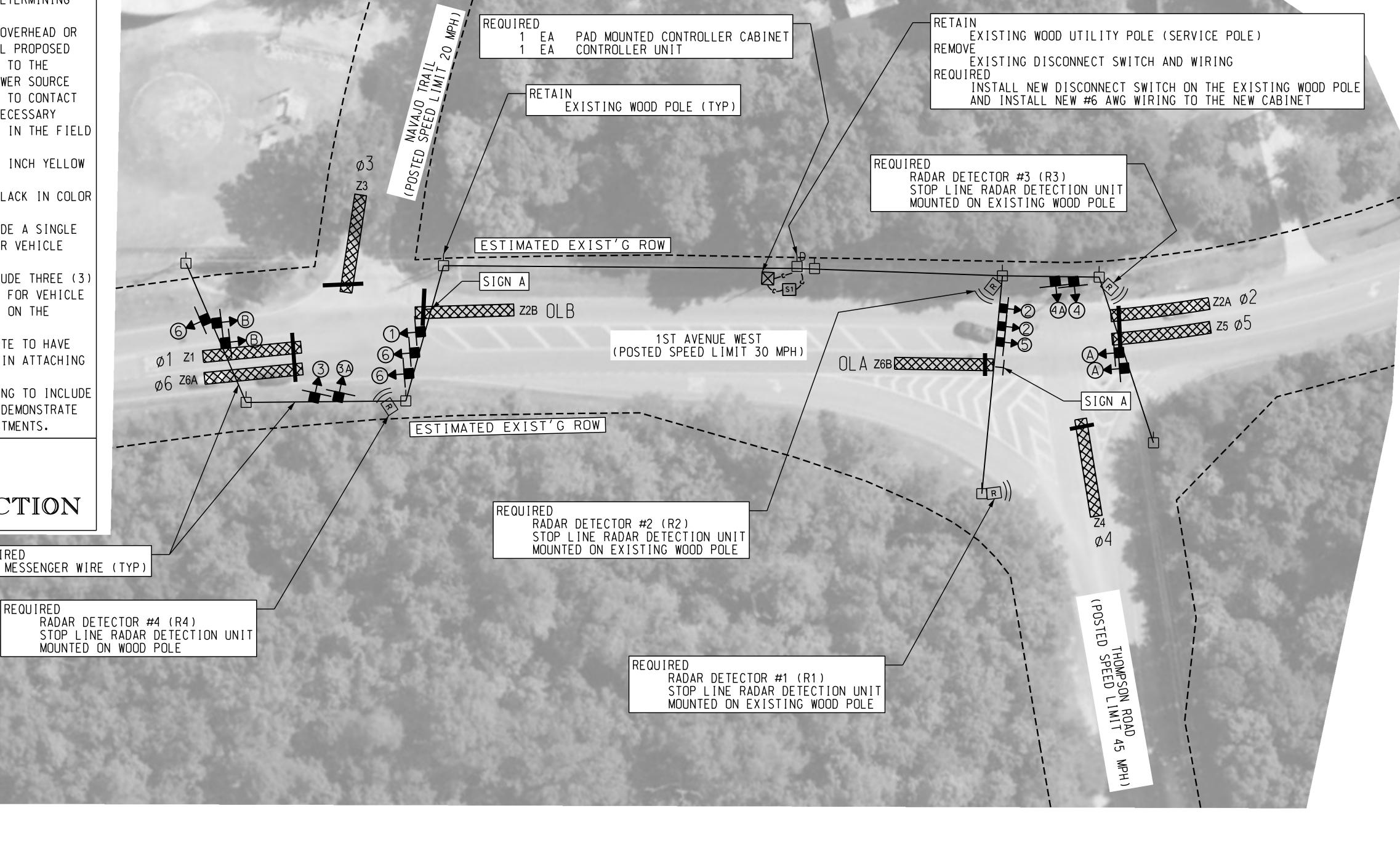
- 1. PAVEMENT MARKINGS ARE FOR ILLUSTRATIVE PURPOSES ONLY, UNLESS OTHERWISE NOTED.
- 2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE FINAL LOCATION OF THE POWER SOURCE.
- 3. THE CONTRACTOR SHALL VERIFY THERE ARE NO OVERHEAD OR UNDERGROUND UTILITIES CONFLICTING WITH ALL PROPOSED DESIGN ELEMENTS INCLUDING BUT NOT LIMITED TO THE LOCATION OF THE CONTROLLER/CABINET AND POWER SOURCE POLE. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONTACT EACH OF THE UTILITY OWNERS AND MAKE THE NECESSARY ARRANGEMENTS TO HAVE EACH UTILITY SPOTTED IN THE FIELD BY THE OWNERS BEFORE ANY EXCAVATION.
- 4. ALL SIGNAL HEAD BACKPLATES SHALL HAVE A 2 INCH YELLOW RETROREFLECTIVE STRIP AROUND THE BORDER.
- 5. VEHICULAR SIGNAL HEADS SHALL BE LED AND BLACK IN COLOR WITH VISORS.
- 6. THE NAVAJO TRAIL INTERSECTION SHALL INCLUDE A SINGLE WAVETRONIX XP21 SENSOR, OR EQUIVALENT, FOR VEHICLE DETECTION.
- 7. THE THOMPSON ROAD INTERSECTION SHALL INCLUDE THREE (3) WAVETRONIX MATRIX SENSORS, OR EQUIVALENT, FOR VEHICLE DETECTION. THESE SENSORS SHALL BE MOUNTED ON THE EXISTING WOOD POLES.
- 8. THE CONTRACTOR SHALL CONTACT AND COORDINATE TO HAVE WAVETRONIX TECHNICIANS ON-SITE TO ASSIST IN ATTACHING THE DETECTOR UNIT TO THE WOOD POLES.
- 9. CONTRACTOR SHALL COORDINATE A FIELD MEETING TO INCLUDE CITY OF ALABASTER AND SAIN ASSOCIATES TO DEMONSTRATE DETECTION SYSTEM AND MAKE NECESSARY ADJUSTMENTS.

PRELIMINARY NOT FOR CONSTRUCTION

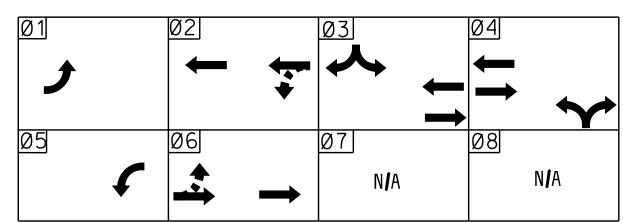
REQUIRED

REQUIRED

TRAFFIC SIGNAL LAYOUT 1ST AVENUE WEST AT THOMPSON ROAD/NAVAHO TRAIL



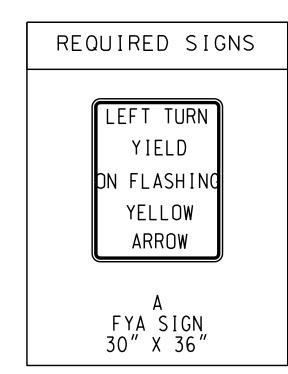
PHASING DIAGRAM

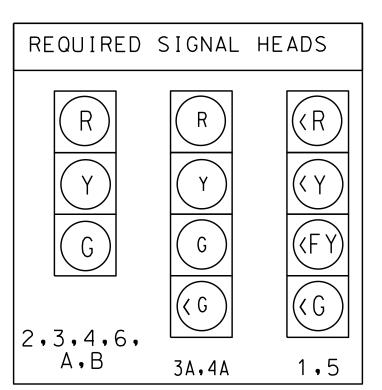


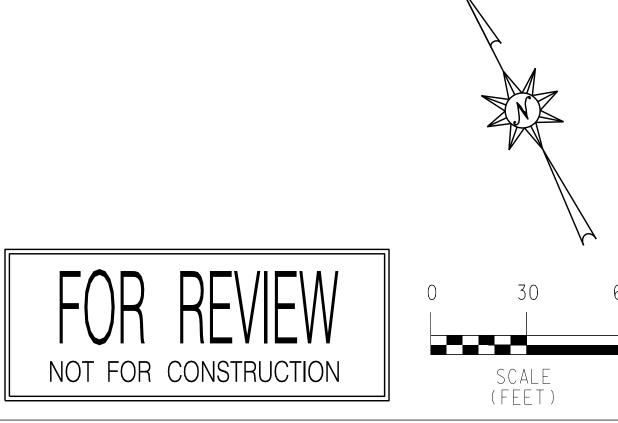
OLA = 3 + 6OLB = 2 + 4 + 5

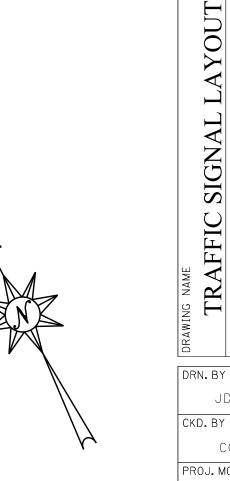
ESTIMATED EQUIPMENT AND
TERIAL SCHEDULE (LUMP SUM)
DESCRIPTION
3/8 in. MESSENGER CABLE
#14 SIGNAL CABLE IMSA 20-1
BACKPLATES WITH HARDWARE
MISCELLANEOUS HARDWARE
FYA SIGN W/ HARDWARE

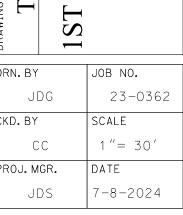
RADAR DETECTOR CHART								
				FROM				
ZONE	PHASE	DETECTOR	SIZE	STOP BAR	ZONE TYPE	DELAY		
1	1	R4	6 X 50	-	-	-		
2A	2	R3	6 X 50	-	-	-		
2B	OLB	R4	6 X 50	-	-	-		
3	3	R4	6 X 50	-	1	-		
4	4	R1	6 X 50	-	-	-		
5	5	R3	6 X 50	-	-	-		
6A	6	R4	6 X 50	-	1	-		
6B	OLA	R2	6 X 50	-	-	-		











W @ THOMPSON RD

SHEET NO. TS3

TRAFFIC SIGNAL DETAILS

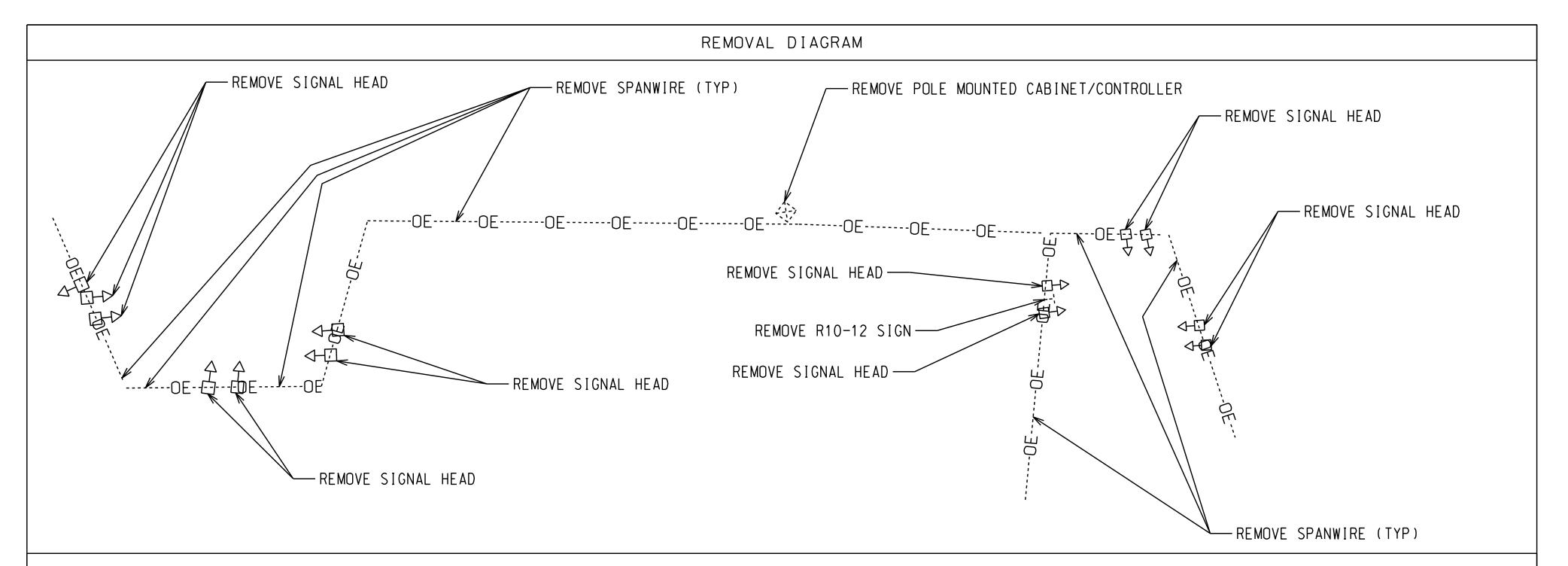
ESTIMATED QUANTITIES **						
ITEM #	DESCRIPTION	UNITS	QTY			
730A-000	Removal of Existing Traffic Control Unit	Lump Sum	1			
730C-000	Furnishing and Installing Traffic Control Unit	Lump Sum	1			
730K-001	Traffic Signal Junction Box, Type S1	Each	1			
730L-002	1", Metallic, Conduit	Linear Foot	50			
730L-005	2", Non-Metallic, Conduit	Linear Foot	150			
730P-022	Vehicular Signal Head, 12 Inch, 3 Section, Type LED	Each	11			
730P-023	Vehicular Signal Head, 12 Inch, 4 Section, Type LED	Each	4			
730R-041	Controller Assembly, Type NEMA, 16 Channels	Each	1			
730R-162	Controller Cabinet, Type Nema, 16 Channels	Each	1			
730T-010	Electrical Power Service Assembly with Wood Pole	Each	1			
*730U-400	Radar Detection System	Lump Sum	1			

В	- STD. DWG. NO. T.S.D730-1 - STD. DWG. NO. T.S.D730-2
C.	
_	- STD. DWG. NO. T.S.D730-2A
D ·	- STD. DWG. NO. T.S.D730-6
E -	- STD. DWG. NO. T.S.D730-10
F -	- STD. DWG. NO. T.S.D730-13
G ·	- STD. DWG. NO. T.S.D730-17
Η -	- STD. DWG. NO. T.S.D730-18

^{**} QUANTITIES SHOWN ARE ESTIMATES ONLY. THE CONTRACTOR IS RESPONSIBLE FOR CALCULATING/ESTIMATING THEIR OWN QUANTITIES.

CONDUIT AND CONDUCTOR SCHEDULE					
CONDUIT	TYPE	CONDUCTOR	FROM	ТО	
3 - 2"	3 - 2" PVC		CONTROLLER	JUNCTION BOX#1	
3 - 2"	PVC		JUNCTION BOX #1	SIGNAL POLE	
1-1"	RGS	3#6-1c 120V	DISCONNECT SWITCH	CONTROLLER	
-	-	RADAR POWER/DATA CABLE	CONTROLLER	EACH DETECTOR UNIT	
-	-	#14-10c IMSA 20-1	CONTROLLER	SIGNAL HEAD 1,5	
-	-	#14-4c IMSA 20-1	SIGNAL HEAD 1	SIGNAL HEAD 6	
-	-	#14-4c IMSA 20-1	SIGNAL HEAD 5	SIGNAL HEAD 2	
-	-	#14-7c IMSA 20-1	CONTROLLER	SIGNAL HEAD 3A,4A	
-	-	#14-4c IMSA 20-1	SIGNAL HEAD 3A	SIGNAL HEAD 3	
-	-	#14-4c IMSA 20-1	SIGNAL HEAD 4A	SIGNAL HEAD 4	
-	-	#14-4c IMSA 20-1	CONTROLLER	SIGNAL HEAD B	
-	-	#14-4c IMSA 20-1	CONTROLLER	SIGNAL HEAD A	

			SUG	GESTED C	ONTROLLE	ER TIMING	S			
PHASE	MIN GREEN	PASSAGE	YELLOW	ALL RED	MAX 1	MAX 2	PED WALK	PED DON'T WALK	RECALL	NON LOCK
1	5	3.0	2.5	3.0	25	25	-	-	-	-
2	30	3.0	4.7	1.7	45	45	-	-	MIN	-
3	5	3.0	2.5	2.7	15	15	-	-	-	-
4	5	3.0	2.5	3.0	30	30	-	-	-	-
5	5	3.0	2.5	2.7	35	40	-	-	-	-
6	20	3.0	4.0	1.7	35	40	-	-	MIN	-
OLA	6*	-	4.0	2.0	-	-	-	-	-	-
OLB	6*	-	4.0	2.0	-	-	-	-	-	-
OLA = 3+6										
OLB = 2+4+	-5									
*TRAILING	GREEN									



NOTES:

REMOVE:
EXISTING TRAFFIC SIGNAL HEADS (13)
EXISTING SPAN MOUNTED R10-12 TRAFFIC SIGN (1)
EXISTING POLE MOUNTED SIGNAL CABINET/CONTROLLER (1)

RETAIN: ALL EXISTING WOOD POLES (9)



PROJ. MGR. 7-8-2024 JDS

SHEET NO.

NOT FOR CONSTRUCTION

^{*} SEE SHEET TS3 NOTES 6,7,8, AND 9.

GENERAL TRAFFIC CONTROL PLAN NOTES

DENOTES NOTES THAT APPLY TO THIS PROJECT

- (700) THE TRAFFIC CONTROL PLAN IS DEVELOPED IN CONFORMANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES PART 6, 2009 EDITION. THE TRAFFIC CONTROL DEVICES INDICATED REPRESENT CONDITIONS KNOWN DURING PLAN DEVELOPMENT. IN THE EVENT ACTUAL PHYSICAL CONDITIONS WARRANT ADDITIONAL TRAFFIC CONTROL DEVICES, THEY SHALL BE INSTALLED IN CONFORMANCE WITH THE M.U.T.C.D. PART 6 AS DIRECTED BY THE ENGINEER. COST SHALL BE PAID FOR UNDER THE APPROPRIATE PAY
- (701) ALL BLACK ON ORANGE CONSTRUCTION SIGNS SHALL BE FABRICATED USING TYPE XI FLUORESCENT ORANGE REFLECTIVE SHEETING MATERIAL FOR THE SIGN BACKGROUND.
- (702) DURING NON-WORKING HOURS NO EQUIPMENT OR MATERIAL SHALL BE PARKED OR STORED CLOSER THAN 30 FEET TO THE EDGE OF ANY ROADWAY CARRYING TRAFFIC. WHEN THIS IS NOT PRACTICAL, IT SHALL BE PLACED IN AN AREA APPROVED BY THE ENGINEER AND DELINEATED BY REFLECTORIZED DRUMS. THIS INCLUDES STORAGE OF TRAFFIC CONTROL DEVICES SUCH AS TRAILER MOUNTED OR OTHER TEMPORARY SIGNS, BARRICADES, DRUMS, ETC. WHICH ARE NOT IN USE DURING NON-WORKING HOURS. TO BE FURNISHED BY THE CONTRACTOR WITHOUT COST TO THE ALDOT. (SEE SKETCH ON SHEET
- (703) WHERE THE LOCATION OF A REQUIRED SIGN FALLS IN A DRIVEWAY, SIDEWALK, BRIDGE, ETC., OR WHERE THE VISIBILITY OF A SIGN IS LÍMITED TO THE TRAVELING PUBLIC, THE LOCATION SHALL BE ADJUSTED AS DIRECTED BY THE ENGINEER.
- 704) THE CONTRACTOR IS TO REMOVE, RELOCATE OR COVER DURING CONSTRUCTION AND THEN RESET OR UNCOVER UPON COMPLETION OF A PARTICULAR SECTION ANY CONFLICTING IN-PLACE ROADWAY SIGNS AND DELINEATORS, AS DIRECTED BY THE ENGINEER. SIGNS REQUIRING REMOVAL SHALL BE STOCKPILED AS DIRECTED BY THE ENGINEER AND SHALL BECOME PROPERTY OF THE ALDOT. COST SHALL BE A SUBSIDIARY OBLIGATION OF
- 705 DURING ALL PHASES OF WORK, NON-APPLICABLE PAVEMENT STRIPING OR MARKINGS SHALL BE REMOVED AND APPROPRIATE PAVEMENT STRIPING OR MARKINGS SHALL BE PLACED AS EXPEDITIOUSLY AS PRACTICAL, BUT IN ALL CASES, SHALL BE IN PLACE BY NIGHTFALL ON ANY ROADWAY CARRYING TRAFFIC, EXCEPT ON SHORT TERM OPERATIONS WHERE IT IS DETERMINED BY THE ENGINEER, THAT SUCH REMOVAL AND REPLACEMENT IS MORE HAZARDOUS THAN LEAVING EXISTING MARKINGS IN PLACE. COST OF ANY REMOVAL SHALL BE PAID FOR UNDER ITEM 701D OR AS A SUBSIDIARY OBLIGATION OF ITEM 701C.
- 706 OMITTED
- (707) THE CONTRACTOR SHALL PLACE ALL ADVANCE WARNING SIGNS BEFORE PROCEEDING WITH HIS WORK. SIGNS SHALL BE PLACED IN ORDER, IN THE DIRECTION OF TRAFFIC AND REMOVED IN REVERSE ORDER
- 708 ALL VEHICLES, EQUIPMENT, PERSONNEL (EXCEPT FLAGGERS), AND THEIR ACTIVITIES, ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
- (709) THE CONTRACTOR SHALL MAINTAIN AT LEAST ONE ACCESS TO BUSINESSES AND RESIDENCES DURING ALL PHASES OF CONSTRUCTION.
- (710) CONSTRUCTION SIGNS MOUNTED ON TEMPORARY SUPPORTS SHALL BE MOUNTED AT A MINIMUM HEIGHT OF 5 FEET.
- (711) FLAGGERS SHALL BE PROPERLY ATTIRED, EQUIPPED WITH STAFF MOUNTED STOP/SLOW PADDLES IN SIGHT OF EACH OTHER, OR HAVE DIRECT COMMUNICATION AT ALL TIMES. FLAGGER STATION LOCATION MAY BE VARIED FROM THOSE SHOWN BASED ON ROADWAY ALIGNMENT AND CONDITIONS AT THE TIME OF THE LANE CLOSURE
- (712) FLAGGERS ARE TO BE USED WHEN DIRECTED BY THE ENGINEER. SIGNS SHALL BE PLACED AT THE APPROPRIATE TIME, AND SHALL BE COVERED OR REMOVED WHEN FLAGGERS ARE NOT ON DUTY AND DURING NON-WORKING
- 713 FOR MOVING OPERATIONS, THE TRAFFIC CONES MAY BE DELETED IF THE FLAGGERS ARE IN SIGHT OF EACH OTHER, OR IF A PILOT CAR IS USED ON A TWO LANE ROADWAY.
- 714 OMITTED
- (715) ALL CONTRACTOR'S EMPLOYEES' PERSONAL VEHICLES, AND CONTRACTOR'S EOUIPMENT NOT IN OPERATION, SHALL BE PARKED A MINIMUM OF THIRTY (30) FEET FROM THE TRAVELED WAY DURING WORKING HOURS, AS NOT TO CREATE A HAZARD.
- (716) THE TRAFFIC CONTROL PLAN IS NOT ALL INCLUSIVE. THE TCP PROVIDES SEVERAL DETAILED DRAWINGS INDICATING THE TRAFFIC CONTROL NECESSARY FOR THE DIFFERENT CONSTRUCTION ACTIVITIES ANTICIPATED FOR THIS PROJECT. THE CONTRACTOR SHALL SELECT THE DETAILED DRAWING THAT BEST FITS THE ACTIVITY TO BE PERFORMED.
- 717 OMITTED
- 718 REQUIRED TEMPORARY ROUTE MARKER ASSEMBLIES THAT ARE TO BE LOCATED IN THE VICINITY OF EXISTING ROUTE MARKERS SHOULD BE PLACED ALONG SIDE OF THOSE ALREADY IN PLACE. SOME EXISTING ROUTE MARKERS MAY HAVE TO BE COVERED OR REMOVED, AS DIRECTED BY THE ENGINEER. COST SHALL BE A SUBSIDIARY OBLIGATION OF ITEM 740B.

- 719 RA-1 (REBUILD ALABAMA) SIGNS SHALL BE REQUIRED FOR EVERY PROJECT RA-1 SIGNS SHALL BE PLACED AT THE BEGINNING OF THE WORK LIMITS OF THE SUBJECT PROJECT ROUTE. RA-1 SIGNS SHALL BE POSTED ON THE RIGHT-HAND SIDE OF THE ROADWAY ON THEIR OWN SUPPORT SYSTEM. THE RA-1 SIGNS SHALL BE REMOVED UPON COMPLETION OF THE PROJECT.
- (720) ALL TRAFFIC CONTROL DEVICES THAT ARE NOT APPLICABLE AT ANY SPECIFIC TIME SHALL BE COVERED OR REMOVED AS DIRECTED BY THE
- 721 OMITTED
- 722 OMITTED
- 723 THE CONTRACTOR SHALL MAKE PROVISIONS FOR THE SAFETY OF PEDESTRIAN TRAFFIC CROSSING THE WORK ZONES DURING CONSTRUCTION.
- 724 OMITTED
- 725 ALL SIGNS SHALL BE POST-MOUNTED IF THE WORK PERIOD EXCEEDS FOUR DAYS, EXCEPT FOR THOSE SIGNS WHICH ARE MOUNTED ON BARRICADES. FOR REPEATED DAY OPERATIONS, SIGNS MAY BE MOUNTED ON TEMPORARY SUPPORTS AND REMOVED AT THE COMPLETION OF THE DAY'S OPERATION.
- 726 W8 SIGNS INTENDED TO WARN MOTORISTS OF SURFACE CONDITIONS EXTENDING FOR GREATER THAN 1 MILE SHALL BE PLACED PRIOR TO THE BEGINNING OF SURFACE CONDITION AND AT 1 MILE INCREMENTS THEREAFTER, WITH THE EXCEPTION SPECIFIED IN NOTE 727.
- 727 DURING THE WIDENING OR RESURFACING OF ANY ROADWAY CARRYING TRAFFIC, THE CONTRACTOR SHALL ADVISE THE MOTORISTS OF ANY EDGE OF PAVEMENT DROP-OFFS 3 INCHES OR GREATER BY PLACING SHOULDER DROP-OFF SIGNS EVERY 1/2 MILE BEGINNING PRIOR TO THE WIDENING OR RESURFACING. REQUIRED SHOULDER WORK TO ELIMINATE THE DROP-OFFS SHALL BE PURSUED IN AN EXPEDITIOUS MANNER FOLLOWING THE WIDENING AND/OR RESURFACING.
- 728 A DIFFERENCE IN ELEVATION OF APPROXIMATELY 2 INCHES OR LESS AT THE CENTERLINE MAY BE ALLOWED DURING NON-WORKING HOURS WITHOUT ADDITIONAL TRAFFIC CONTROL. SPECIAL CONDITIONS MAY EXIST WHERE PROTECTION SHOULD BE PROVIDED WHERE THE DIFFERENCE IS 2 INCHES OR
- 729) SIGNS ON TEMPORARY SUPPORTS ARE TO BE REMOVED OR COVERED WHEN NO WORK IS BEING PERFORMED OR AT THE COMPLETION OF THE DAY'S OPERATION.
- 730 OMITTED
- 731 OMITTED
- (732) CHANNELIZING DRUMS SHOULD BE PLACED ON 10 FOOT INTERVALS IN RADII.
- (733) CHANNELIZING DRUMS PLACED TO PROTECT COMPLETED WORK NOT OPEN TO TRAFFIC, SHOULD BE SPACED AT 50 FOOT INTERVALS.
- 734 CHANNELIZING DRUMS PLACED IN THE EXCAVATED AREA AHEAD OF PAVING OPERATIONS, SHOULD BE SPACED AT 50 FOOT INTERVALS.
- (735) CHANNELIZING DRUMS PLACED ON PAVEMENT DURING WORKING HOURS SHALL BE SHIFTED TO THE EDGE OF SHOULDER DURING NON-WORKING HOURS AND DURING PEAK PERIODS.
- (736) CHANNELIZING DRUMS SHOULD BE PLACED ON 25 FOOT INTERVALS THROUGHOUT ALL TAPERS.
- (737) CHANNELIZING DEVICES SHALL EXTEND TO A POINT WHERE THEY ARE VISIBLE TO APPROACHING TRAFFIC.
- 738 OMITTED
- 739 OMITTED
- 740 OMITTED
- 741 FOR DIVIDED ROADWAYS, THE REQUIRED ADVANCE WARNING SIGNS SHALL BE POSTED ON BOTH THE RIGHT AND LEFT SIDE OF THE ROADWAY.
- (742) THE CONTRACTOR SHALL CLOSE THE LANE ADJACENT TO THE WORK AREA ANYTIME WORK OUTSIDE THE EXISTING TRAVEL LANES ENCROACHES WITHIN 2 FEET OF THE EXISTING EDGE OF PAVEMENT.
- 743 OMITTED
- (744) THE TRANSITION TAPER LENGTH (L) IS SHOWN IN TABLE 6C-4, AND THE BUFFER LENGTH IS SHOWN IN TABLE 6C-2 OF THE MUTCD, PART 6, 2009
- 745 OMITTED
- 746 UNEVEN LANES SIGNS SHALL BE COVERED OR REMOVED WHEN NO UNEVEN PAVEMENT CONDITIONS EXIST
- 747 MOVING OPERATIONS SHALL BE CONFINED TO ONE LANE IN THE DIRECTION OF TRAFFIC.

REFERENCE	FISCAL	SHEET
PROJECT NO	YEAR	NO

- 748 R16-3 (WHEN WORKERS ARE PRESENT BEGIN HIGHER FINES) AND R16-3A (END HIGHER FINES) SIGNS SHALL BE REQUIRED FOR EVERY PROJECT ON STATE ROUTES AND INTERSTATE HIGHWAYS. THESE SIGNS SHALL BE POSTED AT THE BEGINNING AND END OF THE PROJECT WITH AN R2-1 (REGULATORY SPEED SIGN) ALWAYS FOLLOWING THE R16-3 SIGN. R16-3B (HIGHER FINES ZONE) AND R2-1 SIGNS SHALL BE POSTED AT MAXIMÙM INTERVALS OF THREE MILES THROUGHOUT THE PROJECT LIMITS.
- 749 WHEN A CONSTRUCTION WORK ZONE SPEED LIMIT REDUCTION IS NOT REQUIRED AT THE END OF THE WORK DAY, THE CONTRACTOR SHALL COVER OR REMOVE THE REDUCED R2-1 (REGULATORY SPEED SIGNS) AND THE W3-5B (REDUCED SPEED AHEAD) SIGNS UNLESS OTHERWISE DIRECTED BY THE ENGINEER
- 750 DURING REPLACEMENT OF GUARDRAIL AND/OR GUARDRAIL END ANCHORS, A REFLECTORIZED DRUM SHALL BE PLACED BEFORE THE END OF ANY EXPOSED GUARDRAIL AT NIGHT WHERE THE GUARDRAIL END ANCHOR CANNOT BE REPLACED IN ONE DAY'S TIME.
- CONSTRUCTION SIGNS MOUNTED ON A SINGULAR OR DUAL SOUARE TUBULAR OR U-CHANNEL POST SHALL BE INSTALLED AS SHOWN ON SPECIAL DRAWING NOS. IHS-710-21 AND IHS-710-23.
- THE CONTRACTOR AND THE CONSTRUCTION ENGINEER SHALL DISCUSS AND PLAN FOR THE HANDLING OF TRAFFIC FOR ALL HOLIDAYS BEFORE ANY WORK BEGINS. UNLESS OTHERWISE PRE-APPROVED BY THE REGION ENGINEER, THE FOLLOWING SHALL HOLD:

THE CONTRACTOR SHALL NOT HAVE A LANE CLOSURE DURING THE FOLLOWING PERIODS UNLESS OTHERWISE DIRECTED BY THE **ENGINEER OR ALDOT:**

- FOR CHRISTMAS AND NEW YEARS DAY: FROM 11:59 PM DECEMBER 23 THROUGH 6:00 AM JANUARY 2.
- FOR NATIONAL MEMORIAL DAY AND LABOR DAY: FROM 12:00 NOON THE FRIDAY BEFORE THE HOLIDAY THROUGH 11:59 PM THE DAY OF THE HOLIDAY.
- FOR INDEPENDENCE DAY (THE 4TH OF JULY): FROM 12:00 NOON THE DAY BEFORE THÉ HOLIDAY THROUGH 11:59 PM THE DAY OF THE HOLIDAY.
- FOR THANKSGIVING DAY: FROM 12:00 NOON THE WEDNESDAY BEFORE THANKSGIVING DAY THROUGH 11:59 PM THE SUNDAY FOLLOWING THANKSGIVING DAY.
- ANY OTHER STATE HOLIDAYS WILL BE HANDLED AS APPROVED BY THE PROJECT ENGINEER.
- THE CONTRACTOR SHALL COORDINATE WITH THE ENGINEER AND LOCAL GOVERNMENT ON TRAFFIC AND/OR WORK RESTRICTIONS FOR LOCAL HOLIDAYS OR EVENTS NOT LISTED ON ALDOT'S LIST OF OFFICIAL STATE HOLIDAYS.
- THE CONTRACTOR SHALL NOT HAVE A LANE CLOSURE FROM 6:30AM THROUGH 8:30AM AND 3:00PM THROUGH 6:00PM. MONDAY THROUGH FRIDAY.

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DRN. BY 23-0362 CKD. BY SCALE ROJ. MGR.

JDS SHEET NO.

-8-2024

LABAMA DEPARTMENT OF TRANSPORTATION 1409 COLISEUM BOULEVARD MONTGOMERY, AL 36130-3050

\$DATE\$

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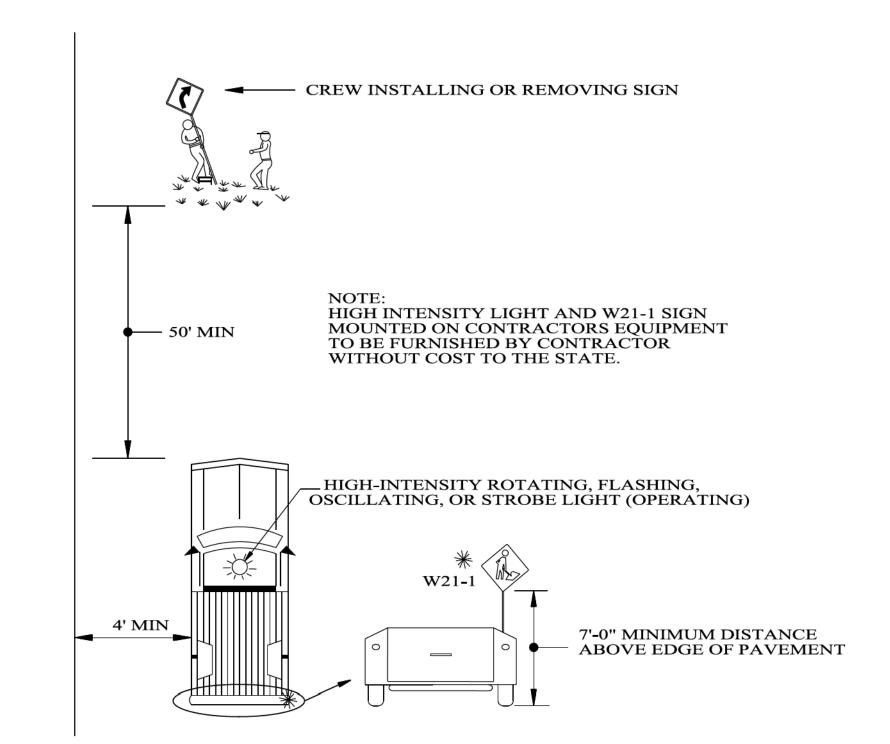
Modified Note 748 for the addition of the R13-3b sign on 9-28-2021 by D.J.W. Aligned text for Note 746 on 12-8-2021 by D.J.W.

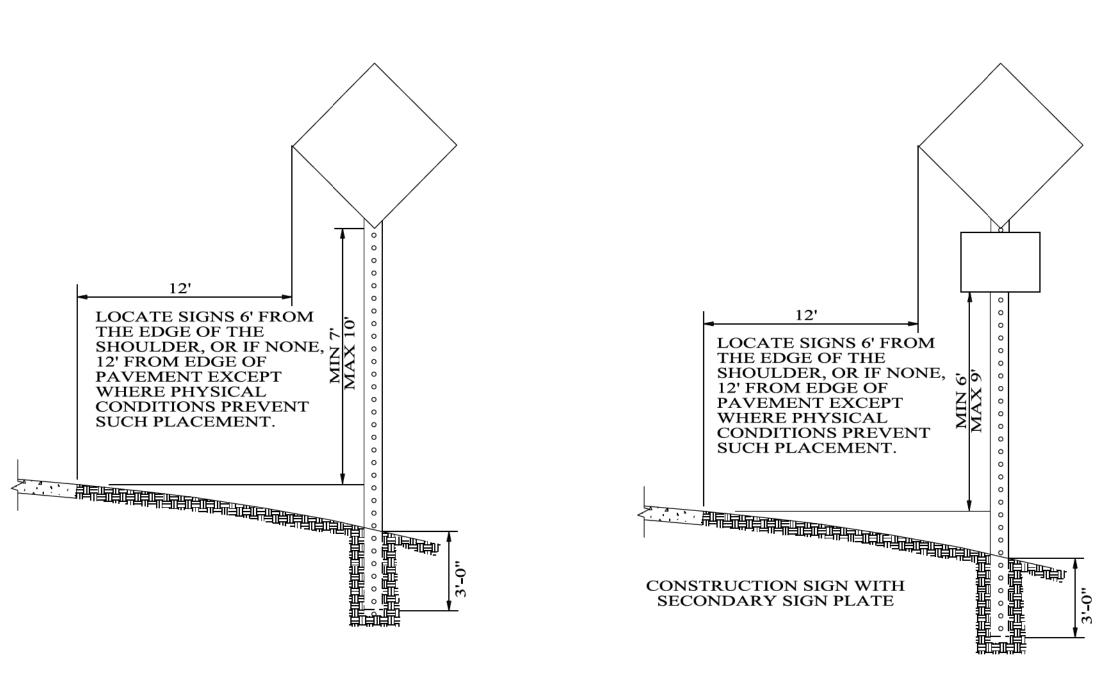
Bureau Std Engr: D.J.W. DRAWN BY: DATE DRAWN: _ EVISED DATE: 12-8-2021

DESIGN BUREAU SPECIAL DRAWING GENERAL TRAFFIC CONTROL

TEMPORARY TRAFFIC CONTROL PLAN SHEET

REFERENCE	FISCAL	SHEET
PROJECT NO	YEAR	NO

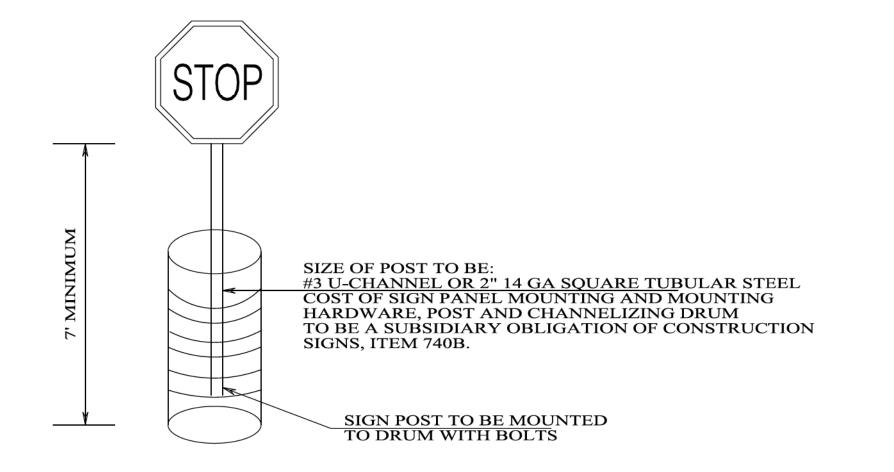




IF THE CONTRACTOR CHOOSES TO SPLICE THE POSTS FOR THE REQUIRED POST MOUNTED CONSTRUCTION SIGNS, THEY SHALL BE SPLICED AS SHOWN ON DRAWING IHS-710-23.

TYPICAL METHOD FOR INSTALLING OR **REMOVING CONSTRUCTION SIGNS**

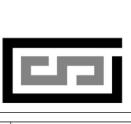
HEIGHT AND LATERAL LOCATION OF POST MOUNTED CONSTRUCTION SIGNS



DETAIL FOR DRUM MOUNTED CONSTRUCTION SIGNS

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NOT FOR CONSTRUCTION



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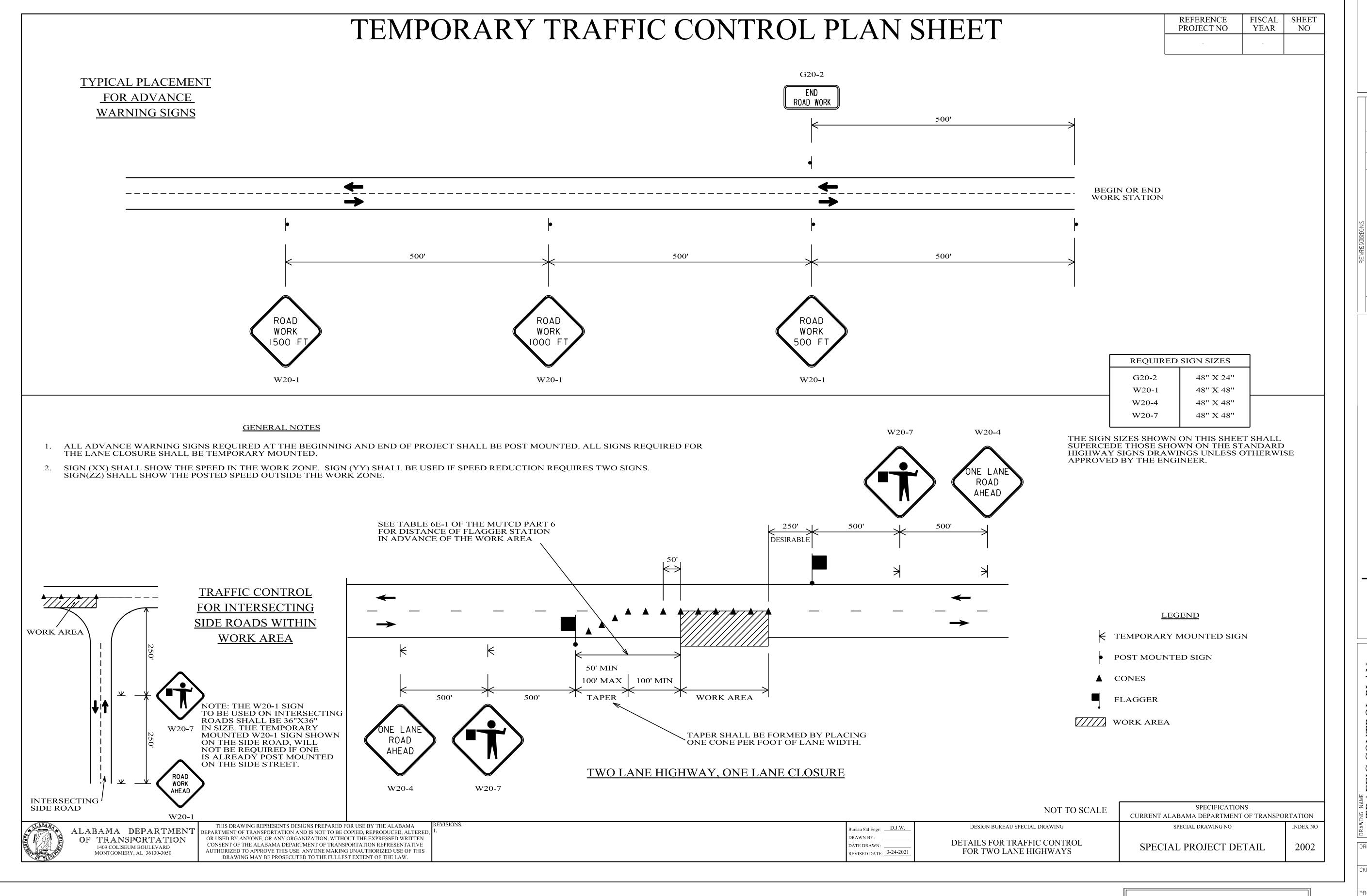
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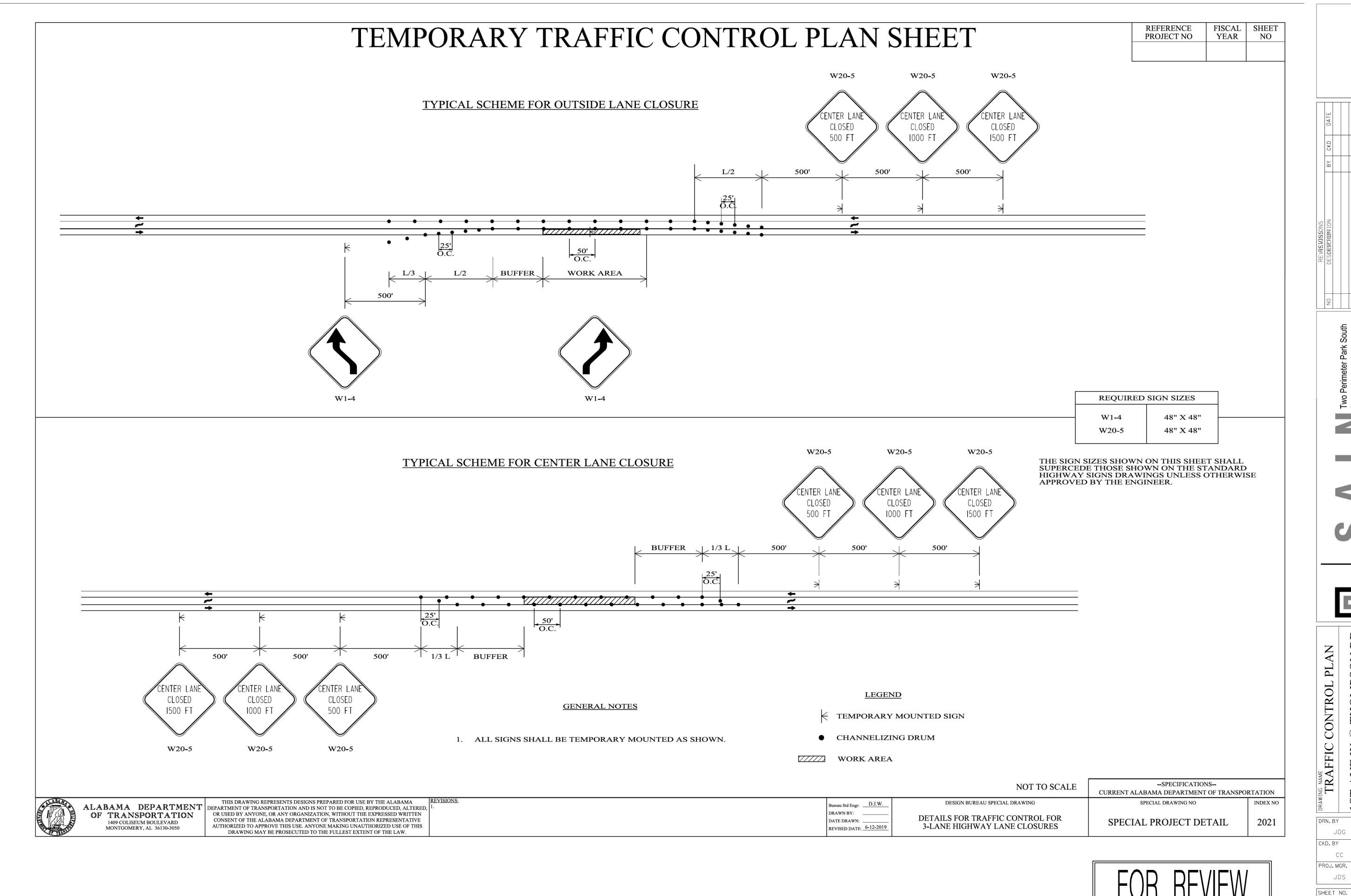


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