

TRAFFIC SIGNAL MODIFICATION PLANS

FOR

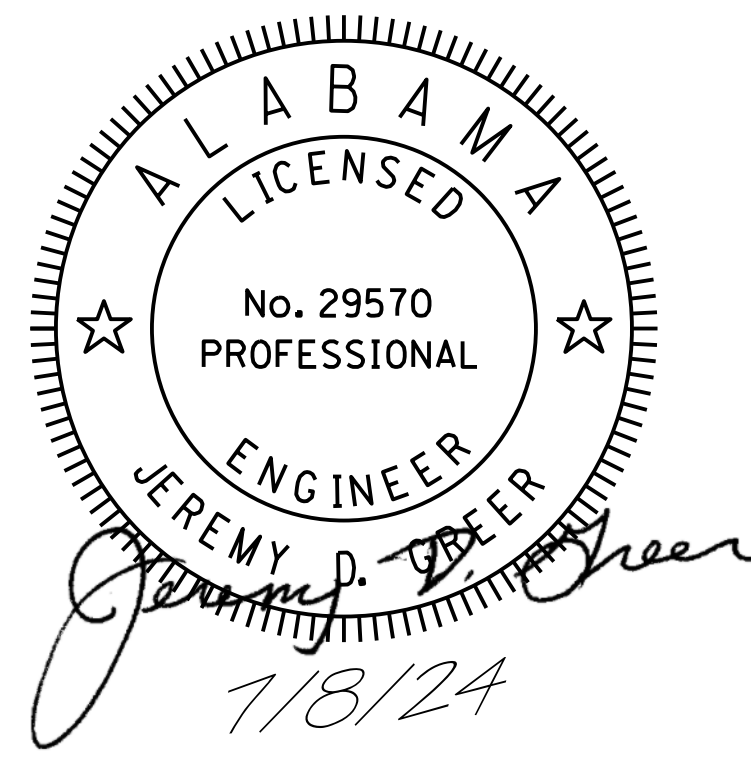
1ST AVE W AT THOMPSON ROAD/NAVAHO TRAIL

ALABASTER, ALABAMA



VICINITY MAP
NOT TO SCALE

FOR REVIEW
NOT FOR CONSTRUCTION



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PREPARED BY:











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SUITE 500 EAST
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PHONE: 205-940-6420
SA# 23-0362



PREPARED FOR

CITY OF ALABASTER
1953 MUNICIPAL WAY
ALABASTER, ALABAMA 35007


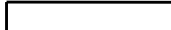
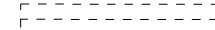
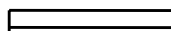




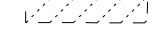





ELECTRICAL BOXES

	EXISTING	REQUIRED
METALLIC PULL BOX		
FIBER OPTIC COMMBX TYPE F1		
FIBER OPTIC COMMBX TYPE F2		
TRAFFIC SIGNAL JUNCTION BOX		

CABLE IN CONDUIT

	EXISTING	REQUIRED
FIBER OPTIC CABLE IN CONDUIT (UNDERGROUND)	----- F0 -----	———— F0 ————
FIBER OPTIC CABLE IN CONDUIT (UNDERGROUND WITH CONCRETE)	----- F0WC -----	———— F0WC ————
FIBER OPTIC CABLE IN CONDUIT (BRIDGE ATTACHED)	--●-- f0 --●--	——●—— F0 ——●——
FIBER OPTIC CABLE (AERIAL INSTALLATION)	----- 0f0 -----	———— 0F0 ————
INTERCONNECT CABLE IN CONDUIT (UNDERGROUND)	----- U1 -----	———— U1 ————
INTERCONNECT CABLE (AERIAL INSTALLATION)	----- 01 -----	———— 01 ————
CONDUIT	----- C -----	———— C ————
ENCASEMENT		
OVERHEAD ELECTRIC	----- OE -----	———— OE ————
BURIED ELECTRIC	----- BE -----	———— BE ————

VEHICULAR DETECTORS

	EXISTING	REQUIRED
PRESENCE LOOP DETECTOR		
QUADRUPOLE LOOP DETECTOR		
6' x 6' LOOP DETECTOR		
VEHICLE DETECTION CAMERA		
VIDEO DETECTION ZONE		
RADAR DETECTION UNIT		
RADAR DETECTION ZONE		

CABINETS

	EXISTING	REQUIRED
CABINET		

CAMERAS













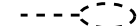

	EXISTING	REQUIRED
CCTV CAMERA, FIXED		
CCTV CAMERA, PTZ		

MISCELLANEOUS EQUIPMENT

	EXISTING	REQUIRED
TRAFFIC SIGNAL HEAD		
TRAFFIC SIGNAL HEAD (3-SECTION) W/ BACKPLATE		
TRAFFIC SIGNAL HEAD (4-SECTION) W/ BACKPLATE		
TRAFFIC SIGNAL HEAD (5-SECTION) W/ BACKPLATE		
PEDESTRIAN SIGNAL HEAD		
8 FOOT PEDESTAL POLE AND PEDESTRIAN SIGNAL HEAD		
PEDESTAL MOUNTED FLASHING WARNING SIGNAL WITH SIGN		
PEDESTAL MOUNTED ILLUMINATED SCHOOL ZONE SIGN		
PUSH BUTTON ASSEMBLY		
SPAN/MASTARM MOUNTED SIGN		
OMNI DIRECTIONAL ANTENNA		
DIRECTIONAL ANTENNA		
EMERGENCY VEHICLE PREEMPTION SENSOR		
BLANKOUT MESSAGE SIGN		
TRAFFIC CONTROL CENTER		
HIGHWAY ADVISORY RADIO		
HUB BUILDING		
DYNAMIC MESSAGE SIGN (OVERHEAD)		
DYNAMIC MESSAGE SIGN (ROADSIDE)		
DYNAMIC MESSAGE SIGN (CANTILEVER)		
BROADBAND RADIO SYSTEM		
WIRELESS SENSOR		
WIRELESS REPEATER		
DIGITAL RADIO		

(NOTE: # INDICATES SIGNAL HEAD NUMBER)

POLES

	EXISTING	REQUIRED
METAL POLE		
CONCRETE POLE		
METAL MASTARM POLE		
CLASS 3 WOOD SERVICE POLE WITH DISCONNECT		
WOOD POLE		
DOWN GUY		
LUMINAIRE		

ABBREVIATIONS

AMERICAN WIRE GUAGE	AWG
CLOSED CIRCUIT TELEVISION	CCTV
CONDUIT.....	C
CURB AND GUTTER	C&G
DYNAMIC MESSAGE SIGN	DMS
EASTBOUND ROADWAY	EBR
EMERGENCY VEHICLE PREEMPTION	EVP
END ANCHOR	E/A
FLASHING BEACON	FB
GALVANIZED RIGID CONDUIT.....	GRC
GUARDRAIL	GR
HIGH DENSITY POLYETHYLENE	HDPE
HIGHWAY ADVISORY RADIO	HAR
INDIVIDUAL LOWERING DEVICE	ILD
INTELLIGENT TRANSPORTATION SYSTEM(S)....	ITS
JUNCTION BOX	JB
LEFT	LT
LIGHT EMITTING DIODE	LED
LIGHT POLE	LP
LUMINAIRE	LUM
MILEPOST.....	MP
NORTHBOUND ROADWAY	NBR
NON-METALLIC CONDUIT	NMC
PAN TILT ZOOM.....	PTZ
PRIMARY FIBER DISTRIBUTION UNIT.....	PFDU
PROPERTY LINE	PL
PUBLIC ACCESS CAMERA.....	PAC
PULL BOX.....	PB
RADIUS	R
RAILROAD	RR
RIGHT	RT
RIGHT OF WAY.....	ROW
ROADWAY	RDWY
SECONDARY FIBER DISTRIBUTION UNIT	SFDU
SCHOOL.....	SCH
SOUTHBOUND ROADWAY	SBR
STANDARD DRAWING	STD-DWG
STATION	STA
TRAFFIC CONTROL CENTER	TCC
TRAFFIC SIGNAL OPERATING PLAN	TSOP
TRANSPORTATION MANAGEMENT CENTER	TMC
TURNOUT	TO
VEHICLE DETECTION CAMERA	VDC
WESTBOUND ROADWAY	WBR

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[illegible]

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ASSOCIATES



TRAFFIC SIGNAL AND ITS LEGEND

1ST AVE W @ THOMPSON RD

CITY OF ALABASTER
ALABASTER, ALABAMA

WRN. BY JDG	JOB NO. 23-0362
CHKD. BY CC	SCALE NOT TO SCALE
PROJ. MGR. JDS	DATE 7-8-2024

HEET NO.

TS1

TRAFFIC SIGNAL NOTES

IN THE EVENT CONFLICTS OCCUR BETWEEN THE PROJECT TRAFFIC SIGNAL NOTES
AND THE MUTCD, THE MUTCD WILL GOVERN.

○ NOTES THAT APPLY TO THIS PROJECT.

500. WHEN THE CONTROLLER IS IN THE FLASHING MODE, THE VEHICULAR TRAFFIC HEADS SHALL FLASH YELLOW ON 1ST AVENUE WEST, RED ON ALL CROSS STREETS, AND RED ON PROTECTED LEFT TURNS.

501. ALL EXISTING TRAFFIC CONTROL EQUIPMENT WHICH IS THE PROPERTY OF THE STATE INCLUDING SIGNAL HEADS, CONTROLLERS, POLES, AND MISCELLANEOUS HARDWARE SHALL BE REMOVED UPON COMPLETION OF THE NEW TRAFFIC CONTROL UNIT (TEMPORARY OR PERMANENT) AND STORED TO COMPLY WITH SECTION 730.03 OF THE STANDARD SPECIFICATIONS. THE SAME SHALL BE DELIVERED TO THE ALABAMA DEPARTMENT OF TRANSPORTATION AS DIRECTED BY THE ENGINEER.

502. ALL EXISTING TRAFFIC CONTROL EQUIPMENT WHICH IS THE PROPERTY OF THE CITY OF ALABASTER INCLUDING SIGNAL HEADS, CONTROLLERS, POLES, AND MISCELLANEOUS HARDWARE SHALL BE REMOVED UPON COMPLETION OF THE NEW TRAFFIC CONTROL UNIT (TEMPORARY OR PERMANENT) AND STORED TO COMPLY WITH SECTION 730.03 OF THE STANDARD SPECIFICATIONS. THE SAME SHALL BE DELIVERED TO THE CITY OF ALABASTER AS DIRECTED BY THE ENGINEER.

503. THE LOCATION OF THE POWER SOURCE AS SHOWN IN THE PLANS IS APPROXIMATE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF THE POWER SOURCE AND THE SHORTEST ROUTE TO SERVE THE TRAFFIC SIGNAL CONTROLLER CABINET AND LUMINAIRES.

504. AS WORK BEGINS RELATED TO OR AFFECTING THE SIGNAL(S), WITHIN THE CONSTRUCTION LIMITS, THE CONTRACTOR SHALL ASSUME RESPONSIBILITY OF ALL EXISTING, TEMPORARY, AND REQUIRED SIGNAL(S). THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUAL OPERATION AND MAINTENANCE OF THE SIGNAL(S) UNTIL ALL SIGNAL WORK OR WORK AFFECTING THE SIGNAL(S) IS ACCEPTED BY THE CITY OF ALABASTER.

505. THE CONTRACTOR SHALL INSTALL TEMPORARY SIGNAL(S) AS REQUIRED BY THE TEMPORARY TRAFFIC SIGNAL PLAN LAYOUT OR THE TRAFFIC CONTROL PLANS. WHEN TEMPORARY SIGNAL(S) ARE NOT REQUIRED, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MODIFYING AND ADJUSTING THE EXISTING AND/OR REQUIRED SIGNAL(S) SO THAT ALL LANE SHIFTS, CLOSURES, AND ANY OTHER CHANGES TO THE ROADWAY DURING CONSTRUCTION ARE CONTROLLED BY THE EXISTING AND/OR REQUIRED SIGNAL(S).

506. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ALL UTILITY COMPANIES TO LOCATE ALL OVERHEAD AND UNDERGROUND UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT. DAMAGE TO UTILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE UTILITY COMPANY AND THE ENGINEER. THE CONTRACTOR SHALL BEAR ALL COST TO REPAIR ANY AND ALL DAMAGES TO THE UTILITIES CAUSED BY THE CONTRACTOR.

507. THE CITY OF ALABASTER RESERVES THE RIGHT TO RESPOND TO TRAFFIC CONTROL UNIT(S) MALFUNCTIONS IN AN EMERGENCY OR NATURAL DISASTER. IN DOING SO THE CONTRACTOR'S LIABILITY AND RESPONSIBILITY RELATED TO MAINTAINING THE TRAFFIC UNIT(S) OR SYSTEM REMAINS IN EFFECT.

508. THE CONTRACTOR SHALL HAVE THE APPROVAL OF THE ENGINEER PRIOR TO THE REMOVAL OF ANY EXISTING TRAFFIC CONTROL UNIT. THE CONTRACTOR SHALL NOT REMOVE AN EXISTING TRAFFIC CONTROL UNIT UNTIL THE REQUIRED TRAFFIC CONTROL UNIT IS INSTALLED AND COMPLETELY OPERATIONAL.

509. EACH REQUIRED TRAFFIC SIGNAL STRAIN POLE AND MAST ARM POLE MAY VARY IN LENGTH AND SIZE. THE CONTRACTOR SHALL ASCERTAIN THAT THE POLE HEIGHTS ARE SUFFICIENT TO PROVIDE THE REQUIRED VEHICULAR TRAFFIC SIGNAL CLEARANCE. EXTENSIONS FOR MOUNTING SIGNALS SHALL BE PROVIDED WHEN NECESSARY.

510. EACH MAST ARM MAY VARY IN LENGTH. THE CONTRACTOR SHALL ASCERTAIN THAT ALL ARM LENGTHS ARE SUFFICIENT SO THAT EACH VEHICULAR SIGNAL HEAD POSITION CONFORMS TO THE MUTCD.

511. THE TRAFFIC SIGNAL POLE LOCATION(S) AS SHOWN IN THE PLANS IS(ARE) APPROXIMATE. THE CONTRACTOR SHALL COORDINATE THE POLE LOCATION(S) WITH THE ENGINEER. THE CONTRACTOR SHALL ASCERTAIN THAT THE FINAL POLE LOCATION(S) PROVIDE FOR THE VEHICULAR TRAFFIC SIGNAL HEADS TO MEET THE DISTANCE REQUIREMENTS TO THE STOP LINE AS REQUIRED BY THE MUTCD. WHEN PEDESTRIAN SIGNAL HEADS AND/OR PEDESTRIAN CROSSWALKS ARE INVOLVED THE SAME SAID POLE LOCATION(S) SHALL ALSO CONFORM TO THE RELATIVE SECTIONS OF THE MUTCD.

512. THE CONTRACTOR SHALL LOCATE EACH REQUIRED AND RELOCATED VEHICULAR TRAFFIC SIGNAL HEAD ON THE SPAN WIRE OR MAST ARM SO THAT EACH HEAD IS LOCATED IN THE APPROACH LANE FOR WHICH IT APPLIES. LOCATION OF SIGNAL HEADS SHALL CONFORM TO THE MUTCD.

513. THE TRAFFIC SIGNAL STRAIN POLE LOCATION(S) AS SHOWN IN THE PLANS IS(ARE) APPROXIMATE. THE ENGINEER SHALL APPROVE ALL FOUNDATION LOCATIONS PRIOR TO THE CONTRACTOR EXCAVATING FOR EACH FOUNDATION.

514. BALANCE ADJUSTERS SHALL BE INSTALLED ON TRAFFIC SIGNAL HEADS FOR PROPER AIM. THE CONTRACTOR SHALL ALIGN THE SIGNAL HEADS IN ACCORDANCE WITH THE MUTCD AND TO THE SATISFACTION OF THE ENGINEER.

515. A 12 INCH DRIP COIL WITH 3 LOOPS SHALL BE PROVIDED TO THE RIGHTS OF EACH VEHICULAR TRAFFIC SIGNAL HEAD. A DRIP LOOP SHALL BE FORMED SO THAT WATER CANNOT ENTER THE ENTRANCE CLAMP. THE WIRE SHALL ENTER THE CLAMP FROM THE BOTTOM OF THE DRIP LOOP.

516. WHEN PVC CONDUIT IS USED FROM THE CONTROLLER TO THE STEEL STRAIN POLE OR MAST ARM POLE, THE CONTRACTOR SHALL BOND THE CONTROLLER TO THE POLE WITH A #6-1C BONDING CABLE.

517. MARKING/WARNING TAPE SHALL BE BURIED OVER CONDUIT. THE TAPE SHALL BE 4 INCH POLYETHYLENE, RED IN COLOR WITH BLACK LETTERING.

518. WHEN EXISTING LOOP WIRE AND VEHICLE LOOP DETECTORS ARE TO BE RETAINED AND REUSED, OR RELOCATED IN A NEW CONTROLLER CABINET, THE CONTRACTOR SHALL ASCERTAIN THE MANUFACTURER AND MODEL NUMBER OF EACH EXISTING DETECTOR AMPLIFIER AND PROVIDE A NEW WIRING HARNESS COMPLETELY WIRED IN THE CONTROLLER CABINET FOR EACH EXISTING DETECTOR AMPLIFIER.

519. WHEN SIGNAL TIMINGS ARE NOT INCLUDED IN THE PLANS FOR TIME BASE OR CLOSED LOOP SYSTEMS, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HIRE A LICENSED PROFESSIONAL ENGINEER TO CALCULATE SIGNAL TIMINGS. THE COST OF CALCULATING SYSTEM TIMINGS SHALL BE A SUBSIDIARY OBLIGATION OF 730C.

520. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HIRE A LICENSED PROFESSIONAL ENGINEER TO INPUT THE TIMINGS AND FINE TUNE THE TIMINGS. THE COST OF INPUTTING AND FINE TUNING TIMINGS SHALL BE A SUBSIDIARY OBLIGATION OF 730C.

521. SAIN ASSOCIATES SHALL BE RESPONSIBLE FOR INPUTTING AND FINE TUNING THE TIMINGS.

522. WHEN EXISTING SPAN WIRE THAT IS TO BE RETAINED HAS SAGGED, THE CONTRACTOR SHALL ADJUST THE SPAN WIRE SO THAT SIGNAL HEADS COMPLY WITH THE CLEARANCE SHOWN ON THE STANDARD DETAIL DRAWING.

523. A NOTICE OF INTENT FOR NPDES PERMIT COVERAGE HAS BEEN FILED WITH ADEM FOR THIS PROJECT. A COPY OF THE CONSTRUCTION BEST MANAGEMENT PRACTICES PLAN (CBMPP) IS AVAILABLE THROUGH OFFICE ENGINEER PRIOR TO BIDDING.

524. THE CONTRACTOR SHALL PROVIDE A SET OF AS-BUILT PLANS TO THE CITY OF ALABASTER.

525. THE CONTRACTOR SHALL INSTALL BACKPLATES WITH A 2 INCH FLUORESCENT YELLOW REFLECTIVE BORDER ON ALL EXISTING AND REQUIRED SIGNAL HEADS AS SHOWN ON PLANS. BACKPLATES ON REQUIRED SIGNAL HEADS SHALL BE PAID FOR AS A SUBSIDIARY OF 730P. BACKPLATES ON EXISTING SIGNAL HEADS SHALL BE PAID FOR AS A SUBSIDIARY OF 730O.

526. WHEN LUMINAIRES PAID FOR AS A SUBSIDIARY OF 730N ARE INCLUDED, THE RELATED PHOTODETECTORS MAY BE MOUNTED ON THE TOP OR SIDE OF THE SIGNAL CABINET WHERE NO SHADOWS ARE EXPECTED TO BE CAST ON THE CABINET THAT WOULD IMPAIR THE FUNCTION OF THE PHOTOCELL WHEN COMPARED TO MOUNTING ON THE DISCONNECT POLE IN ITS TRADITIONAL LOCATION. WHERE THE EFFECT OF SHADOWS IS UNCERTAIN, THE CONTRACTOR SHALL MOUNT THE PHOTODETECTOR IN ITS STANDARD LOCATION.

527. WHEN IMSA SIGNAL CABLE 20-1 IS USED THAT IS 7C OR LARGER, CONTRACTOR SHALL CLEARLY TAG AND MARK IN THE CONTROLLER CABINET THE SIGNAL INDICATION EACH CONDUCTOR CONNECTS TO WHERE THE CONDUCTOR JACKET DOES NOT MATCH IN COLOR THE ASSOCIATED SIGNAL HEAD COLOR.

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[illegible]

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TRAFFIC SIGNAL NOTES

1ST AVE W @ THOMPSON RD

CITY OF ALABASTER
ALABASTER, ALABAMA

TURN BY JDG	JOB NO. 23-0362
CKD BY CC	SCALE NOT TO SCALE
PROJ. MGR. JDS	DATE 7-8-2024

SHEET NO.

S2

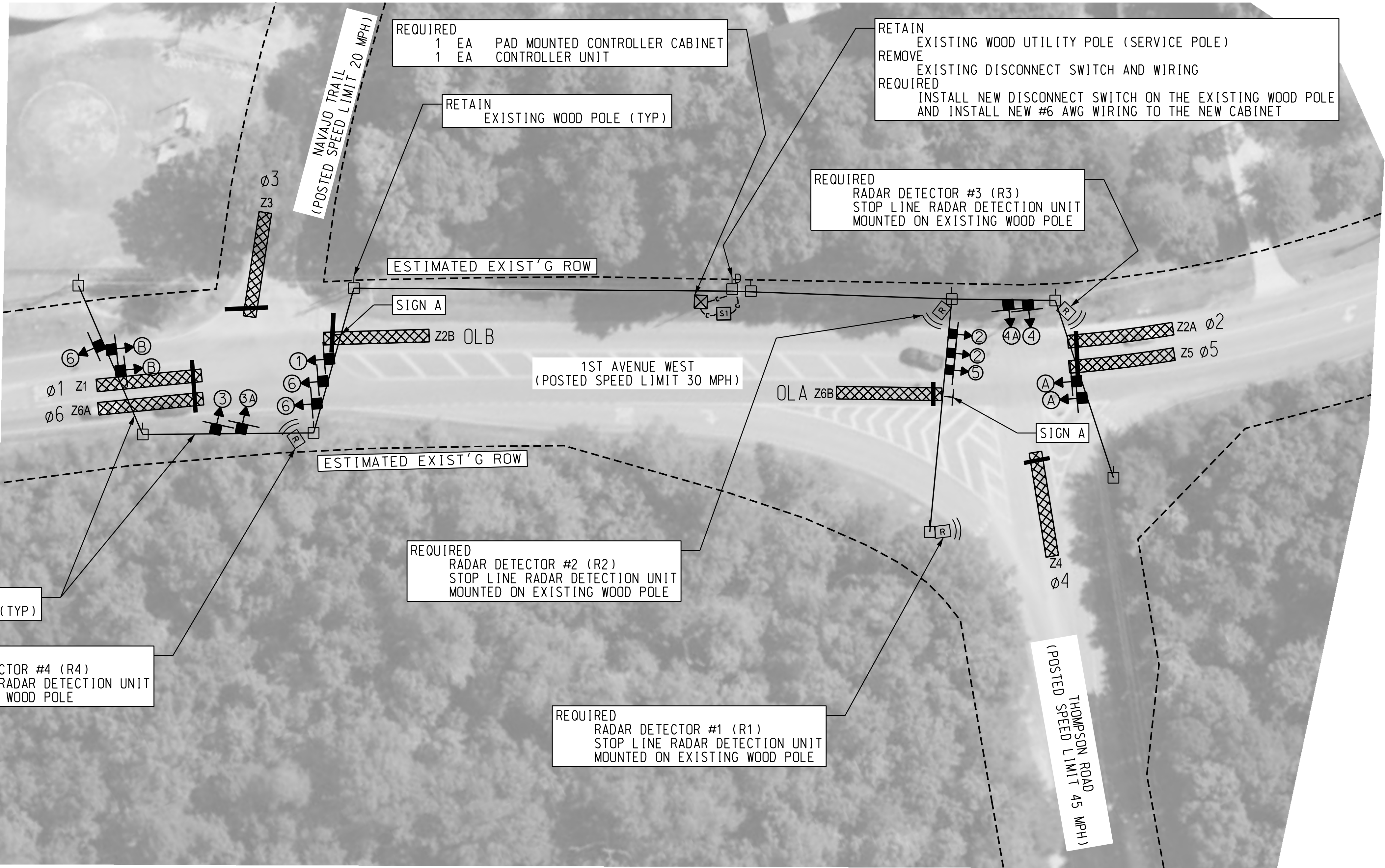
NOTES:

- PAVEMENT MARKINGS ARE FOR ILLUSTRATIVE PURPOSES ONLY, UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE FINAL LOCATION OF THE POWER SOURCE.
- THE CONTRACTOR SHALL VERIFY THERE ARE NO OVERHEAD OR UNDERGROUND UTILITIES CONFLICTING WITH ALL PROPOSED DESIGN ELEMENTS INCLUDING BUT NOT LIMITED TO THE LOCATION OF THE CONTROLLER/CABINET AND POWER SOURCE POLE. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONTACT EACH OF THE UTILITY OWNERS AND MAKE THE NECESSARY ARRANGEMENTS TO HAVE EACH UTILITY SPOTTED IN THE FIELD BY THE OWNERS BEFORE ANY EXCAVATION.
- ALL SIGNAL HEAD BACKPLATES SHALL HAVE A 2 INCH YELLOW RETROREFLECTIVE STRIP AROUND THE BORDER.
- VEHICULAR SIGNAL HEADS SHALL BE LED AND BLACK IN COLOR WITH VISORS.
- THE NAVAJO TRAIL INTERSECTION SHALL INCLUDE A SINGLE WAVETRONIX XP21 SENSOR, OR EQUIVALENT, FOR VEHICLE DETECTION.
- THE THOMPSON ROAD INTERSECTION SHALL INCLUDE THREE (3) WAVETRONIX MATRIX SENSORS, OR EQUIVALENT, FOR VEHICLE DETECTION. THESE SENSORS SHALL BE MOUNTED ON THE EXISTING WOOD POLES.
- THE CONTRACTOR SHALL CONTACT AND COORDINATE TO HAVE WAVETRONIX TECHNICIANS ON-SITE TO ASSIST IN ATTACHING THE DETECTOR UNIT TO THE WOOD POLES.
- CONTRACTOR SHALL COORDINATE A FIELD MEETING TO INCLUDE CITY OF ALABASTER AND SAIN ASSOCIATES TO DEMONSTRATE DETECTION SYSTEM AND MAKE NECESSARY ADJUSTMENTS.

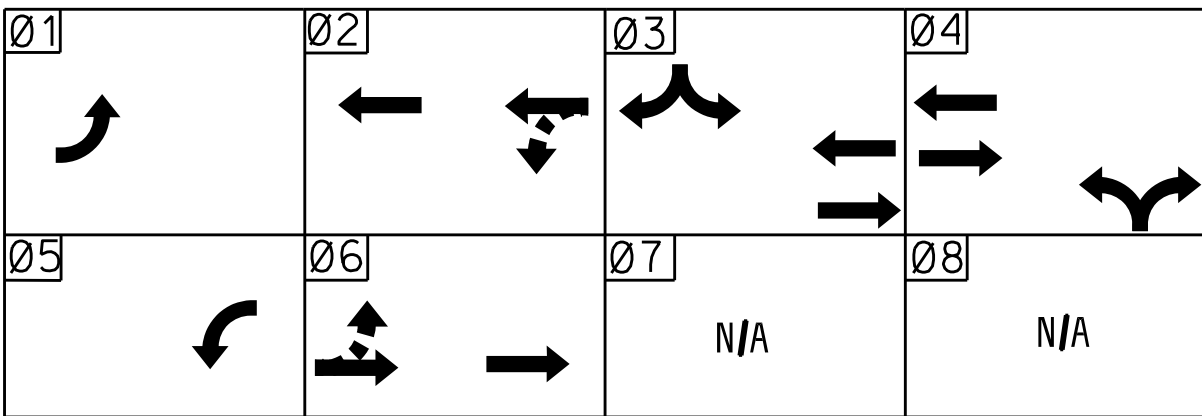
PRELIMINARY
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TRAFFIC SIGNAL LAYOUT

1ST AVENUE WEST AT THOMPSON ROAD/NAVAHO TRAIL



PHASING DIAGRAM

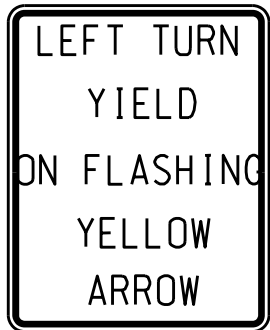


OLA = 3 + 6
OLB = 2 + 4 + 5

ESTIMATED EQUIPMENT AND MATERIAL SCHEDULE (LUMP SUM)	
DESCRIPTION	
3/8 in. MESSENGER CABLE	
#14 SIGNAL CABLE IMSA 20-1	
BACKPLATES WITH HARDWARE	
MISCELLANEOUS HARDWARE	
FYA SIGN W/ HARDWARE	

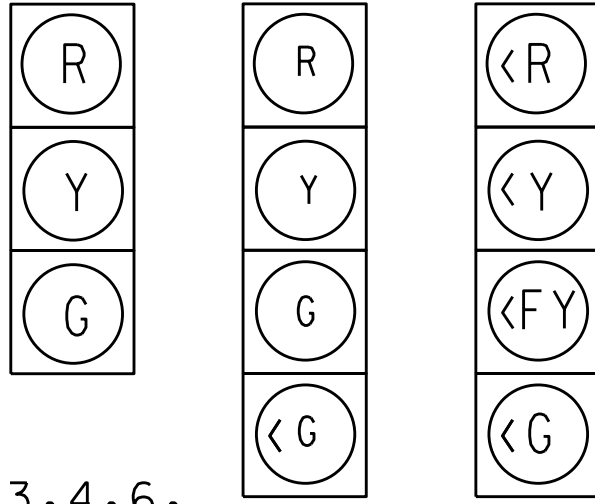
RADAR DETECTOR CHART						
ZONE	PHASE	DETECTOR	SIZE	FROM STOP BAR	ZONE TYPE	DELAY
1	1	R4	6 X 50	-	-	-
2A	2	R3	6 X 50	-	-	-
2B	OLB	R4	6 X 50	-	-	-
3	3	R4	6 X 50	-	-	-
4	4	R1	6 X 50	-	-	-
5	5	R3	6 X 50	-	-	-
6A	6	R4	6 X 50	-	-	-
6B	OLA	R2	6 X 50	-	-	-

REQUIRED SIGNS



A
FYA SIGN
30" X 36"

REQUIRED SIGNAL HEADS

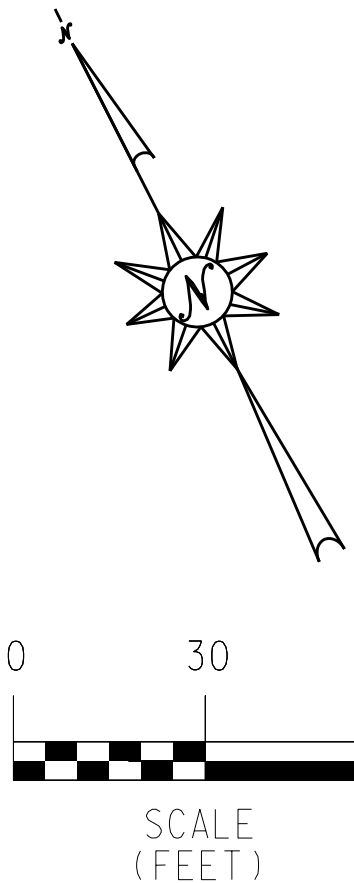


2, 3, 4, 6,
A, B

3A, 4A

1, 5

FOR REVIEW
NOT FOR CONSTRUCTION



DRAWING NAME
TRAFFIC SIGNAL LAYOUT

1ST AVE W @ THOMPSON RD

FOR
CITY OF ALABASTER
ALABASTER, ALABAMA

DRN. BY JDG	JOB NO. 23-0362
CKD. BY CC	SCALE 1" = 30'
PROJ. MGR. JDS	DATE 7-8-2024

SHEET NO.

TS3

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SAIN
ASSOCIATES



NO	REVISIONS DESCRIPTION	BY	CHK	DATE

TRAFFIC SIGNAL DETAILS

ESTIMATED QUANTITIES **			
ITEM #	DESCRIPTION	UNITS	QTY
730A-000	Removal of Existing Traffic Control Unit	Lump Sum	1
730C-000	Furnishing and Installing Traffic Control Unit	Lump Sum	1
730L-001	Traffic Signal Junction Box, Type S1	Each	1
730L-002	1", Metallic, Conduit	Linear Foot	50
730L-005	2", Non-Metallic, Conduit	Linear Foot	150
730P-022	Vehicular Signal Head, 12 Inch, 3 Section, Type LED	Each	11
730P-023	Vehicular Signal Head, 12 Inch, 4 Section, Type LED	Each	4
730R-041	Controller Assembly, Type NEMA, 16 Channels	Each	1
730R-162	Controller Cabinet, Type Nema, 16 Channels	Each	1
730T-010	Electrical Power Service Assembly with Wood Pole	Each	1
*730U-400	Radar Detection System	Lump Sum	1

* SEE SHEET TS3 NOTES 6,7,8, AND 9.

** QUANTITIES SHOWN ARE ESTIMATES ONLY. THE CONTRACTOR IS RESPONSIBLE FOR CALCULATING/ESTIMATING THEIR OWN QUANTITIES.

LEGEND

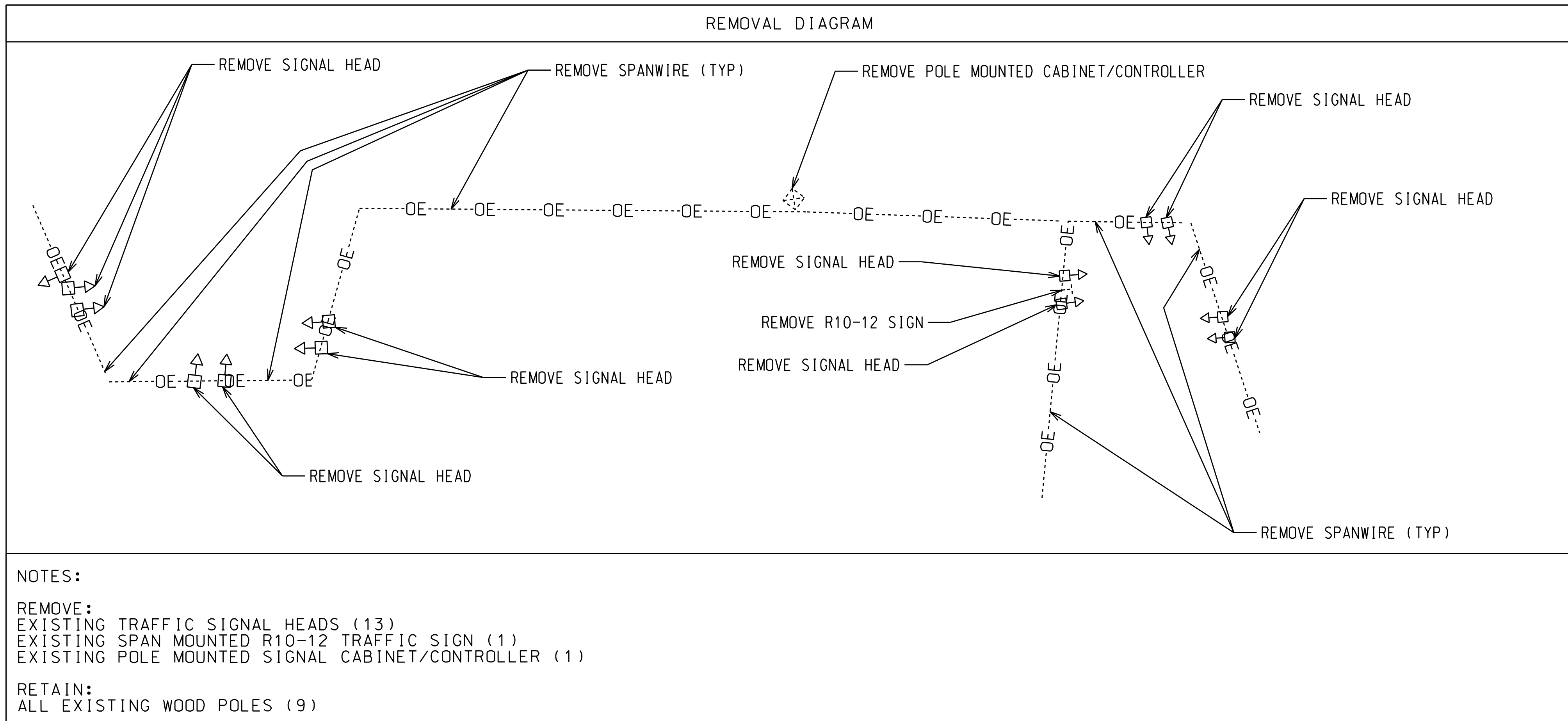
A - STD. DWG. NO. T.S.D.-730-1
B - STD. DWG. NO. T.S.D.-730-2
C - STD. DWG. NO. T.S.D.-730-2A
D - STD. DWG. NO. T.S.D.-730-6
E - STD. DWG. NO. T.S.D.-730-10
F - STD. DWG. NO. T.S.D.-730-13
G - STD. DWG. NO. T.S.D.-730-17
H - STD. DWG. NO. T.S.D.-730-18

CONDUIT AND CONDUCTOR SCHEDULE				
CONDUIT	TYPE	CONDUCTOR	FROM	TO
3 - 2"	PVC		CONTROLLER	JUNCTION BOX #1
3 - 2"	PVC		JUNCTION BOX #1	SIGNAL POLE
1-1"	RGS	3#6-1c 120V	DISCONNECT SWITCH	CONTROLLER
-	-	RADAR POWER/DATA CABLE	CONTROLLER	EACH DETECTOR UNIT
-	-	#14-10c IMSA 20-1	CONTROLLER	SIGNAL HEAD 1,5
-	-	#14-4c IMSA 20-1	SIGNAL HEAD 1	SIGNAL HEAD 6
-	-	#14-4c IMSA 20-1	SIGNAL HEAD 5	SIGNAL HEAD 2
-	-	#14-7c IMSA 20-1	CONTROLLER	SIGNAL HEAD 3A,4A
-	-	#14-4c IMSA 20-1	SIGNAL HEAD 3A	SIGNAL HEAD 3
-	-	#14-4c IMSA 20-1	SIGNAL HEAD 4A	SIGNAL HEAD 4
-	-	#14-4c IMSA 20-1	CONTROLLER	SIGNAL HEAD B
-	-	#14-4c IMSA 20-1	CONTROLLER	SIGNAL HEAD A

SUGGESTED CONTROLLER TIMINGS

[illegible]

REMOVAL DIAGRAM

[illegible]

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**SAIN
ASSOCIATES**



DRAWING NAME

TRAFFIC SIGNAL DETAILS

1ST AVE W @ THOMPSON RD

FOR

CITY OF ALABASTER
ALABASTER, ALABAMA

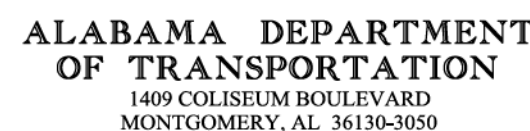
DRN. BY JDG	JOB NO. 23-0362
CKD. BY CC	SCALE NOT TO SCALE
PROJ. MGR. JDS	DATE 7-8-2024

SHEET NO.

TS4

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REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO



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REVISIONS

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DRAWN BY: _____
DATE DRAWN: _____
REVISED DATE: 6-12-2019

DESIGN BUREAU SPECIAL DRAWING

STANDARD DETAILS
FOR TRAFFIC CONTROL PLANS

NOT TO SCALE

--SPECIFICATIONS--
CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION

SPECIAL DRAWING NO.

INDEX NO

FOR REVIEW
NOT FOR CONSTRUCTION

DRAWING NAME

NG NAME
TRAFFIC CONTROL PLAN

1ST AVE W @ THOMPSON RD

FOR
CITY OF ALABASTER
ALABASTER, ALABAMA

DRN. BY JDG	JOB NO. 23-0362
CKD. BY CC	SCALE NOT TO SCALE
PROJ. MGR. JDS	DATE 7-8-2024

SHEET NO.

TS6

[illegible]

Two Perimeter Park South
Suite 500 East
Birmingham, Alabama 35243
Phone: (205) 940-6420
Website: www.sain.com

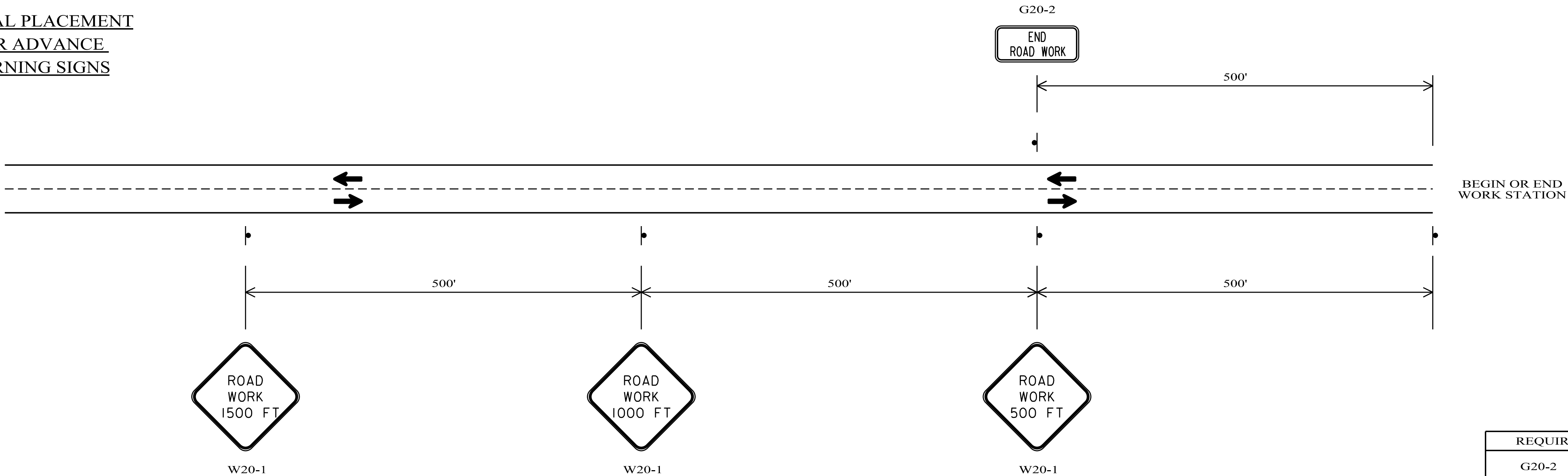
**SAIN
ASSOCIATES**



TEMPORARY TRAFFIC CONTROL PLAN SHEET

REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO
-	-	-

TYPICAL PLACEMENT
FOR ADVANCE
WARNING SIGNS

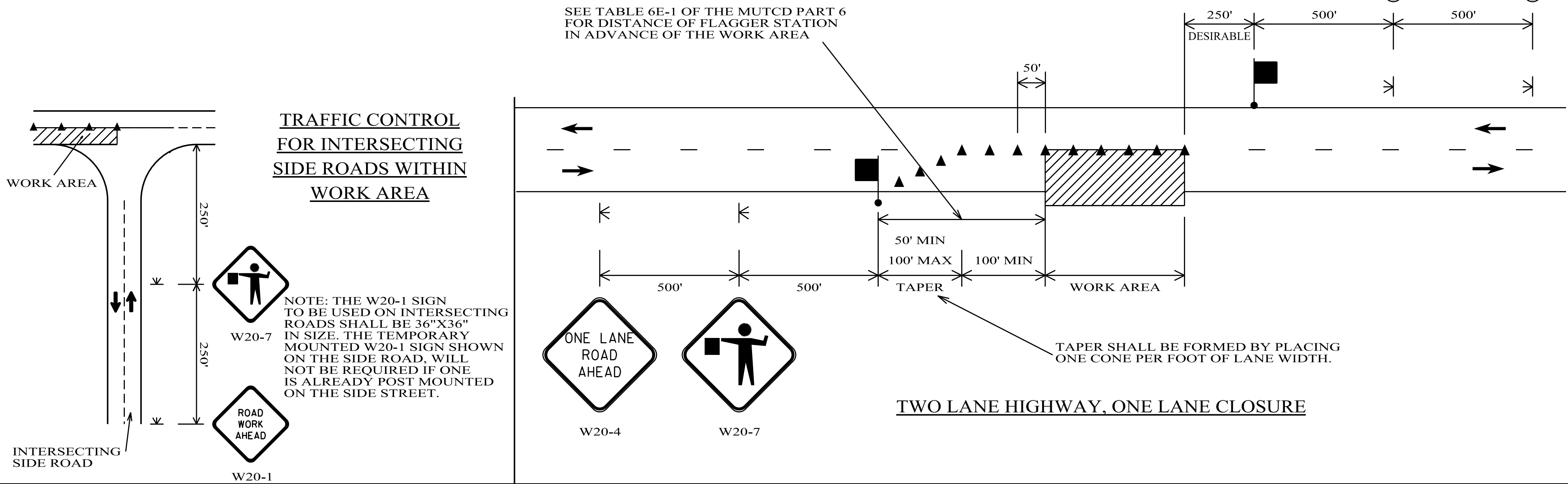


REQUIRED SIGN SIZES	
G20-2	48" X 24"
W20-1	48" X 48"
W20-4	48" X 48"
W20-7	48" X 48"

GENERAL NOTES

- ALL ADVANCE WARNING SIGNS REQUIRED AT THE BEGINNING AND END OF PROJECT SHALL BE POST MOUNTED. ALL SIGNS REQUIRED FOR THE LANE CLOSURE SHALL BE TEMPORARY MOUNTED.
- SIGN (XX) SHALL SHOW THE SPEED IN THE WORK ZONE. SIGN (YY) SHALL BE USED IF SPEED REDUCTION REQUIRES TWO SIGNS. SIGN(ZZ) SHALL SHOW THE POSTED SPEED OUTSIDE THE WORK ZONE.

THE SIGN SIZES SHOWN ON THIS SHEET SHALL SUPERCEDE THOSE SHOWN ON THE STANDARD HIGHWAY SIGNS DRAWINGS UNLESS OTHERWISE APPROVED BY THE ENGINEER.



LEGEND

- TEMPORARY MOUNTED SIGN
- POST MOUNTED SIGN
- CONES
- FLAGGER
- WORK AREA

TWO LANE HIGHWAY, ONE LANE CLOSURE



ALABAMA DEPARTMENT
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MONTGOMERY, AL 36130-3050

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REVISIONS:

Bureau Std Engr: D.J.W.
DRAWN BY: _____
DATE DRAWN: _____
REVISED DATE: 3-24-2021

DESIGN BUREAU SPECIAL DRAWING
DETAILS FOR TRAFFIC CONTROL
FOR TWO LANE HIGHWAYS

NOT TO SCALE

--SPECIFICATIONS--
CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION

SPECIAL DRAWING NO

INDEX NO

SPECIAL PROJECT DETAIL

2002

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DRAWING NAME

TRAFFIC CONTROL PLAN

1ST AVE W @ THOMPSON RD

FOR
CITY OF ALABASTER
ALABASTER, ALABAMA

DRN. BY JDS	JOB NO. 23-0362
CKD. BY CC	SCALE NOT TO SCALE
PROJ. MGR. JDS	DATE 7-8-2024

SHEET NO.

TS7

\$DATE\$

\$TIME\$

\$USERNAME\$

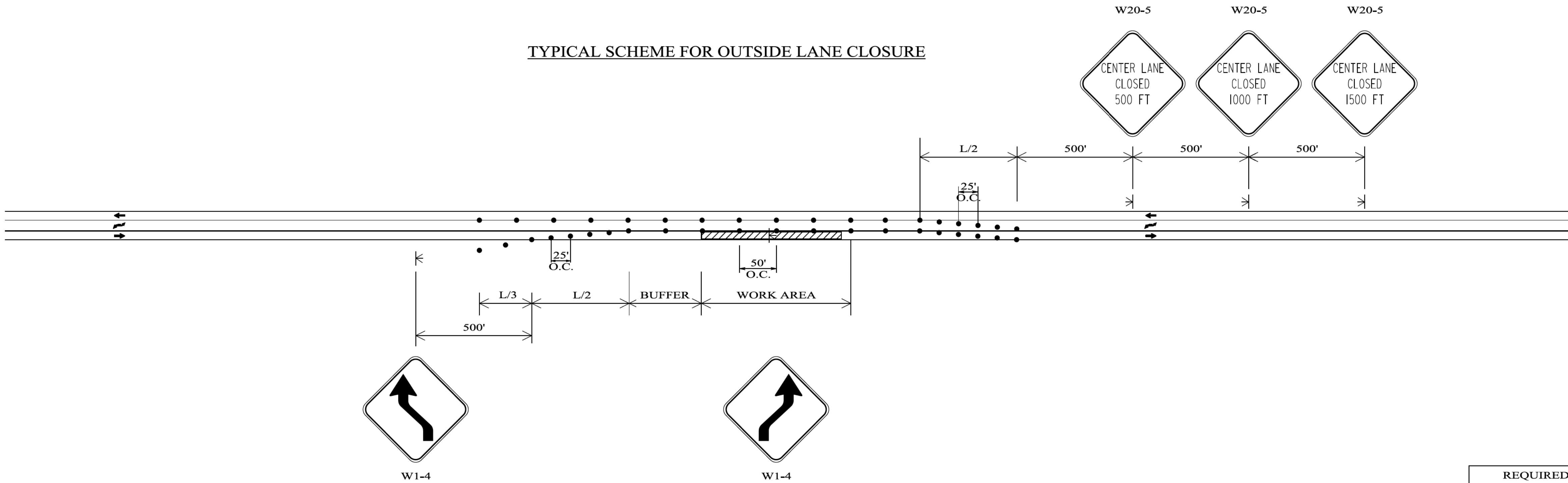
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\$FILE\$

TEMPORARY TRAFFIC CONTROL PLAN SHEET

REFERENCE PROJECT NO	FISCAL YEAR	SHEET NO

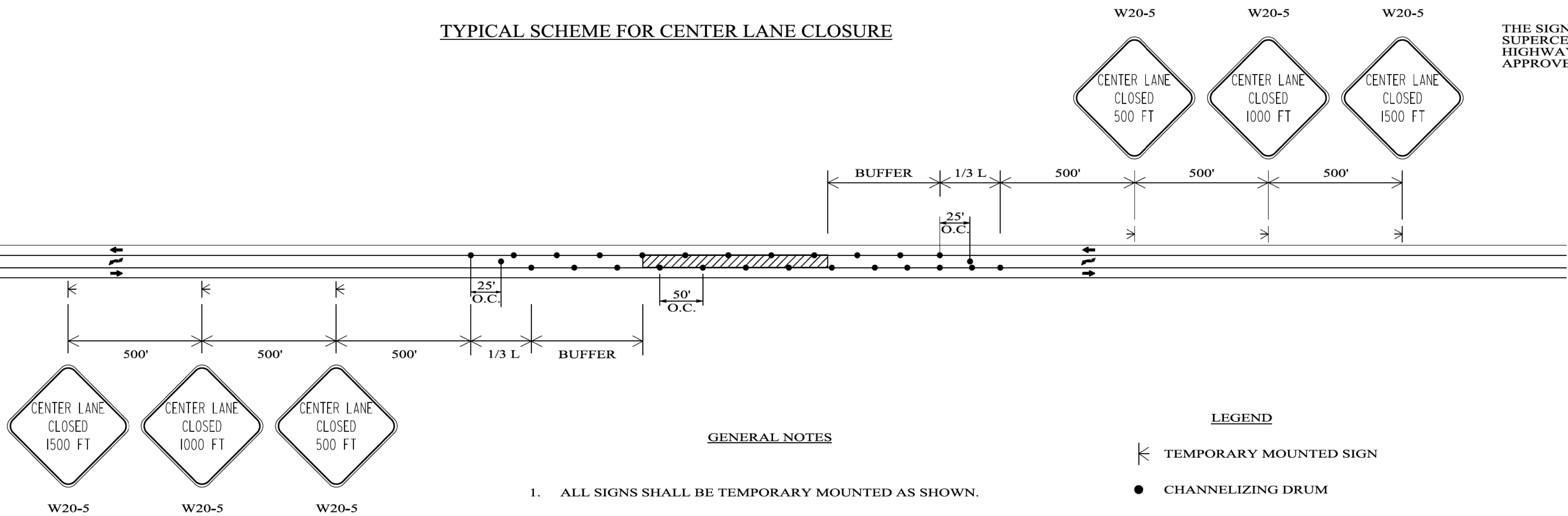
TYPICAL SCHEME FOR OUTSIDE LANE CLOSURE



REQUIRED SIGN SIZES	
W1-4	48" X 48"
W20-5	48" X 48"

THE SIGN SIZES SHOWN ON THIS SHEET SHALL SUPERCEDE THOSE SHOWN ON THE STANDARD HIGHWAY SIGNS DRAWINGS UNLESS OTHERWISE APPROVED BY THE ENGINEER.

TYPICAL SCHEME FOR CENTER LANE CLOSURE



GENERAL NOTES

- ALL SIGNS SHALL BE TEMPORARY MOUNTED AS SHOWN.

LEGEND

- TEMPORARY MOUNTED SIGN
- CHANNELIZING DRUM
- WORK AREA



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REVISIONS:

1.

Bureau Std Engr: D.J.W.
DRAWN BY:
DATE DRAWN:
REVISED DATE: 6-12-2019

DESIGN BUREAU SPECIAL DRAWING

DETAILS FOR TRAFFIC CONTROL FOR
3-LANE HIGHWAY LANE CLOSURES

NOT TO SCALE

--SPECIFICATIONS--
CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION

SPECIAL DRAWING NO

SPECIAL PROJECT DETAIL

INDEX NO

2021

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DRAWING NAME
TRAFFIC CONTROL PLAN

1ST AVE W @ THOMPSON RD

FOR
CITY OF ALABASTER
ALABASTER, ALABAMA

DRN. BY JDG	JOB NO. 23-0362
CKD. BY CC	SCALE NOT TO SCALE
PROJ. MGR. JDS	DATE 7-8-2024

SHEET NO.

TS8